ITEM NO. 07A IDOT LETTING: AUGUST 03, 2018

CO064
TOTAL SHEETS = 52

CONSTRUCTION PLANS

RECONSTRUCT TAXIWAY B

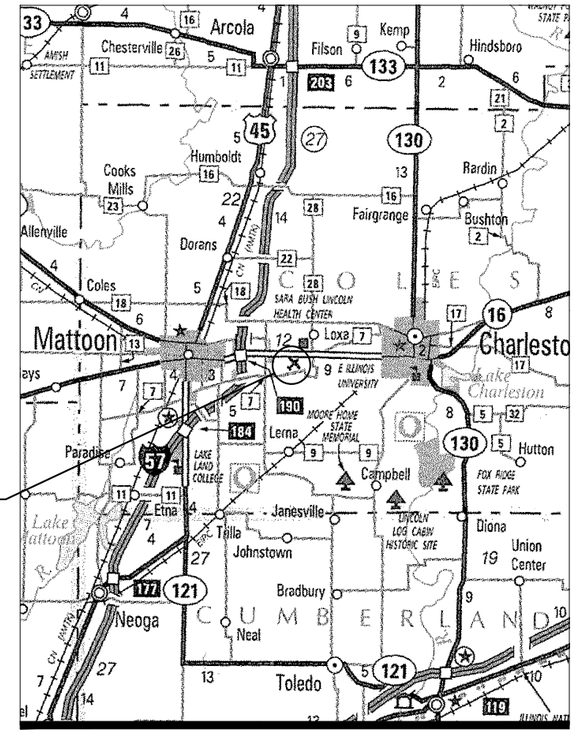
COLES COUNTY MEMORIAL AIRPORT (MTO)
MATTOON-CHARLESTON, COLES COUNTY, ILLINOIS

ILLINOIS PROJECT NO. MTO-4678
SBG PROJECT NO. 3-17-SBGP-TBD

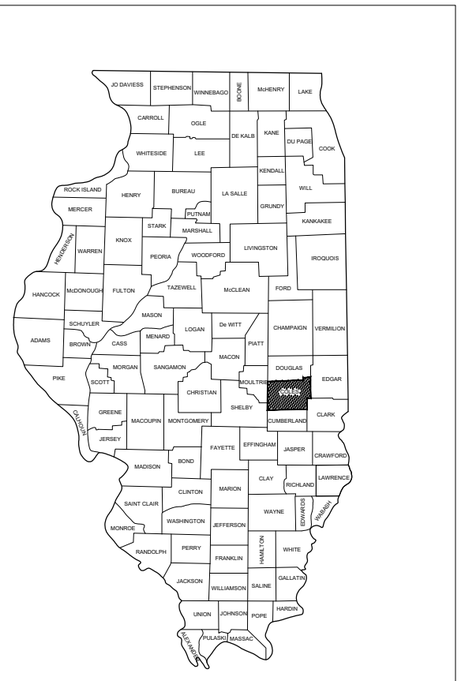
JUNE 15, 2018

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.



VICINITY MAP



LOCATION MAP

No.	Issue/Description	Sheets Changed	Date	By

COVERING
ELECTRICAL DESIGN

KEVIN N. LIGHTFOOT
062-047643
LICENSED PROFESSIONAL ENGINEER
STATE OF ILLINOIS

Kevin N. Lightfoot, P.E. Lic. Exp. 11/30/2019
Electrical Engineer

June 15, 2018
Date

HANSON

HANSON PROFESSIONAL SERVICES INC.
1525 S. Sixth St.
Springfield, Illinois 62703
Telephone: 217.788.2450
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KYLE B. SCHWEIZER
062-064386
LICENSED PROFESSIONAL ENGINEER
STATE OF ILLINOIS

Kyle Schweizer, P.E. Lic. Exp. 11/30/2019
Project Engineer

June 15, 2018
Date

COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

COLES COUNTY MEMORIAL AIRPORT
COLES COUNTY AIRPORT AUHORITY
432 Airport Road
Mattoon, Illinois 61938
Telephone: 217.234.7120
Fax: 217.234.7116

Andrew J. Fearn
Airport Manager

6-20-18
Date

I:\18\OBS\18A0003\0\18A0003\CAD\AIRPORT\SHEETS\C-001-CVR.DWG

INDEX TO SHEETS

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR125943	ADJUST INPAVEMENT LIGHT	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	8,067	
AR156510	SILT FENCE	L.F.	264	
AR156530	TEMPORARY SEEDING	ACRE	1.7	
AR401614	BIT. SURF. CSE.-METHOD II, SUPERPAVE	TON	3,300	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	1,885	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	35	
AR403614	BIT. BASE CSE.-METHOD II, SUPERPAVE	TON	3,300	
AR403630	BITUMINOUS BASE TEST SECTION	EACH	1	
AR501120	RUBBLIZE PAVEMENT	S.Y.	27,924	
AR501550	PCC PAVEMENT MILLING	S.Y.	27,924	
AR603510	BITUMINOUS TACK COAT	GAL.	2,800	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	9,547	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	9,680	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	7,239	
AR705546	6" NON PERFORATED UNDERDRAIN	L.F.	194	
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	5	
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	6	
AR705640	UNDERDRAIN CLEANOUT	EACH	8	
AR705900	REMOVE UNDERDRAIN	L.F.	1,613	
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	13	
AR901510	SEEDING	ACRE	1.7	
AR908510	MULCHING	ACRE	1.7	

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY_____COLES
CITY_____MATTOON
TOWNSHIP_____LAFAYETTE
SECTION NO._____14,15,22,23
ADDRESS_____COLES COUNTY MEMORIAL AIRPORT
432 AIRPORT ROAD
MATTTON, ILLINOIS 61938

GENERAL NOTES

1. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.

2. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.

3. ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.

4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.

5. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.

6. THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

7. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.

9. THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.

10. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.

11. NPDES PERMIT – THIS PROJECT WILL DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED.

12. MATERIAL CERTIFICATIONS – MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.

13. CERTIFIED PAYROLLS – THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

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#184-001084

COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV
	5/2018			

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-002-SOQ.DWG
DESIGN BY: JAP 5/2018
DRAWN BY: JAP 5/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

SUMMARY OF QUANTITIES, INDEX TO SHEETS, AND GENERAL NOTES

CONSTRUCTION SAFETY PLAN

1. SCOPE OF WORK – THE PROPOSED WORK CONSISTS OF RECONSTRUCTING THE WEST HALF OF TAXIWAY "B" BY CONCRETE RUBBLIZATION AND BITUMINOUS RESURFACING. ASSOCIATED WORK ITEMS INCLUDE PCC RUBBLIZATION, PAVEMENT MILLING AND REMOVAL, BITUMINOUS PAVING, PAVEMENT MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING.

2. GENERAL – THE COLES COUNTY MEMORIAL AIRPORT IS NON–TOWER CONTROLLED, PART 139, GENERAL AVIATION AIRPORT COMPRISED OF TWO PAVED RUNWAYS AND ONE SOD SHORT TAKEOFF AND LANDING (STOL) RUNWAY. THE PROPOSED CONSTRUCTION WILL NECESSITATE THE CLOSING OF THE WEST HALF OF TAXIWAY "B" AND NORTH PORTION OF TAXIWAY "C" FOR THE PROJECT DURATION, AND THE PERIODIC CLOSURES OF RUNWAY 11–29. RUNWAY 6–24 SHALL REMAIN OPEN AT ALL TIMES.

4. AIRFIELD SAFETY ASSURANCE – AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON–COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR CURRENT ADDITION WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION FROM ENTERING THE AIR OPERATIONS AREA ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.

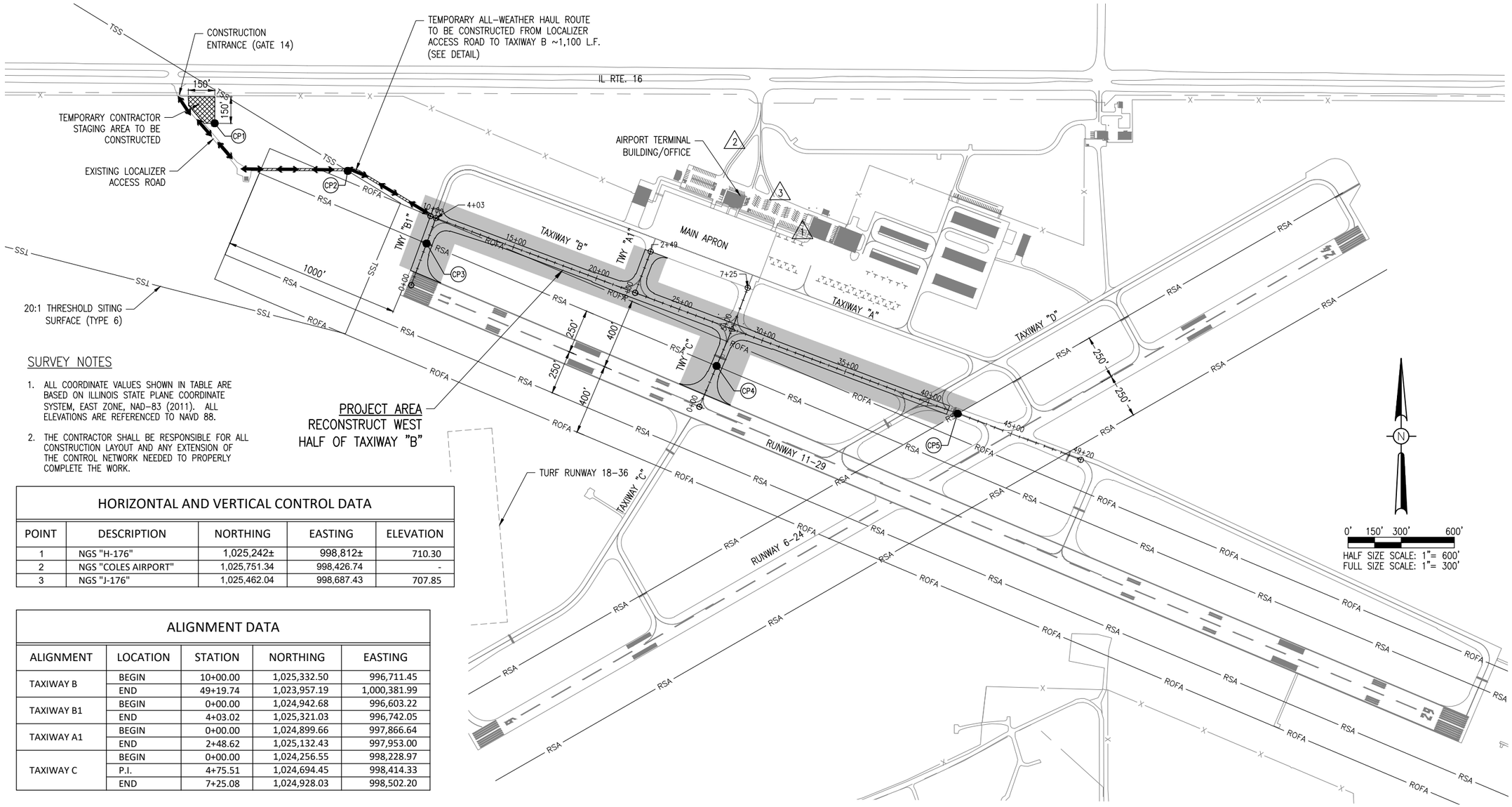
5. AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES SHALL BE ALLOWED WITHIN THE PROJECT LIMITS. GATES SHALL BE CLOSED AT ALL TIMES UNLESS THE CONTRACTOR IS IN A CONTINUOUS HAULING OPERATIONS, DURING WHICH TIME HE WILL PROVIDE A PERSON TO MONITOR THE GATE AREA.

6. RADIO CONTROL – THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO–WAY RADIO CONTACT WITH THE AIRPORT UNICOM (122.70 MHz) ANY TIME THERE ARE WORKERS OR EQUIPMENT ON THE AIRFIELD.

7. THE TEMPORARY HAUL ROUTE SHALL BE USED ONLY FOR THE TRANSPORTING OF WORKERS, EQUIPMENT, AND MATERIALS. NO STATIONARY EQUIPMENT OR STOCKPILES MAY BE PLACED ON OR ALONG THE ROUTE. ALL EQUIPMENT MUST BE IN A LOWERED POSITION DURING TRANSPORT.

CRITICAL POINTS

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)
1	CONST. EQUIP.	N039° 29' 01.437"	W088° 17' 36.592"	710.0	25'
2	CONST. EQUIP.	N039° 28' 58.779"	W088° 17' 27.016"	708.0	15'
3	CONST. EQUIP.	N039° 28' 54.727"	W088° 17' 21.346"	713.6	25'
4	CONST. EQUIP.	N039° 28' 47.891"	W088° 17' 00.474"	712.4	25'
5	CONST. EQUIP.	N039° 28' 45.226"	W088° 16' 43.129"	714.8	25'



SURVEY NOTES

1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD–83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

HORIZONTAL AND VERTICAL CONTROL DATA

POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	NGS "H-176"	1,025,242±	998,812±	710.30
2	NGS "COLES AIRPORT"	1,025,751.34	998,426.74	-
3	NGS "J-176"	1,025,462.04	998,687.43	707.85

ALIGNMENT DATA

ALIGNMENT	LOCATION	STATION	NORTHING	EASTING
TAXIWAY B	BEGIN	10+00.00	1,025,332.50	996,711.45
	END	49+19.74	1,023,957.19	1,000,381.99
TAXIWAY B1	BEGIN	0+00.00	1,024,942.68	996,603.22
	END	4+03.02	1,025,321.03	996,742.05
TAXIWAY A1	BEGIN	0+00.00	1,024,899.66	997,866.64
	END	2+48.62	1,025,132.43	997,953.00
TAXIWAY C	BEGIN	0+00.00	1,024,256.55	998,228.97
	P.I.	4+75.51	1,024,694.45	998,414.33
	END	7+25.08	1,024,928.03	998,502.20

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678

SBG Project No: 3-17-SBGP-TBD

Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-101-SOW.DWG
DESIGN BY: KBS
DRAWN BY: JAP
REVIEWED BY: RAW 06/15/2018

SHEET TITLE

SCOPE OF WORK AND SAFETY PLAN

JUN 20, 2018 1:12 PM SCHWE01296
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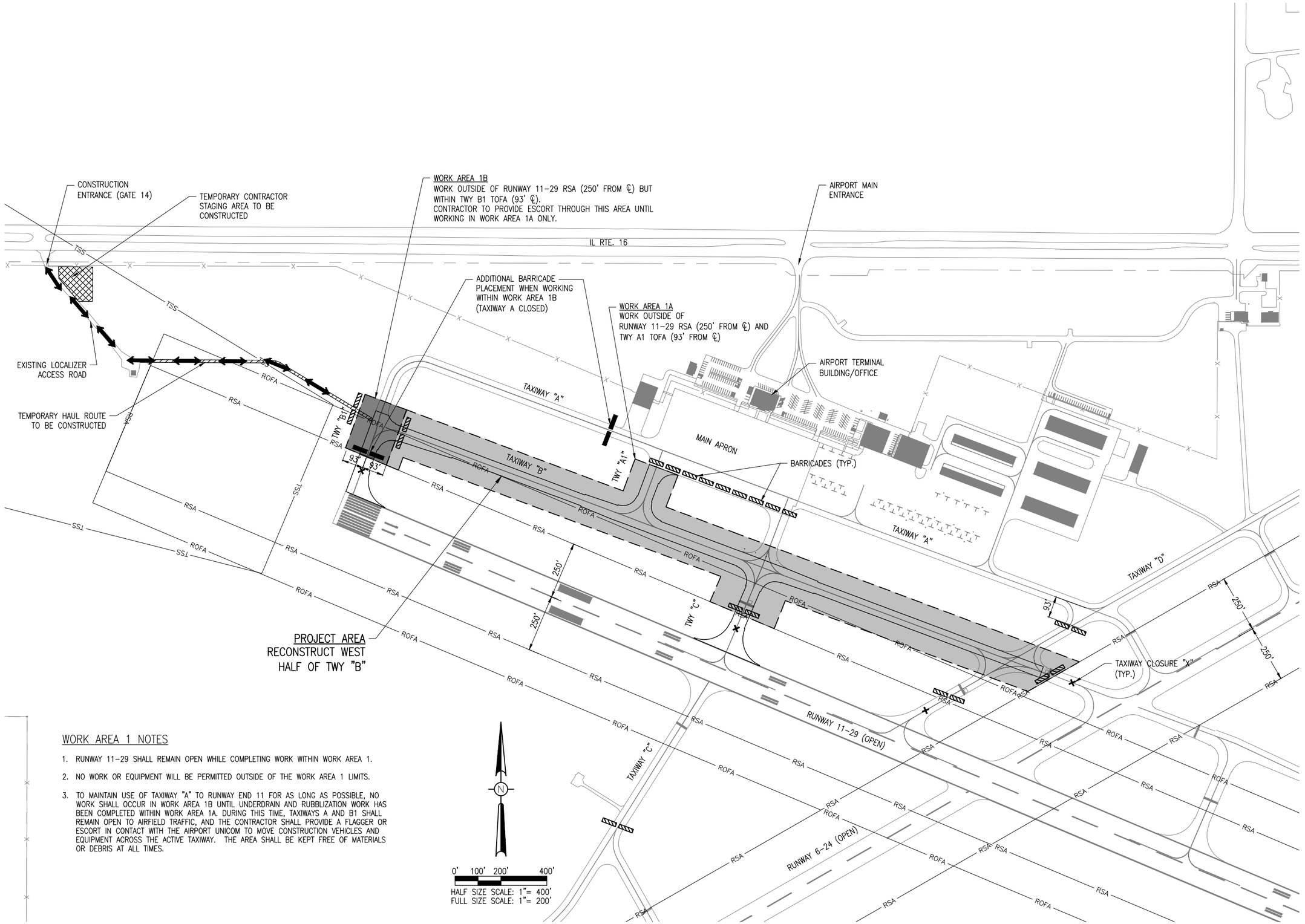

WORK AREA 1 NOTES

1. RUNWAY 11–29 SHALL REMAIN OPEN WHILE COMPLETING WORK WITHIN WORK AREA 1.
2. NO WORK OR EQUIPMENT WILL BE PERMITTED OUTSIDE OF THE WORK AREA 1 LIMITS.
3. TO MAINTAIN USE OF TAXIWAY "A" TO RUNWAY END 11 FOR AS LONG AS POSSIBLE, NO WORK SHALL OCCUR IN WORK AREA 1B UNTIL UNDERDRAIN AND RUBBLIZATION WORK HAS BEEN COMPLETED WITHIN WORK AREA 1A. DURING THIS TIME, TAXIWAYS A AND B1 SHALL REMAIN OPEN TO AIRFIELD TRAFFIC, AND THE CONTRACTOR SHALL PROVIDE A FLAGGER OR ESCORT IN CONTACT WITH THE AIRPORT UNICOM TO MOVE CONSTRUCTION VEHICLES AND EQUIPMENT ACROSS THE ACTIVE TAXIWAY. THE AREA SHALL BE KEPT FREE OF MATERIALS OR DEBRIS AT ALL TIMES.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV
	05/2018			

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
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DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

CONSTRUCTION SAFETY PLAN - WORK AREA 1

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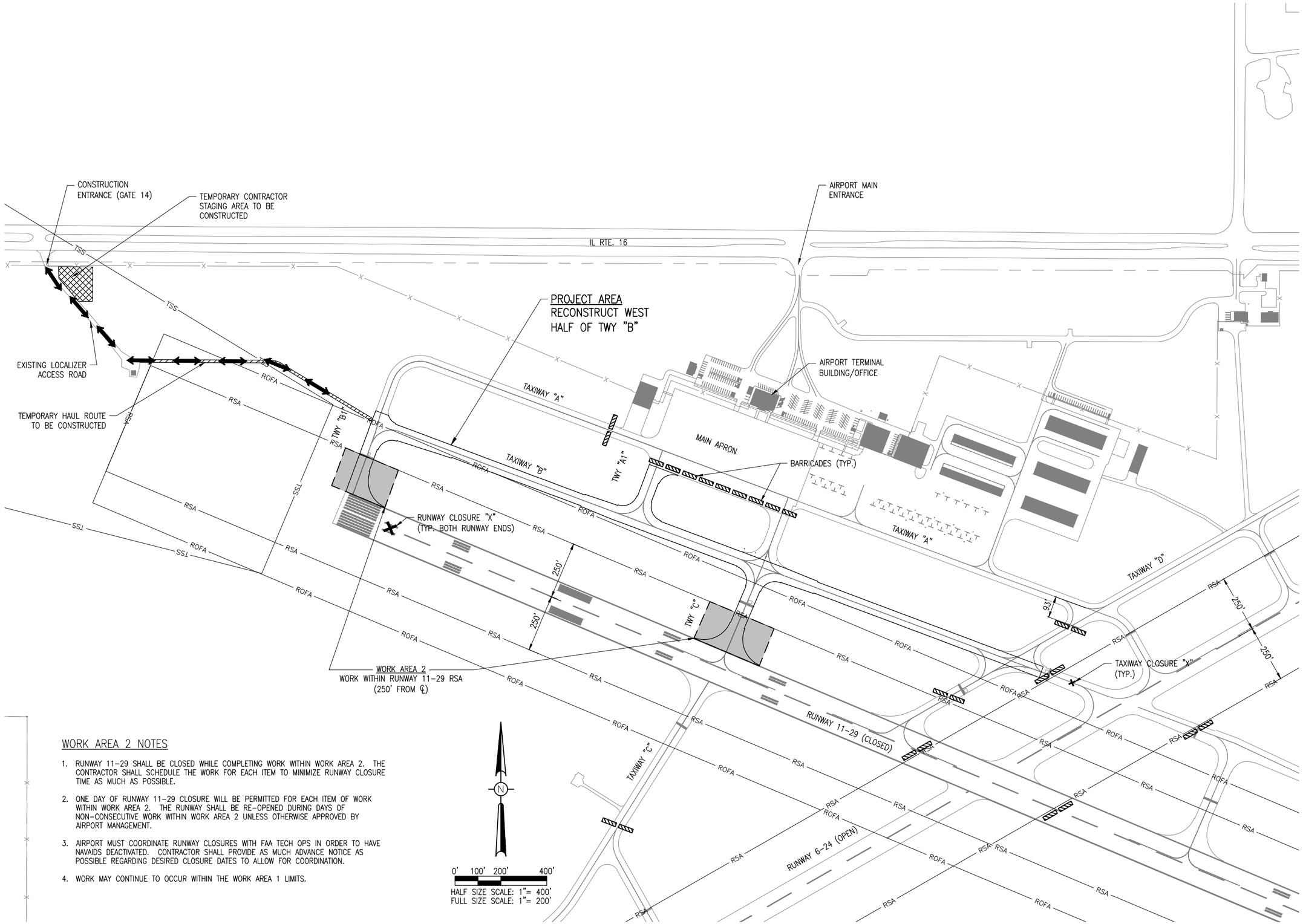

WORK AREA 2 NOTES

1. RUNWAY 11-29 SHALL BE CLOSED WHILE COMPLETING WORK WITHIN WORK AREA 2. THE CONTRACTOR SHALL SCHEDULE THE WORK FOR EACH ITEM TO MINIMIZE RUNWAY CLOSURE TIME AS MUCH AS POSSIBLE.
2. ONE DAY OF RUNWAY 11-29 CLOSURE WILL BE PERMITTED FOR EACH ITEM OF WORK WITHIN WORK AREA 2. THE RUNWAY SHALL BE RE-OPENED DURING DAYS OF NON-CONSECUTIVE WORK WITHIN WORK AREA 2 UNLESS OTHERWISE APPROVED BY AIRPORT MANAGEMENT.
3. AIRPORT MUST COORDINATE RUNWAY CLOSURES WITH FAA TECH OPS IN ORDER TO HAVE NAVAIDS DEACTIVATED. CONTRACTOR SHALL PROVIDE AS MUCH ADVANCE NOTICE AS POSSIBLE REGARDING DESIRED CLOSURE DATES TO ALLOW FOR COORDINATION.
4. WORK MAY CONTINUE TO OCCUR WITHIN THE WORK AREA 1 LIMITS.

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COLES COUNTY AIRPORT AUTHORITY
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ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
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DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

CONSTRUCTION SAFETY PLAN - WORK AREA 2

JUN 22, 2018 4:41 PM SCHWE01286
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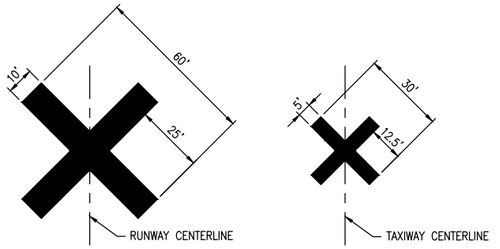


1. TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"

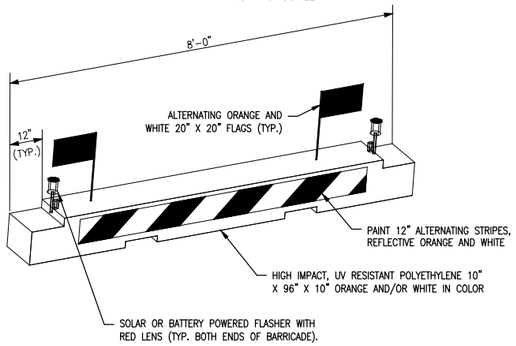
2. TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.

3. TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.

7. COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.

TEMPORARY CLOSURE CROSS DETAIL

NOT TO SCALE



LOW PROFILE AIRCRAFT BARRICADE DETAIL

BARRICADE NOTES

1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.

2. BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT WITH A MAXIMUM SPACING OF 10' BETWEEN ENDS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.

3. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.

4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.

5. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.

6. THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.

7. COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.

SAFETY NOTES

1. ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.

2. THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.

3. NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.

4. PRIOR TO ACCESSING THE AIRFIELD, ANY DESIGNATED CONTRACTOR OR SUBCONTRACTOR EMPLOYEES WHO WILL BE OPERATING OR ESCORTING A VEHICLE ON AN ACTIVE AREA OF THE AIRFIELD MUST ATTEND A 1 HOUR AIRFIELD SAFETY TRAINING AND ORIENTATION PROVIDED BY THE AIRPORT. PRIOR TO THE TRAINING, THE EMPLOYEES MUST BE FAMILIAR WITH THE "FAA GUIDE TO GROUND VEHICLE OPERATIONS", AND KEEP A HARD COPY IN THE VEHICLE FOR REFERENCE. THE GUIDE CAN BE FOUND AT:
https://www.faa.gov/airports/runway_safety/media/Ground_Vehicle_Guide_Proof_Final.pdf

5. CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.

6. ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.

7. NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.

8. CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.

9. NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.

10. OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.

11. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.

12. NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.

13. SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.

14. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.

15. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.

16. CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.

17. CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.

18. THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.

19. CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.

20. CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.

21. CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.

22. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.

23. UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.

24. THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO PROCEED".

25. ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.

26. RUNWAY/TAXIWAY CLOSURE PROCEDURES:
    - CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME..
    - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
    - PLACEMENT OF CROSSES AND BARRICADES.
    - ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

    RUNWAY/TAXIWAY RE-OPENING PROCEDURES:
    - ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
    - INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
    - CONTACT AIRPORT MANAGEMENT OR REPRESENTATIVE FOR FINAL INSPECTION OF THE AREA.
    - REMOVE BARRICADES AND CROSSES.
    - ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678

SBG Project No:
3-17-SBGP-TBD

Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-102-SFY.DWG
DESIGN BY: JAP 05/2018
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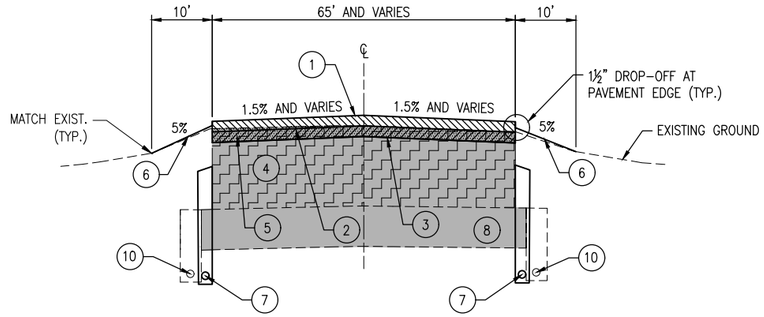
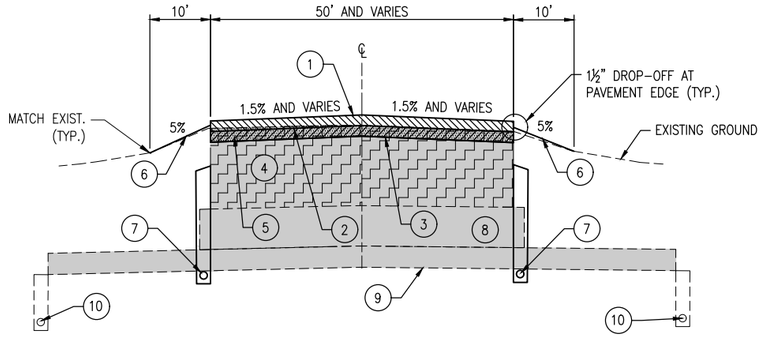
SHEET TITLE

SAFETY NOTES AND DETAILS



TYPICAL SECTION "B–B" – TAXIWAYS B1, A1, & C (SOUTH OF TAXIWAY B)

NOT TO SCALE



TYPICAL SECTION "A–A" – TAXIWAY B

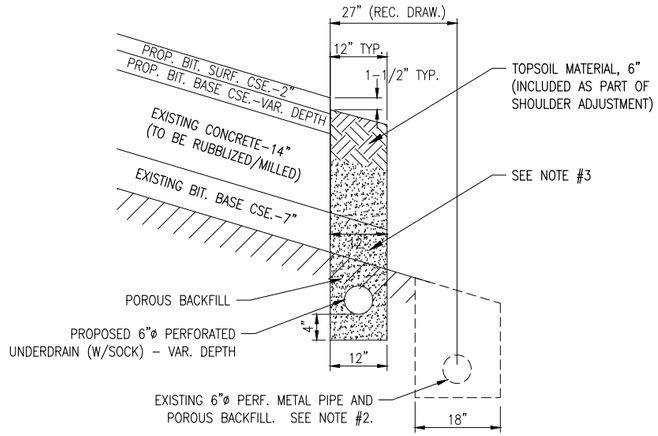
NOT TO SCALE

LEGEND FOR TYPICAL SECTIONS A–A AND B–B

1. PROPOSED BITUMINOUS SURFACE COURSE – 2" DEPTH (ITEM 401)
2. PROPOSED BITUMINOUS TACK COAT APPLIED AT 0.05–0.15 GAL./S.Y. (ITEM 603)
3. PROPOSED BITUMINOUS BASE COURSE – 2" DEPTH (ITEM 403)
4. PROPOSED CONCRETE PAVEMENT RUBBLIZATION – 14" DEPTH (ITEM 501120)
5. PROPOSED VAR.–DETPH CONCRETE PAVEMENT MILLING (FOLLOWING RUBBLIZATION) (ITEM 501550)
6. PROPOSED SHOULDER ADJUSTMENT (ITEM 152), SEEDING (ITEM 901), & HYDRO–MULCH (ITEM 908)
7. PROPOSED UNDERDRAIN – SEE DETAIL (ITEM 705)
8. EXISTING BITUMINOUS BASE, 7"
9. EXISTING ORIGINAL BITUMINOUS RUNWAY PAVEMENT, ~6–8"
10. EXISTING UNDERDRAIN TO BE ABANDONED OR REMOVED (NOT PRESENT IN TAXIWAYS A1 AND C–NORTH OF TWY B)

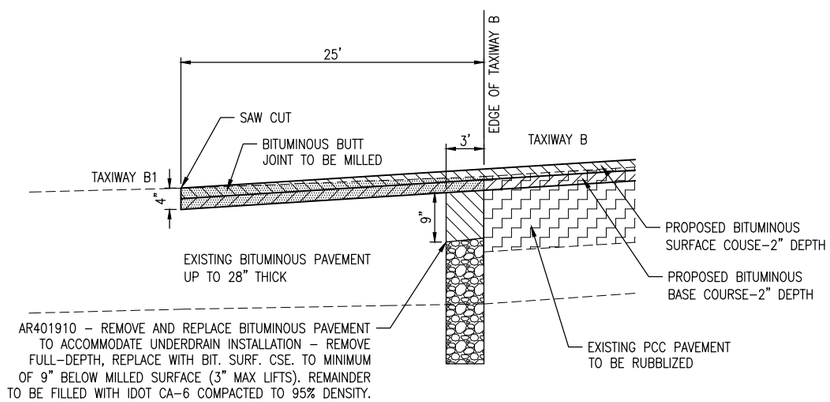
NOTES:

1. MILLING OF CONCRETE PAVEMENT TO GRADE SHALL TAKE PLACE FOLLOWING PAVEMENT RUBBLIZATION.
2. EXISTING UNDERDRAINS SHALL BE REMOVED AND PAID FOR WHERE CALLED OUT IN THE PLANS. THEY SHALL BE PLUGGED AND ABANDONED IN ANY OTHER AREAS WHERE ENCOUNTERED AS AN INCIDENTAL ITEM.
3. REMOVAL OR TRENCHING THROUGH EXISTING EXTENDED 12" WIDE BITUMINOUS BASE, AND THE ORIGINAL BITUMINOUS RUNWAY PAVEMENT UNDER TAXIWAY B SHALL BE INCIDENTAL TO THE PROPOSED UNDERDRAIN INSTALLATION.



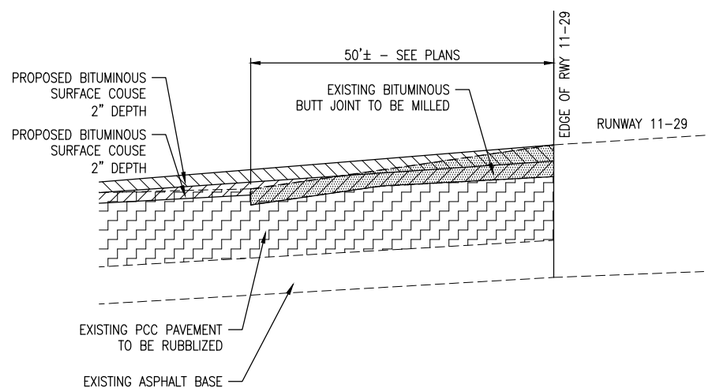
TYPICAL SECTION FOR EDGE DRAINS

NOT TO SCALE



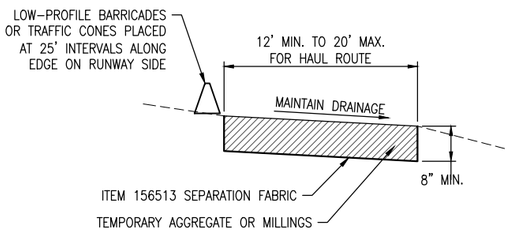
BUTT JOINT DETAIL "A" – TXY B/B1

NOT TO SCALE



BUTT JOINT DETAIL "B"
TXY B1/RWY 11–29 AND TXY C/RWY 11–29

NOT TO SCALE



HAUL ROUTE/STAGING AREA DETAIL

N.T.S.

NOTES:

1. HAUL ROUTE TO BE REMOVED AND RESTORED TO ORIGINAL CONDITION AT COMPLETION OF PROJECT. RESTORATION INCLUDES TILLAGE OF ANY EXISTING FARM GROUND NECESSARY DUE TO COMPACTION.
2. A TEMPORARY CULVERT WILL NEED TO BE INSTALLED OVER A SMALL DITCH LINE APPROXIMATELY 200' WEST OF THE TAXIWAY PAVEMENT EDGE – MIN. DIA. 18". CULVERT AND ANY OTHER GRADING OR DRAINAGE ITEMS NECESSARY TO MAINTAIN DRAINAGE ALONG HAUL ROUTE AND STAGING AREA ARE INCIDENTAL TO HAUL ROUTE ITEM.
3. RESIDENT ENGINEER/TECHNICIAN WILL CONFIRM HAUL ROUTE LIMITS/LOCATION PRIOR TO INSTALLATION. CONTRACTOR MAY INCREASE WIDTH/DEPTH OF HAUL ROUTE AT THEIR EXPENSE IF NECESSARY. MAX SIZE OF STAGING AREA IS 150' X 150'.
4. ALL WORK AND MATERIALS INVOLVING HAUL ROUTE SHALL BE PAID FOR UNDER ITEM AR150540 "HAUL ROUTE" – PER LUMP SUM.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678

SBG Project No: 3-17-SBGP-TBD

Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-301-TYP.DWG
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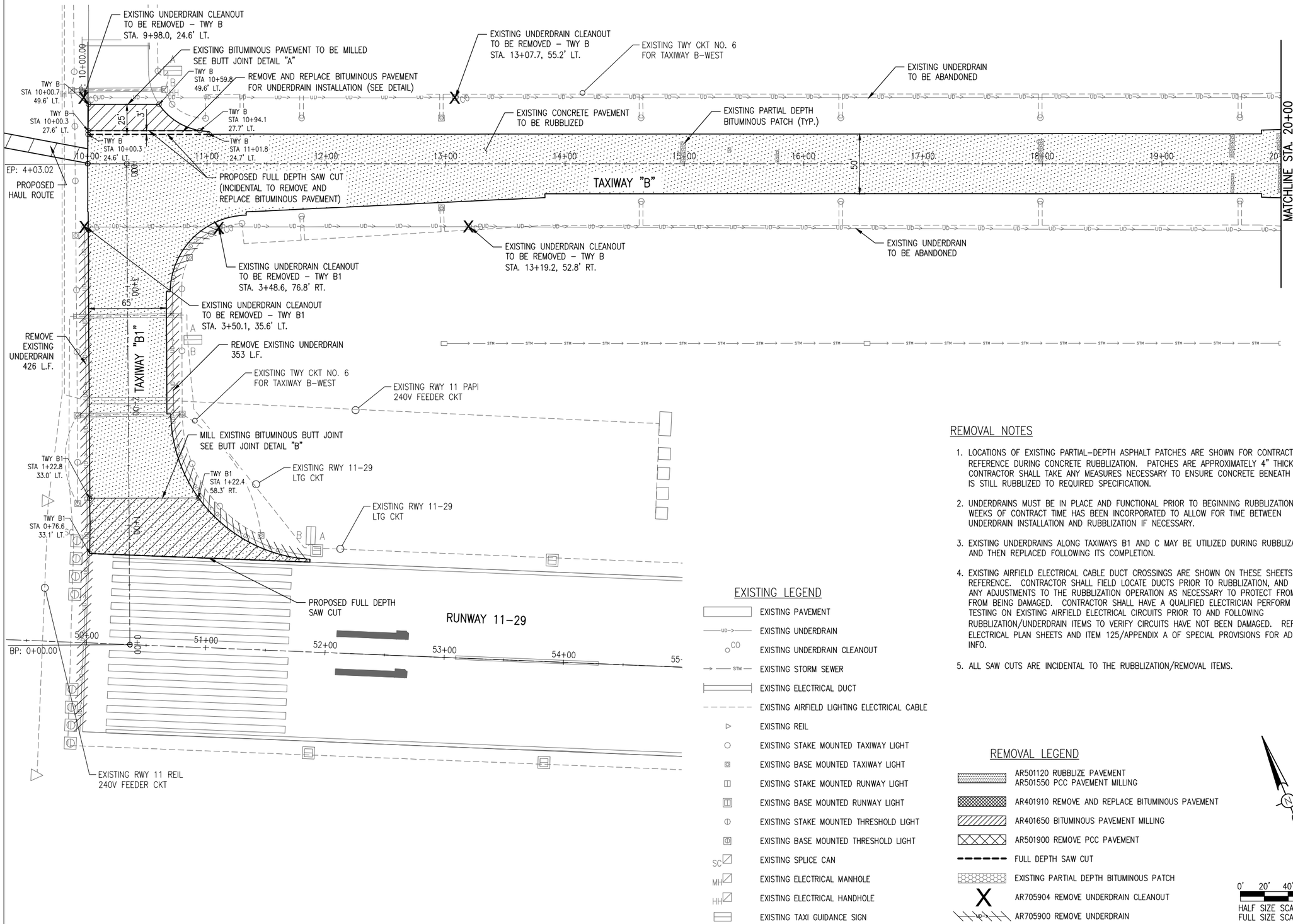
DRAWN BY: CWS 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TYPICAL SECTIONS AND DETAILS



EXISTING LEGEND

- EXISTING PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING AIRFIELD LIGHTING ELECTRICAL CABLE
- EXISTING REIL
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED THRESHOLD LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING SPLICE CAN
- EXISTING ELECTRICAL MANHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING TAXI GUIDANCE SIGN

REMOVAL NOTES

1. LOCATIONS OF EXISTING PARTIAL–DEPTH ASPHALT PATCHES ARE SHOWN FOR CONTRACTORS REFERENCE DURING CONCRETE RUBBLIZATION. PATCHES ARE APPROXIMATELY 4" THICK. CONTRACTOR SHALL TAKE ANY MEASURES NECESSARY TO ENSURE CONCRETE BENEATH PATCHES IS STILL RUBBLIZED TO REQUIRED SPECIFICATION.
2. UNDERDRAINS MUST BE IN PLACE AND FUNCTIONAL PRIOR TO BEGINNING RUBBLIZATION. TWO WEEKS OF CONTRACT TIME HAS BEEN INCORPORATED TO ALLOW FOR TIME BETWEEN UNDERDRAIN INSTALLATION AND RUBBLIZATION IF NECESSARY.
3. EXISTING UNDERDRAINS ALONG TAXIWAYS B1 AND C MAY BE UTILIZED DURING RUBBLIZATION AND THEN REPLACED FOLLOWING ITS COMPLETION.
4. EXISTING AIRFIELD ELECTRICAL CABLE DUCT CROSSINGS ARE SHOWN ON THESE SHEETS FOR REFERENCE. CONTRACTOR SHALL FIELD LOCATE DUCTS PRIOR TO RUBBLIZATION, AND MAKE ANY ADJUSTMENTS TO THE RUBBLIZATION OPERATION AS NECESSARY TO PROTECT FROM THEM FROM BEING DAMAGED. CONTRACTOR SHALL HAVE A QUALIFIED ELECTRICIAN PERFORM MEGGER TESTING ON EXISTING AIRFIELD ELECTRICAL CIRCUITS PRIOR TO AND FOLLOWING RUBBLIZATION/UNDERDRAIN ITEMS TO VERIFY CIRCUITS HAVE NOT BEEN DAMAGED. REFER TO ELECTRICAL PLAN SHEETS AND ITEM 125/APPENDIX A OF SPECIAL PROVISIONS FOR ADDITIONAL INFO.
5. ALL SAW CUTS ARE INCIDENTAL TO THE RUBBLIZATION/REMOVAL ITEMS.

REMOVAL LEGEND

- AR501120 RUBBLIZE PAVEMENT
  AR501550 PCC PAVEMENT MILLING
- AR401910 REMOVE AND REPLACE BITUMINOUS PAVEMENT
- AR401650 BITUMINOUS PAVEMENT MILLING
- AR501900 REMOVE PCC PAVEMENT
- FULL DEPTH SAW CUT
- EXISTING PARTIAL DEPTH BITUMINOUS PATCH
- AR705904 REMOVE UNDERDRAIN CLEANOUT
- AR705900 REMOVE UNDERDRAIN

0' 20' 40' 80'
HALF SIZE SCALE: 1"= 80'
FULL SIZE SCALE: 1"= 40'

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

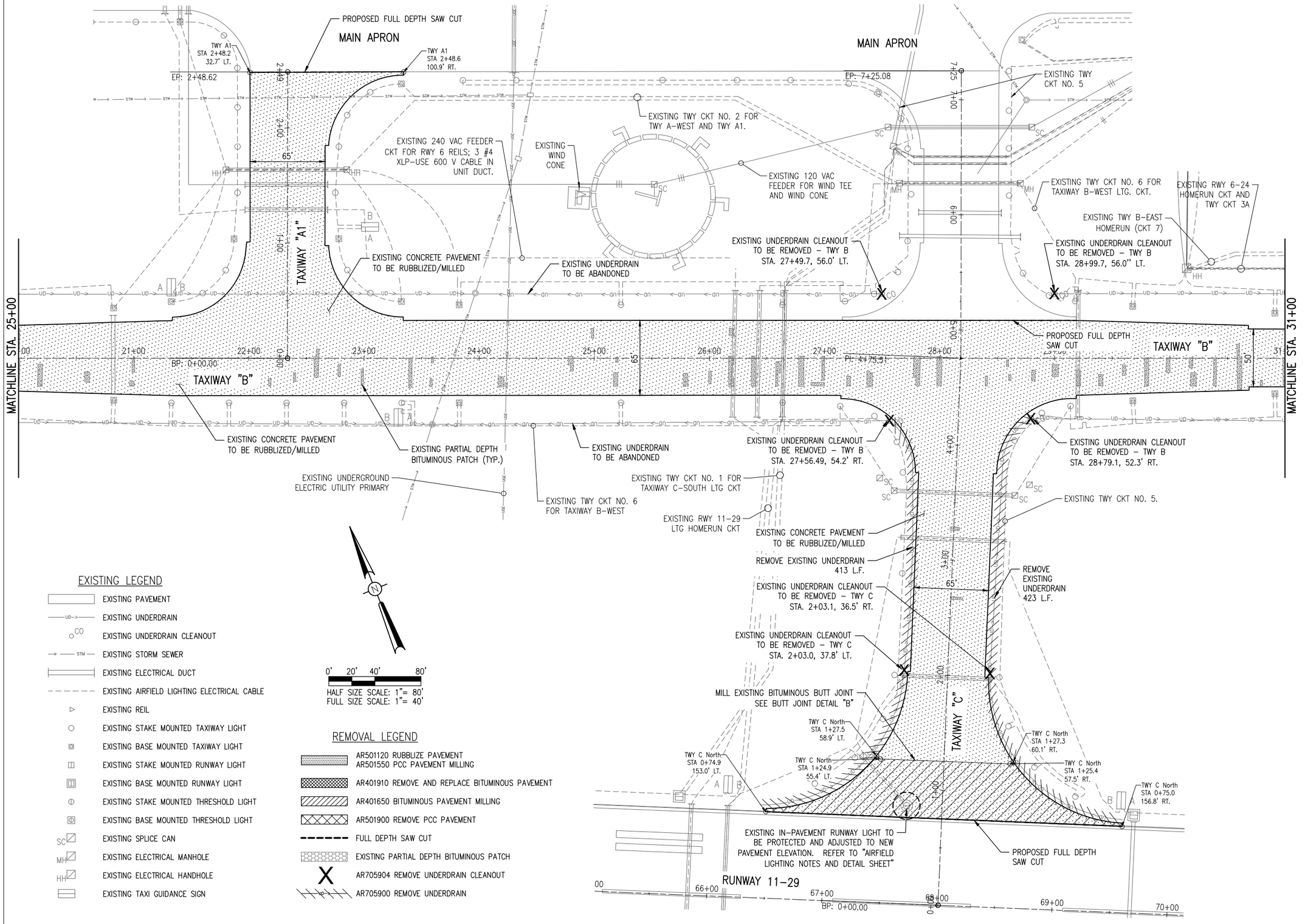
IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-111-REM.DWG
DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

REMOVAL PLAN STA. 10+00 TO 20+00



EXISTING LEGEND

- EXISTING PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING AIRFIELD LIGHTING ELECTRICAL CABLE
- EXISTING REIL
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED THRESHOLD LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING SPLICE CAN
- EXISTING ELECTRICAL MANHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING TAXI GUIDANCE SIGN

REMOVAL LEGEND

- AR501120 RUBBLIZE PAVEMENT
  AR501550 PCC PAVEMENT MILLING
- AR401910 REMOVE AND REPLACE BITUMINOUS PAVEMENT
- AR401650 BITUMINOUS PAVEMENT MILLING
- AR501900 REMOVE PCC PAVEMENT
- FULL DEPTH SAW CUT
- EXISTING PARTIAL DEPTH BITUMINOUS PATCH
- AR705904 REMOVE UNDERDRAIN CLEANOUT
- AR705900 REMOVE UNDERDRAIN

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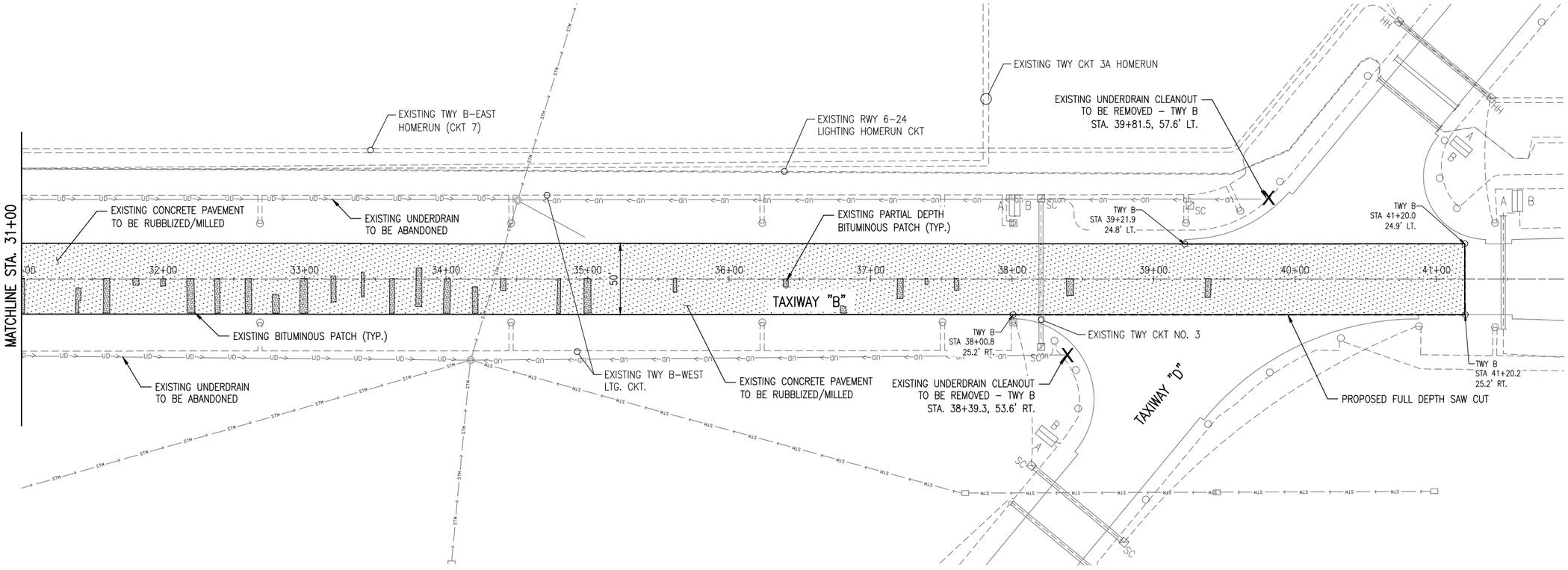
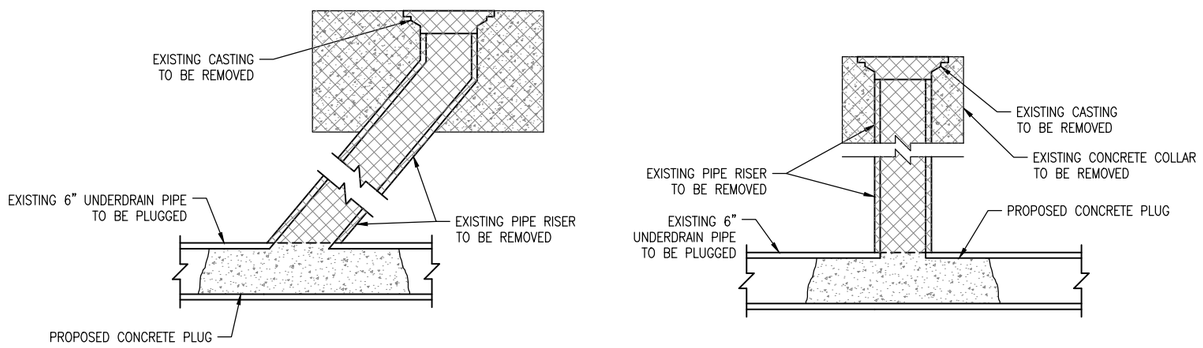
CAD FILE: C-111-REM.DWG
DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

REMOVAL PLAN STA.
20+00 TO 31+00





CLEANOUT REMOVAL DETAILS
NOT TO SCALE

EXISTING LEGEND

- EXISTING PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING AIRFIELD LIGHTING ELECTRICAL CABLE
- EXISTING REIL
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED THRESHOLD LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING SPLICE CAN
- EXISTING ELECTRICAL MANHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING TAXI GUIDANCE SIGN

0' 20' 40' 80'
HALF SIZE SCALE: 1"= 80'
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REMOVAL LEGEND

- AR501120 RUBBLIZE PAVEMENT
  AR501550 PCC PAVEMENT MILLING
- AR401910 REMOVE AND REPLACE BITUMINOUS PAVEMENT
- AR401650 BITUMINOUS PAVEMENT MILLING
- AR501900 REMOVE PCC PAVEMENT
- FULL DEPTH SAW CUT
- EXISTING PARTIAL DEPTH BITUMINOUS PATCH
- AR705904 REMOVE UNDERDRAIN CLEANOUT
- AR705900 REMOVE UNDERDRAIN

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RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678
SBG Project No:
3-17-SBGP-TBD
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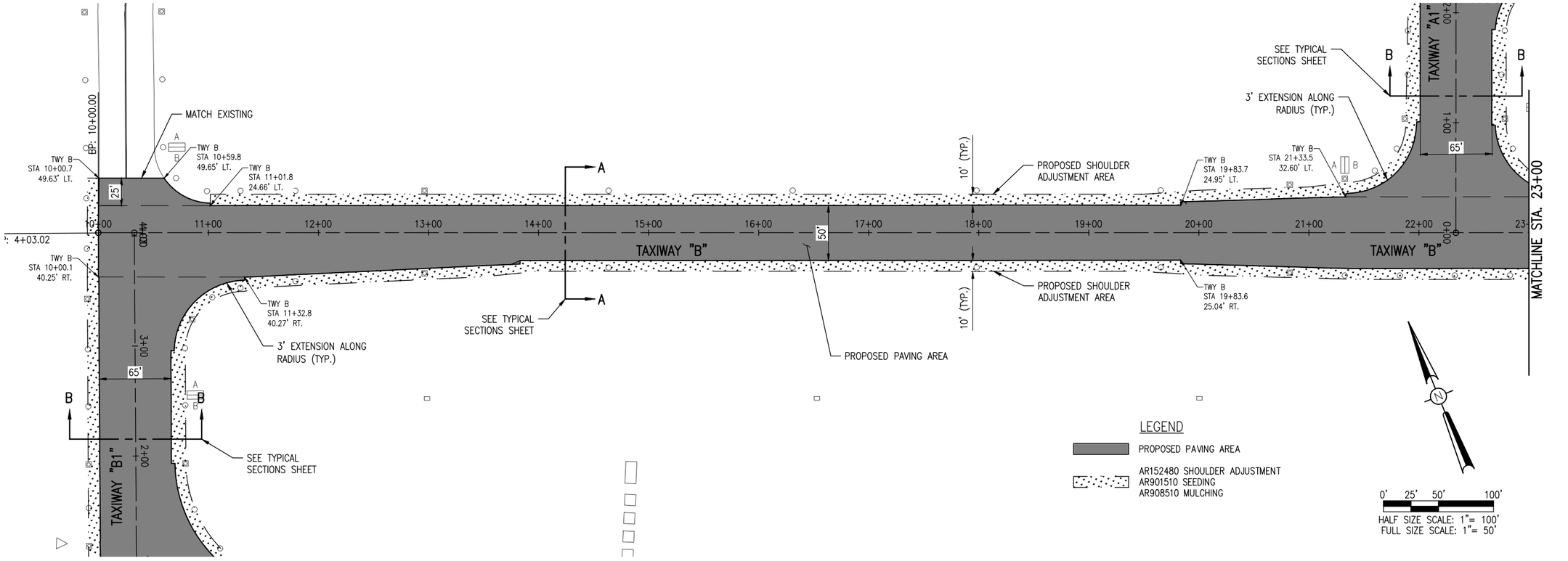
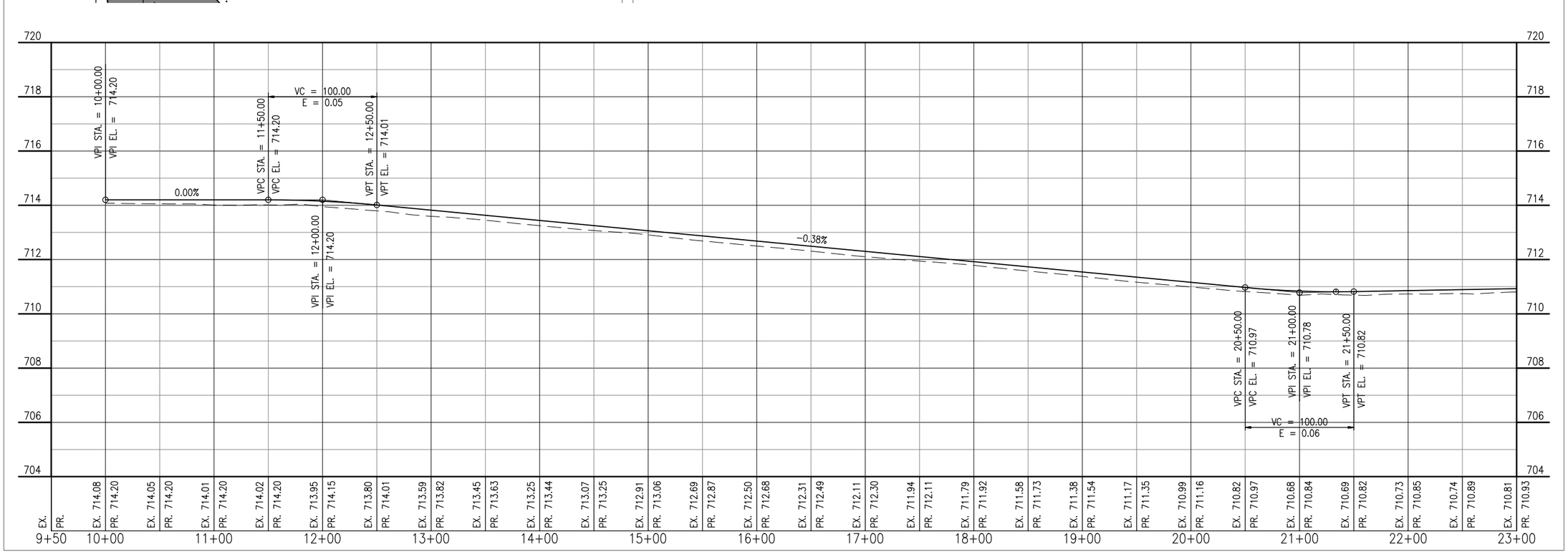
DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

REMOVAL PLAN STA.
31+00 TO 41+20





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IDA No: MTO-4678
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ISSUE: JUNE 15, 2018
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CAD FILE: C-701-PP-TXYB.DWG
DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

PLAN AND PROFILE
TAXIWAY B STA.
10+00 TO 23+00

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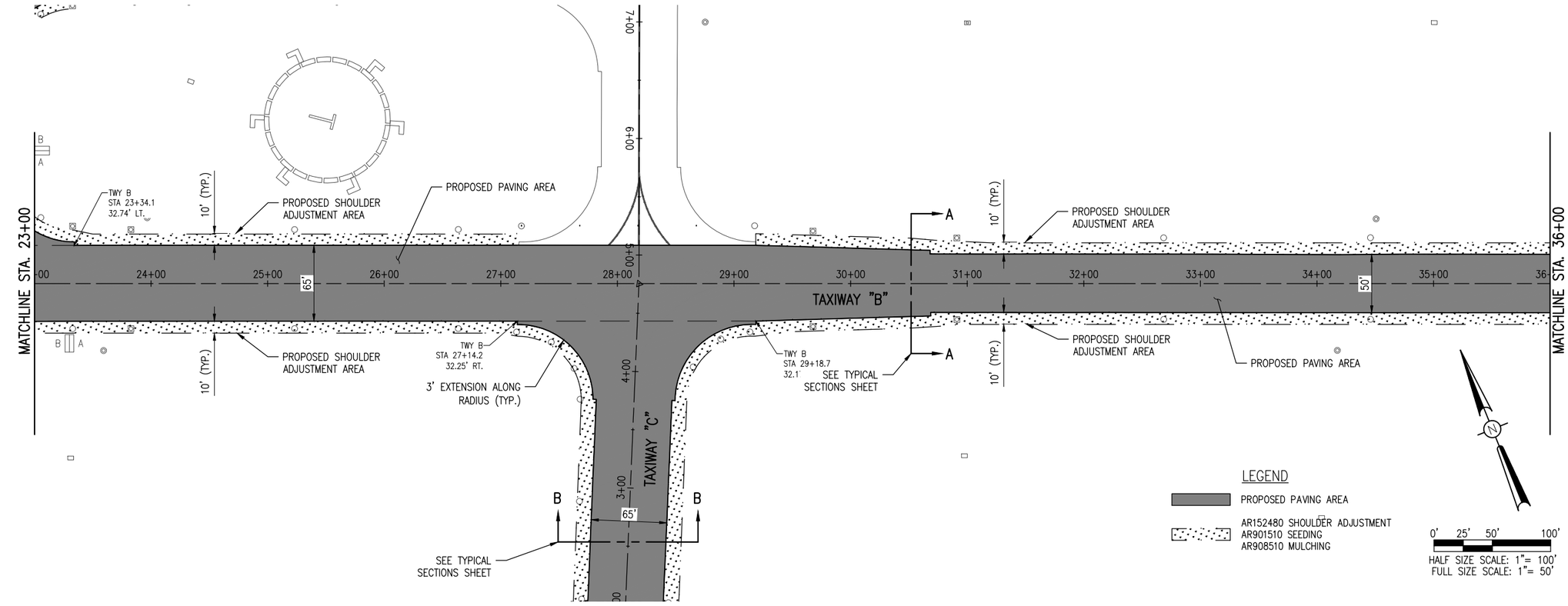

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COLES COUNTY AIRPORT AUTHORITY
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RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678
SBG Project No:
3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION	DES	DWN	REV

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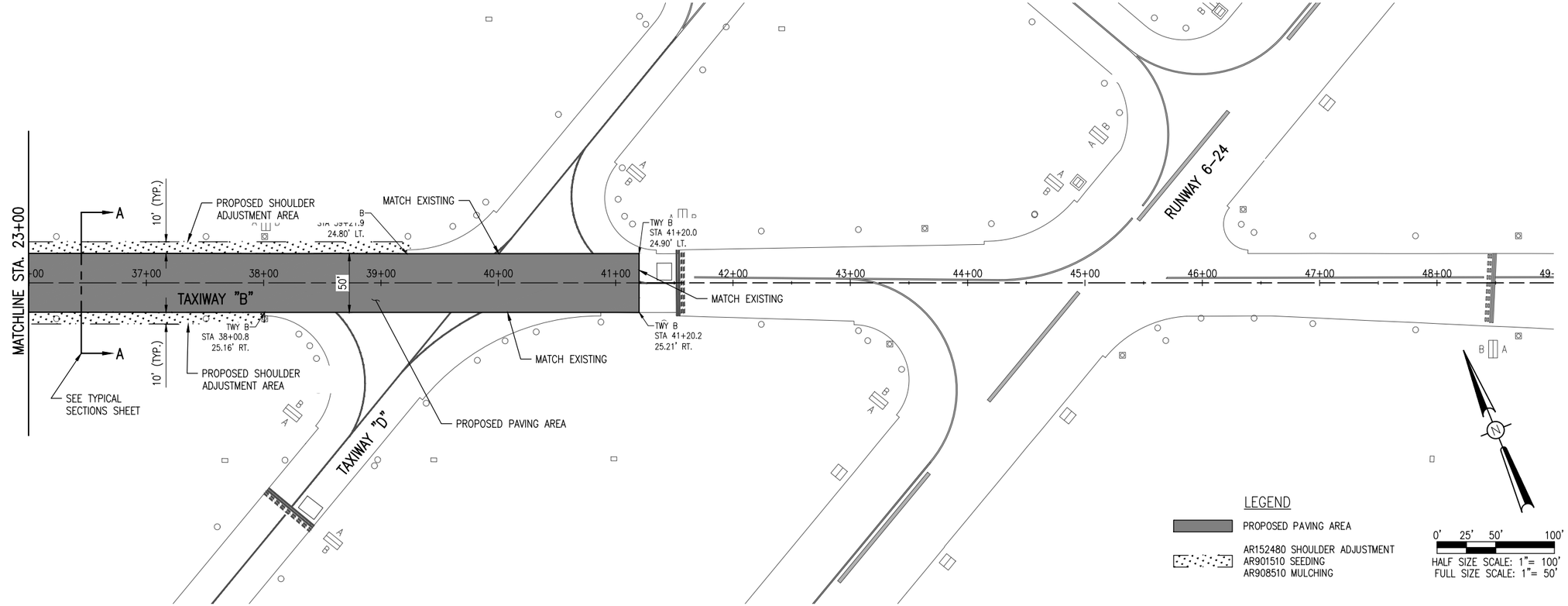
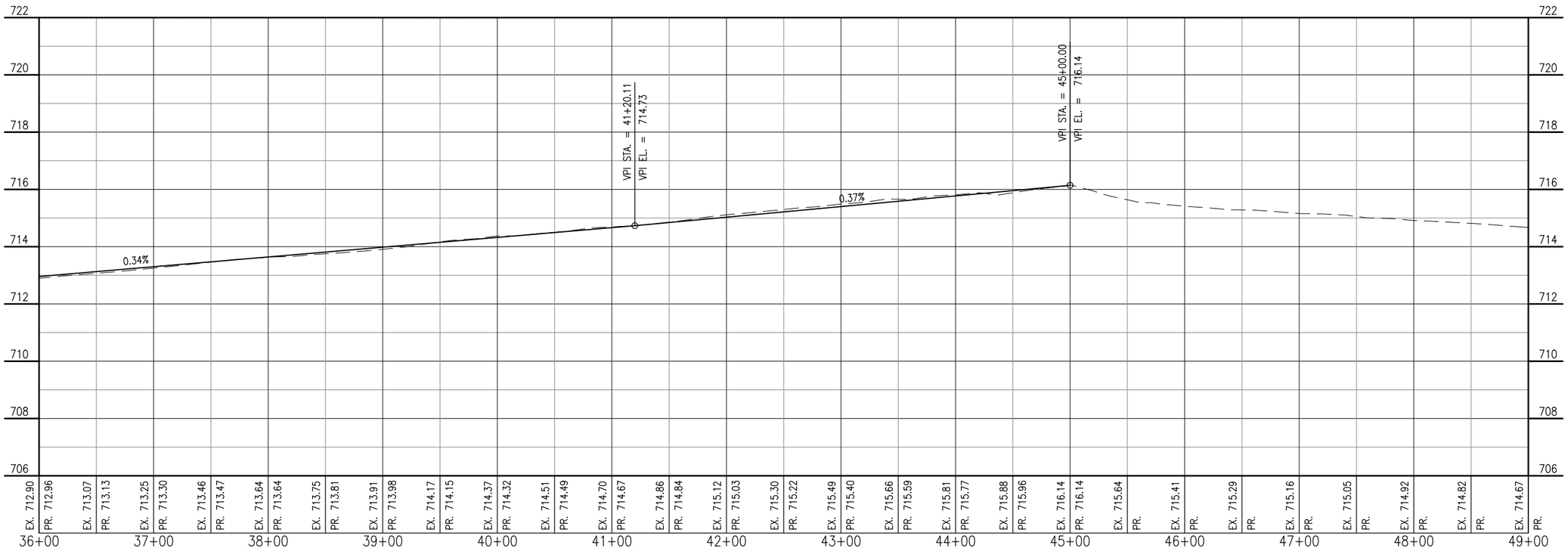
PLAN AND PROFILE
TAXIWAY B STA.
23+00 TO 36+00





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PLAN AND PROFILE
TAXIWAY B STA.
36+00 TO 49+00

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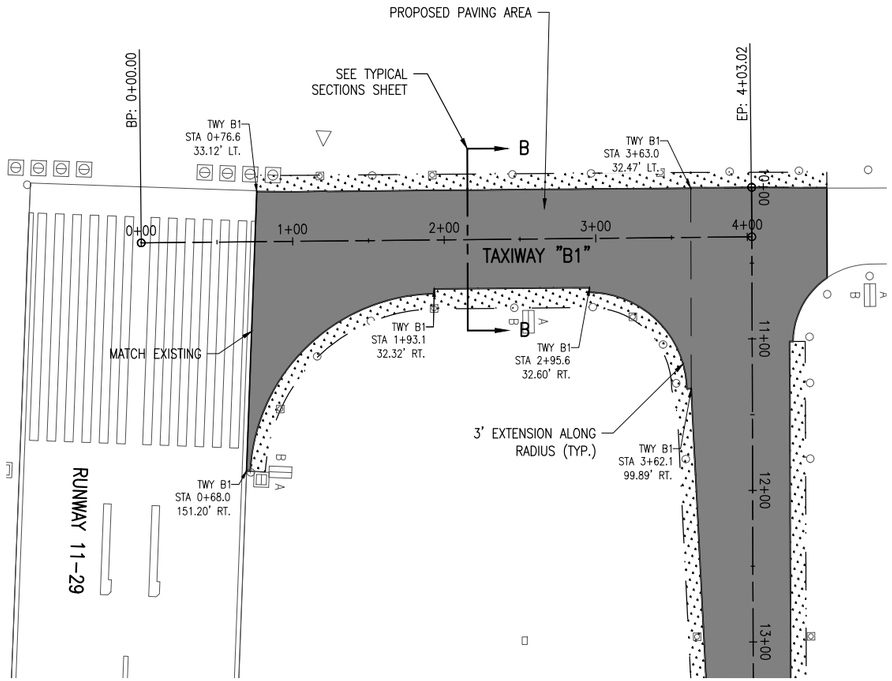
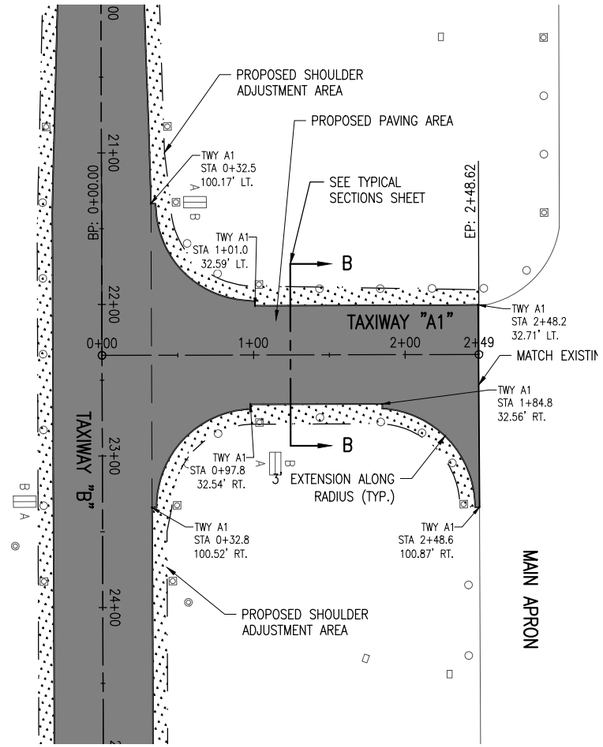
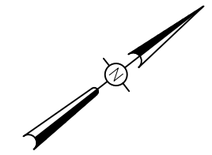


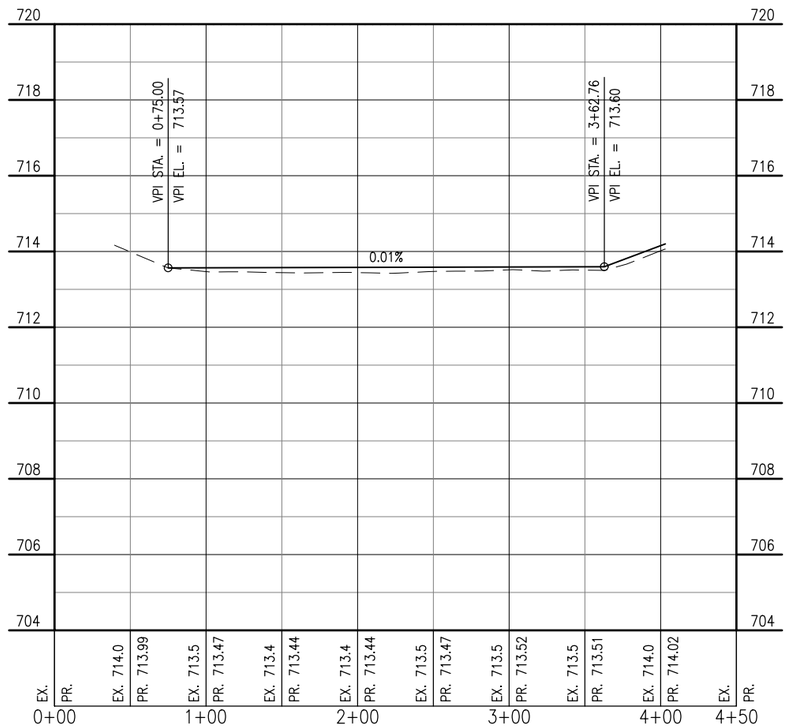


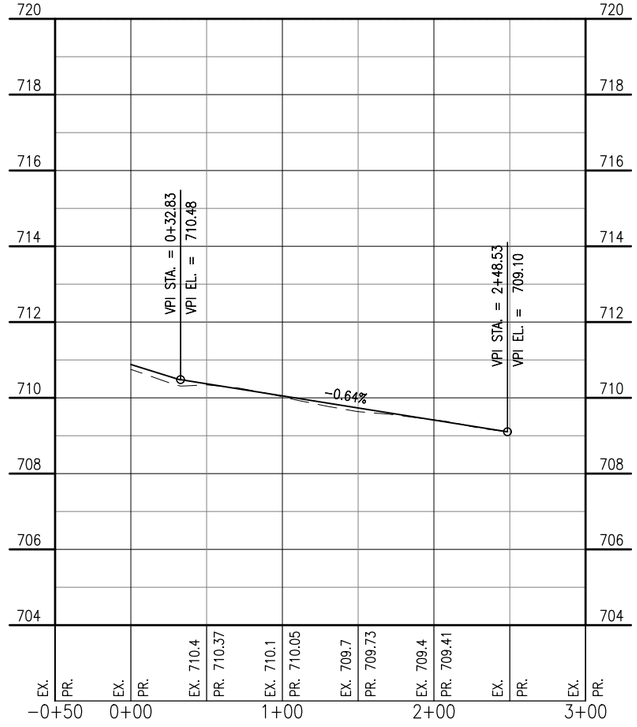


LEGEND

PROPOSED PAVING AREA

AR152480 SHOULDER ADJUSTMENT
AR901510 SEEDING
AR908510 MULCHING





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RECONSTRUCT
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IDA No: MTO-4678
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Contract No. CO064

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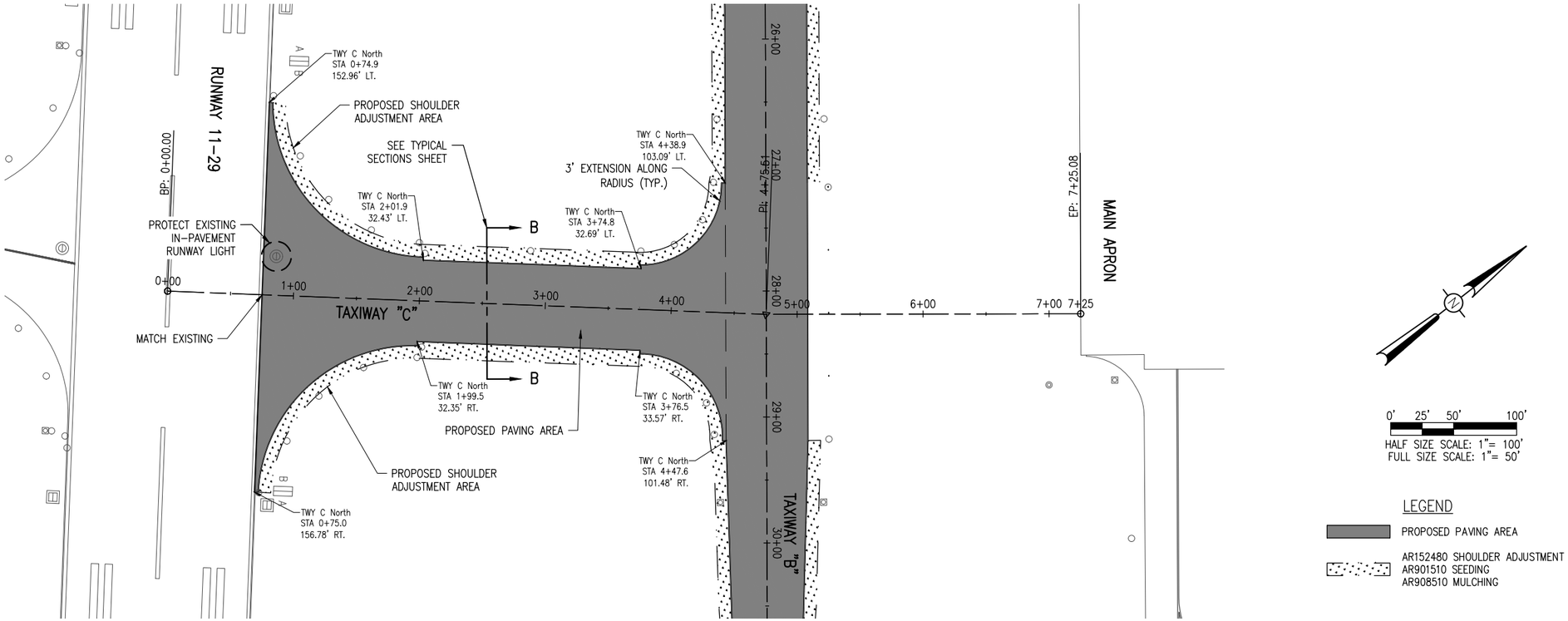
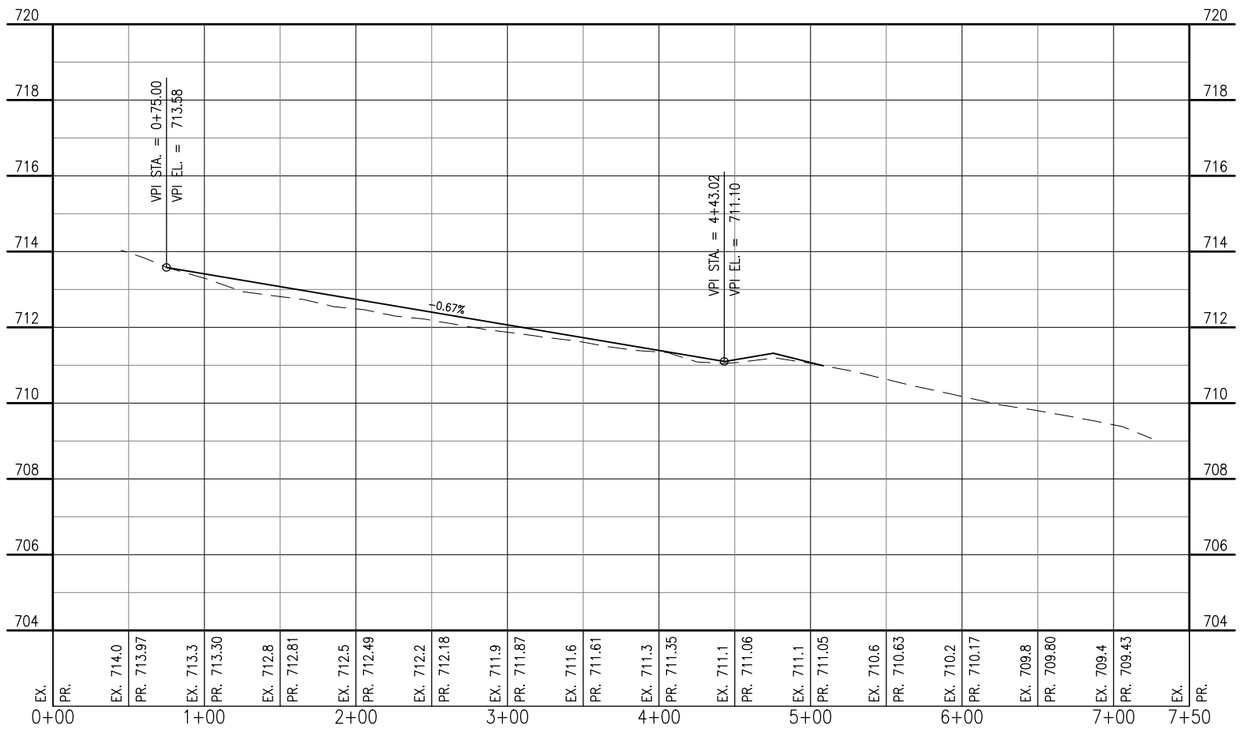
SHEET TITLE

PLAN AND PROFILE
TAXIWAY B1 AND A1





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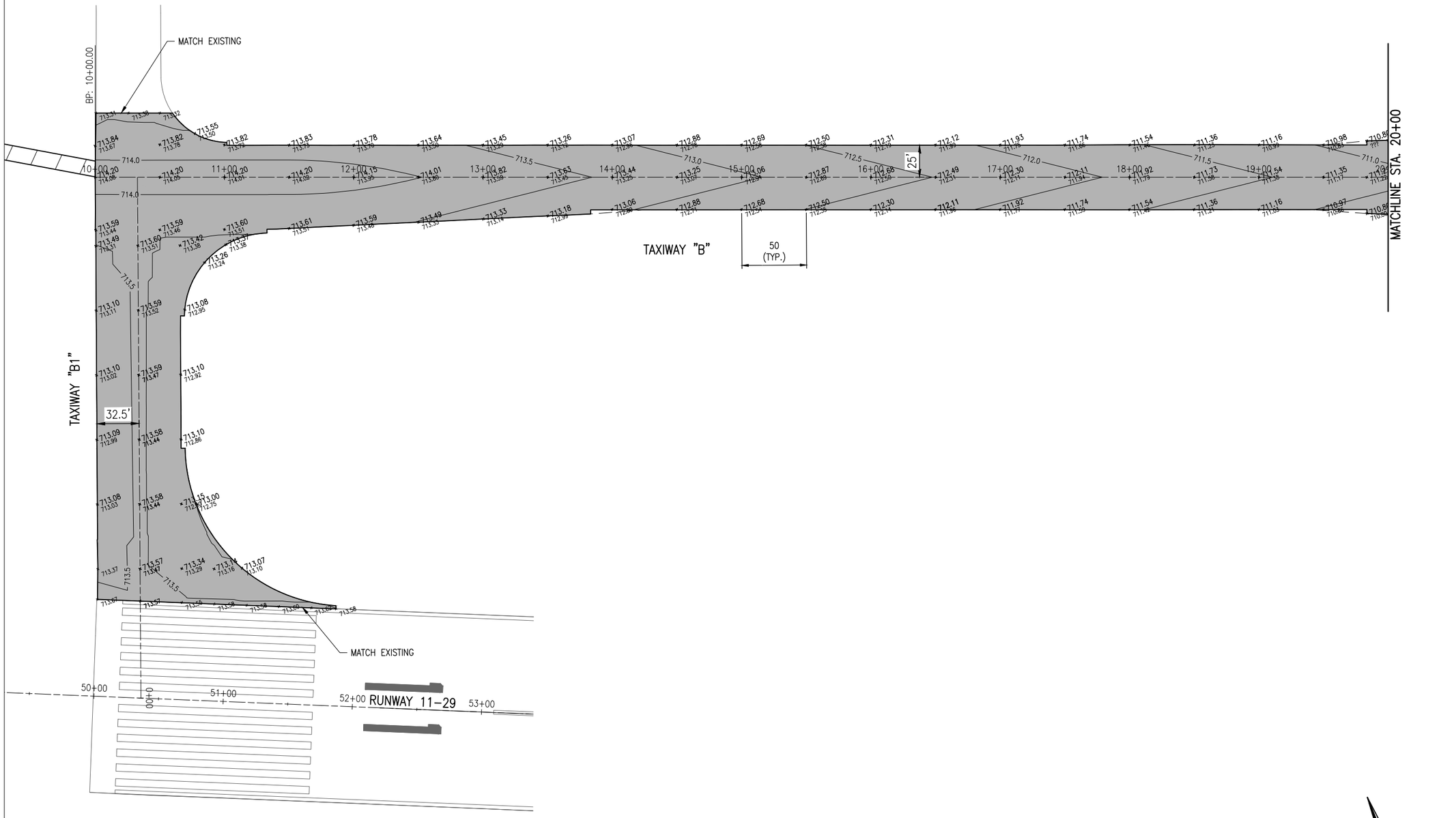
PLAN AND PROFILE
TAXIWAY C



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- +713.75 EXISTING GRADE
- +713.75 PROPOSED GRADE
- 715 PROPOSED CONTOURS

0' 20' 40' 80'

HALF SIZE SCALE: 1"= 80'
FULL SIZE SCALE: 1"= 40'

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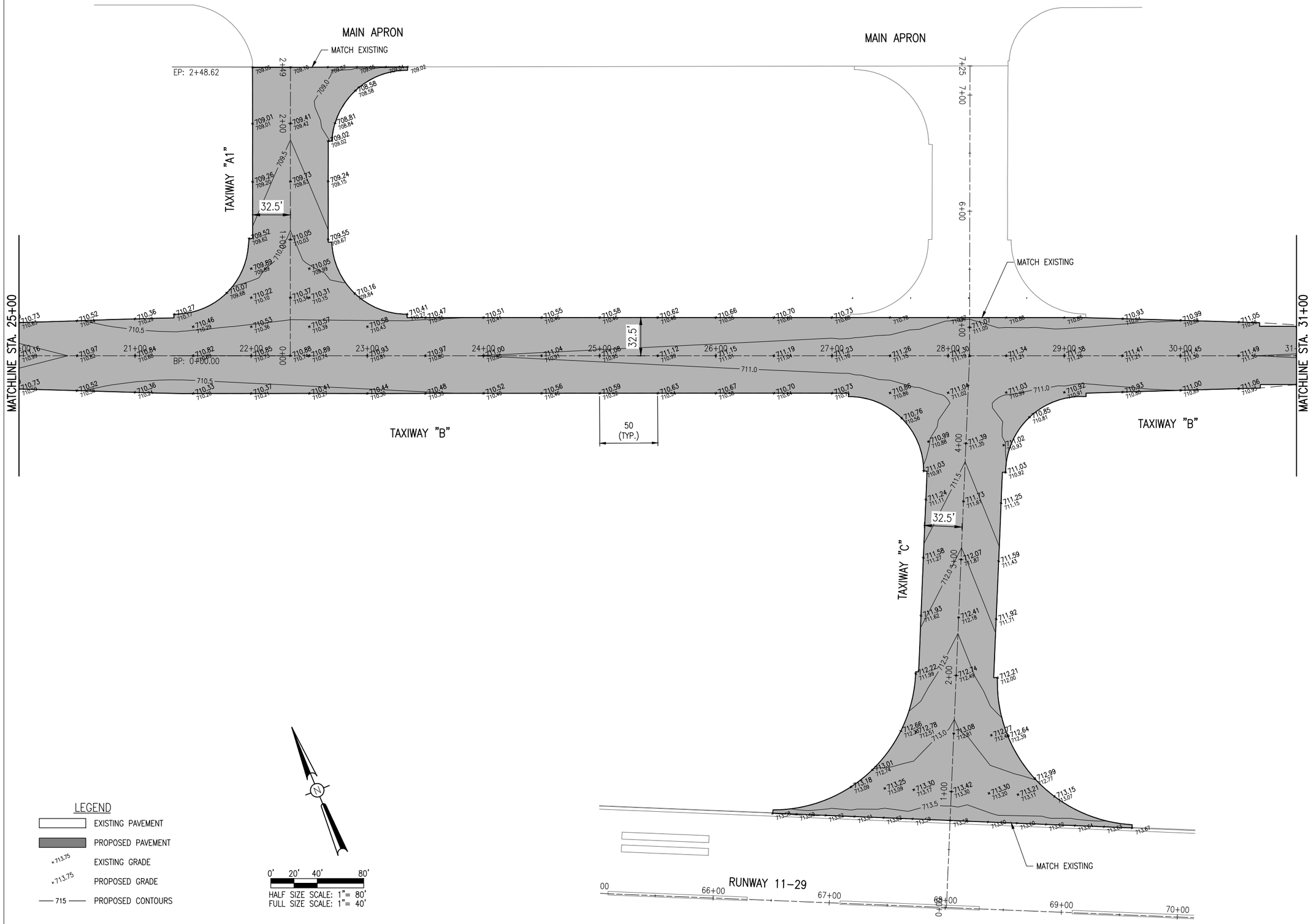
SHEET TITLE

PROPOSED STAKING
PLAN STA. 10+00 TO
20+00



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING GRADE
- PROPOSED GRADE
- PROPOSED CONTOURS

0' 20' 40' 80'

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RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

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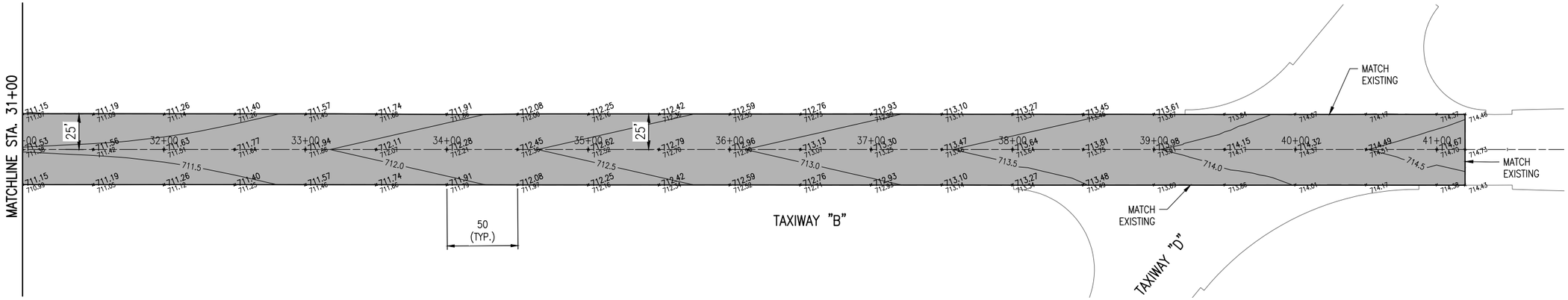
SHEET TITLE

PROPOSED STAKING PLAN STA. 20+00 TO 31+00



LEGEND

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- PROPOSED PAVEMENT
- ×713.75 EXISTING GRADE
- ×713.75 PROPOSED GRADE
- 715 PROPOSED CONTOURS

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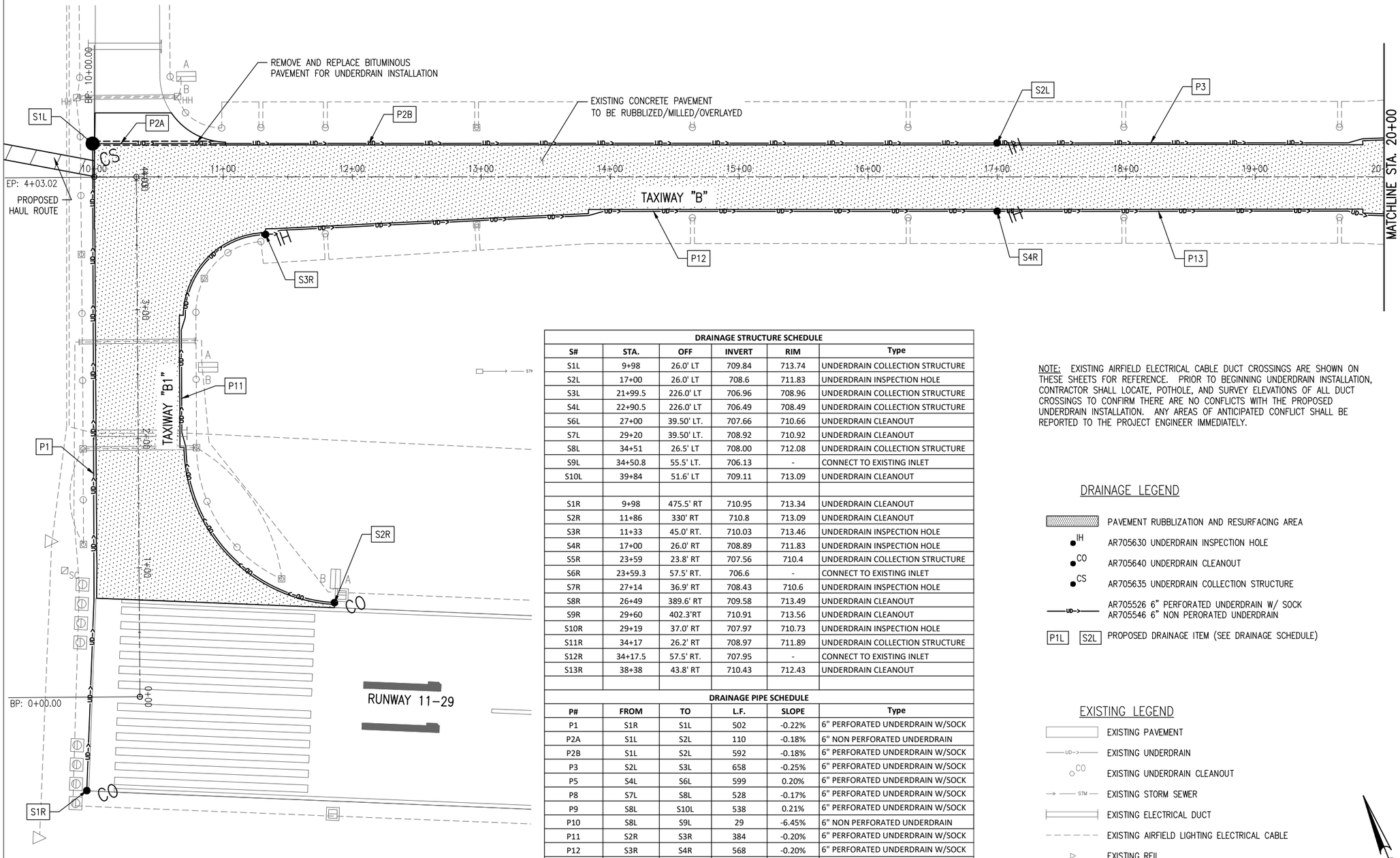
SHEET TITLE

PROPOSED STAKING
PLAN STA. 31+00 TO
41+20



DRAINAGE STRUCTURE SCHEDULE					
S#	STA.	OFF	INVERT	RIM	Type
S1L	9+98	26.0' LT	709.84	713.74	UNDERDRAIN COLLECTION STRUCTURE
S2L	17+00	26.0' LT	708.6	711.83	UNDERDRAIN INSPECTION HOLE
S3L	21+99.5	226.0' LT	706.96	708.96	UNDERDRAIN COLLECTION STRUCTURE
S4L	22+90.5	226.0' LT	706.49	708.49	UNDERDRAIN COLLECTION STRUCTURE
S6L	27+00	39.50' LT.	707.66	710.66	UNDERDRAIN CLEANOUT
S7L	29+20	39.50' LT.	708.92	710.92	UNDERDRAIN CLEANOUT
S8L	34+51	26.5' LT	708.00	712.08	UNDERDRAIN COLLECTION STRUCTURE
S9L	34+50.8	55.5' LT.	706.13	-	CONNECT TO EXISTING INLET
S10L	39+84	51.6' LT	709.11	713.09	UNDERDRAIN CLEANOUT
S1R	9+98	475.5' RT	710.95	713.34	UNDERDRAIN CLEANOUT
S2R	11+86	330' RT	710.8	713.09	UNDERDRAIN CLEANOUT
S3R	11+33	45.0' RT.	710.03	713.46	UNDERDRAIN INSPECTION HOLE
S4R	17+00	26.0' RT	708.89	711.83	UNDERDRAIN INSPECTION HOLE
S5R	23+59	23.8' RT	707.56	710.4	UNDERDRAIN COLLECTION STRUCTURE
S6R	23+59.3	57.5' RT.	706.6	-	CONNECT TO EXISTING INLET
S7R	27+14	36.9' RT	708.43	710.6	UNDERDRAIN INSPECTION HOLE
S8R	26+49	389.6' RT	709.58	713.49	UNDERDRAIN CLEANOUT
S9R	29+60	402.3'RT	710.91	713.56	UNDERDRAIN CLEANOUT
S10R	29+19	37.0' RT	707.97	710.73	UNDERDRAIN INSPECTION HOLE
S11R	34+17	26.2' RT	708.97	711.89	UNDERDRAIN COLLECTION STRUCTURE
S12R	34+17.5	57.5' RT.	707.95	-	CONNECT TO EXISTING INLET
S13R	38+38	43.8' RT	710.43	712.43	UNDERDRAIN CLEANOUT

DRAINAGE PIPE SCHEDULE					
P#	FROM	TO	L.F.	SLOPE	Type
P1	S1R	S1L	502	-0.22%	6" PERFORATED UNDERDRAIN W/SOCK
P2A	S1L	S2L	110	-0.18%	6" NON PERFORATED UNDERDRAIN
P2B	S1L	S2L	592	-0.18%	6" PERFORATED UNDERDRAIN W/SOCK
P3	S2L	S3L	658	-0.25%	6" PERFORATED UNDERDRAIN W/SOCK
P5	S4L	S6L	599	0.20%	6" PERFORATED UNDERDRAIN W/SOCK
P8	S7L	S8L	528	-0.17%	6" PERFORATED UNDERDRAIN W/SOCK
P9	S8L	S10L	538	0.21%	6" PERFORATED UNDERDRAIN W/SOCK
P10	S8L	S9L	29	-6.45%	6" NON PERFORATED UNDERDRAIN
P11	S2R	S3R	384	-0.20%	6" PERFORATED UNDERDRAIN W/SOCK
P12	S3R	S4R	568	-0.20%	6" PERFORATED UNDERDRAIN W/SOCK
P13	S4R	S5R	668	-0.20%	6" PERFORATED UNDERDRAIN W/SOCK
P15	S5R	S7R	348	0.25%	6" PERFORATED UNDERDRAIN W/SOCK
P14	S5R	S6R	24	-4.00%	6" NON PERFORATED UNDERDRAIN
P16	S7R	S8R	459	0.25%	6" PERFORATED UNDERDRAIN W/SOCK
P18	S9R	S10R	472	-0.62%	6" PERFORATED UNDERDRAIN W/SOCK
P19	S10R	S11R	499	0.20%	6" PERFORATED UNDERDRAIN W/SOCK
P20	S11R	S12R	31	-3.29%	6" NON PERFORATED UNDERDRAIN
P21	S11R	S13R	424	0.34%	6" PERFORATED UNDERDRAIN W/SOCK

NOTE: EXISTING AIRFIELD ELECTRICAL CABLE DUCT CROSSINGS ARE SHOWN ON THESE SHEETS FOR REFERENCE. PRIOR TO BEGINNING UNDERDRAIN INSTALLATION, CONTRACTOR SHALL LOCATE, POTHOLE, AND SURVEY ELEVATIONS OF ALL DUCT CROSSINGS TO CONFIRM THERE ARE NO CONFLICTS WITH THE PROPOSED UNDERDRAIN INSTALLATION. ANY AREAS OF ANTICIPATED CONFLICT SHALL BE REPORTED TO THE PROJECT ENGINEER IMMEDIATELY.

DRAINAGE LEGEND

- PAVEMENT RUBBLIZATION AND RESURFACING AREA
- IH — AR705630 UNDERDRAIN INSPECTION HOLE
- CO — AR705640 UNDERDRAIN CLEANOUT
- CS — AR705635 UNDERDRAIN COLLECTION STRUCTURE
- UD-> — AR705526 6" PERFORATED UNDERDRAIN W/ SOCK; AR705546 6" NON PERORATED UNDERDRAIN
- P1L, S2L — PROPOSED DRAINAGE ITEM (SEE DRAINAGE SCHEDULE)

EXISTING LEGEND

- EXISTING PAVEMENT
- UD-> — EXISTING UNDERDRAIN
- CO — EXISTING UNDERDRAIN CLEANOUT
- STM — EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING AIRFIELD LIGHTING ELECTRICAL CABLE
- EXISTING REIL
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED THRESHOLD LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT

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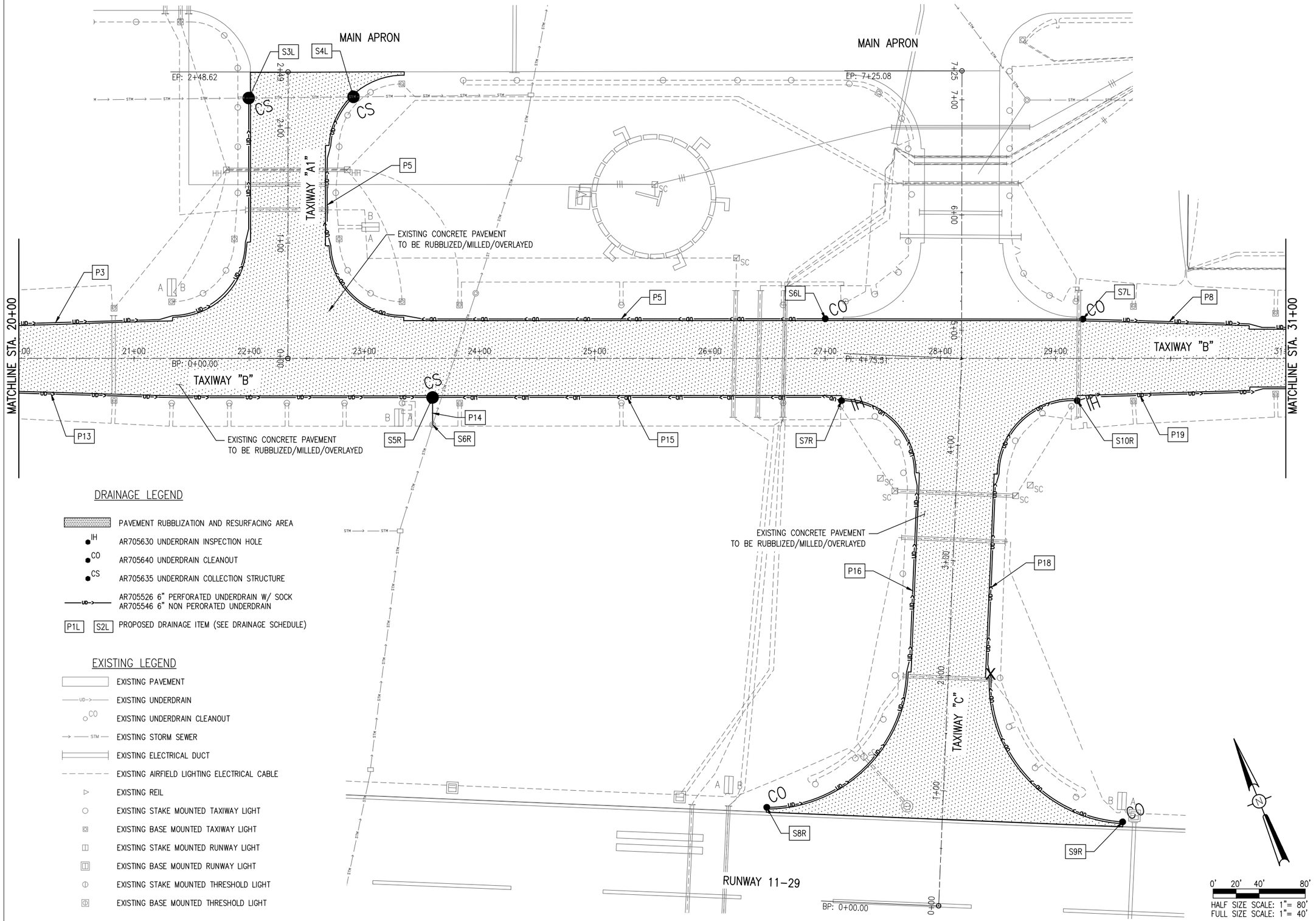
STA. 10+00 TO 20+00



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- EXISTING UNDERDRAIN CLEANOUT
- EXISTING STORM SEWER
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UNDERDRAIN PLAN
STA. 20+00 TO 31+00

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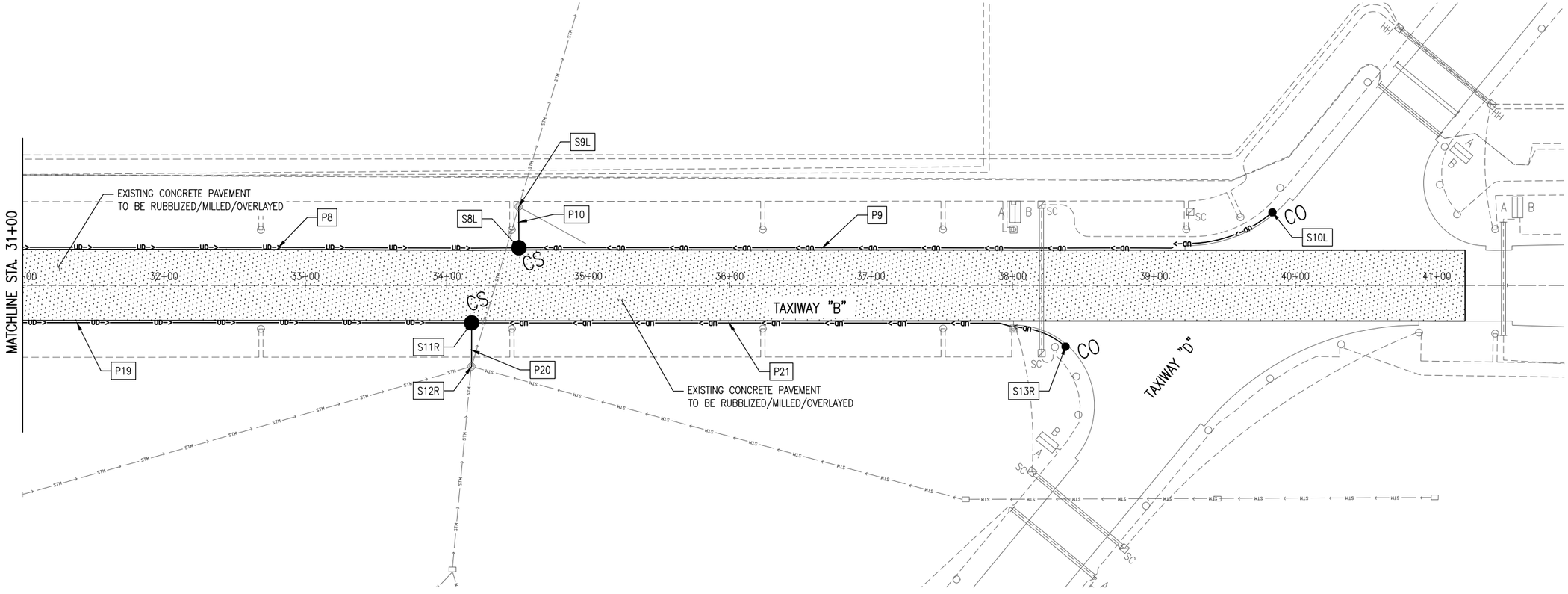

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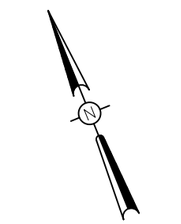

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RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678

SBG Project No:
3-17-SBGP-TBD

Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

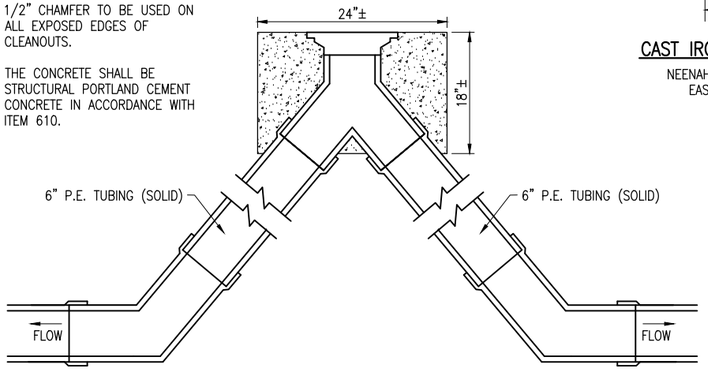
ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-130-DRPL.DWG
DESIGN BY: JAP 05/2018
DRAWN BY: JAP 05/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

UNDERDRAIN PLAN
STA. 31+00 TO 41+20

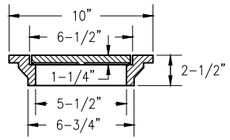
CLEANOUT NOTES

1. TOP OF CLEANOUTS SHALL BE 2" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.
2. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
3. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE IN ACCORDANCE WITH ITEM 610.



DUAL UNDERDRAIN CLEANOUT

NO SCALE

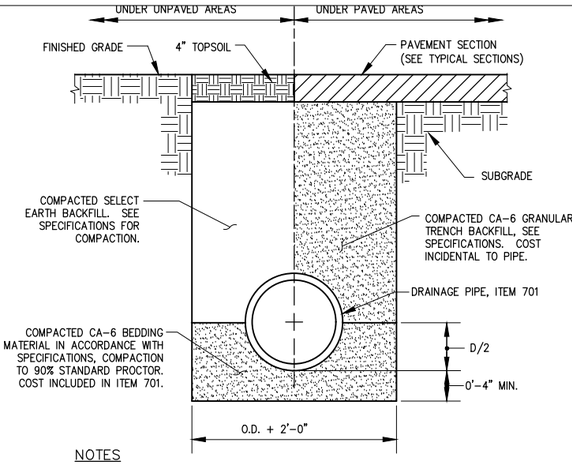


CAST IRON FRAME AND COVER

NEENAH R-6013, DEETER 1810, EAST JORDAN 2790-6 OR APPROVED EQUAL

UNDERDRAIN CLEANOUT

NO SCALE



NOTES

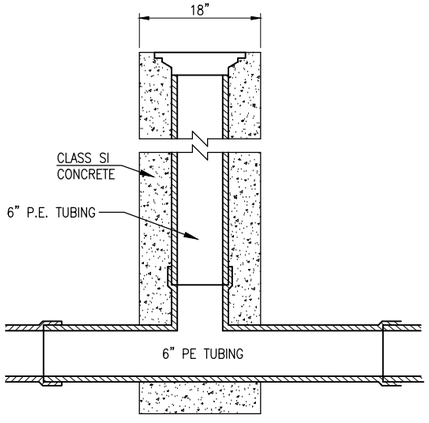
1. UNSUITABLE MATERIAL ENCOUNTERED DURING PLACEMENT OF BEDDING SHALL BE REMOVED AND REPLACED.
2. WITHIN 3 FEET OF PAVED AREA, GRANULAR BACKFILL IS TO BE USED INSTEAD OF EARTH BACKFILL.
3. AT CONTRACTOR'S OPTION IDOT CONTROLLED LOW STRENGTH MATERIAL WITH A HIGH EARLY STRENGTH, "FLASH FILL", MAY BE USED INSTEAD OF GRANULAR TRENCH BACKFILL UNDER PAVEMENTS.

PIPE TRENCH

N.T.S.

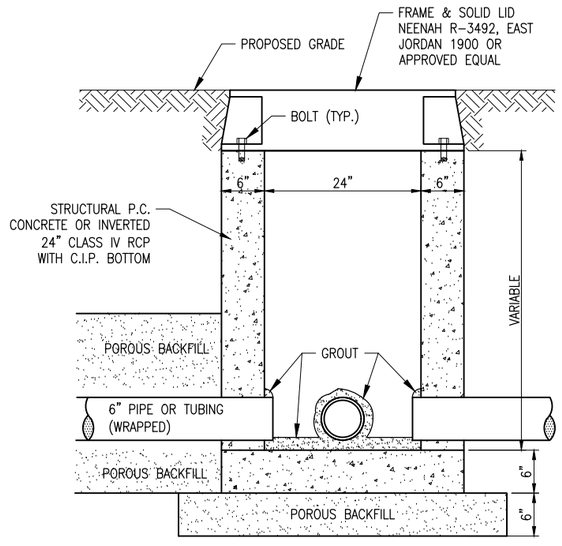
NOTES

1. ALL STORM SEWER AND UNDERDRAIN CONNECTIONS ARE INCIDENTAL TO THE APPLICABLE PIPE OR STRUCTURE PAY ITEM.
2. UNDERDRAIN COLLECTION STRUCTURE WILL BE PLACED IMMEDIATELY ADJACENT TO THE PROPOSED COURSE OF RUBBLIZED 501 CONCRETE PAVEMENT (HORIZONTALLY) AND THE TUBING DIRECTED TO INTERSECT AS SHOWN.



PROPOSED INSPECTION HOLE DETAIL

NOT TO SCALE



PROPOSED UNDERDRAIN COLLECTION STRUCTURE DETAIL

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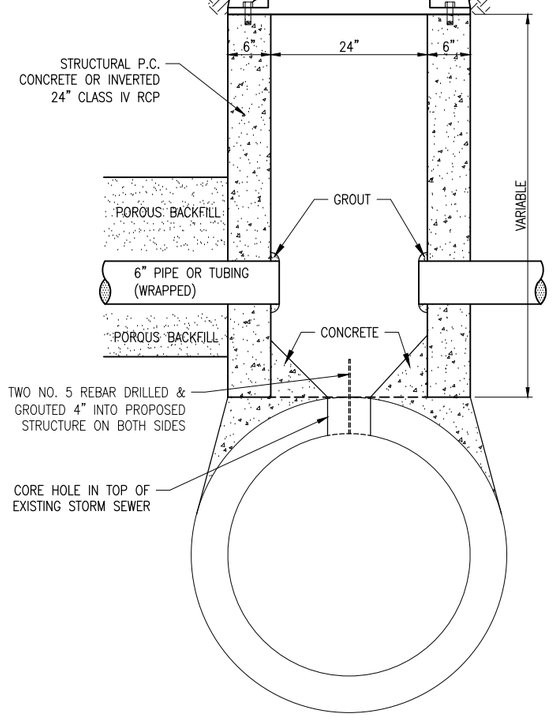

WHEN INSTALLED OVER EXISTING PIPE

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
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REVIEWED BY: KBS 06/14/2018

SHEET TITLE

UNDERDRAIN DETAILS

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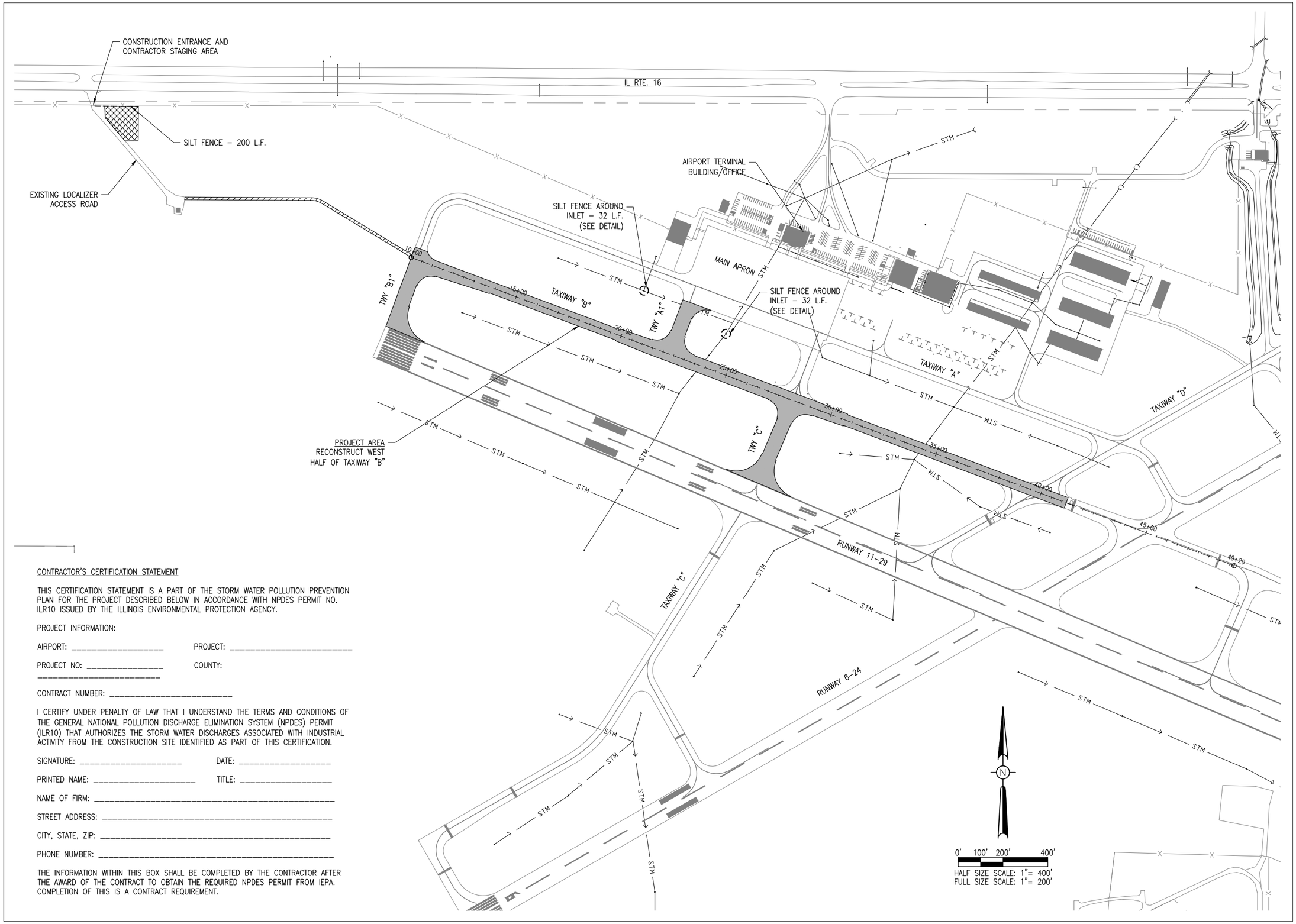

CONTRACTOR'S CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

AIRPORT: _____ PROJECT: _____

PROJECT NO: _____ COUNTY: _____

CONTRACT NUMBER: _____

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

NAME OF FIRM: _____

STREET ADDRESS: _____

CITY, STATE, ZIP: _____

PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

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SHEET TITLE

STORMWATER
POLLUTION
PREVENTION PLAN

JUN 20, 2018 1:14 PM SCHWE01296
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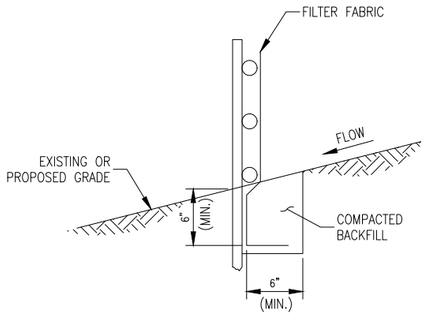
NOTES:

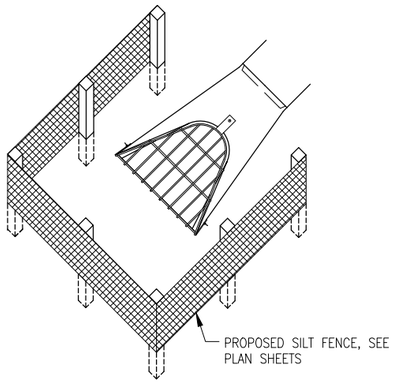
1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i, AS AMENDED), OR EQUIVALENT.

NOTES:

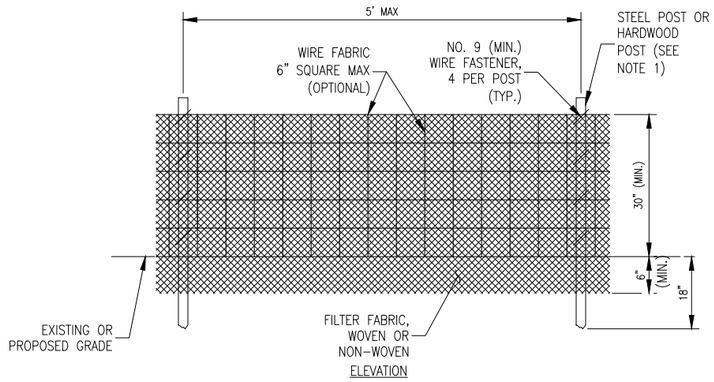
1. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
3. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
4. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
5. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
6. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
7. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
8. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



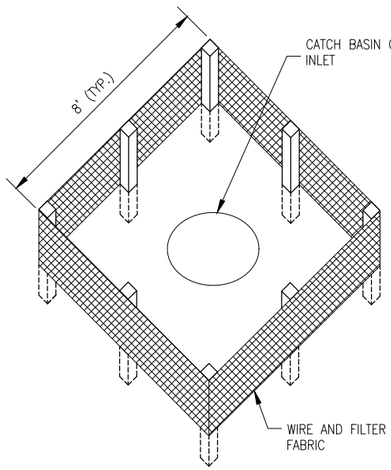
FABRIC ANCHOR DETAIL



SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)



ELEVATION



NOTES:

1. FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
2. INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
3. SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH PER LANDSCAPING PLAN. COST OF REMOVAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE.
4. AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO SILT FENCE.
5. FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
6. PAID UNDER AR156510 SILT FENCE.

SILT FENCE AT MANHOLES IN PERVIOUS AREAS

STORM WATER POLLUTION PREVENTION NOTES

GENERAL
THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES
THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

SEDIMENTATION AND EROSION CONTROL NOTES:

A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.

B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
- UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
- AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.

C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.

D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 AND/OR RR-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.

E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.

F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.

G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.

H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.

I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.

J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.

K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.

L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.

M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.

N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

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RECONSTRUCT TAXIWAY B

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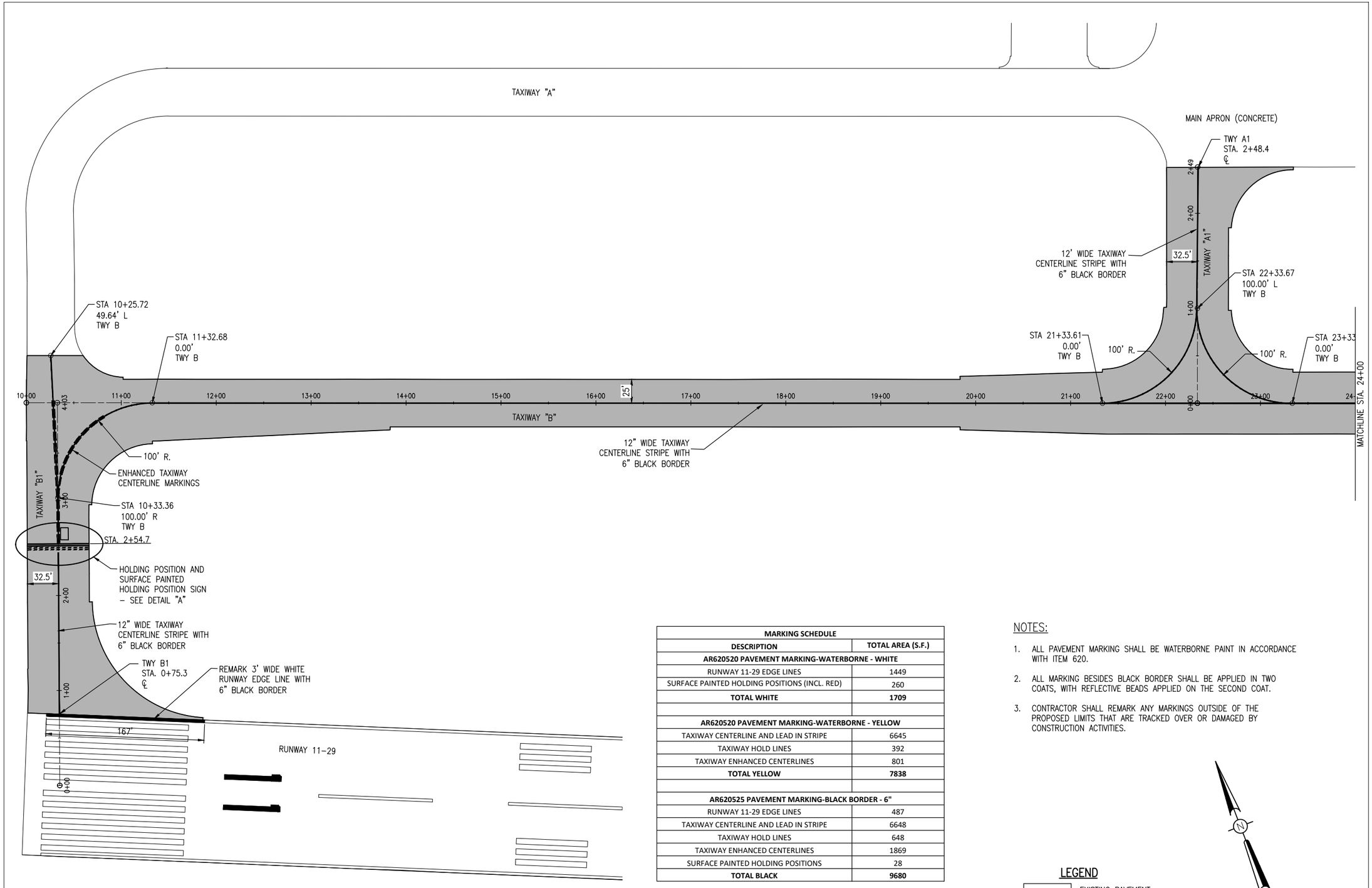
STORMWATER POLLUTION PREVENTION PLAN DETAILS



MARKING SCHEDULE	
DESCRIPTION	**TOTAL AREA (S.F.)**
AR620520 PAVEMENT MARKING-WATERBORNE - WHITE	
RUNWAY 11-29 EDGE LINES	1449
SURFACE PAINTED HOLDING POSITIONS (INCL. RED)	260
TOTAL WHITE	**1709**
AR620520 PAVEMENT MARKING-WATERBORNE - YELLOW	
TAXIWAY CENTERLINE AND LEAD IN STRIPE	6645
TAXIWAY HOLD LINES	392
TAXIWAY ENHANCED CENTERLINES	801
TOTAL YELLOW	**7838**
AR620525 PAVEMENT MARKING-BLACK BORDER - 6"	
RUNWAY 11-29 EDGE LINES	487
TAXIWAY CENTERLINE AND LEAD IN STRIPE	6648
TAXIWAY HOLD LINES	648
TAXIWAY ENHANCED CENTERLINES	1869
SURFACE PAINTED HOLDING POSITIONS	28
TOTAL BLACK	**9680**

NOTES:

1. ALL PAVEMENT MARKING SHALL BE WATERBORNE PAINT IN ACCORDANCE WITH ITEM 620.
2. ALL MARKING BESIDES BLACK BORDER SHALL BE APPLIED IN TWO COATS, WITH REFLECTIVE BEADS APPLIED ON THE SECOND COAT.
3. CONTRACTOR SHALL REMARK ANY MARKINGS OUTSIDE OF THE PROPOSED LIMITS THAT ARE TRACKED OVER OR DAMAGED BY CONSTRUCTION ACTIVITIES.

LEGEND

EXISTING PAVEMENT

PROPOSED PAVING AREA

PROPOSED MARKING

0' 25' 50' 100'

HALF SIZE SCALE: 1"= 100'
FULL SIZE SCALE: 1"= 50'

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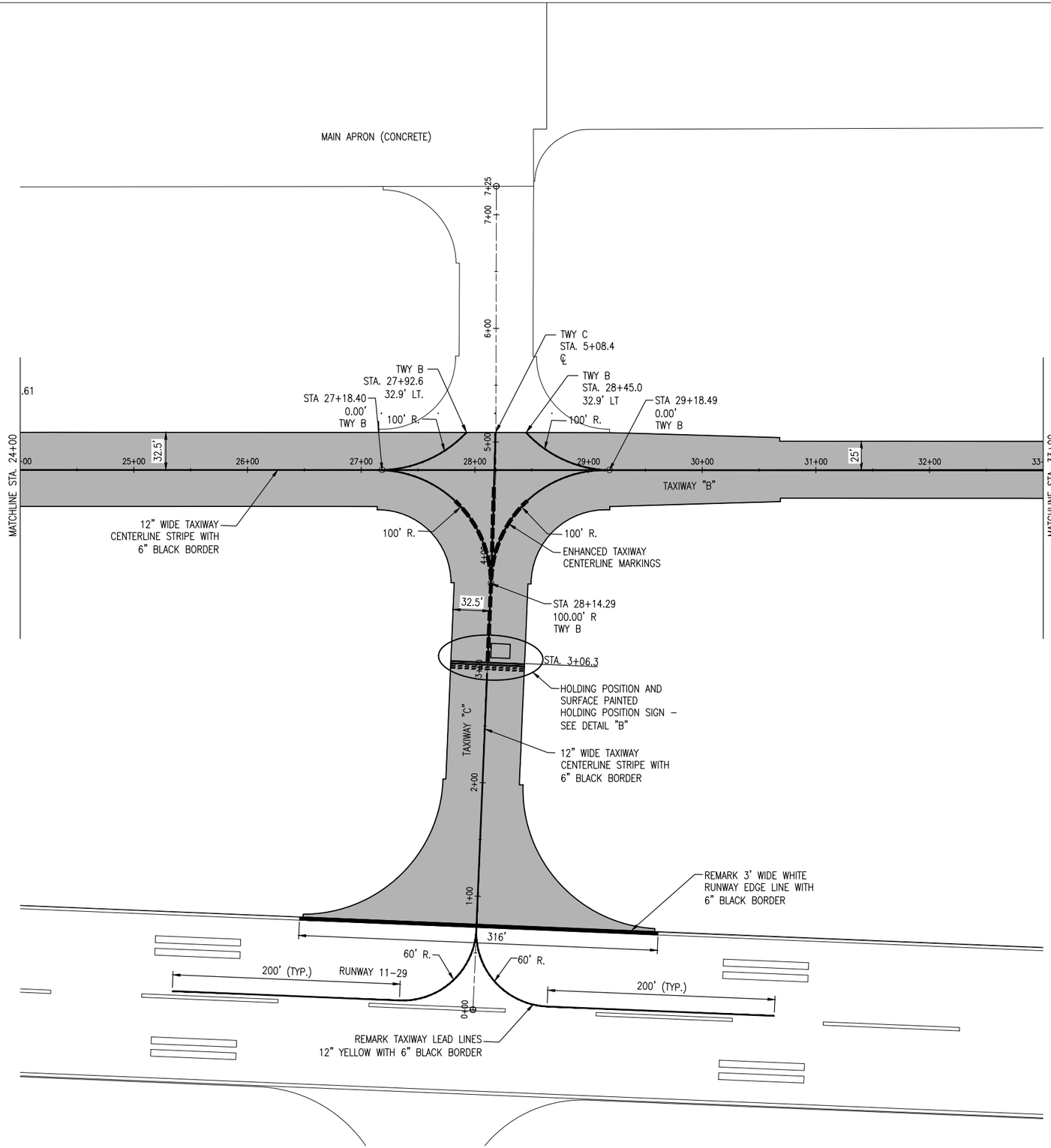
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

MARKING PLAN STA.
10+00 TO 24+00



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVING AREA
- PROPOSED MARKING

0' 25' 50' 100'

HALF SIZE SCALE: 1"= 100'
FULL SIZE SCALE: 1"= 50'

JUN 20, 2018 1:14 PM SCHWE01296
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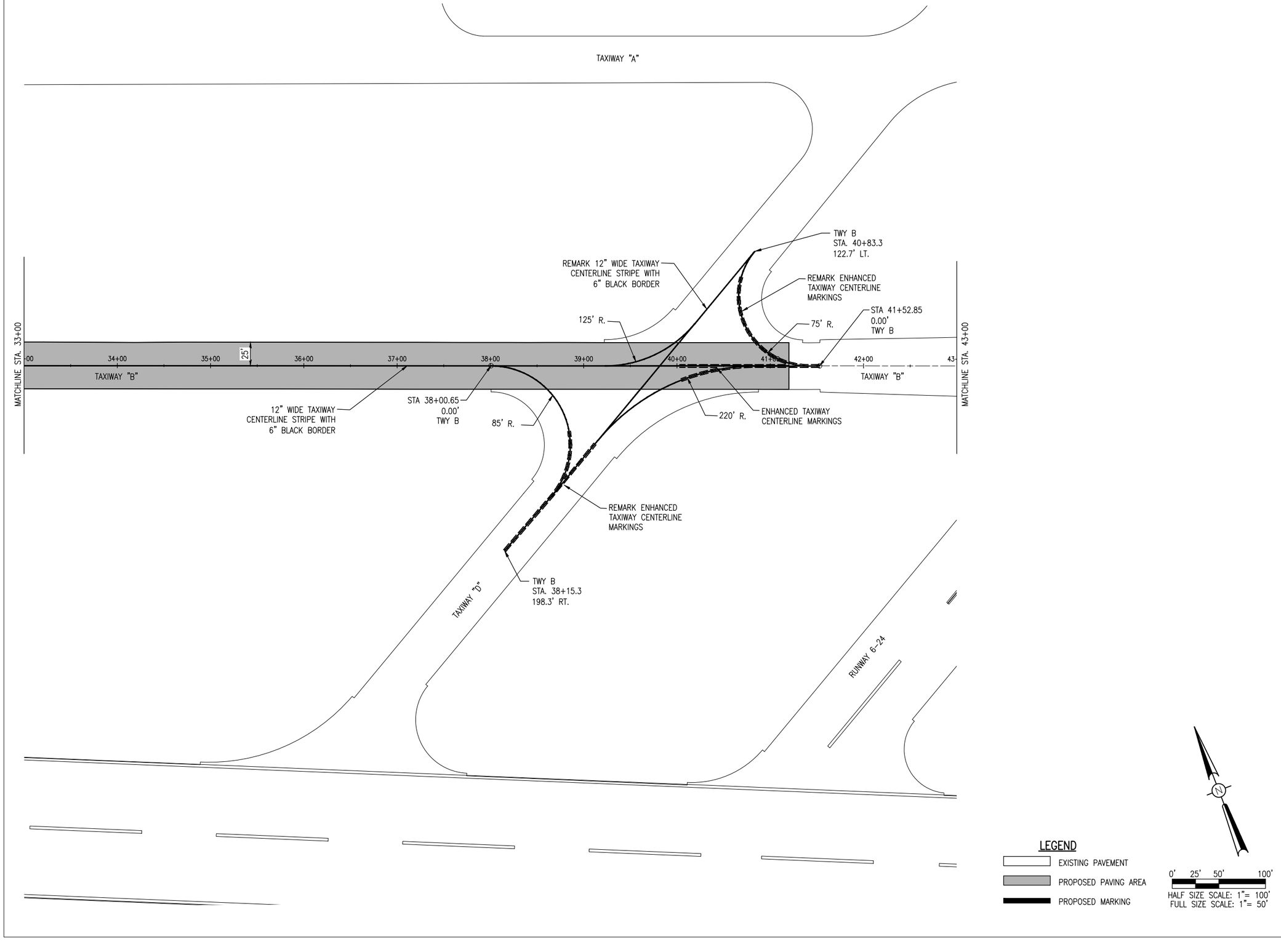
MARKING PLAN STA.
24+00 TO 33+00



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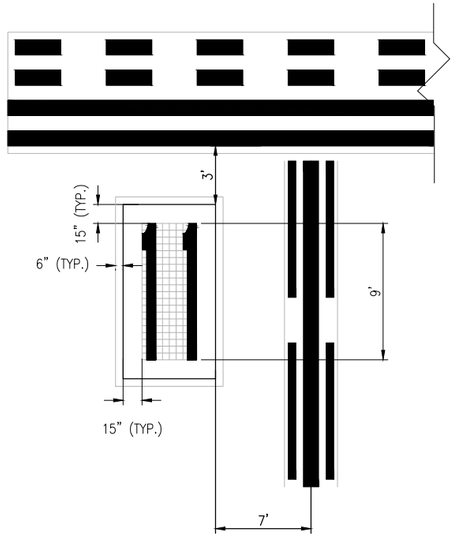
DRAWN BY: CWS 05/10/2018
REVIEWED BY: KBS 06/14/2018

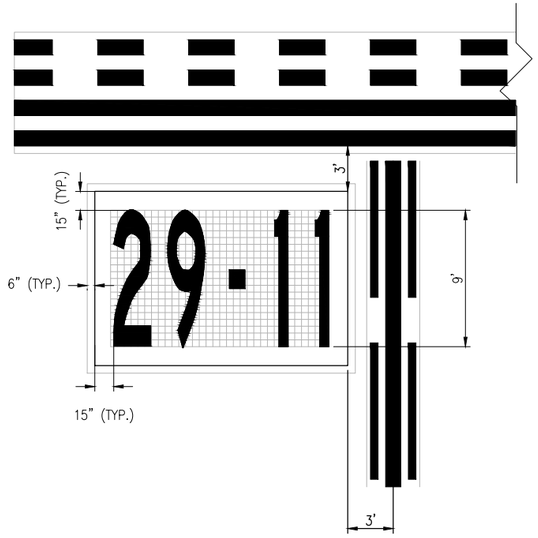
SHEET TITLE

MARKING PLAN STA.
33+00 TO 43+00



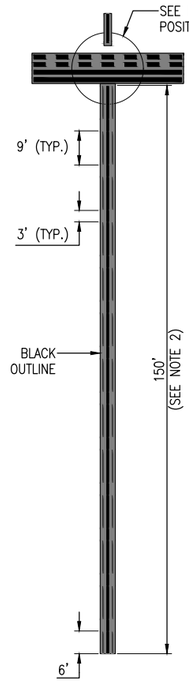
SURFACE PAINTED HOLDING POSITION SIGN DETAIL "A"
NOT TO SCALE

NOTE: GRID SHOWN FOR PROPER SIZING OF INSCRIPTIONS – NOT TO BE PAINTED. GRID SPACING IS 0.45 FEET. THIS SIGN WILL BE PAINTED USING WATERBORNE PAINT



SURFACE PAINTED HOLDING POSITION SIGN DETAIL "B"
NOT TO SCALE

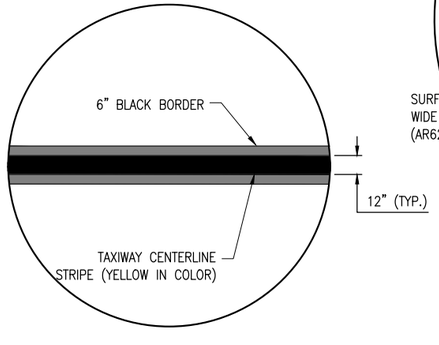
NOTE: GRID SHOWN FOR PROPER SIZING OF INSCRIPTIONS – NOT TO BE PAINTED. GRID SPACING IS 0.45 FEET. THIS SIGN WILL BE PAINTED USING WATERBORNE PAINT



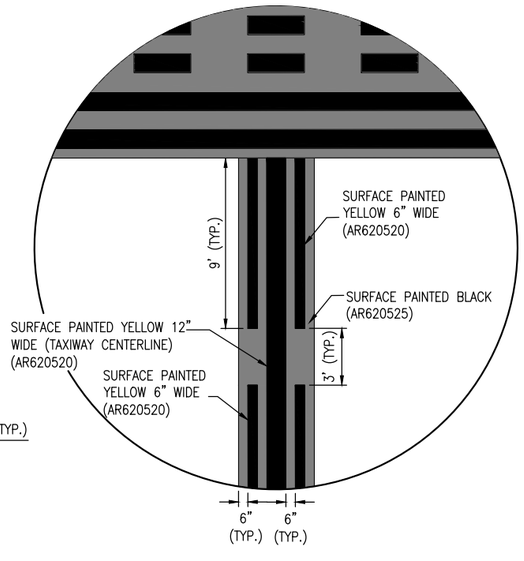
ENHANCED TAXIWAY CENTERLINE MARKING DETAIL
NOT TO SCALE

ENHANCED TAXIWAY CENTERLINE MARKING NOTES:

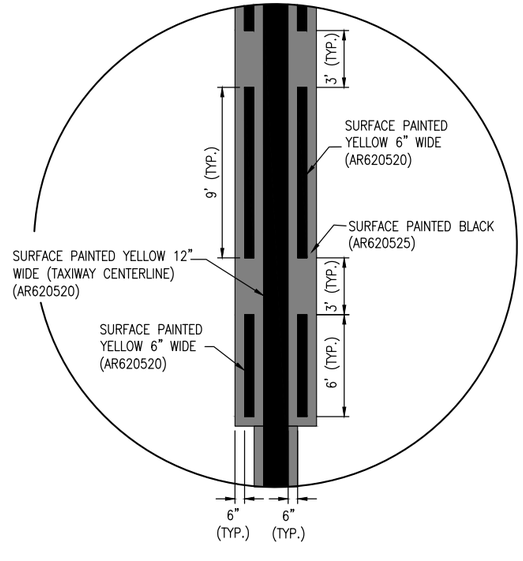
1. TAXIWAY CENTERLINE MARKINGS SHALL BE YELLOW IN COLOR AND OUTLINED IN BLACK.
2. TAXIWAY CENTERLINE MARKINGS SHALL BE ENHANCED FOR 150–FT PRIOR TO A RUNWAY HOLDING POSITION MARKING, UNLESS NOTED OTHERWISE. FOR A CURVED TAXIWAY CENTERLINE, THIS DISTANCE SHALL BE MEASURED ALONG THE CENTERLINE BEING ENHANCED TO A DISTANCE OF 150–FT.
3. WHERE TWO TAXIWAY CENTERLINES CONVERGE AT OR BEFORE THE RUNWAY HOLDING POSITION MARKING, PARTIAL INNER DASHED LINES LESS THAN 5 FEET AT THE POINT OF CONVERGENCE MAY BE OMITTED.
4. DASHES ON EITHER SIDE OF THE TAXIWAY CENTERLINE MUST BE ALIGNED, STARTING AND STOPPING WITH THE DASHES ON THE OPPOSITE SIDE OF THE CENTERLINE. TO ACCOMPLISH THIS FOR CURVED TAXIWAY CENTERLINES, THE MEASUREMENTS FOR THE DASHES AND GAPS SHALL BE MADE AT THE CENTERLINE AND EXTENDED PERPENDICULAR FROM THE CENTERLINE TO OBTAIN THE LOCATIONS OF THE DASHES.
5. ENHANCED TAXIWAY CENTERLINE MARKINGS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF FAA AC 150/5340–1, STANDARDS FOR AIRPORT MARKING.
6. IF THE CONTRACTOR ELECTS TO "BLOCK PAINT" THE BLACK PAINT AND THEN PAINT EITHER YELLOW OR WHITE PAINT OVER THE BLACK PAINT; ONLY THE VISIBLE BLACK PAINT WILL BE ELIGIBLE FOR PAYMENT.



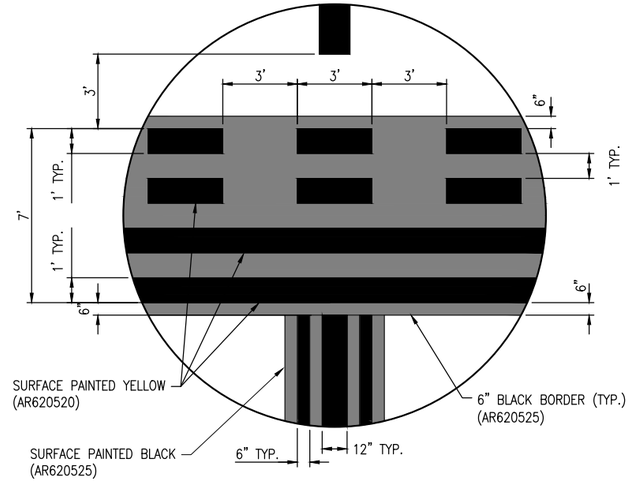
TAXIWAY CENTERLINE DETAIL
NOT TO SCALE



ENHANCED TAXIWAY CENTERLINE MARKING DETAIL (BEGIN)
NOT TO SCALE



ENHANCED TAXIWAY CENTERLINE MARKING DETAIL (END)
NOT TO SCALE



HOLDING POSITION DETAIL
NOT TO SCALE

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

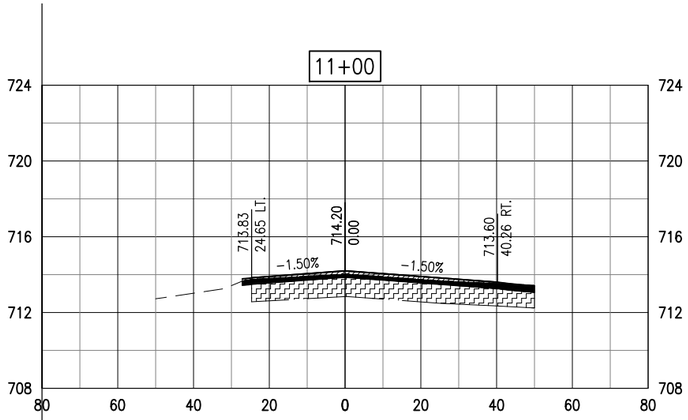
IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

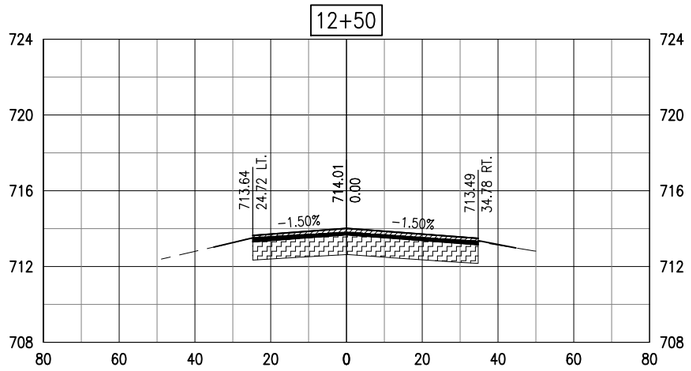
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

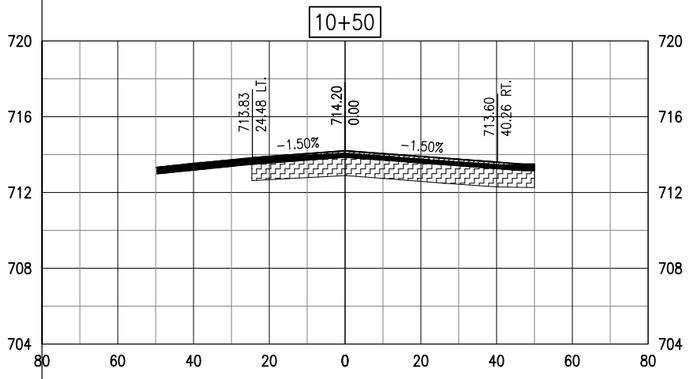
ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-151-MRK.DWG
DESIGN BY: KBS 05/08/2018
DRAWN BY: CWS 05/10/2018
REVIEWED BY: KBS 06/14/2018

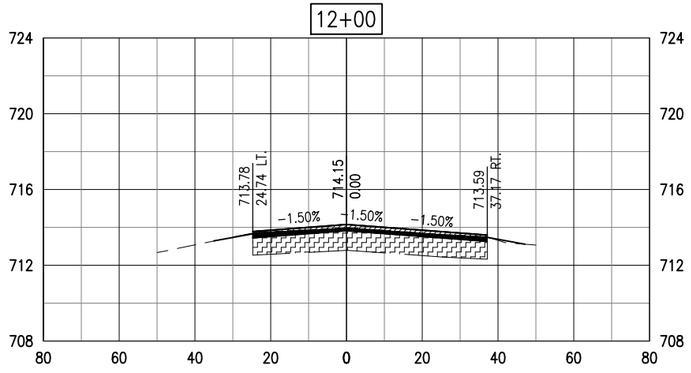
SHEET TITLE

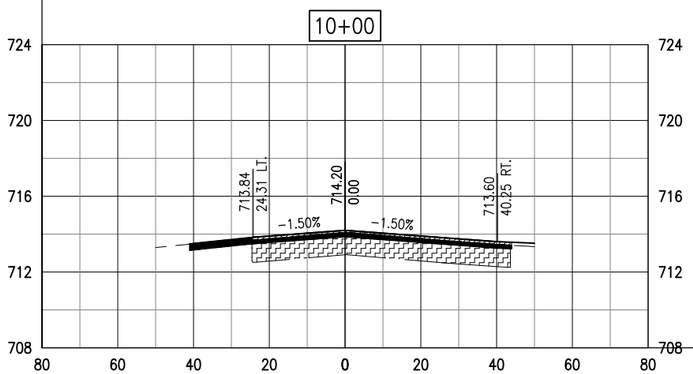
MARKING DETAILS

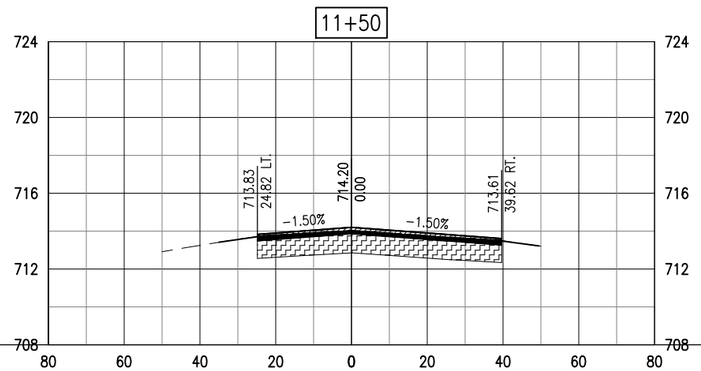












LEGEND

- AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)
- AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
  AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")
- AR501550 – P.C.C. PAVEMENT MILLING
- AR501120 – RUBBILIZE P.C.C. PAVEMENT

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RECONSTRUCT
TAXIWAY B

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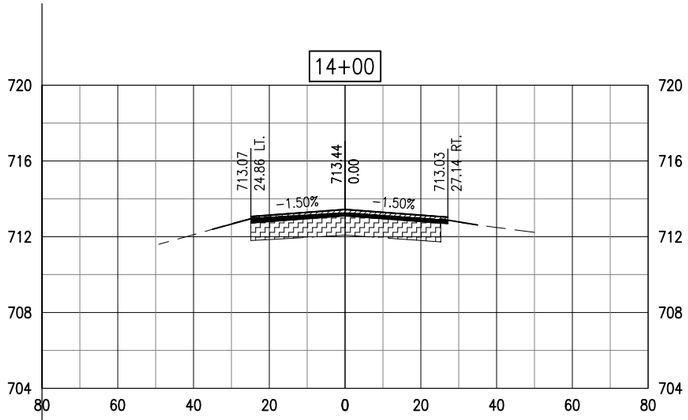
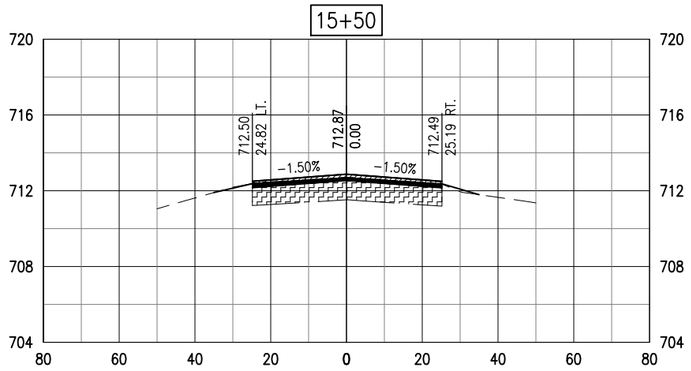
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 1





LEGEND

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AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
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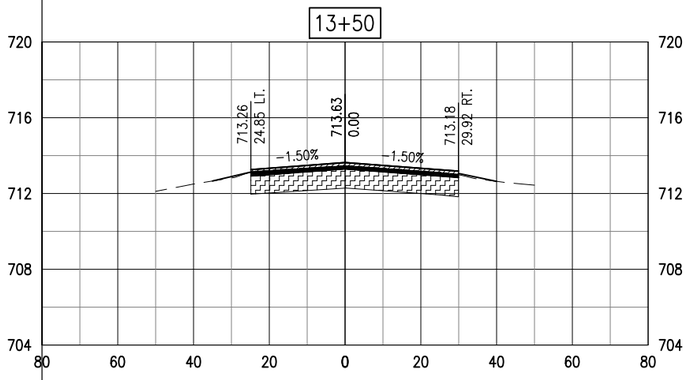
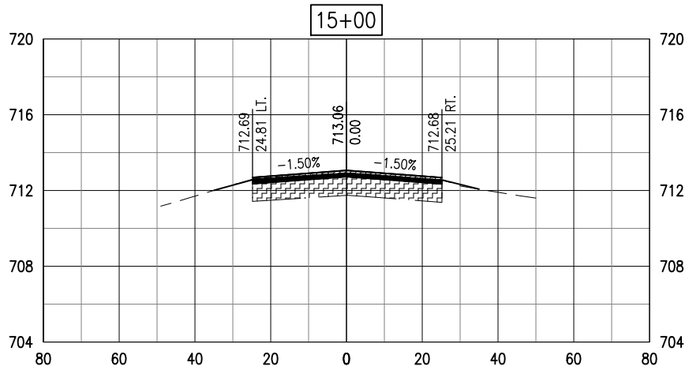
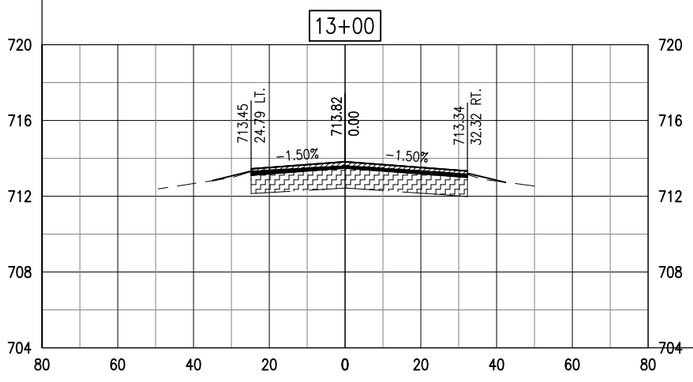
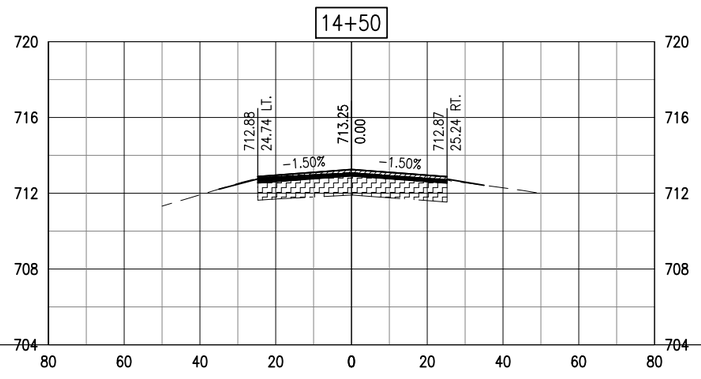
AR501120 – RUBBILIZE P.C.C. PAVEMENT









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TAXIWAY B

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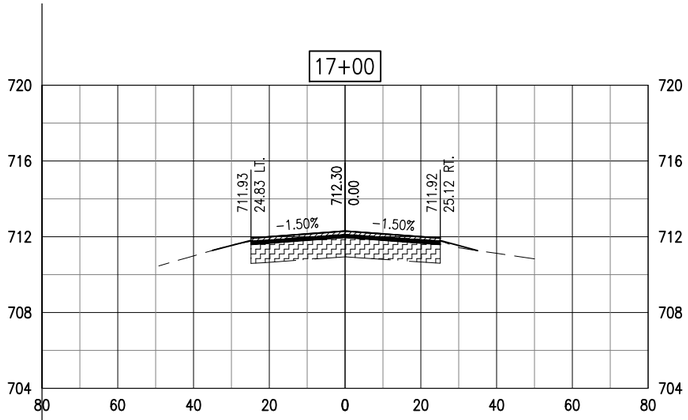
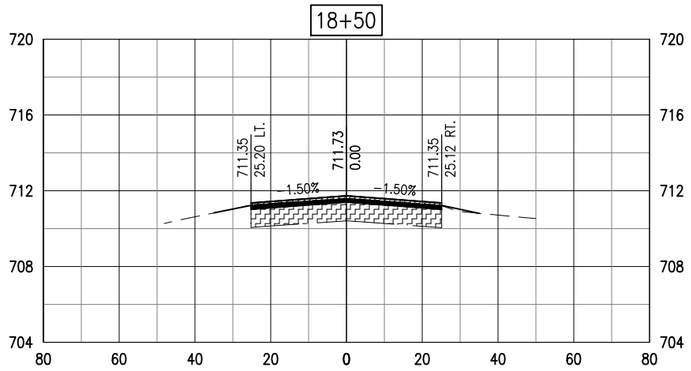
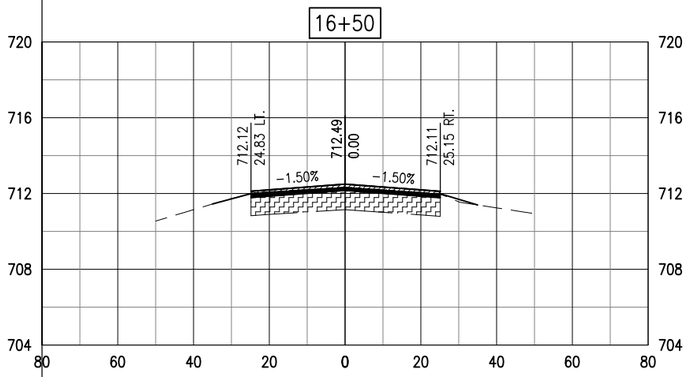
REVIEWED BY: KBS 06/14/2018

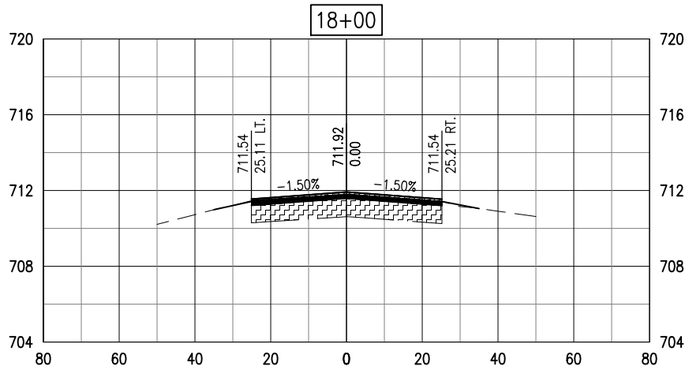
SHEET TITLE

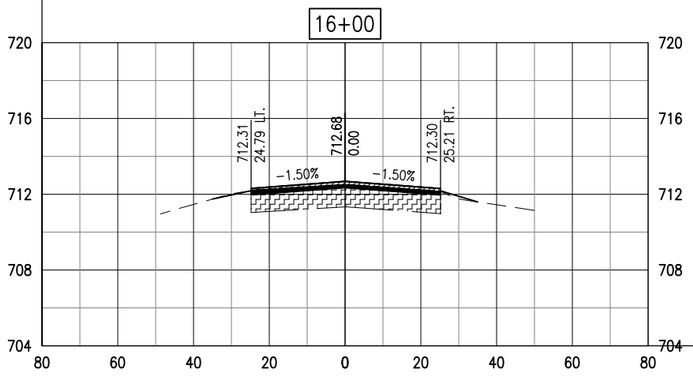
TAXIWAY B CROSS SECTIONS - SHEET 2

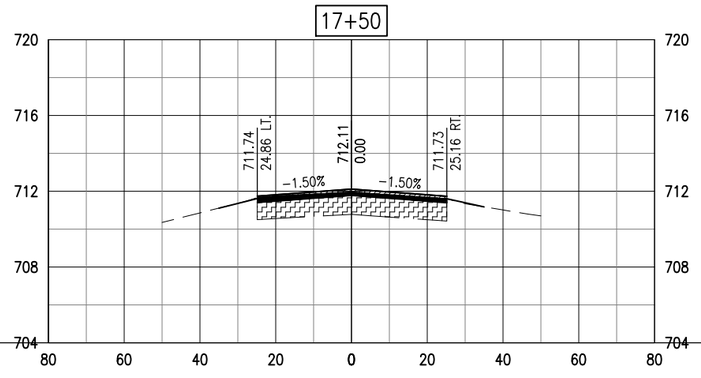












LEGEND

- AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)
- AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
  AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")
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SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 3

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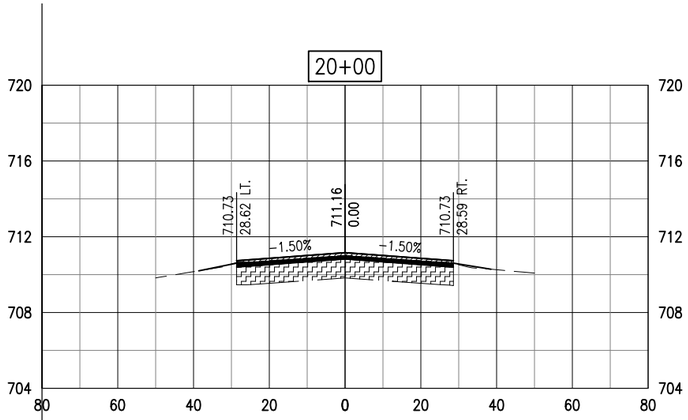

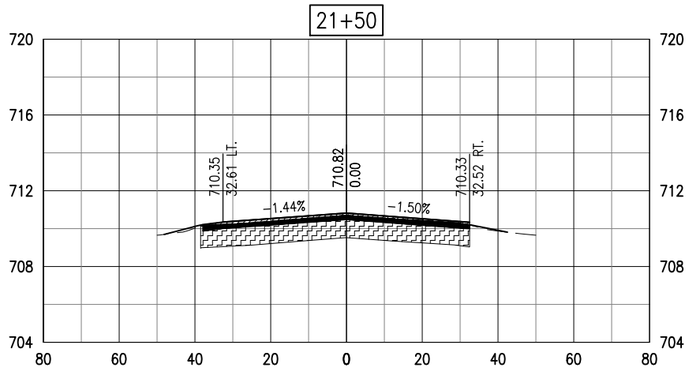

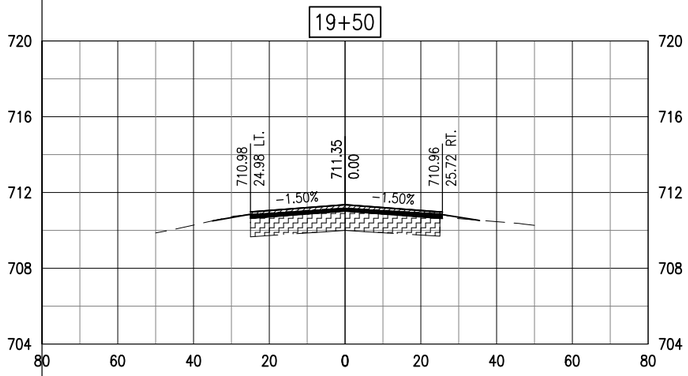

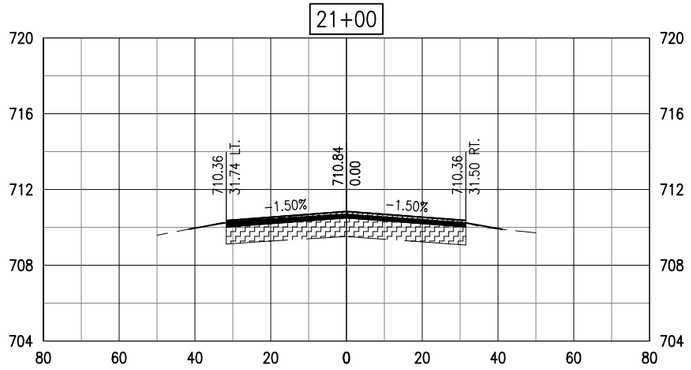

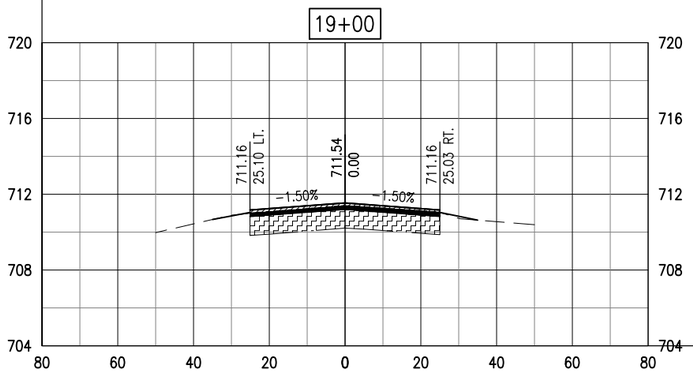

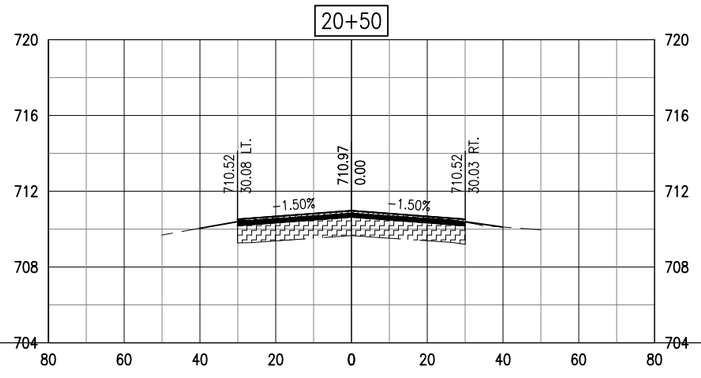

LEGEND

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COLES COUNTY AIRPORT AUTHORITY
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RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678

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DRAWN BY: MLH 05/08/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 4

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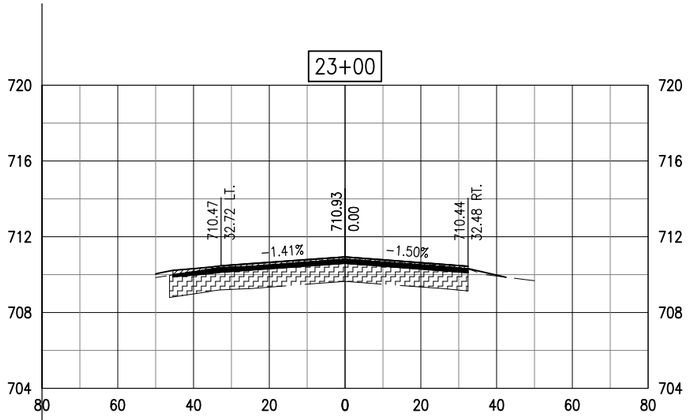

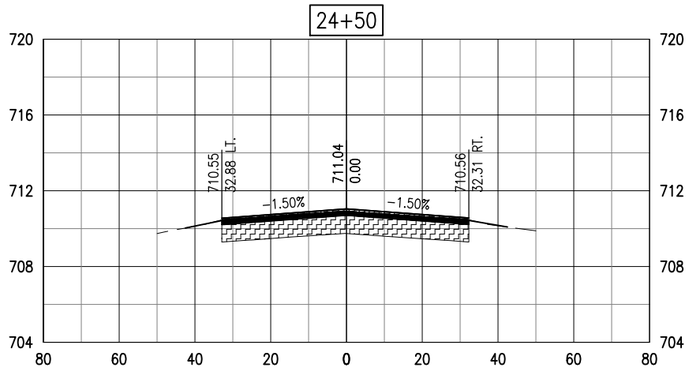

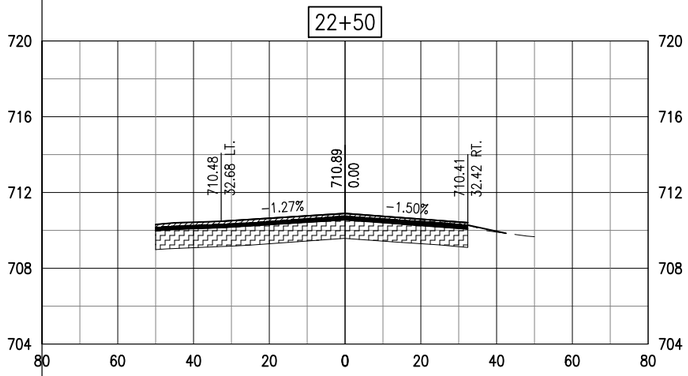

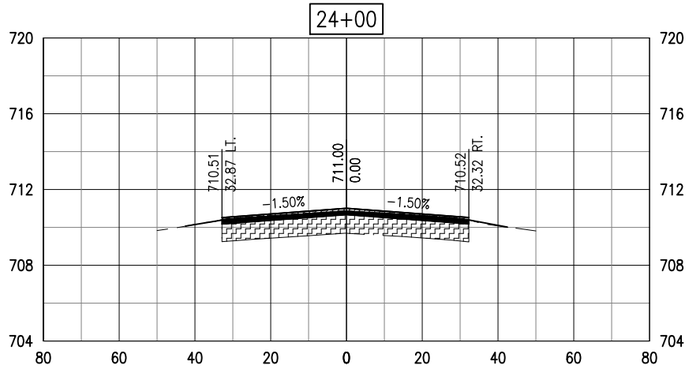

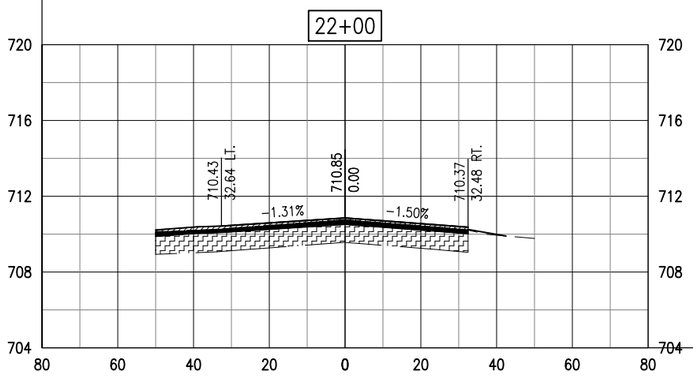

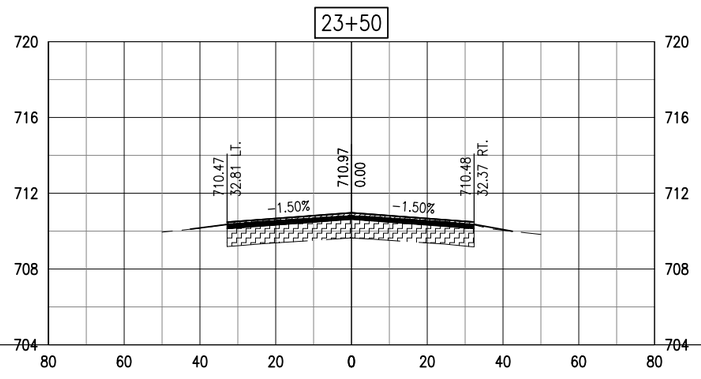

LEGEND

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RECONSTRUCT
TAXIWAY B

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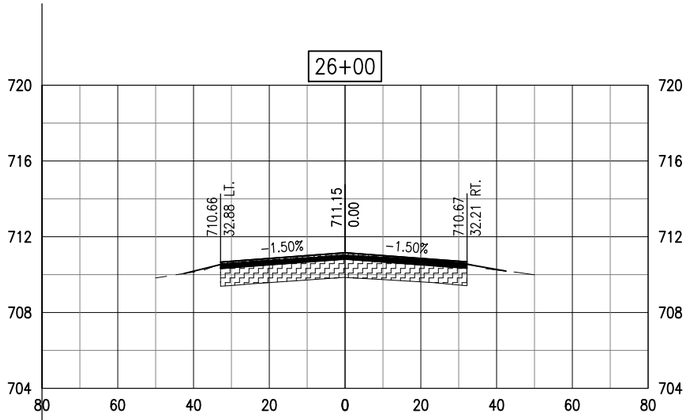
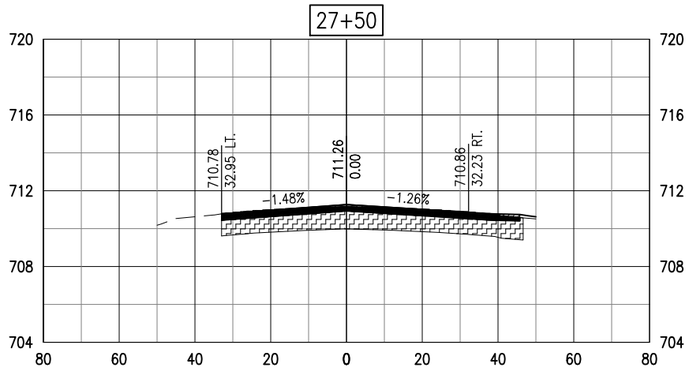
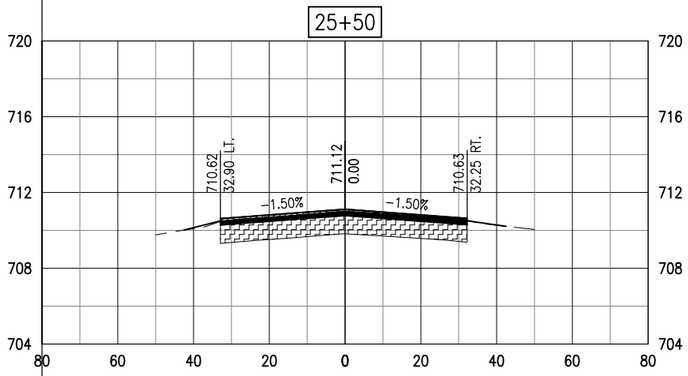
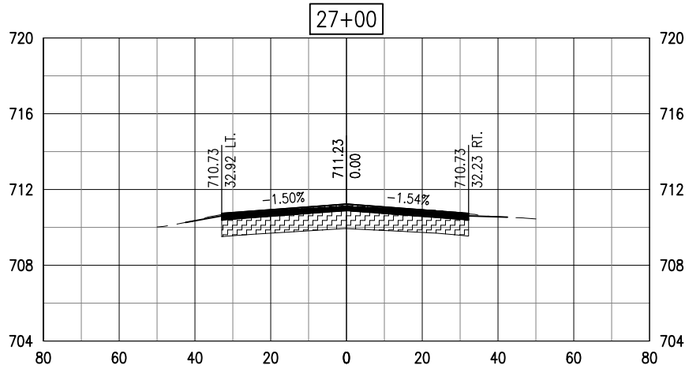
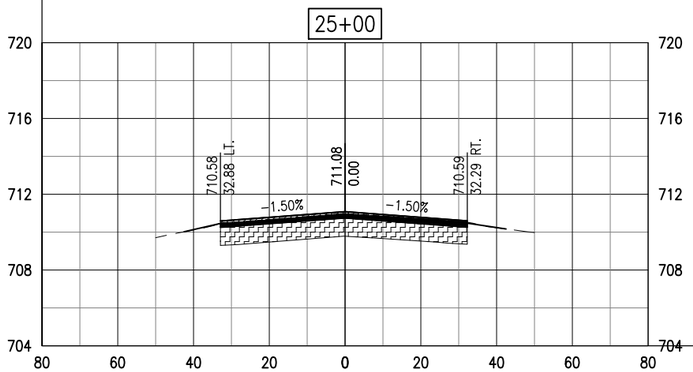
TAXIWAY B CROSS SECTIONS - SHEET 5

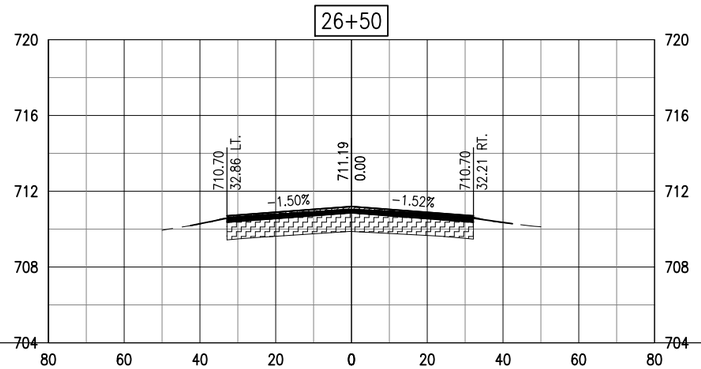












LEGEND

- AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)
- AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
  AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")
- AR501550 – P.C.C. PAVEMENT MILLING
- AR501120 – RUBBILIZE P.C.C. PAVEMENT

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COLES COUNTY AIRPORT AUTHORITY
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RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678

SBG Project No:
3-17-SBGP-TBD

Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: C-301-XS-1.DWG
DESIGN BY: KBS 05/08/2018
DRAWN BY: MLH 05/08/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 6

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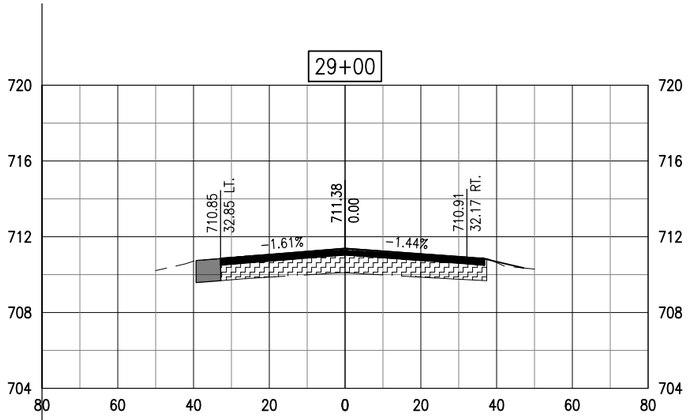

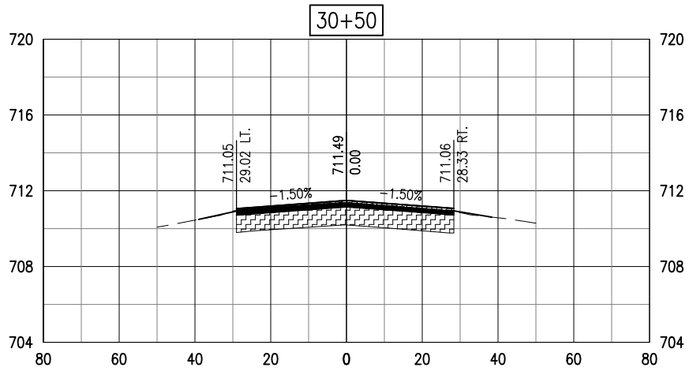

LEGEND

AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)

AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
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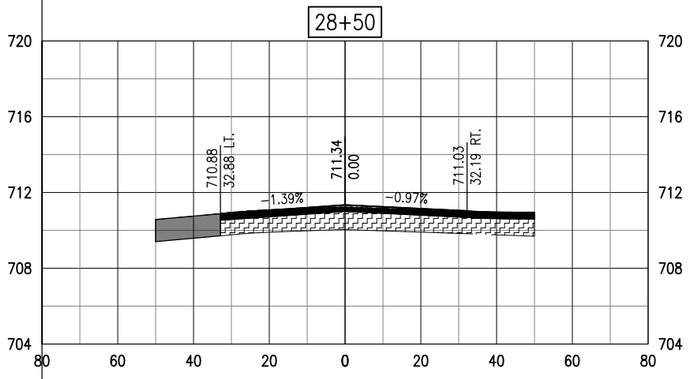
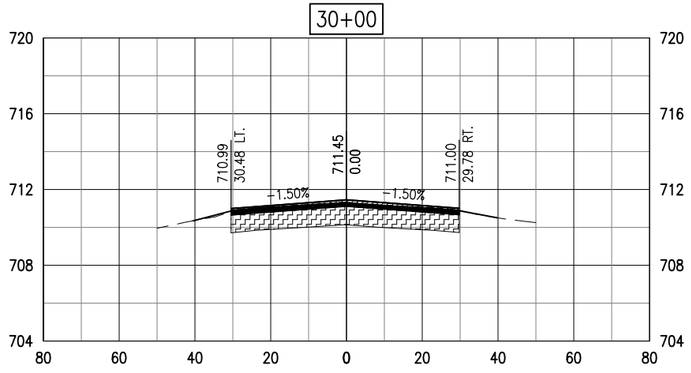
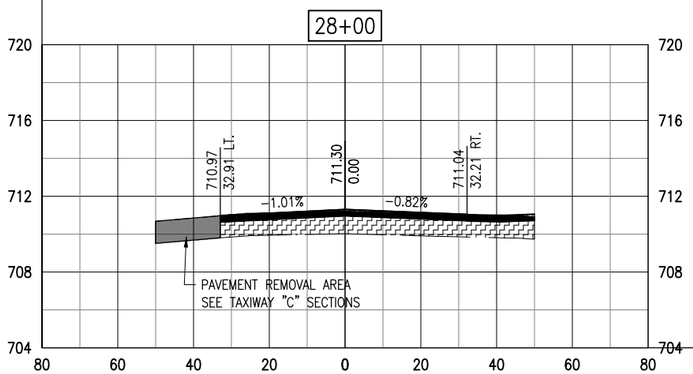
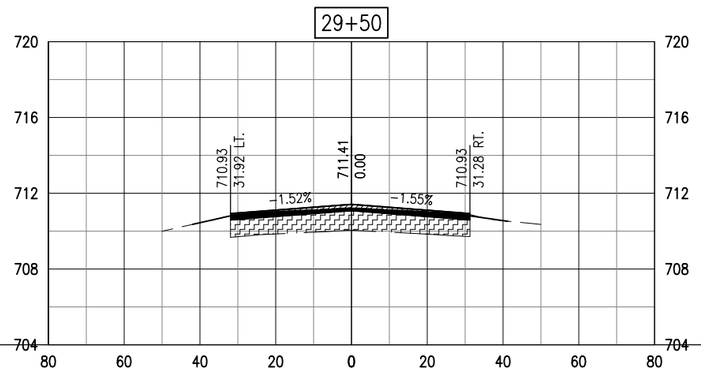
AR501120 – RUBBILIZE P.C.C. PAVEMENT









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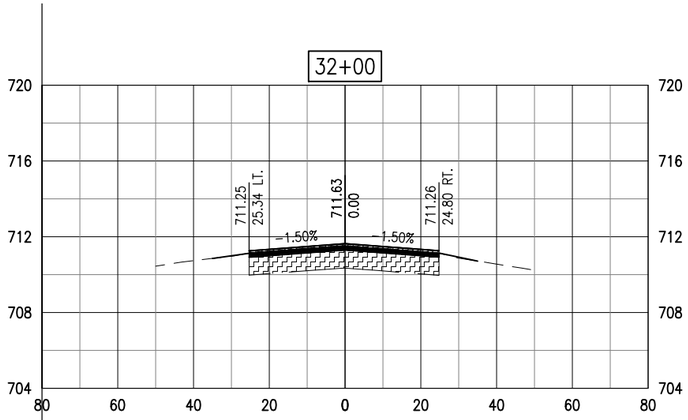
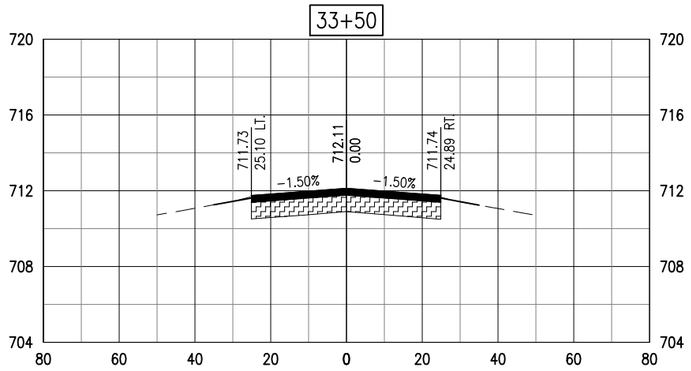
DRAWN BY: MLH 05/08/2018
REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 7





LEGEND

- AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)
- AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
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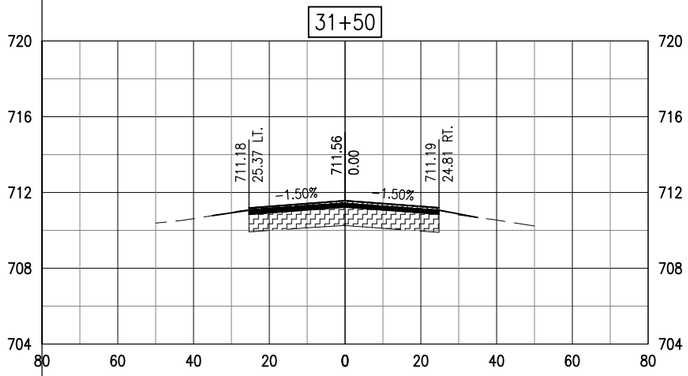
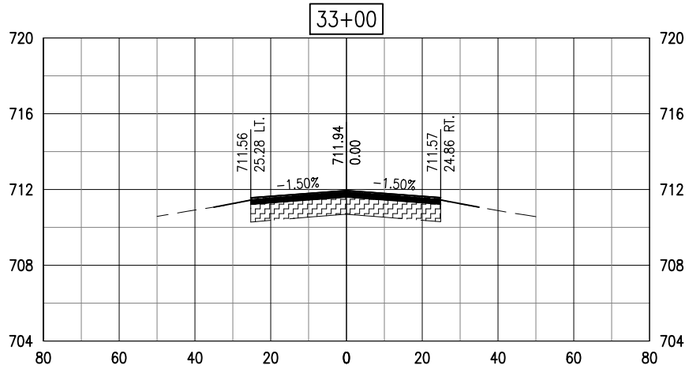
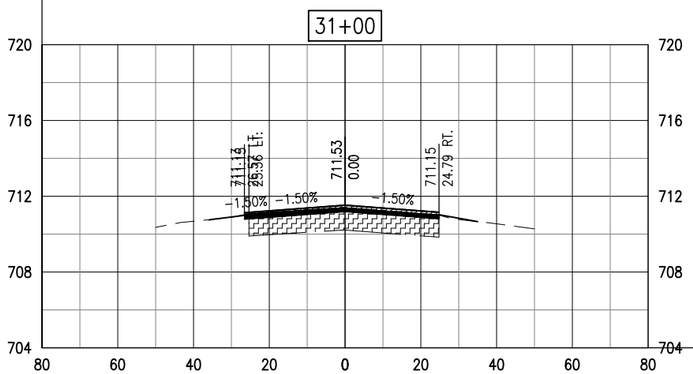
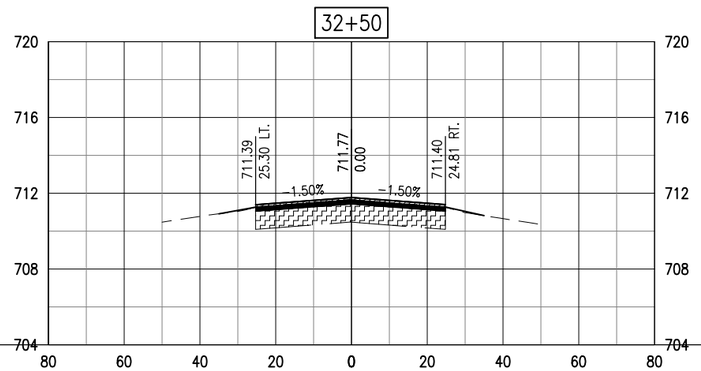
- AR501120 – RUBBILIZE P.C.C. PAVEMENT









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REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 8

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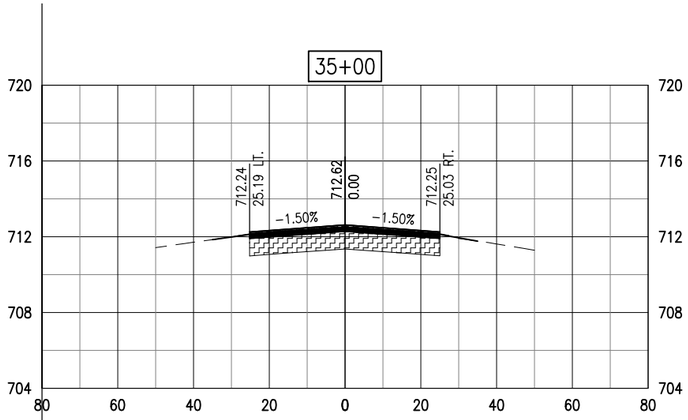

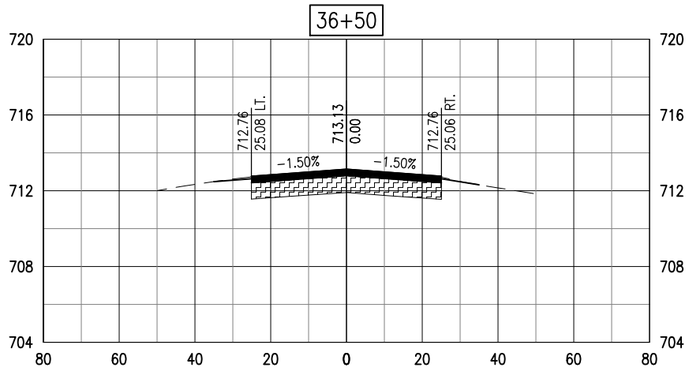

LEGEND

AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)

AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
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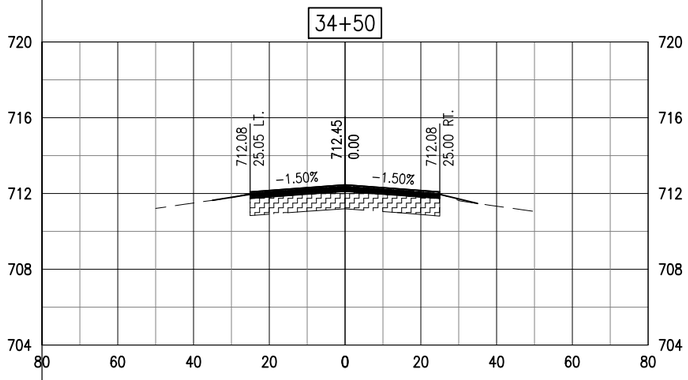
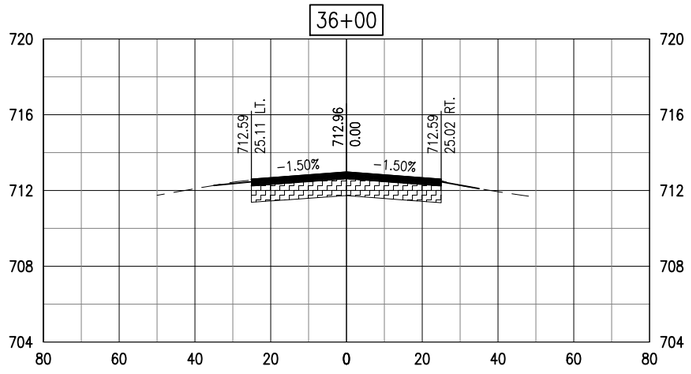
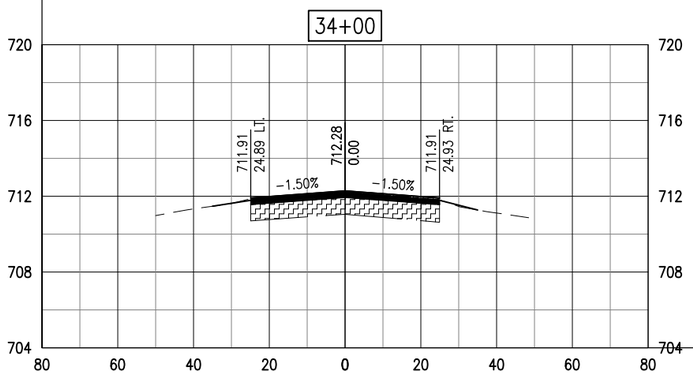
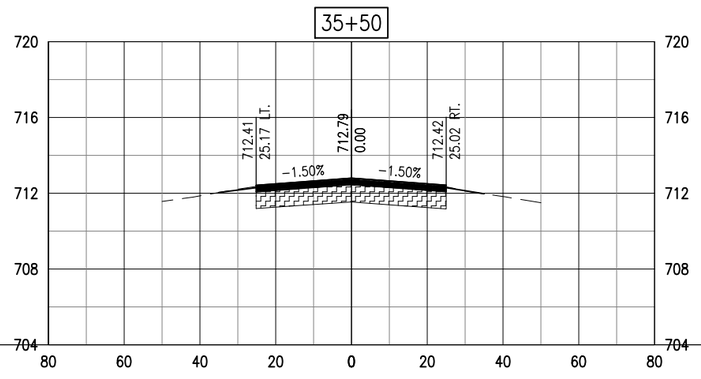
AR501120 – RUBBILIZE P.C.C. PAVEMENT









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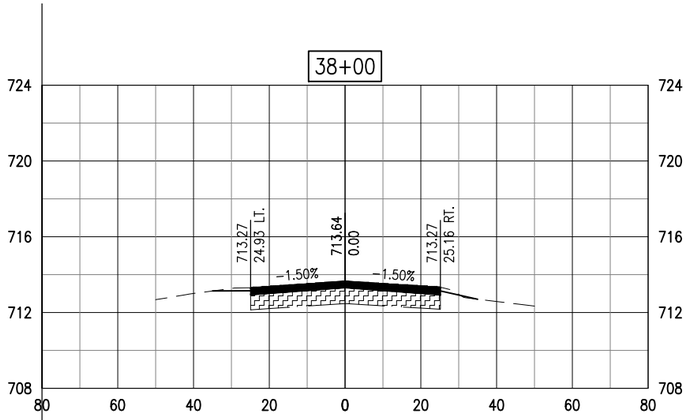
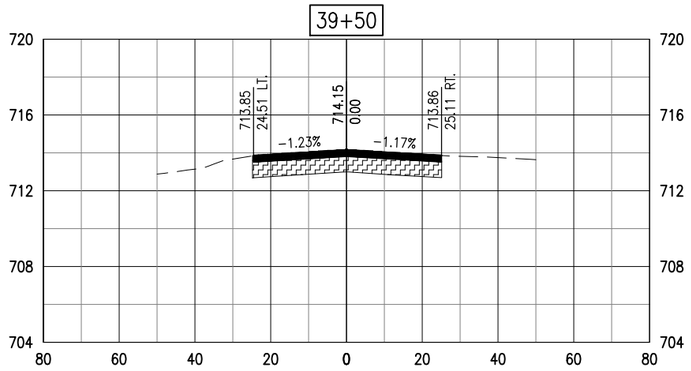
SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 9





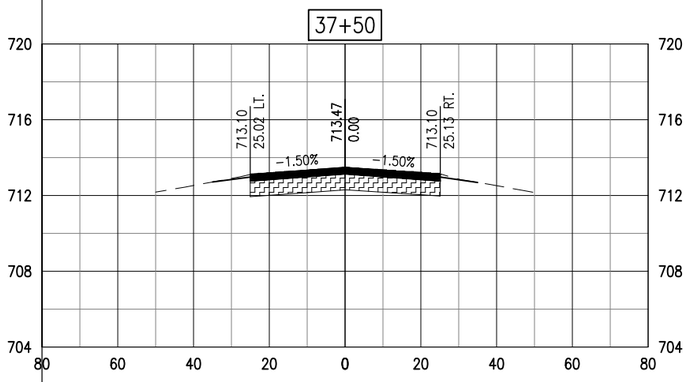
LEGEND

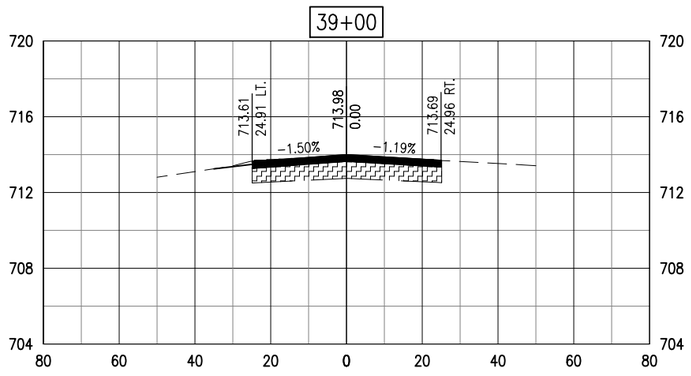
AR501900 – REMOVE P.C.C. PAVEMENT (INCLUDING BITUMINOUS BASE)

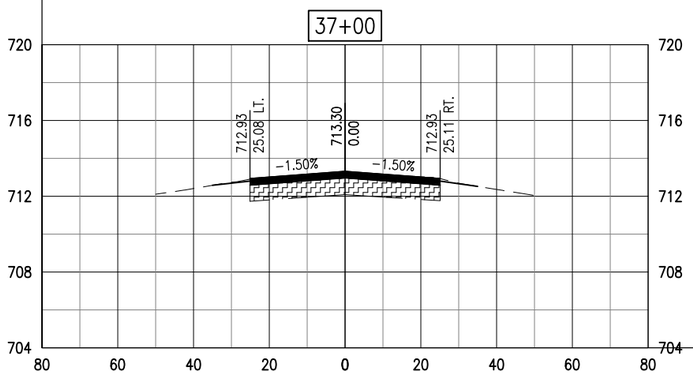
AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")

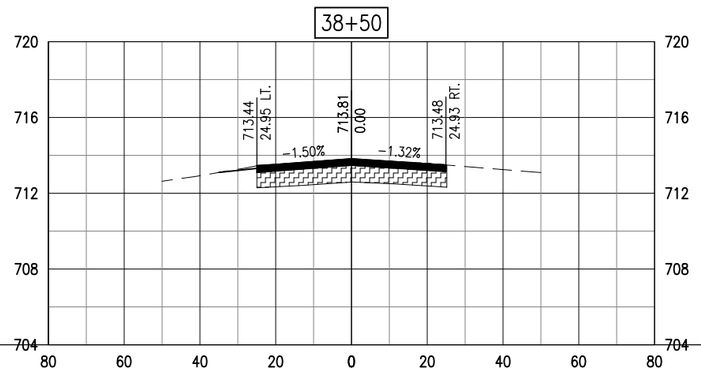
AR501550 – P.C.C. PAVEMENT MILLING

AR501120 – RUBBILIZE P.C.C. PAVEMENT









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IDA No: MTO-4678

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SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 10

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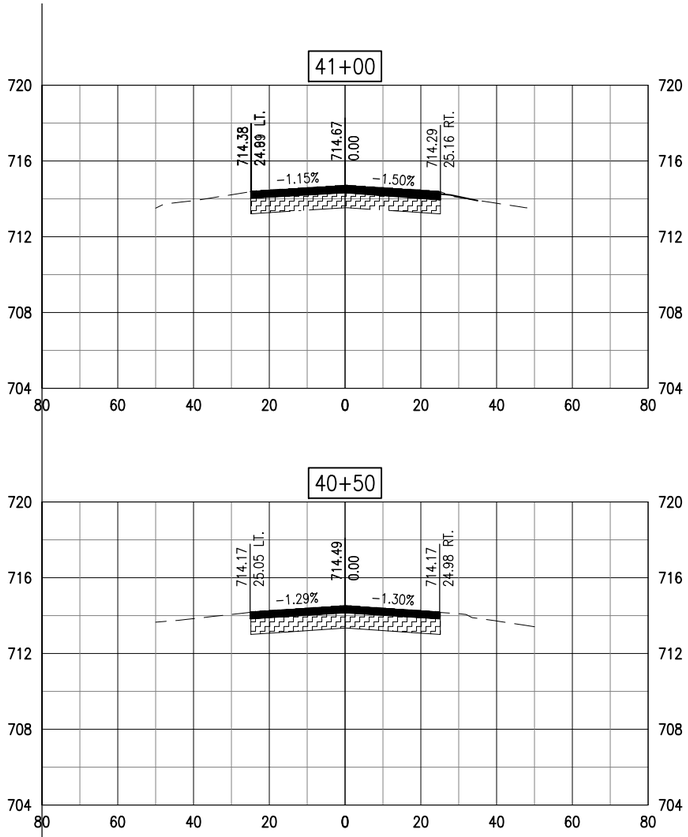

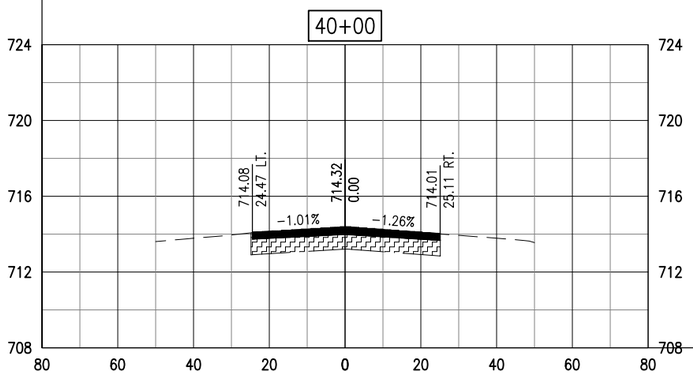

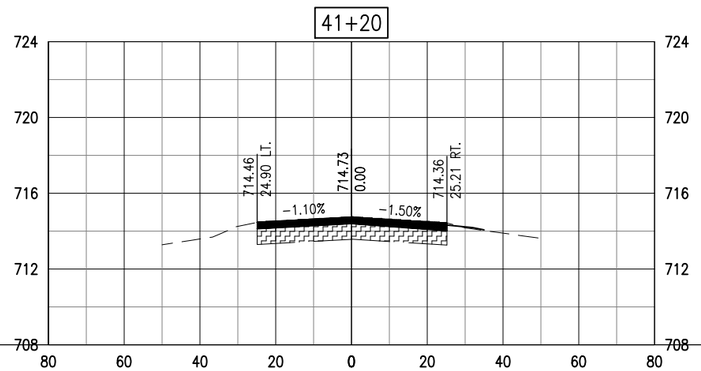

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- AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
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- AR501120 – RUBBILIZE P.C.C. PAVEMENT

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REVIEWED BY: KBS 06/14/2018

SHEET TITLE

TAXIWAY B CROSS SECTIONS - SHEET 11

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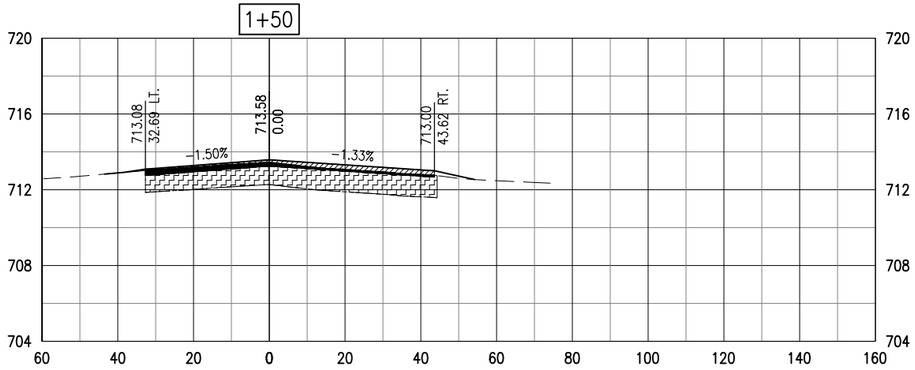

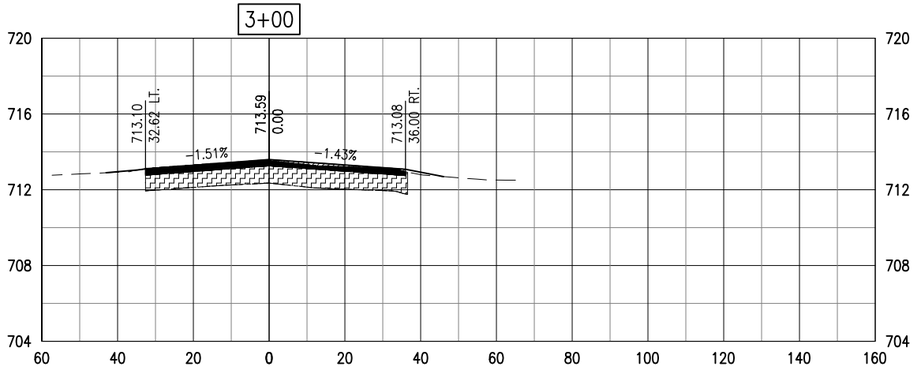

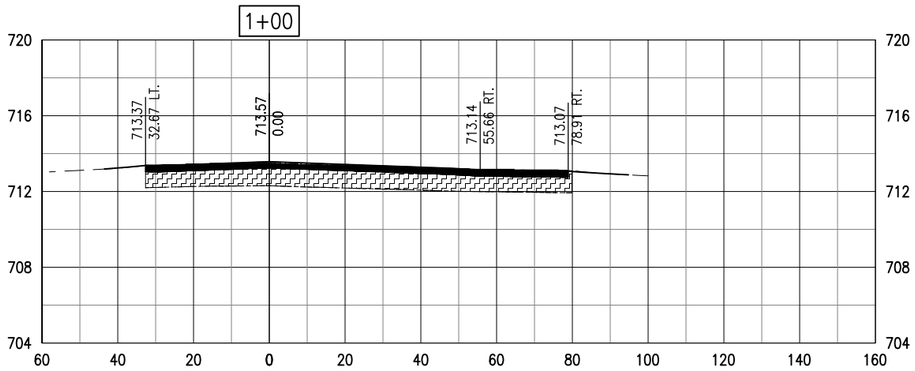

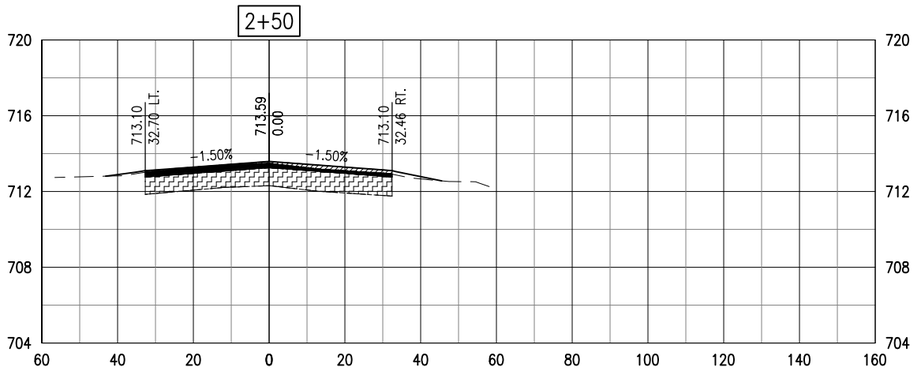

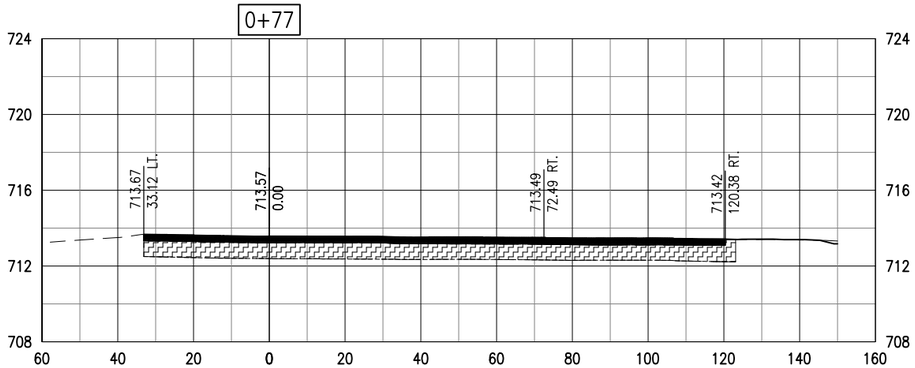

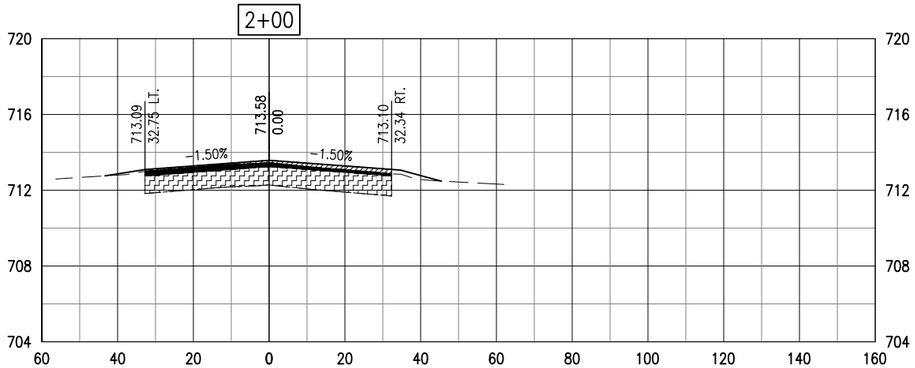

LEGEND

AR401614 – BITUMINOUS SURFACE COURSE, METHOD 2, SUPERPAVE (2")
AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")

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REVIEWED BY: KBS 06/14/2018

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TAXIWAY B1 CROSS SECTIONS - SHEET 1

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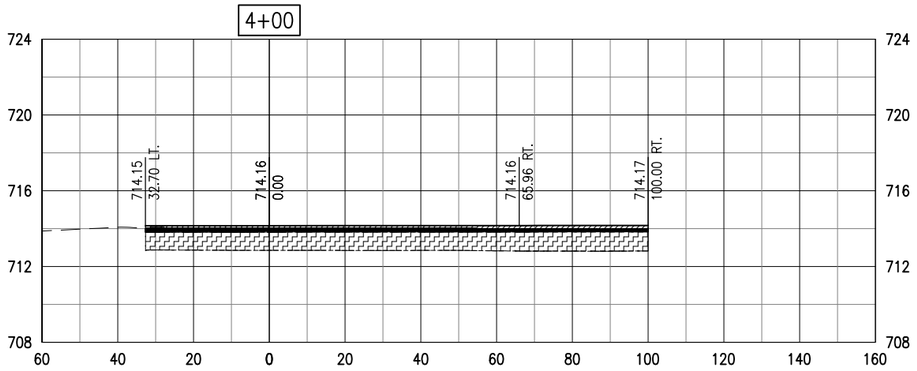

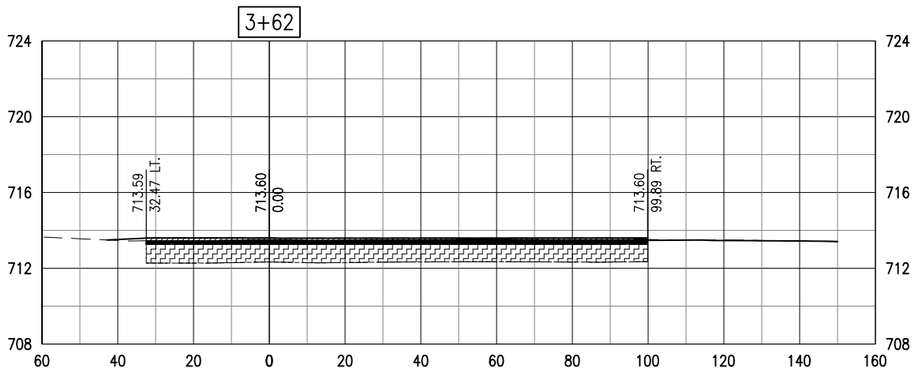

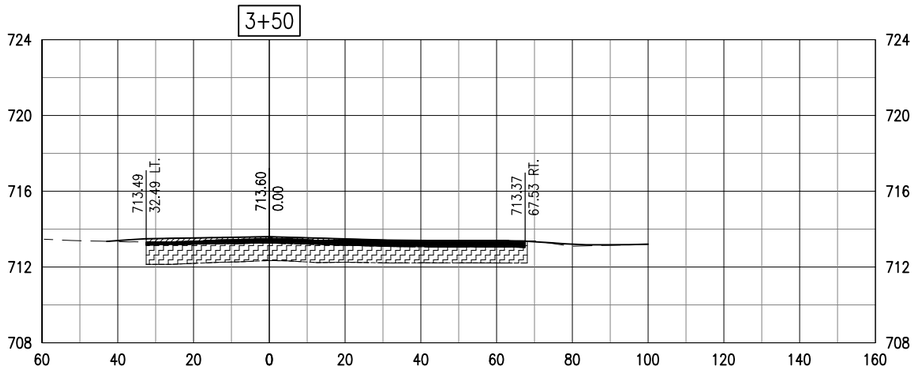

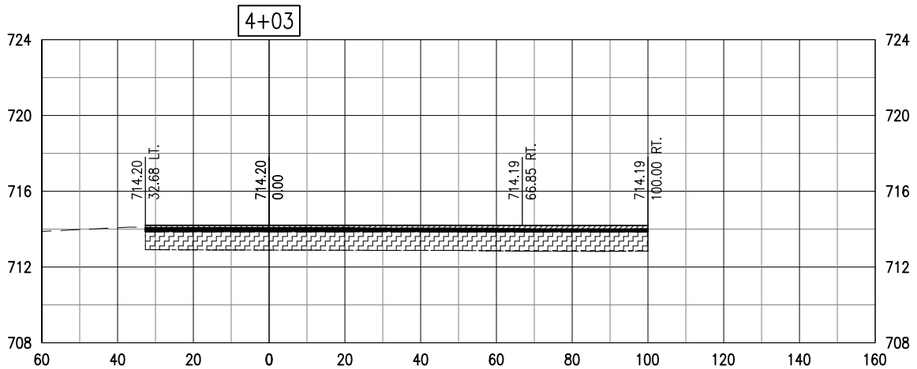

LEGEND

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AR403614 – BITUMINOUS BASE COURSE, METHOD 2, SUPERPAVE (2")

AR501550 – P.C.C. PAVEMENT MILLING

AR501120 – RUBBILIZE P.C.C. PAVEMENT

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678
SBG Project No:
3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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REVIEWED BY: KBS 06/14/2018

SHEET TITLE

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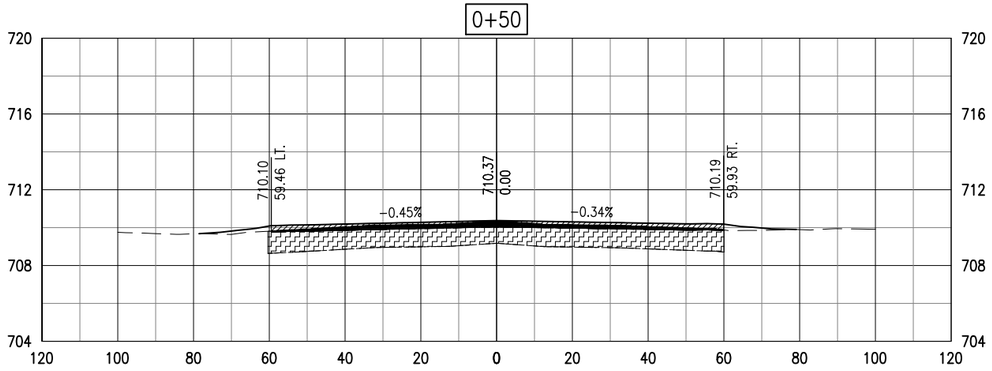

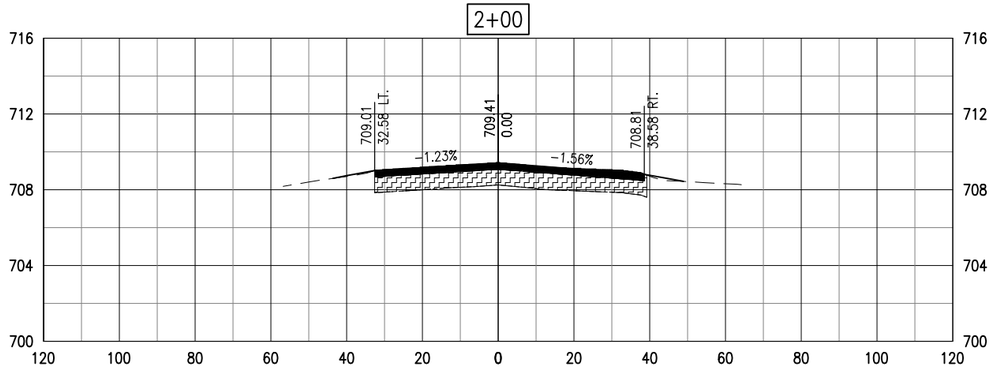

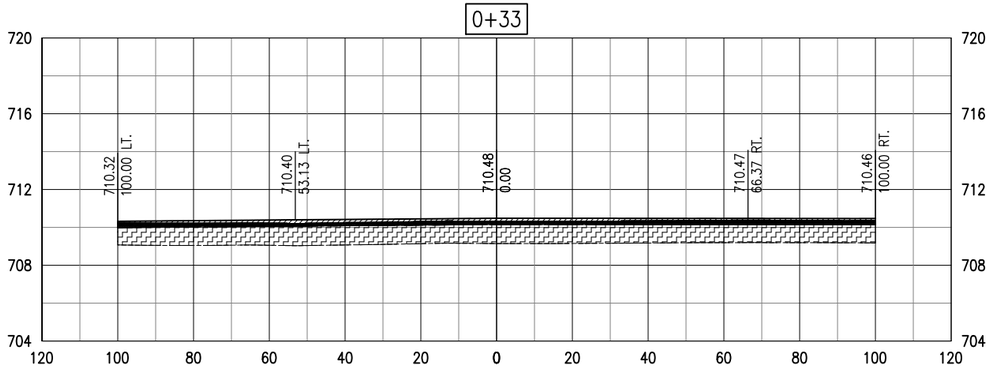

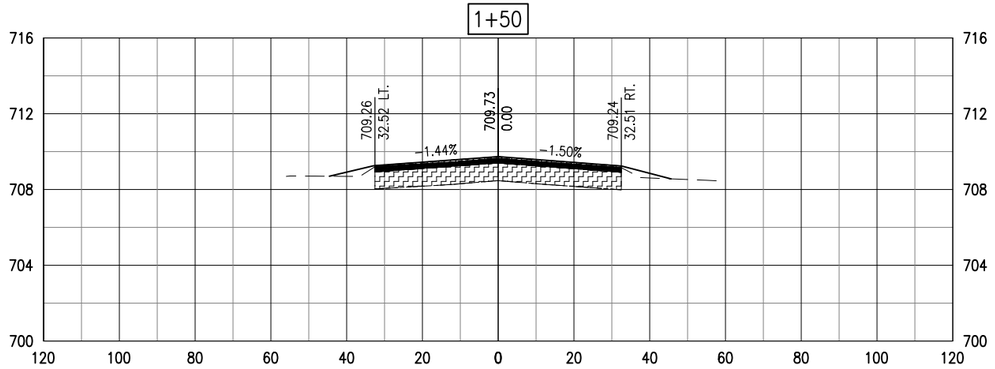

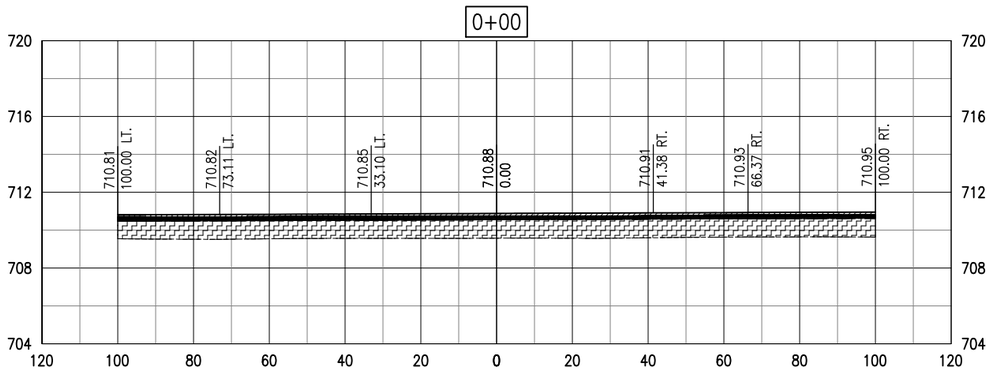

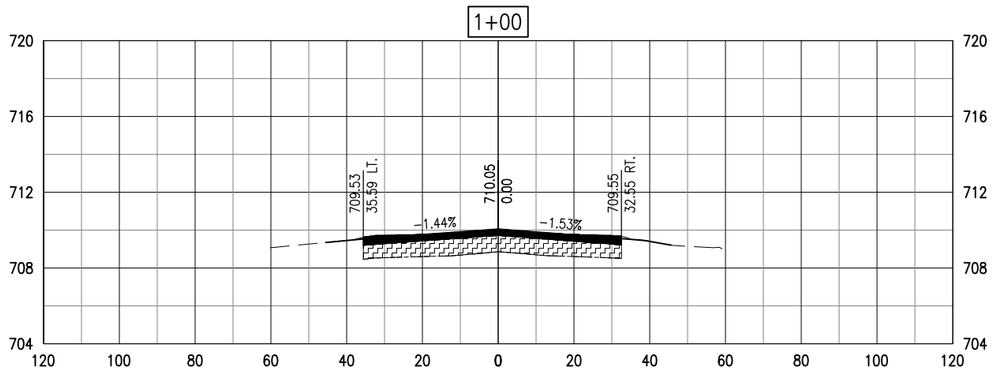

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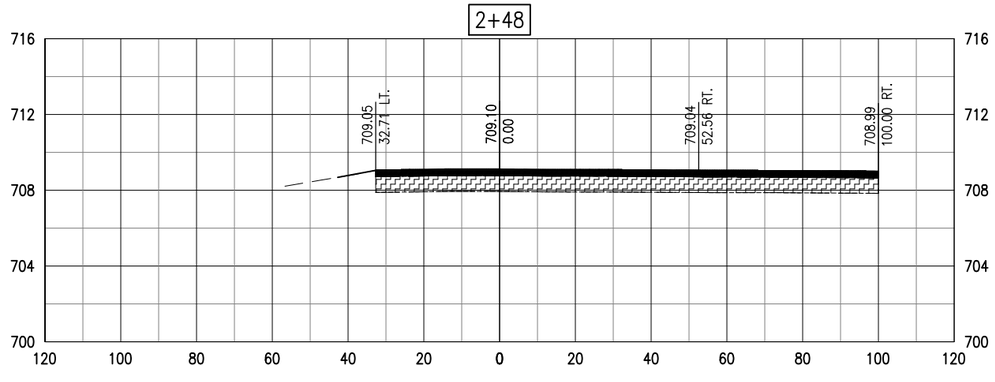

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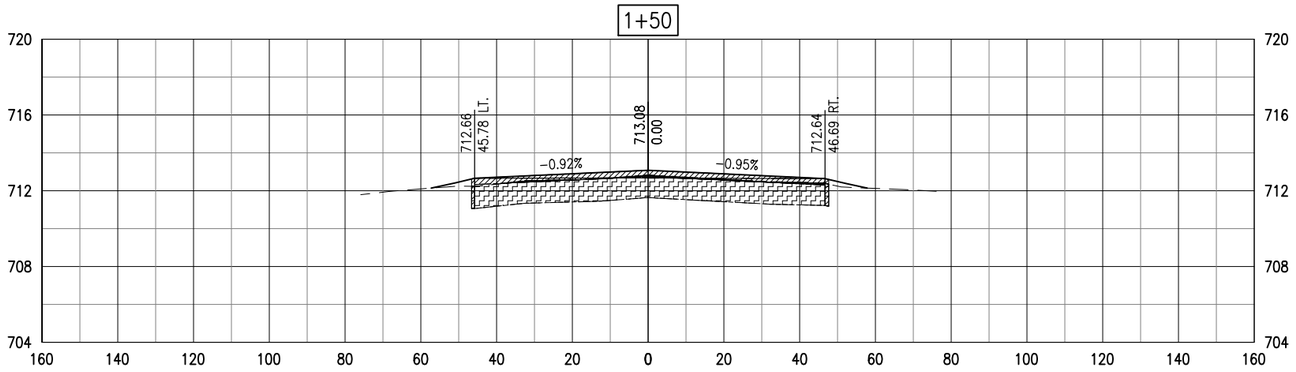

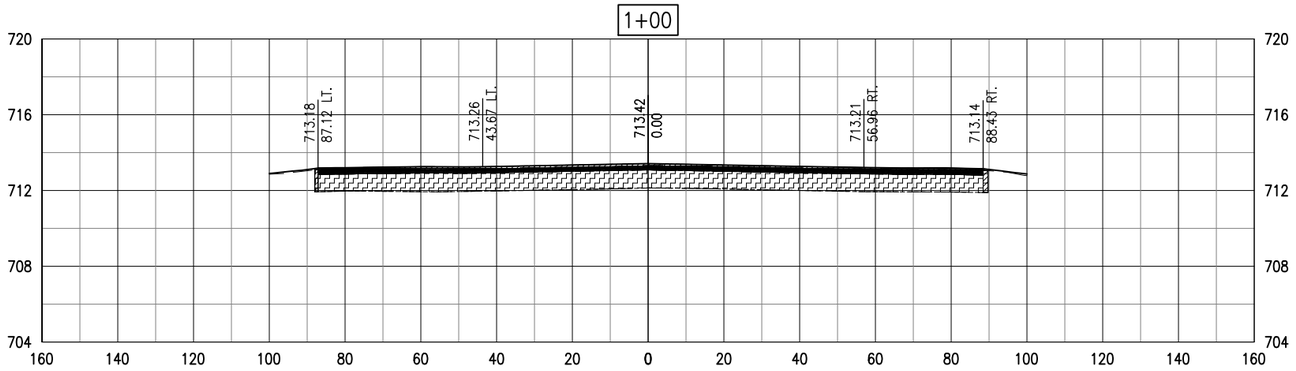

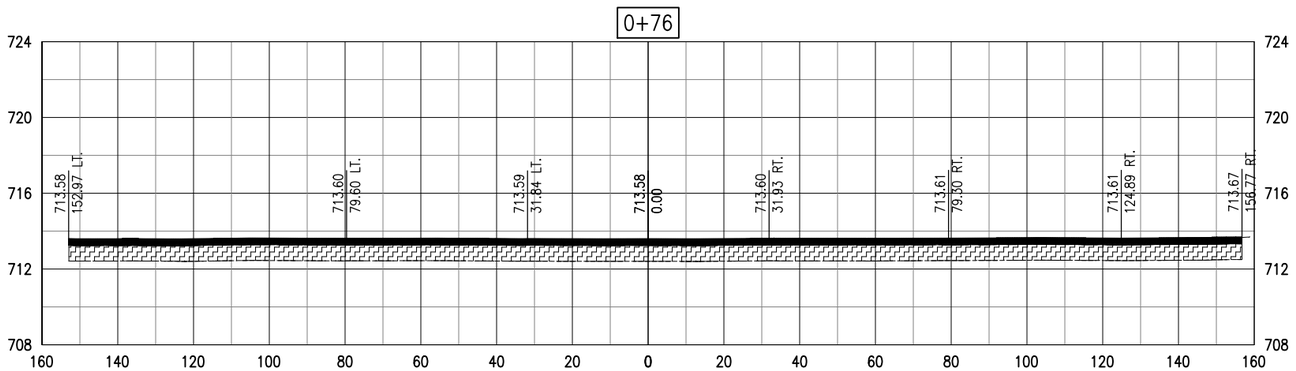

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TAXWAY C CROSS
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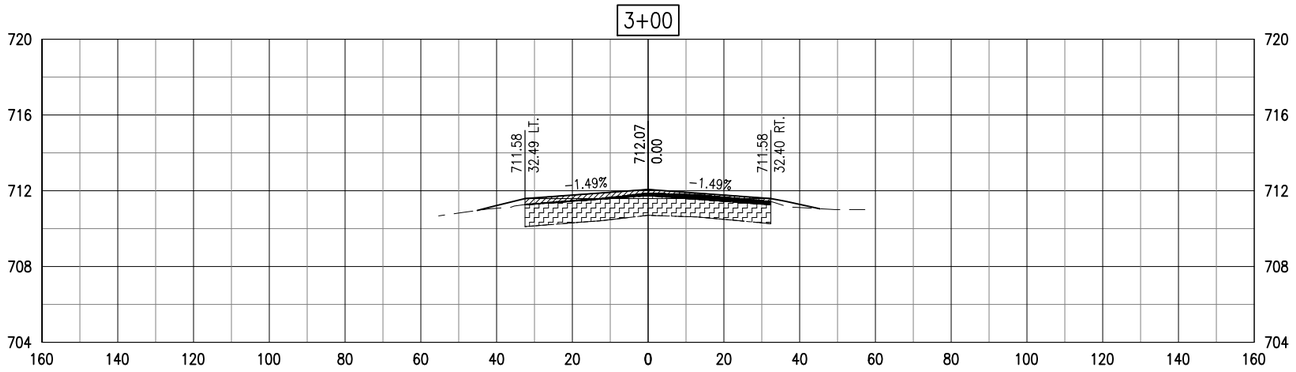

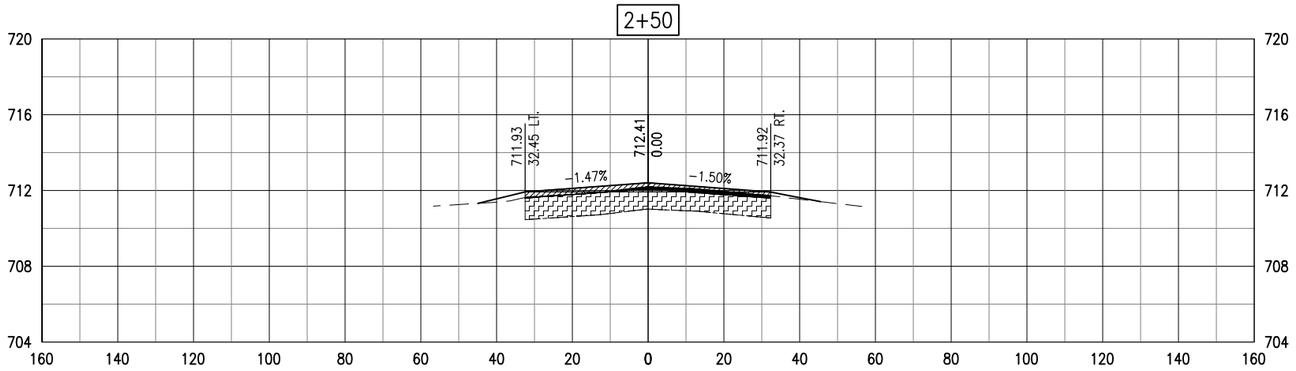

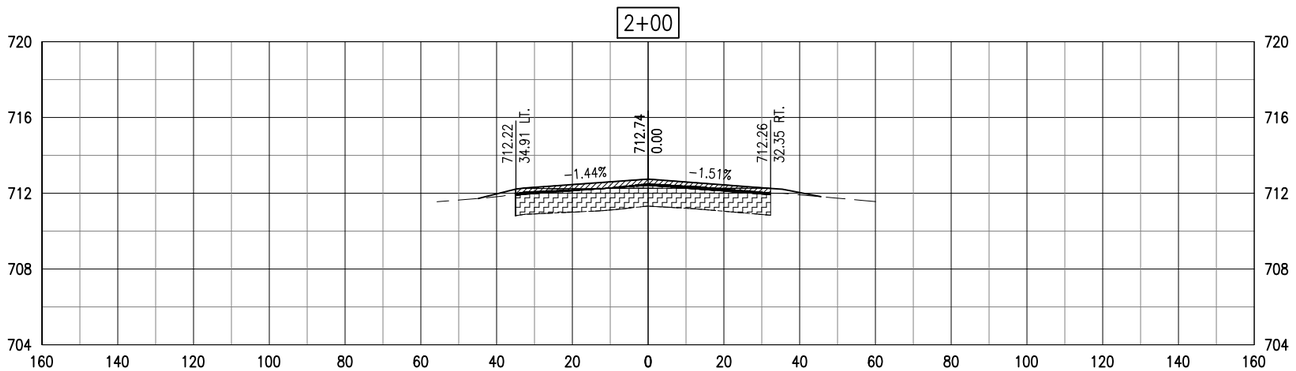

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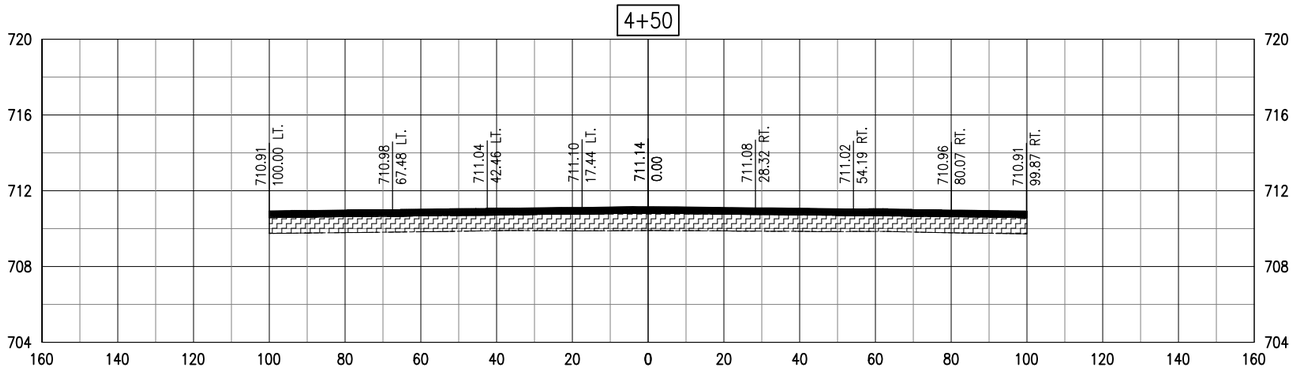

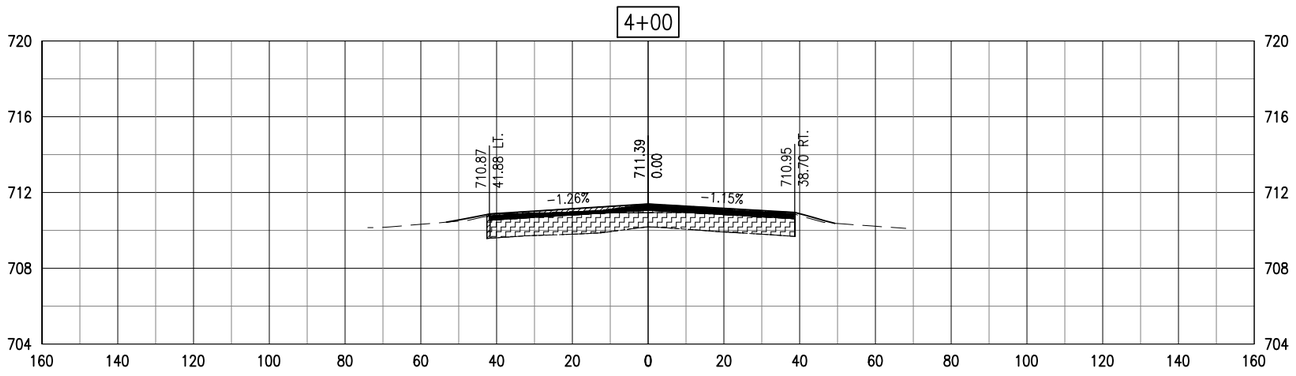

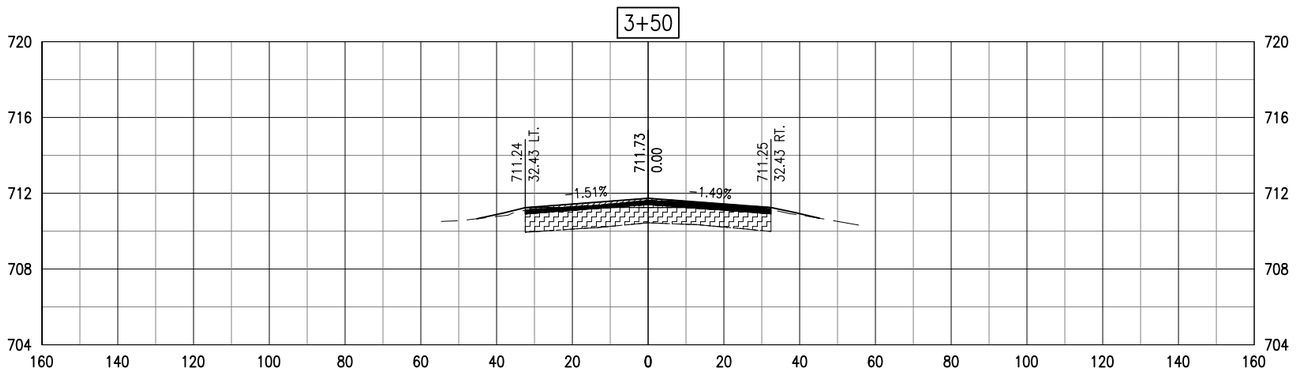

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SECTIONS - SHEET 3

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AIRFIELD LIGHTING ADJUSTMENT AND INSTALLATION NOTES

1. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR/MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).

2. CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING OR DISCONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICE.

3. CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".

4. CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E – STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.

5. ALL ELECTRICAL EQUIPMENT (INCLUDING AIRFIELD LIGHTING AND NAVAIDS) SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 – NATIONAL ELECTRIC CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS, AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERTEK TESTING SERVICES VERIFICATION/ ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.

6. PRIOR TO BEGINNING AIRFIELD LIGHTING MODIFICATIONS, CABLE OR DUCT INSTALLATION, AND/OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS, ALL EXISTING SERIES CIRCUIT CABLES SHALL BE MEGGER TESTED WITH AN INSULATION RESISTANCE TESTER AND RECORDED AT THE VAULT. ALL EXISTING SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE MEASURED WITH AN OHMMETER AND RECORDED FOR EACH CIRCUIT AT THE VAULT. EACH CONSTANT CURRENT REGULATOR SHALL BE TESTED WITH RESULTS RECORDED. PROVIDE A TRUE RMS AMMETER FOR CURRENT MEASUREMENTS. COPIES OF TEST RESULTS SHALL BE PROVIDED TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AND THE RESPECTIVE PROJECT ENGINEER WITHIN 5 BUSINESS DAYS OF CONDUCTING THE RESPECTIVE SET OF TESTS. SEE THE TESTING FORMS IN APPENDIX A, OF THE SPECIAL PROVISION SPECIFICATIONS.

7. AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS HAVE BEEN COMPLETED, SERIES CIRCUIT CABLES SHALL BE MEGGER TESTED WITH AN INSULATION RESISTANCE TESTER AND RECORDED AT THE VAULT. ALL SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE MEASURED WITH AN OHMMETER AND RECORDED FOR EACH CIRCUIT AT THE VAULT. EACH CONSTANT CURRENT REGULATOR SHALL BE TESTED WITH RESULTS RECORDED. PROVIDE A TRUE RMS AMMETER FOR CURRENT MEASUREMENTS. COPIES OF TEST RESULTS SHALL BE PROVIDED TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AND THE RESPECTIVE PROJECT ENGINEER WITHIN 5 BUSINESS DAYS OF CONDUCTING THE RESPECTIVE SET OF TESTS. SEE THE TESTING FORMS IN APPENDIX A, OF THE SPECIAL PROVISION SPECIFICATIONS.

8. INSULATION RESISTANCE TESTING EQUIPMENT FOR USE WITH 5,000 VOLT SERIES CIRCUIT CABLES SHALL USE AN INSULATION RESISTANCE TESTER CAPABLE OF TESTING THE CABLES AT 5,000 VOLTS. OLDER SERIES CIRCUIT CABLES AND/OR CABLES IN POOR CONDITION MAY REQUIRE THE TEST VOLTAGE TO BE PERFORMED AT A VOLTAGE LOWER THAN 5,000 VOLTS (EXAMPLE 1,000 VOLTS, 500 VOLTS, OR LESS THAN 500 VOLTS). THE RESPECTIVE TEST VOLTAGE SHALL BE RECORDED FOR EACH CABLE INSULATION RESISTANCE TEST RESULT. INSULATION RESISTANCE TESTING EQUIPMENT FOR USE WITH 600 VOLT RATED CABLES SHALL USE A 500 VOLT INSULATION RESISTANCE TESTER. THE RESPECTIVE TEST VOLTAGE SHALL BE RECORDED FOR EACH CABLE INSULATION RESISTANCE TEST RESULT.

9. EXISTING AIRFIELD LIGHITNG, DUCTS, CONDUITS, CABLES, SPLICE CANS, HANDHOLES, AND/OR MANHOLES SCHEDULED TO REMAIN SHALL BE PROTECTED FROM DAMAGE. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING FACILITIES AND COORDINATE THE PAVEMENT REMOVAL WORK TO AVOID AND DAMAGE TO AIRFIELD LIGHTING SYSTEMS AND OTHER FACILITIES.

10. IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED CABLE AND DUCT CROSSES AN EXISTING CABLE, THE CONTRACTOR IS REQUIRED TO HAND DIG THE TRENCH NECESSARY FOR THE PROPOSED CABLE AND DUCT. AT OTHER LOCATIONS, THE PROPOSED CABLE AND DUCT MAY BE TRENCHED OR PLOWED INTO PLACE. HAND DIGGING, TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED CABLES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ONLY CABLE IN DUCT OR UNIT DUCT SHALL BE INSTALLED BY PLOWING METHOD.

11. CONCRETE STEEL REINFORCEMENT SHALL BE TYPE ASTM A615 OR A706 GRADE 60. ALL REINFORCEMENT SHALL HAVE A 3" MINIMUM CONCRETE COVER. REINFORCEMENT MAY BE ADJUSTED TO MISS INTERFERENCES. CONCRETE SHALL CONFORM TO ITEM 610 STRUCTURAL PORTLAND CEMENT CONCRETE.

12. RUNWAY LIGHTING CIRCUITS SHALL BE ACTIVE AT THE END OF EACH CONSTRUCTION DAY FOR AN OPEN RUNWAY. THE CONTRACTOR SHALL PROVIDE TEMPORARY CABLE & CONNECTIONS WHERE NECESSARY TO MAINTAIN A RUNWAY OR TAXIWAY LIGHTING SYSTEM. TEMPORARY CABLE SHALL BE 1/C #8 FAA L-824 5KV UG CABLE IN DUCT OR UNIT DUCT

13. ALL ABOVEGROUND JUMPERS SHALL BE IN A DUCT WITH ALL CONNECTIONS SEALED. THE CONTRACTOR SHALL SECURE, IDENTIFY AND PLACE ALL TEMPORARY EXPOSED WIRING IN CONDUIT, DUCT, OR UNIT DUCT TO PREVENT ELECTROCUTION AND FIRE IGNITION SOURCES AS PER THE REQUIREMENTS OF FAA 150/5370-2G, OPERATION SAFETY ON AIRPORTS DURING CONSTRUCTION, SECTION 2.18.3 "LIGHTING AND VISUAL NAVAIDS". ALL LABOR, MATERIALS, AND TIME NECESSARY TO COMPLY WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

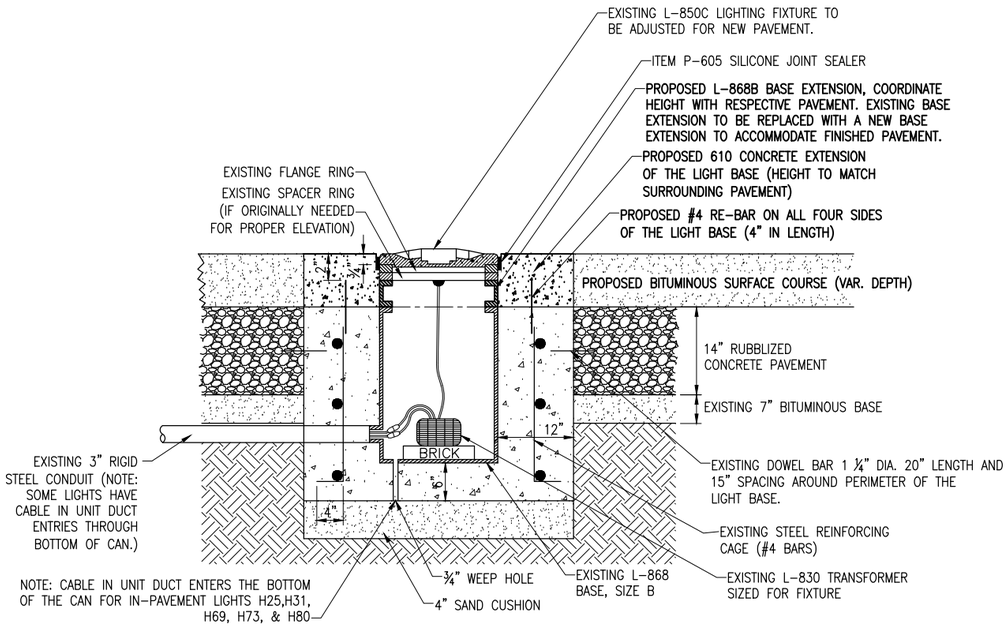
14. HOMERUN CABLES FOR A RESPECTIVE CIRCUIT THAT ARE INSTALLED IN CONDUIT OR DUCT SHALL BE RUN TOGETHER IN THE SAME RACEWAY OR DUCT.

15. WHEN A RESPECTIVE RUNWAY IS CLOSED THE RESPECTIVE RUNWAY LIGHTING AND NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF.

16. PER FAA AC 150/5270-10G "STANDARDS FOR SPECIFYING CONSTRUCTION OF AIRPORTS", ITEM L-108 "UNDERGROUND POWER CABLE FOR AIRPORT", EVERY AIRFIELD LIGHTING CABLE SPLICER SHALL BE QUALIFIED IN MAKING CABLE SPLICES AND TERMINATIONS ON CABLES RATED ABOVE 5,000 VOLTS AC. CABLE SPLICING/TERMINATING PERSONNEL SHALL HAVE A MINIMUM OF THREE (3) YEARS CONTINUOUS EXPERIENCE IN TEMINATING/SPLICING MEDIUM VOLTAGE CABLE.

17. HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, DUCT, RACEWAY, JUNCTION STRUCTURE, OR HANDHOLE.

18. NO CONNECTION TO AN ACTIVE LIGHTING CIRCUIT WILL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE WITH NOTE 1.



IN-PAVEMENT RUNWAY LIGHT ADJUSTMENT DETAIL

"NOT TO SCALE"

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER/TECHNICIAN SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
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SHEET TITLE

AIRFIELD LIGHTING NOTES & DETAIL

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ELECTRICAL LEGEND – ONE–LINE DIAGRAM	
	CABLE TERMINATOR/LUG
	TRANSFORMER
	DISCONNECT SWITCH
	FUSIBLE DISCONNECT SWITCH
	CIRCUIT BREAKER
	THERMAL MAGNETIC CIRCUIT BREAKER
	FUSE
	TRANSIENT VOLTAGE SURGE SUPPRESSOR OR SURGE PROTECTOR DEVICE
	GROUND – GROUND ROD, GROUNDING ELECTRODE, OR AT EARTH POTENTIAL
	INDICATING LIGHT
M	MOTOR
#	LOAD, MOTOR, # = HORSEPOWER
	ELECTRIC UTILITY METER BASE
	JUNCTION BOX WITH SPLICE
XXX	EQUIPMENT, XXX = DEVICE DESCRIPTION
GND	GROUND BUS OR TERMINAL
S/N	NEUTRAL BUS
	PANELBOARD WITH MAIN LUGS
	PANELBOARD WITH MAIN BREAKER
	FUSE PANEL WITH MAIN FUSE PULLOUT
	DUPLEX RECEPTACLE 120V SINGLE PHASE GROUNDING TYPE
	CONTROL STATION
N EM L	TRANSFER SWITCH
G	ENGINE GENERATOR SET

ELECTRICAL LEGEND – SCHEMATIC	
	NORMALLY OPEN (N.O.) CONTACT
	NORMALLY CLOSED (N.C.) CONTACT
S*	STARTER COIL, * = STARTER NUMBER
OL	OVERLOAD RELAY CONTACT
CR*	CONTROL RELAY, * = CONTROL RELAY NUMBER
R*	RELAY, * = RELAY NUMBER
	TOGGLE SWITCH / 2 POSITION SWITCH
OFF AUTO OX	2–POSITION SELECTOR SWITCH
OFF HAND AUTO XOO OOX	3–POSITION SELECTOR SWITCH (H–O–A SHOWN)
	2 POLE DISCONNECT SWITCH
	3 POLE DISCONNECT SWITCH
PH	PHOTOCELL
	TERMINAL BLOCK, * = TERMINAL NUMBER
	DEVICE TERMINAL, * = DEVICE TERMINAL NUMBER
	INTERNAL PANEL WIRING
	FIELD WIRING
	FUSE
GND	GROUND BUS OR TERMINAL
S/N	NEUTRAL BUS
	GROUND, GROUND ROD, GROUND BUS
	INDUSTRIAL CONTROL RELAY OR LIGHTING CONTACTOR
P	S1 CUTOUT HANDLE REMOVED
I	S1 CUTOUT HANDLE INSERTED
	N.O. THERMAL SWITCH
	N.C. THERMAL SWITCH
	L–830 SERIES ISOLATION TRANSFORMER

ELECTRICAL ABBREVIATIONS	
A.F.F.	ABOVE FINISHED FLOOR
A, AMP	AMPERES
ATS	AUTOMATIC TRANSFER SWITCH
AWG	AMERICAN WIRE GAUGE
BKR	BREAKER
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CR	CONTROL RELAY
CU	COPPER
DPDT	DOUBLE POLE DOUBLE THROW
DPST	DOUBLE POLE SINGLE THROW
EM	EMERGENCY
EMT	ELECTRICAL METALLIC TUBING
ENCL	ENCLOSURE
EP	EXPLOSION PROOF
ES	EMERGENCY STOP
ETL	INTERTEK – ELECTRICAL TESTING LABS
ETM	ELAPSE TIME METER
GFCI	GROUND FAULT CIRCUIT INTERRUPTER
GFI	GROUND FAULT INTERRUPTER
GND	GROUND
GRSC	GALVANIZED RIGID STEEL CONDUIT
HID	HIGH INTENSITY DISCHARGE
HOA	HAND OFF AUTOMATIC
HP	HORSEPOWER
HPS	HIGH PRESSURE SODIUM
J	JUNCTION BOX
KVA	KILOVOLT AMPERE(S)
KW	KILOWATTS
LC	LIGHTING CONTACTOR
LTFMC	LIQUID TIGHT FLEXIBLE METAL CONDUIT (UL LISTED)
LTG	LIGHTING
LP	LIGHTING PANEL
MAX	MAXIMUM
MCB	MAIN CIRCUIT BREAKER
MCM	THOUSAND CIRCULAR MIL
MDP	MAIN DISTRIBUTION PANEL
MFR	MANUFACTURER
MH	METAL HALIDE
MIN	MINIMUM
MLO	MAIN LUGS ONLY
NEC	NATIONAL ELECTRICAL CODE (NFPA 70)
NC	NORMALLY CLOSED
NO	NORMALLY OPEN
NTS	NOT TO SCALE
OHE	OVERHEAD ELECTRIC
OL	OVERLOAD

ELECTRICAL ABBREVIATIONS (CONTINUED)	
PB	PULL BOX
PC	PHOTO CELL
PDB	POWER DISTRIBUTION BLOCK
PNL	PANEL
RCPT	RECEPTACLE
R	RELAY
S	STARTER
SPD	SURGE PROTECTION DEVICE
SPST	SINGLE POLE SINGLE THROW
TVSS	TRANSIENT VOLTAGE SURGE SUPPRESSOR
TYP	TYPICAL
UG	UNDERGROUND
UGE	UNDERGROUND ELECTRIC
UL	UNDERWRITER'S LABORATORIES
V	VOLTS
W/	WITH
W/O	WITHOUT
WP	WEATHER PROOF
XFER	TRANSFER
XFMR	TRANSFORMER

AIRPORT EQUIPMENT/FACILITY ABBREVIATIONS	
ASOS	AUTOMATED SURFACE OBSERVING SYSTEM
ATCT	AIR TRAFFIC CONTROL TOWER
AWOS	AUTOMATED WEATHER OBSERVING SYSTEM
CCR	CONSTANT CURRENT REGULATOR
DME	DISTANCE MEASURING EQUIPMENT
FAR	FEDERAL AVIATION REGULATION
GS	GLIDE SLOPE FACILITY
HIRL	HIGH INTENSITY RUNWAY LIGHT
ILS	INSTRUMENT LANDING SYSTEM
IM	INNER MARKER
LIR	LOW IMPACT–RESISTANT
LOC	LOCALIZER FACILITY
MALS	MEDIUM INTENSITY APPROACH LIGHTING SYSTEM
MALSR	MEDIUM INTENSITY APPROACH LIGHTING SYSTEM WITH RUNWAY ALIGNMENT INDICATING LIGHTS
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MITL	MEDIUM INTENSITY TAXIWAY LIGHT
NDB	NON–DIRECTIONAL BEACON
PAPI	PRECISION APPROACH PATH INDICATOR
PLASI	PULSE LIGHT APPROACH SLOPE INDICATOR
RAIL	RUNWAY ALIGNMENT INDICATING LIGHTS
REIL	RUNWAY END IDENTIFIER LIGHT
RVR	RUNWAY VISUAL RANGE
VADI	VISUAL APPROACH DESCENT INDICATOR
VASI	VISUAL APPROACH SLOPE INDICATOR
VOR	VERY HIGH FREQUENCY OMNIDIRECTIONAL RANGE FACILITY
WC	WIND CONE

NOTES:

1. ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 – NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERTEK TESTING SERVICES VERIFICATION/ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
2. ALL VAULT WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
3. SEE RESPECTIVE SITE PLANS FOR SITE LEGEND INFORMATION.
4. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO WORKING ON, REMOVING OR DISCONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICE.
5. HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, DUCT, RACEWAY, JUNCTION STRUCTURE OR HANDHOLE.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT
TAXIWAY B

IDA No: MTO-4678
SBG Project No:
3-17-SBGP-TBD
Contract No. CO064

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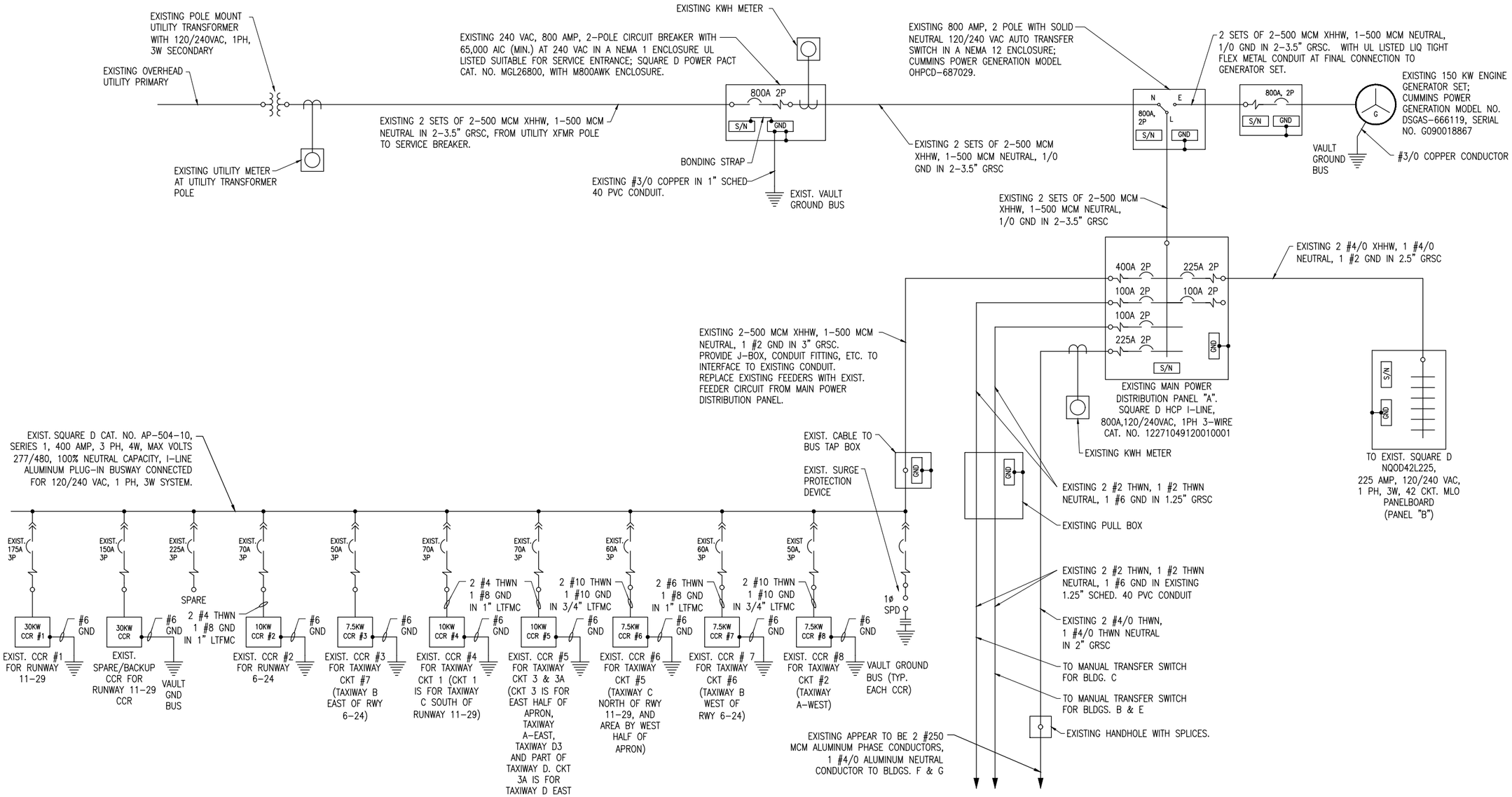
ELECTRICAL LEGEND AND ABBREVIATIONS



EXISTING ELECTRICAL ONE LINE DIAGRAM FOR VAULT

NOTE: INFORMATION SHOWN ON THIS SHEET IS TO HELP WITH IDENTIFYING EXISTING CONDITIONS.

NOTES:

1. EXISTING ONE-LINE DIAGRAM WIRING IS BASED ON FIELD DATA AND INFORMATION PROVIDED BY OTHERS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND WIRING AND REPORT ANY VARIATIONS TO THE RESIDENT PROJECT REPRESENTATIVE.
2. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND THE AIRPORT MAINTENANCE STAFF. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
3. CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING OR CONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICE.
4. CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
5. CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E – STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
6. WHEN A RUNWAY IS CLOSED THE RUNWAY LIGHTING AND ASSOCIATED AIRFIELD NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF.
7. WHEN A TAXIWAY IS CLOSED THE RESPECTIVE TAXIWAY LIGHTING SHALL BE SHUT OFF.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

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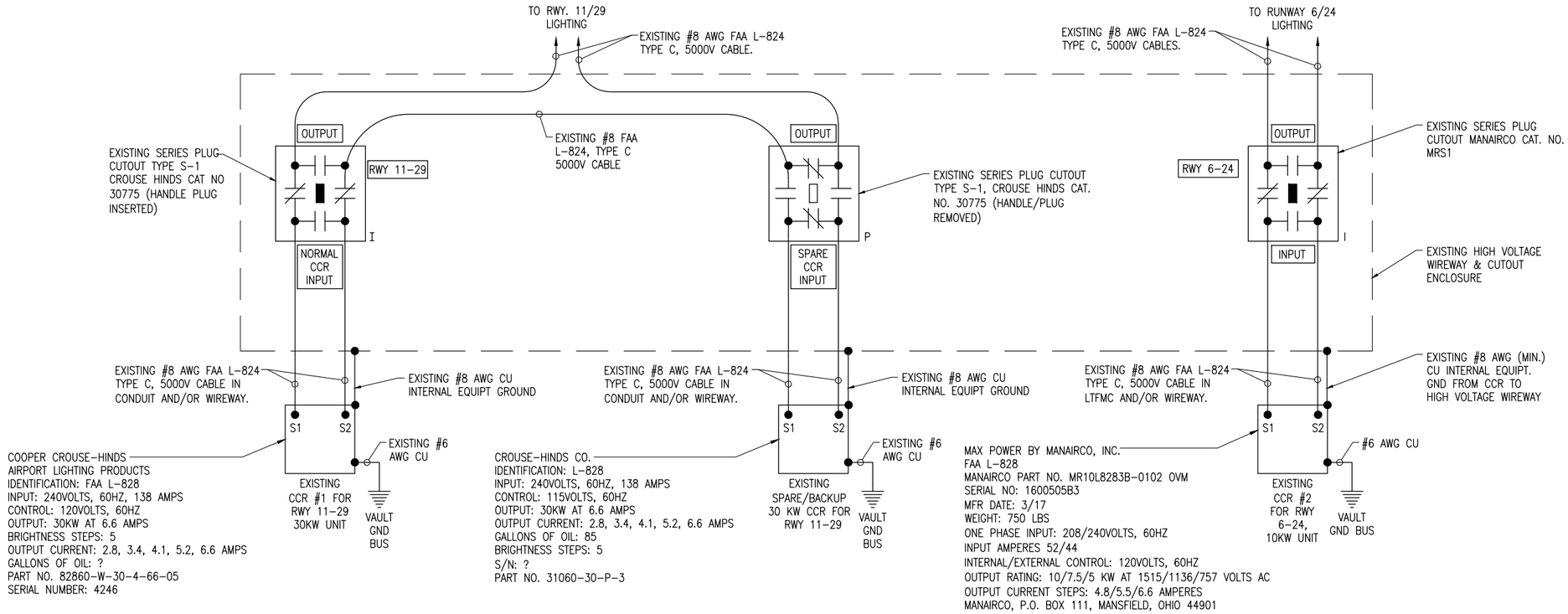
EXISTING ELECTRICAL ONE LINE FOR VAULT



EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAYS

LEGEND

"I"	DENOTES PLUG CUTOUT WITH PLUG INSERTED
"P"	DENOTES PLUG CUTOUT WITH PLUG PULLED
"CCR"	DENOTES CONSTANT CURRENT REGULATOR

NOTE: INFORMATION SHOWN ON THIS SHEET IS TO HELP WITH IDENTIFYING EXISTING CONDITIONS.

NOTES:

1. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
2. CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
3. THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
4. CONTRACTOR SHALL EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
5. MEGGER TEST (WITH AN INSULATION RESISTANCE TESTER) AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR OTHER WORK HAS BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE, (WITH AN OHMMETER).
6. THE RESPECTIVE RUNWAY AND TAXIWAY LIGHTING CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, ADDITIONS, AND/OR OTHER WORK AND AFTER THE RESPECTIVE WORK HAS BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT ENGINEER/TECHNICIAN. TEST RESULTS SHALL BE PROVIDED TO THE PROJECT ENGINEER AND RESIDENT ENGINEER/TECHNICIAN WITHIN 5 BUSINESS DAYS.
7. REFER TO INSTRUCTIONS IN THE VAULT FOR TRANSFER PROCEDURE TO BACKUP CCR FOR RUNWAY 11-29.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

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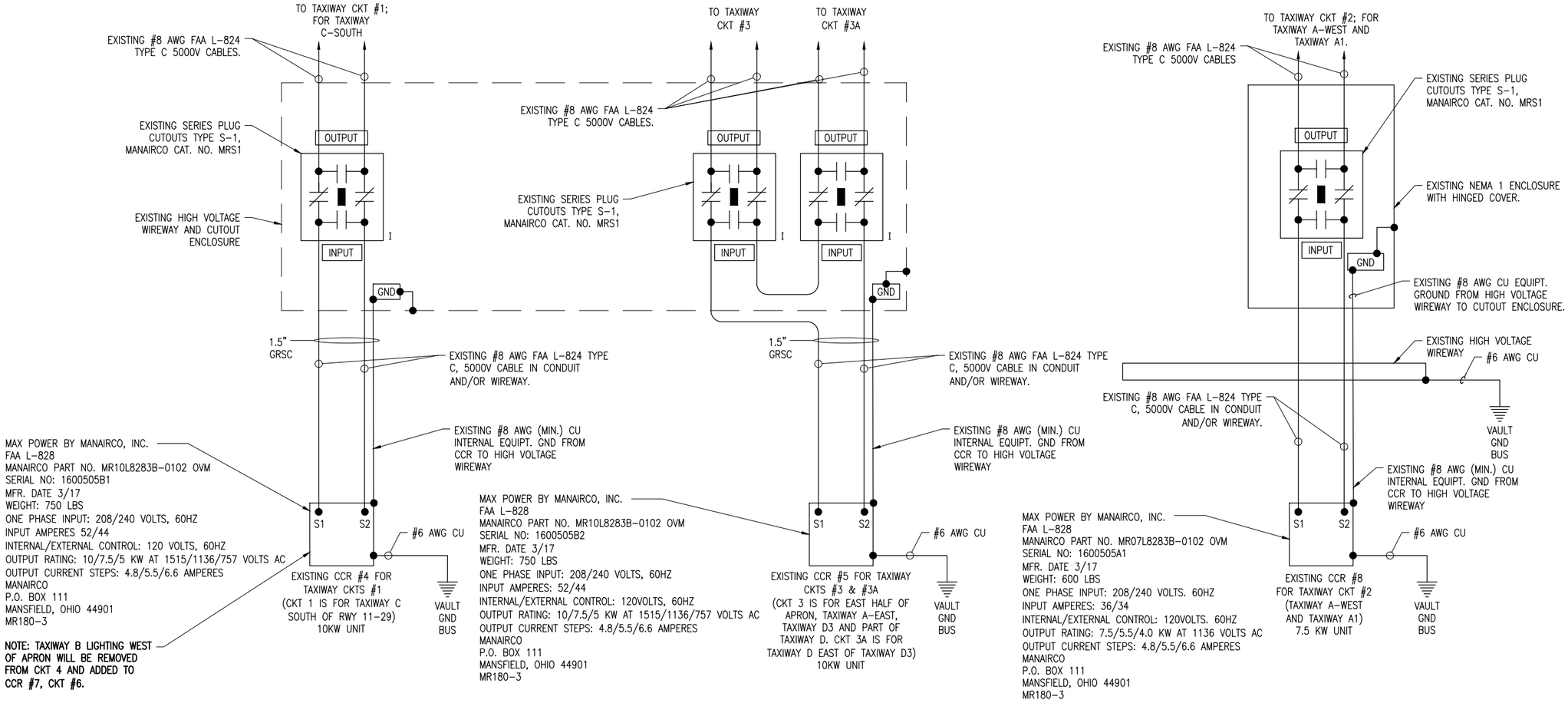
EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAYS



EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAY CKTS 1, 2, 3, & 3A

NOTES:

1. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
2. CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
3. THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
4. CONTRACTOR SHALL EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
5. MEGGER TEST (WITH AN INSULATION RESISTANCE TESTER) AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR OTHER WORK HAS BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE, (WITH AN OHMMETER).
6. THE RESPECTIVE RUNWAY AND TAXIWAY LIGHTING CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, ADDITIONS, AND/OR OTHER WORK, AND AFTER THE RESPECTIVE WORK HAS BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT ENGINEER/TECHNICIAN. TEST RESULTS SHALL BE PROVIDED TO THE PROJECT ENGINEER AND RESIDENT ENGINEER/TECHNICIAN WITHIN 5 BUSINESS DAYS.

LEGEND

"I" DENOTES PLUG CUTOUT WITH PLUG INSERTED

"P" DENOTES PLUG CUTOUT WITH PLUG PULLED

"CCR" DENOTES CONSTANT CURRENT REGULATOR

NOTE: INFORMATION SHOWN ON THIS SHEET IS TO HELP WITH IDENTIFYING EXISTING CONDITIONS.

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678

SBG Project No: 3-17-SBGP-TBD

Contract No. CO064

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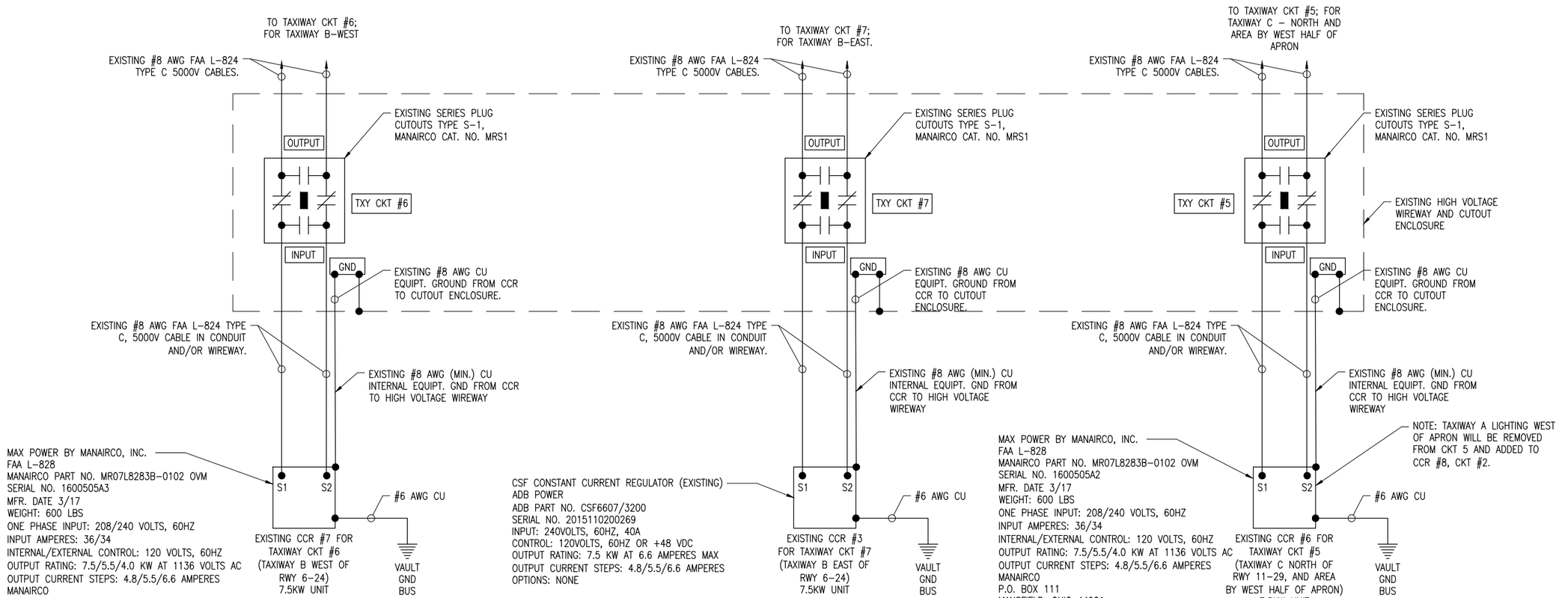
EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TWY CKTS 1,2,3&3A



EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAY CKTS 5, 6, & 7

NOTES:

1. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
2. CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
3. THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
4. CONTRACTOR SHALL EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
5. MEGGER TEST (WITH AN INSULATION RESISTANCE TESTER) AND RECORD EXISTING SERIES CIRCUITS PRIOR TO CABLE WORK OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR OTHER WORK HAS BEEN COMPLETED. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE, (WITH AN OHMMETER).
6. THE RESPECTIVE RUNWAY AND TAXIWAY LIGHTING CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, ADDITIONS, AND/OR OTHER WORK, AND AFTER THE RESPECTIVE WORK HAS BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT ENGINEER/TECHNICIAN. TEST RESULTS SHALL BE PROVIDED TO THE PROJECT ENGINEER AND RESIDENT ENGINEER/TECHNICIAN WITHIN 5 BUSINESS DAYS.

NOTE: INFORMATION SHOWN ON THIS SHEET IS TO HELP WITH IDENTIFYING EXISTING CONDITIONS.

LEGEND

"I"	DENOTES PLUG CUTOUT WITH PLUG INSERTED
"P"	DENOTES PLUG CUTOUT WITH PLUG PULLED
"CCR"	DENOTES CONSTANT CURRENT REGULATOR

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COLES COUNTY AIRPORT AUTHORITY
COLES COUNTY MEMORIAL AIRPORT

RECONSTRUCT TAXIWAY B

IDA No: MTO-4678
SBG Project No: 3-17-SBGP-TBD
Contract No. CO064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 15, 2018
PROJECT NO: 18A0003
CAD FILE: E-605.DWG
DESIGN BY: KNL 05/05/2018
DRAWN BY: CWS 05/07/2018
REVIEWED BY: LDH 05/09/2018

SHEET TITLE

EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TWY CKTS 5,6&7