

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED
IN CITY OF CHICAGO

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

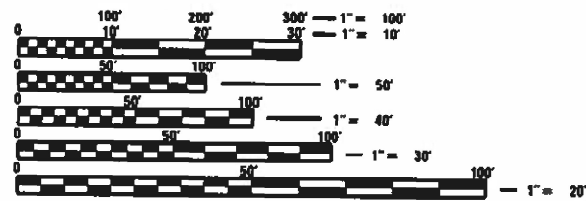
FAI 94 / I-94 (BISHOP FORD EXPRESSWAY)
AT 111TH STREET
SECTION 1112-701HB-BR
PROJECT NHPP - 1GJU(062)
BRIDGE REPAIRS, BRIDGE DECK OVERLAY
AND BRIDGE PAINTING
COOK COUNTY

C-91-210-12

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	112-701HB-BR	COOK	59	1
		ILLINOIS	CONTRACT NO. 60R77	



TRAFFIC DATA				
Functional Classification	ADT	POSTED SPEED LIMIT		
111th Minor Arterial	15,200	35 MPH		
I-94 Interstate	145,700	55 MPH		

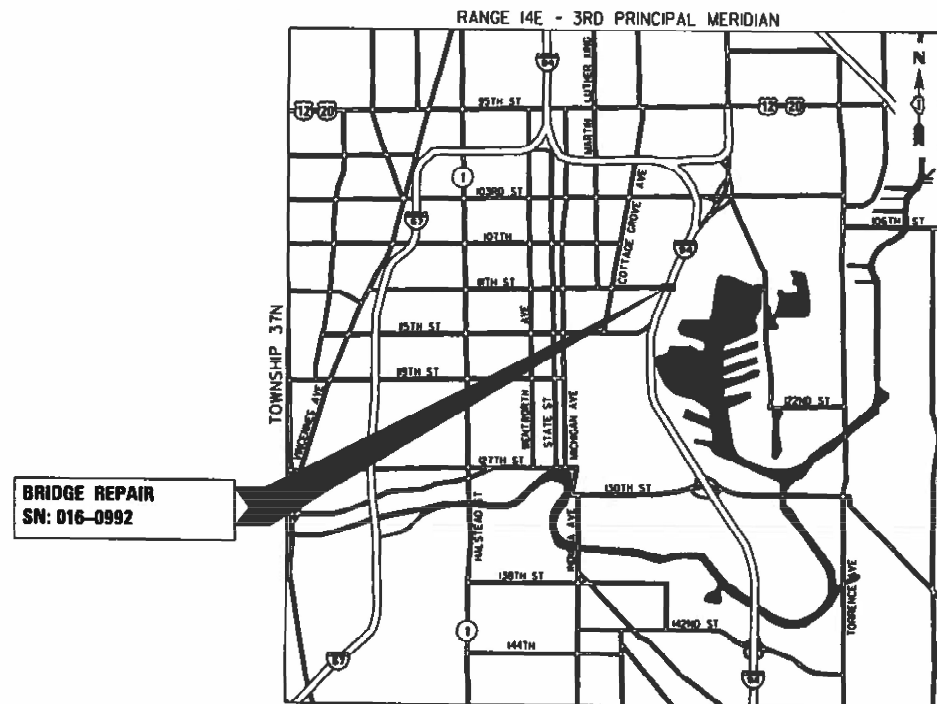


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

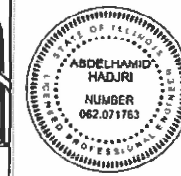
DIGGER - CHICAGO UTILITY AREA NETWORK (C.U.A.N.)
1-312-744-7000

PROJECT ENGINEER: Praveen Kaini, PE (847) 705-4237
PROJECT MANAGER: J. Alain Midy, PE (847) 221-3056

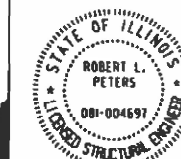
CONTRACT NO. 60R77



GROSS LENGTH = 260.2 FT. = 0.049 MILE
NET LENGTH = 260.2 FT. = 0.049 MILE



ABNA CORP.
DATE: 03-16-2023
ABDELNAIM HADJI
EXPIRES: 11-30-2023
SHEETS NO. MOT SHEETS



BY: Robert L. Peters DATE: 03/16/2023
ROBERT L. PETERS EXPRES: #/30/2024

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED: MAY 5 2023
Jane Rina Lopez
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

June 30, 2023
Scott A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

June 30, 2023
Stephen M. ...
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION



CZAPLICKI LOPEZ, PLLC
201 KENMORE DRIVE
BURR RIDGE, IL, 60527
630-915-8861

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INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, INDEX OF SHEETS, AND HIGHWAY STANDARDS
- 3 - 6 SUMMARY OF QUANTITIES
- 7 - 16 MAINTENANCE OF TRAFFIC
- 17 ROADWAY PLAN
- 18 PAVEMENT MARKING PLAN
- 19 - 43 STRUCTURE PLANS
- 44 FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
- 45 FREEWAY SINGLE AND MULTI LANE WEAVE (TC-09)
- 46 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
- 47 - 48 MULTI-LANE FREEWAY PAVEMENT MARKING (TC-12)
- 49 DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
- 50 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
- 51 DETOUR SIGNING FOR CLOSING STATE HIGHWAYS (TC-21)
- 52 ARTERIAL ROAD INFORMATION SIGN (TC-22)
- 53 - 55 CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)
- 56 - 59 REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL (BM-21)

DISTRICT 1 DETAILS

- TC-08 FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS
- TC-09 FREEWAY SINGLE AND MULTI LANE WEAVE
- TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
- TC-12 MULTI-LANE FREEWAY PAVEMENT MARKING
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-17 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
- TC-21 DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
- TC-22 ARTERIAL ROAD INFORMATION SIGN
- TC-24 CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS
- BM-21 REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL

HIGHWAY STANDARDS

- 000001-08 STANDARD SYMBOL, ABBREVIATION AND PATTERNS
- 420401-13 BRIDGE APPROACH PAVEMENT CONNECTOR
- 701106-02 OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY
- 701400-11 APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701401-13 LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤40 MPH
- 701428-01 TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701901-08 TRAFFIC CONTROL DEVICES
- 704001-08 TEMPORARY CONCRETE BARRIER
- 782006-01 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

1. THESE PLANS HAVE BEEN PREPARED FROM EXISTING PLANS AND NOTES RECEIVED FROM IDOT BRIDGE INSPECTIONS.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER - CHICAGO UTILITY AREA NETWORK (C.U.A.N) (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. (MINIMUM 48 HOUR NOTIFICATION IS REQUIRED.)
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE LOCAL MUNICIPALITY AS REQUIRED.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPOERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
6. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
7. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
8. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES, SYMBOLS, LETTERS, AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING AND MARKERS SHALL BE AS DIRECTED BY THE ENGINEER.
9. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS. STATIONS ARE SHOWN FOR REFERENCE ONLY AND ARE APPROXIMATE.
10. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO VARIATIONS FOUND IN THE FIELD. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. ANY ADJUSTMENTS PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE.
11. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
12. THE CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES TO ASSURE THAT NO DEBRIS FALL INTO THE WATERWAY OR ONTO THE PAVEMENTS BELOW THE STRUCTURE. THE COST OF THE WORK SHALL BE INCLUDED IN THE ASSOCIATED PAY ITEMS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS
AND HIGHWAY STANDARDS
111TH STREET OVER I-94

SHEET NO. OF SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	2
			CONTRACT NO. 60R77	
		ILLINOIS	FED. PROJECT	

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				90% FED/ 10% STATE BRIDGE 0059 SN 016-0992
28000510	INLET FILTERS	EACH	6	6
31101000	SUBBASE GRANULAR MATERIAL, TYPE B	TON	80	80
42000080	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB	SQ YD	352.1	352.1
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	220	220
44213208	TIE BARS, 1 1/4"	EACH	137	137
50102400	CONCRETE REMOVAL	CU YD	31.0	31.0
50157300	PROTECTIVE SHIELD	SQ YD	1,238	1,238
50300225	CONCRETE STRUCTURES	CU YD	46.5	46.5
50300255	CONCRETE SUPERSTRUCTURE	CU YD	35.6	35.6
50300300	PROTECTIVE COAT	SQ YD	1,729	1,729
50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	292.5	292.5
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	92,380	92,380
52000110	PREFORMED JOINT STRIP SEAL	FOOT	229	229
58700300	CONCRETE SEALER	SQ FT	1,051	1,051
59000200	EPOXY CRACK INJECTION	FOOT	103	103

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
111TH STREET OVER I-94**

SHEET NO. 1 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	3
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 60R77

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				90% FED/ 10% STATE BRIDGE 0059 SN 016-0992
60605000	COMBINATION CURB & GUTTER, TYPE B-6.24	FOOT	100	100
63302400	REMOVE AND REERECT EXISTING GUARDRAIL TERMINAL, TYPE 5	EACH	2	2
63302700	REMOVE AND REERECT EXISTING GUARDRAIL TERMINAL, TYPE 6	EACH	2	2
67100100	MOBILIZATION	L SUM	1	1
X7010238	CHANGEABLE MESSAGE SIGN(SPECIAL)	CAL MO	6	6
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	7,233	7,233
70307125	TEMPORARY PAVEMENT MARKING - LINE 5" - TYPE V TAPE	FOOT	1,853	1,853
70400100	TEMPORARY CONCRETE BARRIER	FOOT	475	475
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	475	475
70600270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	4	4
70600330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	4	4
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4,590	4,590
* 78009004	MODIFIED URETHANE PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1,651	1,651

* SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
111TH STREET OVER I-94**

SHEET NO. 2 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	4
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 60R77

SUMMARY OF QUANTITIES

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				90% FED / 10% STATE BRIDGE 0059 SN 016-0992
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	308	308
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	69	69
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	8	8
* 78112000	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	8	8
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	12	12
78300201	PAVEMENT MARKING REMOVAL- GRINDING	SQ FT	1,530	1,530
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	4,212	4,212
X5060704	CLEANING AND PAINTING EXPOSED REBAR	SQ FT	120	120
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	1,126	1,126
X5067500	BRIDGE CLEANING AND PAINTING WARRANTY	L SUM	1	1
X6700407	ENGINEER'S FIELD OFFICE, TYPE A(D1)	CAL MO	12	12
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	30	30
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	100	100

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
111TH STREET OVER I-94**

SHEET NO. 3 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R77	

SUMMARY OF QUANTITIES

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				90% FED / 10% STATE BRIDGE 0059 SN 016-0992
Z0001905	STRUCTURAL STEEL REPAIR	POUND	15,990	15,990
Z0004552	APPROACH SLAB REMOVAL	SQ YD	890	890
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	1,142	1,142
Z0007101	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 1	L SUM	1	1
Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	L SUM	1	1
Z0010615	CLEANING EXISTING INLETS	EACH	6	6
Z0012130	BRIDGE DECK SCARIFICATION, 3/4"	SQ YD	1,142	1,142
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	384	384
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	175	175
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	2.0	2.0
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	2	2
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	1,677	1,677
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	3	3
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	78	78
* Z0049100	RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT	EACH	100	100
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	11	11
Ø Z0076600	TRAINEES	HOURS	500	500
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500

Ø 0042

* SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
111TH STREET OVER I-94**

SHEET NO. 4 OF 4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	6
ILLINOIS			FED. AID PROJECT	

MAINTENANCE OF TRAFFIC – STAGING NOTES

PRESTAGE AND DETOUR

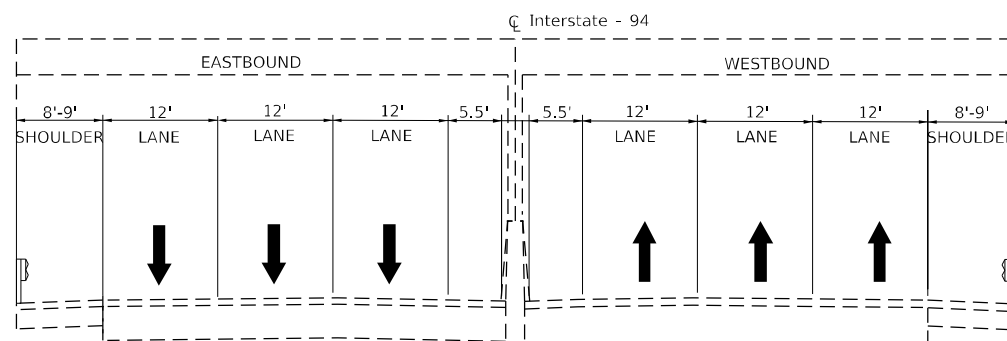
1. CLOSE THE WESTBOUND I-94 OFF RAMP TO 111th STREET USING DISTRICT STANDARD TC-08
2. CLOSE THE WESTBOUND I-94 OFF RAMP TO THE FRONTAGE ROAD LEADING TO 111th STREET EXIT USING DISTRICT STANDARD TC-08
3. CLOSE 111th STREET AT THE BRIDGE OVER I-94 FOR BOTH DIRECTIONS.
4. DETOUR RAMP TRAFFIC. SEE DETOUR PLANS AND DISTRICT STANDARD TC-21. DETOUR THE WESTBOUND I-94 TRAFFIC EXITING AT 111th STREET TO 103rd STREET.

STAGE 1

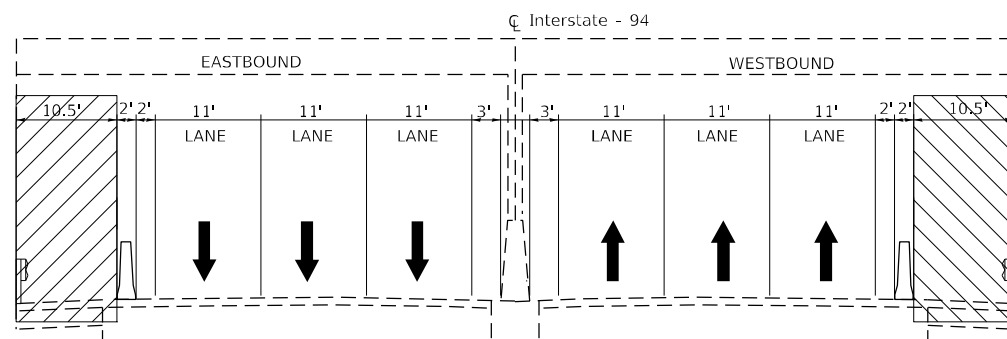
1. I-94-MAINTAIN 3 LANES OF TRAFFIC IN EACH DIRECTION EXCEPT DURING OFF PEAK HOURS AND NIGHTTIME OPERATIONS.
2. UTILIZE THE MAINTENANCE OF TRAFFIC PLANS, INSTALL TEMPORARY PAVEMENT MARKING, TEMPORARY CONCRETE BARRIER, SHIFT AND NARROW THE LANES ON I-94 TOWARD THE INSIDE SHOULDER TO PROVIDE WORKING ROOM AT THE ABUTMENT FOR BRIDGE WORK AS SHOWN ON THE PLANS.

STAGE 2

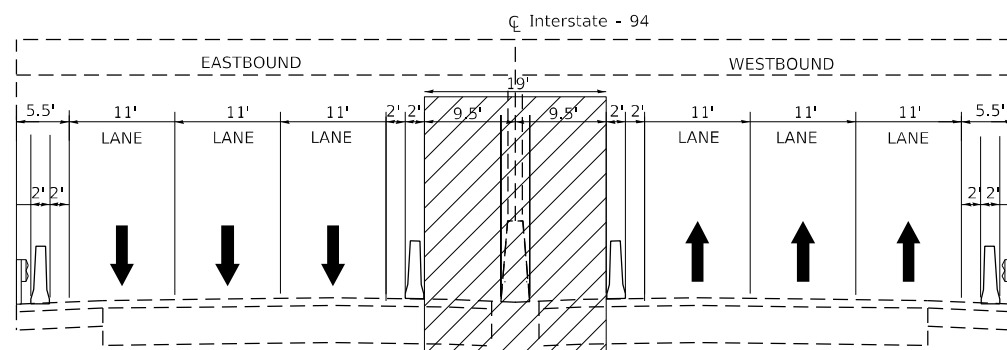
1. I-94-MAINTAIN 3 LANES OF TRAFFIC IN EACH DIRECTION EXCEPT DURING OFF PEAK HOURS AND NIGHTTIME OPERATIONS.
2. UTILIZE THE MAINTENANCE OF TRAFFIC PLANS, REMOVE CONFLICTING TEMPORARY MARKING, INSTALL NEW TEMPORARY PAVEMENT MARKING, RELOCATE TEMPORARY CONCRETE BARRIER, SHIFT AND NARROW THE LANES ON I-94 TOWARD THE OUTSIDE SHOULDER FOR BOTH DIRECTIONS TO PROVIDE WORKING ROOM IN THE MEDIAN ABUTMENT FOR BRIDGE WORK AT THE MIDDLE BENT AS SHOWN ON THE PLANS.



EXISTING TYPICAL SECTION
LOOKING NORTH



TYPICAL SECTION SHOULDER WORK STAGE 1
LOOKING NORTH





TYPICAL SECTION MEDIAN WORK STAGE 2
STAGING FOR I-94 PIERS
LOOKING NORTH

MAINTENANCE OF TRAFFIC NOTES:

1. THE CONTRACTOR SHALL REQUEST AND GAIN THE APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY FOUR (24) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP AND SHOULDER CLOSURES AND 7 DAYS IN ADVANCE OF ALL PERMANENT AND WEEKEND CLOSURES ON ALL REEWAYS AND/OR EXPRESSWAYS IN DISTRICT 1. THIS ADVANCE NOTIFICATION IS CALCULATED BASED ON WORK WEEK OF MONDAY THROUGH FRIDAY AND SHALL NOT INCLUDE WEEKENDS OR HOLIDAYS.
2. A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGES CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.
3. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN A MINIMUM OF 3 11' THROUGH LANES IN EACH DIRECTION ON I-94 EXCEPT AS OTHERWISE INDICATED ON THE PLANS. LANE CLOSURES AT NIGHT WILL BE ALLOWED FOR CONSTRUCTION ACTIVITY ONLY AS APPROVED BY THE ENGINEER.
4. THE CONTRACTOR SHALL PROVIDE ALL SIGNS, VERTICAL PANELS, TYPE III BARRICADES, CHANNELIZATION DRUMS, TYPE II BARRICADES, ALL TEMPORARY CONCRETE BARRIERS AND PROTECTION NECESSARY FOR WORK ZONE TRAFFIC CONTROL AND PROTECTION, OR AS DIRECTED BY THE ENGINEER.
5. THE CONTRACTOR SHALL USE TEMPORARY PAVEMENT MARKING ON SURFACES TO BE REMOVED AND IN AREAS THAT DO NOT CONFLICT WITH OTHER STAGES. DURING ALL WINTER STAGING MONTHS (NOVEMBER 1 - APRIL 1) PAVEMENT MARKINGS SHALL BE PAINT. THE PLACEMENT AND REMOVAL OF PAVEMENT MARKINGS FOR WINTER MONTHS SHALL BE PAID FOR AT THE CONTRACT UNIT COST FOR TEMPORARY PAVEMENT MARKING OF THE SIZE SPECIFIED ON THE PLANS AND PAVEMENT MARKING REMOVAL-WATER BLASTING.
6. ALL DRUMS, VERTICAL PANELS, AND TYPE II BARRICADES (IF REQUIRED) SHALL BE EQUIPPED WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS ON MULTILANE ROADWAYS, WHILE BI-DIRECTIONAL LIGHTS ARE REQUIRED ON ALL TWO-WAY, TWO-LANE ROADWAYS.
7. ALL DRUMS, VERTICAL PANELS, AND TYPE II BARRICADES SHALL BE SPACED AT 40 FEET CENTER TO CENTER THROUGHOUT THE WORK ZONE, EXCEPT IN TAPER AREAS, GORE AREAS, AND ALONG CORNER RADII, WHERE THEY SHALL BE SPACED AT 20 FEET CENTER TO CENTER EXCEPT AS OTHERWISE INDICATED ON THE PLANS.
8. TYPE II BARRICADES SHALL BE USED IN LIEU OF DRUMS IN LOCATIONS WHERE TYPE II MUST BE PLACED IN EXCAVATED AREAS DUE TO LIMITED SPACE. THESE BARRICADES SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH IDOT HIGHWAY STANDARD 701901. DRUMS AND BARRICADES SHALL NOT BE PLACED TOGETHER WITHIN AN INDIVIDUAL RUN OF DEVICES.
9. TEMPORARY CONCRETE BARRIERS SHALL BE EQUIPPED WITH TYPE "C" REFLECTORS, IN ACCORDANCE WITH HIGHWAY STANDARD 704001 SPACED AT 25' CENTERS.
10. ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND.
11. ALL CONSTRUCTION WARNING SIGN DIMENSIONS SHALL BE 48" X 48", EXCEPT AS OTHERWISE
12. ALL "ROAD CONSTRUCTION AHEAD" WARNING SIGNS SHALL BE EQUIPPED WITH HIGH INTENSITY MONO-DIRECTIONAL FLASHING LIGHTS.
13. THE CONTRACTOR SHALL INSTALL AND COVER ALL TEMPORARY SIGNING BEFORE EXISTING SIGNS ARE REMOVED. THE CONTRACTOR SHALL RELOCATE EXISTING SIGNS AS INDICATED ON THE PLANS.
14. THE CONTRACTOR SHALL INSTALL AND COVER ALL PERMANENT SIGNING BEFORE TEMPORARY SIGNS ARE REMOVED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION - X7010216 (SPECIAL) & X701015 (EXPRESSWAYS).
15. ALL EXISTING GUIDE SIGNS (i.e. STREET NAME SIGNS, ADVANCE STREET NAME SIGNS, ROUTE MARKERS, ETC.) SHALL BE MAINTAINED AND VISIBLE TO TRAFFIC DURING CONSTRUCTION.
16. EXISTING, TEMPORARY AND PROPOSED TRAFFIC/GUIDE SIGNS IN CONFLICT WITH STAGING SHALL BE COVERED AS INDICATED ON THE MAINTENANCE OF TRAFFIC PLANS AND/OR AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE CONSIDERED INCLUDED IN THE CONTRACT COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
17. THE FABRICATION, INSTALLATION, MAINTENANCE AND REMOVAL OF TEMPORARY GUIDE SIGNS, TEMPORARY SIGN PANELS AND BLANK-OUT PANELS, AS SHOWN IN THE MAINTENANCE OF TRAFFIC SIGN PANEL DETAILS AND PLANS WILL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL). THE RELOCATION OF THESE TEMPORARY PANELS/SIGNS FOR SUBSEQUENT STAGES SHALL ALSO BE CONSIDERED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
18. DIRECTION INDICATOR BARRICADES SHALL BE USED AT LANE CLOSURE TAPER LOCATIONS FOR ROADWAYS WITH SPEEDS GREATER THAN OR EQUAL TO 45 MPH OR AS OTHERWISE DIRECTED BY THE ENGINEER.
19. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL BACKFILL OR COVER ALL TRENCHES AND OPEN EXCAVATION HOLES FOR PROPOSED STORM SEWER, DRAINAGE STRUCTURES, AND WATER MAINS IN ORDER TO PROVIDE A SAFE CONDITION FOR MOTORISTS AND THE PUBLIC DURING NON-WORKING HOURS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
20. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MAINTENANCE OF TRAFFIC PLAN. ALL CHANGES TO THE MAINTENANCE OF TRAFFIC PLANS SHALL BE APPROVED BY TRAFFIC.

TYPICAL LEGENDS

-  WORK AREA
-  DIRECTION OF TRAVEL

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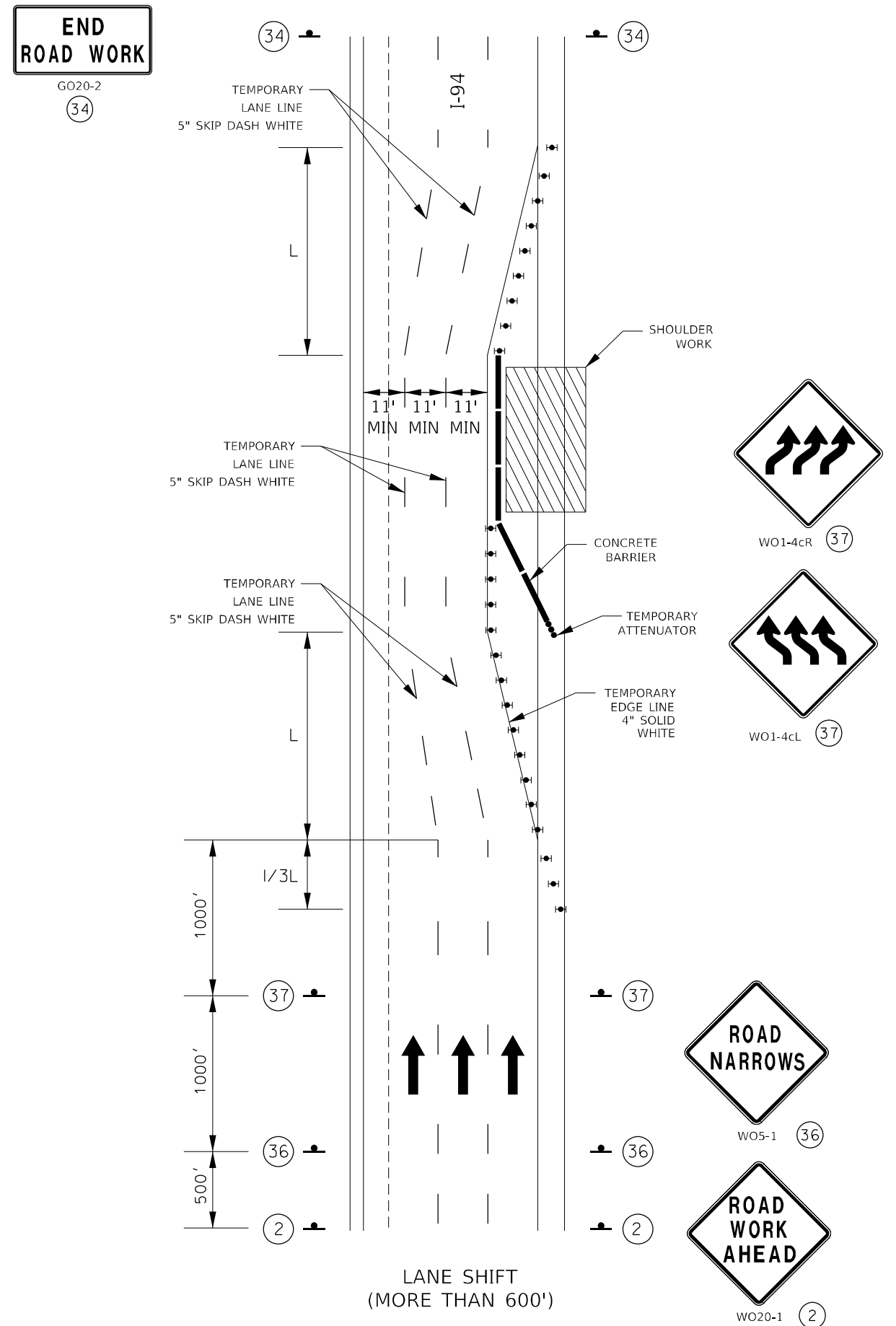
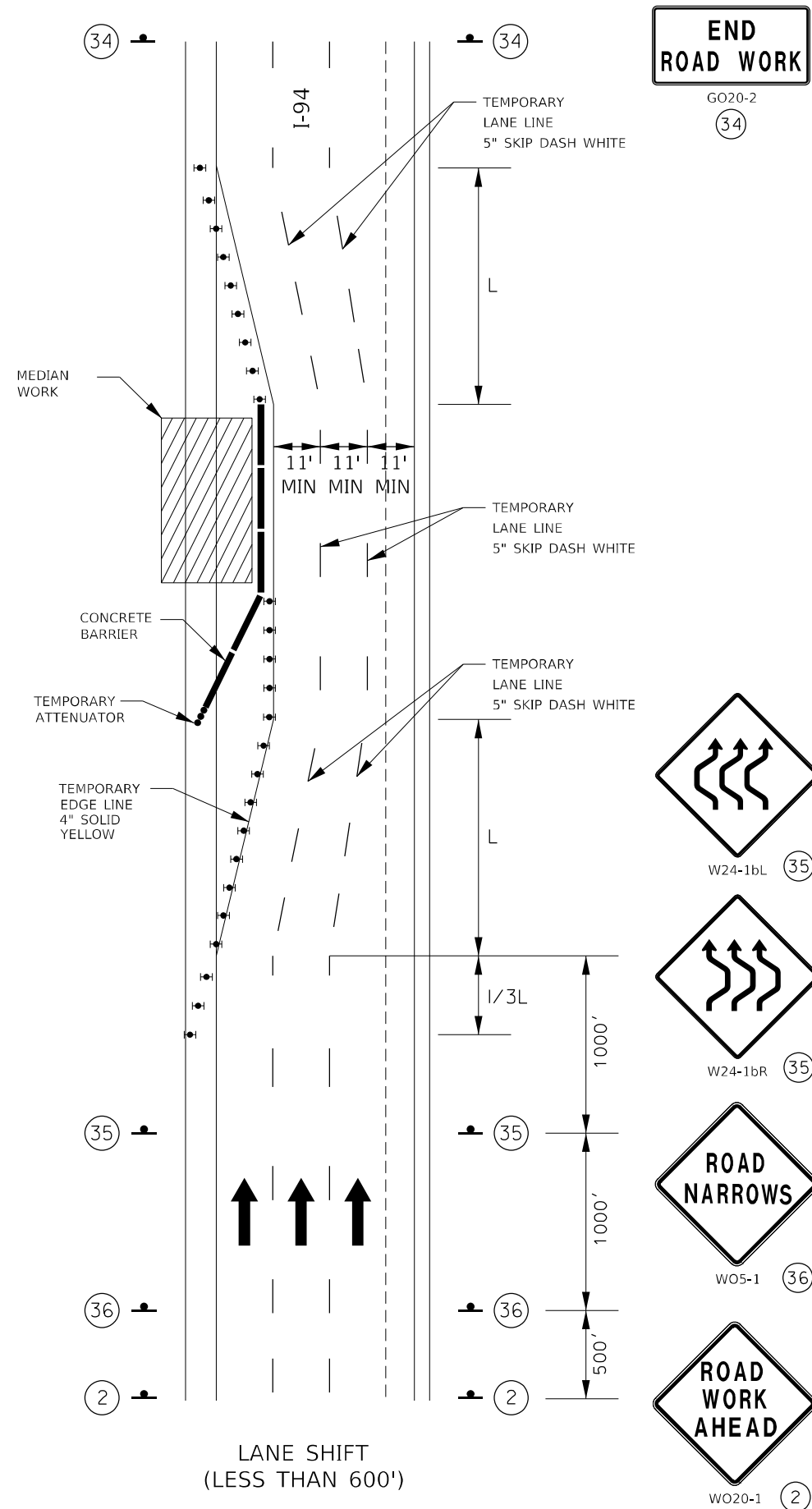
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC – TYPICAL SECTIONS STAGE 1 & 2
 111TH STREET OVER I-94

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	7
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 1 & 2
 111TH STREET OVER I-94**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	8
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC – DETOUR SIGN LEGEND

1 ROAD CLOSED AHEAD
W20-3
48"x48"
BLACK ON ORANGE

2 DETOUR AHEAD
W20-2
48"x48"
BLACK ON ORANGE

3 ROAD CLOSED 500 FT
W20-3
48"x48"
BLACK ON ORANGE

4 RAMP CLOSED AHEAD
W20-1
48"x48"
BLACK ON ORANGE

5 ROAD CLOSED 1/2 MILE
W20-3
48"x48"
BLACK ON ORANGE

6 EAST
M3-2
24"x12"
WHITE ON GREEN

7 DETOUR
M4-9R
24"x24"
BLACK ON ORANGE

8 DETOUR
M4-9L
24"x24"
BLACK ON ORANGE

9 RAMP CLOSED
M1-50
24"x15"
BLACK ON ORANGE

10 EXIT CLOSED
E5-2a
48"x36"
BLACK ON ORANGE

11 ROAD CONSTRUCTION AHEAD
W20-1
48"x48"
BLACK ON ORANGE

12 ON RAMP
W13-4
18"x18"
BLACK ON ORANGE

13 END DETOUR
M4-8A
24"x18"
BLACK ON ORANGE

14 ROAD CLOSED 1/2 MILE
W20-3
48"x48"
BLACK ON ORANGE

15 ROAD CLOSED 1/2 MILE
W20-3
48"x48"
BLACK ON ORANGE

16 ROAD CLOSED
R11-2-4830
ROAD CLOSED
TYPE III BARICADES

17 ROAD CONSTRUCTION AHEAD
W20-1
48"x48"

18 ON RAMP
W13-4
18"x18"

19 DETOUR WEST 94
M4-8 24"x12"
M3-4 24"x12"
M1-1 24"x24"
M5-1R 21"x15"

20 DETOUR WEST 94
M4-8 24"x12"
M3-4 24"x12"
M1-1 24"x24"
M6-3 21"x15"

21 DETOUR WEST 94
M4-8 24"x12"
M3-4 24"x12"
M1-1 24"x24"
M5-1L 21"x15"

22 DETOUR WEST 94
M4-8 24"x12"
M3-4 24"x12"
M1-1 24"x24"
M6-1R 21"x15"

23 DETOUR WEST 94
M4-8 24"x12"
M3-4 24"x12"
M1-1 24"x24"
M6-1L 21"x15"

24 WEST 111TH ST DETOUR
M4-3(O) 24"x12"
SPECIAL 24"x18" 6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M4-9 30"x24"

25 WEST 111TH ST DETOUR
M4-3(O) 24"x12"
SPECIAL 24"x18" 6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M4-9R 30"x24"

26 WEST 111TH ST DETOUR
M4-3(O) 24"x12"
SPECIAL 24"x18" 6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M4-9L 30"x24"

27 WEST 111TH ST DETOUR
M4-3(O) 24"x12"
SPECIAL 24"x18" 6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M4-9R 30"x24"

28 WEST 111TH ST DETOUR
M4-3(O) 24"x12"
SPECIAL 24"x18" 6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M4-9L 30"x24"

29 END DETOUR WEST 94
M4-8A 24"x18"
M3-4 24"x12"
M1-1 24"x24"

30 END DETOUR WEST 94
M4-8A 24"x18"
M3-4 24"x12"
M1-1 24"x24"

31 DETOUR AHEAD WEST 94
W20-3 48"x48"
M3-4 24"x12"
M1-1 24"x24"

32 No Right Turn Sign
R3-2
36"x36"
BLACK ON WHITE

33 DETOUR WEST 111TH ST
M4-8 24"x12"
M1-50 24"x15"
M6-3 21"x15"

34 DETOUR WEST 111TH ST
M4-8 24"x12"
M1-50 24"x15"
M6-3 21"x15"

35 DETOUR WEST 111TH ST
M4-8 24"x12"
M1-50 24"x15"
M6-3 21"x15"

SIGN SPACING FOR ADVANCE SIGN		
SPEED (P) MPH	NON-DIVIDED HIGHWAYS (S)	DIVIDED HIGHWAYS (S)
0-35	200 FT	200 FT
40-45	350 FT	500 FT
50-55	500 FT	1000 FT
60-70	SA-1000 FT, SB-1500 FT, SC-2640 FT **	

(1) SPACING BETWEEN SIGNS AND SPACING BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER, OR SIGNED CONDITION

(2) SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS

TAPER LENGTH (L)
L = W X P FOR 40 MPH OR MORE
L = $\frac{WP^2}{60}$ FOR 35 MPH OR LESS
L = TAPER LENGTH IN FEET
W = LATERAL SHIFT IN FEET
P = POSTED SPEED PRIOR TO ROAD WORK IN MPH

MAINTENANCE OF TRAFFIC – DETOUR NOTES

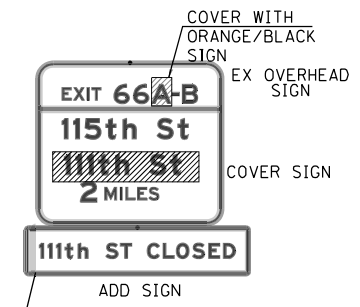
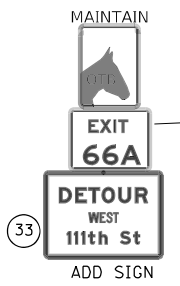
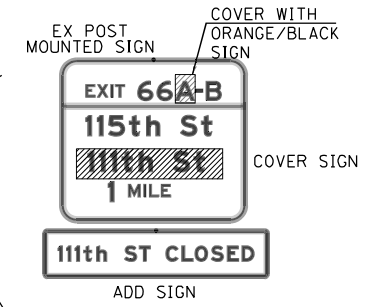
1. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO THE OPENING OF THE DETOUR ROUTE, INCIDENTAL TO THE PAY ITEM, "TRAFFIC CONTROL AND PROTECTION (SPECIAL)". COVERS SHALL BE REMOVED AT THE TIME THE DETOUR SIGNS ARE REMOVED.
2. THE SIGNAGE AS SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".
3. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND AREA IS REOPENED TO THRU TRAFFIC. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR.
4. IDOT DISTRICT 1 TC-08, FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS FOR ALL RAMP CLOSURES
5. THE CONTRACTOR SHALL PROVIDED TEMPORARY TRAFFIC SIGNAL TIMING, AS DIRECTED BY THE ENGINEER, AT THE FOLLOWING INTERSECTIONS:
- E. 111TH STREET @ S. COTTAGE GROVE AVENUE
- S. COTTAGE GROVE AVENUE @ E. 115TH STREET

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TRAFFIC DETOUR ROUTE NOTES

1. THE CONTRACTOR SHALL PROCEED WITH THE WORK IN AN EXPEDIENT MANNER TO REDUCE THE LENGTH OF TIME THAT THE DETOUR IS IN EFFECT.
2. LONGITUDINAL DIMENSIONS SHOWN ON THESE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
3. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. A MINIMUM DISTANCE OF 30 FEET SHALL BE MAINTAINED. IN ANY EVENT, ARTICLES 701 THRU 703 OF THE STANDARD SPECIFICATIONS SHALL APPLY.

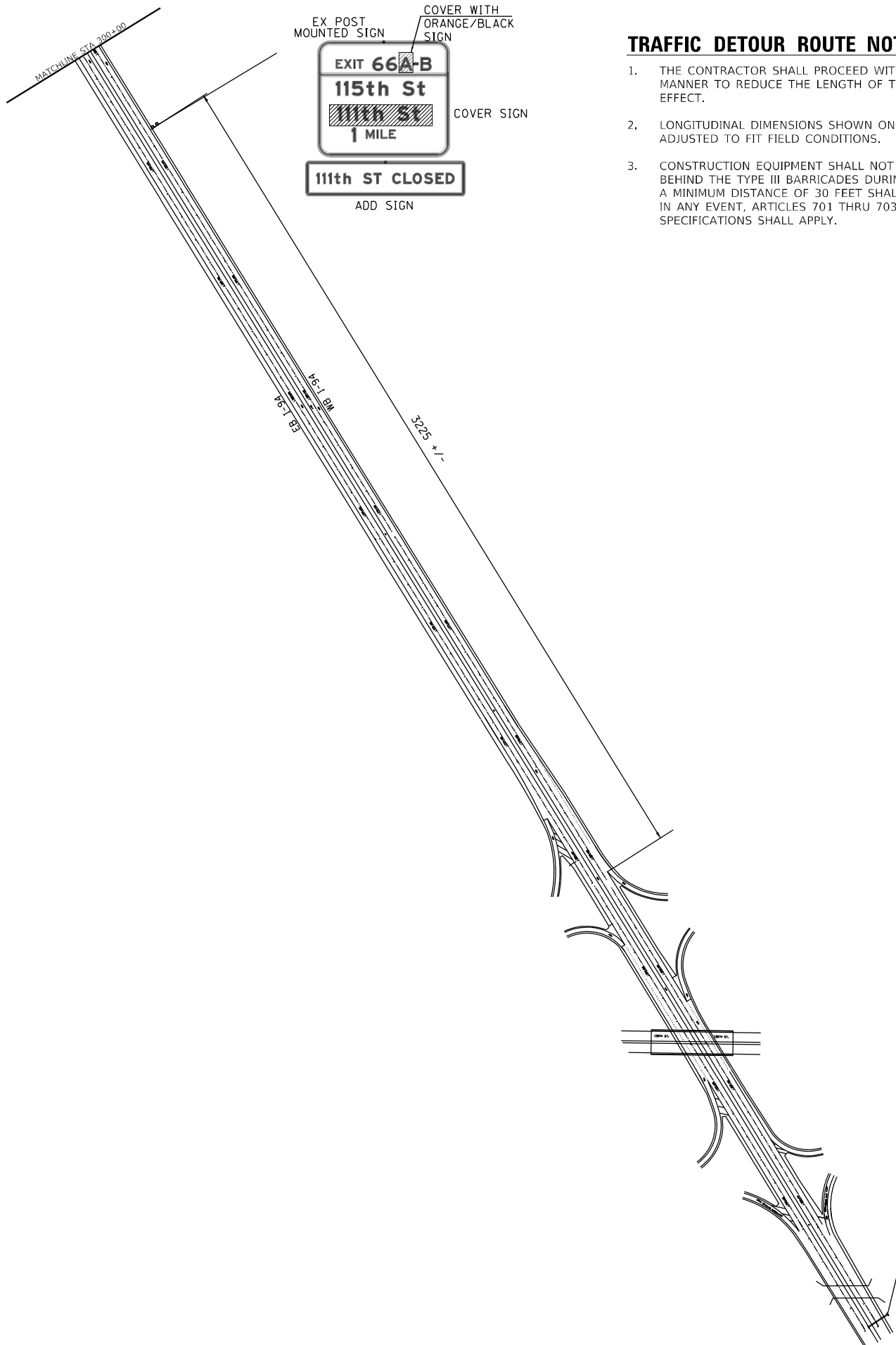
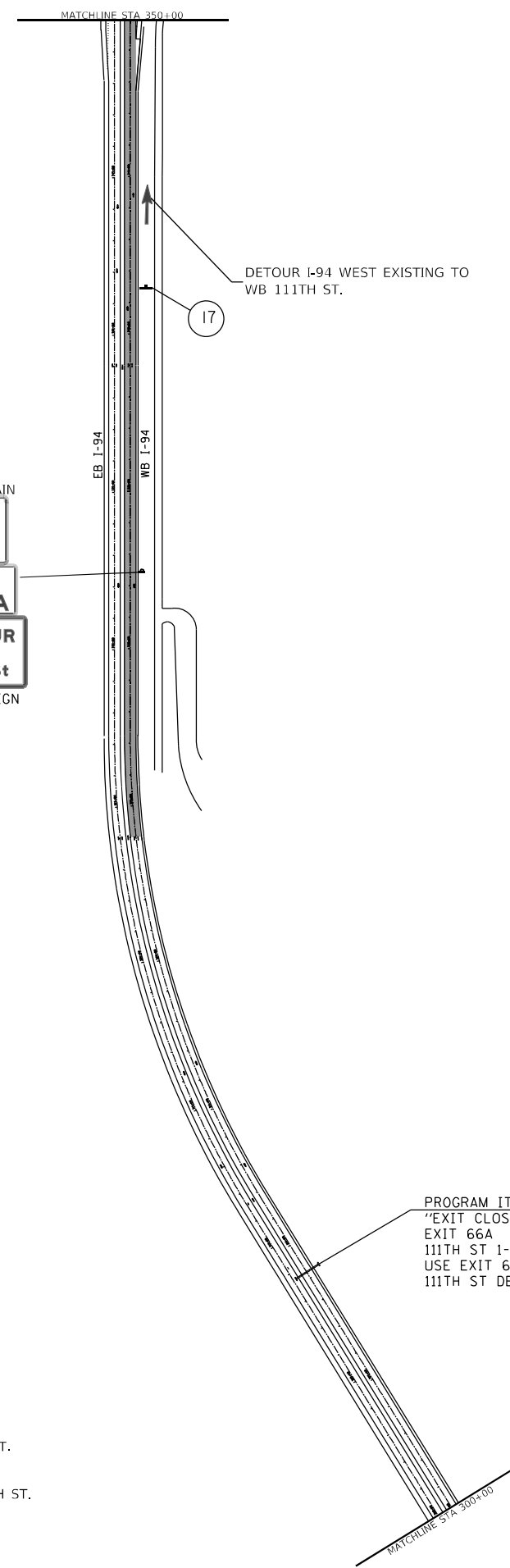


PROGRAM ITS SIGN
 "EXIT CLOSED
 EXIT 66A
 111TH ST 1-MILE
 USE EXIT 66B
 111TH ST DETOUR

DETOUR I-94 WEST EXISTING TO WB 111TH ST.

DETOUR PLAN LEGEND

- SIGN
- SIGN NUMBER
- TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED
- COMPLETELY CLOSED (WORK AREA)
- CLOSE RAMP W/ DRUMS PER STANDARD 701451-01
- RAMP CLOSED
- DETOUR EB 111TH ST. TO I-94 WEST
- DETOUR FRONTAGE ROAD TO WB 111TH ST.
- DETOUR I-94 WEST EXISTING TO WB 111TH ST.



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – DETOUR PLAN
 111TH STREET OVER I-94**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	10
CONTRACT NO. 60R77			ILLINOIS FED. AID PROJECT	



MAINTAIN
EXITS 66A-B
130th St
2 1/2 MILES

MAINTAIN
EXIT 66B
115th St
1/2 MILE

MAINTAIN
EXIT 66A
111th St ↗

DETOUR FRONTAGE RD. TO
 WB 111TH ST.
 &
 DETOUR I-94 WEST EXISTING
 TO WB 111TH ST.

DETOUR EB 111TH ST.
 TO I-94 WEST

MAINTAIN
94 WEST
Chicago Loop
LEFT 2 LANES

MAINTAIN
EXIT 65
103rd St
Stony Island Ave
RIGHT 2 LANES

DETOUR EB 111TH ST. TO I-94 WEST

500' 500' 200' 300'

WB I-94 EXIT RAMP
 TO 111TH ST CLOSED
 USE IDOT D1 STD TC-08

EXIT
CLOSED
 COVER EXISTING
 SIGN

COVER EXISTING
 SIGN

EXIT
66A ↗

RAMP CLOSED

RAMP TO 111th STREET
CLOSED
FOLLOW DETOUR

SPECIAL 84"x30"
 6" BLACK LETTERS ON ORANGE
 REFLECTIVE BACKGROUND

DETOUR FRONTAGE RD. TO
 WB 111TH ST.

DETOUR I-94 WEST EXISTING TO
 WB 111TH ST.

WB I-94 EXIT RAMP
 TO 111TH ST CLOSED
 USE IDOT D1 STD TC-08

EXIT 66A
111th St ↗

RAMP CLOSED

MAINTAIN
EXIT
66B ↗

MAINTAIN
Pullman National
Monument and
Historic Sites
EXIT 66A

EX OVERHEAD MOUNTED SIGNS
 MAINTAIN

EXIT 65
103rd St
Stony Island Ave
1 1/4 MILES

ADD SIGN

EXIT 66A
111th St
1/4 MILE

RAMP CLOSED

MAINTAIN

EXIT 66B
115th St ↗

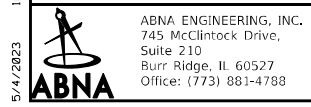
DETOUR
WEST
111th St

ADD SIGN

DETOUR PLAN LEGEND

- SIGN
- # SIGN NUMBER
- TT TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED
- COMPLETELY CLOSED (WORK AREA)
- ○ ○ ○ ○ CLOSE RAMP W/ DRUMS PER STANDARD 701451-01
- ▨ RAMP CLOSED
- ▨ DETOUR EB 111TH ST. TO I-94 WEST
- ▨ DETOUR FRONTAGE ROAD TO WB 111TH ST.
- ▨ DETOUR I-94 WEST EXISTING TO WB 111TH ST.

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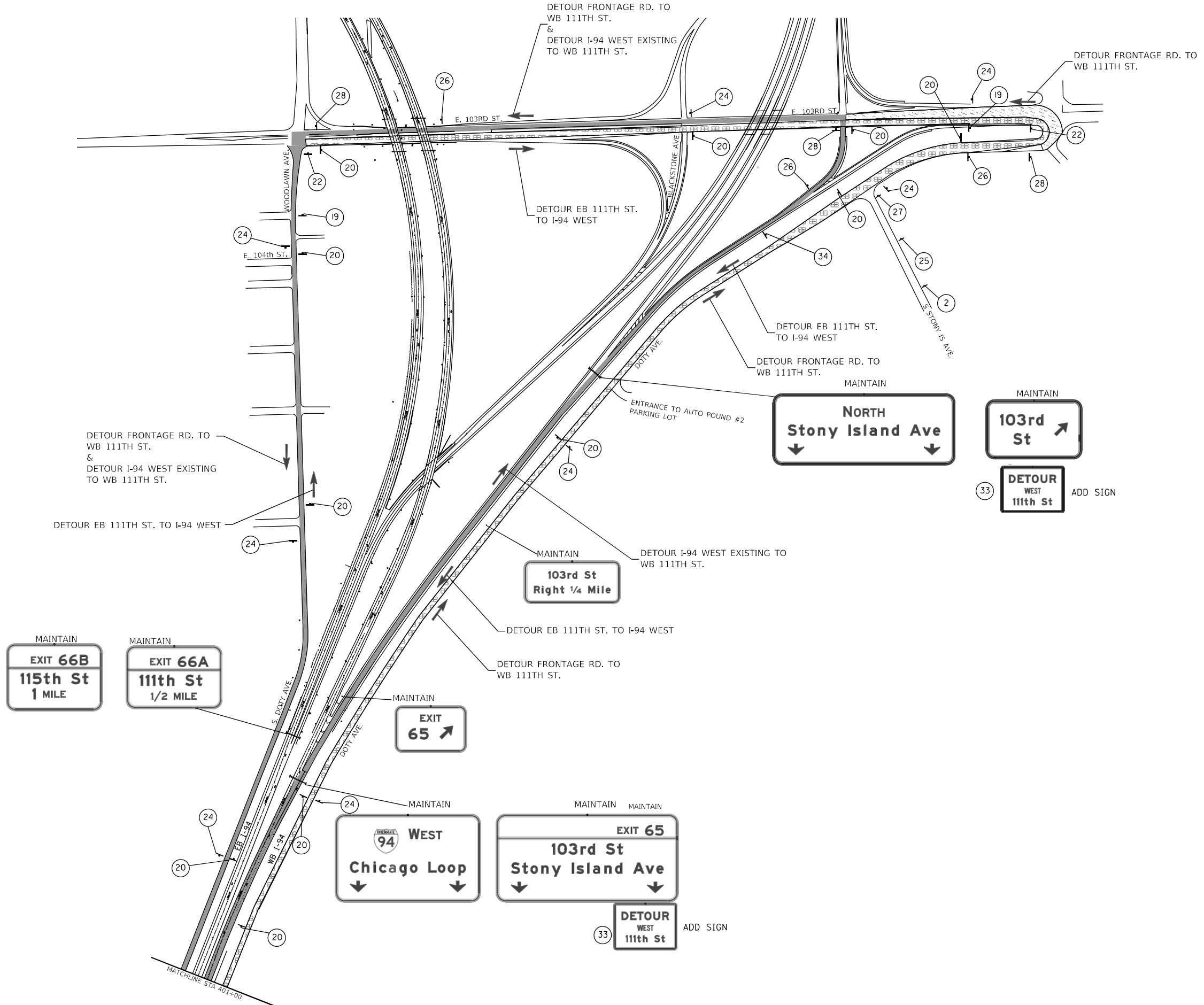
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC – DETOUR PLAN
111TH STREET OVER I-94

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	11
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



DETOUR PLAN LEGEND

- SIGN
- SIGN NUMBER
- TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED
- COMPLETELY CLOSED (WORK AREA)
- CLOSE RAMP W/ DRUMS PER STANDARD 701451-01
- RAMP CLOSED
- DETOUR EB 111TH ST. TO I-94 WEST
- DETOUR FRONTAGE ROAD TO WB 111TH ST.
- DETOUR I-94 WEST EXISTING TO WB 111TH ST.

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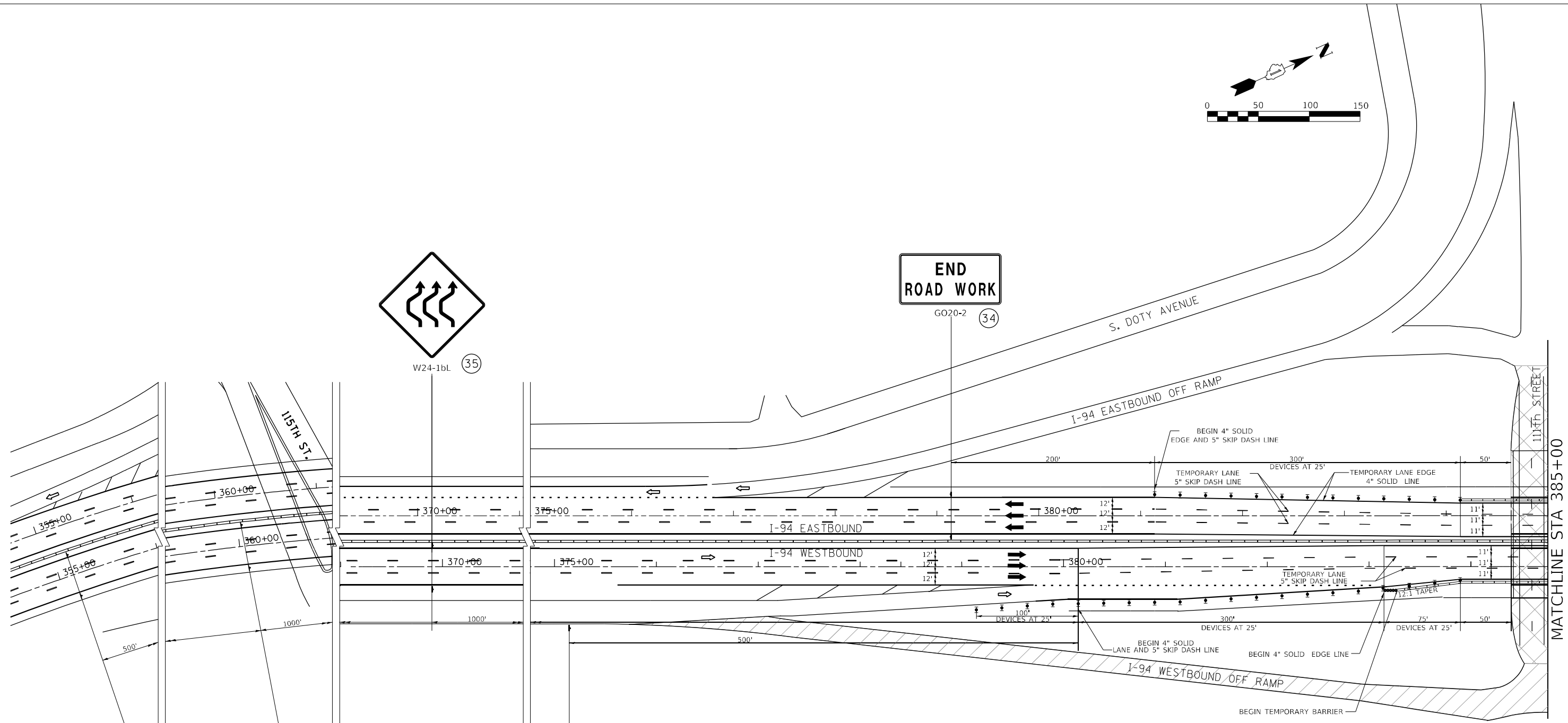
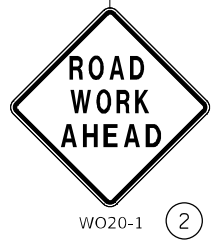
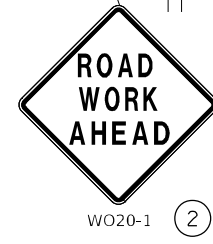
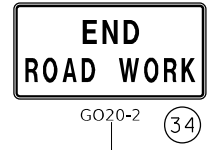
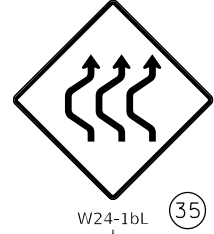
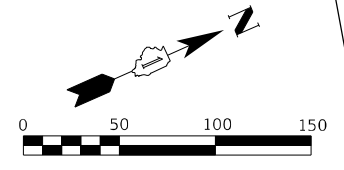
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC – DETOUR PLAN
111TH STREET OVER I-94

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	12
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



- SYMBOLS**
- WORK AREA
 - RAMP CLOSED
 - TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY CONCRETE BARRIER
 - IMPACT ATTENUATOR

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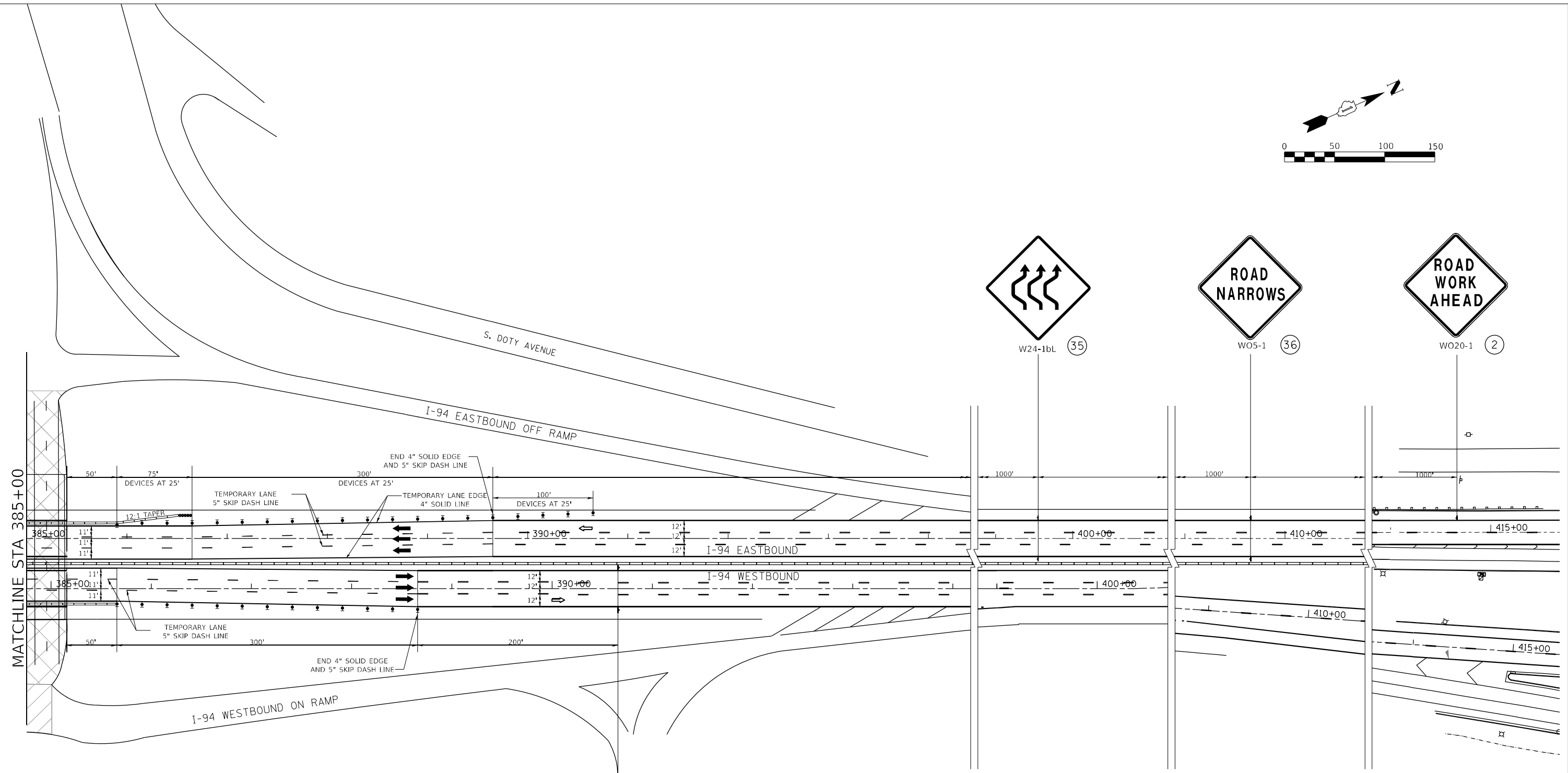
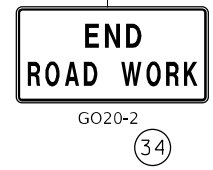
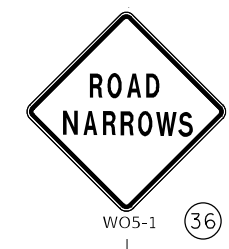
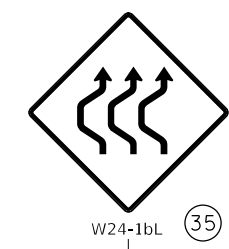
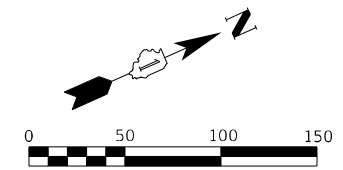
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – LANE SHIFT PLANS I-94 STAGE 1
 111TH STREET OVER I-94**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	13
				CONTRACT NO. 60R77
ILLINOIS FED. AID PROJECT				



- SYMBOLS**
- WORK AREA
 - RAMP CLOSED
 - TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY CONCRETE BARRIER
 - IMPACT ATTENUATOR

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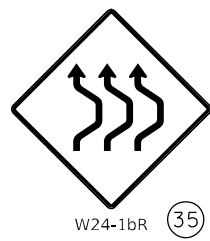
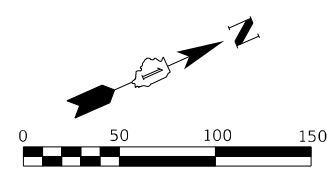
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – LANE SHIFT PLANS I-94 STAGE 1
 111TH STREET OVER I-94**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	14
				CONTRACT NO. 60R77
ILLINOIS FED. AID PROJECT				



**END
ROAD WORK**

GO20-2 (34)

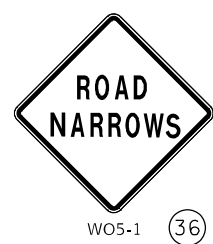
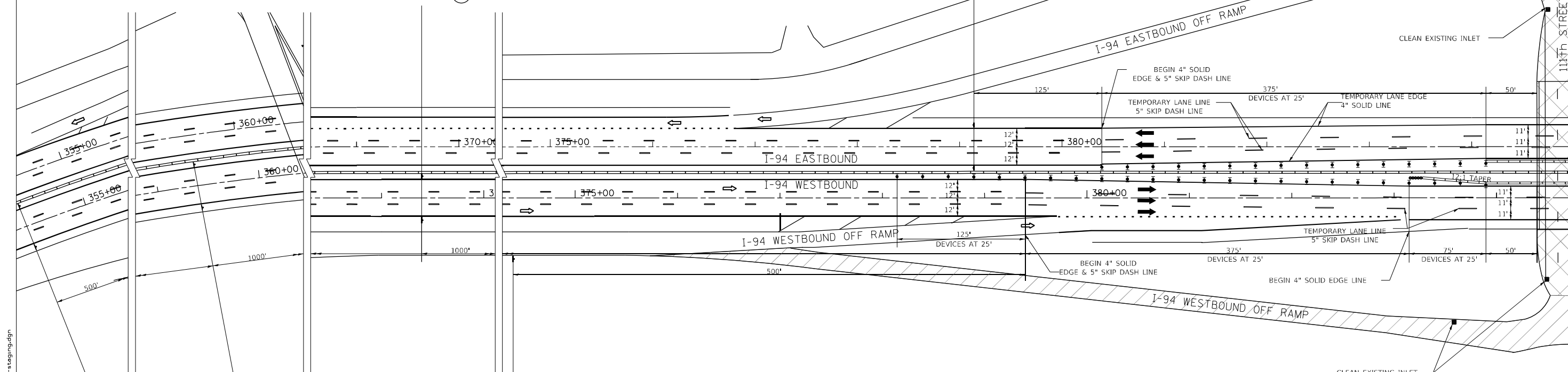
S. DOTY AVENUE

I-94 EASTBOUND OFF RAMP

CLEAN EXISTING INLET

111TH STREET

MATCHLINE STA 385+00



SYMBOLS

- WORK AREA
- RAMP CLOSED
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR

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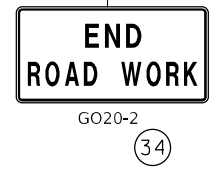
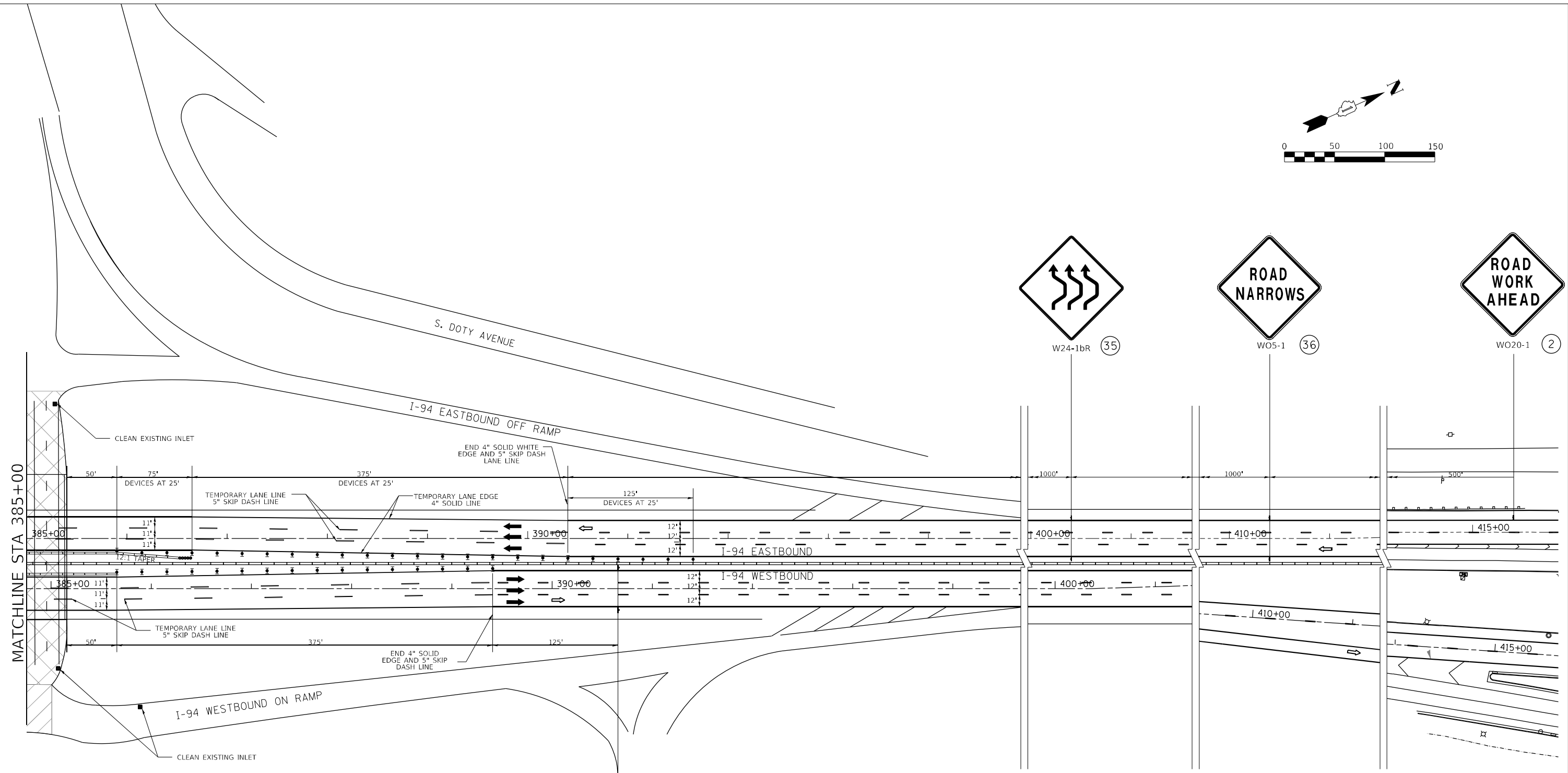
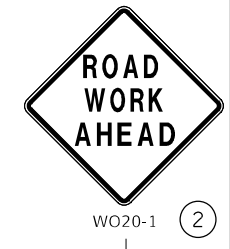
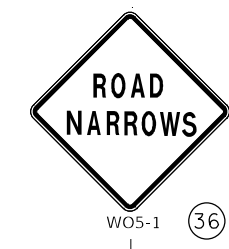
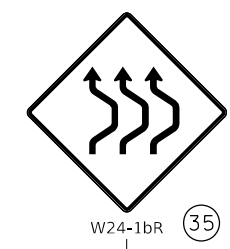
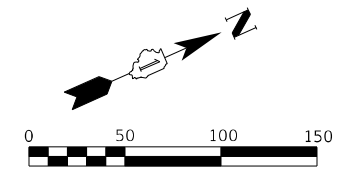
ABNA ENGINEERING, INC.
745 McClintock Drive,
Suite 210
Burr Ridge, IL 60527
Office: (773) 881-4788

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DRAWN	IG	REVISED	
CHECKED	HH	REVISED	
PLOT DATE = 5/9/2023			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – LANE SHIFT PLANS I-94 STAGE 2
111TH STREET OVER I-94**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	15
CONTRACT NO. 60R77			ILLINOIS FED. AID PROJECT	



- SYMBOLS**
- WORK AREA
 - RAMP CLOSED
 - TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - TEMPORARY CONCRETE BARRIER
 - IMPACT ATTENUATOR

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 Burr Ridge, IL 60527
 Office: (773) 881-4788

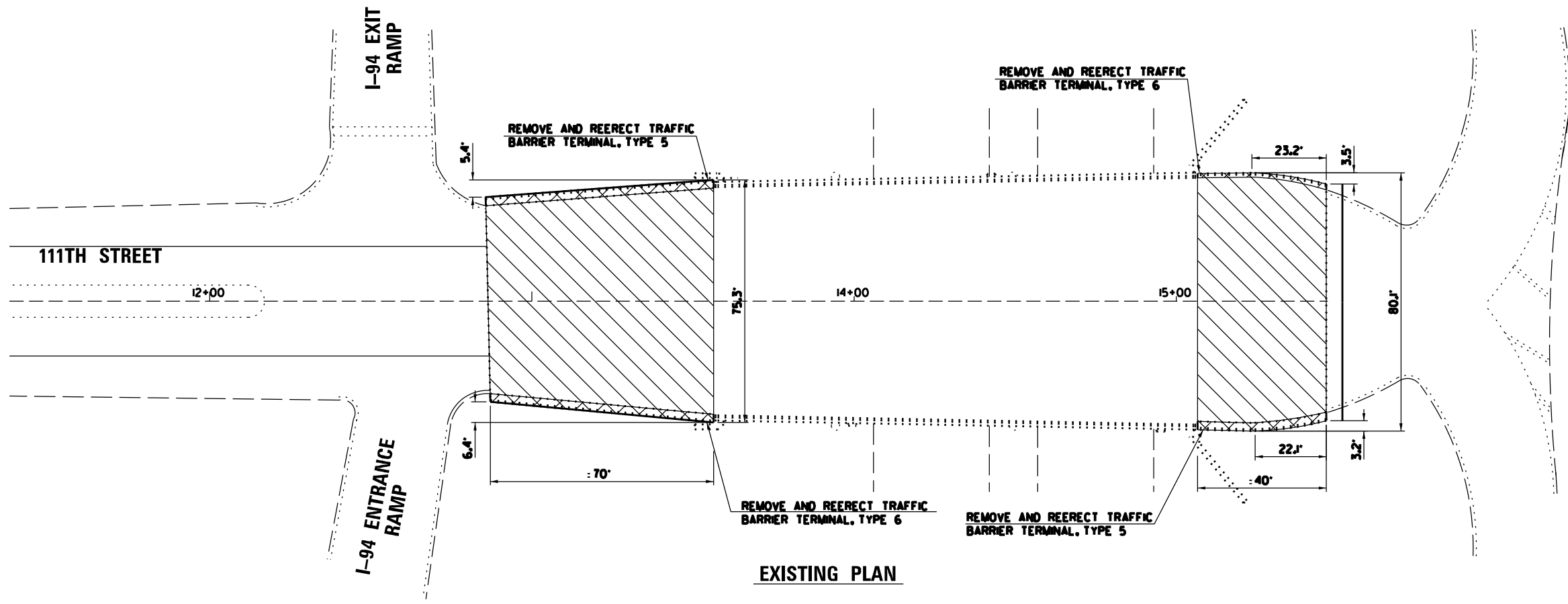
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PLOT DATE = 5/9/2023

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC – LANE SHIFT PLANS I-94 STAGE 2
111TH STREET OVER I-94

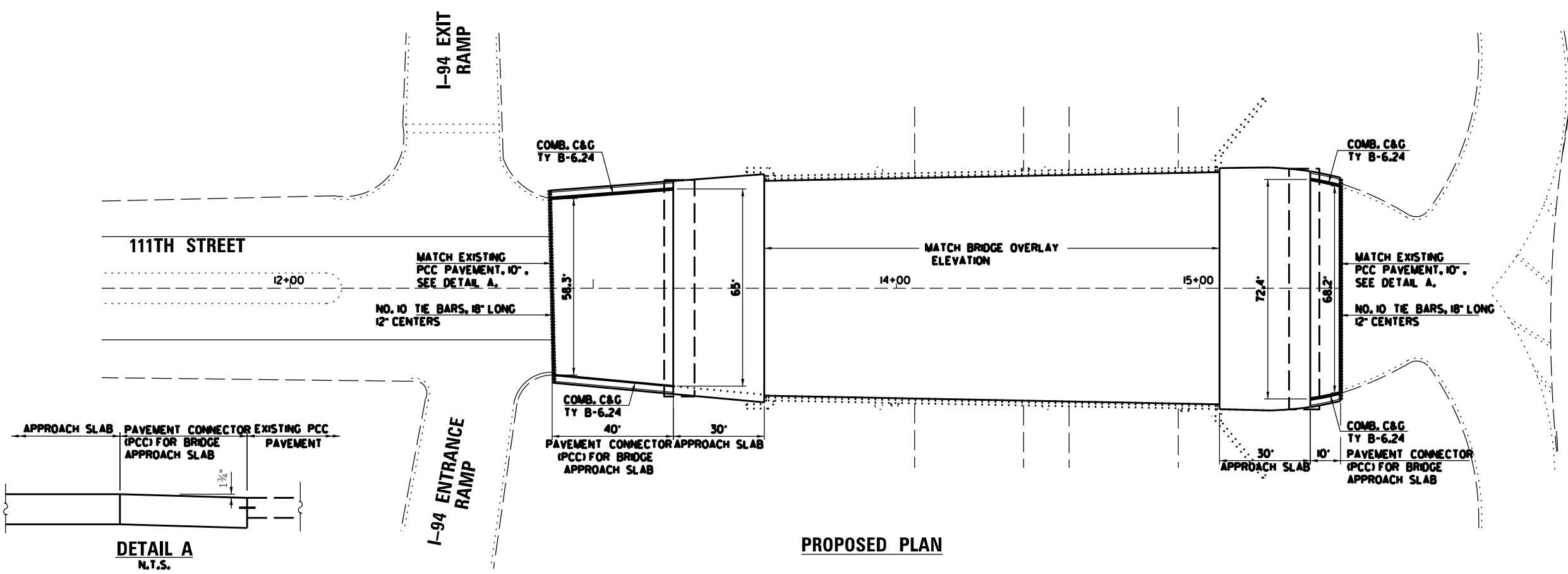
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94	1112-701HB-BR	COOK	59	16
				CONTRACT NO. 60R77
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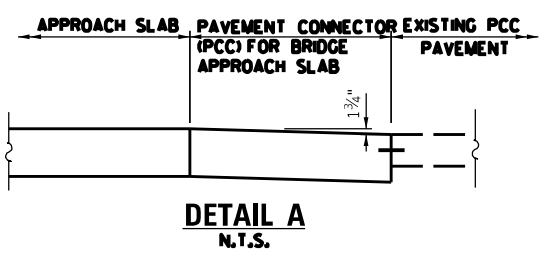
EXISTING PLAN

LEGEND

- APPROACH SLAB REMOVAL
- CURB & GUTTER REMOVAL



PROPOSED PLAN



**DETAIL A
N.T.S.**

NOTES:

1. REFER TO STANDARD 42040I-13.
2. USE AGGREGATE BASE COURSE, TYPE B TO MATCH EXISTING SUBBASE AS SHOWN IN THE STANDARD DRAWINGS.
3. MATCH EXISTING PCC PAVEMENT, 10" AT END OF PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB.

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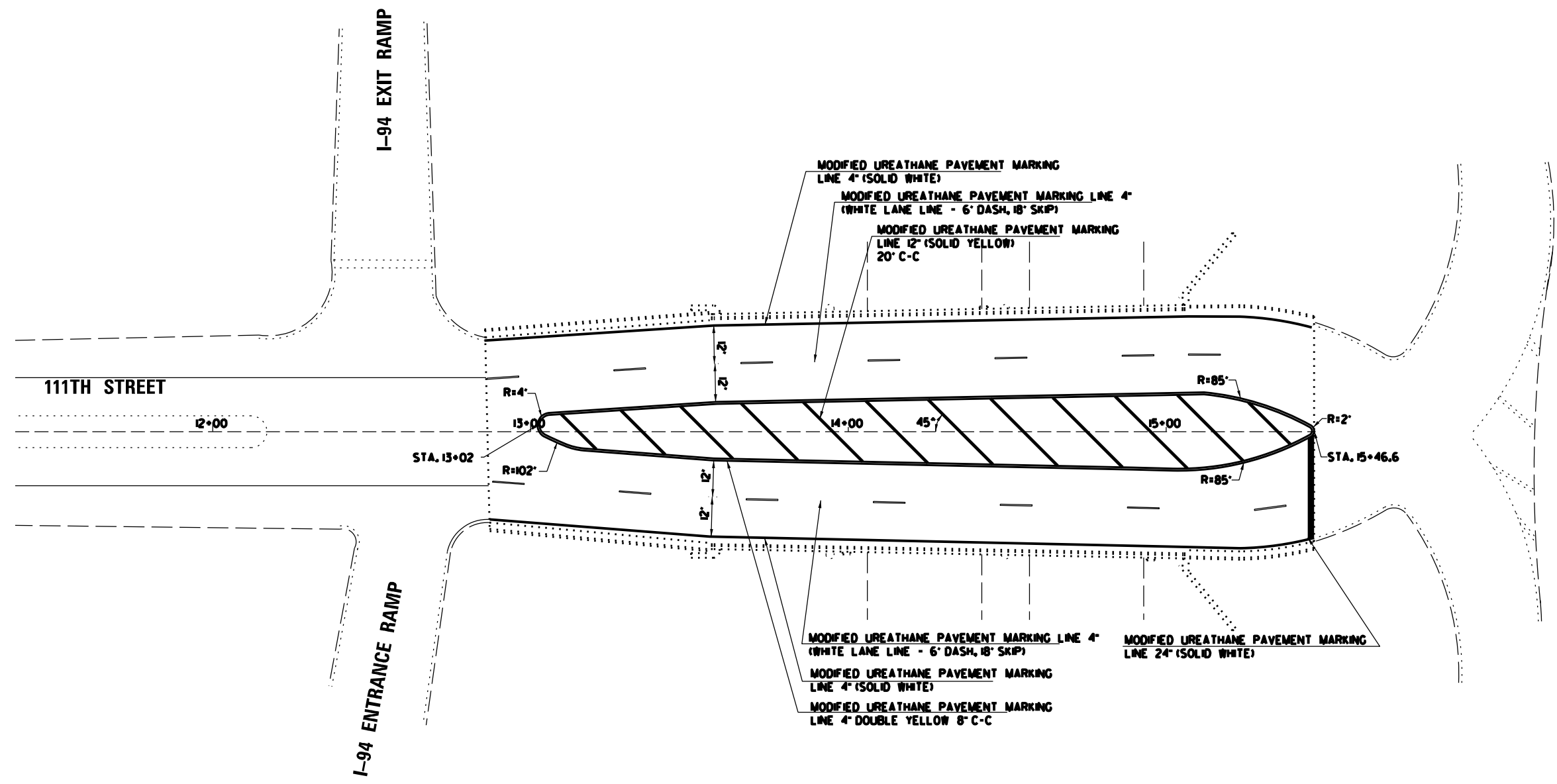
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STATE OF ILLINOIS
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APPROACH ROADWAY TRANSITIONS
111TH STREET OVER I-94

SHEET NO. 1 OF 1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	17
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



NOTE:
WORK THIS DRAWING WITH DISTRICT STANDARD TC-24.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
111TH STREET OVER I-94

SHEET NO. 1 OF 1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	18
ILLINOIS			FED. AID PROJECT	

BENCH MARK:

TBM: Square Cut in top of Concrete wall along \bar{C} of Calumet Expressway at the South end of Pier 2 of E.B. 111th Street Bridge
Elev. = 588.38

EXISTING STRUCTURE:

Built in 1960 as F.A.I. Route 90, Section 1112-701HB. Structure consist of a 3 span wide flange pin connect configuration. The structure is continuous from the pin to the East Abutment and simple supported from the pin to the West Abutment. The structure has a variable bridge width of 74'-0" to 79'-6 $\frac{3}{4}$ " 0. to 0. and a 151'-2 $\frac{1}{4}$ " Bk to Bk. of Abutment. Structure is to be rehabilitated.

Road to be closed and traffic to be directed through detour.

SCOPE OF WORK:

1. Reconstruct PJS deck joints with preformed strip seal joints.
2. $\frac{3}{4}$ " Scarification of bridge deck.
3. Place 2 $\frac{3}{4}$ " latex concrete overlay on bridge deck.
4. Diamond grind $\frac{1}{4}$ " and Bridge Grooving (Longitudinal) on bridge deck overlay.
5. Apply Protective Coat over new latex concrete overlay and approach slab.
6. Seal parapets.
7. Install steel repair plates on beams.
8. Clean and paint all structural steel.
9. Structural repairs of concrete to substructures.
10. Remove and Replace Approach Slabs.

DESIGN STRESSES

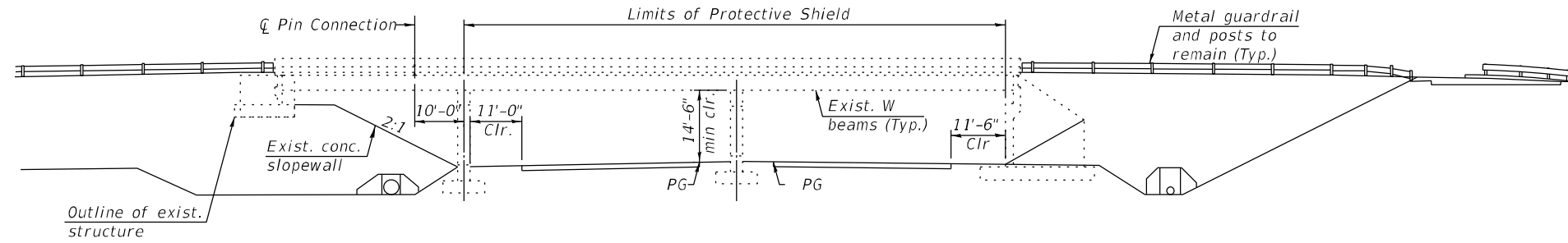
$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure)
 $f_y = 36,000$ psi (M270 Grade 36)
 $f_y = 60,000$ psi (reinf.)

DESIGN SPECIFICATIONS

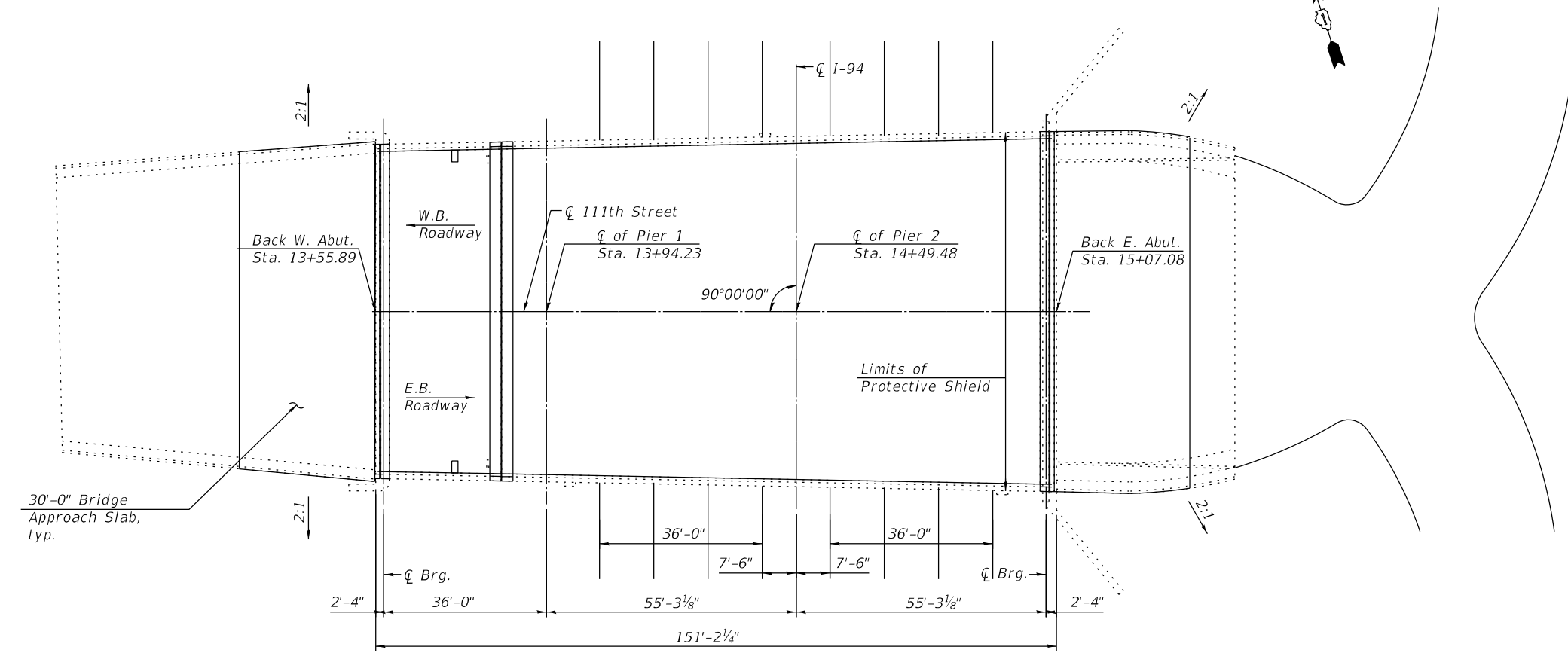
2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

LOADING HS 20-44

Original Construction



ELEVATION

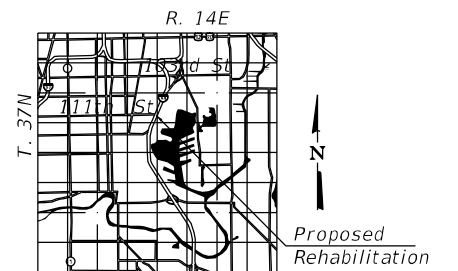


PLAN

Note:
Protective shield shall not interfere with existing underpass lighting.



BY: *Robert L. Peters* DATE: 03/16/2023
ROBERT L. PETERS EXPRES: 11/30/2024



LOCATION SKETCH

**GENERAL PLAN AND ELEVATION
111TH STREET OVER
I-94 (BISHOP FORD)
FAU RTE 1582 SEC 1112-701HB-BR
COOK COUNTY
STATION 41+62.67
STRUCTURE NO. 016-0992**

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630-915-8861

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-0992**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	19
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

SHEET NO. 1 OF 25 SHEETS

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

All new fasteners shall be high strength bolts. Holes shall be 15/16" dia. for 7/8" dia. bolts, unless otherwise noted.

The existing structural steel coating contains lead. The Contractor shall take appropriate precaution to deal with the presence of lead on this project.

Existing reinforcement extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Cost included with Concrete Removal. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All beams, bearings and other structural steel shall be cleaned per Near White Blast Cleaning (SSPC-SP10).

The designated areas cleaned per Near White Blast Cleaning (SSPC-SP10) shall be painted according to the requirements of the Organic Zinc-Rich / Epoxy / Urethane paint system. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown (Munsell No. 2. 5 YR3/4).

SSPC QP1 and SSPC QP2 Certification is required for this Contract.

Before structural steel repairs are started, a temporary revised clearance must be submitted in accordance with the directions on form OPER1306.

After the repairs, the revised clearance must be measured and submitted in accordance with the directions on form OPER1306.

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Typical Deck Section
4. Deck Repair Plan
5. Deck Details
6. Joint Replacement - West Abutment
7. Joint Replacement - East Abutment
8. Joint Replacement - Pin Connection
9. Preformed Joint Strip Seal
10. West Bridge Approach Slab Details
11. West Bridge Approach Slab Details
12. East Bridge Approach Slab Details
13. East Bridge Approach Slab Details
14. Steel Repairs
15. Steel Repairs
16. Substructure Repairs
17. Substructure Repairs
- 18 - 25. Record Drawings

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	31.0	-	31.0
Protective Shield	Sq. Yd.	1,238	-	1,238
Concrete Structures	Cu. Yd.	46.5	-	46.5
Concrete Superstructure	Cu. Yd.	35.6	-	35.6
Protective Coat	Sq. Yd.	1,729	-	1,729
Concrete Superstructure (Approach Slab)	Cu. Yd.	292.5	-	292.5
Reinforcement Bars, Epoxy Coated	Pound	92,380	-	92,380
Preformed Joint Strip Seal	Foot	229	-	229
Concrete Sealer	Sq. Ft.	1,051	-	1,051
Epoxy Crack Injection	Foot	103	-	103
Cleaning and Painting Exposed Rebar	Sq. Ft.	120	-	120
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1,126	-	1,126
Bridge Cleaning and Painting Warranty	L. Sum	1	-	1
Structural Steel Repair	Pound	15,990	-	15,990
Bridge Deck Latex Concrete Overlay, 2¾ Inches	Sq. Yd.	1,142	-	1,142
Containment and Disposal of Lead Paint Cleaning Residues No. 1	L. Sum	1	-	1
Cleaning and Painting Steel Bridge No. 1	L. Sum	1	-	1
Bridge Deck Scarification ¾"	Sq. Yd.	1,142	-	1,142
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	-	384	384
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.	-	175	175
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	2.0	-	2.0
Drainage Scuppers to be Adjusted	Each	2	-	2
Diamond Grinding (Bridge Section)	Sq. Yd.	1,677	-	1,677
Temporary Shoring and Cribbing	Each	-	11	11

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 630-915-8861
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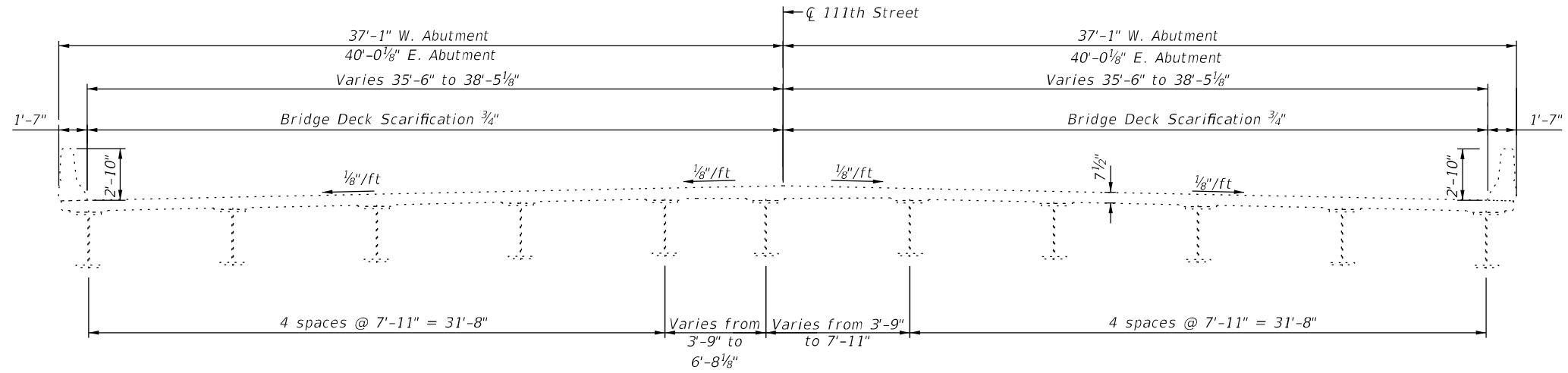
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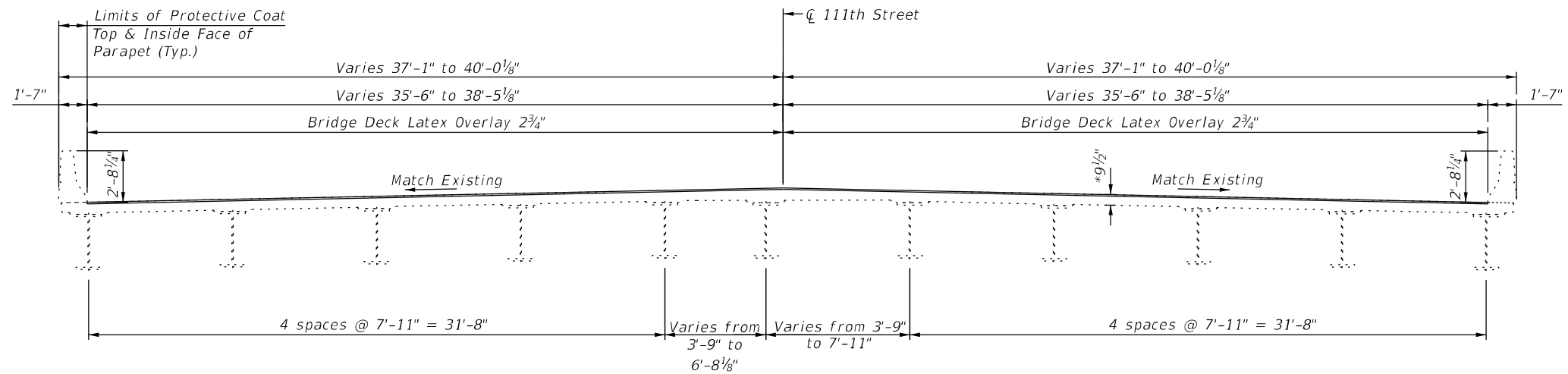
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 016-0992
 SHEET NO. 2 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	20
CONTRACT NO. 60R77				
		ILLINOIS	FED. AID PROJECT	



EXISTING CROSS SECTION
Looking East



PROPOSED CROSS SECTION
Looking East
* Prior to Grinding

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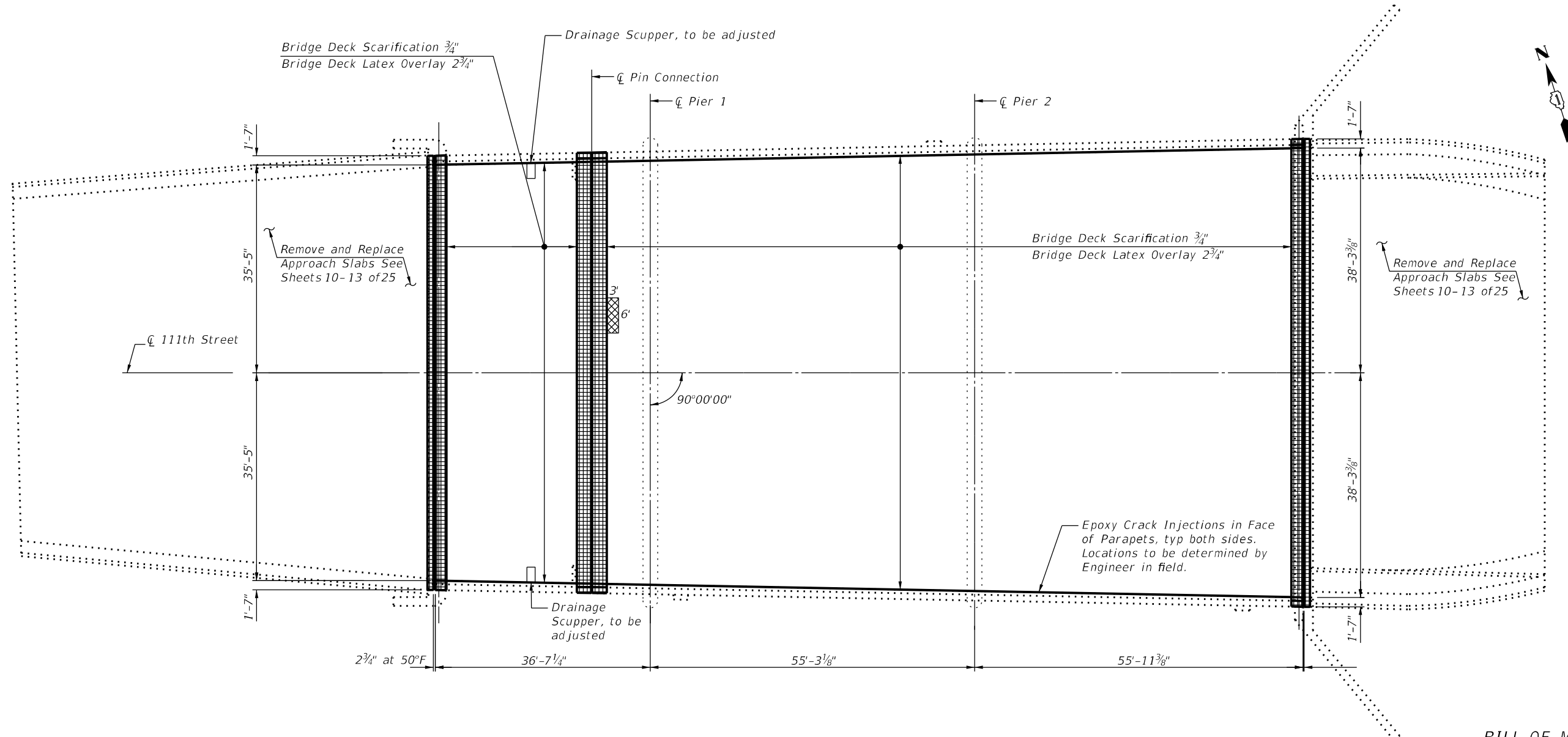
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL DECK SECTION
STRUCTURE NO. 016-0992

SHEET NO. 3 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	21
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



DECK PLAN

LEGEND

	Deck Slab Repair (Full Depth, Type II)
	Area of Joint Repair

NOTE:
 Repair of the existing deck slab shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	2.0
Protective Coat	Sq. Yd.	1,729
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1,126
Concrete Sealer	Sq. Ft.	1,051
Epoxy Crack Injection	Foot	103
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	1,142
Bridge Deck Scarification 3/4"	Sq. Yd.	1,142
Diamond Grinding (Bridge Section)	Sq. Yd.	1,677

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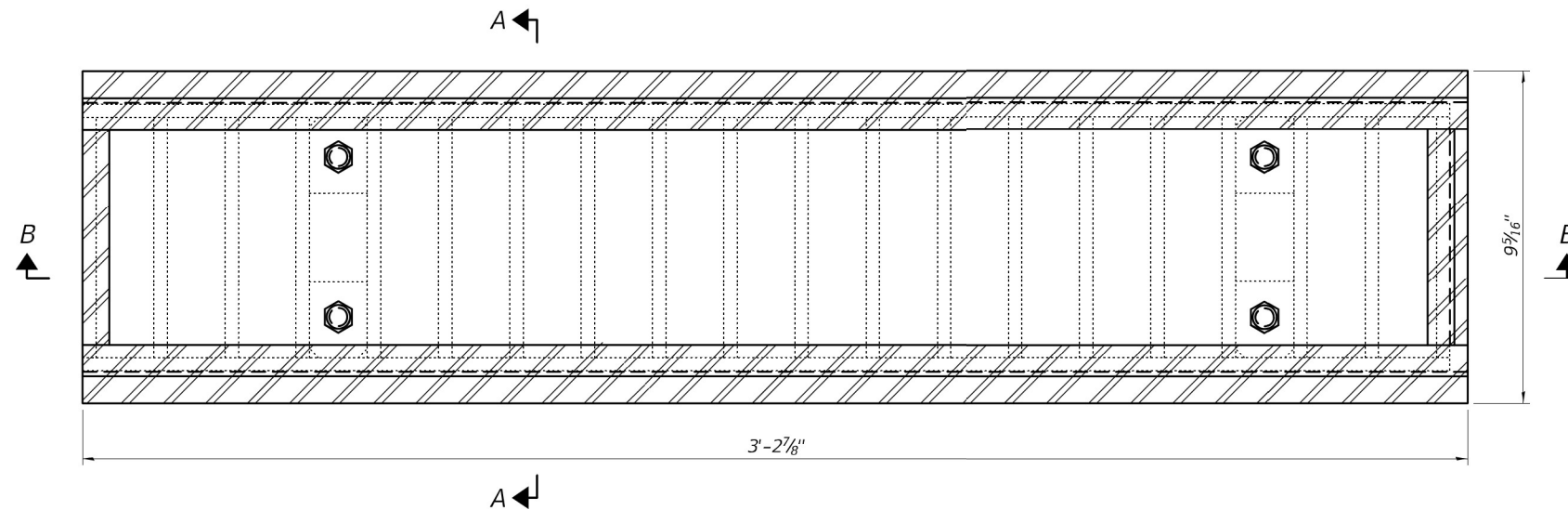
**DECK REPAIR PLAN
 STRUCTURE NO. 016-0992**

SHEET NO. 4 OF 25 SHEETS

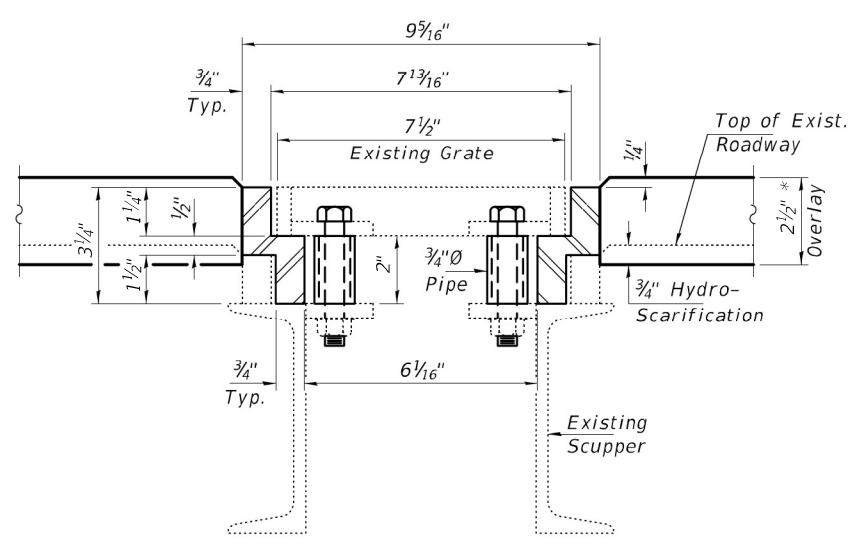
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94	1112-701HB-BR	COOK	59	22
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

NOTES

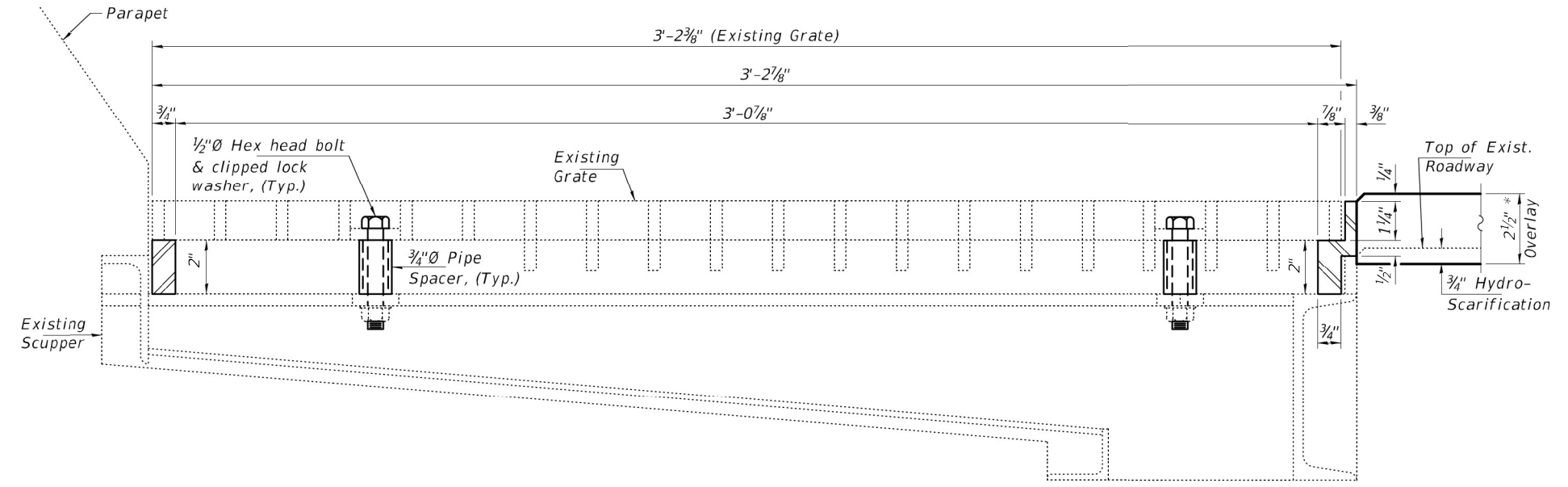
All structural steel shall conform to AASHTO Classification M-270 Gr. 36.
 The adjusting scupper ring, 3/4"Ø pipe sleeve spacers, bolts, and lock washers shall be hot dip galvanized according to Special Provision "Hot Dip Galvanizing for Structural Steel".
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.
 The contractor shall ensure that no damage is done to existing grates to be reused.
 Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for Drainage Scuppers to be Adjusted.



PLAN



SECTION A-A
* After 1/4" Grinding



SECTION B-B
* After 1/4" Grinding

- Adjusting Steel Scupper Ring

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper to be Adjusted	Each	2

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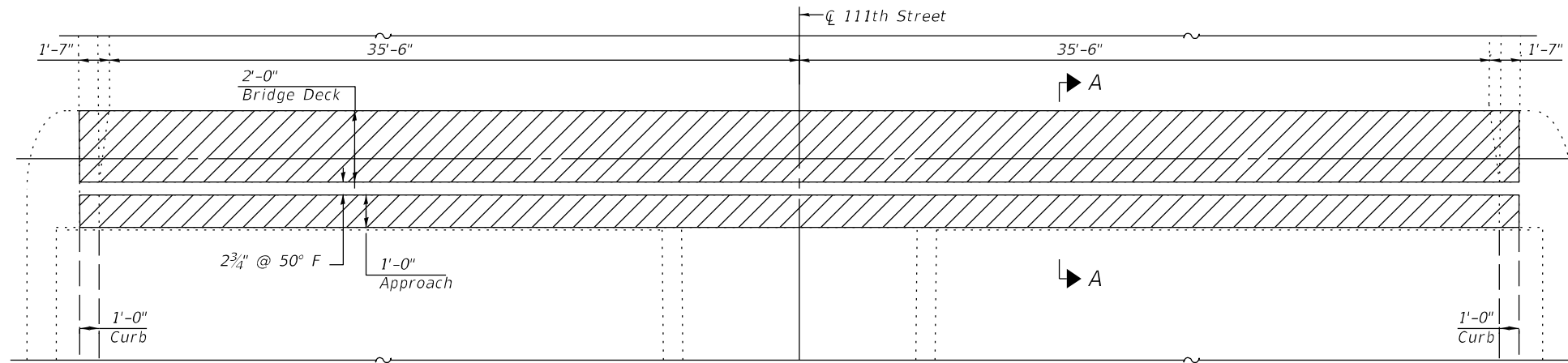
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DEPARTMENT OF TRANSPORTATION

DECK DETAILS
STRUCTURE NO. 016-0992

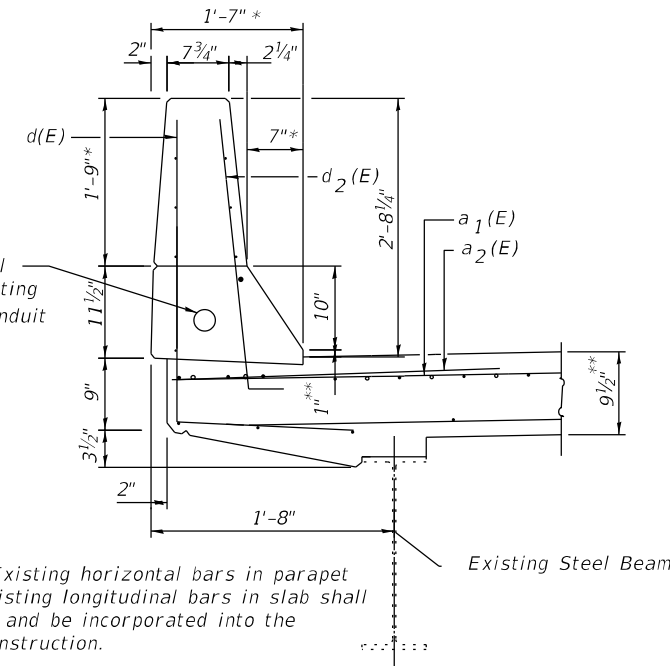
SHEET NO. 5 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	23
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 60R77



EXISTING WEST ABUTMENT



SECTION THRU PARAPET AT JOINT

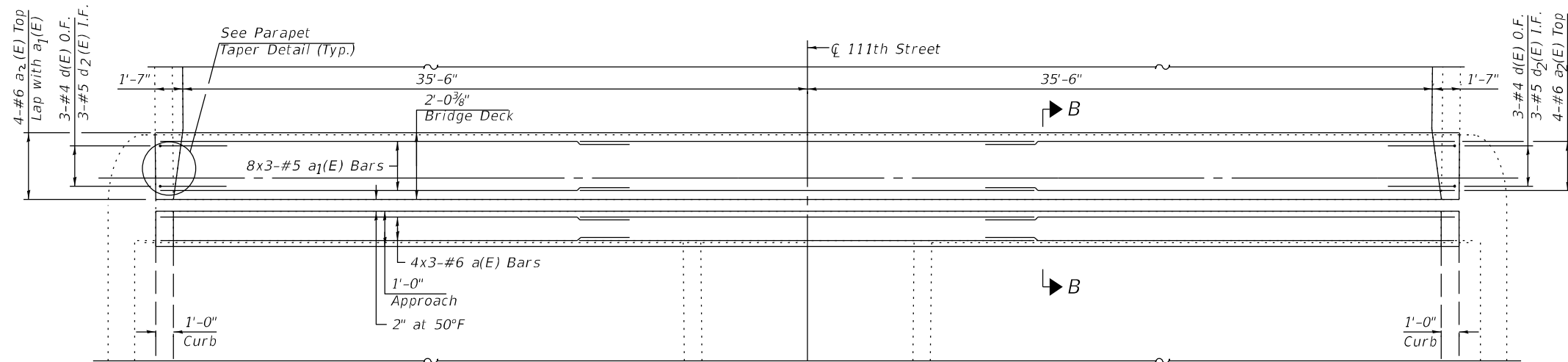
* Match Existing Taper
** Before grinding

Contractor shall maintain Existing 2 1/2\"/>

Contract shall maintain Existing 2 1/2\"/>

Note: Existing horizontal bars in parapet and existing longitudinal bars in slab shall remain and be incorporated into the new construction.

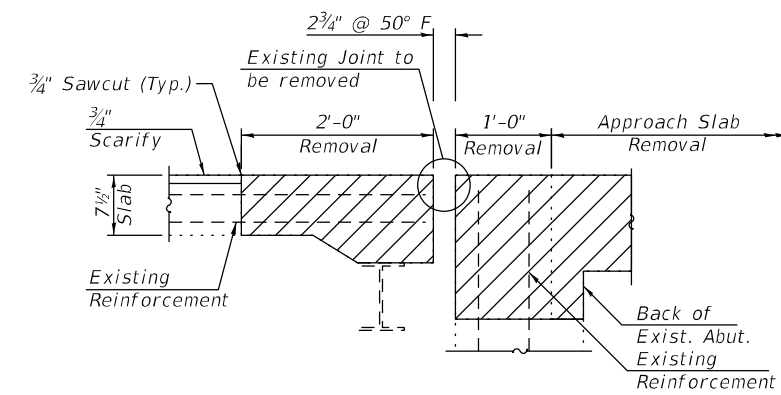
Existing Steel Beam



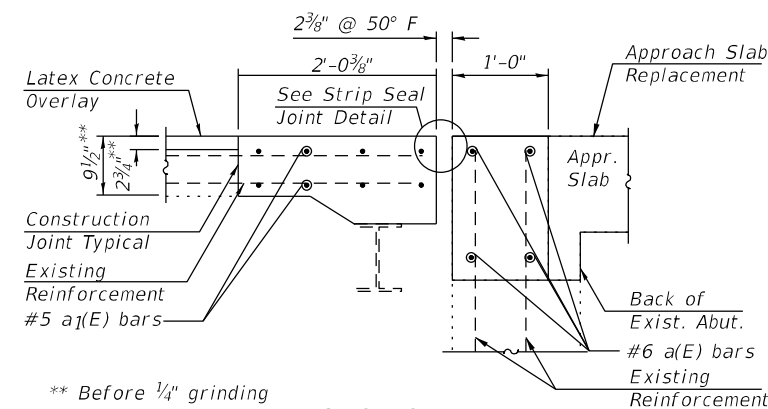
PROPOSED WEST ABUTMENT

JOINT REPAIR NOTES

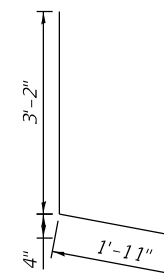
- Existing reinforcement bars shown are to be cleaned and incorporated into new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.
- Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.
- If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.
- Traffic barrier terminals that fall within the limits of Concrete Removal shall be removed and reinstalled in their original locations in accordance with District 1 Std. BM-21.
- The Contractor shall exercise extreme care with any existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.



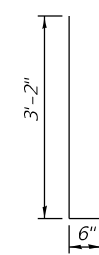
SECTION A-A



SECTION B-B



BAR d(E)



BAR d2(E)

LEGEND



BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
a (E)	12	#6	27'-3"	—
a1(E)	24	#5	26'-9"	—
a2(E)	8	#6	6'-6"	—
d(E)	6	#4	5'-1"	┌
d2(E)	6	#5	3'-9"	┌
Reinforcement Bars, Epoxy Coated			Pound	1,290
Concrete Removal			Cu. Yd.	8.8
Concrete Superstructure			Cu. Yd.	10.4

Bars indicated thus 4 x 2-#4 etc. indicates 4 lines of bars with 2 lengths per line.

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DRAWN	PAF
CHECKED	RLP
PLOT DATE = 5/10/2023	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

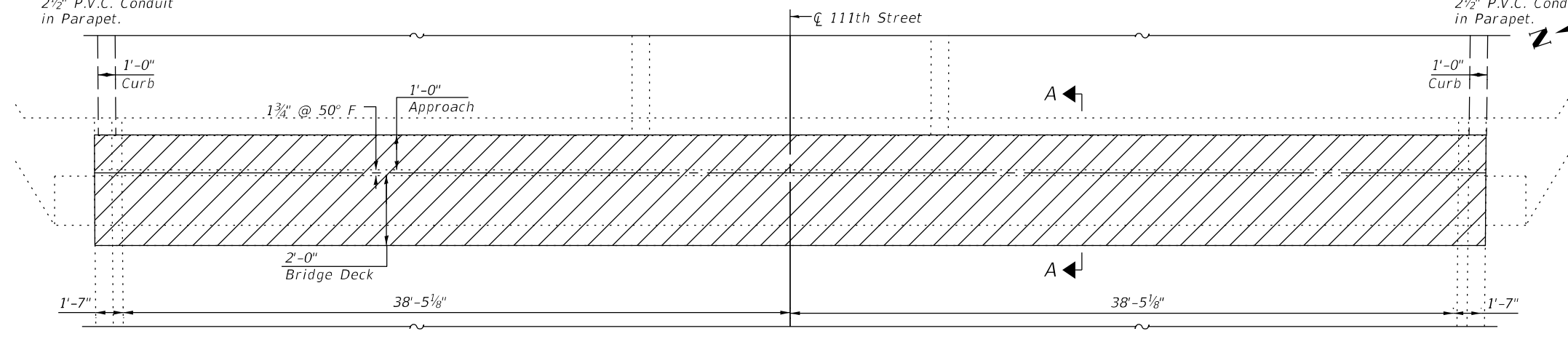
JOINT REPLACEMENT - WEST ABUTMENT
STRUCTURE NO. 016-0992

SHEET NO. 6 OF 25 SHEETS

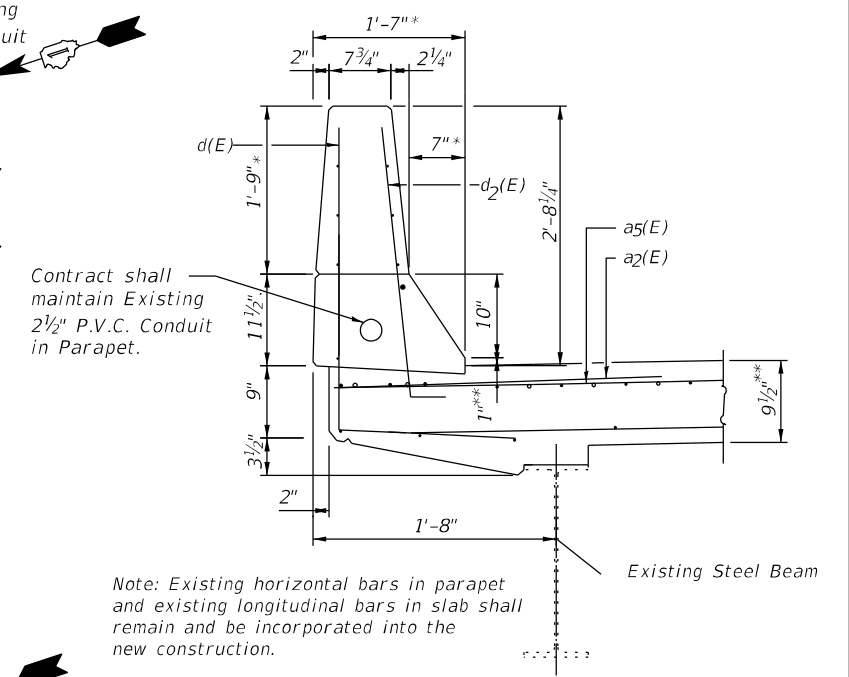
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	24
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

Contract shall maintain Existing 2 1/2" P.V.C. Conduit in Parapet.

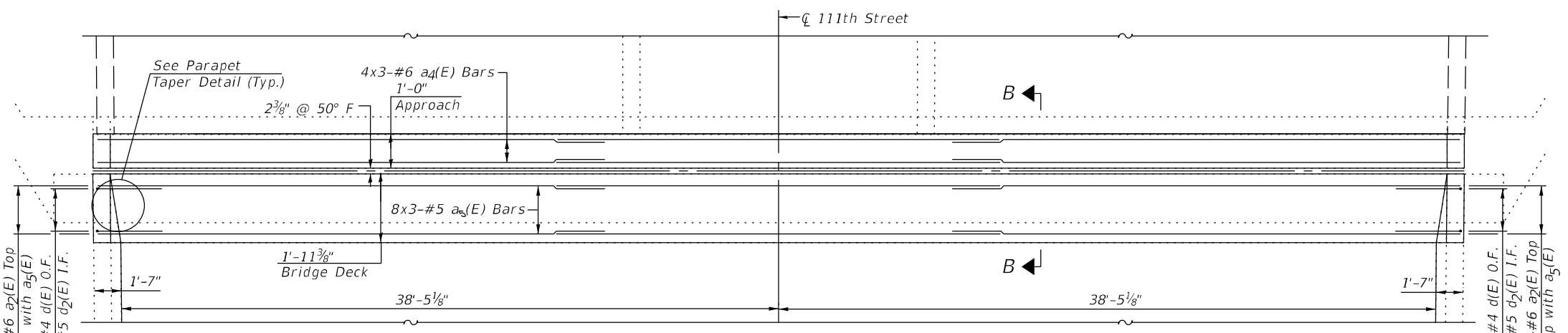
Contract shall maintain Existing 2 1/2" P.V.C. Conduit in Parapet.



EXISTING EAST ABUTMENT



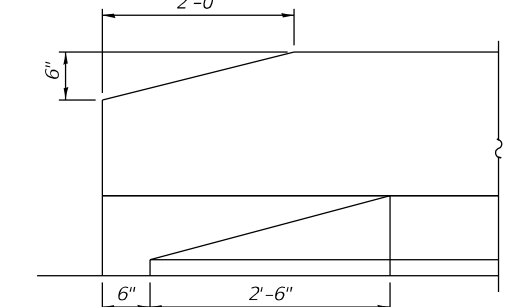
Note: Existing horizontal bars in parapet and existing longitudinal bars in slab shall remain and be incorporated into the new construction.



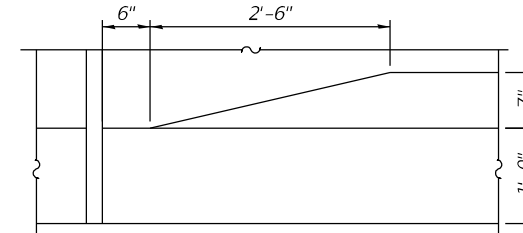
PROPOSED EAST ABUTMENT

SECTION THRU PARAPET AT JOINT

* Match Existing Taper
** Before 1/4" grinding



ELEVATION



PLAN

PARAPET TAPER DETAIL

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
a2(E)	8	#6	6'-6"	—
a4(E)	12	#6	29'-3"	—
a5(E)	24	#5	28'-9"	—
d(E)	6	#4	5'-1"	—
d2(E)	6	#5	3'-9"	—
Reinforcement Bars, Epoxy Coated			Pound	1,370
Concrete Removal			Cu. Yd.	10.6
Concrete Superstructure			Cu. Yd.	11.1

Bars indicated thus 4 x 2-#4 etc. indicates 4 lines of bars with 2 lengths per line

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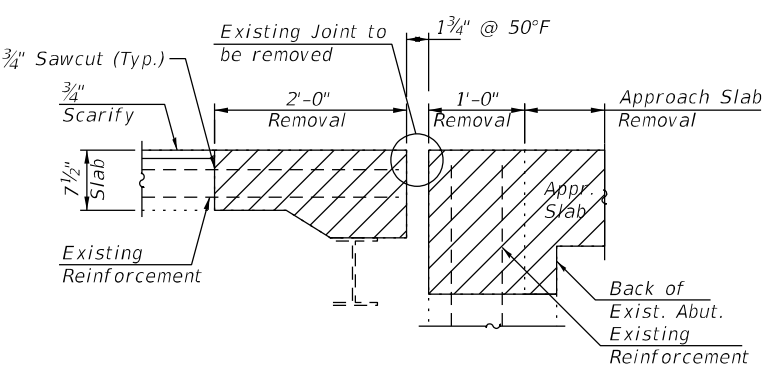
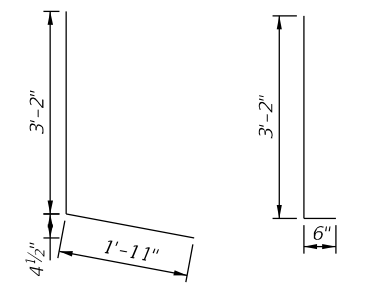
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

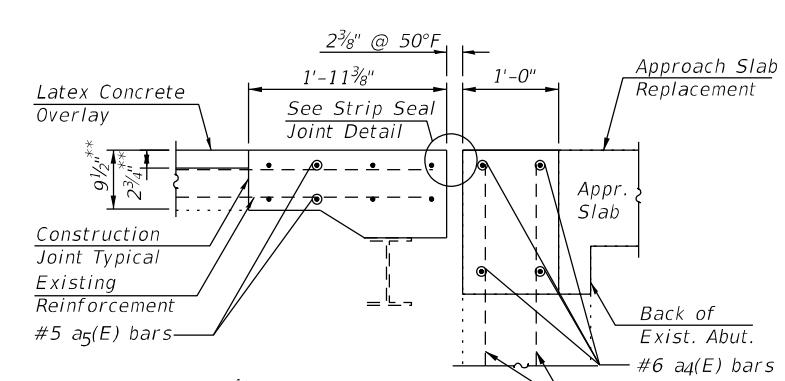
JOINT REPLACEMENT - EAST ABUTMENT
STRUCTURE NO. 016-0992

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	25
CONTRACT NO. 60R77				
ILLINOIS		FED. AID PROJECT		

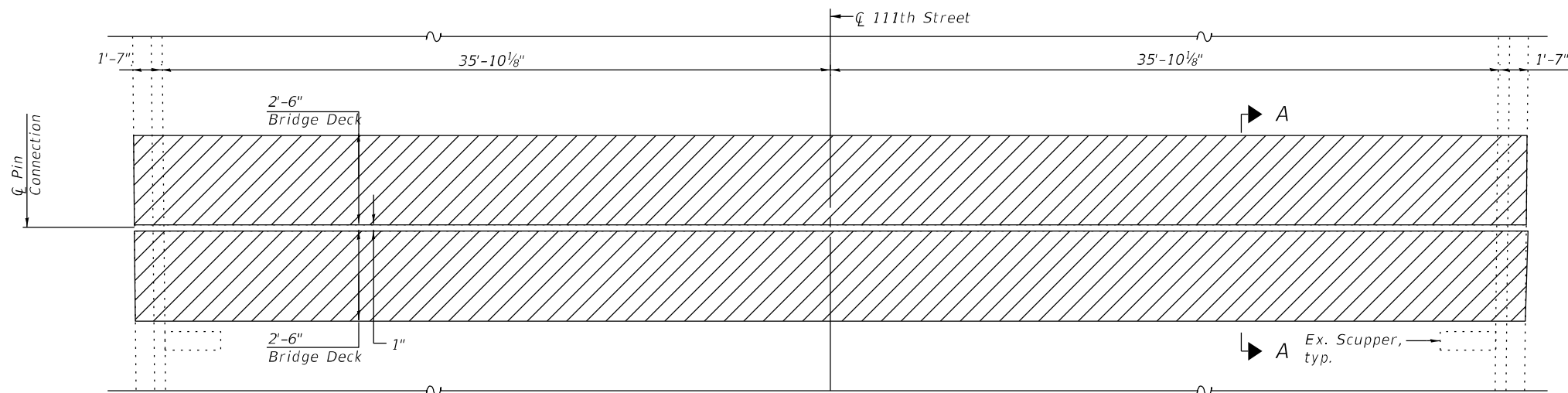
LEGEND



SECTION A-A



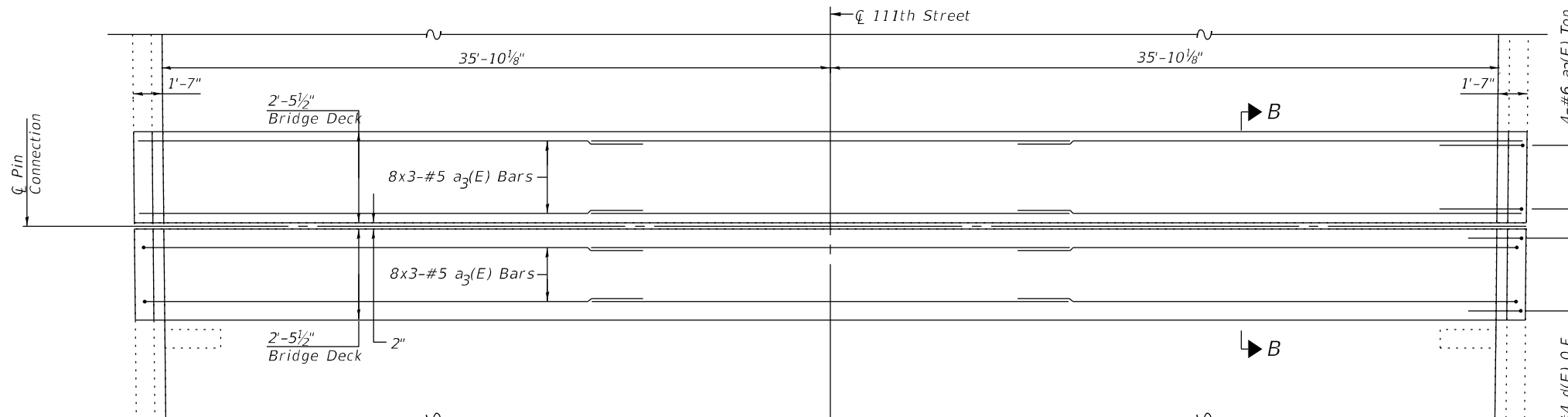
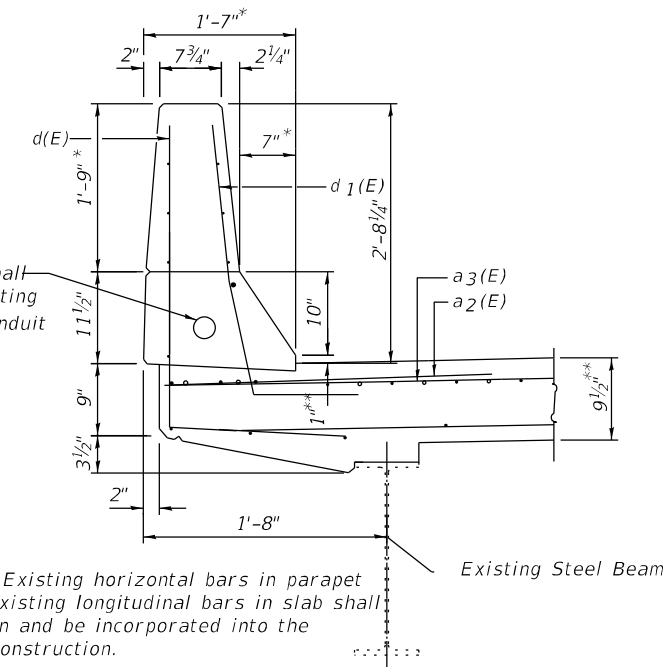
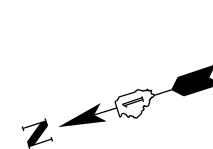
SECTION B-B



Contractor shall maintain existing 2 1/2" P.V.C. Conduit in Parapet.

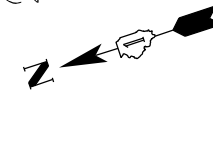
EXISTING PIN CONNECTION JOINT

Contractor shall maintain existing 2 1/2" P.V.C. Conduit in Parapet.



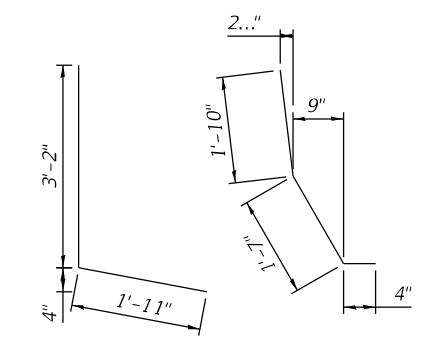
PROPOSED PIN CONNECTION JOINT

4-#6 a2(E) Top Lap with a3(E) (Typ. ea. corner 4 Total)
3-#4 d1(E) O.F. (Typ. ea. corner 4 total)

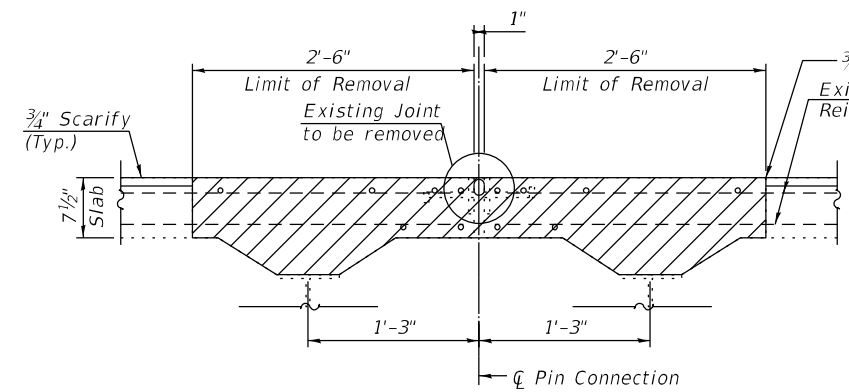


SECTION THRU PARAPET AT JOINT

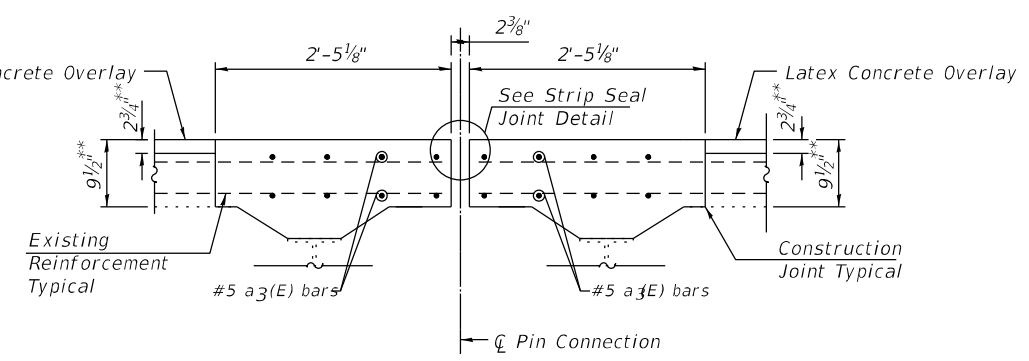
* Match Existing Taper
** Before 1/4" Grinding



BAR d(E) BAR d1(E)



SECTION A-A



SECTION B-B

** Before 1/4" Grinding

LEGEND

	Concrete Removal
--	------------------

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE	
a2(E)	16	#6	6'-6"	—	
a3(E)	48	#5	27'-3"	—	
d(E)	12	#4	5'-1"	┌	
d1(E)	12	#5	3'-9"	┌	
Reinforcement Bars, Epoxy Coated				Pound	1,610
Concrete Removal				Cu. Yd.	11.6
Concrete Superstructure				Cu. Yd.	14.1

Bars indicated thus 4 x 2-#4 etc. indicates 4 lines of bars with 2 lengths per line

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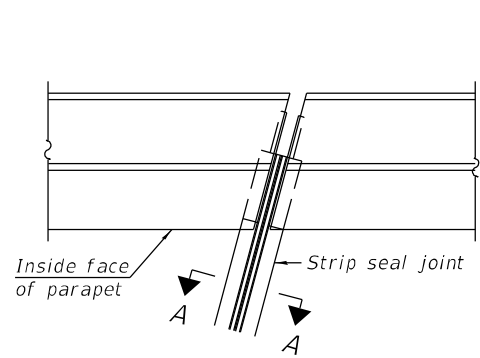
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**JOINT REPLACEMENT - PIN CONNECTION
STRUCTURE NO. 016-0992**

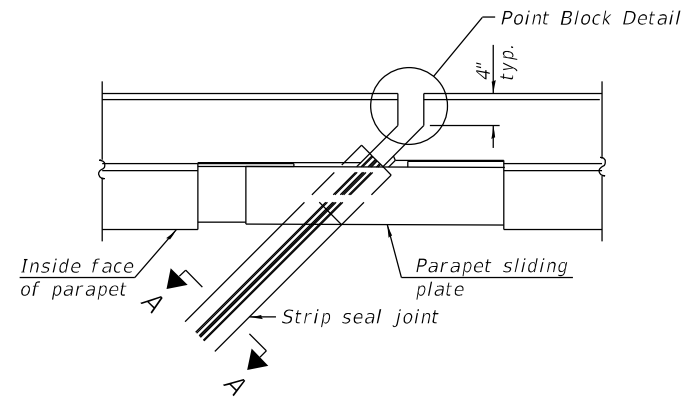
SHEET NO. 8 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	26
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

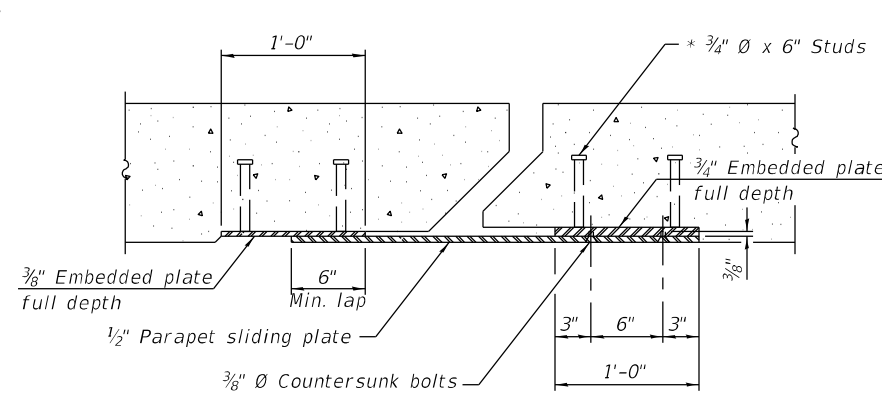


FOR SKEWS ≤ 30°

PLAN AT PARAPET



FOR SKEWS > 30°



SECTION B-B

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

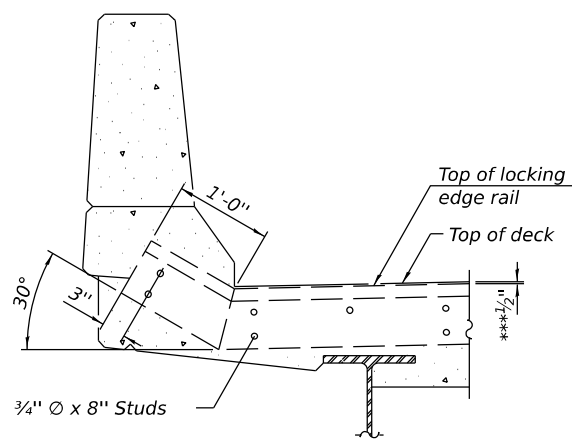
The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

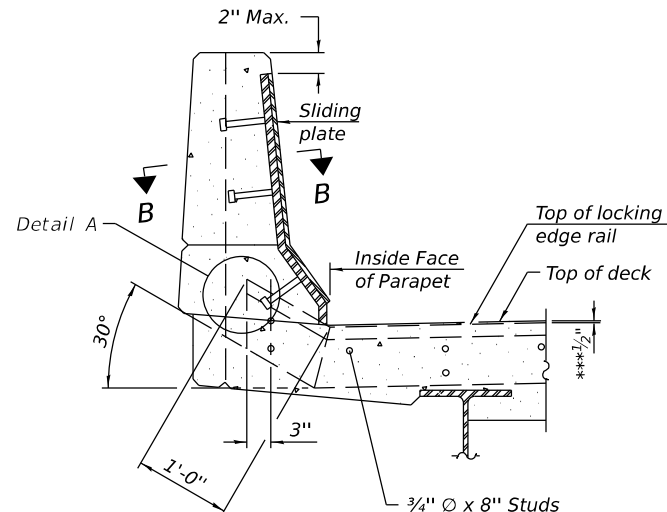
The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



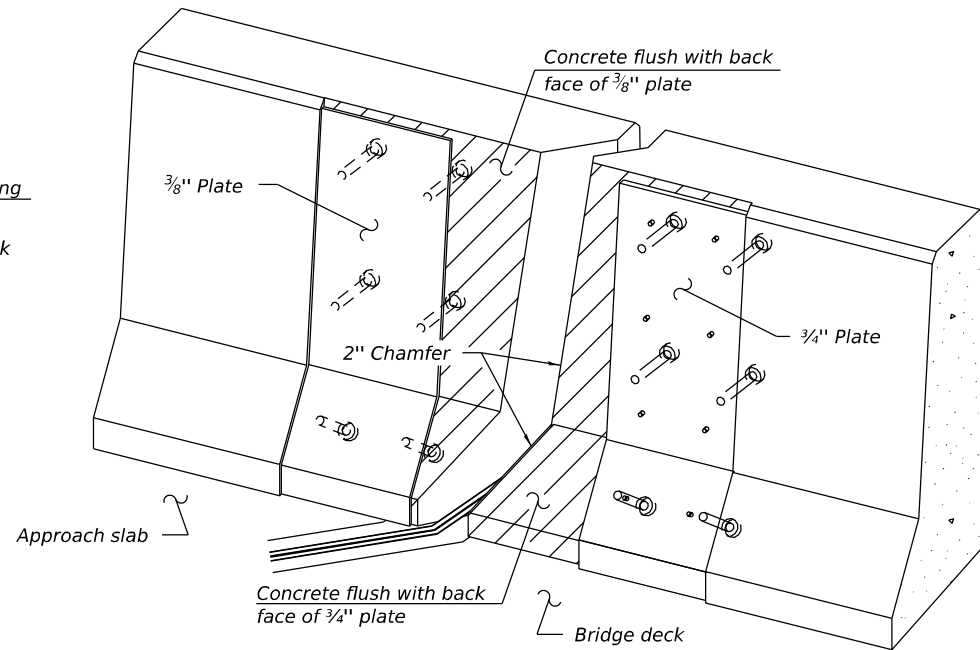
DETAIL A
At Pin Connection



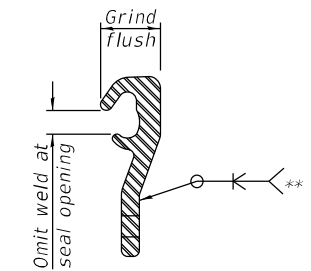
SECTION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

*** Prior to 1/4" Grinding



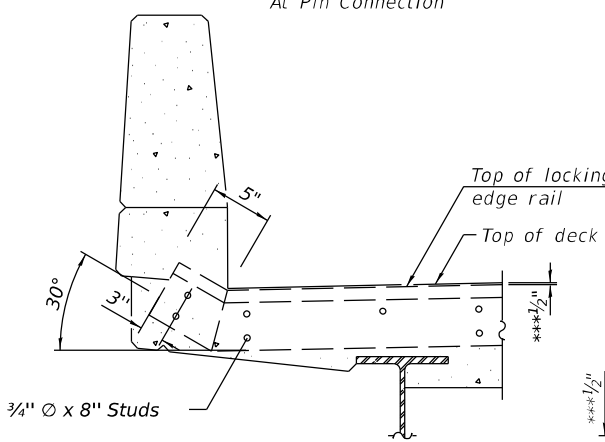
TRIMETRIC VIEW
(Showing embedded plates only)



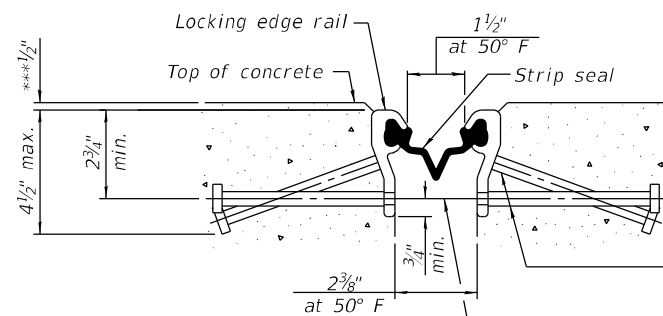
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

Rolled rail shown, welded rail similar.

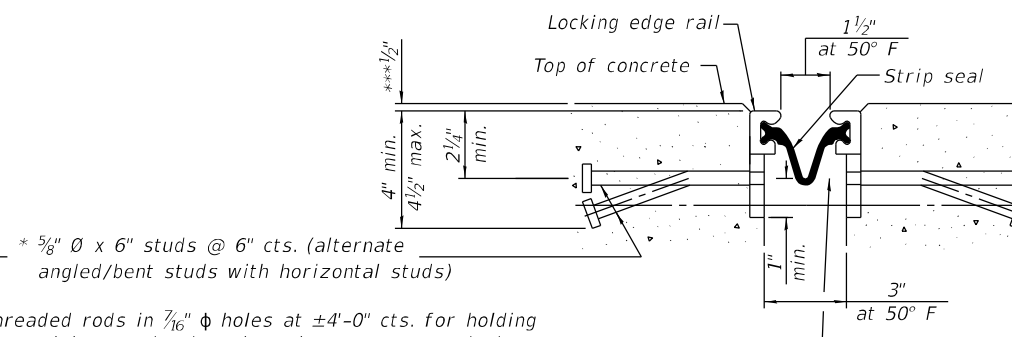


DETAIL A
At Abutments



SHOWING ROLLED RAIL JOINT

*** Prior to 1/4" Grinding



SHOWING WELDED RAIL JOINT

*** Prior to 1/4" Grinding

3/8" φ threaded rods in 7/16" φ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

* 5/8" φ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

SECTION A-A

* Granular or solid ux lled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

** Back gouge not required if complete joint penetration is verified by mock-up.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	229

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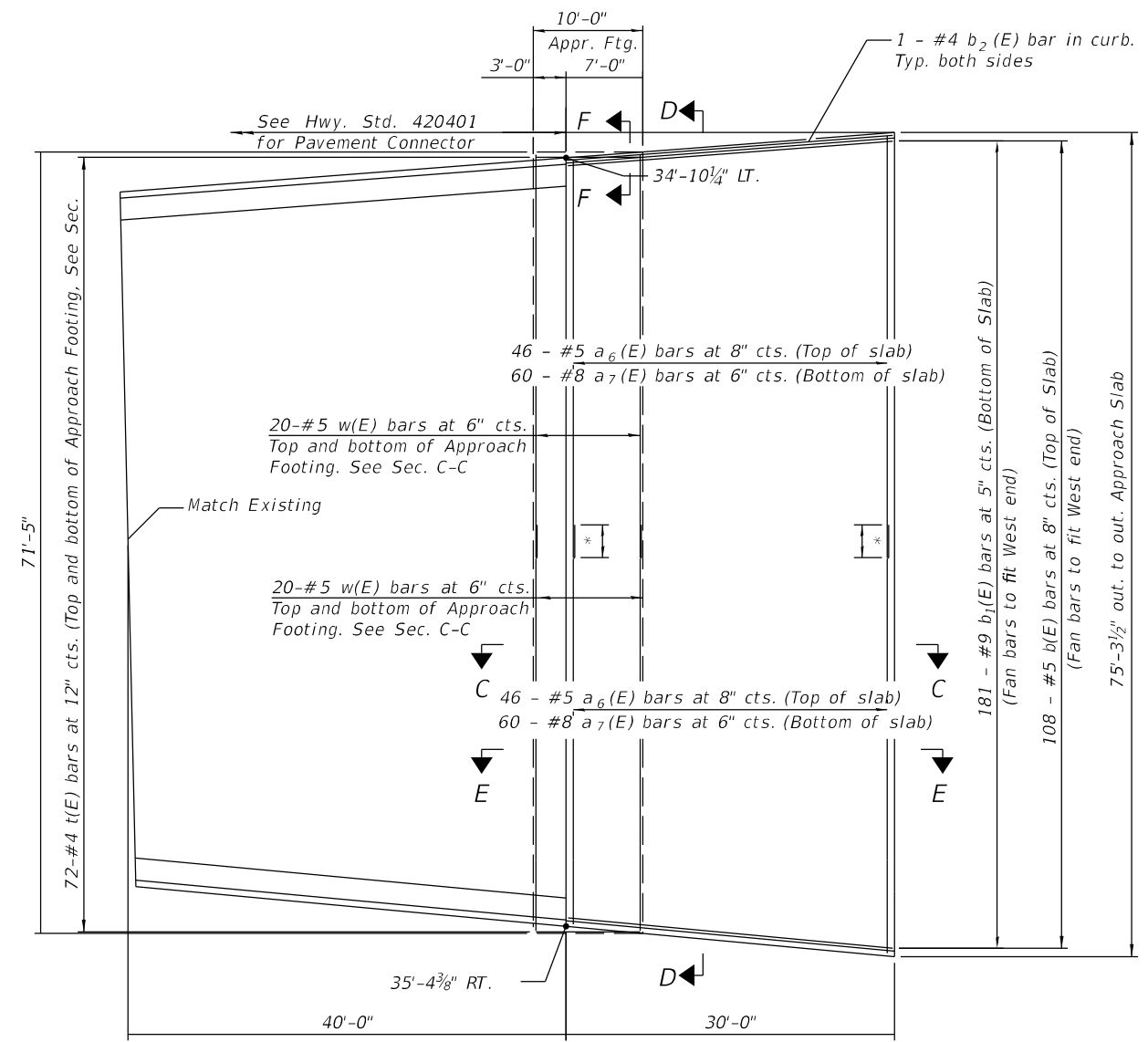
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0992

SHEET NO. 9 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	27
CONTRACT NO. 60R77			ILLINOIS FED. AID PROJECT	



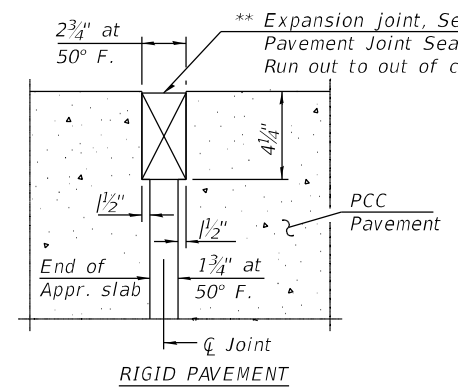
PLAN

* Adjust laps between $a_6(E)$ and $a_7(E)$ bars to fit bars within Approach Slab

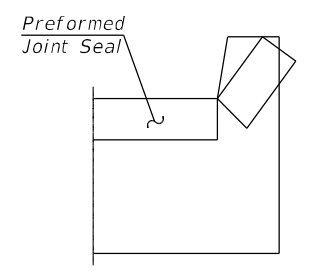
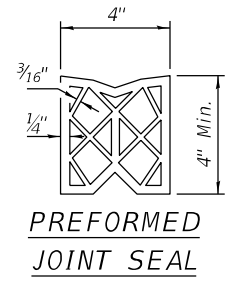


Notes:
 See Sheet 11 of 25 for Sections C-C & D-D and View E-E.
 $a_2(E)$, $a_3(E)$, $a_4(E)$, and $a_7(E)$ bar spacings measured along ϕ Rdwy.
 Taper Approach Slab & Bridge Approach Pavement Connector (PCC) evenly between bridge overlay and existing PCC Pavement, 10".

** Cost included with Concrete Superstructure (Approach Slab).



DETAIL A



SECTION F-F

Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.

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CZAPLICKI LOPEZ, PLLC
 201 KENMARE DRIVE
 BURR RIDGE, IL 60527
 630-915-8861

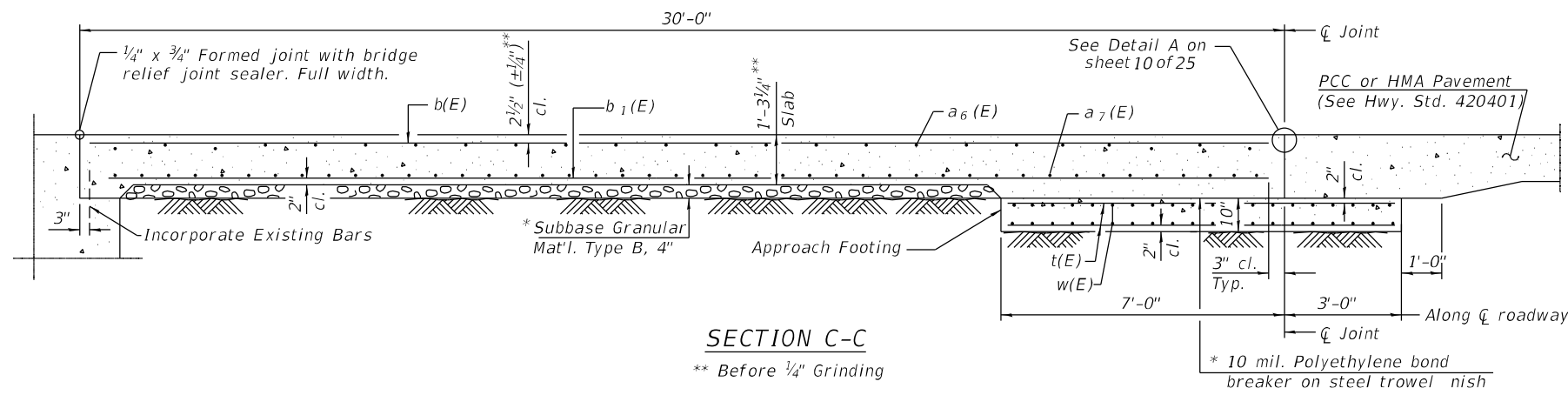
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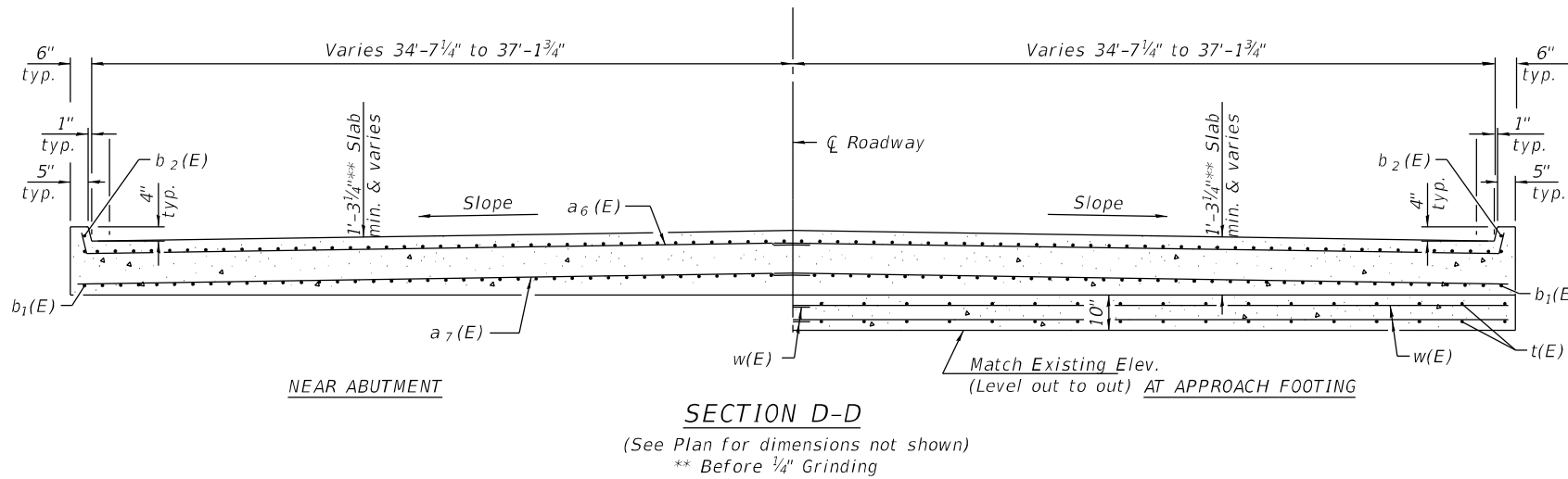
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WEST APPROACH SLAB DETAILS
STRUCTURE NO. 016-0992
 SHEET NO. 10 OF 25 SHEETS

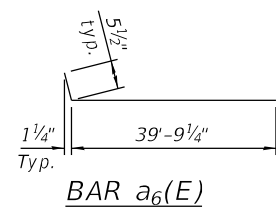
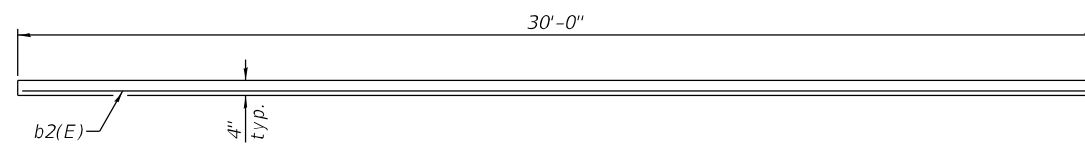
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	28
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



Notes:
 Approach slab and curb concrete shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.



* Cost included with Concrete Superstructure (Approach Slab).



**WEST APPROACH
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a ₆ (E)	92	#5	40'-2"	┌───┐
a ₇ (E)	120	#8	40'-8"	┌───┐
b(E)	108	#5	29'-8"	┌───┐
b ₁ (E)	180	#9	29'-8"	┌───┐
b ₂ (E)	2	#4	29'-8"	┌───┐
t(E)	144	#4	9'-8"	┌───┐
w(E)	80	#5	38'-9"	┌───┐
Concrete Superstructure (Approach Slab)			Cu. Yd.	138.2
Concrete Structures			Cu. Yd.	22.0
Reinforcement Bars, Epoxy Coated			Pound	42,590

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CHECKED	RLP

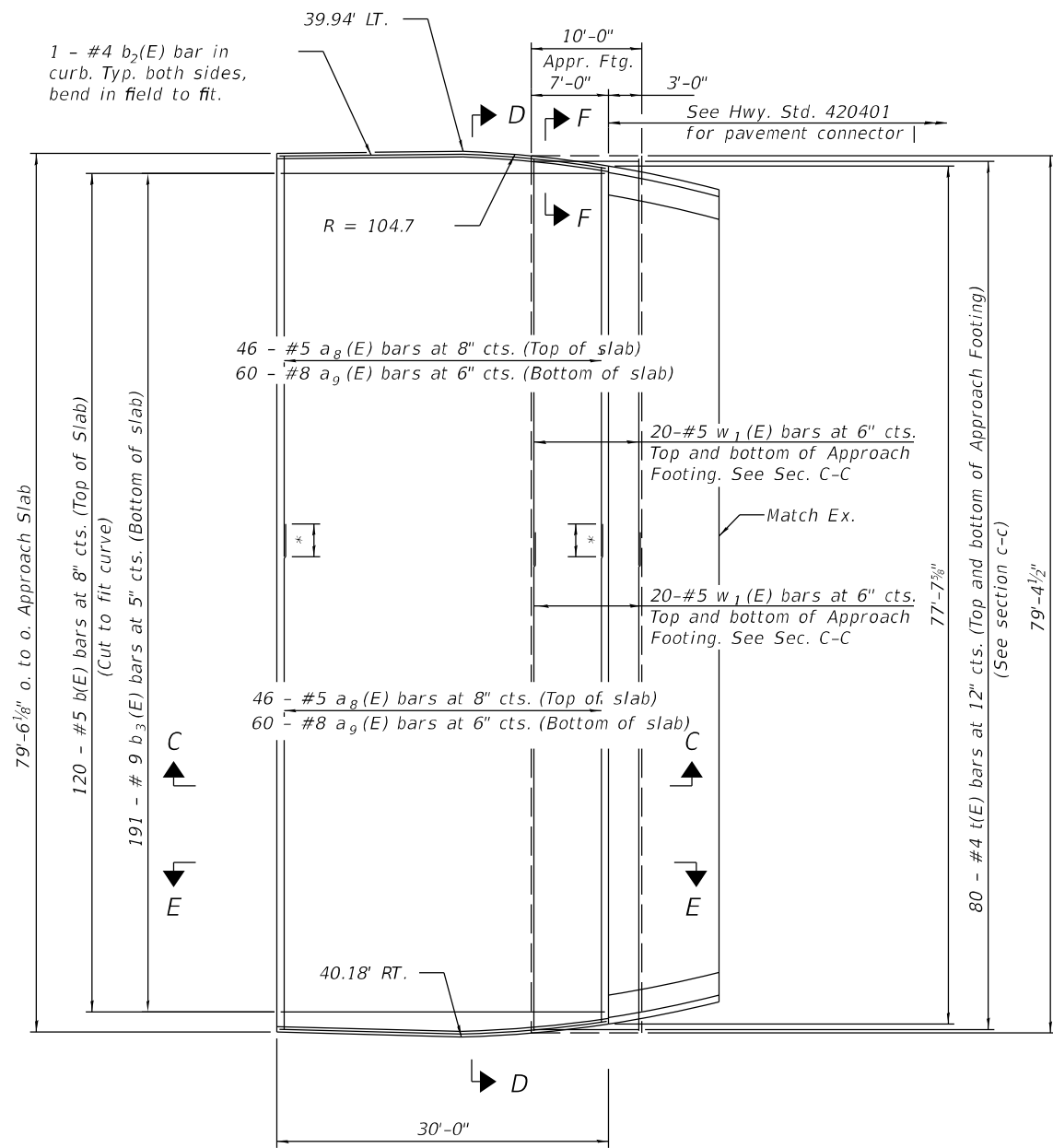
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DESIGNED	PAF
CHECKED	RLP
DRAWN	PAF
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**WEST BRIDGE APPROACH SLAB DETAILS
 STRUCTURE NO. 016-0992**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	29
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



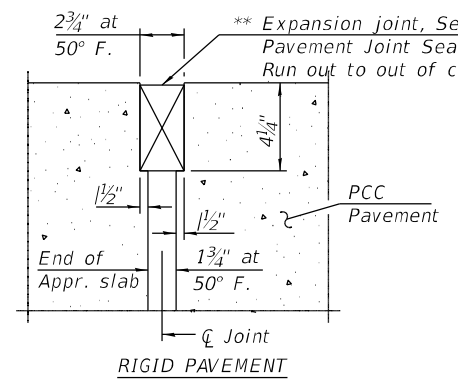
PLAN

* Adjust laps between $a_8(E)$ and $a_9(E)$ bars to fit within Approach Slab

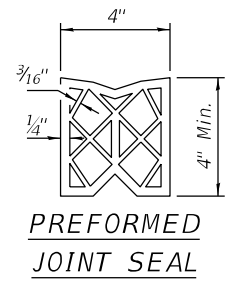


Notes:
 See sheet 13 of 25 for Sections C-C & D-D and View E-E.
 $a_8(E)$ and $a_9(E)$ bar spacings measured along ϕ Rdwy.
 Taper Approach Slab & Bridge Approach Pavement Connector (PCC) evenly between bridge overlay and existing pcc pavement, 10".

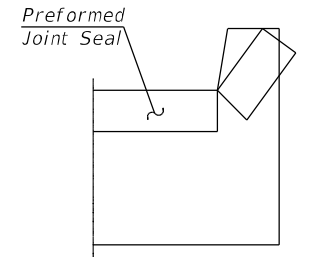
** Cost included with Concrete Superstructure (Approach Slab).



DETAIL A



PREFORMED JOINT SEAL



SECTION F-F

Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.

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DESIGNED	PAF
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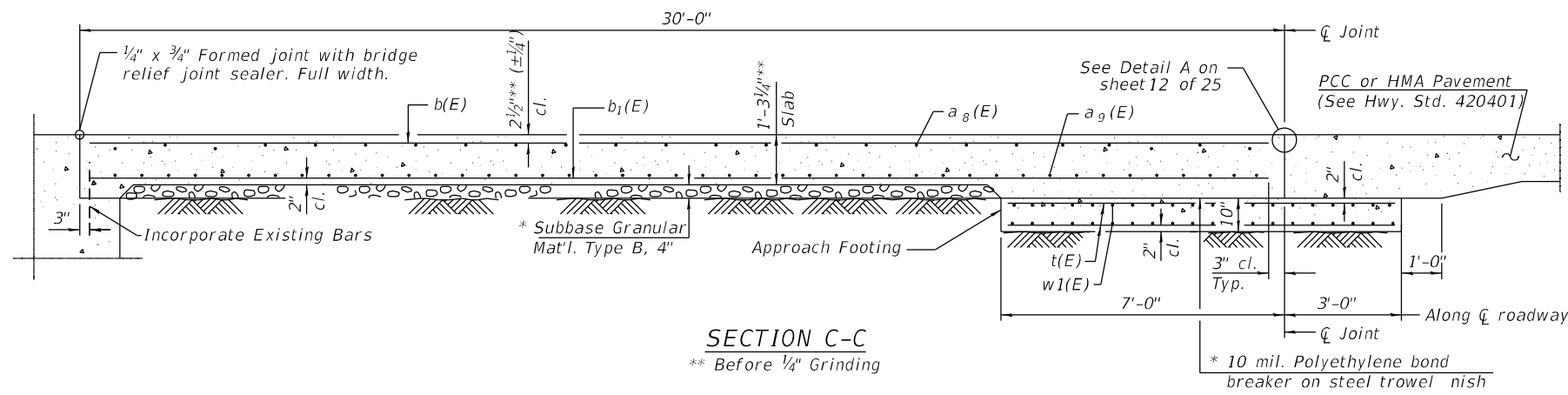
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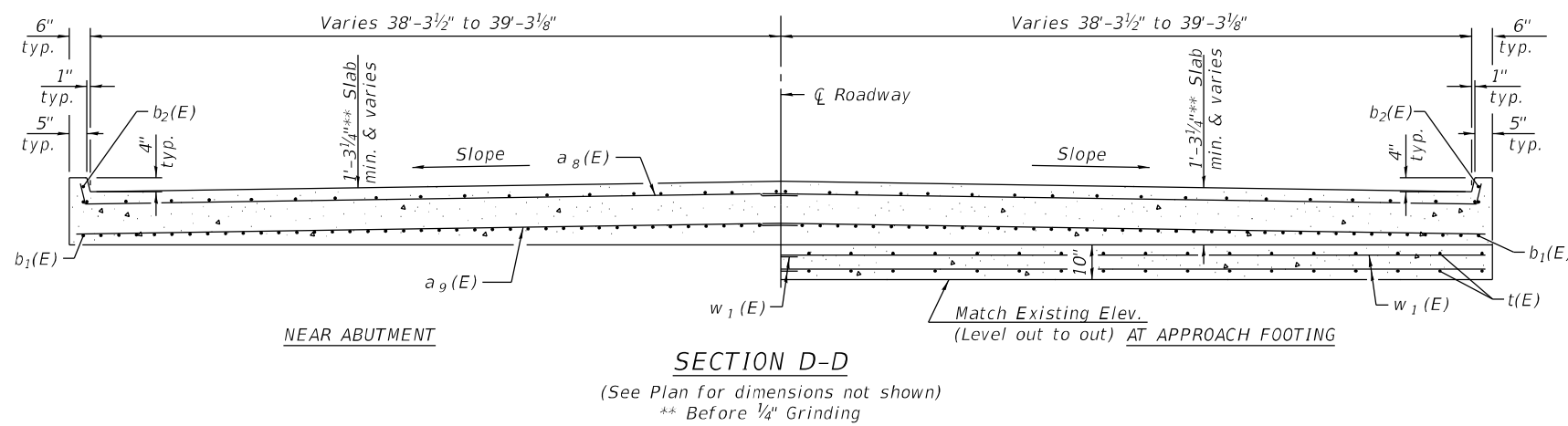
EAST BRIDGE APPROACH SLAB DETAILS
 STRUCTURE NO. 016-0992

SHEET NO. 12 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	30
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



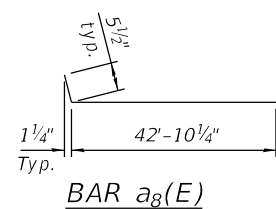
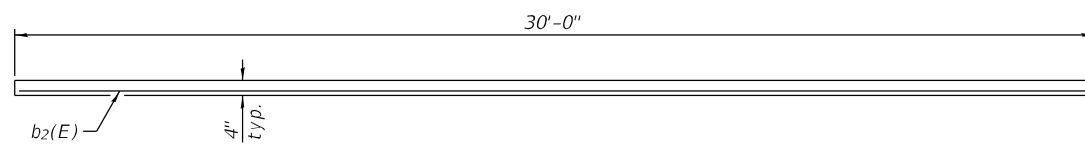
Notes:
 Approach slab and curb concrete shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.



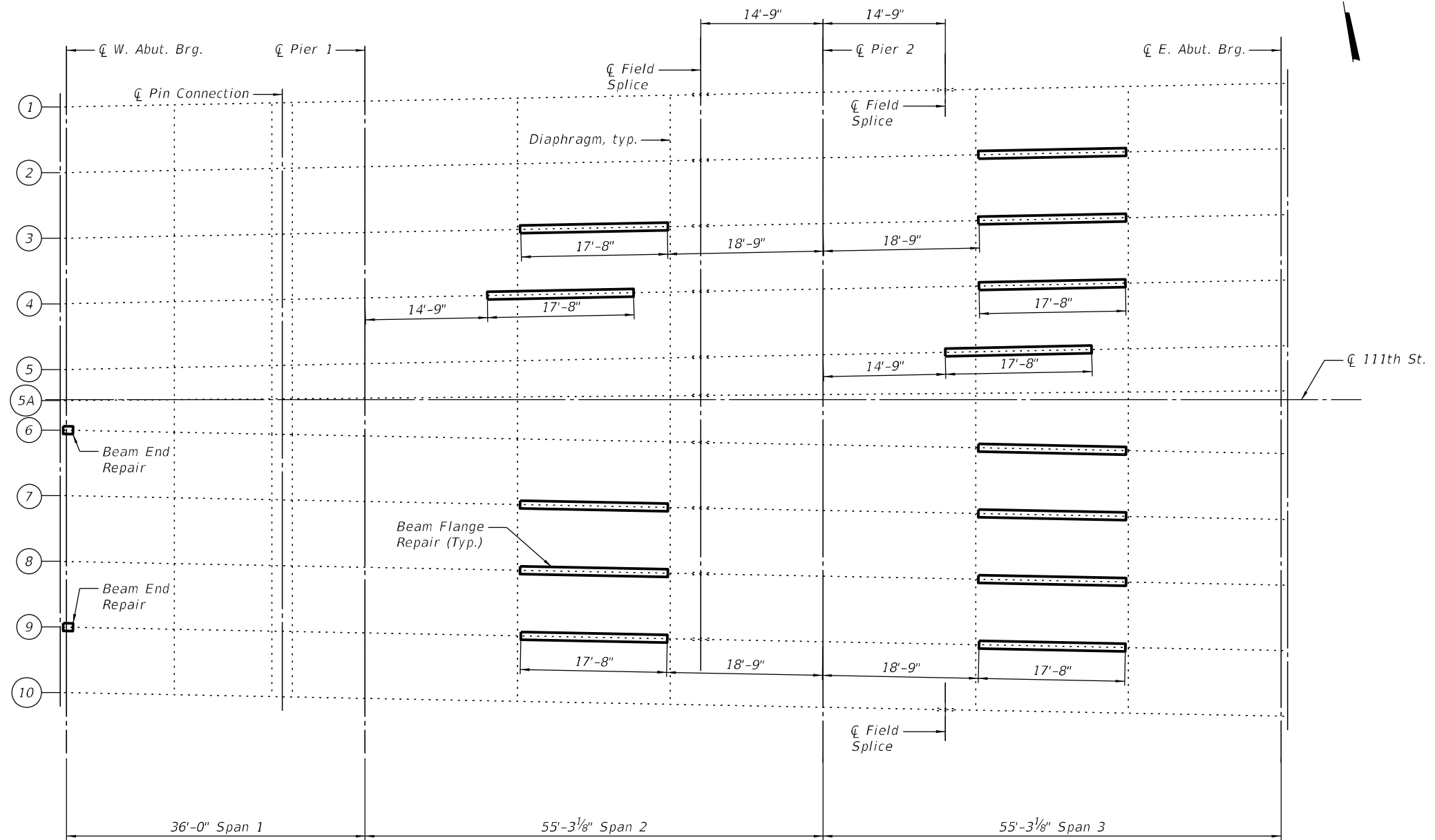
* Cost included with Concrete Superstructure (Approach Slab).

**EAST APPROACH
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a ₈ (E)	92	#5	43'-3"	┌───┐
a ₉ (E)	120	#8	42'-10"	┌───┐
b(E)	120	#5	29'-8"	┌───┐
b ₁ (E)	2	#4	29'-8"	┌───┐
b ₂ (E)	191	#9	29'-8"	┌───┐
t(E)	160	#4	9'-8"	┌───┐
w ₁ (E)	80	#5	43'-0"	┌───┐
Concrete Superstructure (Approach Slab)			Cu. Yd.	154.3
Concrete Structures			Cu. Yd.	24.5
Reinforcement Bars, Epoxy Coated			Pound	45,520



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FRAMING PLAN

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DRAWN	PAF	REVISED
CHECKED	RLP	REVISED

DESIGNED	PAF	REVISED
CHECKED	RLP	REVISED
DRAWN	PAF	REVISED
CHECKED	RLP	REVISED

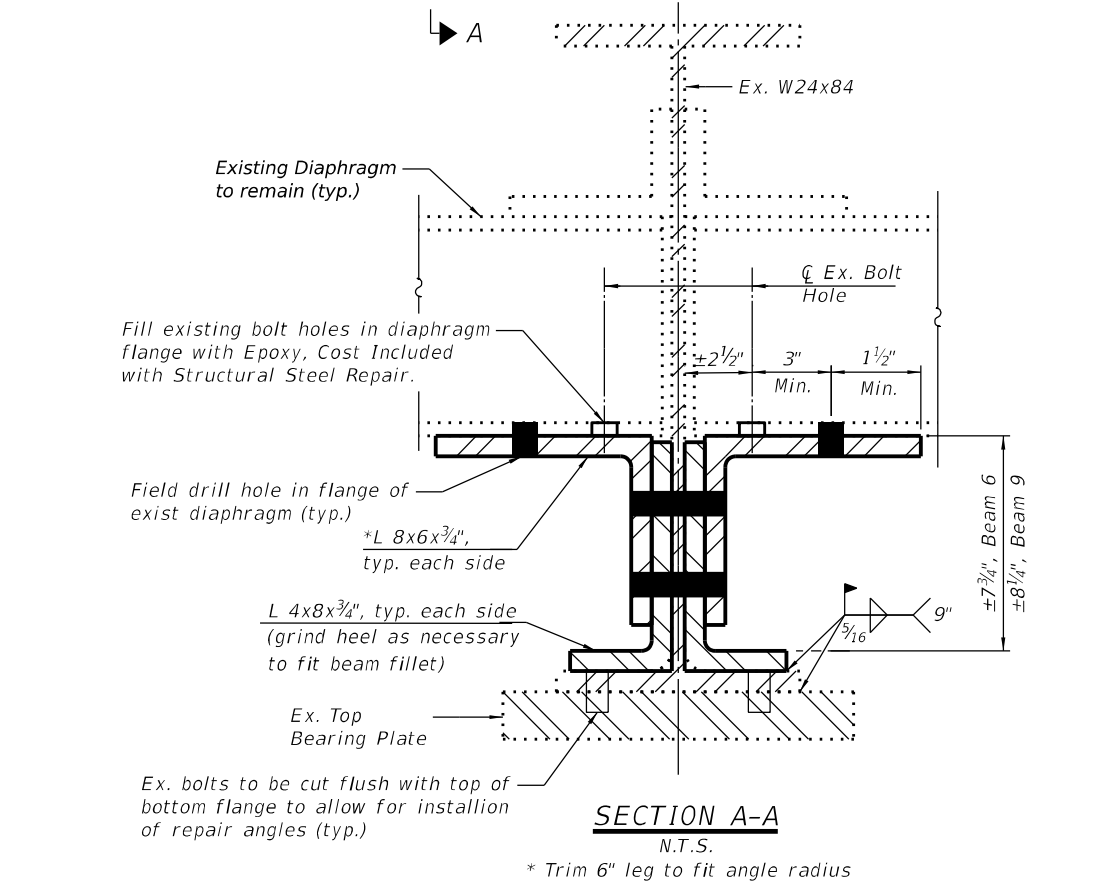
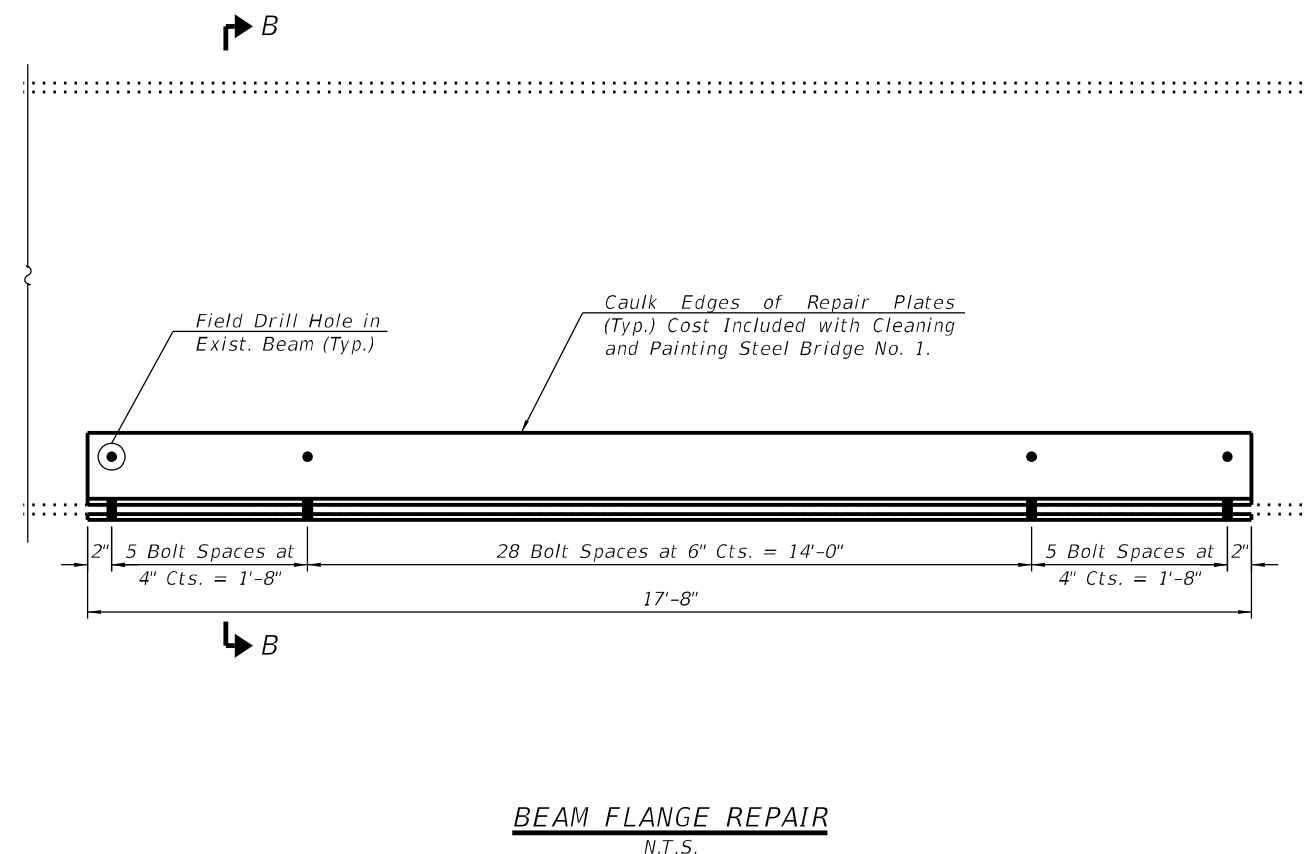
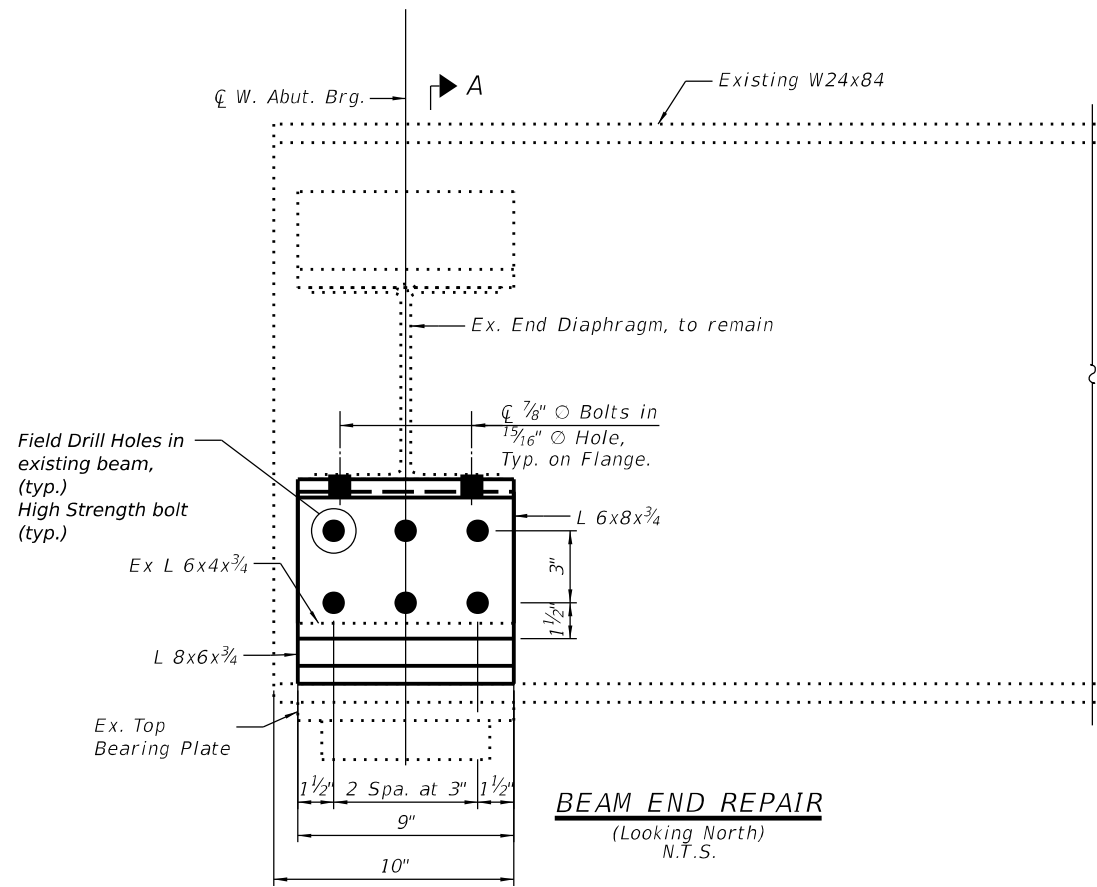
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STEEL REPAIRS
STRUCTURE NO. 016-0992

SHEET NO. 14 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	32
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

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Notes:

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Burn existing anchor bolts flush with top of existing bottom bearing plate. Grind existing anchor bolts smooth and seal with epoxy. Cost incidental to Structural Steel Repair.

Existing clip angle to be removed using the air-arc method and grind smooth all weld material remaining on the web. Cost included with Structural Steel Removal.

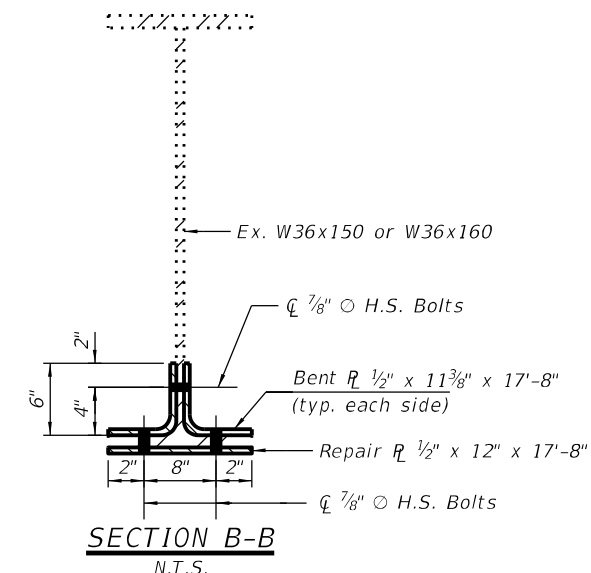
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Fasteners shall be high strength bolts. Bolts $\frac{7}{8}$ ", open holes $\frac{15}{16}$ ", unless otherwise noted.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions, including cost of drilling holes in existing steel members, shall be included in Structural Steel Repair.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures." All welds shall be ground smooth.

Height of support angles shown is approximate. Adjust position of support angles to align with bottom of existing or proposed diaphragms in the field. Field-drill holes in support angles.



BILL OF MATERIAL

Item	Unit	Total
Structural Steel Repair	Lbs.	15,990



DESIGNED	PAF
CHECKED	RLP
DRAWN	PAF
CHECKED	RLP

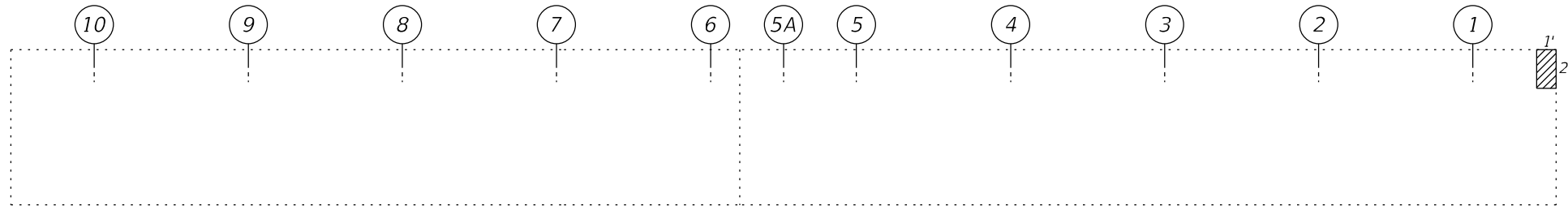
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

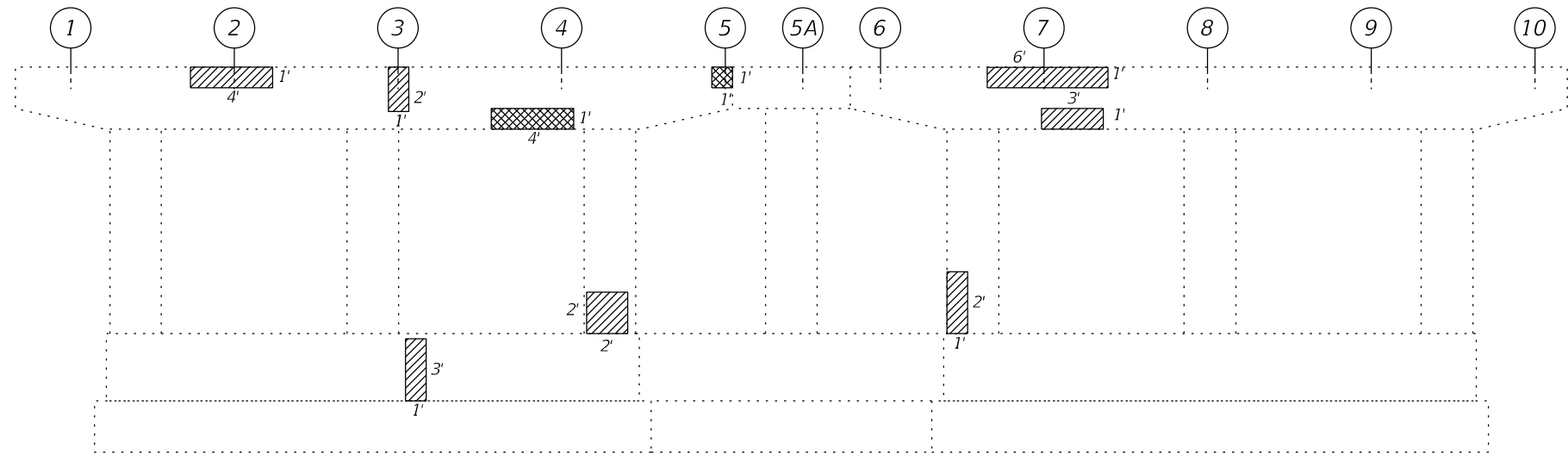
**STEEL REPAIRS
STRUCTURE NO. 016-0992**

SHEET NO. 15 OF 25 SHEETS

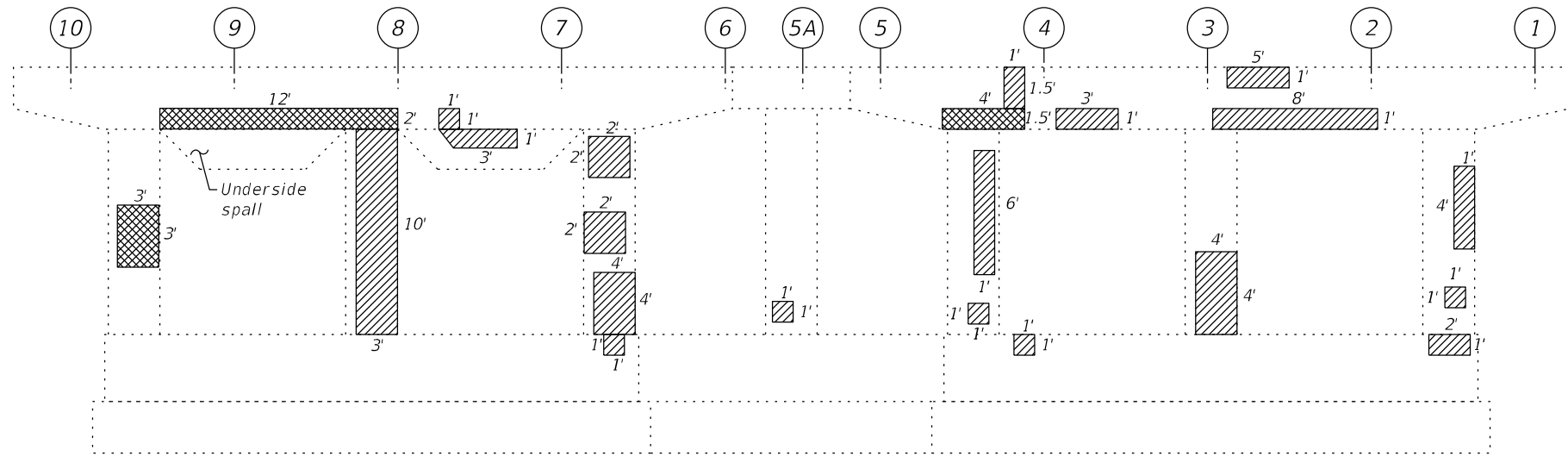
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	33
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



WEST ABUTMENT
Temporary Shoring & Cribbing at Beam 1.



PIER 1
(West Face)
Temporary Shoring & Cribbing at Beams 2, 3, 5, & 7.



PIER 1
(East Face)
Temporary Shoring & Cribbing at Beams 2, 3, 5, & 7.

DL* Reactions	
	Kips
W. Abut.	15
Pier 1	58

* Reactions shown include proposed overlay.

NOTE:
Repair of the existing abutment and piers shall include, but may not be limited to, the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
	Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	135
	Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.	31
	Temporary Shoring and Cribbing	Each	5

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CZAPLICKI LOPEZ, PLLC
201 KENMARE DRIVE
BURR RIDGE, IL 60527
630-915-8861

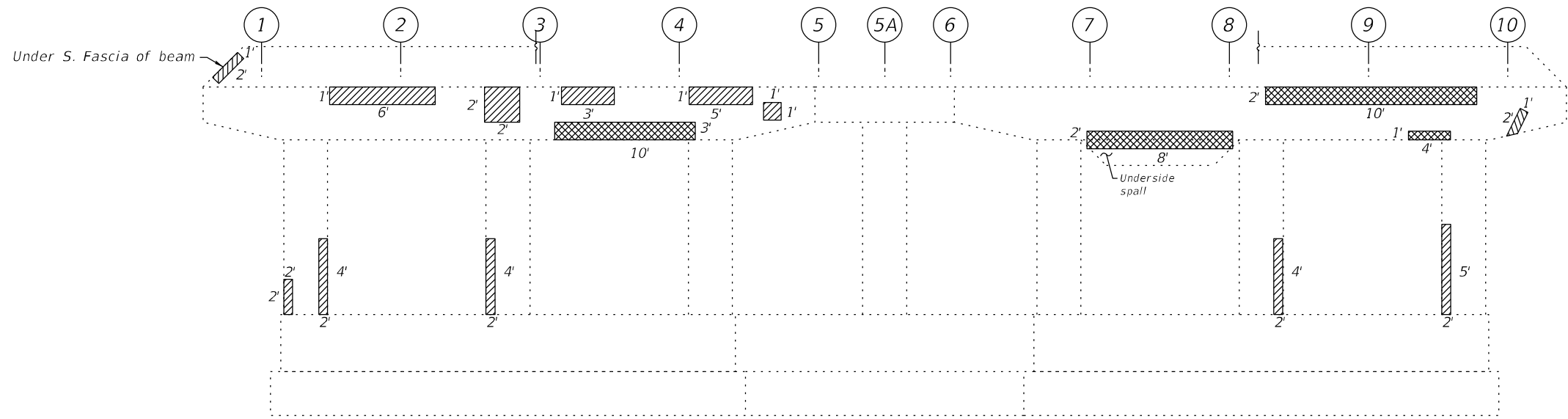
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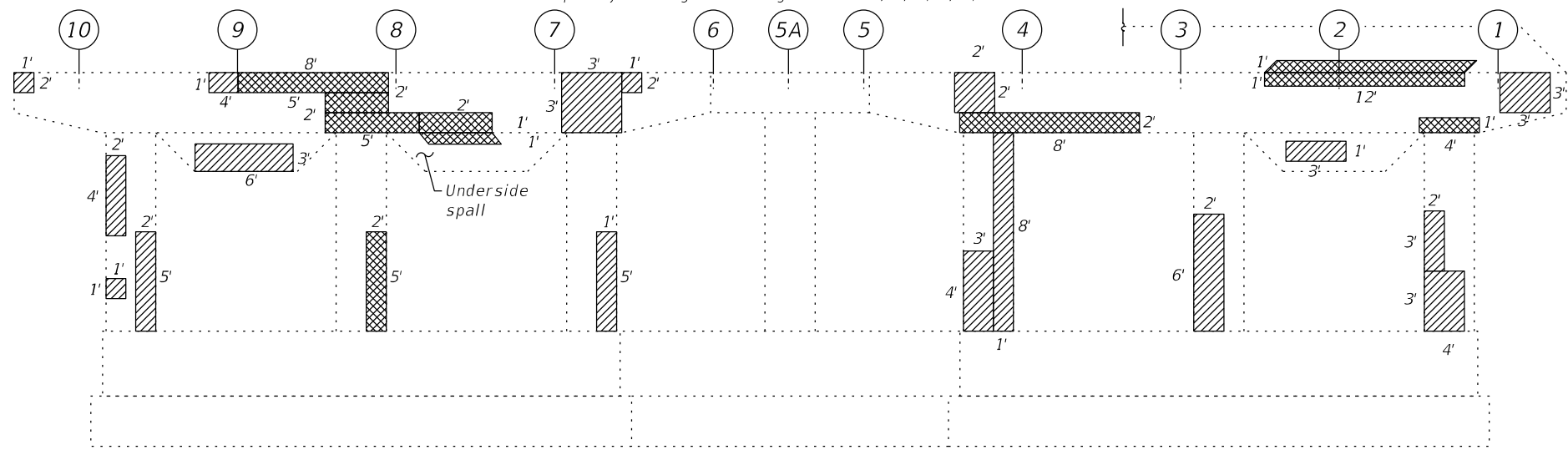
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIRS
STRUCTURE NO. 016-0992

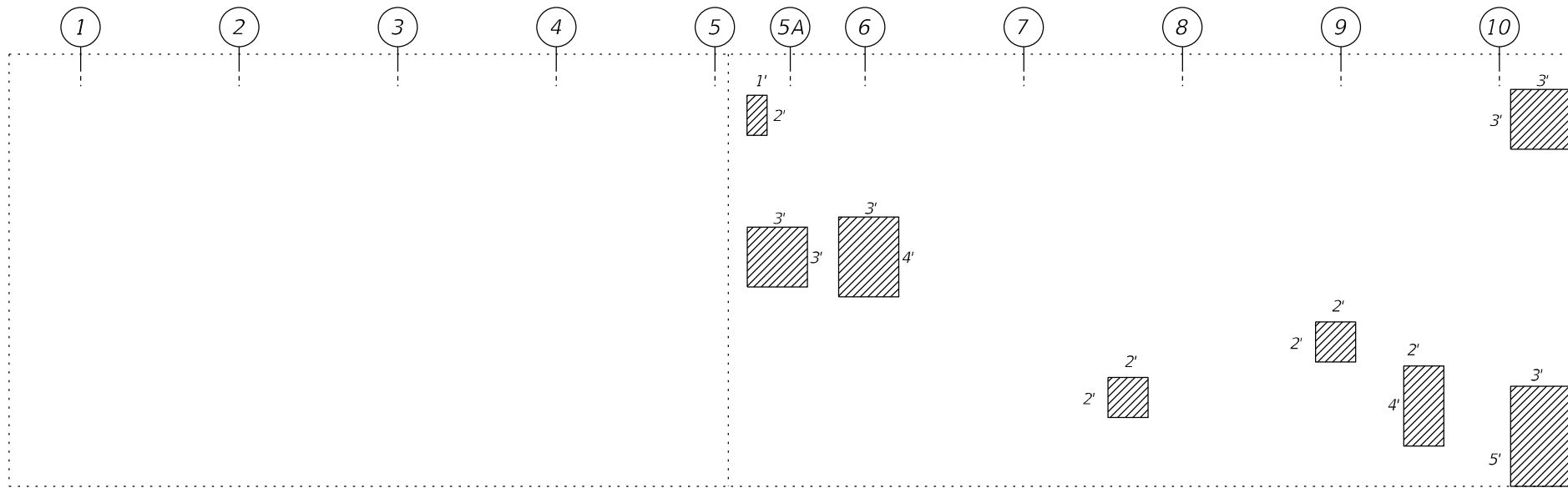
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	34
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				



PIER 2
(West Face)
Temporary Shoring & Cribbing at Beam 2, 3, 7, 8, 9, & 10.



PIER 2
(East Face)
Temporary Shoring & Cribbing at Beam 2, 3, 7, 8, 9, & 10.



EAST ABUTMENT

DL* Reaction	
	Kips
Pier 2	82

* Reaction shown include proposed overlay.

NOTE:
Repair of the existing abutment and piers shall include, but may not be limited to, the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
	Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	249
	Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.	144
	Temporary Shoring and Cribbing	Each	6

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CZAPLICKI LOPEZ, PLLC
201 KENMARE DRIVE
BURR RIDGE, IL. 60527
630-915-8861

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DRAWN	PAF	REVISED	
CHECKED	RLP	REVISED	

DESIGNED	PAF	REVISED	
CHECKED	RLP	REVISED	
DRAWN	PAF	REVISED	
CHECKED	RLP	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIRS
STRUCTURE NO. 016-0992

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	35
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

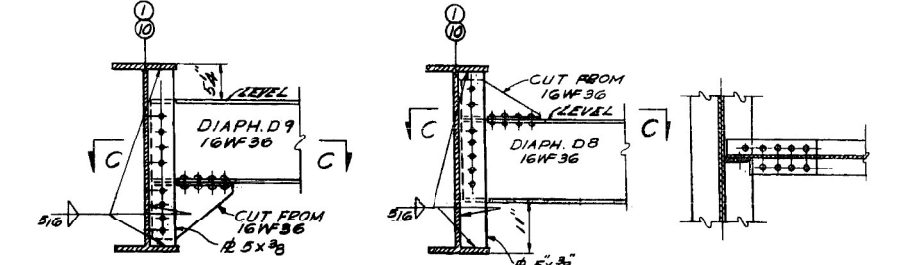
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 90	1112-7018B	COOK	59	23
STA.	TO STA.		PROJECT	
R. P. R. REG. NO. 4	ILLINOIS			

BEAM ELEVATION

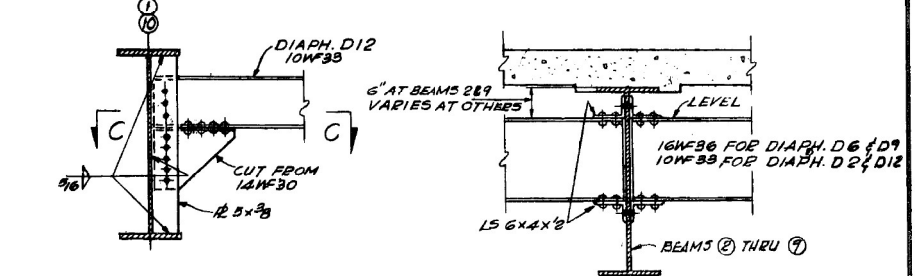
ELEVATIONS ARE GIVEN TO THE TOP OF THE BEAMS (FOR FABRICATION ONLY)

BEAM LOCATION	1	2	3	4	5	6	7	8	9	10
¢ BRG. W. ABUT.	23.845	23.914	23.983	24.051	24.120	24.120	24.051	23.983	23.914	23.845
¢ PIN CONN.	23.907	23.976	24.045	24.113	24.182	24.182	24.113	24.045	23.976	23.907
¢ PIER NO. 1	23.873	23.942	24.011	24.079	24.148	24.148	24.079	24.011	23.942	23.873
¢ SPLICE	23.793	23.802	23.871	23.939	24.008	24.008	23.939	23.871	23.802	23.793
¢ PIER NO. 2	23.674	23.743	23.812	23.880	23.949	23.949	23.880	23.812	23.743	23.674
¢ BRG. E. ABUT.	23.455	23.524	23.593	23.661	23.730	23.730	23.661	23.593	23.524	23.455

NOTE: ELEVATIONS ARE BASED ON A DIMENSION OF 74" FROM THE THEORETICAL TOP OF SLAB TO TOP OF BEAMS AT ¢ OF ABUTMENT BEARINGS, ¢ OF PIN CONN. AND AT ¢ OF SPLICES. THE TOP FLANGE IS CONSIDERED A STRAIGHT LINE BETWEEN THESE POINTS. ELEVATION AT ¢ SPLICE IS GIVEN TO TOP OF 36WF160.

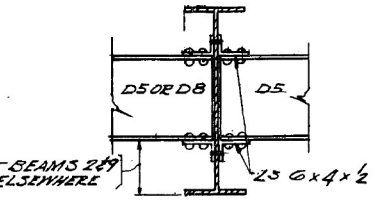


EXT. CONNECTION FOR DIAPHRAGM D9 SCALE: 3/4"=1'-0"
EXT. CONNECTION FOR DIAPHRAGM D8 SCALE: 3/4"=1'-0"



EXT. CONNECTION FOR DIAPHRAGM D12 SCALE: 3/4"=1'-0"
INT. CONNECTION FOR DIAPHRAGMS D6, D9 & D12 SCALE: 3/4"=1'-0"

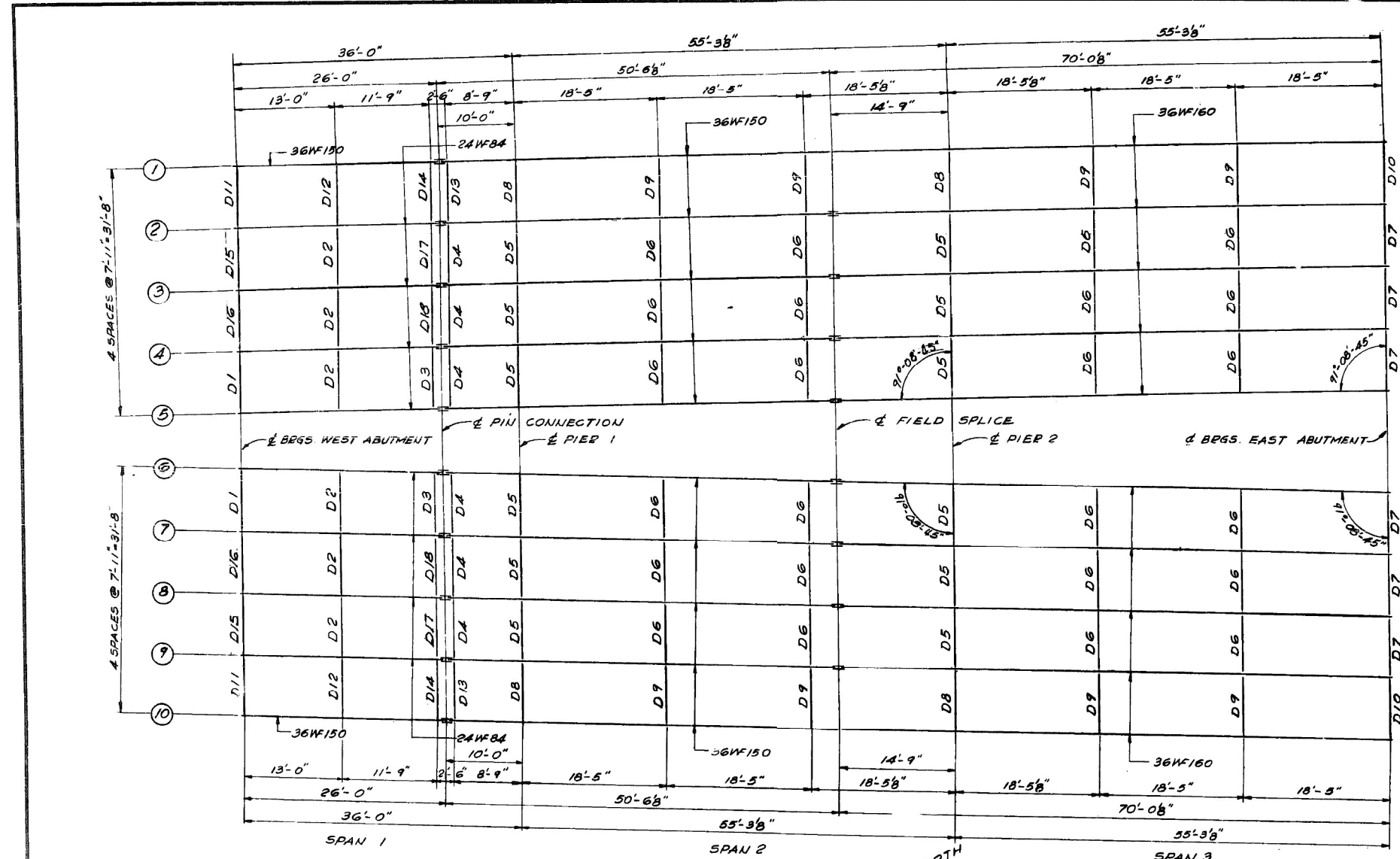
DIAPHRAGM	SIZE
D1, D3, D11, D14 THRU D18	16WF35
D2 & D12	16WF33
D4, D7, D10 & D13	12WF40
D5, D6, D8 & D9	16WF36



INT. CONNECTION FOR DIAPHRAGMS D5 & D8

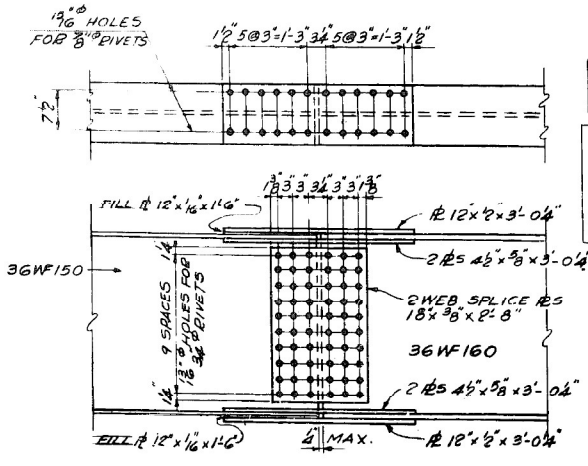
FOR INFORMATION ONLY

ILLINOIS DIVISION OF HIGHWAYS
 SOUTH EXPRESSWAY
 III-TH. ST. GRADE SEPARATION
 STEEL FRAMING PLAN AND DETAILS
 SCALE: AS NOTED DATE: JULY 24, 1955

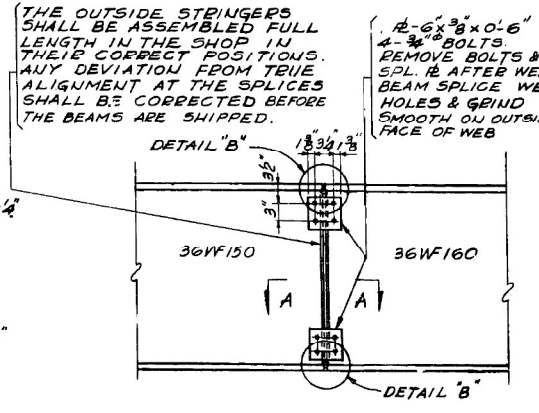


STEEL FRAMING PLAN

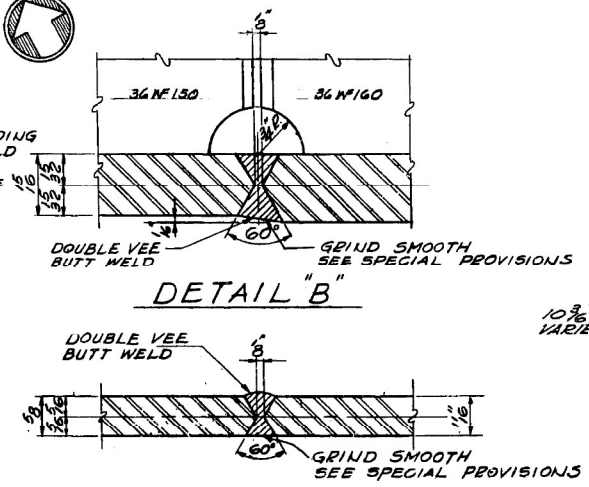
SCALE: 3/8"=1'-0"



SPLICE DETAIL FOR 36WF150 & 36WF160
 SCALE: 3/4"=1'-0"



FIELD SPLICE DETAIL FOR BEAMS 1 & 10



SECTION A-A

THE OUTSIDE STRINGERS SHALL BE ASSEMBLED FULL LENGTH IN THE SHOP IN THEIR CORRECT POSITIONS. ANY DEVIATION FROM TRUE ALIGNMENT AT THE SPLICES SHALL BE CORRECTED BEFORE THE BEAMS ARE SHIPPED.

1/2"x6"x8"x0'-6" 4-3/4" BOLTS REMOVE BOLTS & SPL. PL. AFTER WELDING BEAM SPLICE WELD HOLES & GRIND SMOOTH ON OUTSIDE FACE OF WEB

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PLT DATE = 5/4/2023

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-7018B-BR	COOK	59	36
ILLINOIS			FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	DATE	SHEET NO.
94	+	COOK	98	36
SHEET NO. 13				27 SHEETS
* 1978-320BR-78				

* INTERIOR BEAM MOMENT TABLE

	0.4 Sp. #1	Pier #1	0.4 Sp. #2	Pier #2	0.6 Sp. #3
<i>I</i> (in ⁴)	2370	9040	9040	9750	9750
<i>D</i> (K/ft.)	0.858	0.918	0.918	0.928	0.928
<i>M₀</i> (K)	72.50	154.40	117.10	316.20	213.70
<i>M₁</i> (K)	114.40	257.20	313.90	242.50	318.30
<i>M₂</i> (K)	34.30	77.20	87.00	67.20	88.20
<i>M</i> (Total) (K)	221.20	488.80	518.00	625.90	620.20
<i>f_s</i> (k.s.i.)	13.54	11.64	12.33	13.86	13.73

* For New Beam 5A. Values given in this table are based on Service Loads.

* INTERIOR GIRDER REACTION TABLE

	W. Abut.	Pier #1	Pier #2	E. Abut.
<i>R₀</i> (K)	11.15	42.17	59.65	19.93
<i>R₁</i> (K)	25.72	40.11	38.68	31.16
<i>Imp.</i> (K)	7.72	12.03	10.71	8.63
<i>R</i> (Total) (K)	44.59	94.31	109.04	59.72

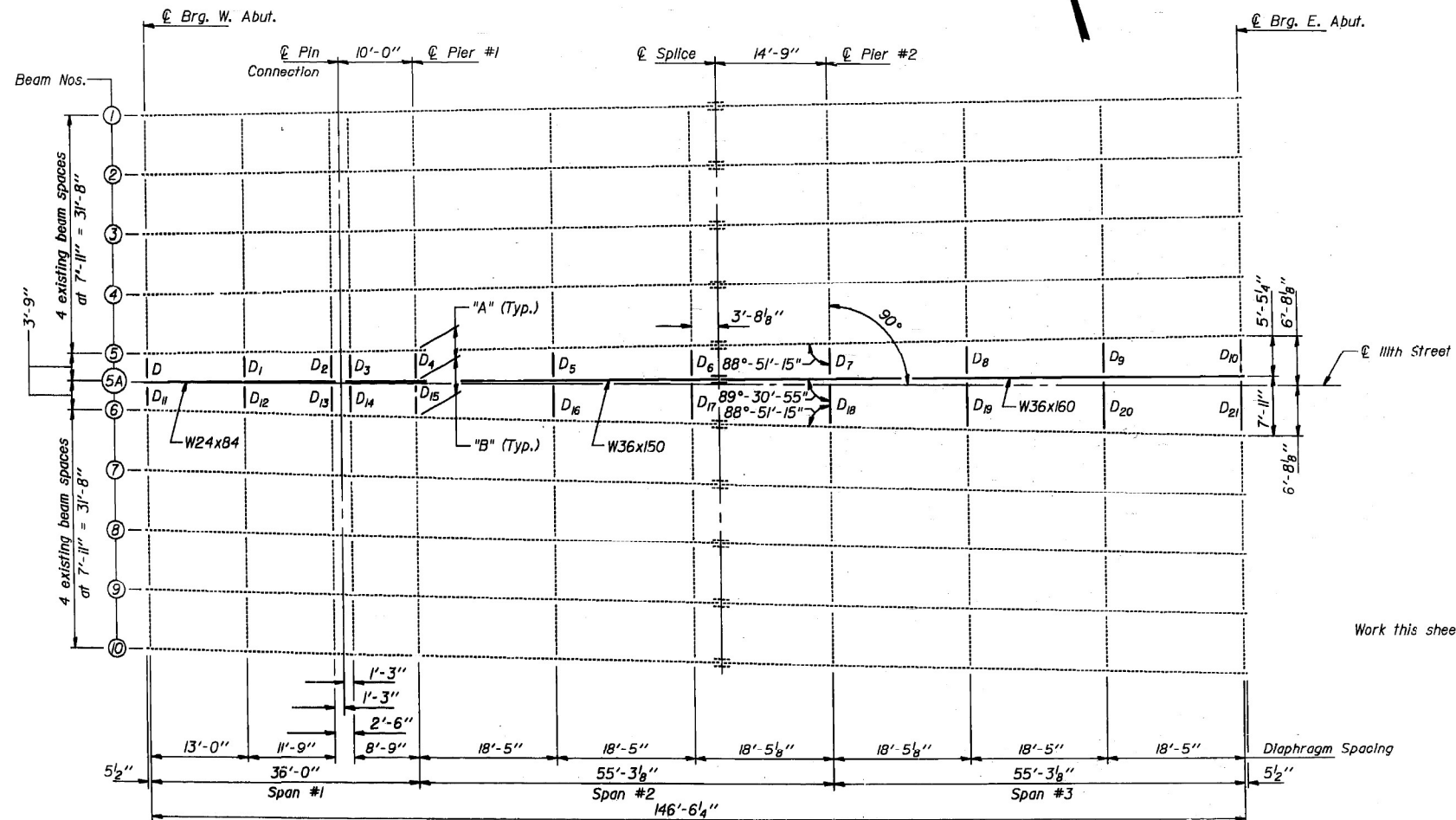
** TOP OF FLANGE ELEVATIONS

Location	Beam 5A
⊕ Brg. W. Abut.	604.07
⊕ Pin Connection	604.12
⊕ Pier #1	604.10
⊕ Splice	603.94
⊕ Pier #2	603.88
⊕ Brg. E. Abut.	603.68

** For Fabrication Only

TABLE OF "A" & "B" DIMENSIONS

Diaphragm Loc.	"A"	"B"
D	3'-9"	
D ₁	3'-10 ³ / ₄ "	
D ₂	4'-0 ³ / ₈ "	
D ₃	4'-0 ³ / ₄ "	
D ₄	4'-2"	
D ₅	4'-4 ¹ / ₂ "	
D ₆	4'-7"	
D ₇	4'-9 ⁵ / ₈ "	
D ₈	5'-0 ¹ / ₈ "	
D ₉	5'-2 ³ / ₄ "	
D ₁₀	5'-5 ¹ / ₄ "	
D ₁₁		3'-9"
D ₁₂		4'-1 ³ / ₈ "
D ₁₃		4'-5 ¹ / ₂ "
D ₁₄		4'-6 ¹ / ₄ "
D ₁₅		4'-9 ¹ / ₄ "
D ₁₆		5'-3 ¹ / ₂ "
D ₁₇		5'-9 ¹ / ₈ "
D ₁₈		6'-4 ¹ / ₈ "
D ₁₉		6'-10 ³ / ₈ "
D ₂₀		7'-4 ³ / ₄ "
D ₂₁		7'-11"



FRAMING PLAN

DESIGNED *Al. K. K...*
CHECKED *DKM*
DRAWN *R. Doty*
CHECKED *GKA* *DKM*

EXAMINED *Dr. J. K...*
PASSED *James J. ...*
APPROVED *...*

July 14, 1966

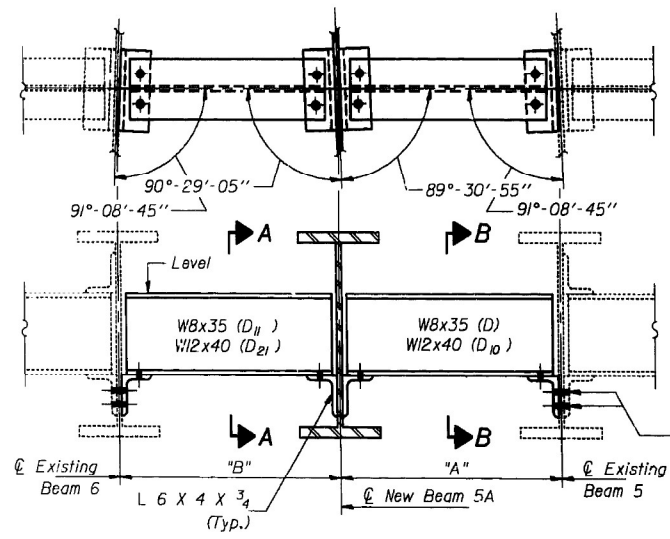
STRUCTURAL STEEL
F.A.I. RT. 94 SEC. 1918-320BR-78
COOK COUNTY
STA. 41+62.67

FOR INFORMATION ONLY



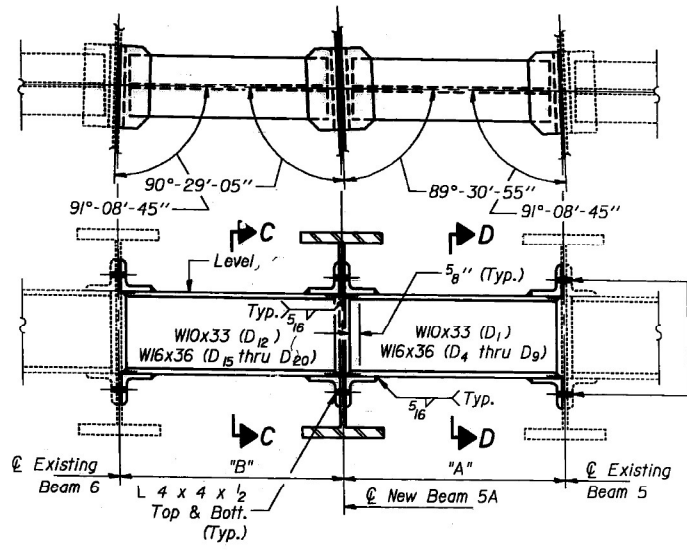
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
94	4	COOK	37	27 SHEETS
SHEET NO. 14				



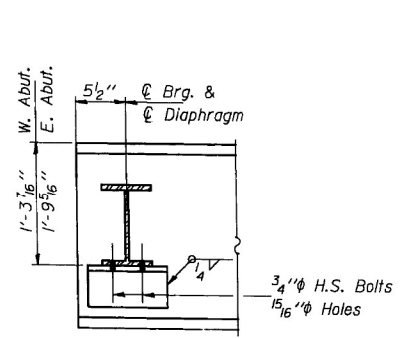
Remove existing rivets
(Cost incidental to "Structural Steel")
and replace with 3/4" H.S. Bolts
at existing beams only.

DIAPHRAGM D, D₁₀, D₁₁ & D₂₁

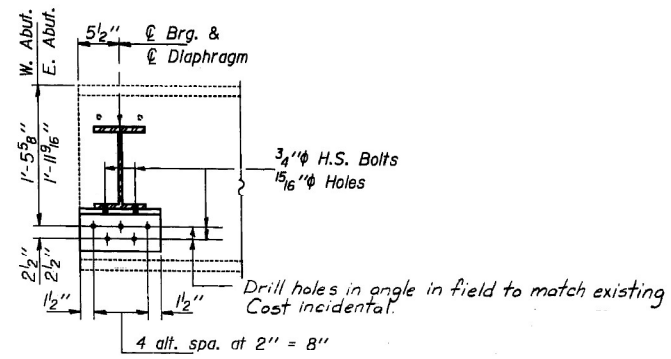


Remove existing rivets
(Cost incidental to "Structural Steel")
and replace with 3/4" H.S. Bolts
at existing beams only.

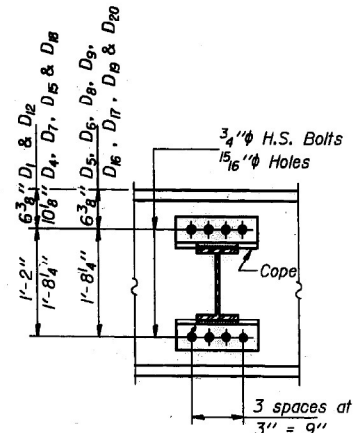
DIAPHRAGM D₁, D₄ THRU D₉, D₁₂ & D₁₅ THRU D₂₀



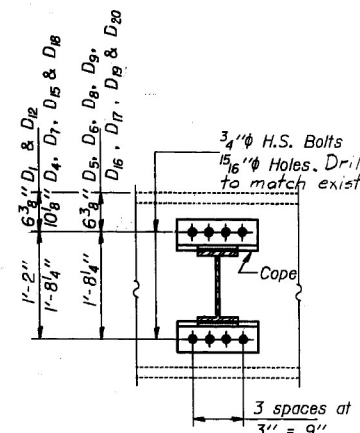
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

DESIGNED	<i>Al. Michael</i>	EXAMINED	<i>July 14, 1986</i>
CHECKED	<i>DKM</i>	PASSED	<i>James J. Kasper</i>
DRAWN	R. Doty	APPROVED	<i>James J. Kasper</i>
CHECKED	<i>DKM</i>		

Note: Two hardened washers shall be required over all 1 5/16" holes.
All contact surfaces of joints shall be free of paint or lacquer.
See sheet #13 of 27 for dimensions "A" & "B".

STRUCTURAL STEEL DETAILS
F.A.I. RT. 94 SEC. 1973-320BR-78
FOR INFORMATION ONLY
COOK COUNTY
STA. 41+62.67



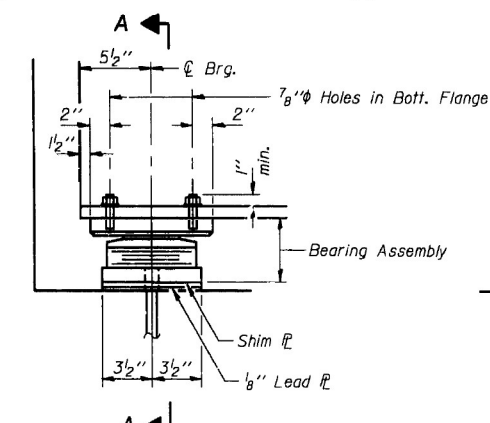
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DESIGNED	PAF	REVISED	
CHECKED	RLP	REVISED	
DRAWN	PAF	REVISED	
CHECKED	RLP	REVISED	

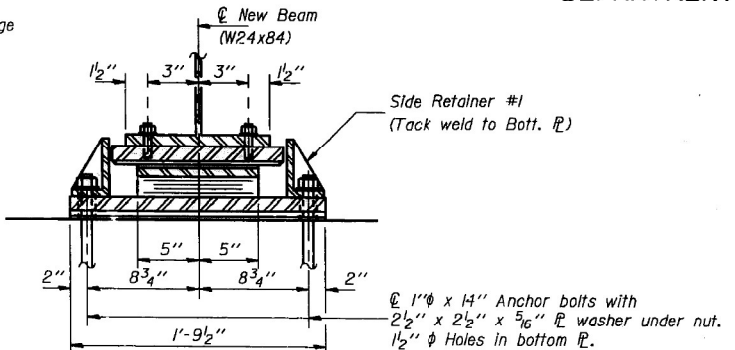
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	39
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO. 17
94	*	COOK	88	40
PROJECT NO. 1978-320BR-78				27 SHEETS

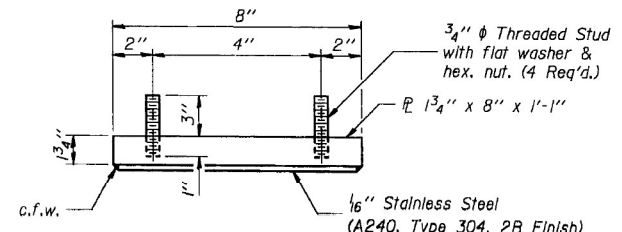


ELEVATION AT WEST ABUT.
(New Beam 5A - Looking North)

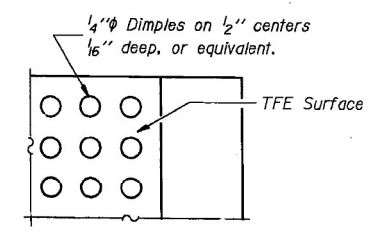


SECTION A-A
Note: See sheet #21 of 27 for Anchor Bolt installation details.

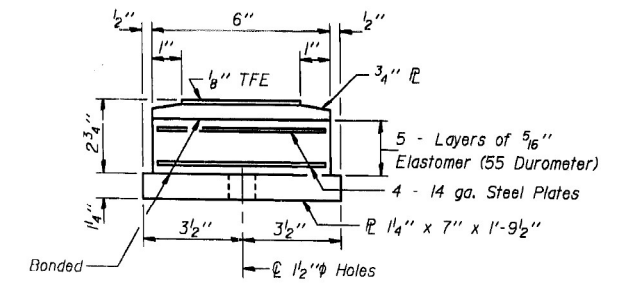
TYPE II TFE ELASTOMERIC EXP. BRG.



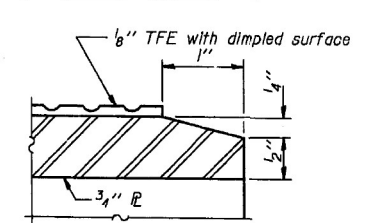
TOP BEARING ASSEMBLY



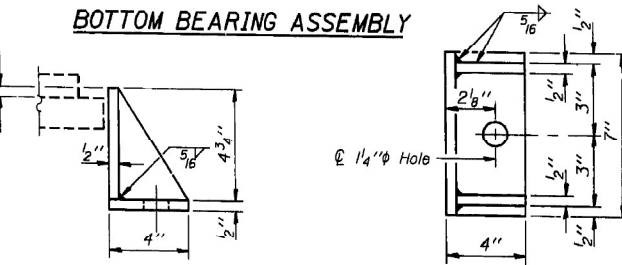
PLAN-TFE SURFACE



BOTTOM BEARING ASSEMBLY



SECTION THRU TFE



SIDE RETAINER #1

(22 Required)
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

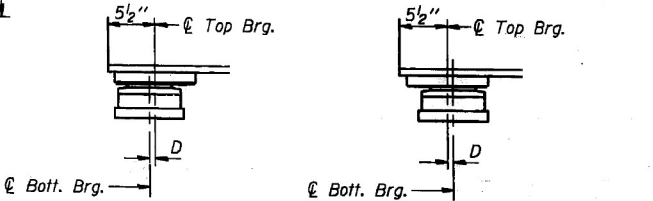
DESIGNED *Ch. K. Abard*
CHECKED *R. Doty*
DRAWN *R. Doty*
CHECKED *TKM*

EXAMINED *Dr. J. O. Kasper*
PASSED *James J. Robinson*
APPROVED

I-2-E2 12-1-83

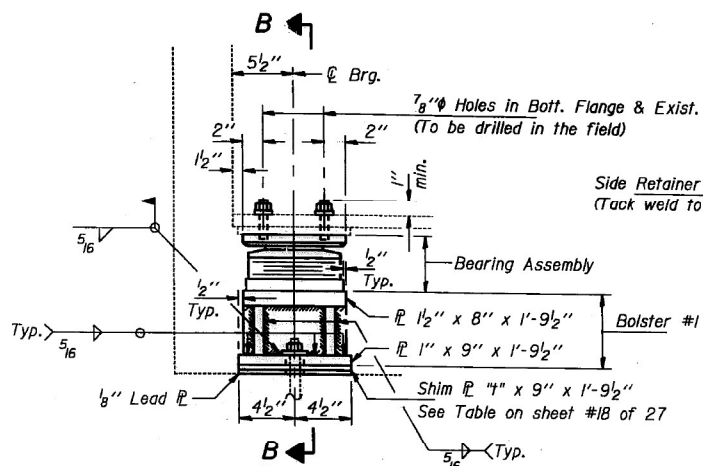
Note: The 1/8" TFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 1/8" TFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.



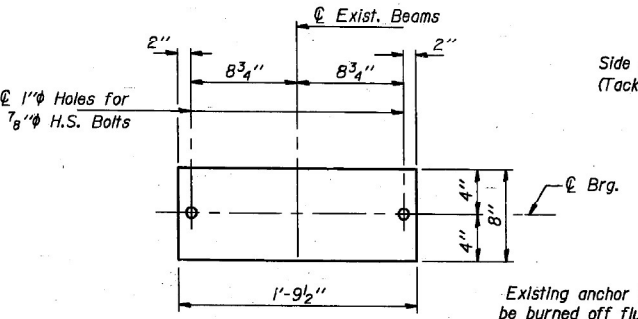
SETTING ANCHOR BOLTS AT EXP. BRG.

D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

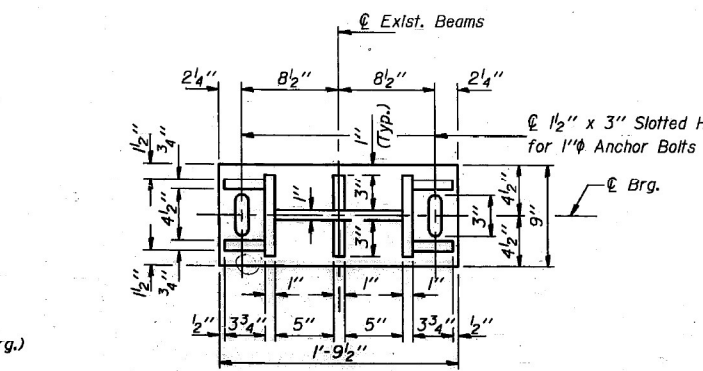


ELEVATION AT WEST ABUT.
(At existing beams)

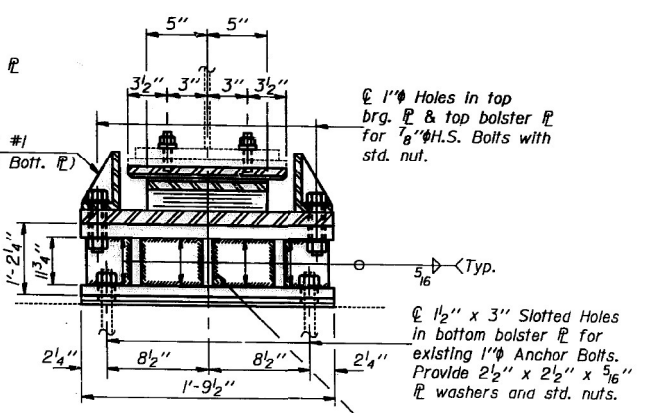
TYPE II TFE ELASTOMERIC EXP. BRG.



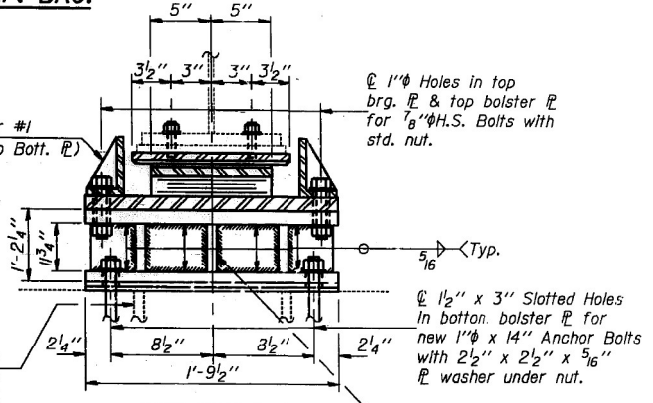
PLAN - TOP BOLSTER PLATE AT WEST ABUT.



PLAN - BOTT. BOLSTER PLATE AT WEST ABUT.



SECTION B-B
(At Existing Exterior Beams)



SECTION B-B
(At Existing Interior Beams)

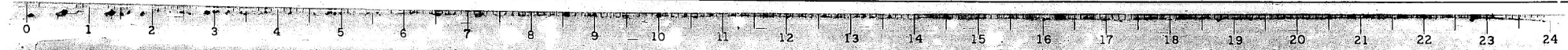
Notes: Weight of steel shim plates, lead plates, bolts with nuts & washers, side retainers and bolsters is included in "Structural Steel." Removal and disposal of existing bearing plates and rockers is incidental to "Removal of Existing Concrete Deck."

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type II	Each	11

BEARING DETAILS AT W. ABUT.
F.A.I. RT. 94 SEC. 1978-320BR-78
COOK COUNTY
STA. 41+62.67

FOR INFORMATION ONLY



C:\Users\Parker Foust\Project\CADD\22009 11111 St. ABNAINEW0160892_XXXX_SHT_DWG_REP.dgn 4:44:36 PM 5/4/2023

CZAPLUCKI LOPEZ, PLLC
201 KENMARE DRIVE
BURR RIDGE, IL 60527
630-915-8861

DESIGNED PAF
CHECKED RLP
DRAWN PAF
CHECKED RLP

REVISOR
REVISOR
REVISOR
REVISOR

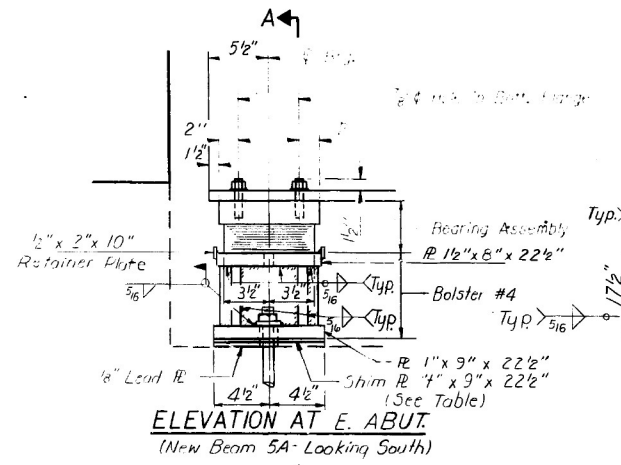
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RECORD DRAWINGS
STRUCTURE NO. 016-0992
SHEET NO. 22 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	40
CONTRACT NO. 60R77			ILLINOIS FED. AD PROJECT	

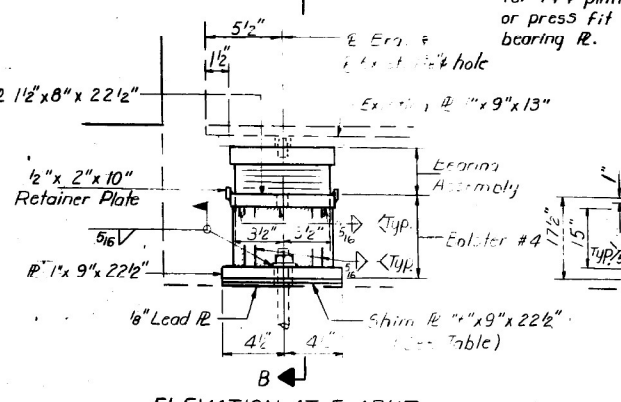
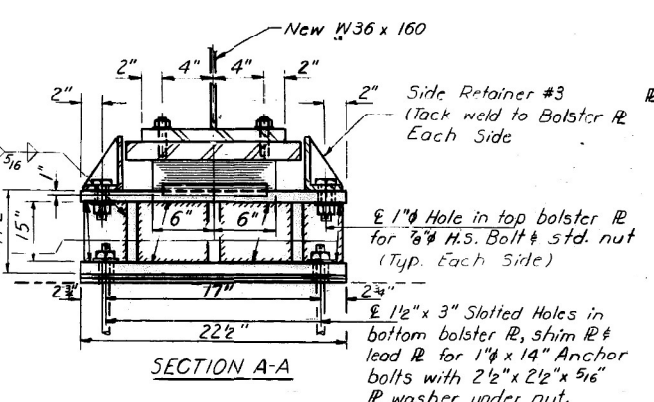
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	94	CONTRACT	COOK 98	41	SHEET NO.	27
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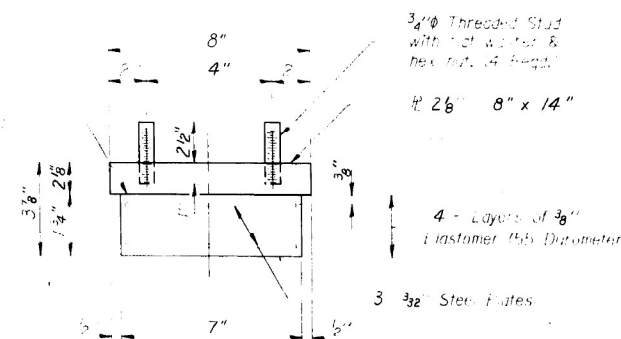
ELEVATION AT E. ABUT
(New Beam 5A- Looking South)

TYPE I ELASTOMERIC EXP. BRG.



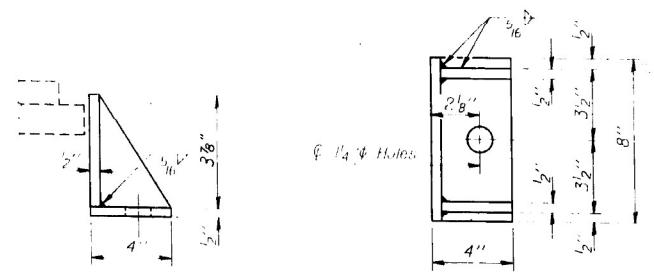
ELEVATION AT E. ABUT
(Existing Beams- Looking South)

TYPE I ELASTOMERIC EXP. BRG.



BEARING ASSEMBLY
(For New Beam 5A)
(1- Required)

Note: Shim plates shall not be placed in the Bearing Assembly.



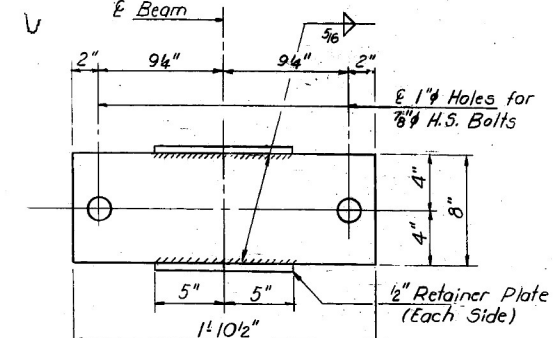
SIDE RETAINER #3
(22- Required)

Equivalent to metal angle with stiffeners will be allowed in lieu of welded plates.

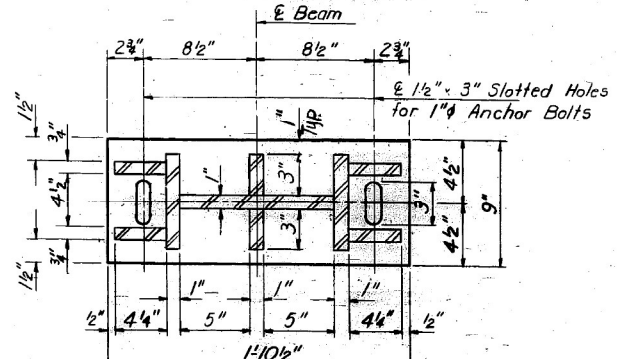
DESIGNED	<i>[Signature]</i>
CHECKED	<i>[Signature]</i>
DRAWN	R. Doty
CHECKED	<i>[Signature]</i> DKM

EXAMINED	<i>[Signature]</i>	July 14, 1986
PASSED	<i>[Signature]</i>	ENGINEER OF BRIDGE DESIGN
APPROVED	<i>[Signature]</i>	ENGINEER OF BRIDGES & STRUCTURES
		DIRECTOR OF HIGHWAYS

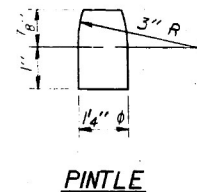
Notes: See sheet #21 of 27 for Anchor Bolt installation details.
Weight of shim plates, H.S. Bolts, nuts, washers, lead plates, side retainers and bolster assemblies is included in "Structural Steel".
Grease pintles before erection.
Removal and disposal of existing bearing plates and rockers is incidental to "Removal of Existing Concrete Deck."



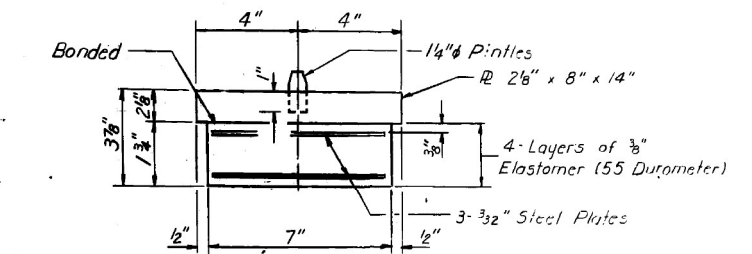
PLAN-TOP BOLSTER PLATE



PLAN-BOTTOM BOLSTER PLATE



PINTLE



BEARING ASSEMBLY
(For Existing Beams)
(10- Required)

TABLE OF "t" DIMENSIONS

Location	Bms. 1#10	Bms. 2#9	Bms. 3#8	Bms. 4#7	Bms. 5#6	Bm. 5A
West Abut.			4"	3 3/8"	5 3/8"	
Pier #1			3 3/8"	1/2"	3 3/8"	
Pier #2	1 1/8"	1 1/8"	3 1/8"	5 1/8"	9 1/8"	
East Abut.	1/4"		5 1/8"	7 1/8"	11 1/8"	11 1/8"

BILL OF MATERIAL

Item	Unit	Qty
Elastomeric Bearing Assembly Type I		11

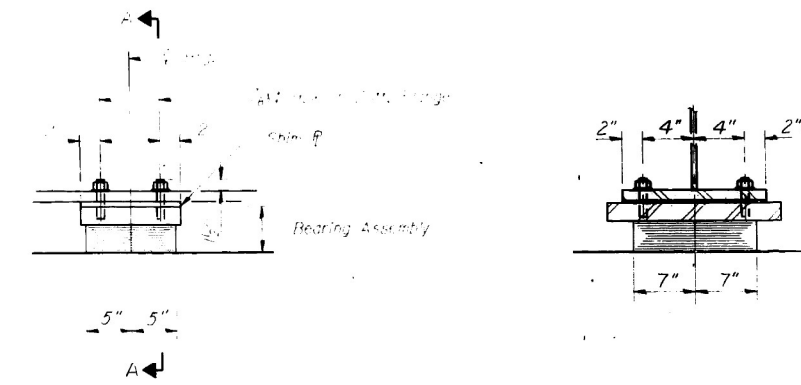
BEARING DETAILS
AT EAST ABUTMENT
F.A.I. RT. 94 SEC. 1978-320BR-78
COOK COUNTY
STA. 41+62.67

FOR INFORMATION ONLY

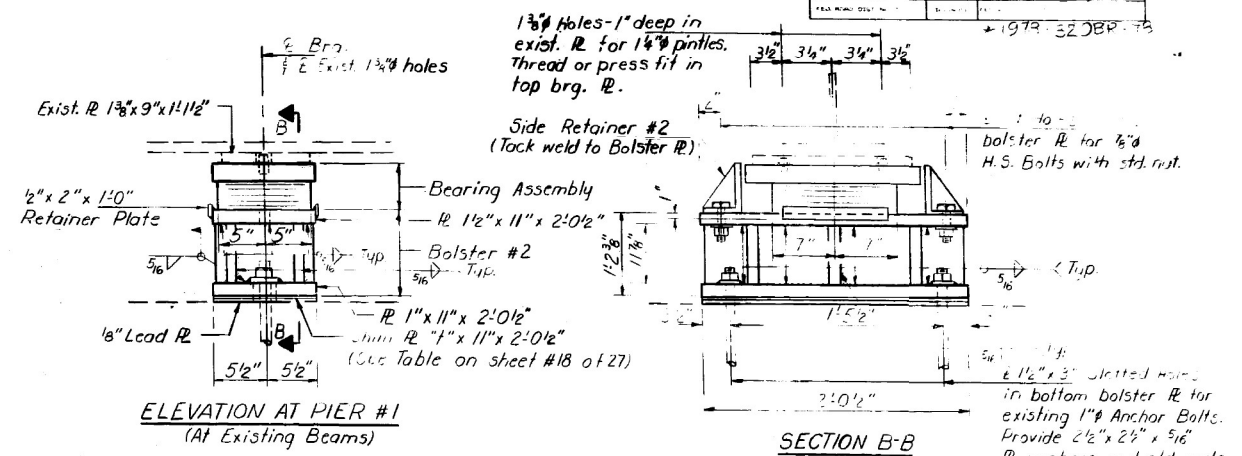
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

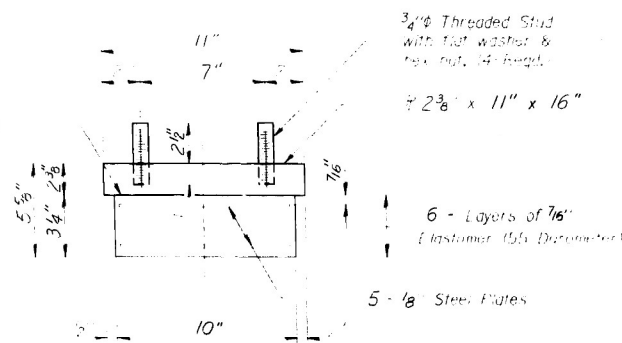
ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
94	*	COOK	38	42
SHEET NO. 13				27 SHEETS



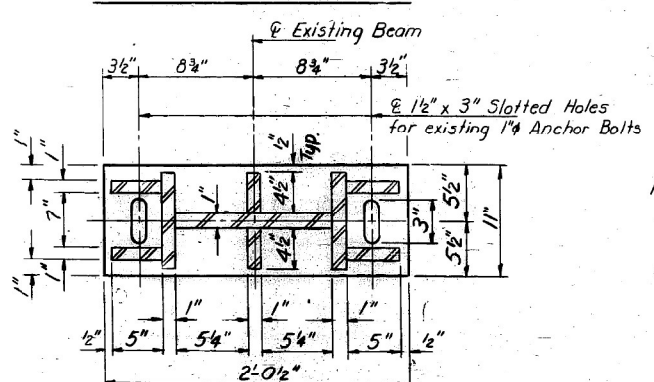
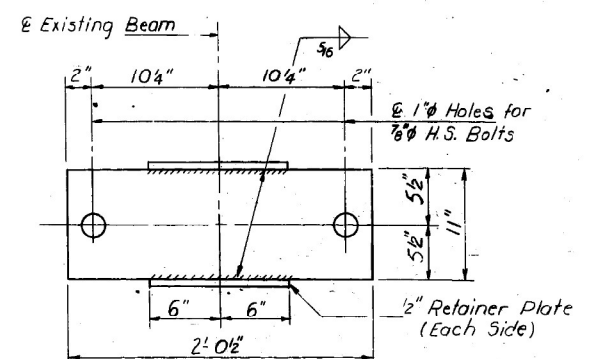
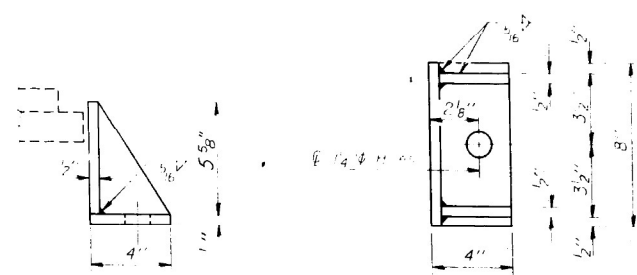
TYPE I ELASTOMERIC EXP. BRG.



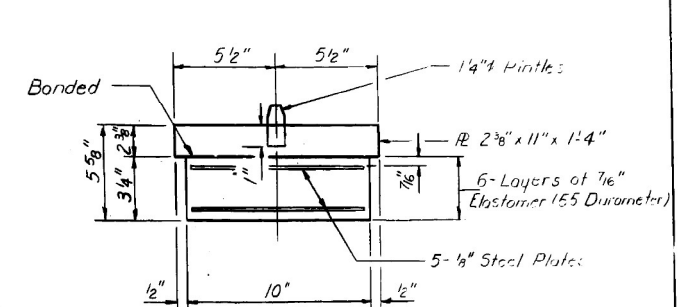
TYPE I ELASTOMERIC EXP. BRG.



Note: Shim plates shall not be placed under Bearing Assembly.



Notes:
Shim plates shall not be placed under Bearing Assembly.
Weight of steel shim plates, lead plates, bolts with nuts and washers, side retainers and bolsters is included in "Structural Steel."
Removal and disposal of existing bearing plates and rockers is incidental to "Removal of Existing Concrete Deck."
Grease pintles before erection.



BILL OF MATERIAL

Item	Unit	Qty
Elastomer's Bearing Assembly Type I	Ea.	11

BEARING DETAILS AT PIER #1
F.A.I. RT. 94 SEC. 1978-320BR-78
COOK COUNTY
STA. 44+62.67

FOR INFORMATION ONLY

DESIGNED: *[Signature]*
CHECKED: *[Signature]*
DRAWN: R. Doty
CHECKED: *[Signature]* DKA

EXAMINED: *[Signature]*
PASSED: *[Signature]*
APPROVED: *[Signature]*
DIRECTOR OF HIGHWAYS

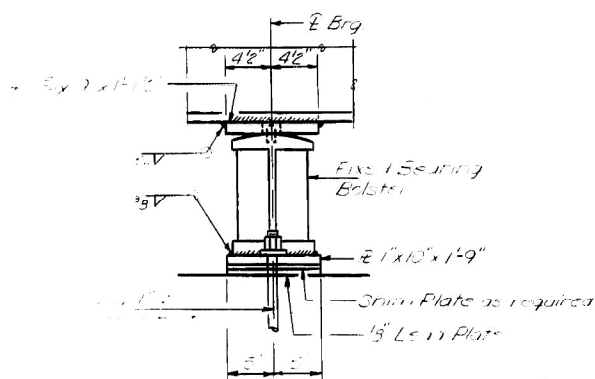
I-2-EI 12-1-83



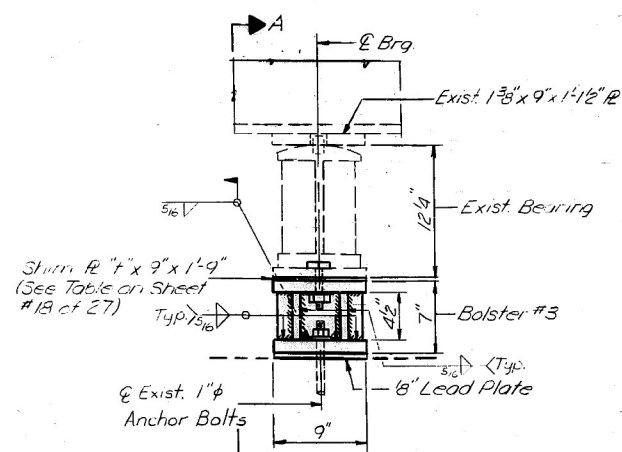
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

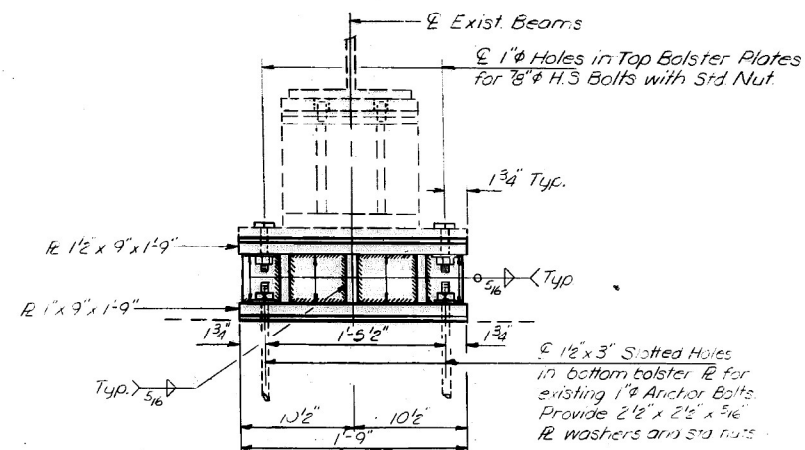
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. RT. 94	4	COOK	98	43
SHEET NO. 20				
CONTRACT NO. 60R77				
*1978-320BR-78				



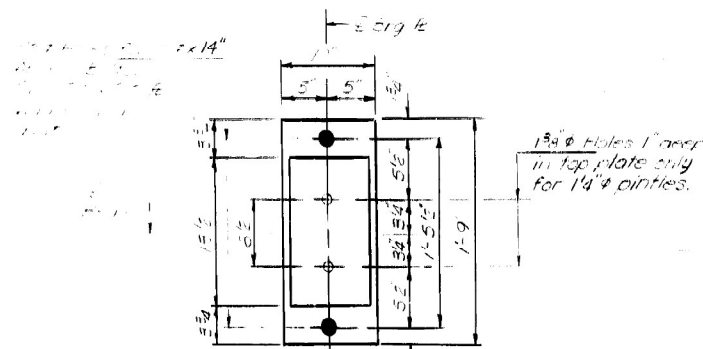
ELEVATION AT PIER #2
(New Beam 5A)



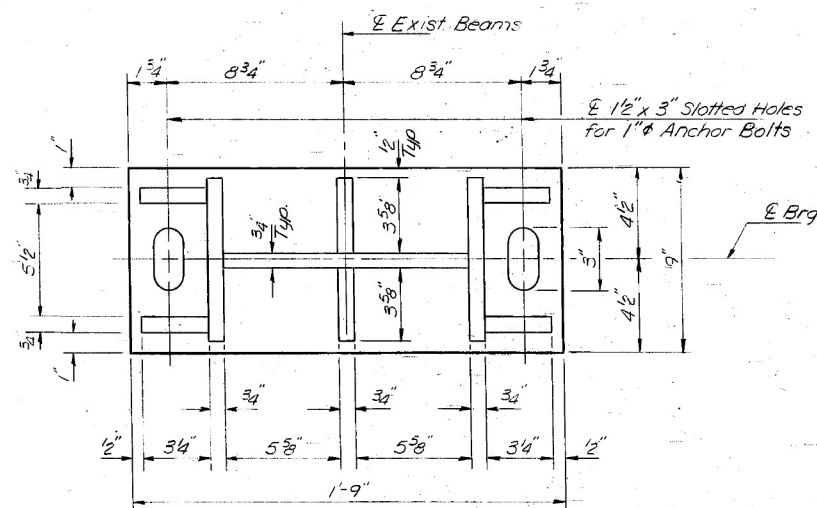
ELEVATION AT PIER #2
(Existing Beams)



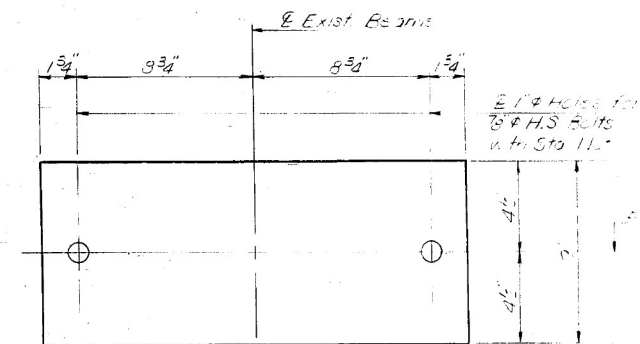
SECTION A-A



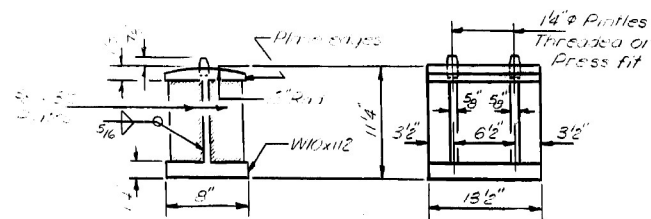
PLAN



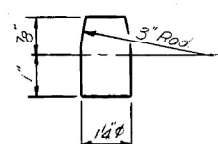
PLAN-BOTT. BOLSTER PLATE



PLAN-TOP BOLSTER PLATE



DETAIL OF FIXED BEARING BOLSTER
AT PIER #2
(1 Required)



PINTLE

Note: Weight of steel shim plates, bolts with nuts and washers, steel plates and bolsters is included in "Structural Steel".
Grease pintles before erection.
See sheet #21 of 27 for Anchor Bolt installation details.
New anchor bolts at fixed bearings may be built into the masonry.

BEARING DETAILS AT PIER #2
F.A.I. RT. 94 SEC. 1978-320BR-78
COOK COUNTY
STA. 41+62.67

FOR INFORMATION ONLY

DESIGNED	<i>G.R. Alford</i>	EXAMINED	<i>Draj J. Slesar</i>
CHECKED	<i>D.K.M. Muller</i>	PASSED	<i>James T. Korburn</i>
DRAWN	L. Wanless	APPROVED	
CHECKED	<i>G.R.A.</i>		

PLOT DATE = 5/4/2023

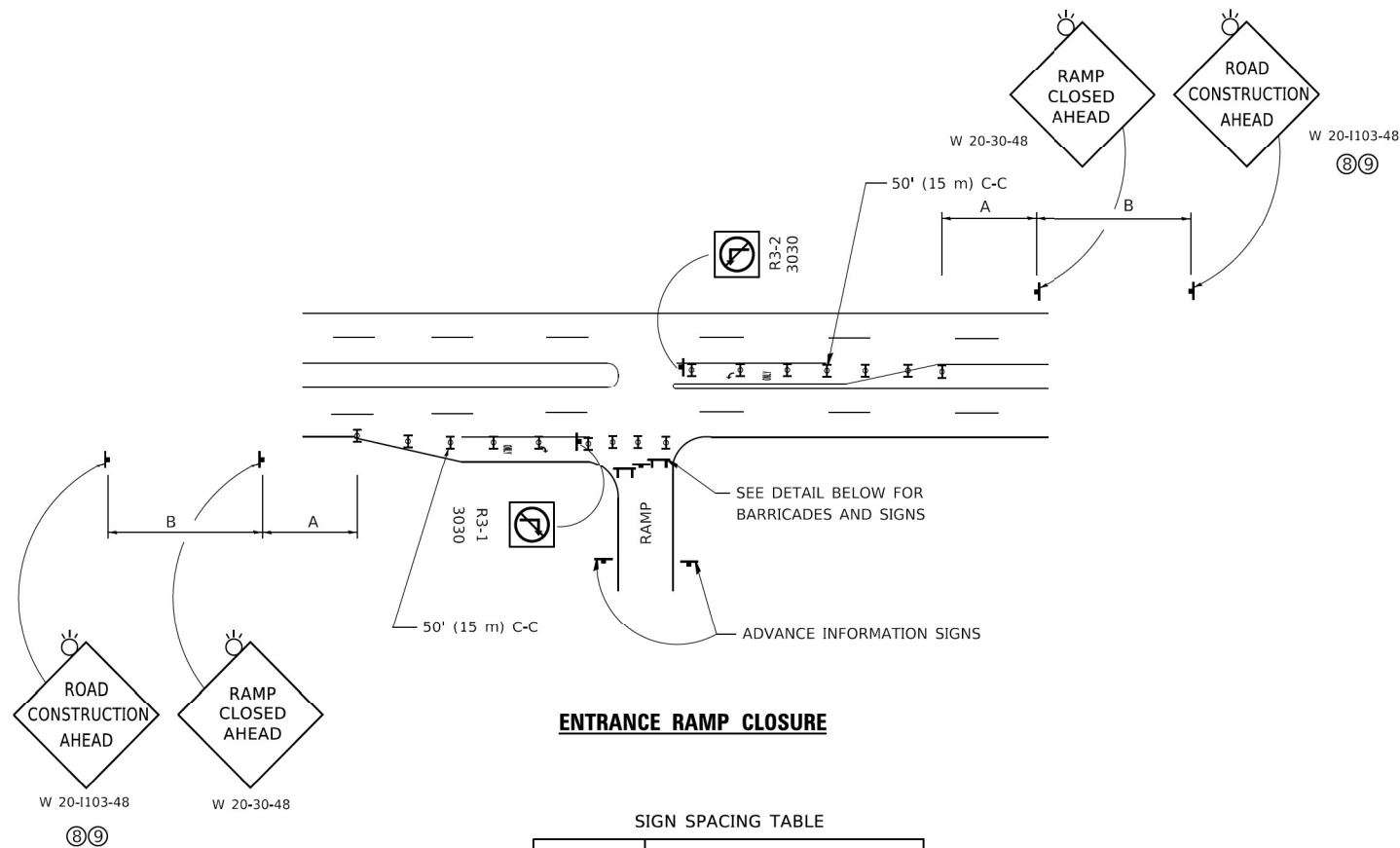
DESIGNED	PAF	REVISED	
CHECKED	RLP	REVISED	
DRAWN	PAF	REVISED	
CHECKED	RLP	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RECORD DRAWINGS
STRUCTURE NO. 016-0992

SHEET NO. 25 OF 25 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1112-701HB-BR	COOK	59	43
CONTRACT NO. 60R77				
ILLINOIS FED. AID PROJECT				

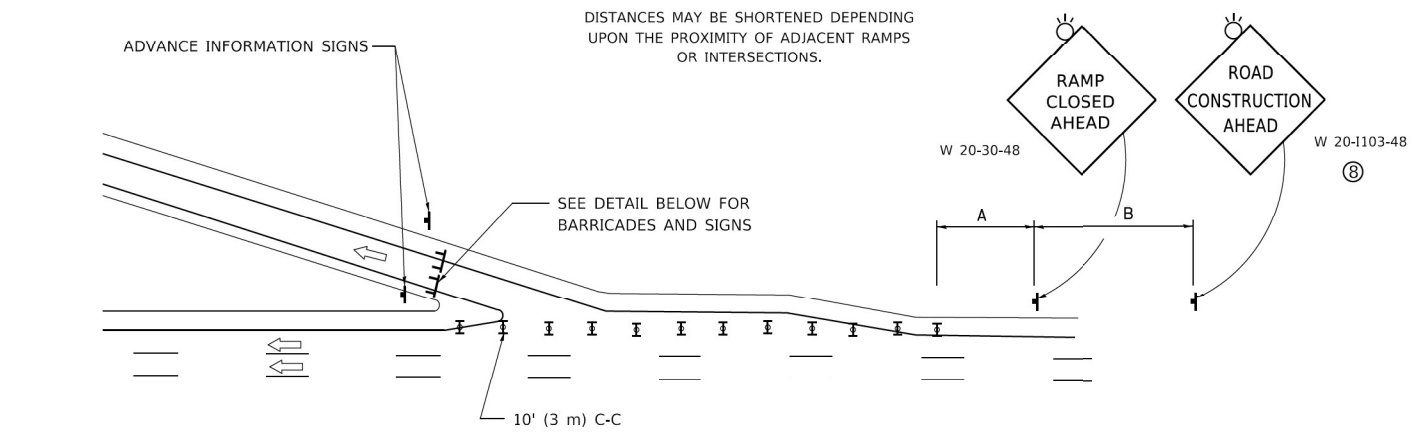


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

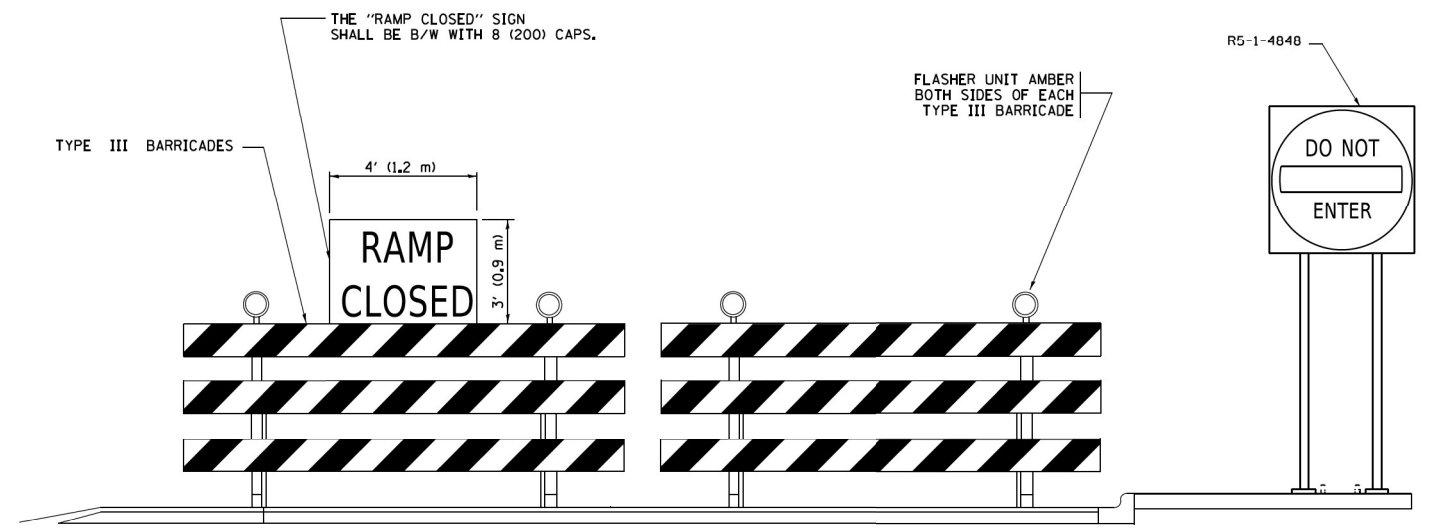
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

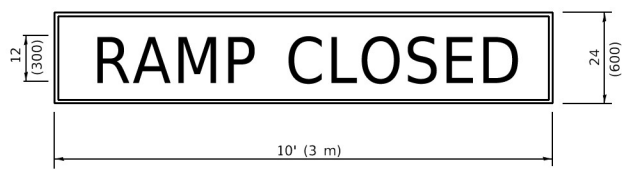
SYMBOLS

- ▬ TYPE II BARRICADE OR DRUM
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

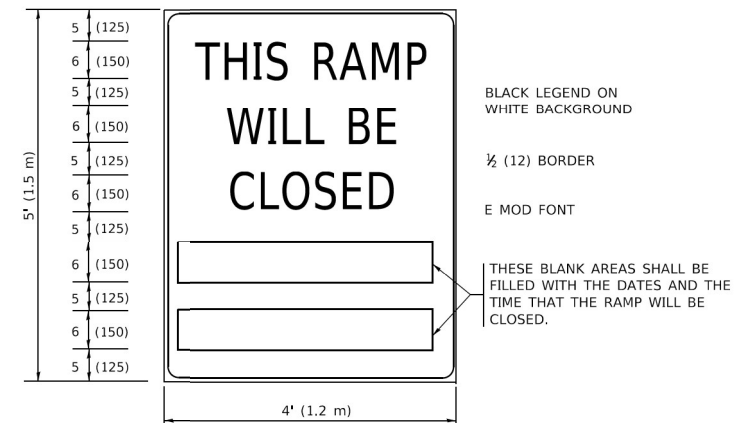
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
½ (12) BORDER
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: P:\110848\BID\ITEC\Illinois.gov\PIVDOT\Documents\DOT_Offices\Dirfrct_T\Projects\SHS\22\23\CADD\HAI\CAD\Sheets\1008.dgn

USER NAME = footemj	DESIGNED - D.W.S.	REVISED - S.P.B._01-07
	DRAWN -	REVISED - S.P.B._12-09
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - M.D._06-13
PLOT DATE = 3/4/2019	DATE - 02-83	REVISED - M.D._01-18

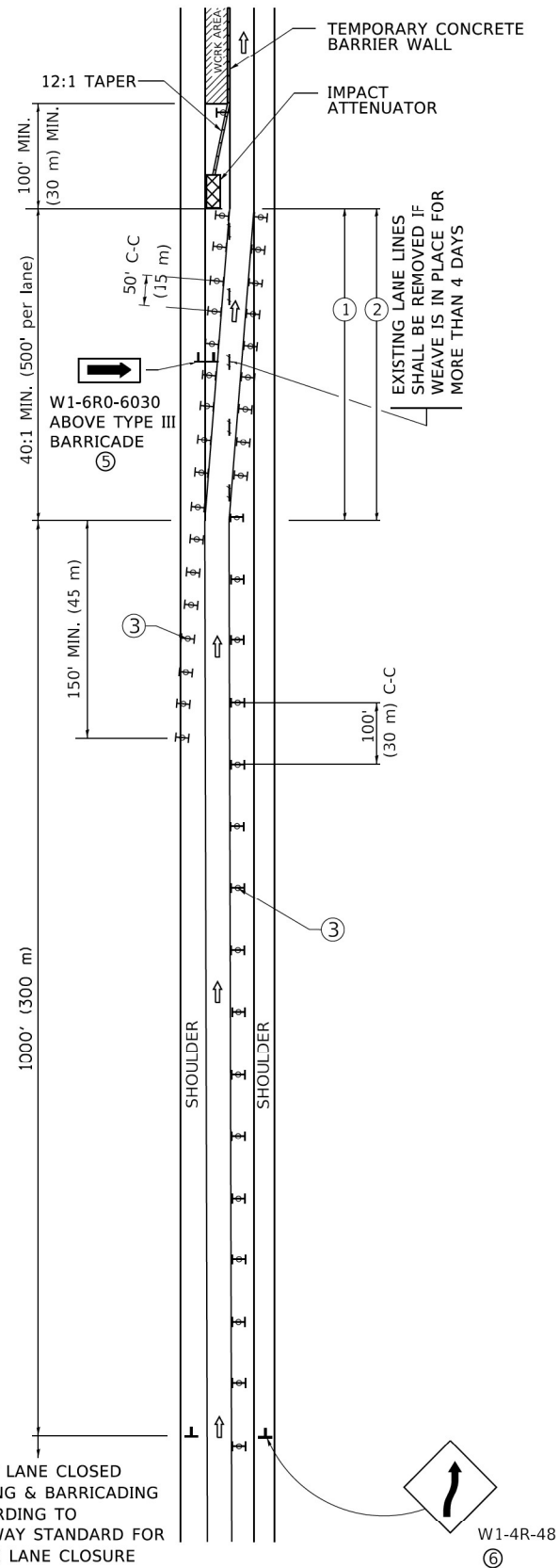
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

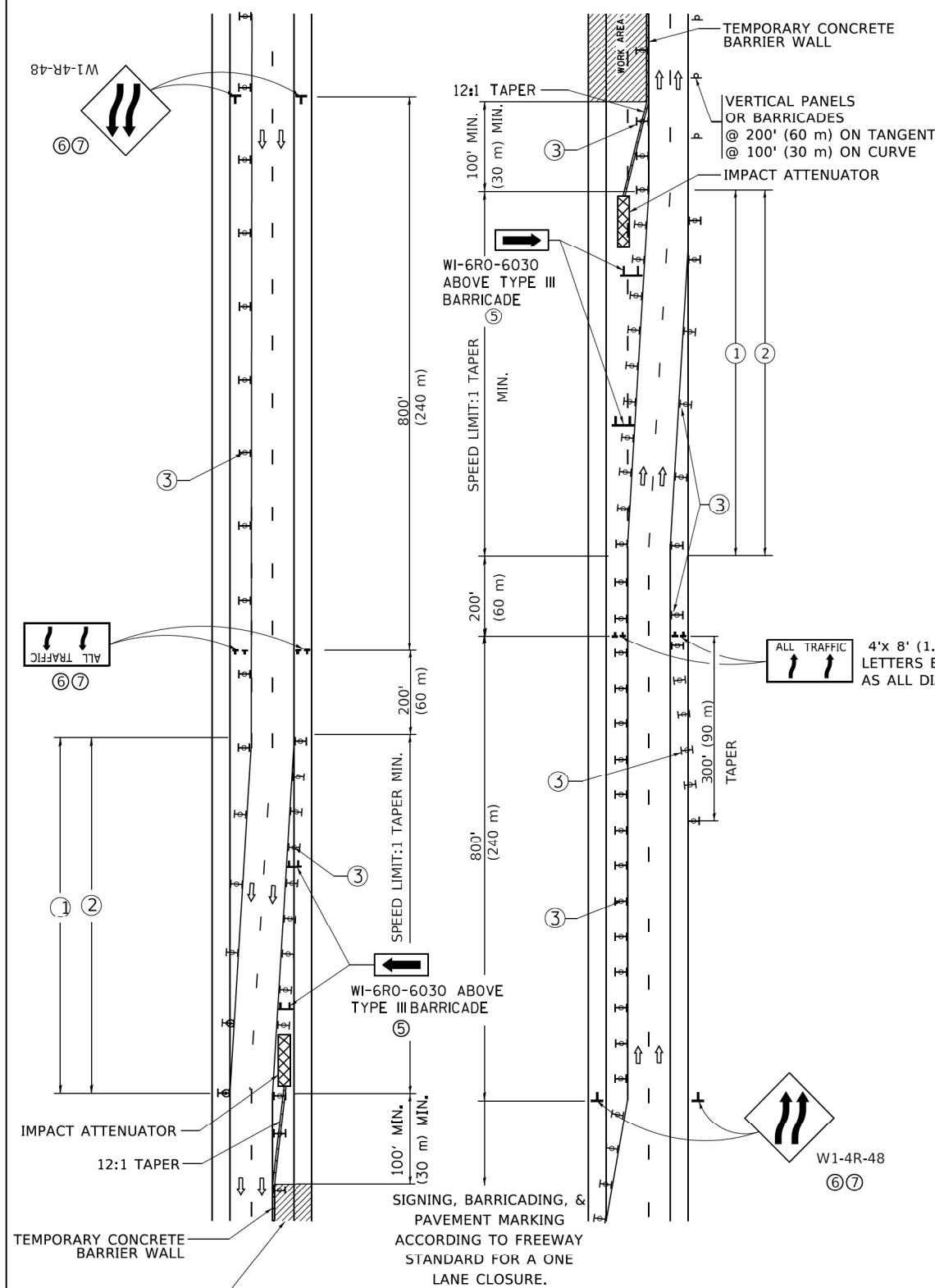
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-08		59	44
CONTRACT NO.			ILLINOIS FED. AID PROJECT	

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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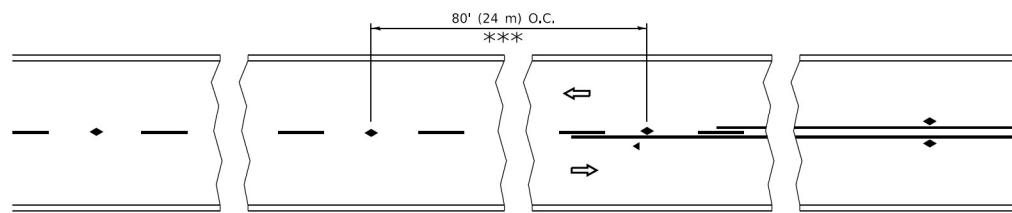
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PLOT DATE = 3/4/2019	DATE - 02-87	REVISED - M.D. 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE**

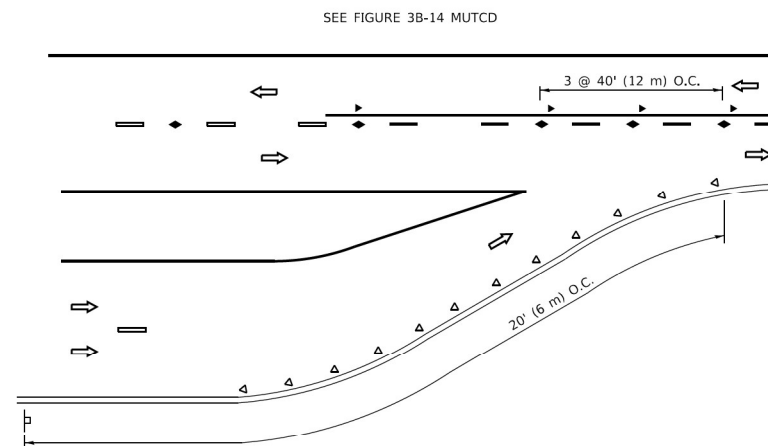
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CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

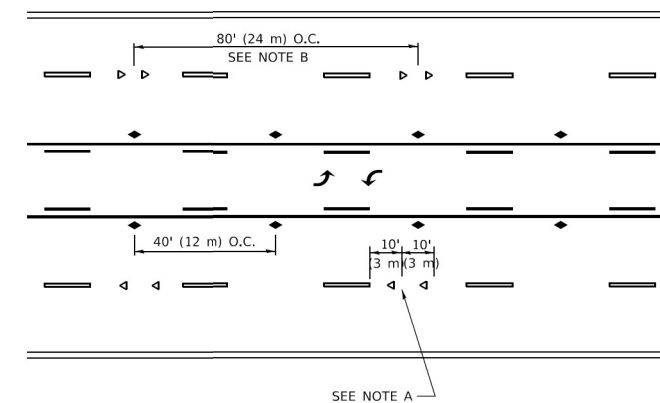


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

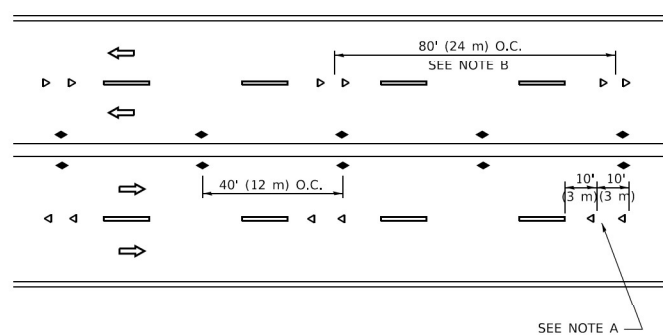
TWO-LANE/TWO-WAY



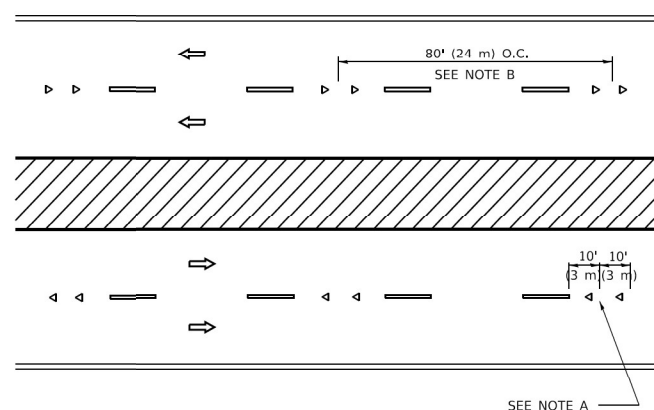
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

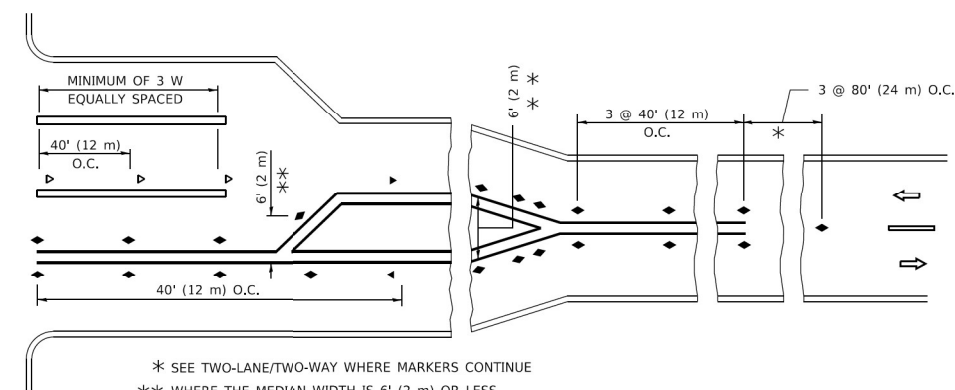
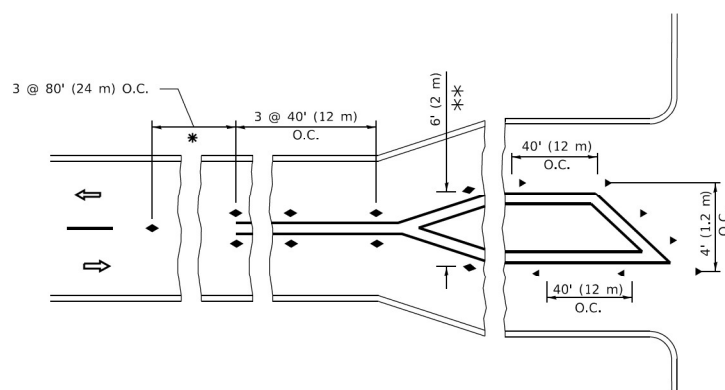
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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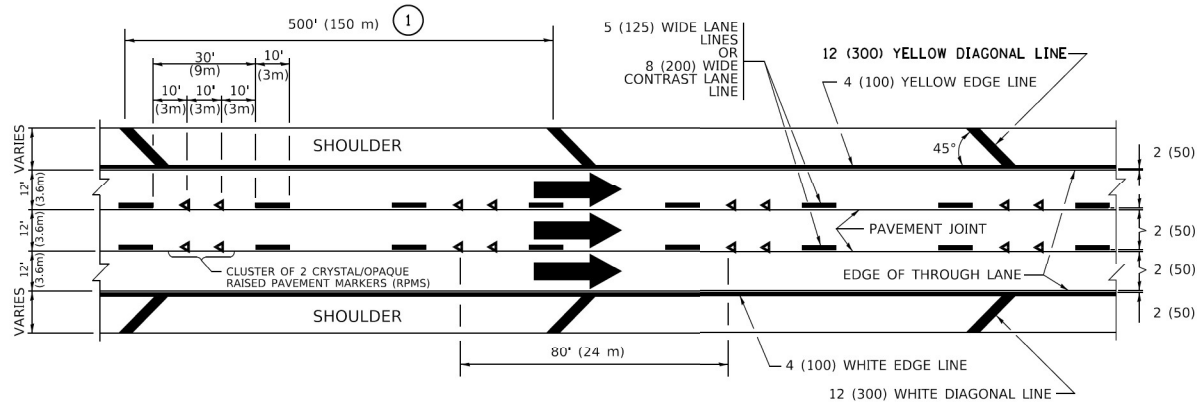
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	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

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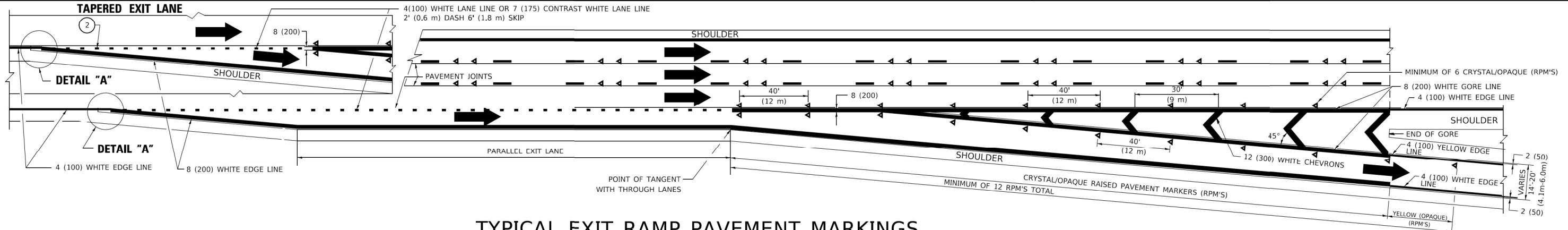
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TC-11			CONTRACT NO.	
ILLINOIS			FED. AID PROJECT	



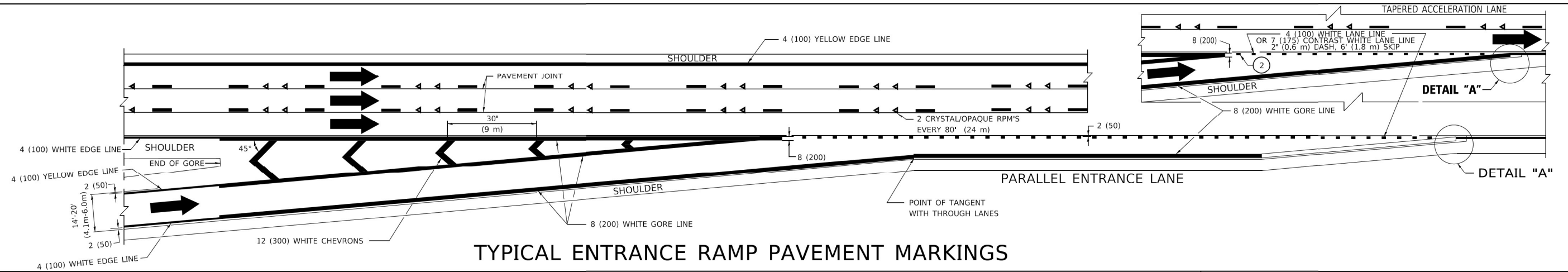
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

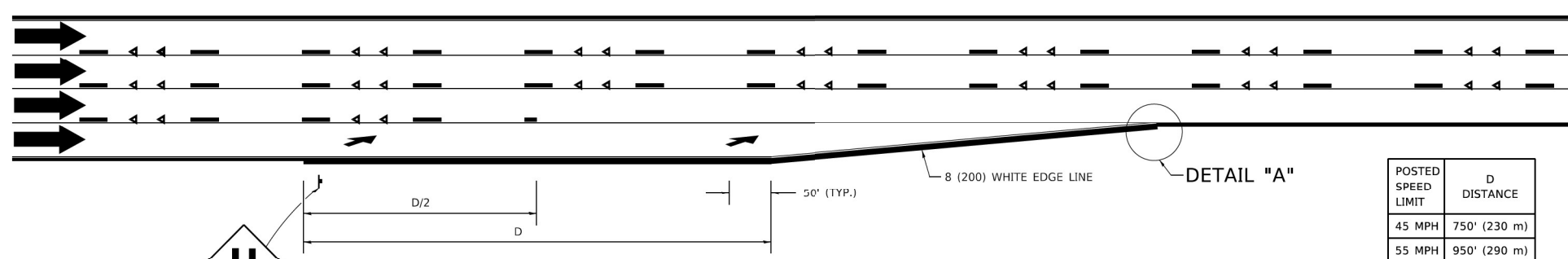
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

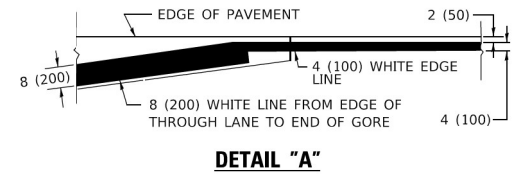


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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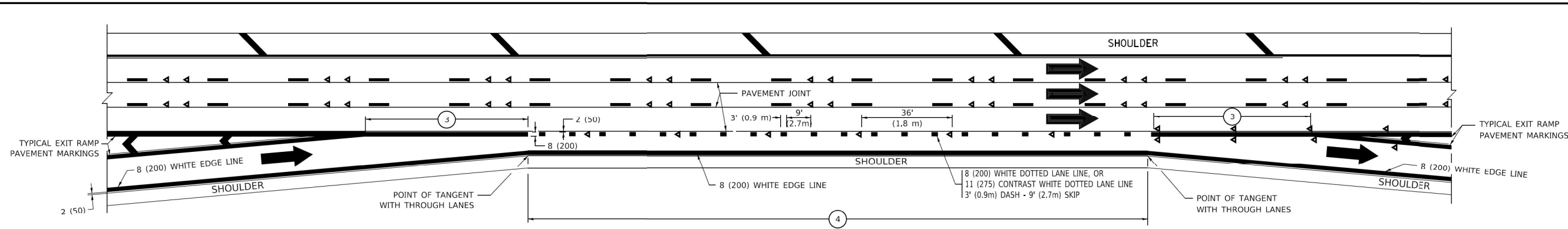
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

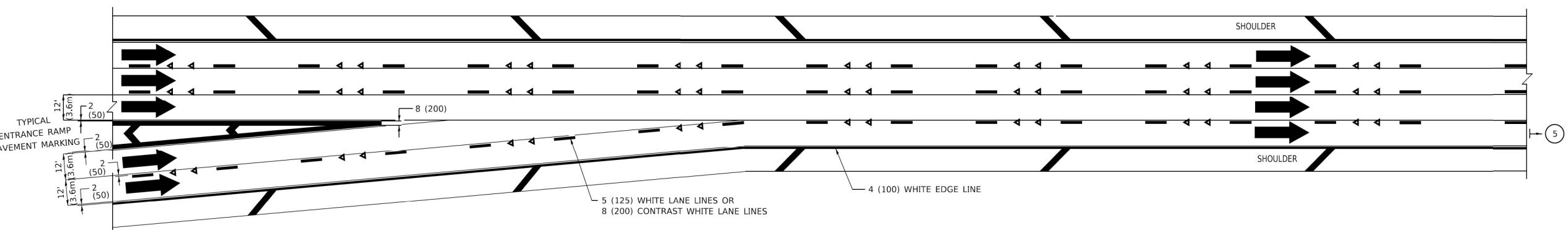
MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

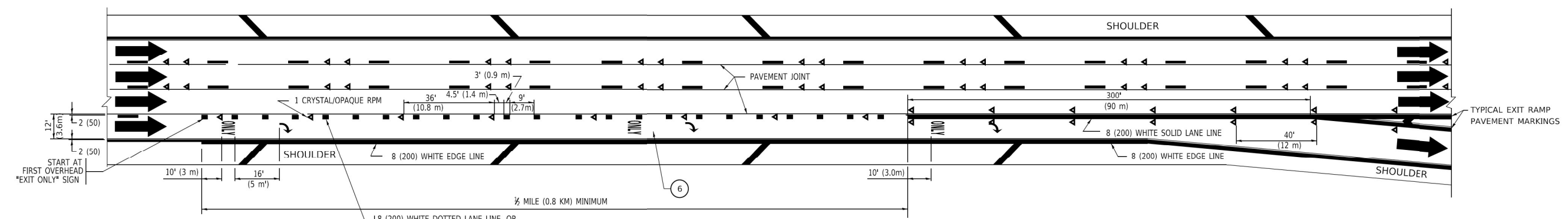
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ILLINOIS FED. AID PROJECT			CONTRACT NO.	



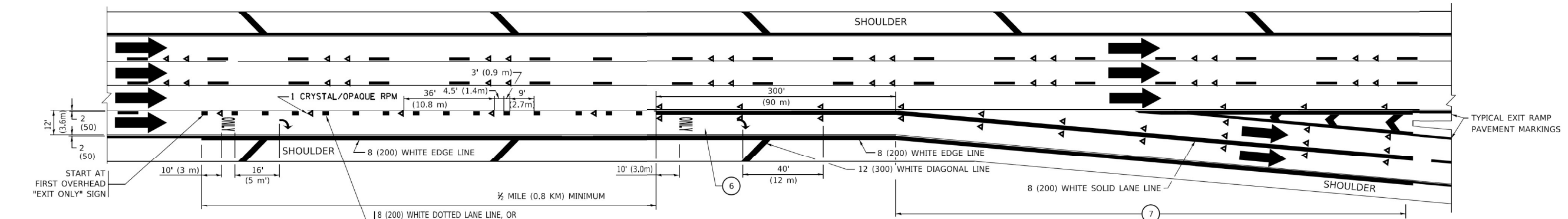
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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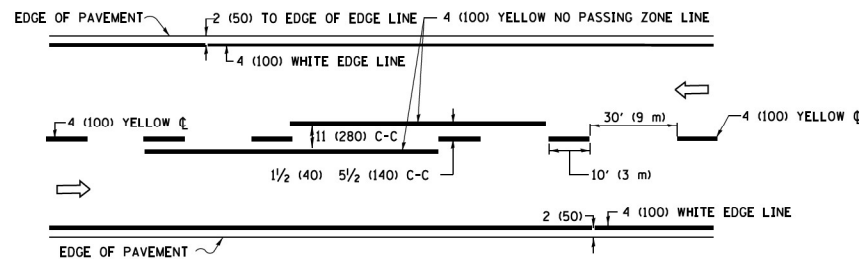
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

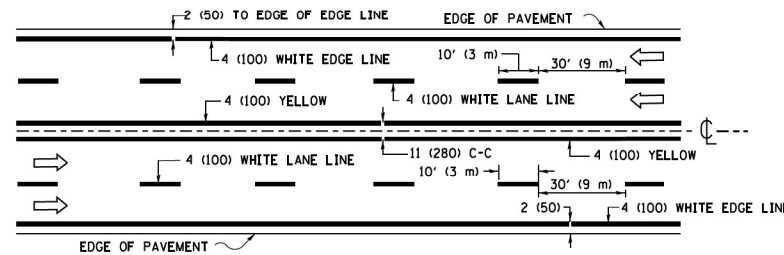
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

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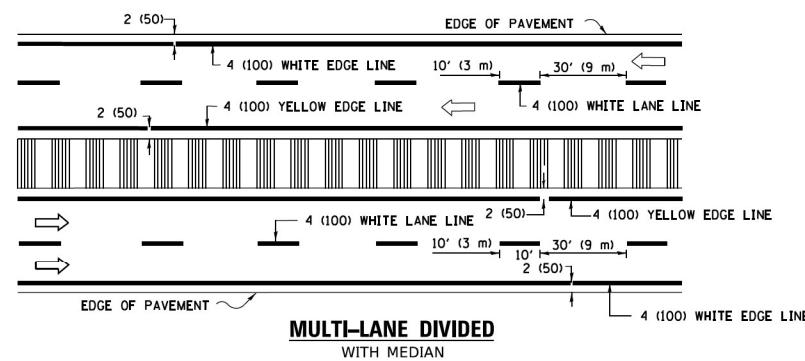
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TC-12		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		



2-LANE ROADWAY

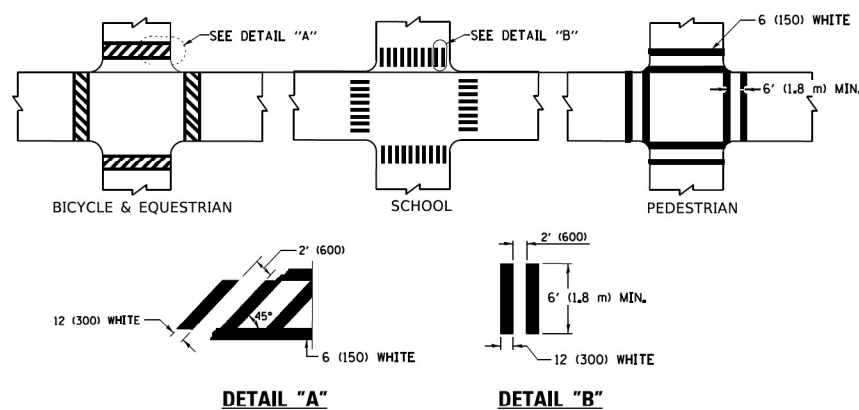


MULTI-LANE UNDIVIDED



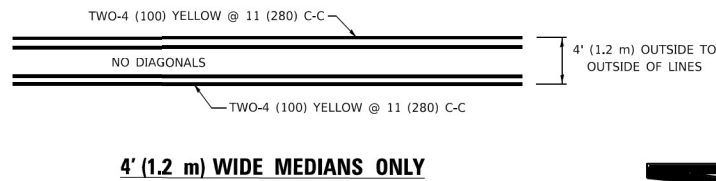
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

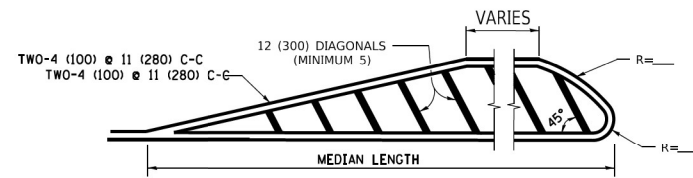


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

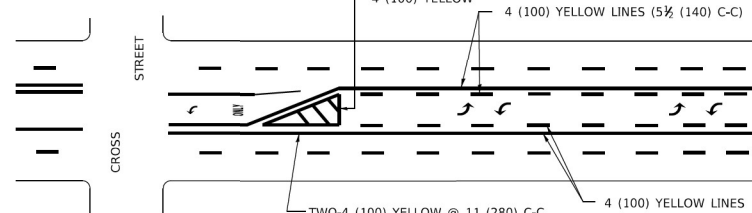


4' (1.2 m) WIDE MEDIANS ONLY



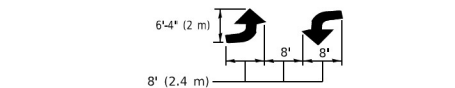
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

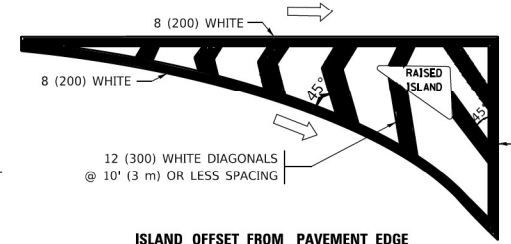


TYPICAL LEFT (OR RIGHT) TURN LANE

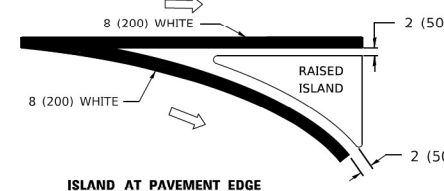
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

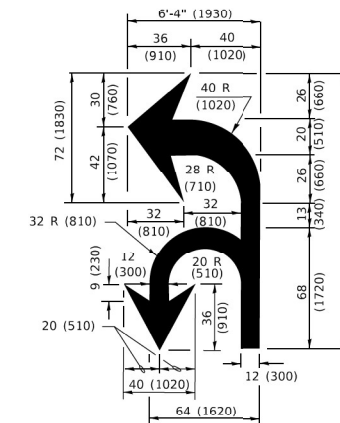


ISLAND OFFSET FROM PAVEMENT EDGE

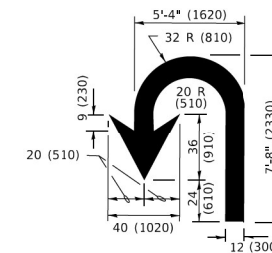


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

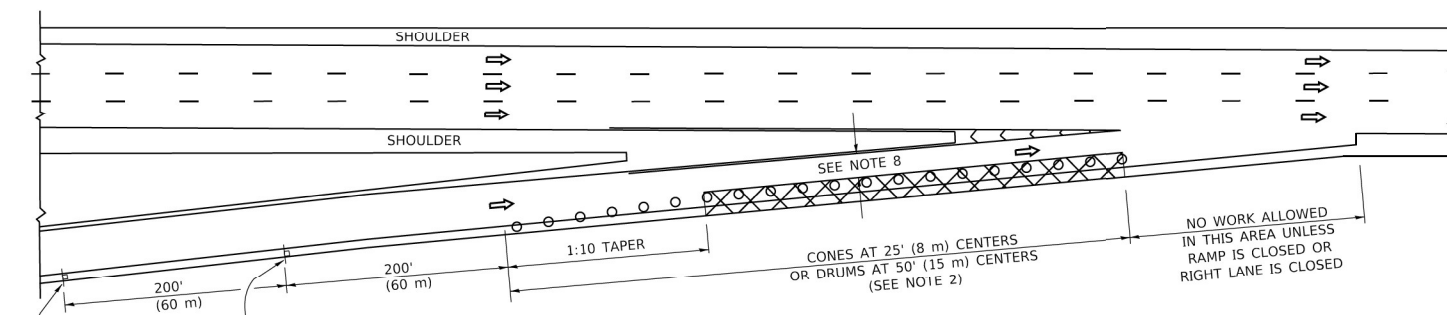
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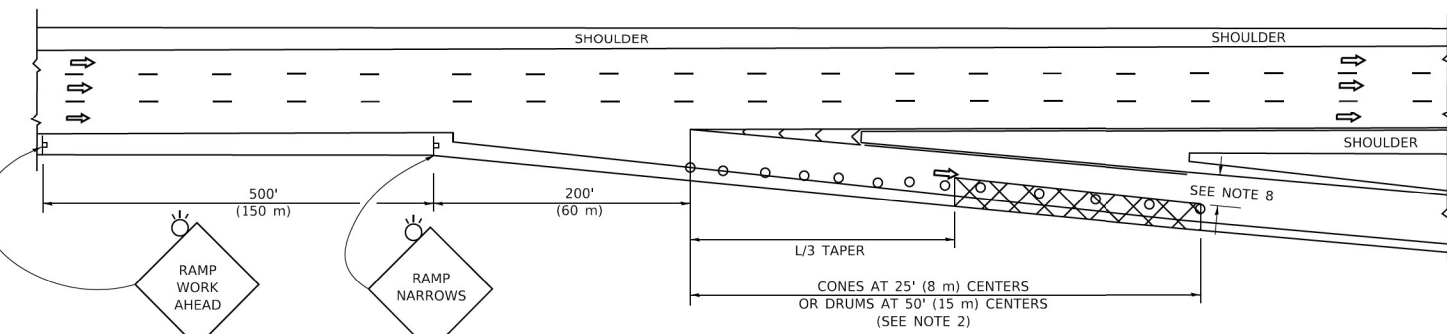
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS				F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET 1	OF 2 SHEETS	STA. TO STA.	TC-13		ILLINOIS	59	49
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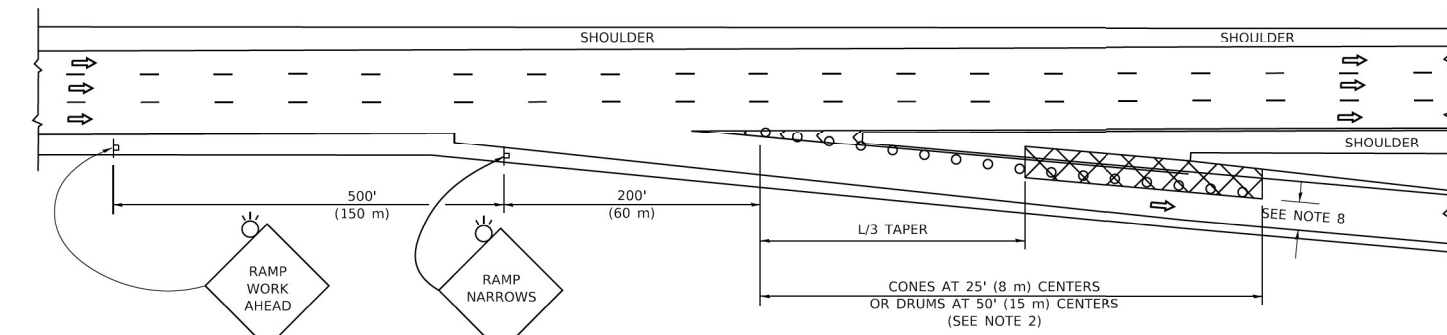
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

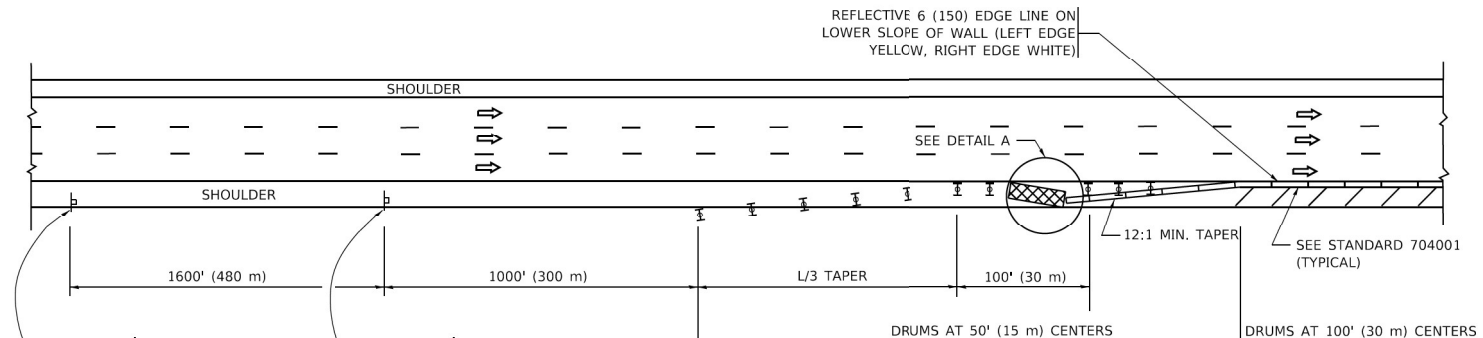
GENERAL NOTES:

- THE "L" DISTANCE EQUALS:

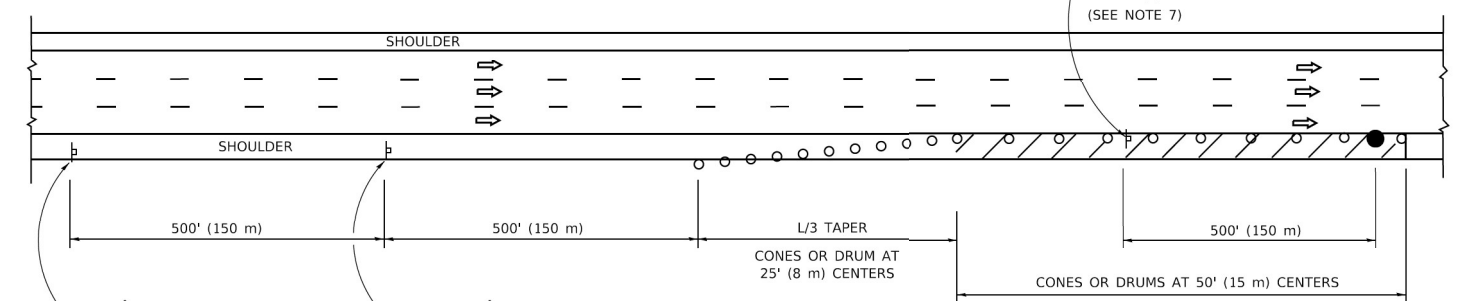
SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

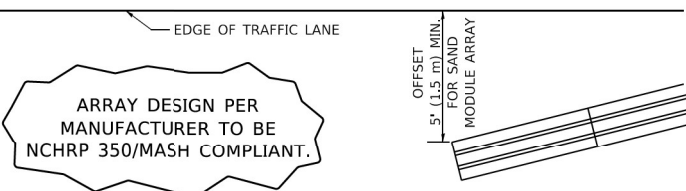


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Defaul
FILE: \\mspc-pw\illinois\B&E\BID\NTEC\Illinois\gov\PIVDOT\Documents\DOT - Offices\B&E\1\Projects\ID\5422\2-24\CADD\B&E\CAD\sheet\111.cgm

USER NAME = footemj	DESIGNED -	REVISED - S.P.B. 01-07
	DRAWN - D.W.S.	REVISED - S.P.B. 12-09
PLOT SCALE = 50.0000 "/td> <td>CHECKED -</td> <td>REVISED - M.D. 06-13</td>	CHECKED -	REVISED - M.D. 06-13
PLOT DATE = 3/4/2019	DATE - 11-96	REVISED - M.D. 01-18


**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**


TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	50
TC-17			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

MODEL: Defaul
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ROUTE MARKERS


 FOR U.S. ROUTES
M1-40-2424


 FOR ILLINOIS ROUTES
M1-50-2424


 R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND


ARROWS SIGNS

 M5-1L-2115


 M5-1R-2115


 M6-1-2115


 M6-2-2115


 M6-3-2115


CARDINAL DIRECTION & DETOUR SIGNS

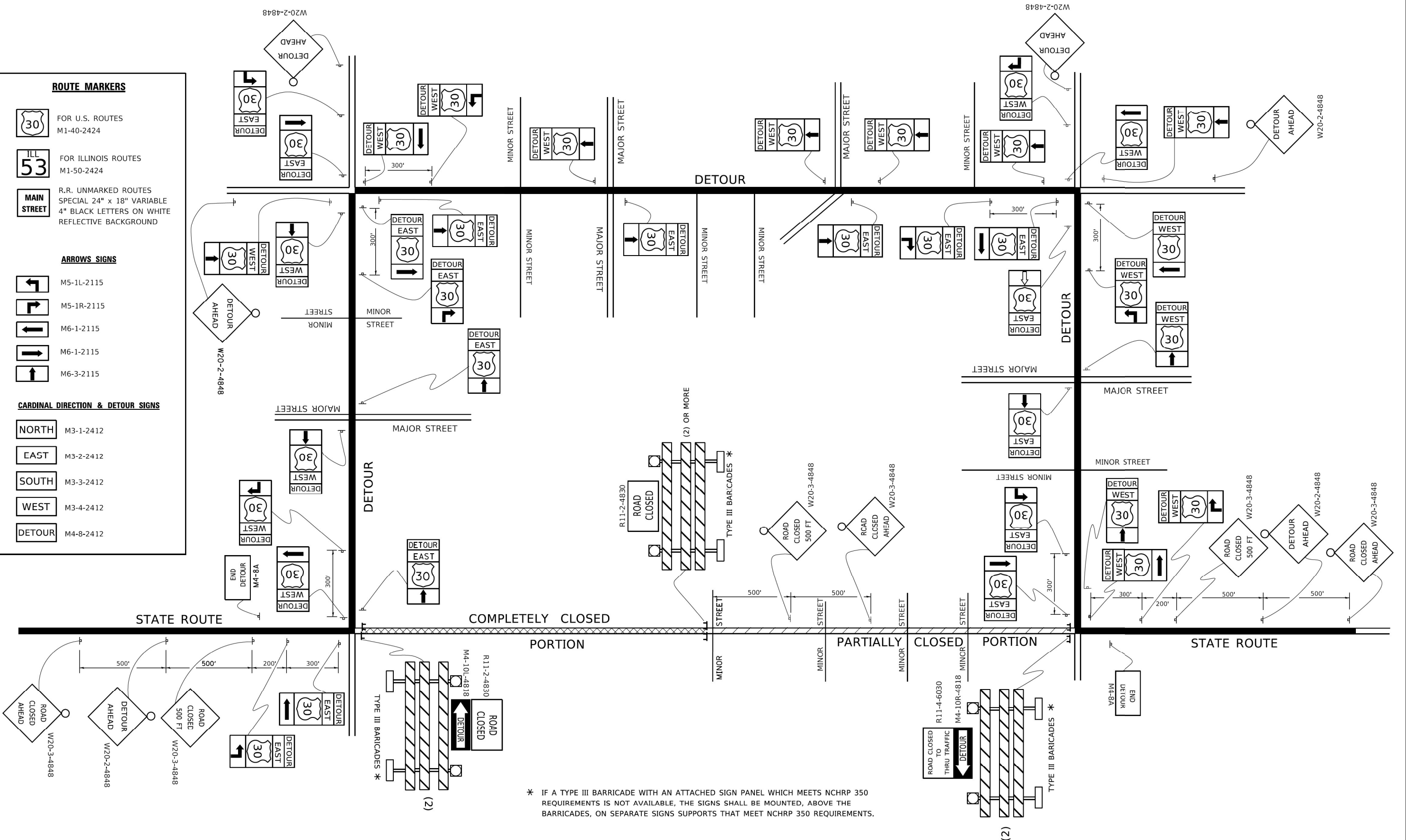
 NORTH M3-1-2412

 EAST M3-2-2412

 SOUTH M3-3-2412

 WEST M3-4-2412

 DETOUR M4-8-2412



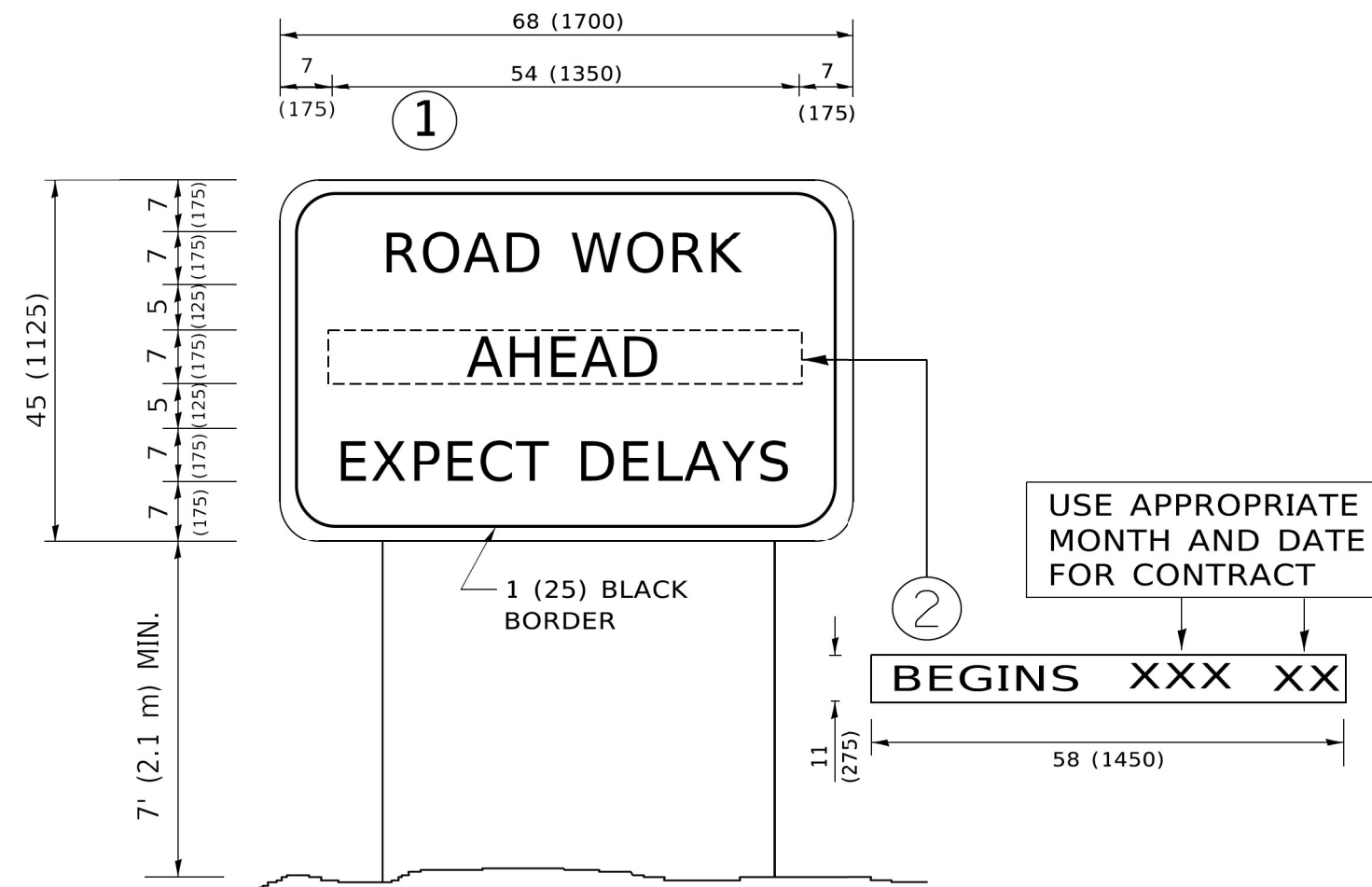
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

USER NAME = footemj	DESIGNED -	REVISED - 10-18-02
	DRAWN -	REVISED - R. BORO 09-14-09
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-21		59	51
ILLINOIS		CONTRACT NO.		
FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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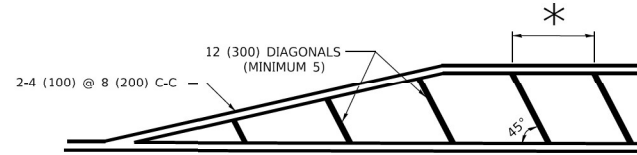
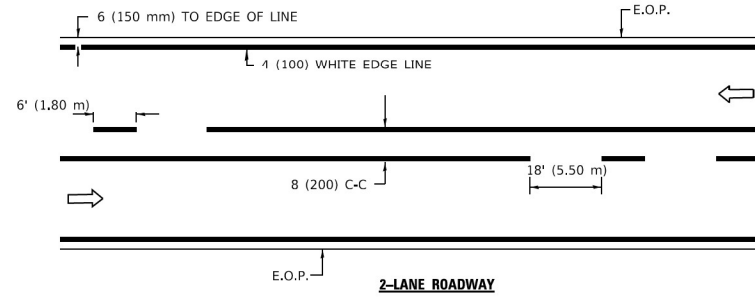
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	DRAWN -	REVISED - R. MIRS 12-11-97
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PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

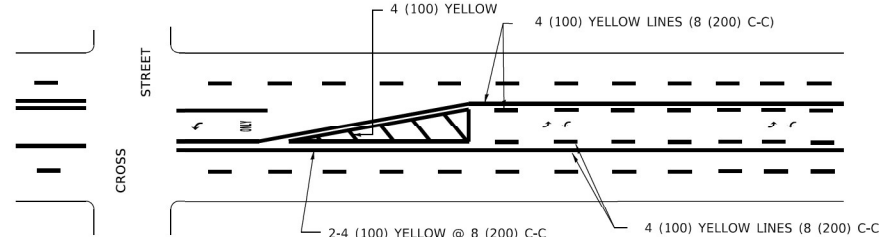
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	52
TC-22			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				



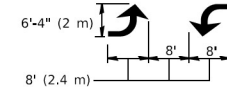
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

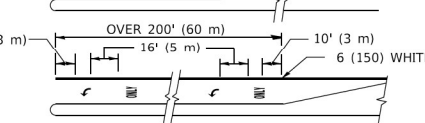
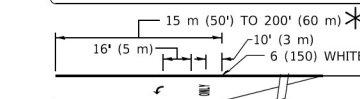
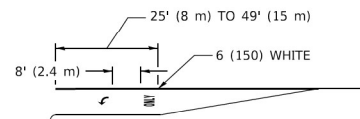
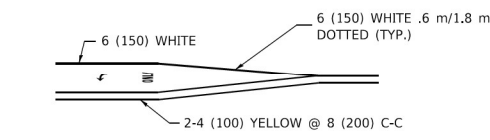


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

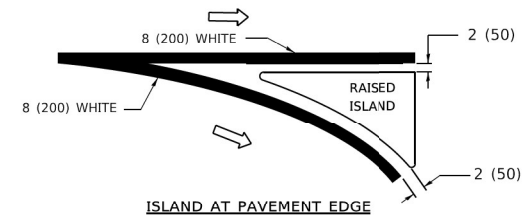
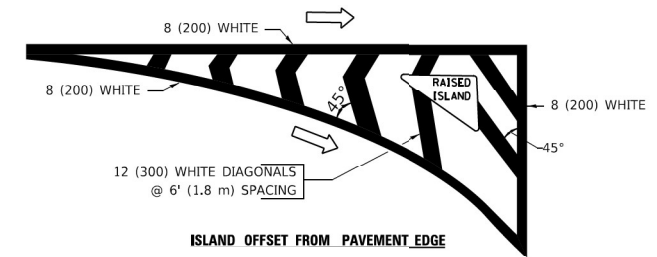


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

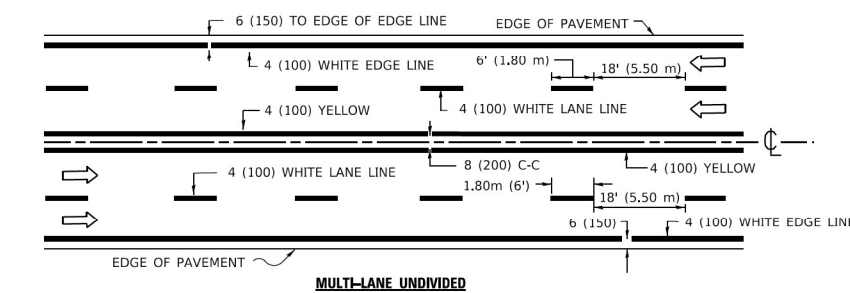


TYPICAL ISLAND MARKING

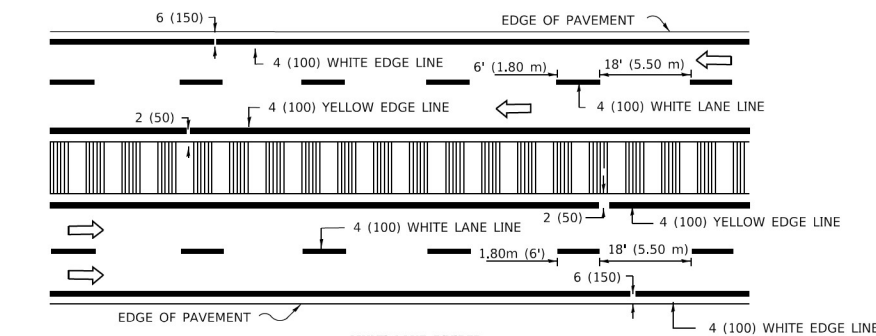
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



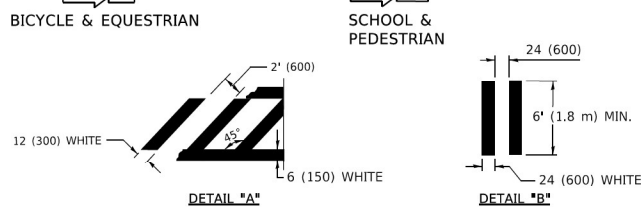
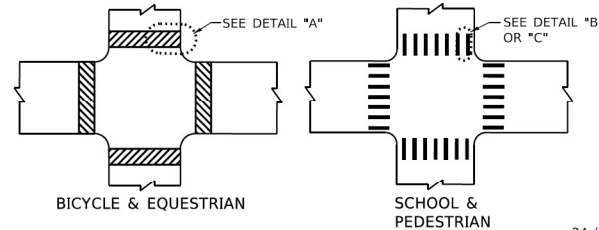
MULTI-LANE UNDIVIDED



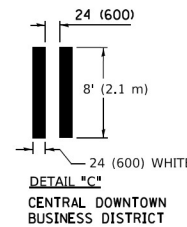
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

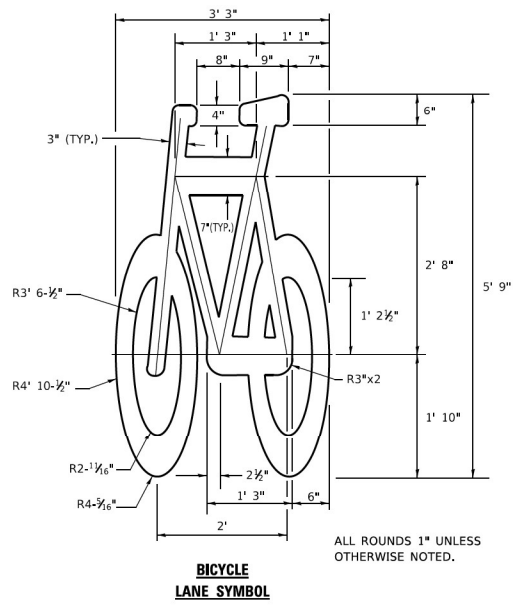
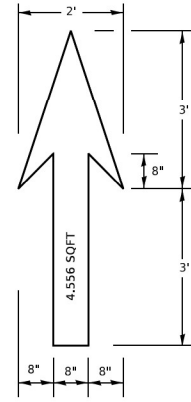


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

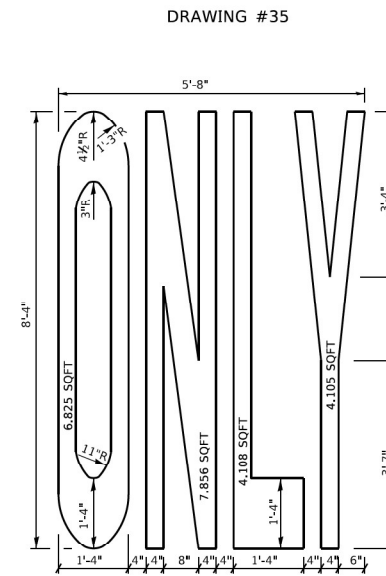
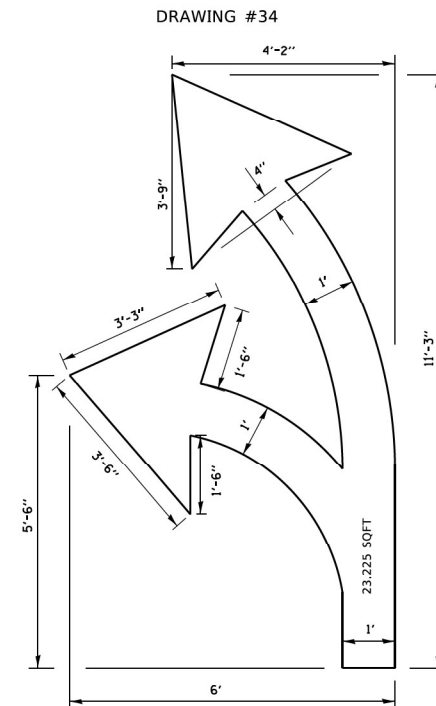
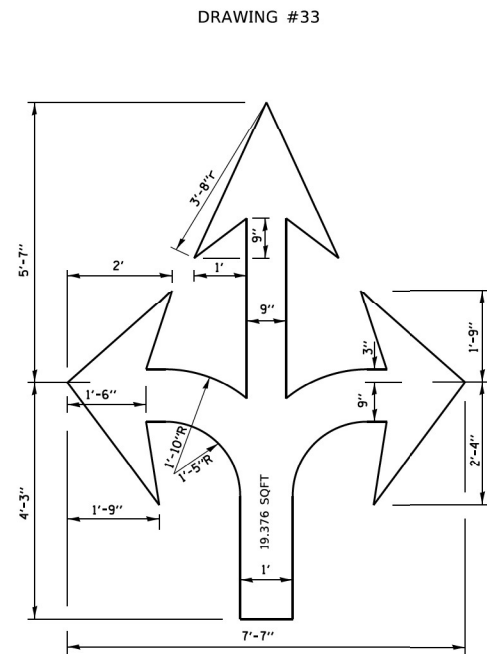
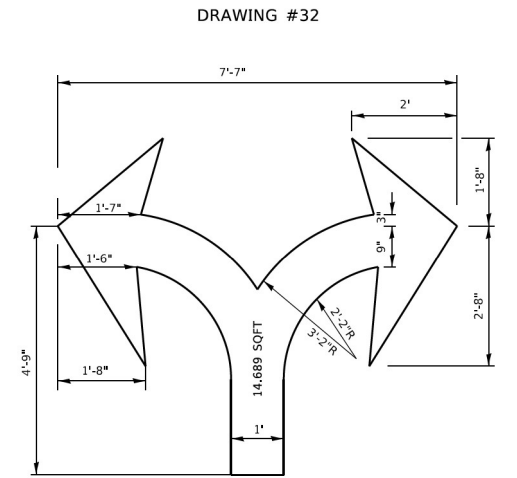
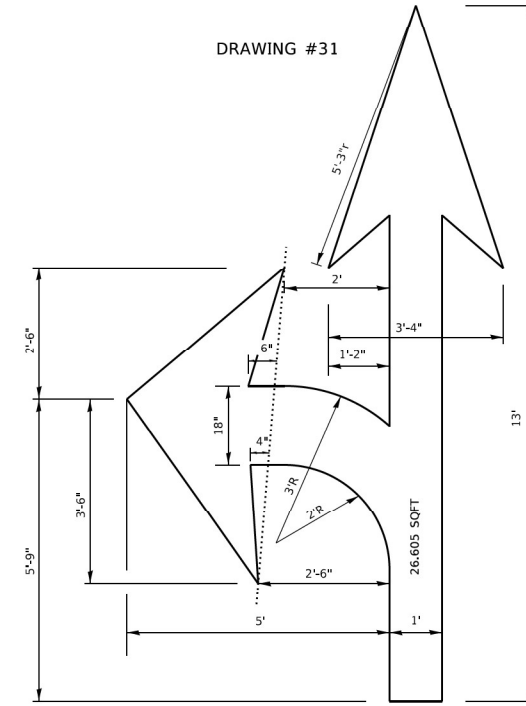
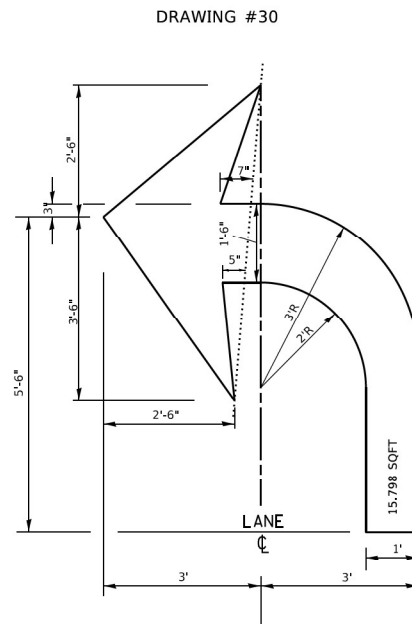
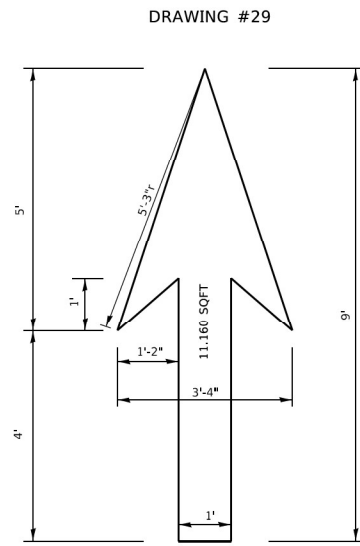
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	53
TC-24		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		



NOTE:

1. FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
2. THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS
DRAWING #28**



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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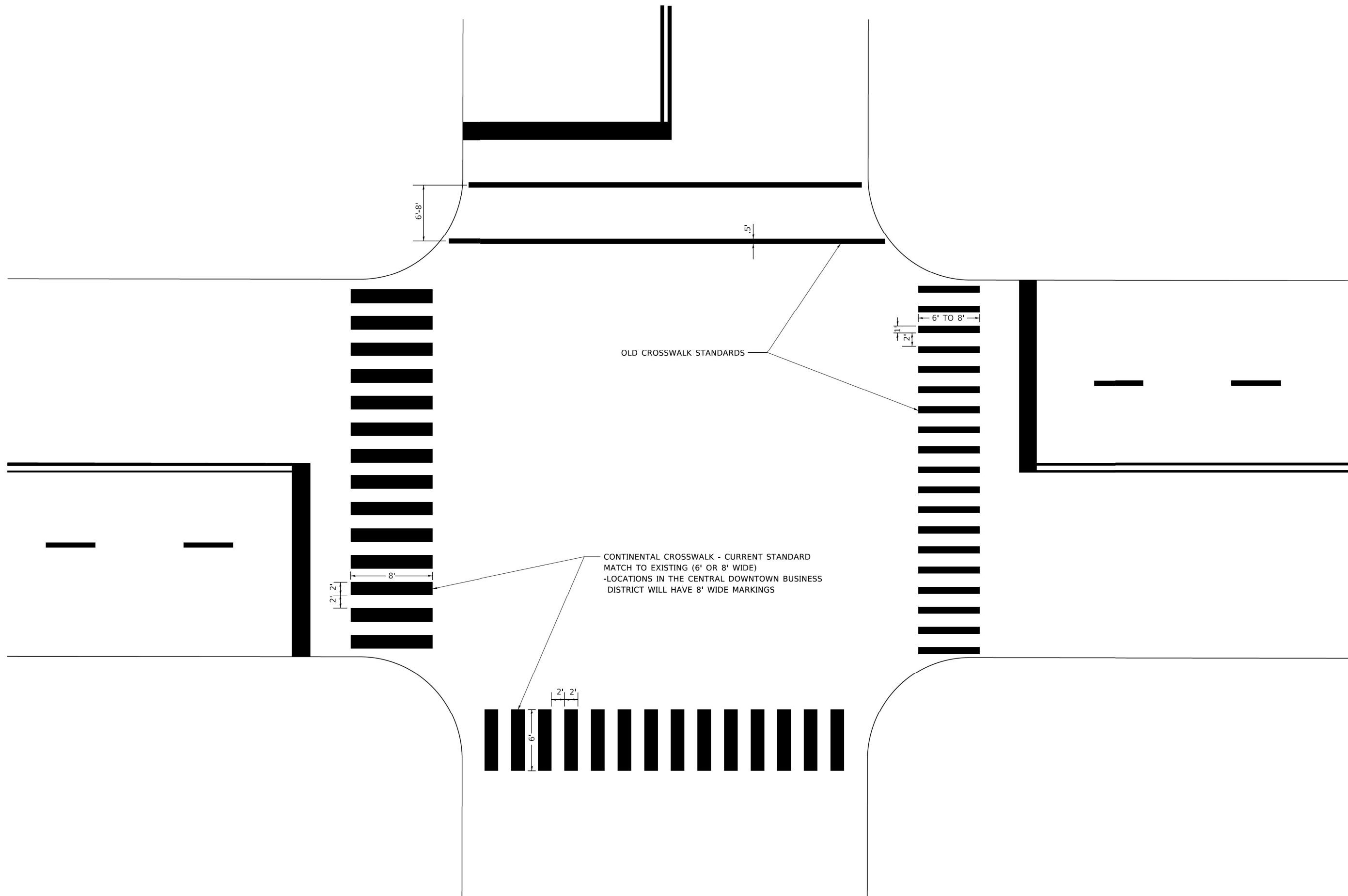
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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 2	OF 3 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	54
TC-24			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

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CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

OLD CROSSWALK STANDARDS

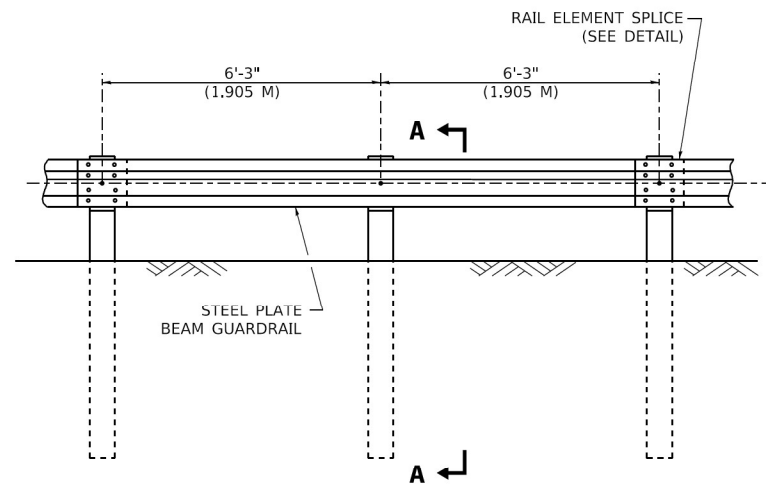
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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

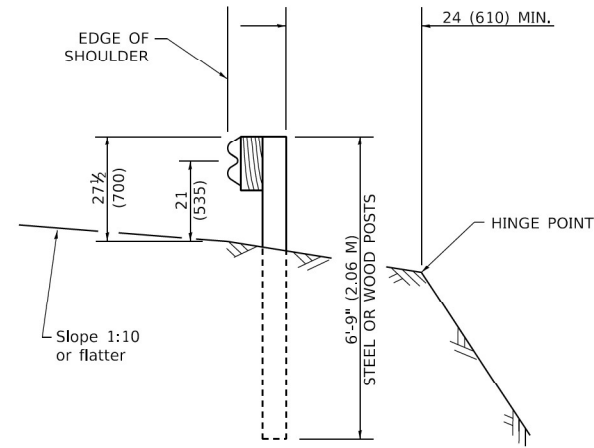
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			59	55
TC-24			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				



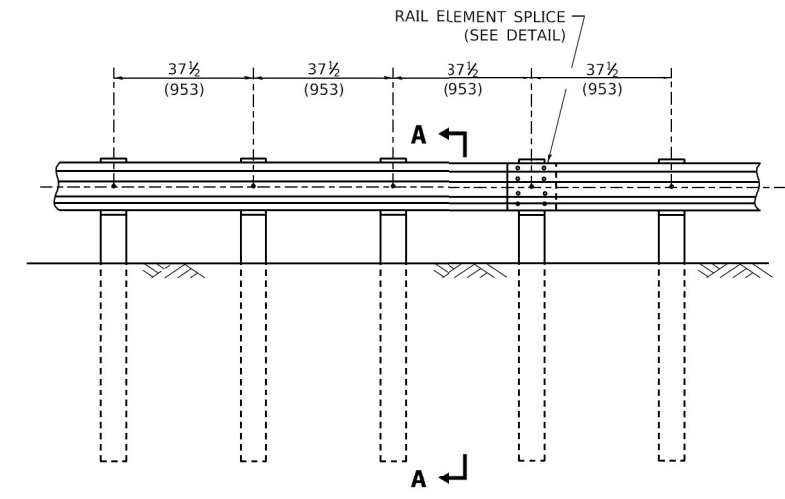
ELEVATION

TYPE A

6'-3" (1.905 M) TYPICAL POST SPACING



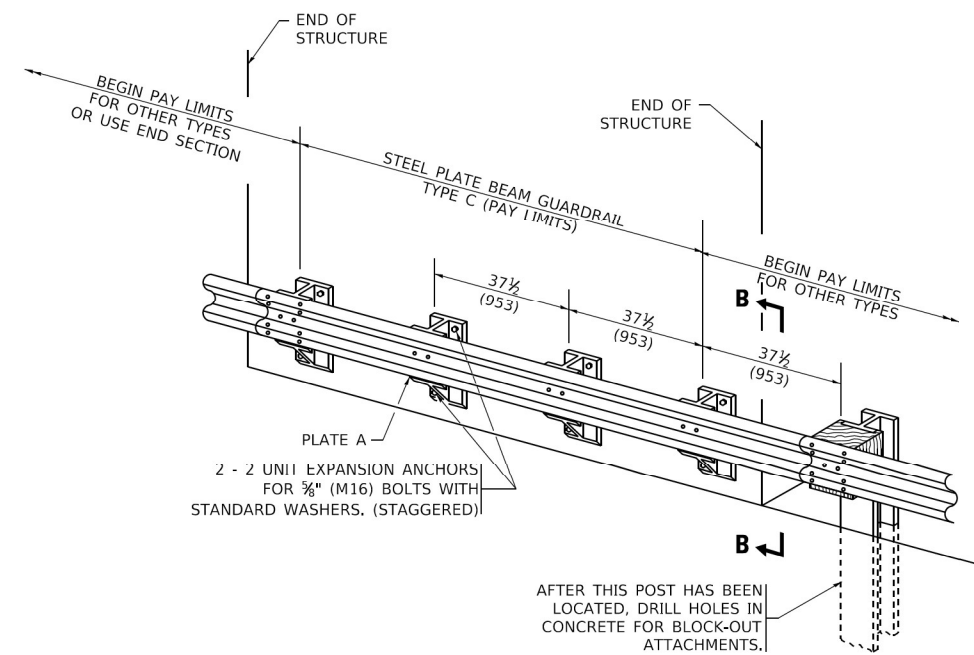
SECTION A-A



ELEVATION

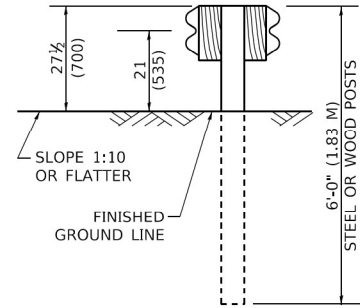
TYPE A

37 1/2 (953) CLOSED POST SPACING

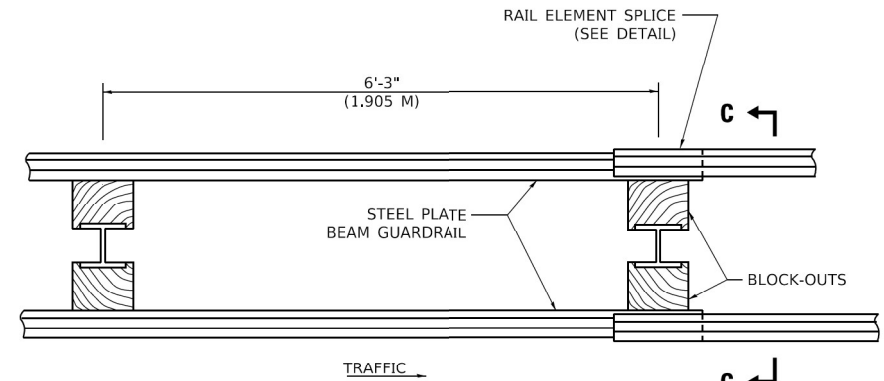


TYPE C

37 1/2 (953) BLOCK-OUT SPACING



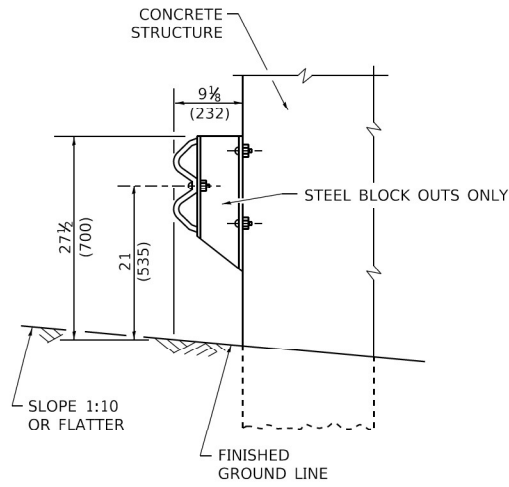
SECTION C-C



PLAN

TYPE D

DOUBLE STEEL PLATE BEAM GUARDRAIL
6'-3" (1.905 M) TYPICAL POST SPACING



SECTION B-B

GENERAL NOTES

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.

THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

MODEL: Default
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3/11/2019 2:13:09 PM User: d...
3/11/2019 2:13:09 PM User: d...

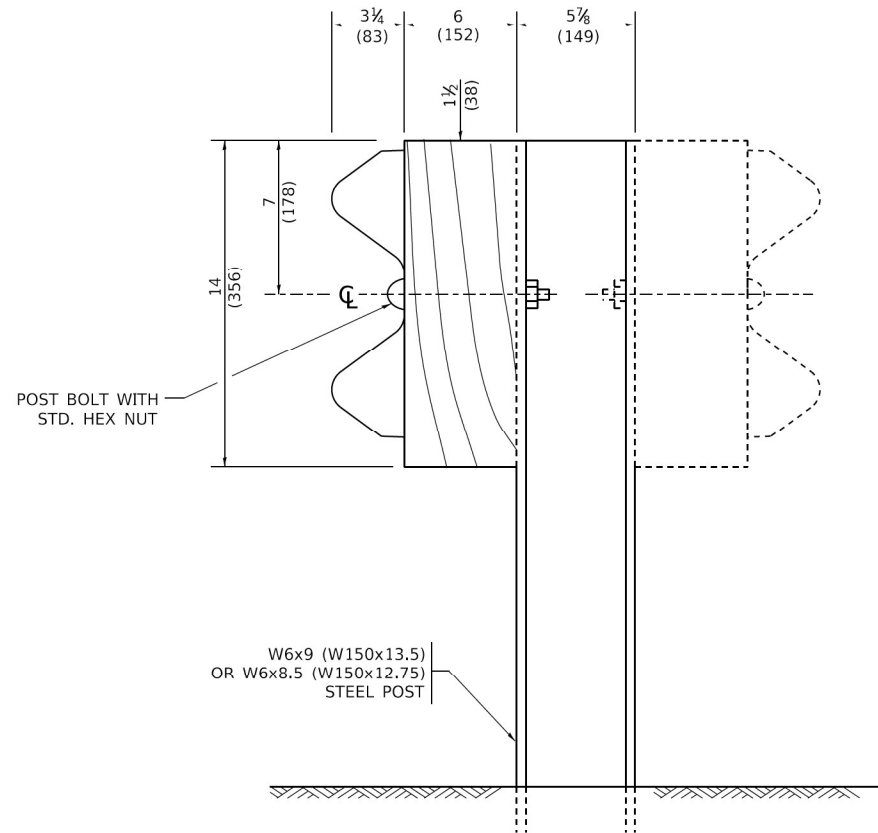
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PLOT SCALE = 50.0000 "/in.	DRAWN -	REVISED -
PLOT DATE = 3/11/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

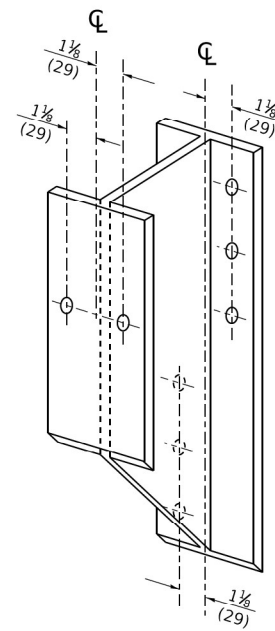
**REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET 1 OF 4 SHEETS STA. TO STA.

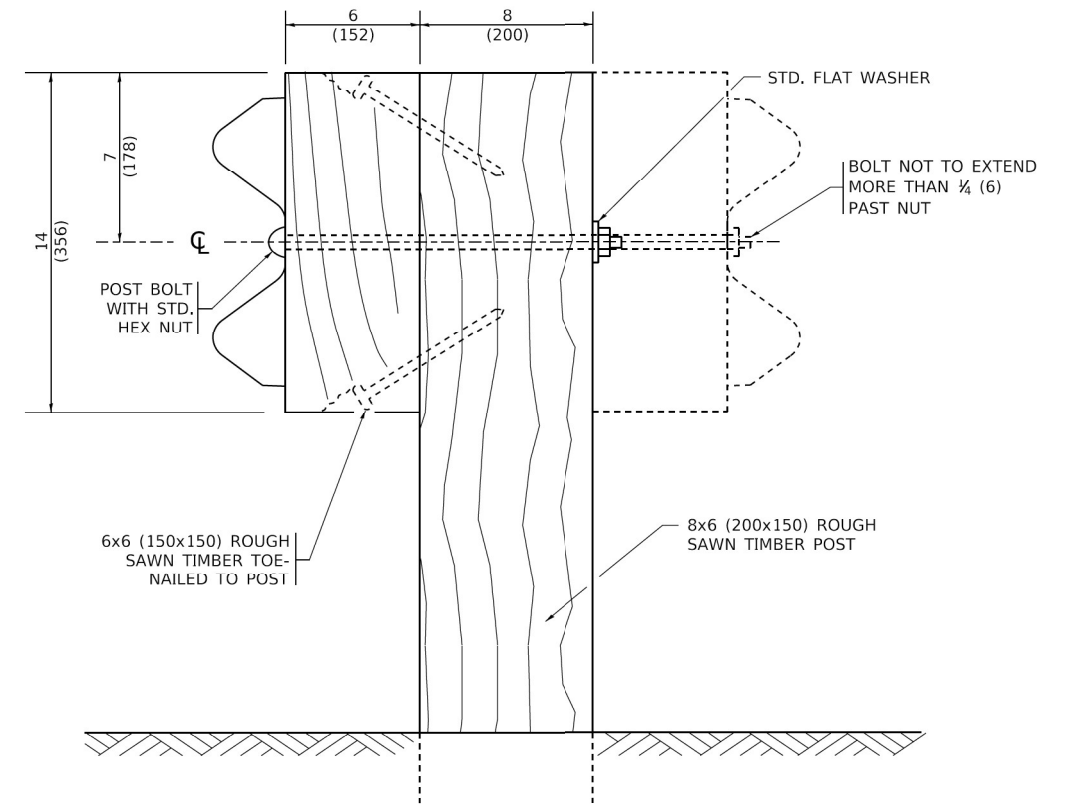
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	BM-21		59	56
CONTRACT NO.			ILLINOIS FED. AID PROJECT	



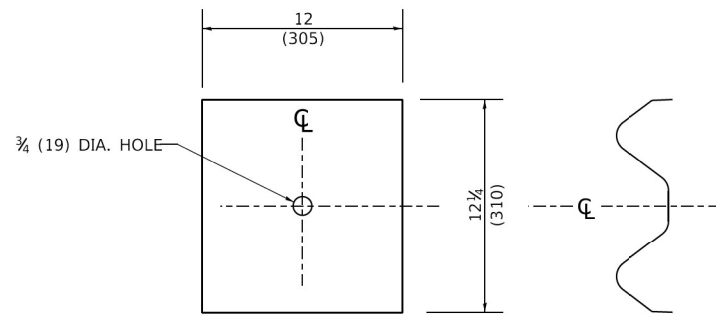
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



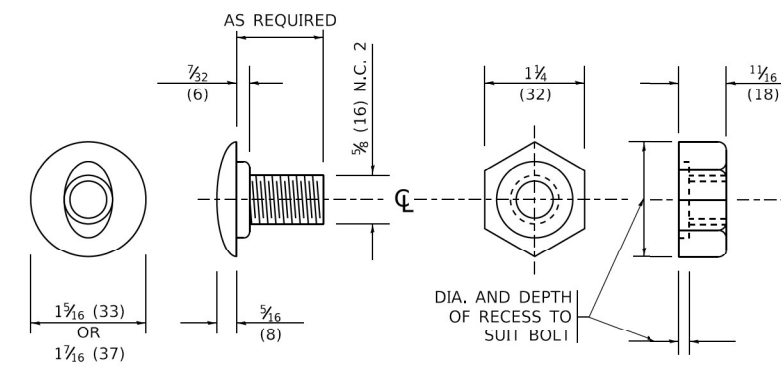
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



POST OR SPLICE BOLT & NUT

MODEL: Default
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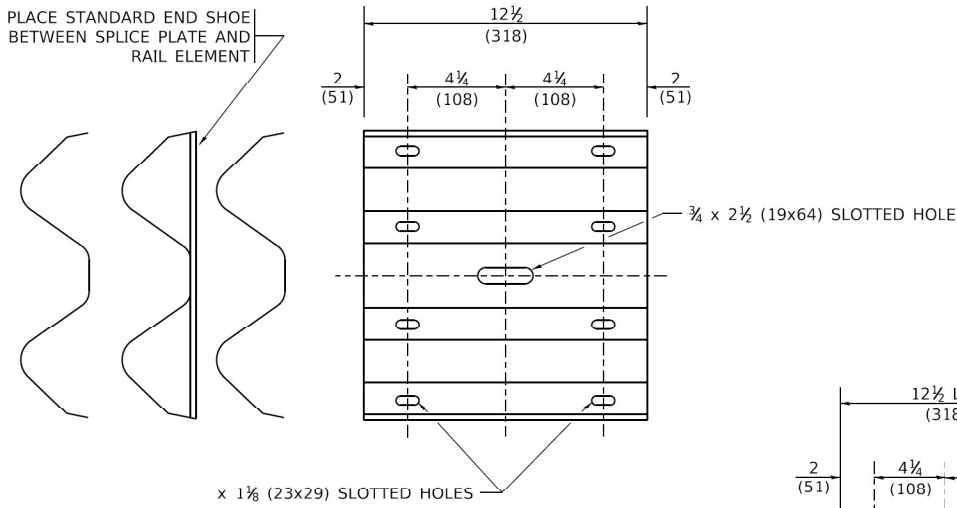
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PLOT SCALE = 50.0000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 3/11/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

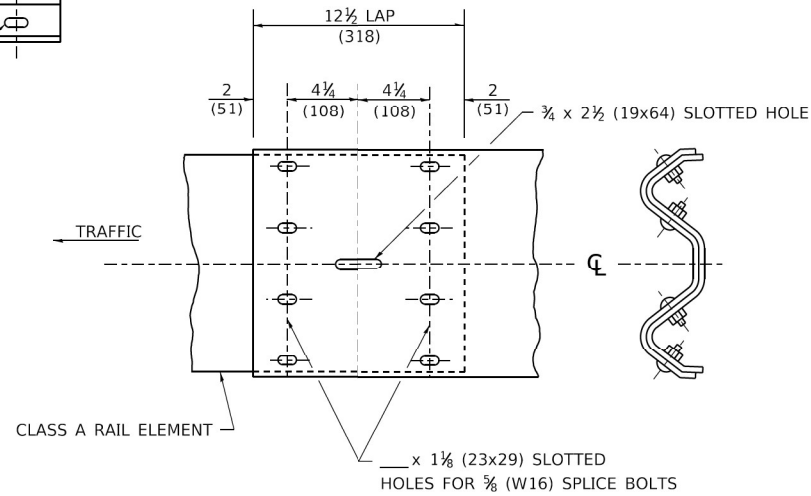
REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			
SCALE: NONE	SHEET 2 OF 4 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	57
BM-21			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

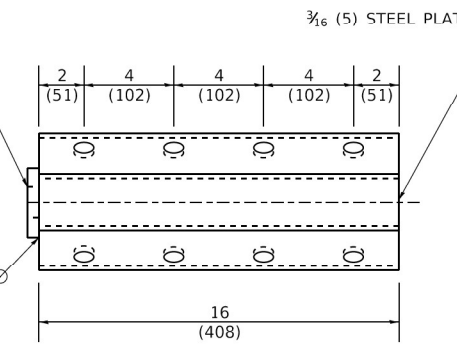
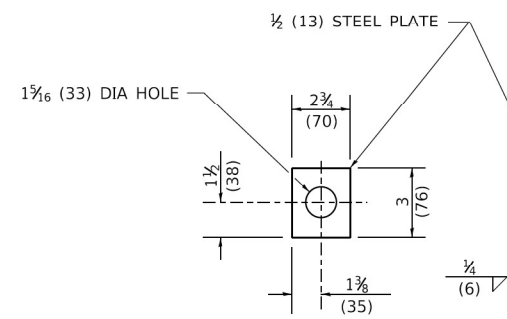
PLACE STANDARD END SHOE BETWEEN SPLICE PLATE AND RAIL ELEMENT



SPLICE PLATE

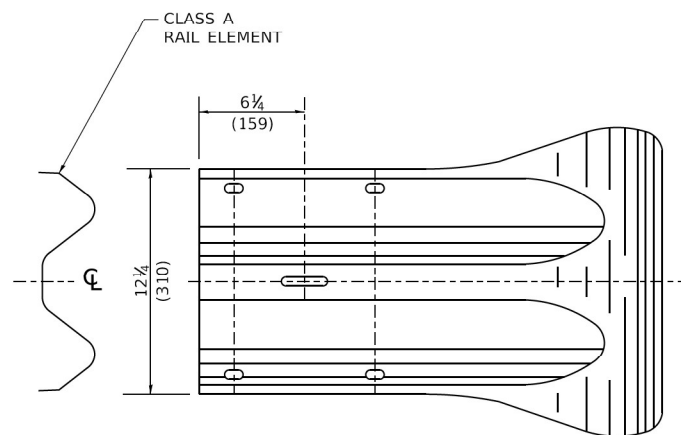
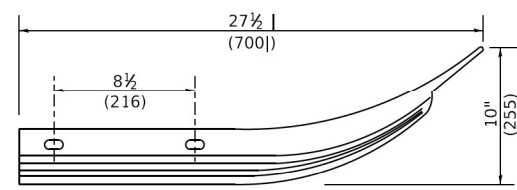
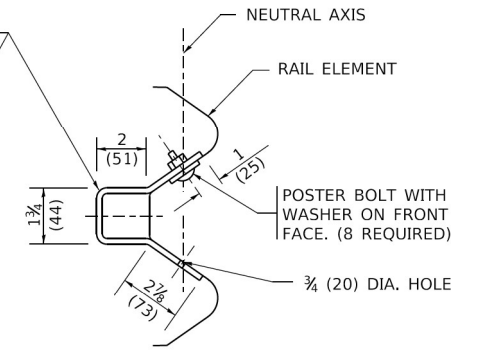


RAIL ELEMENT SPLICE

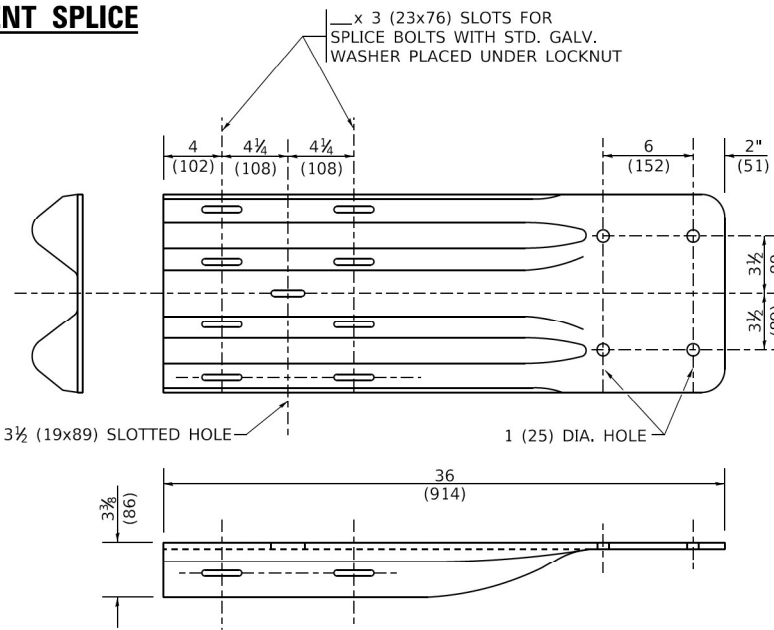


NOTE:
ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

ANCHORE PLATE T DETAILS



END SECTION

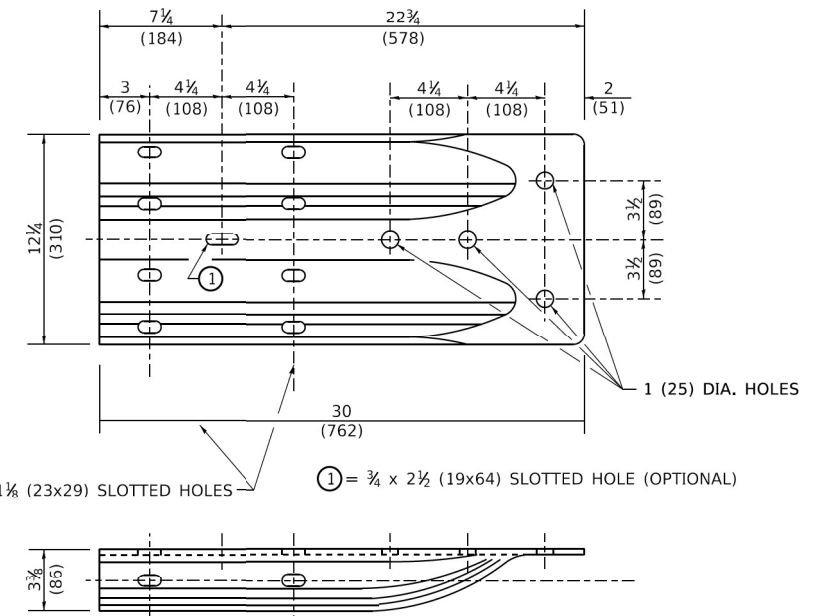


END SHOE

NOTE:
WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.



ALTERNATE END SHOE

MODEL: Default
FILE: \\media1.pw.org\pub\WIDOT\Documents\DOT Office\Bartlett_1\Projects\IDHS4272\2A\CAD\Drawn\CADsheets\bm21.dwg

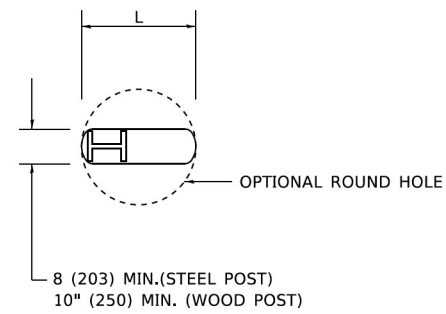
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	DATE -	REVISED -

**STATE OF ILLINOIS
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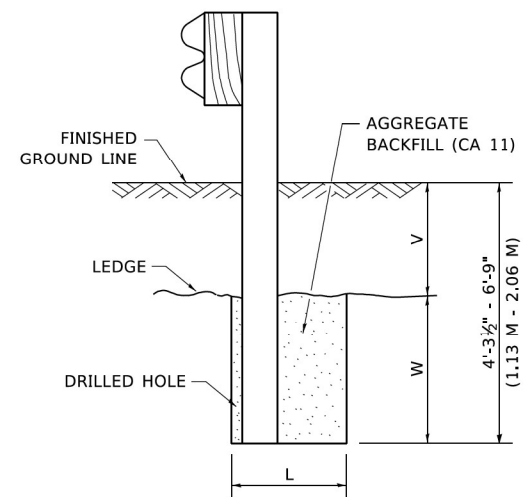
**REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET 3 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BM-21		59	58
ILLINOIS FED. AID PROJECT			CONTRACT NO.	



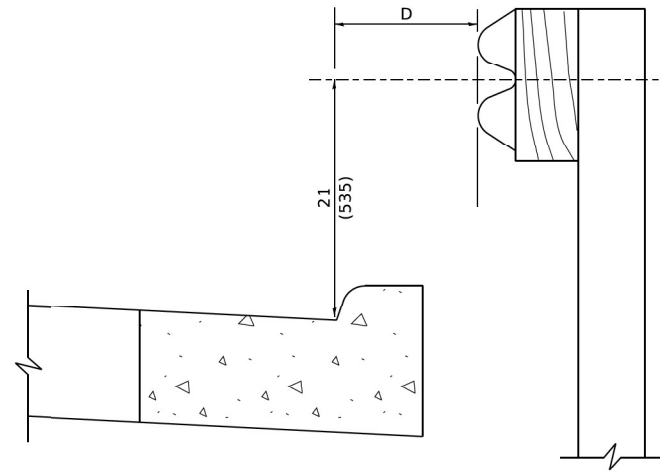
PLAN



NOTE:
LEDGE LINE IS TOP OF ROCK
LEDGE OR HARD SLAG FILL.

ELEVATION

**FOOTING FOR POST WHEN IMPERVIOUS
MATERIAL IS ENCOUNTERED**



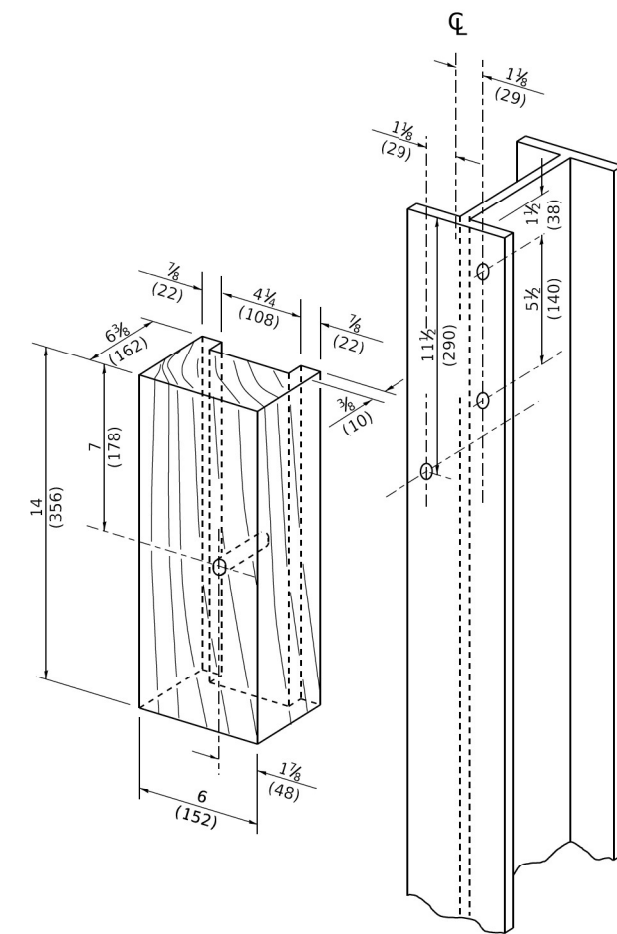
NOTE:

IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0" (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

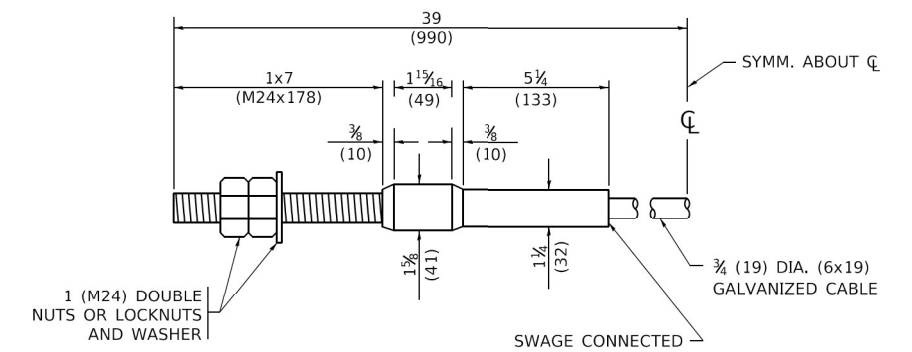
GUARDRAIL PLACED BEHIND CURB

(D = 0 DESIRABLE TO 12 (300) MAXIMUM)

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



**WOOD BLOCK - OUT AND
STEEL POST DETAILS**



CABLE ASSEMBLY

(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)
TIGHTEN TO TAUT TENSION

MODEL: Defaul
FILE: \\mtrc-pw\plgharcom.dct\hrcs\pwr\DOT\Documents\DOT_Coffice\Bartlett_1\Projects\11545272\24\CADD\BAY\CAD\sheet\bm21.dgn

USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50.0000 "/in.	DRAWN -	REVISED -
PLOT DATE = 3/11/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET 4 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			59	59
BM-21			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				