

| FAU | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|------|-----------|--------|--------------|-----------|
| 3578 | 1314 RS-3 | COOK | 23 | 1 |

CONTRACT 62673

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

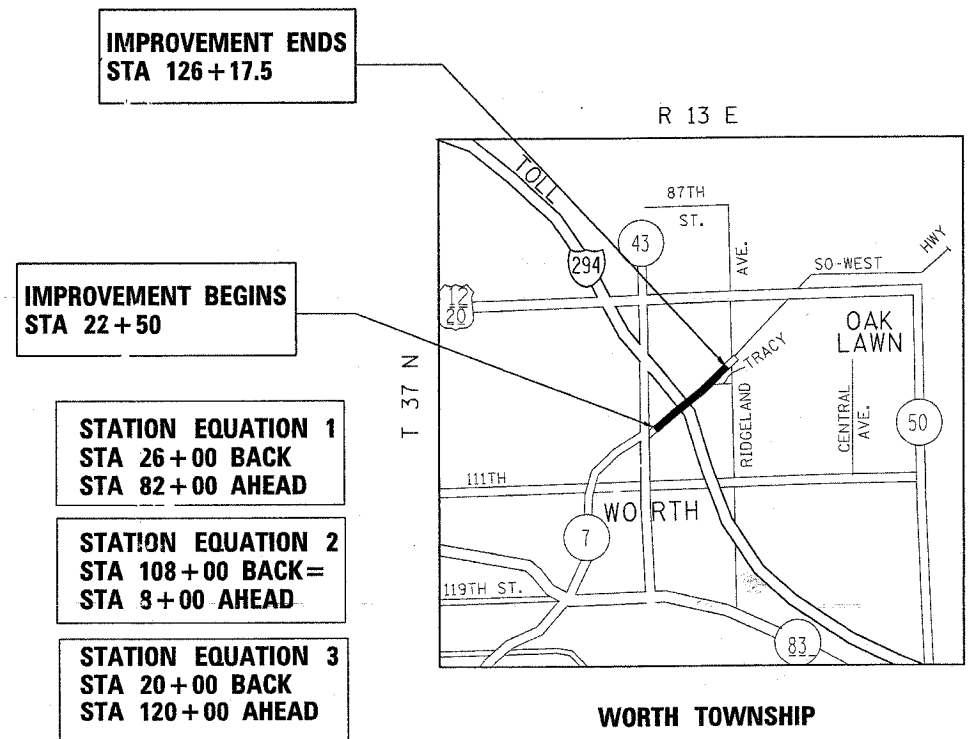
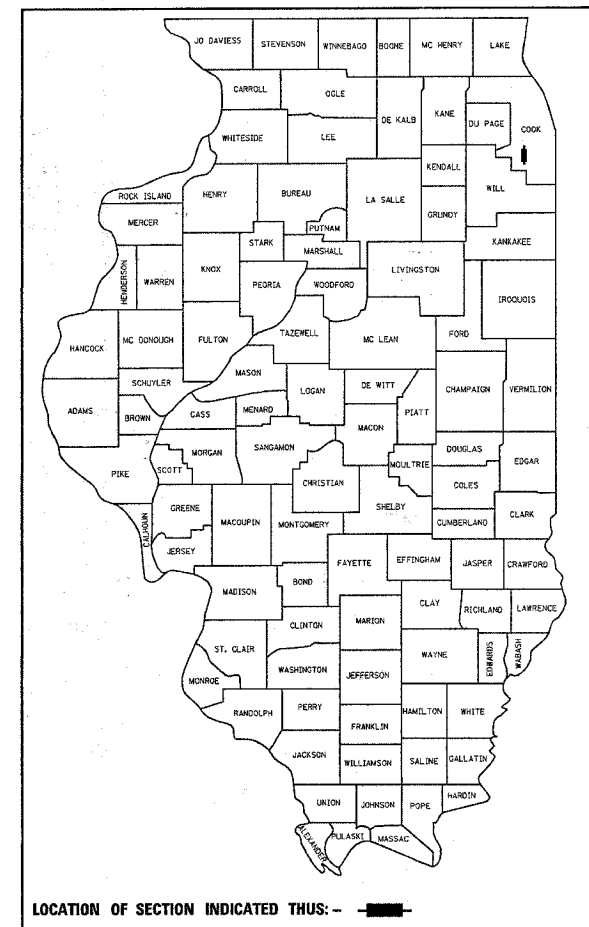
PLANS FOR PROPOSED HIGHWAY

FAU 3578: SOUTHWEST HWY
IL 43 (HARLEM AVE) TO RIDGELAND AVE
SECTION: 1314 RS-3
RESURFACING (MAINTENANCE)
COOK COUNTY
C-91-036-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE VILLAGES OF CHICAGO RIDGE AND WORTH

D-91-036-04



IMPROVEMENT ENDS
STA 126 + 17.5

IMPROVEMENT BEGINS
STA 22 + 50

STATION EQUATION 1
STA 26 + 00 BACK
STA 82 + 00 AHEAD

STATION EQUATION 2
STA 108 + 00 BACK =
STA 8 + 00 AHEAD

STATION EQUATION 3
STA 20 + 00 BACK
STA 120 + 00 AHEAD

TRAFFIC DATA
2002 ADT = 27400
POSTED SPEED LIMIT = 40 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

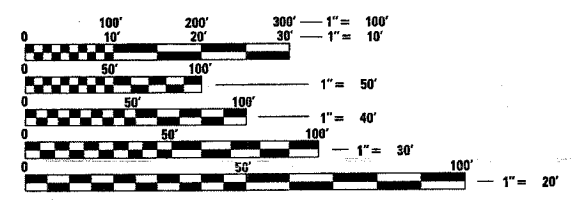
SUBMITTED March 29, 20 05
Dina O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13, 20 05
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 20 05
Victor Modesto
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GROSS AND NET LENGTH OF IMPROVEMENT = 4768 FEET = 0.90 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 62673

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER:
KEN ENG/JENPAI CHANG (847)705-4432

| | | | | |
|-----------------------|-----------|------------------|--------------|-----------|
| F. A. U. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |

CONTRACT 62673

INDEX OF SHEETS:

| SHEET NO. | DESCRIPTION: |
|-----------|---|
| 1 | TITLE SHEET |
| 2 | INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES |
| 3 | SUMMARY OF QUANTITIES |
| 4-7 | TYPICAL SECTIONS |
| 8-11 | ROADWAY AND PAVEMENT MARKINGS |
| 12-13 | DETECTOR LOOP REPLACEMENT PLANS |
| 14 | PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT |
| 15 | BUTT JOINTS AND BITUMINOUS TAPER |
| 16 | TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |
| 17 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) |
| 18 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS |
| 19 | TRAFFIC CONTROL & PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) |
| 20 | TEMPORARY INFORMATION SIGNING |
| 21 | CURB AND GUTTER REMOVAL AND REPLACEMENT |
| 22 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING |
| 23 | DISTRICT ONE DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING |

STATE STANDARDS:

| | |
|-----------|---|
| 000001-04 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 442201-01 | CLASS C & D PATCHES |
| 701101-01 | OFF ROAD OPERATIONS, MULTILANE, 15' to 24" FROM PAVEMENT EDGE |
| 701301-02 | LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS |
| 701426-02 | LANE CLOSURE MULTILANE, INTERMITTENT OR MOVING OPERATIONS |
| 701606-04 | URBAN LANE CLOSURE, MULTILANE 2-WAY WITH MOUNTABLE MEDIAN |
| 701701-04 | URBAN LANE CLOSURE MULTILANE INTERSECTION |
| 701801-03 | LANE CLOSURE MULTILANE 1-WAY OR 2-WAY CROSSWALK OR SIDEWALK CLOSURE |
| 702001-05 | TRAFFIC CONTROL DEVICES |
| 886001 | DETECTOR LOOP INSTALLATION |
| 886006 | TYPICAL LAYOUT FOR DETECTION LOOPS |

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ANY CULVERTS THAT NEED TO BE CLEANED WILL BE PAID FOR AS STORM SEWERS TO BE CLEANED.

WORK ZONE PUBLIC INFORMATION SIGNS SHALL BE ERECTED.

MIXTURE REQUIREMENTS

| MIXTURE TYPE | PG TYPE | VOIDS | MAX RAP % |
|---|------------------|----------------|-----------|
| POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 | SBS/SBR PG 70-22 | 4% @ 90 GYR. | 0 |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50 | SBS/SBR PG 76-28 | 2.5% @ 50 GYR. | 0 |
| BITUMINOUS REPLACEMENT OVER PATCHES | PG 64-22 | 4% @ 70 GYR. | 15 |
| CLASS D PATCHES | PG 64-22 | 4% @ 70 GYR. | 15 |

NOTE: 112 LBS/SY-IN WAS USED AS THE CONVERSION FOR ALL BITUMINOUS CONCRETE IN QUANTITY CALCULATIONS

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

**SOUTHWEST HWY
INDEX OF SHEETS, STATE STANDARDS,
AND GENERAL NOTES**

VERT.
SCALE: HORIZ.
DATE 3/31/2005

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| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | |
|-----------------------|---|-------|------------------------|---------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN I000-2A | | | | |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 11 | 11 | | | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 55 | 55 | | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 3 | 3 | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 1 | 1 | | | | |
| 40600980 | BITUMINOUS SURFACE REMOVAL - BUTT JOINT | SQ YD | 510 | 510 | | | | |
| 40601000 | BITUMINOUS REPLACEMENT OVER PATCHES | TON | 260 | 260 | | | | |
| 44000008 | BITUMINOUS SURFACE REMOVAL 2 1/2" | SQ YD | 27513 | 27513 | | | | |
| 44000110 | BITUMINOUS REMOVAL OVER PATCHES 2 1/2" | SQ YD | 1800 | 1800 | | | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 80 | 80 | | | | |
| 44004610 | SIDEWALK REMOVAL AND REPLACEMENT (SPECIAL) | SQ FT | 500 | 500 | | | | |
| 44201765 | CLASS D PATCHES, TYPE II, 10 INCH | SQ YD | 240 | 240 | | | | |
| 44201769 | CLASS D PATCHES, TYPE III, 10 INCH | SQ YD | 370 | 370 | | | | |
| 44201771 | CLASS D PATCHES, TYPE IV, 10 INCH | SQ YD | 680 | 680 | | | | |
| 50300300 | PROTECTIVE COAT | SQ YD | 123 | 123 | | | | |
| 55039700 | STORM SEWERS TO BE CLEANED | FOOT | 1500 | 1500 | | | | |
| 60300105 | FRAMES AND GRATES TO BE ADJUSTED | EACH | 5 | 5 | | | | |
| 60404940 | FRAMES AND GRATES, TYPE 23 | EACH | 3 | 3 | | | | |
| 60404950 | FRAMES AND GRATES, TYPE 24 | EACH | 5 | 5 | | | | |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 30 | 30 | | | | |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 50 | 50 | | | | |
| 67000200 | ENGINEER'S FIELD OFFICE, TYPE A | EACH | 2 | 2 | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | |
| 70102630 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701601 | L SUM | 1 | 1 | | | | |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | 1 | | | | |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 | 1 | | | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 4680 | 4680 | | | | |
| 70300210 | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 115 | 115 | | | | |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | |
|-----------------------|---|-------|------------------------|---------------|--|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN I000-2A | | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 10770 | 10770 | | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 400 | 400 | | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 500 | 500 | | | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 86 | 86 | | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 520 | 520 | | | | |
| *78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 115 | 115 | | | | |
| *78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 10770 | 10770 | | | | |
| *78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 400 | 400 | | | | |
| *78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 500 | 500 | | | | |
| *78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 86 | 86 | | | | |
| *78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 489 | 489 | | | | |
| *78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 100 | 100 | | | | |
| *88600500 | DETECTOR LOOP REMOVAL | FOOT | 426 | 426 | | | | |
| *88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 426 | 426 | | | | |
| X0322256 | TEMPORARY INFORMATION SIGNING | SQ FT | 51.4 | 51.4 | | | | |
| X0656100 | DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT | SQ YD | 232 | 232 | | | | |
| X4066548 | POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 | TON | 2696 | 2696 | | | | |
| X4067100 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50 | TON | 1156 | 1156 | | | | |
| XX003560 | CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 500 | 500 | | | | |
| Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 12 | 12 | | | | |
| Z0018600 | DRAINAGE STRUCTURES TO BE RECONSTRUCTED | EACH | 3 | 3 | | | | |

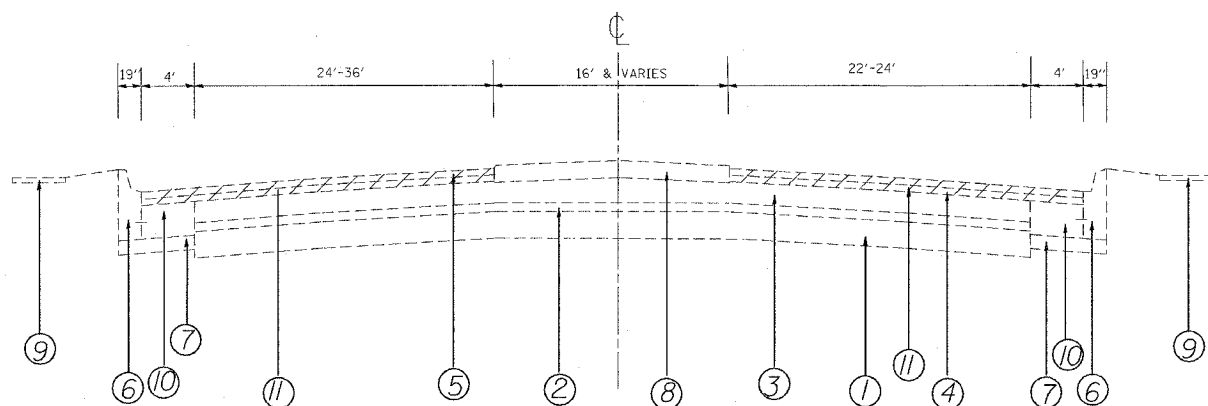
* SPECIALITY ITEM

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
SOUTHWEST HIGHWAY

| | | | | |
|--------------------------------|-----------|------------------|--------------|-----------|
| F.A.D. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 4 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS | | FED. AID PROJECT | | |

CONTRACT 62673



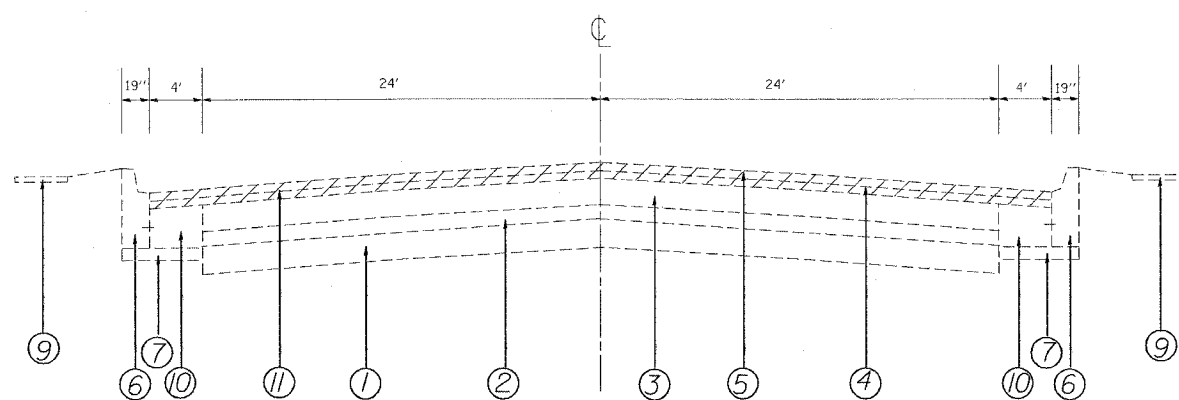
EXISTING TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 22+50 TO STA 83+40

NOTE: B-6.12 CURB & GUTTER ENDS AT STA 83+46
B-6.24 CURB & GUTTER BEGINS AT STA 83+46

LEGEND

- ① EXISTING PCC PAVEMENT (9'-7")
- ② EXISTING BITUMINOUS RESURFACING +- 3"
- ③ EXISTING BITUMINOUS AGGREGATE MIXTURE (6" OVERLAY)
- ④ EXISTING BITUMINOUS CONCRETE BINDER COURSE (1 1/2")
- ⑤ EXISTING BITUMINOUS CONCRETE SURFACE COURSE CLASS 1 (1 1/2")
- ⑥ EXISTING COMBINATION CURB AND GUTTER TYPE B-6J2
- ⑦ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A (6")
- ⑧ EXISTING CORRUGATED MEDIAN
- ⑨ EXISTING PCC SIDEWALK
- ⑩ EXISTING PCC BASE COURSE WIDENING (9")
- ⑪ PROPOSED BITUMINOUS SURFACE REMOVAL, (2 1/2")
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-475, N50 (3/4")
- ⑬ PROPOSED POLYMERIZED BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 (1 3/4")



EXISTING TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 83+40 TO STA 84+08

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
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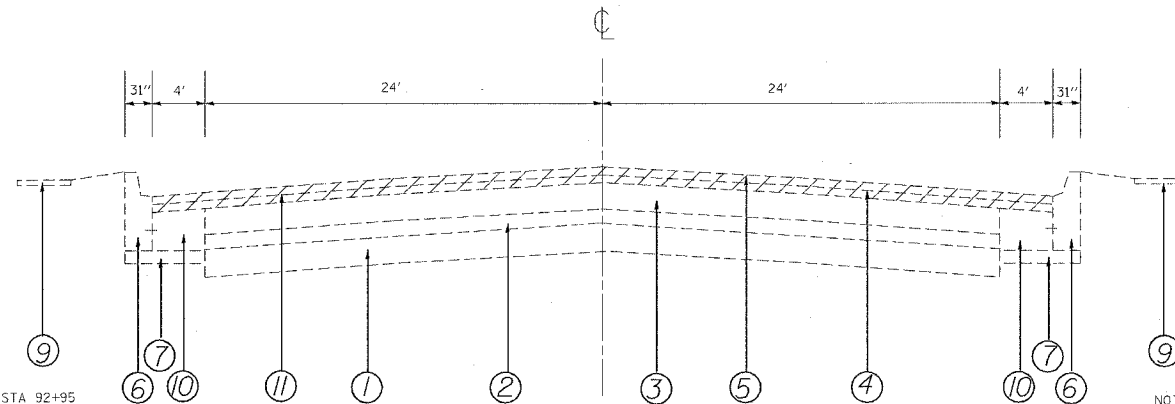
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| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 5 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | | |

CONTRACT 62673



NOTE: PCC SIDEWALK ENDS AT STA 92+95
PCC SIDEWALK BEGINS AT STA 96+09
PCC SIDEWALK ENDS AT STA 98+80
PCC SIDEWALK BEGINS AT STA 14+10
PCC SIDEWALK ENDS AT STA 15+35

EXISTING TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 84+08 TO STA 126+17.5

NOTE: PCC SIDEWALK ENDS AT STA 84+25
PCC SIDEWALK BEGINS AT STA 105+46
PCC SIDEWALK ENDS AT STA 106+36
PCC SIDEWALK BEGINS AT STA 107+56
PCC SIDEWALK ENDS AT STA 11+08
PCC SIDEWALK BEGINS AT STA 125+38
PCC SIDEWALK ENDS AT STA 126+17.5

LEGEND

- ① EXISTING PCC PAVEMENT (9'-7")
- ② EXISTING BITUMINOUS RESURFACING + 3"
- ③ EXISTING BITUMINOUS AGGREGATE MIXTURE (6" OVERLAY)
- ④ EXISTING BITUMINOUS CONCRETE BINDER COURSE (1 1/2")
- ⑤ EXISTING BITUMINOUS CONCRETE SURFACE COURSE CLASS 1 (1 1/2")
- ⑥ EXISTING COMBINATION CURB AND GUTTER TYPE B-6.24
- ⑦ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A (6")
- ⑧ EXISTING CORRUGATED MEDIAN
- ⑨ EXISTING PCC SIDEWALK
- ⑩ EXISTING PCC BASE COURSE WIDENING (9')
- ⑪ PROPOSED BITUMINOUS SURFACE REMOVAL, (2 1/2")
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-475, N50 (3/4")
- ⑬ PROPOSED POLYMERIZED BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 (1 3/4")

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ILLINOIS DEPARTMENT OF TRANSPORTATION

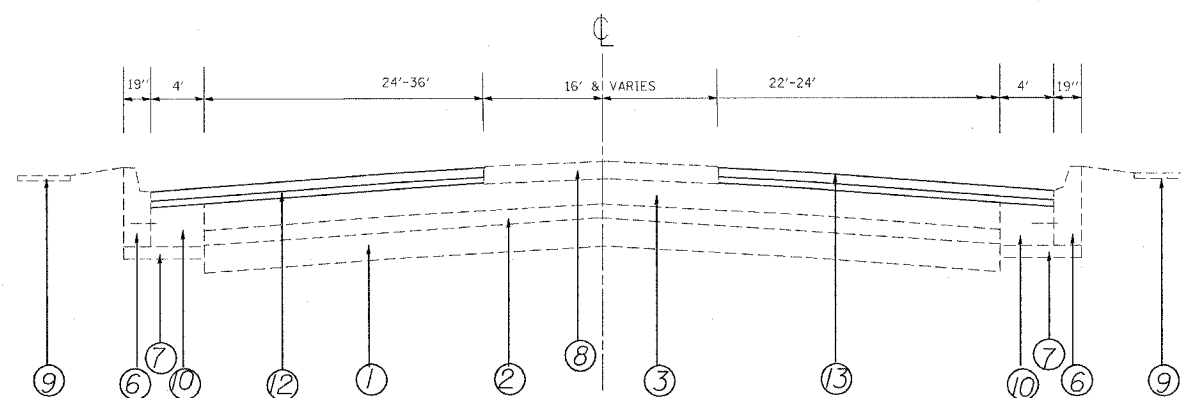
TYPICAL SECTIONS

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| F.A.U. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 6 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

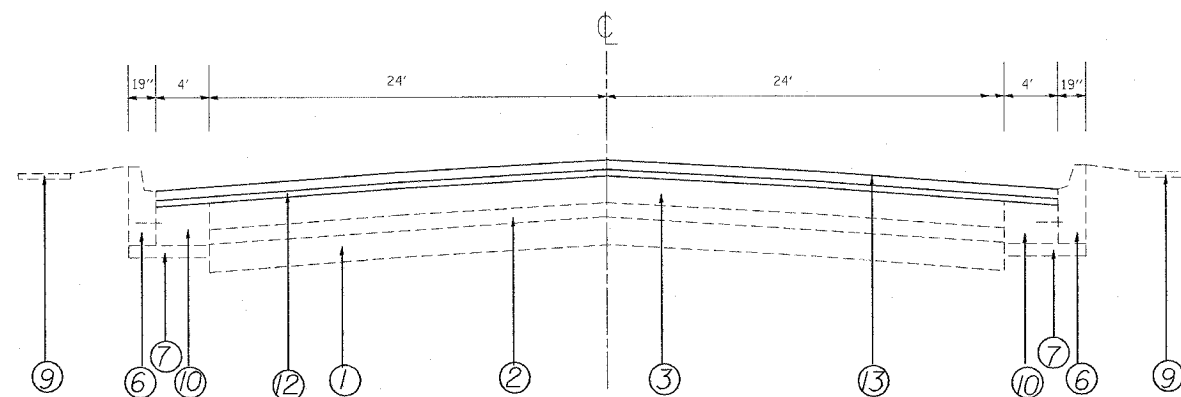
CONTRACT 62673



PROPOSED TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 22+50 TO STA 83+40

NOTE: B-6.12 CURB & GUTTER ENDS AT STA 83+46
B-6.24 CURB & GUTTER BEGINS AT STA 83+46



PROPOSED TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 83+40 TO STA 84+08

LEGEND

- ① EXISTING PCC PAVEMENT (9'-7")
- ② EXISTING BITUMINOUS RESURFACING +- 3"
- ③ EXISTING BITUMINOUS AGGREGATE MIXTURE (6" OVERLAY)
- ④ EXISTING BITUMINOUS CONCRETE BINDER COURSE (1 1/2")
- ⑤ EXISTING BITUMINOUS CONCRETE SURFACE COURSE CLASS I (1 1/2")
- ⑥ EXISTING COMBINATION CURB AND GUTTER TYPE B-6.24
- ⑦ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A (6")
- ⑧ EXISTING CORRUGATED MEDIAN
- ⑨ EXISTING PCC SIDEWALK
- ⑩ EXISTING PCC BASE COURSE WIDENING (9")
- ⑪ PROPOSED BITUMINOUS SURFACE REMOVAL (2 1/2")
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-475, N50 (3/4")
- ⑬ PROPOSED POLYMERIZED BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 (1 3/4")

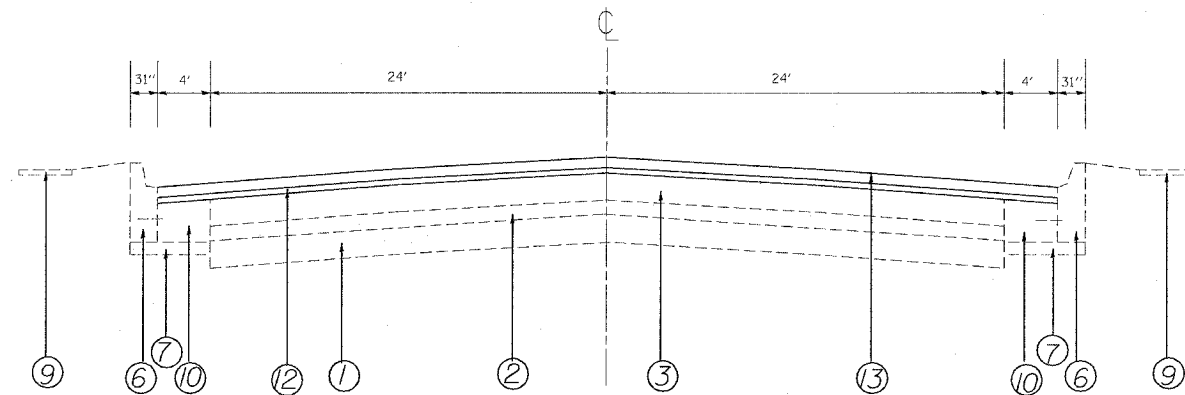
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SCALE: VERT. NTS
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 7 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT 62673



NOTE: PCC SIDEWALK ENDS AT STA 92+95
PCC SIDEWALK BEGINS AT STA 96+09
PCC SIDEWALK ENDS AT STA 98+80
PCC SIDEWALK BEGINS AT STA 14+10
PCC SIDEWALK ENDS AT STA 15+35

PROPOSED TYPICAL CROSS-SECTION
SOUTHWEST HWY

STA 84+08 TO STA 126+17.5

NOTE: PCC SIDEWALK ENDS AT STA 84+25
PCC SIDEWALK BEGINS AT STA 105+46
PCC SIDEWALK ENDS AT STA 106+36
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PCC SIDEWALK ENDS AT STA 126+17.5

LEGEND

- ① EXISTING PCC PAVEMENT (9'-7")
- ② EXISTING BITUMINOUS RESURFACING + 3"
- ③ EXISTING BITUMINOUS AGGREGATE MIXTURE (6" OVERLAY)
- ④ EXISTING BITUMINOUS CONCRETE BINDER COURSE (1/2")
- ⑤ EXISTING BITUMINOUS CONCRETE SURFACE COURSE CLASS 1 (1/2")
- ⑥ EXISTING COMBINATION CURB AND GUTTER TYPE B-6.24
- ⑦ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A (6")
- ⑧ EXISTING CORRUGATED MEDIAN
- ⑨ EXISTING PCC SIDEWALK
- ⑩ EXISTING PCC BASE COURSE WIDENING (9")
- ⑪ PROPOSED BITUMINOUS SURFACE REMOVAL (2 1/2")
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-475, N50 (3/4")
- ⑬ PROPOSED POLYMERIZED BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 (1 3/4")

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ILLINOIS DEPARTMENT OF TRANSPORTATION

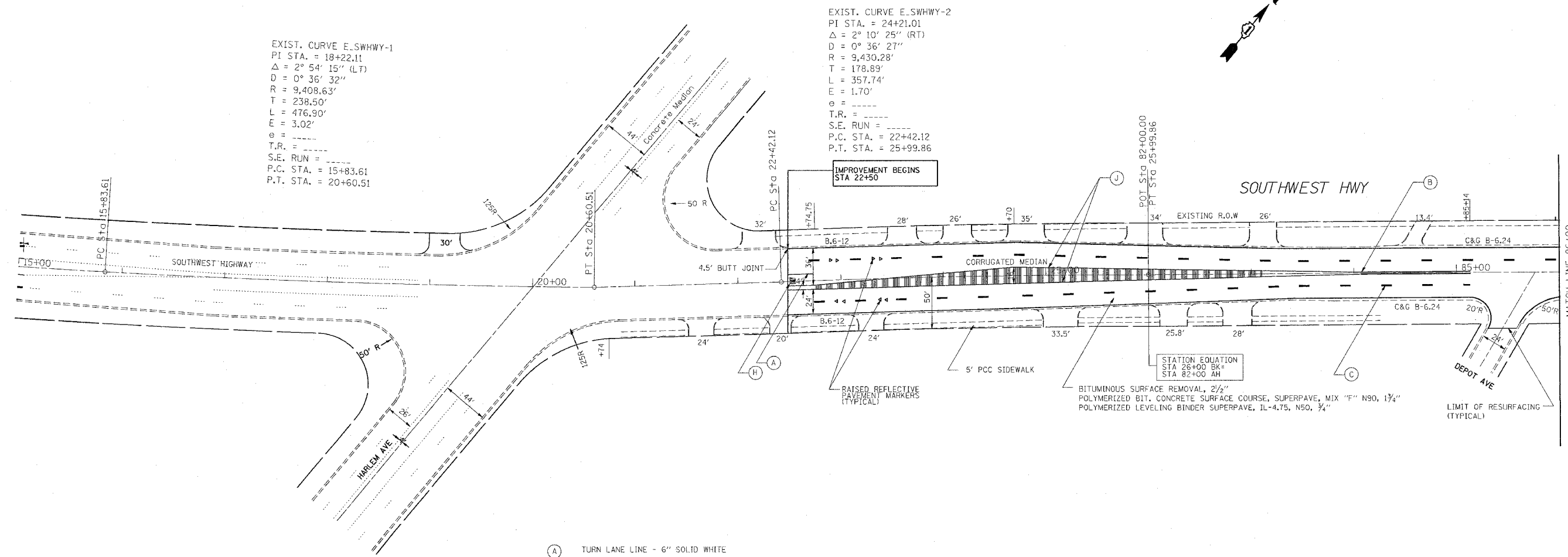
TYPICAL SECTIONS

SCALE: VERT. NTS
HORIZ.
DATE 3/28/2005

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|--------------------------|-----------|---|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 8 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |
| CONTRACT 62673 | | | | |



- (A) TURN LANE LINE - 6" SOLID WHITE
- (B) CENTERLINE - 4" SOLID YELLOW (2 @ 11" C-C)
- (C) SKIP-DASH LANE LINE - 4" WHITE (10' DASH & 30' SKIP)
- (D) FIRE LANE - 6" SOLID WHITE (RT. 45 DIAGONALS, 4' C-C)
- (E) STOP & TRANSVERSE LINES 24" SOLID WHITE
- (F) CROSSWALK LINE - 6" SOLID WHITE (2 @ 6' C-C)
- (G) DOTTED LANE LINE - 6" WHITE (2' LINE, 6' SPACE)
- (H) LETTERS & SYMBOLS, 8" WHITE (LEFT & RIGHT TURN ONLY)
- (I) 12" SOLID YELLOW (RT. 45 DIAGONALS)
- (J) MEDIAN EDGE LINE - 4" SOLID YELLOW

NOTE: PAVEMENT MARKINGS (TYPICAL FOR ALL SHEETS)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | SOUTHWEST HWY PROPOSED ROADWAY & PAVEMENT MARKING PLANS |
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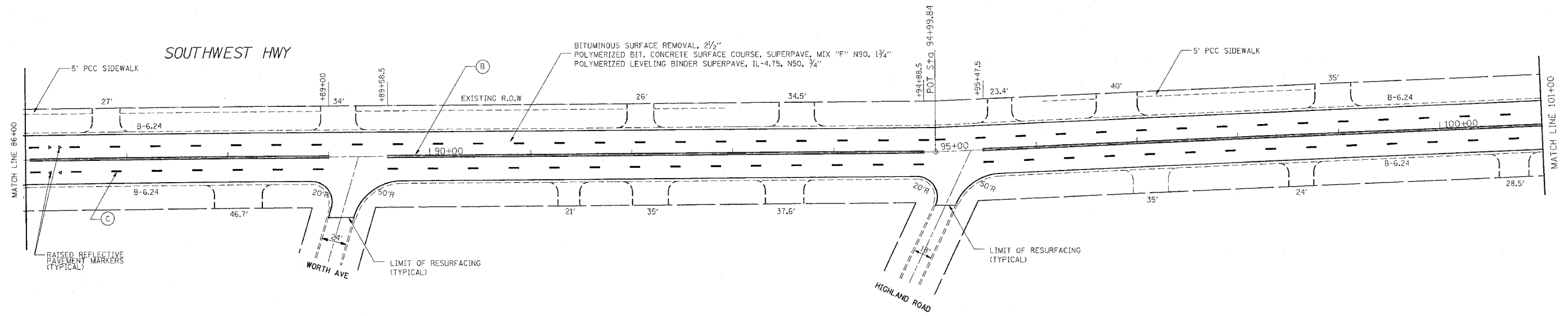
SCALE: VERT. 1" = 50'
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 DATE 3/30/2005

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 9 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT 62673 | | | | |



NOTE: PAVEMENT MARKINGS (TYPICAL FOR ALL SHEETS)

- (A) TURN LANE LINE - 6" SOLID WHITE
- (B) CENTERLINE - 4" SOLID YELLOW (2 @ 11" C-C)
- (C) SKIP-DASH LANE LINE - 4" WHITE (10' DASH & 30' SKIP)
- (D) FIRE LANE - 6" SOLID WHITE (RT. 45 DIAGONALS, 4' C-C)
- (E) STOP & TRANSVERSE LINES 24" SOLID WHITE
- (F) CROSSWALK LINE - 6" SOLID WHITE (2 @ 6' C-C)
- (G) DOTTED LANE LINE - 6" WHITE (2' LINE, 6' SPACE)
- (H) LETTERS & SYMBOLS, 8" WHITE (LEFT & RIGHT TURN ONLY)
- (I) 12" SOLID YELLOW (RT. 45 DIAGONALS)
- (J) MEDIAN EDGE LINE - 4" SOLID YELLOW

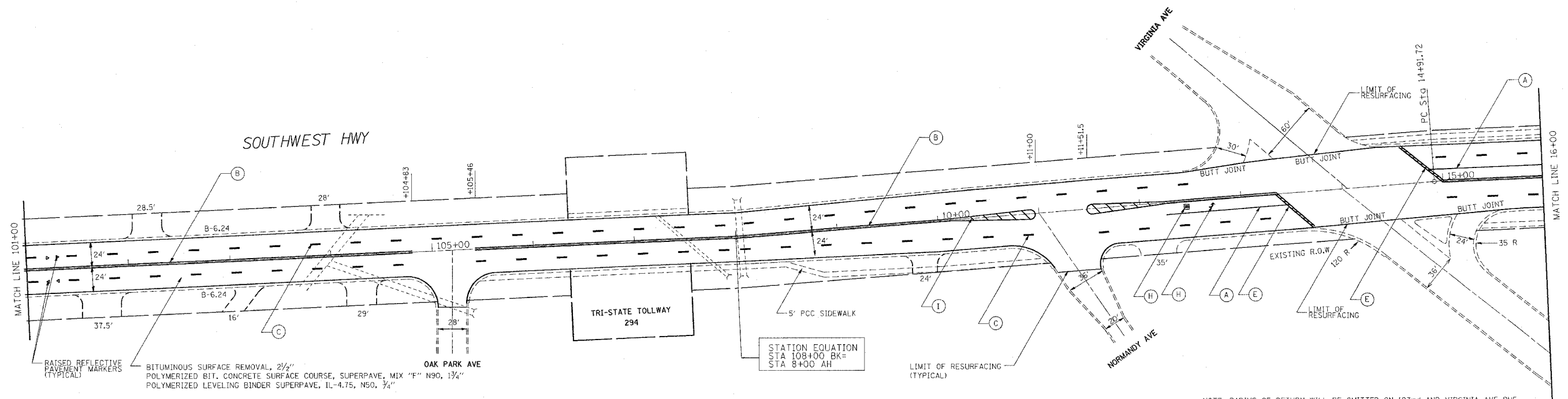
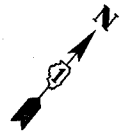
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|----------------------|------|--|
| NAME | DATE | |
| | | SOUTHWEST HWY PROPOSED ROADWAY & PAVEMENT MARKING PLANS |
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| SCALE: VERT. 1"= 50' | | DRAWN BY |
| DATE 3/28/2005 | | |

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 10 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT 62673 | | | | |



RAISED REFLECTIVE PAVEMENT MARKERS (TYPICAL)

BITUMINOUS SURFACE REMOVAL, 2 1/2"
POLYMERIZED BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F" N90, 1 3/4"
POLYMERIZED LEVELING BINDER SUPERPAVE, IL-4.75, N50, 3/4"

OAK PARK AVE

STATION EQUATION
STA 108+00 BK=
STA 8+00 AH

NOTE: RADIUS OF RETURN WILL BE OMITTED ON 103-r-d AND VIRGINIA AVE DUE TO RECENT RECONSTRUCTION

NOTE: PAVEMENT MARKINGS (TYPICAL FOR ALL SHEETS)

- (A) TURN LANE LINE - 6" SOLID WHITE
- (B) CENTERLINE - 4" SOLID YELLOW (2 @ 11" C-C)
- (C) SKIP-DASH LANE LINE - 4" WHITE (10' DASH & 30' SKIP)
- (D) FIRE LANE - 6" SOLID WHITE (RT. 45 DIAGONALS, 4' C-C)
- (E) STOP & TRANSVERSE LINES 24" SOLID WHITE
- (F) CROSSWALK LINE - 6" SOLID WHITE (2 @ 6' C-C)
- (G) DOTTED LANE LINE - 6" WHITE (2' LINE, 6' SPACE)
- (H) LETTERS & SYMBOLS, 8" WHITE (LEFT & RIGHT TURN ONLY)
- (I) 12" SOLID YELLOW (RT. 45 DIAGONALS)
- (J) MEDIAN EDGE LINE - 4" SOLID YELLOW

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

SOUTHWEST HWY

PROPOSED ROADWAY & PAVEMENT MARKING PLANS

SCALE: VERT. 1"= 50'
HORIZ. DATE 3/28/2005

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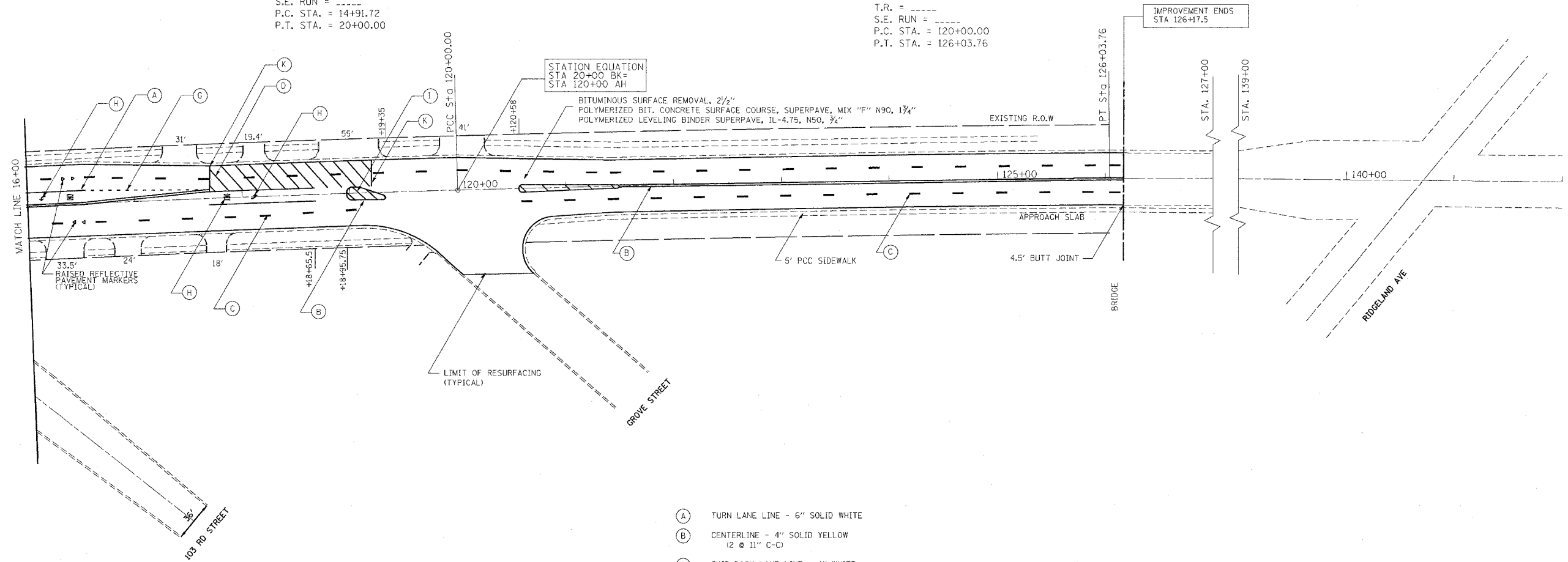
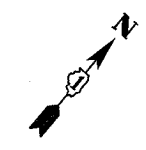
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------|--------|--------------|-----------|
| 3578 | 1314 RS-3 | COOK | 23 | 11 |
| STA. _____ TO STA. _____ | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT 62673

EXIST. CURVE E.SWHY-3-1
 PI STA. = 17+45.87
 $\Delta = 1^\circ 05' 05''$ (RT)
 $D = 0^\circ 12' 48''$
 $R = 26,849.16'$
 $T = 254.15'$
 $L = 508.28'$
 $E = 1.20'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 14+91.72$
 $P.T. STA. = 20+00.00$

EXIST. CURVE E.SWHY-3-2
 PI STA. = 123+01.89
 $\Delta = 1^\circ 17' 18''$ (RT)
 $D = 0^\circ 12' 48''$
 $R = 26,849.16'$
 $T = 301.89'$
 $L = 603.76'$
 $E = 1.70'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 120+00.00$
 $P.T. STA. = 126+03.76$



NOTE: PAVEMENT MARKINGS (TYPICAL FOR ALL SHEETS)

- (A) TURN LANE LINE - 6" SOLID WHITE
- (B) CENTERLINE - 4" SOLID YELLOW (2 @ 11" C-C)
- (C) SKIP-DASH LANE LINE - 4" WHITE (10' DASH & 30' SKIP)
- (D) FIRE LANE - 6" SOLID WHITE (RT. 45 DIAGONALS, 4' C-C)
- (E) STOP & TRANSVERSE LINES 24" SOLID WHITE
- (F) CROSSWALK LINE - 6" SOLID WHITE (2 @ 6' C-C)
- (G) DOTTED LANE LINE - 6" WHITE (2' LINE, 6' SPACE)
- (H) LETTERS & SYMBOLS, 8" WHITE (LEFT & RIGHT TURN ONLY)
- (I) 12" SOLID YELLOW (RT. 45 DIAGONALS)
- (J) MEDIAN EDGE LINE - 4" SOLID YELLOW
- (K) TRANSVERSE LINE, 12" SOLID WHITE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

SOUTHWEST HWY

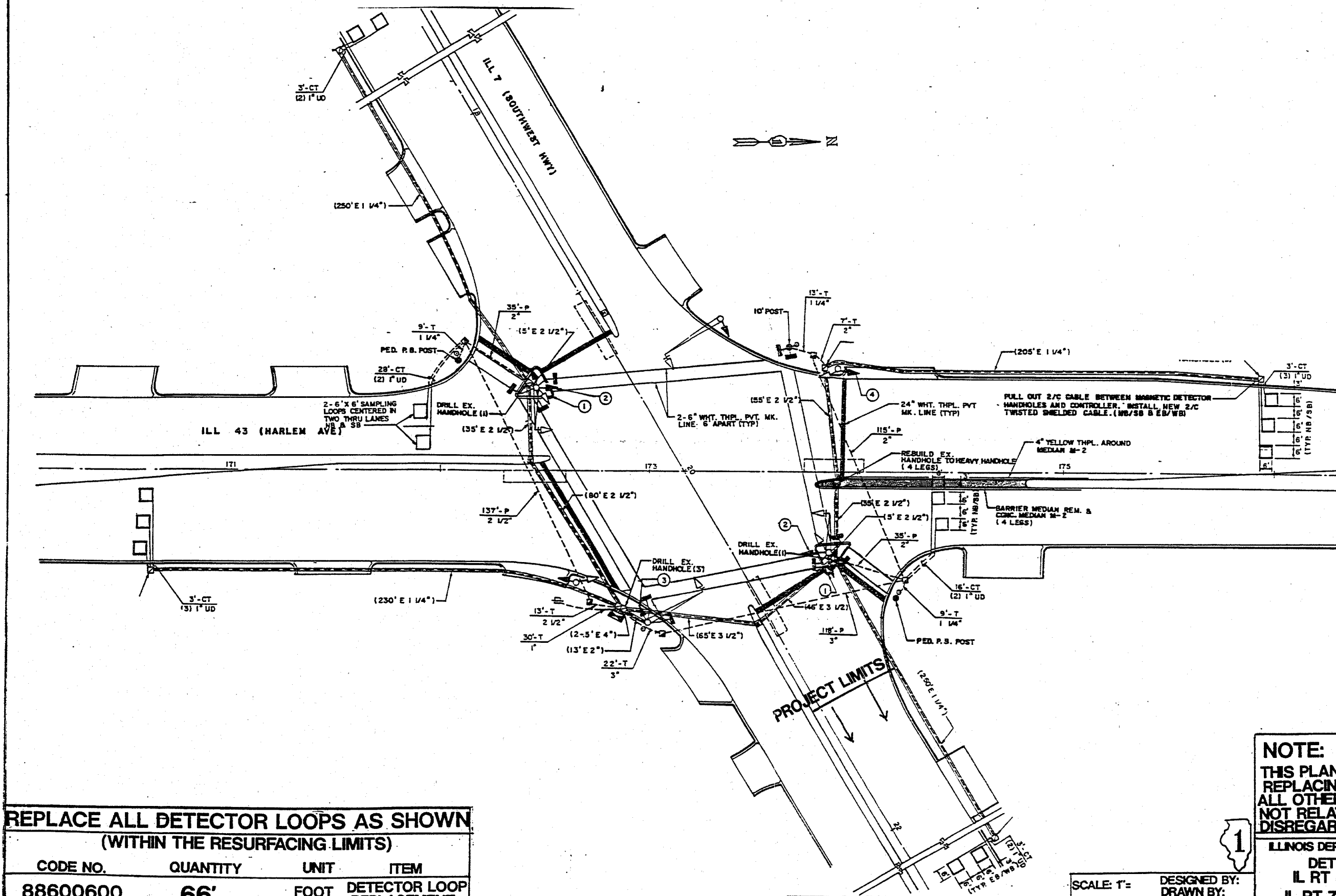
PROPOSED ROADWAY & PAVEMENT MARKING PLANS

SCALE: VERT. 1"= 50'
 HORIZ. DATE 3/28/2005

DRAWN BY
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|-----------------------|-----------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 12 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |

CONTRACT 62673



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 88600600 | 66' | FOOT | DETECTOR LOOP REPLACEMENT |

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

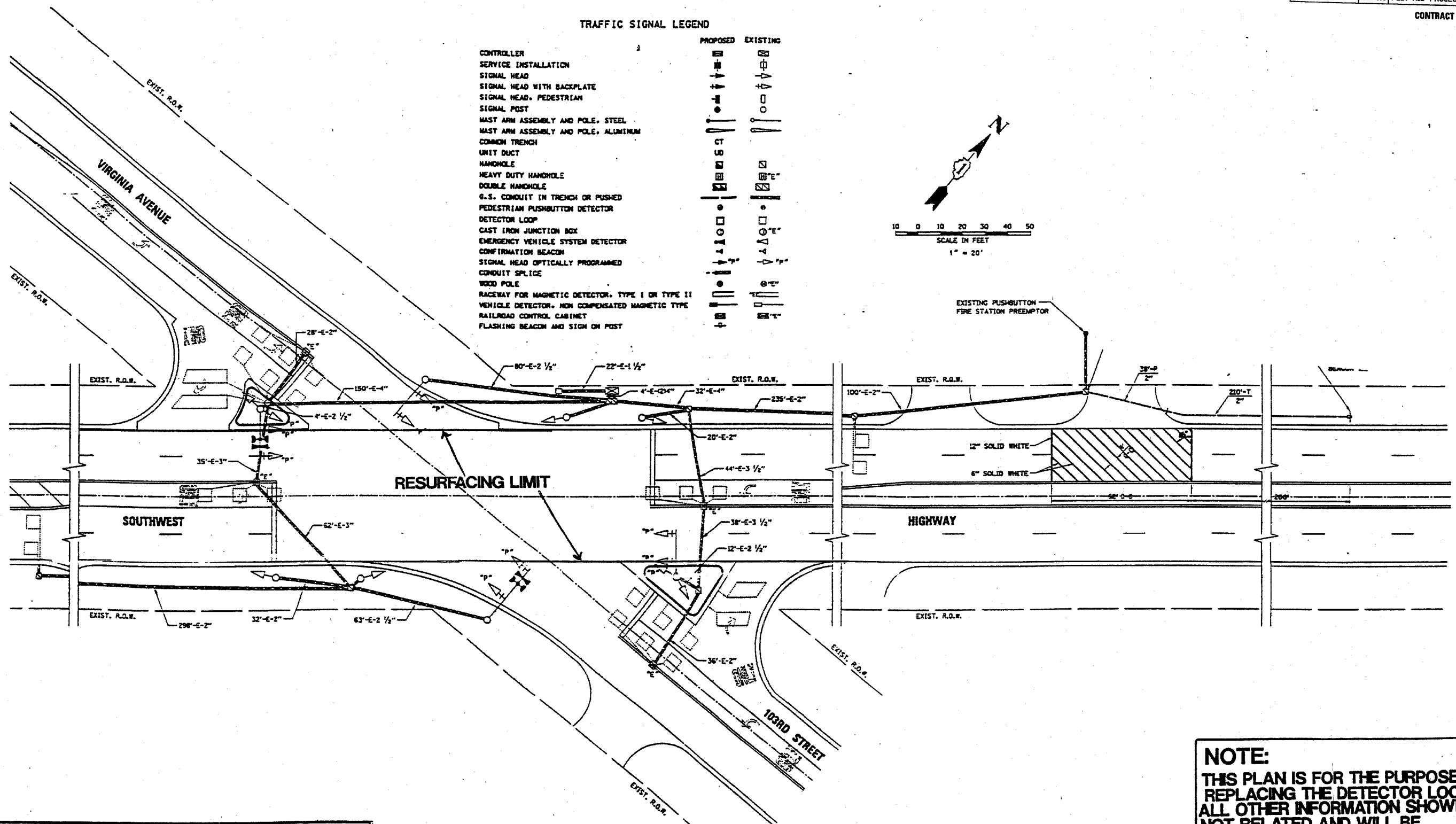
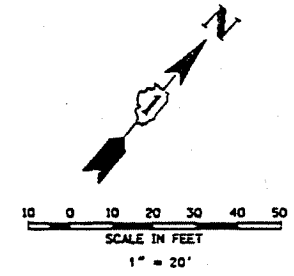
1

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DETECTOR LOOPS AT:
 IL RT 43 (HARLEM AVE) &
 IL RT 7 (SOUTHWEST HWY)

SCALE: 1"= _____
 DESIGNED BY: _____
 DRAWN BY: _____
 CHECKED BY: _____
 DATE _____

TRAFFIC SIGNAL LEGEND

| | | |
|--|----------|----------|
| | PROPOSED | EXISTING |
| CONTROLLER | | |
| SERVICE INSTALLATION | | |
| SIGNAL HEAD | | |
| SIGNAL HEAD WITH BACKPLATE | | |
| SIGNAL HEAD, PEDESTRIAN | | |
| SIGNAL POST | | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | | |
| COMMON TRENCH | | |
| UNIT DUCT | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| G.S. CONDUIT IN TRENCH OR PUSHED | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | |
| DETECTOR LOOP | | |
| CAST IRON JUNCTION BOX | | |
| EMERGENCY VEHICLE SYSTEM DETECTOR | | |
| CONFIRMATION BEACON | | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | |
| CONDUIT SPLICE | | |
| WOOD POLE | | |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | |
| RAILROAD CONTROL CABINET | | |
| FLASHING BEACON AND SIGN ON POST | | |



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 88600600 | 360 | FOOT | DETECTOR LOOP REPLACEMENT |

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

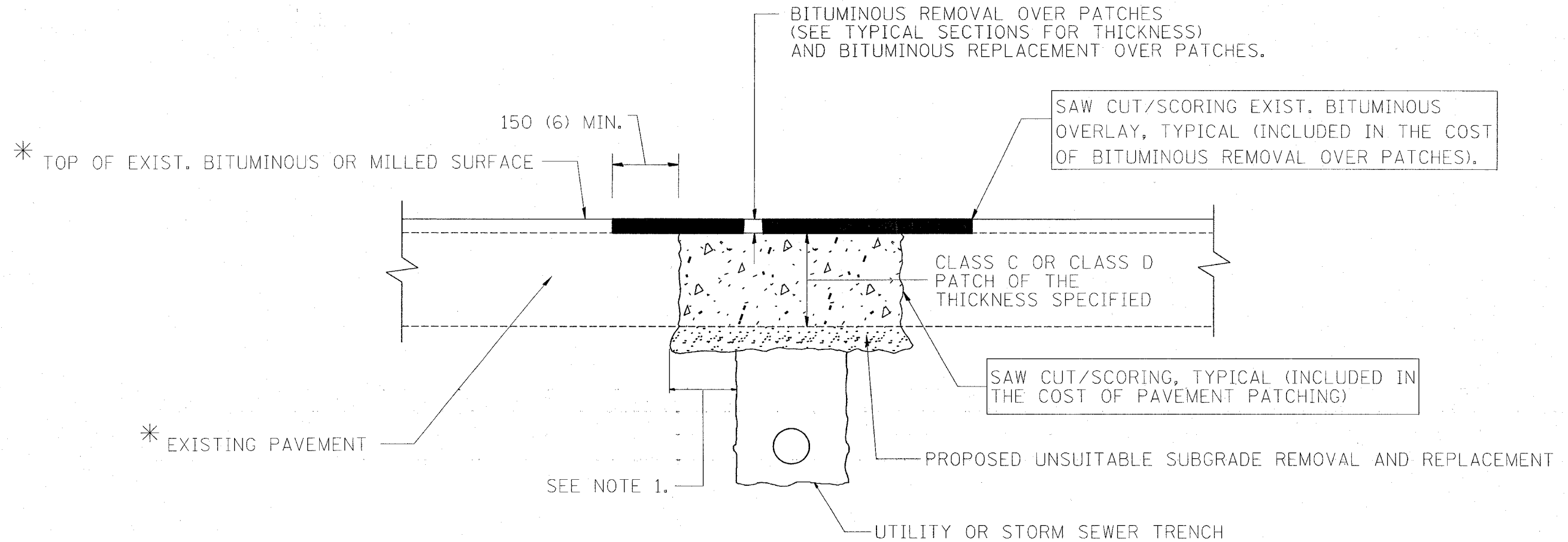
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOPS AT:
IL RT 7 (SOUTHWEST HWY) &
103RD STREET

SCALE: 1" = 20'
DESIGNED BY:
DRAWN BY:
DATE 11-3-03 CHECKED BY:



| | | | | |
|---------------------|-----------|------------------|--------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 14 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

62673



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| R. SHAH | 10/25/94 |
| R. SHAH | 01/14/95 |
| R. SHAH | 03/23/95 |
| R. SHAH | 04/24/95 |
| A. HOUSEH | 03/15/96 |
| A. ABBAS | 03/21/97 |
| A. ABBAS | 01/20/98 |
| ART ABBAS | 04/27/98 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

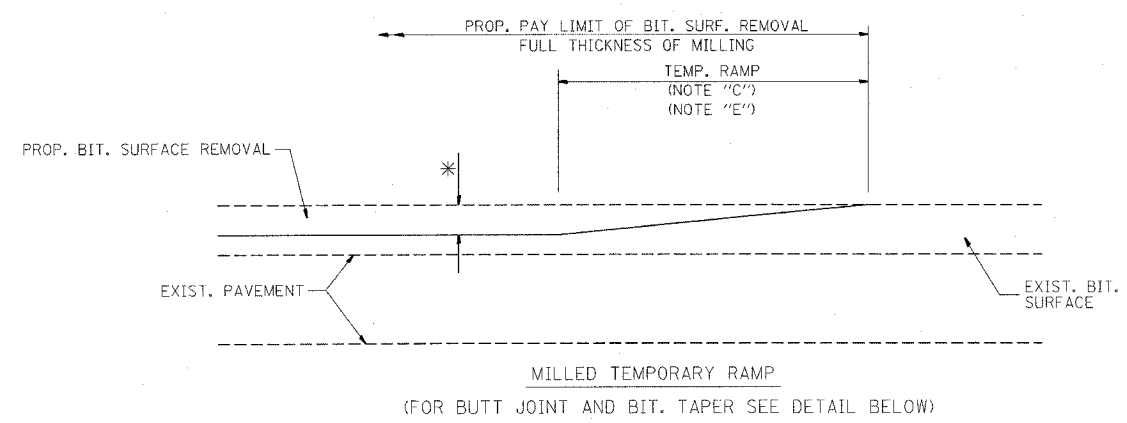
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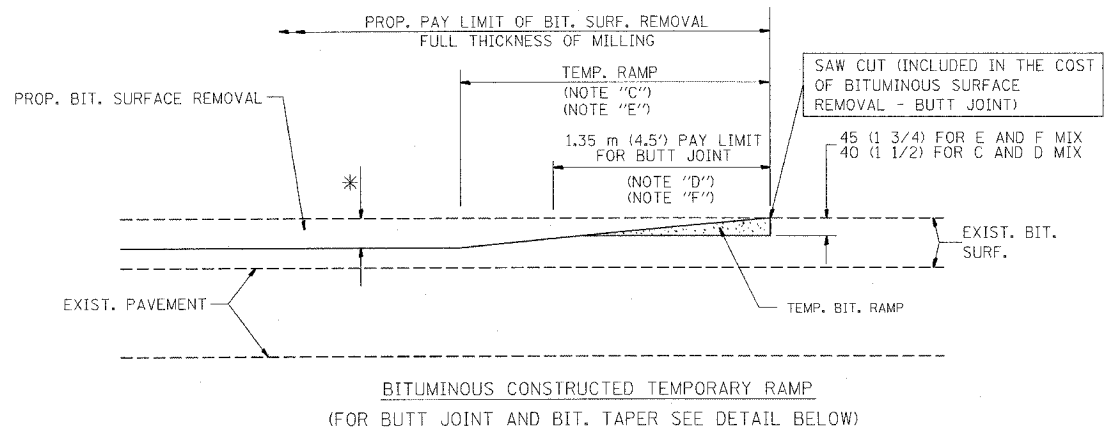
BD400-04 (80-22) REVISION DATE: 04/27/98

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|---------------------|-----------|------------------|-----------------|--------------|
| FAU NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 15 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

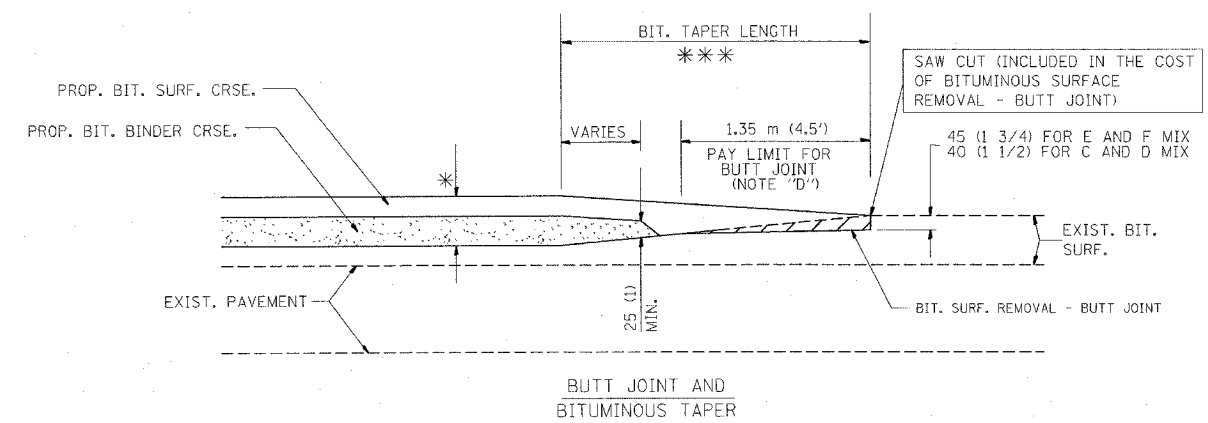
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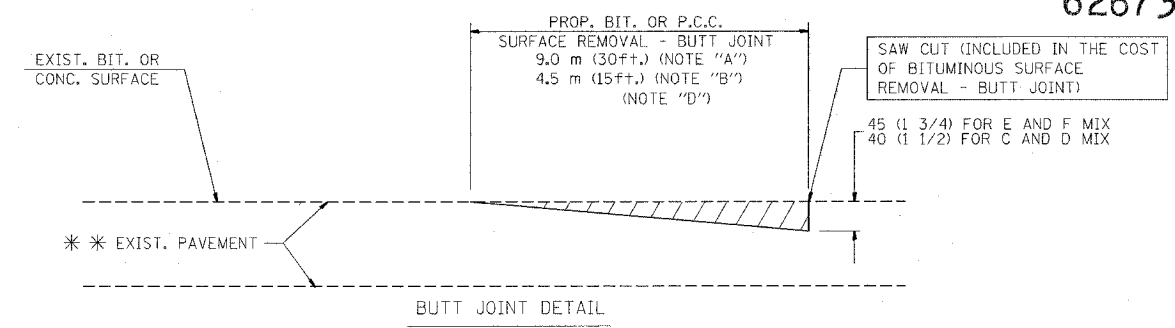
OPTION 1



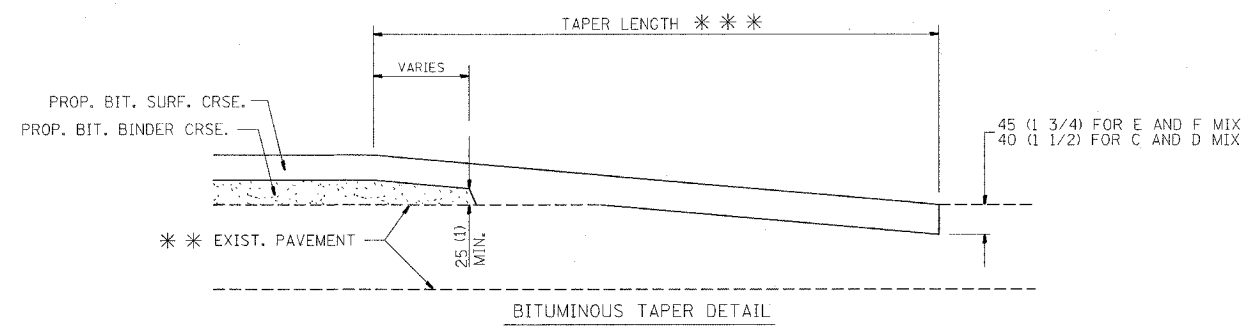
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



BITUMINOUS TAPER DETAIL

TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 6-13-90 |
| M. DE YONG | 7-3-90 |
| M. DE YONG | 3-27-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| A. ABBAS | 03/21/97 |
| M. GOMEZ | 04/06/01 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
BITUMINOUS TAPER
DETAILS

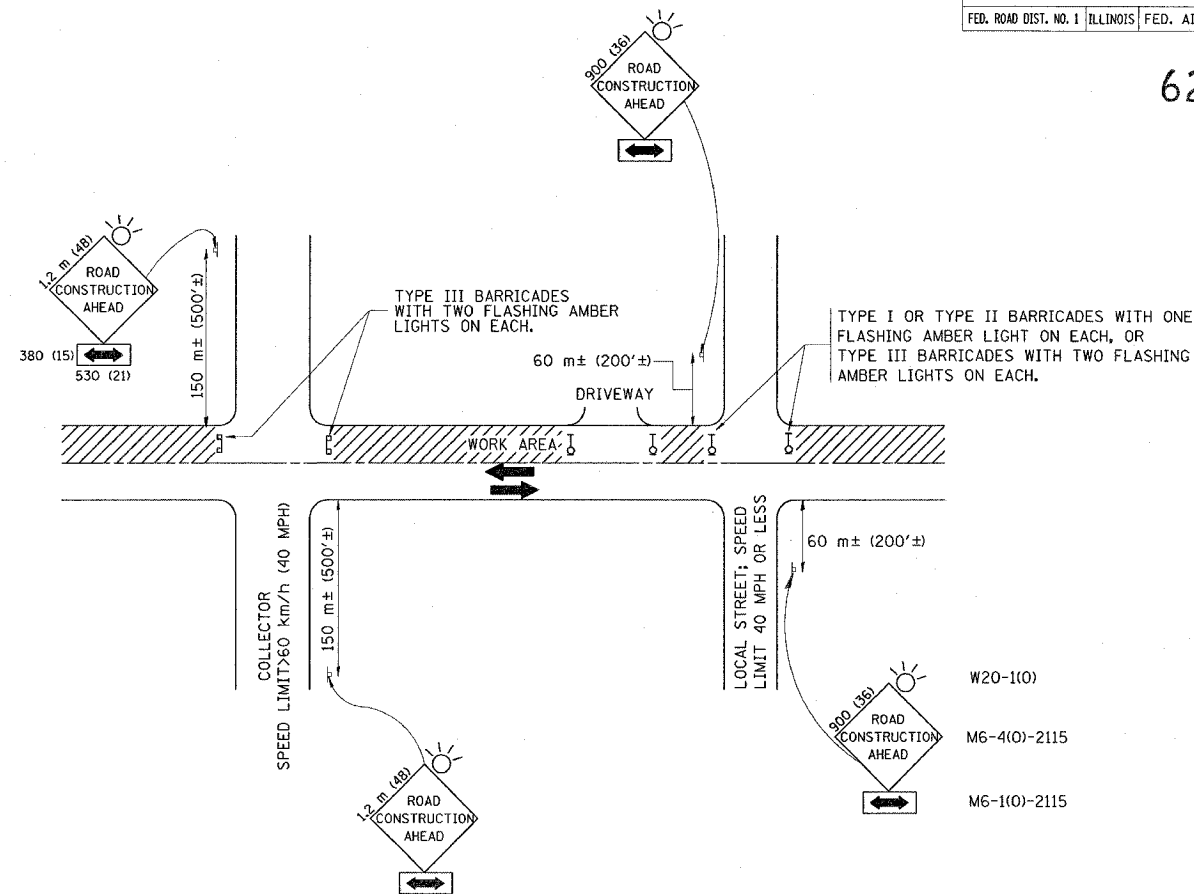
SCALE: NONE
DATE PLOTTED: 3/28/2005

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CHECKED BY
BD400-05 (VI-BD32)
REVISION DATE: 04/06/01

03/28/2005 03:05:07

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

62673



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| LHA | 6/89 |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE | 10/18/95 |
| A. HOUSEH | 03/06/96 |
| A. HOUSEH | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
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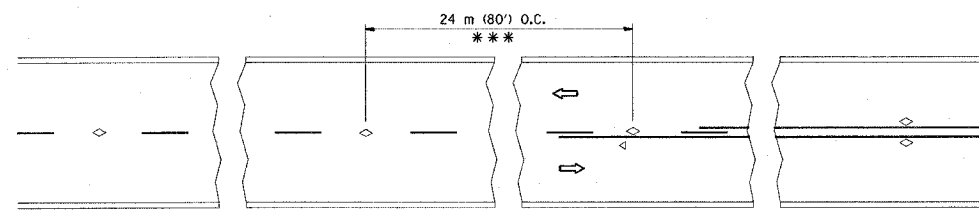
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION
FOR
SIDE ROADS, INTERSECTIONS, AND
DRIVEWAYS

SCALE: VERT.
HORIZ.
DATE 3/28/2005

DRAWN BY
CHECKED BY
TC-10

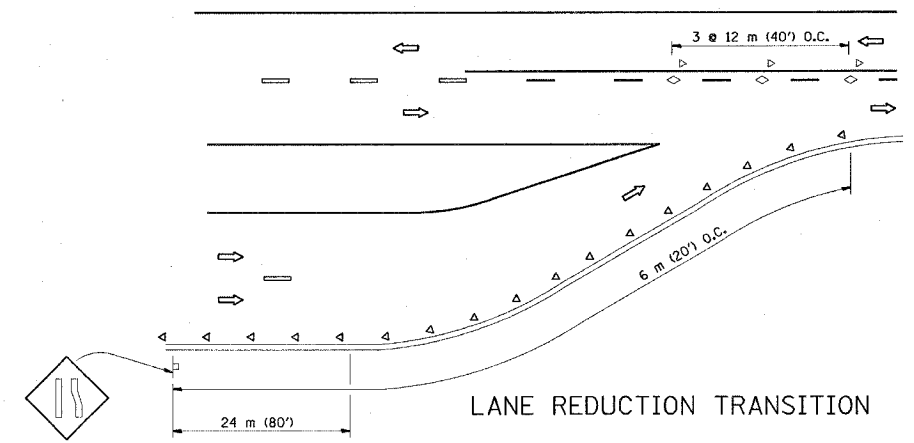
REVISION DATE: 01/06/00

62673

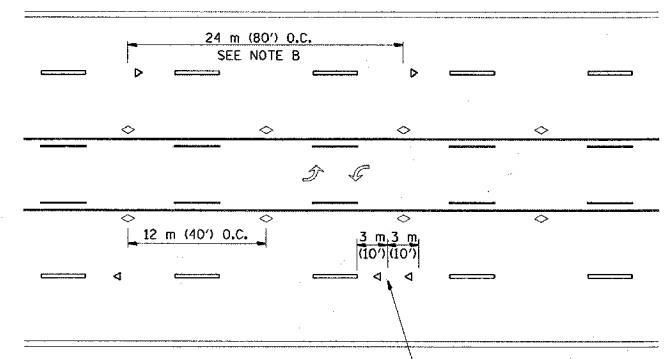


*** REDUCE TO 12 m (40') O.C. ON CURVES WITH POSTED OR ADVISORY SPEED TO km/h (45 M.P.H.) OR LESS.

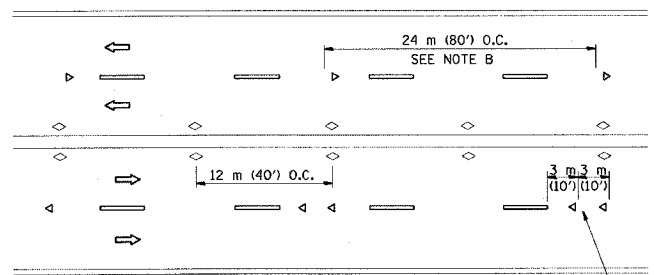
TWO-LANE/TWO-WAY



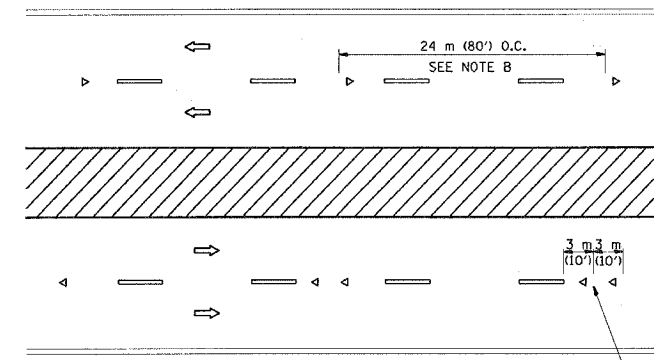
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

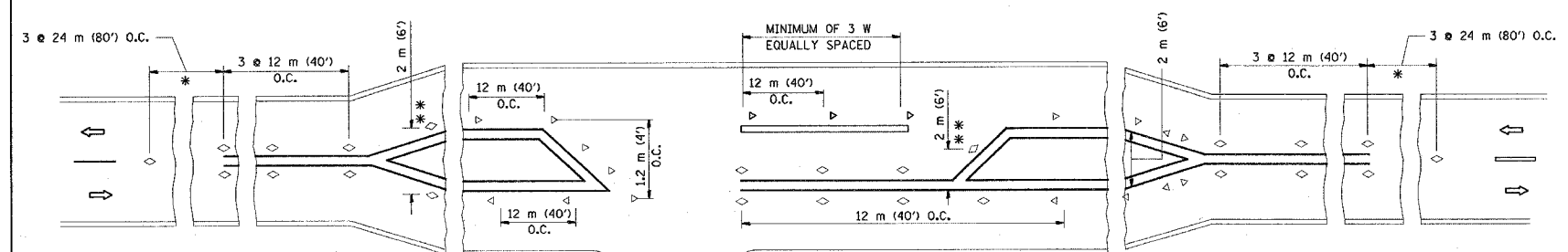
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

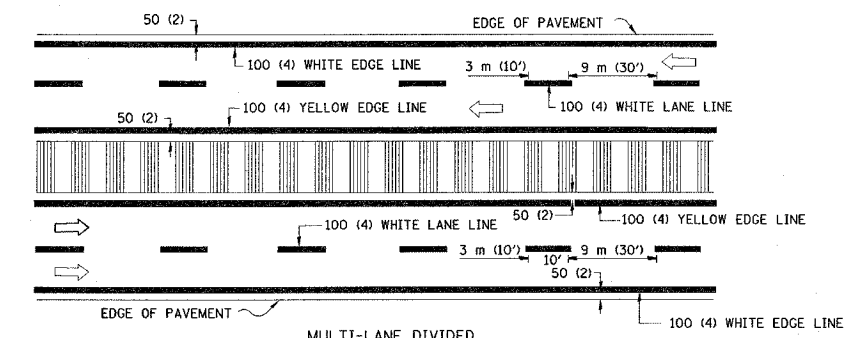
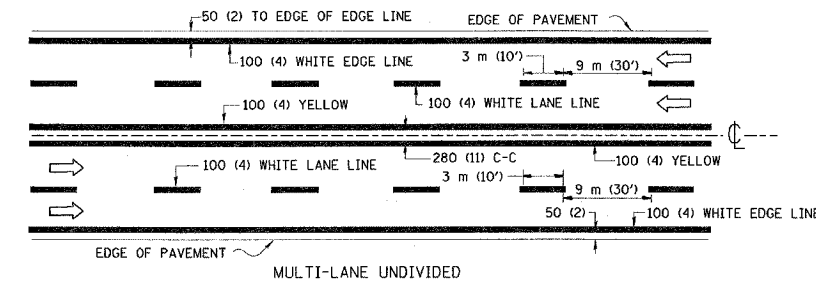
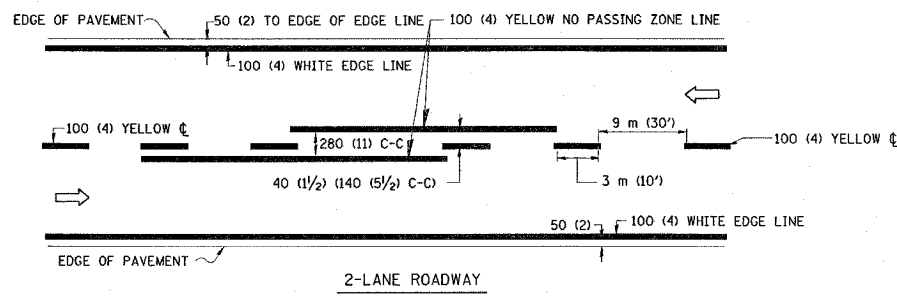
All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS
 (SNOW-PLOW RESISTANT)

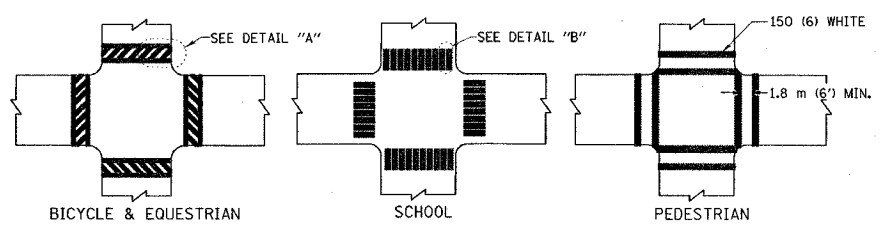
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09-19-94 |
| T. RAMMACHER | 03-12-99 |
| T. RAMMACHER | 01-06-00 |
| | |
| | |

SCALE: NONE
 DATE: 3/28/2005
 DRAWN BY: CADD
 CHECKED BY: TC-11
 REVISION DATE: 01/06/00

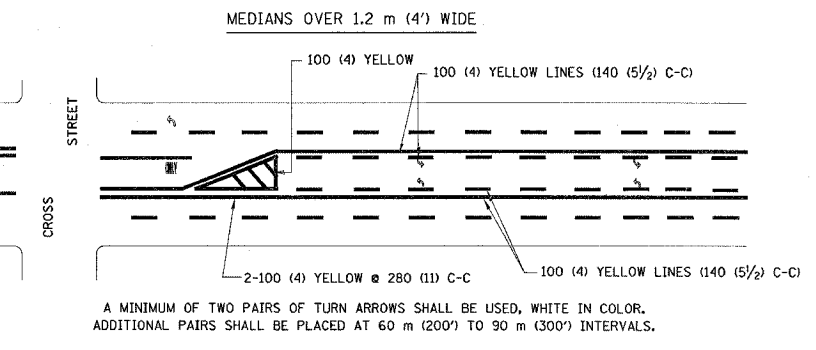
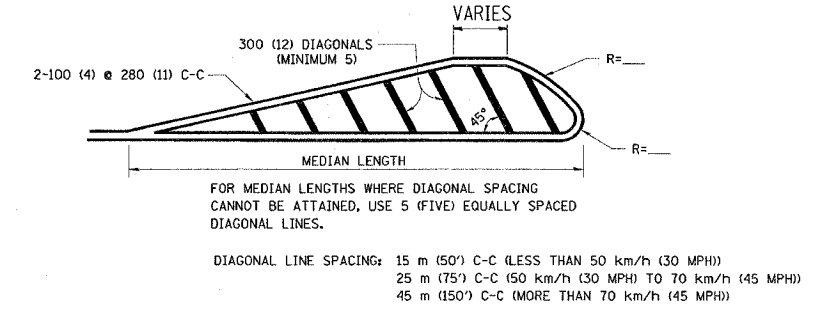
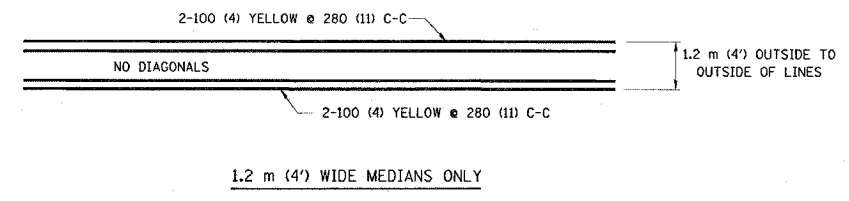
62673



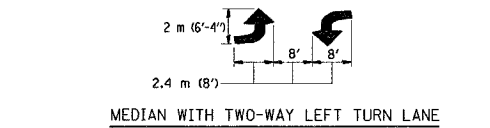
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

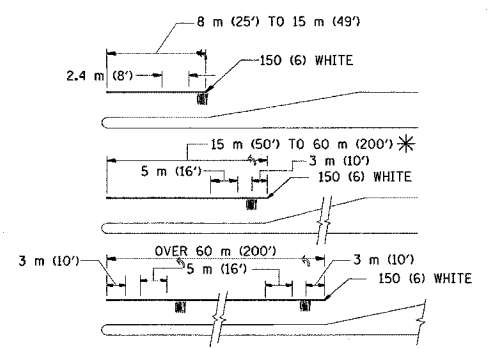


TYPICAL PAINTED MEDIAN MARKING



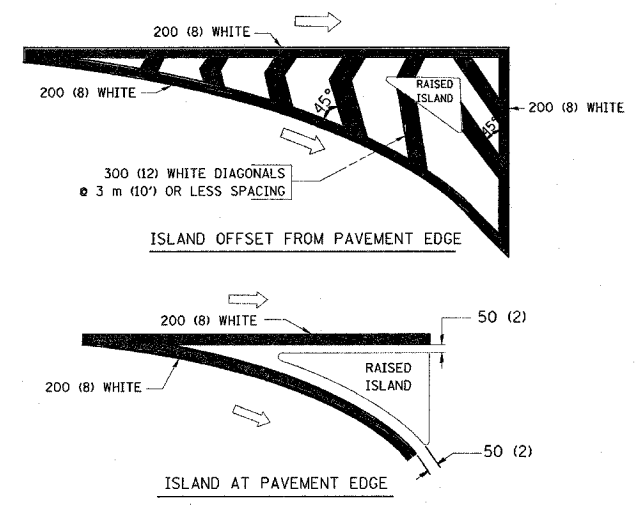
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL LEFT (OR RIGHT) TURN LANE



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 100 (4) | SKIP-DASH | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION | 100 (4) | SOLID | YELLOW | 140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE |
| FOR BOTH DIRECTIONS | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 100 (4) 125 (5) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 3 m (10') LINE WITH 9 m (30') SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 600 (2') LINE WITH 1.8 m (6') SPACE |
| EDGE LINES | 100 (4) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8')) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 100 (4) EACH DIRECTION | SKIP-DASH AND SOLID | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE |
| | 2.4 m (8') LEFT ARROW | IN PAIRS | WHITE | SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) | 2 @ 150 (6) | SOLID | WHITE | NOT LESS THAN 1.8 m (6') APART |
| A. DIAGONALS (BIKE & EQUESTRIAN) | 300 (12) @ 45° | SOLID | WHITE | 600 (2') APART |
| B. LONGITUDINAL BARS (SCHOOL) | 300 (12) @ 90° | SOLID | WHITE | 600 (2') APART |
| | | | | SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 600 (24) | SOLID | WHITE | PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 200 (8) WITH 300 (12) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH)) |
| RAILROAD CROSSING | 600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.) |
| SHOULDER DIAGONALS | 300 (12) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (Inches) unless otherwise shown.

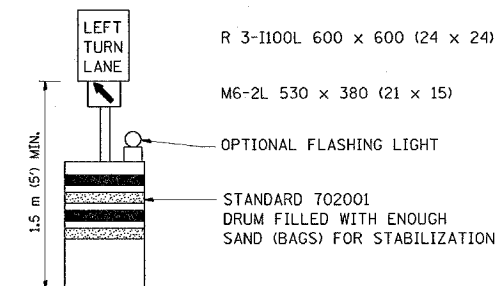
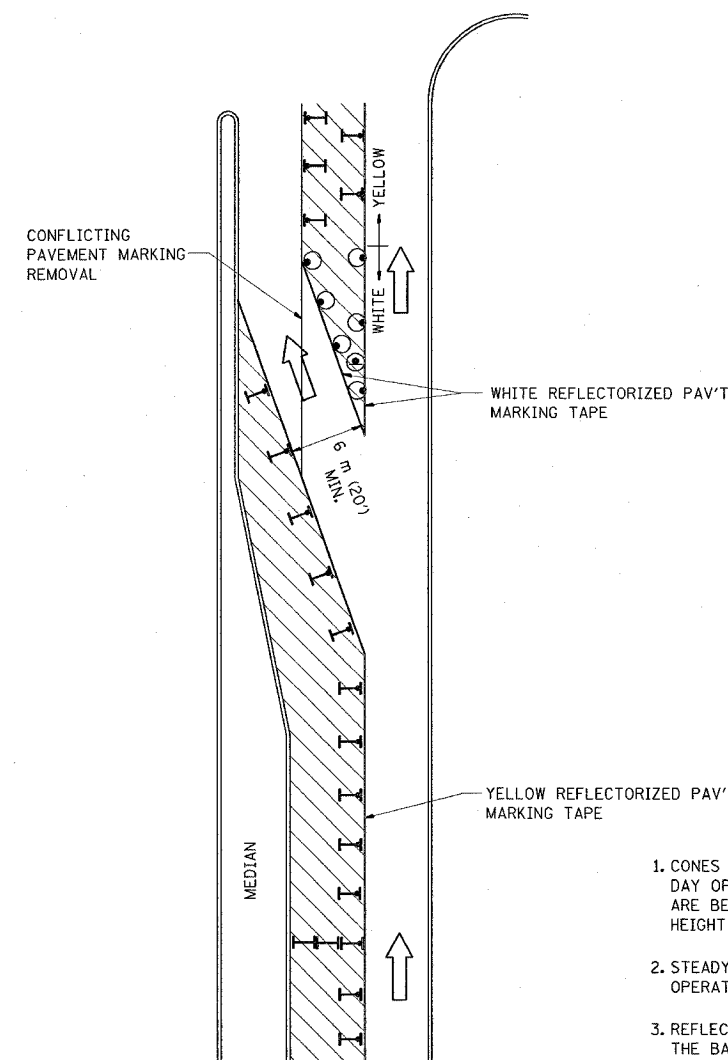
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| EVERS | 03-19-90 |
| T. RAMMACHER | 10-27-94 |
| ALEX HOUSEH | 10-09-96 |
| ALEX HOUSEH | 10-17-96 |
| T. RAMMACHER | 01-06-00 |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

SCALE: NONE
 DATE: 3/28/2005
 DRAWN BY: CADD
 CHECKED BY:

| | | | | |
|---------------------|-----------|------------------|--------------|----------|
| FAU | SECTION | COUNTY | TOTAL SHEETS | SHEET NO |
| 3578 | 1314 RS-3 | COOK | 23 | 19 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

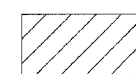





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GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/08/94 |
| A. HOUSEH | 11/07/95 |
| A. HOUSEH | 10/12/96 |
| T. RAMMACHER | 01/06/00 |
| | |
| | |

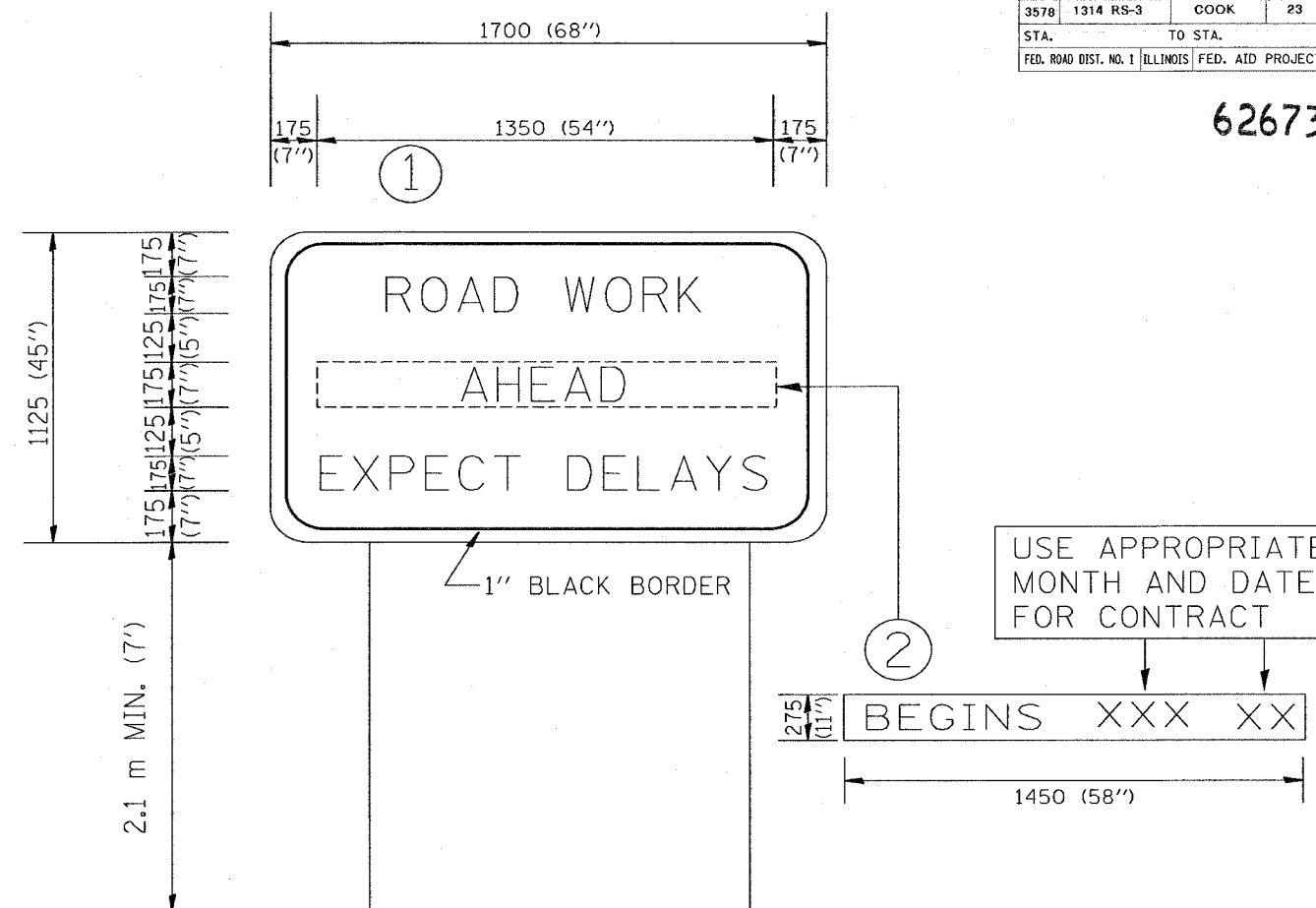
SCALE: NONE
 DATE: 3/28/2005

DRAWN BY
 CHECKED BY LHA
 TC-14

REVISION DATE: 01/06/00

| | | | | |
|-----------------------|-----------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

62673



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

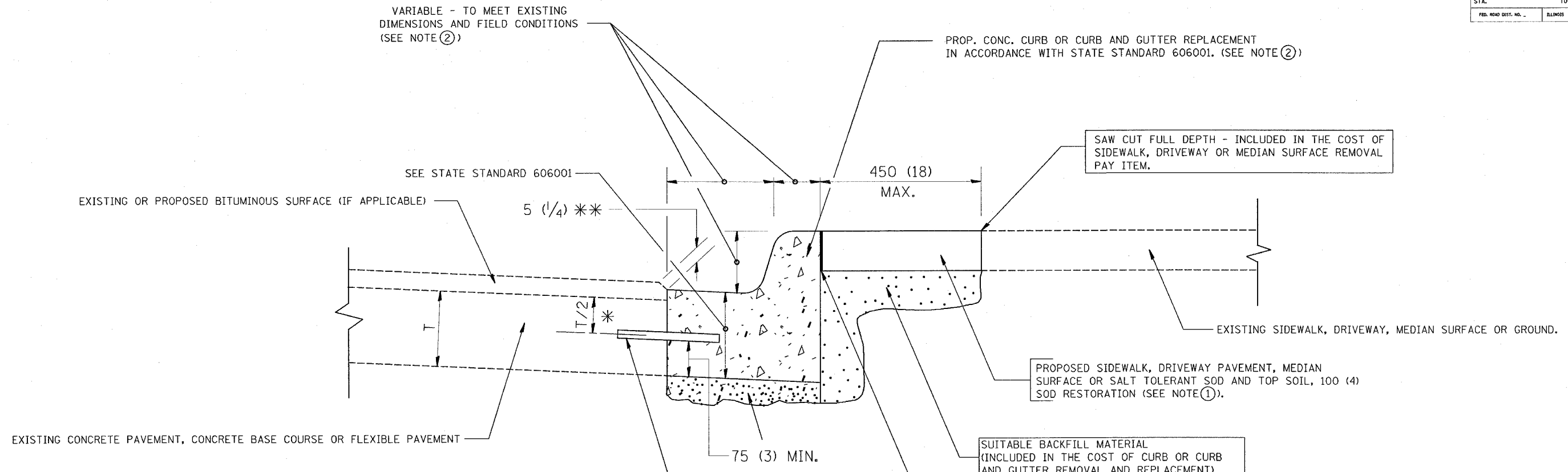
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|--------------|----------|---------------------------------------|
| NAME | DATE | |
| R. MIRS | 9-15-97 | TEMPORARY INFORMATION SIGNING |
| R. MIRS | 12-11-97 | |
| T. RAMMACHER | 2-2-99 | |
| | | |
| | | |
| | | |
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| | | |
| | | |
| | | |

SCALE: DATE 3/28/2005 DRAWN BY: BUR. OF DESIGN CHECKED BY

TC22 REVISION DATE: 02/02/99

| | | | | |
|---------------------|-----------|----------|------------------|--------------|
| FAU NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 21 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

62673



VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

450 (18) MAX.

EXISTING OR PROPOSED BITUMINOUS SURFACE (IF APPLICABLE)

5 (1/4) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 100 (4) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

75 (3) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 20 (3/4) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 05/28/91 |
| A. HOUSEH | 03/11/94 |
| R. SHAH | 02/24/95 |
| R. SHAH | 03/02/95 |
| R. SHAH | 08/19/96 |
| R. SHAH | 09/12/96 |
| R. SHAH | 09/19/96 |
| R. SHAH | 10/03/96 |
| A. ABBAS | 03/21/97 |
| M. GOMEZ | 01/22/01 |

ILLINOIS DEPARTMENT OF TRANSPORTATION
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

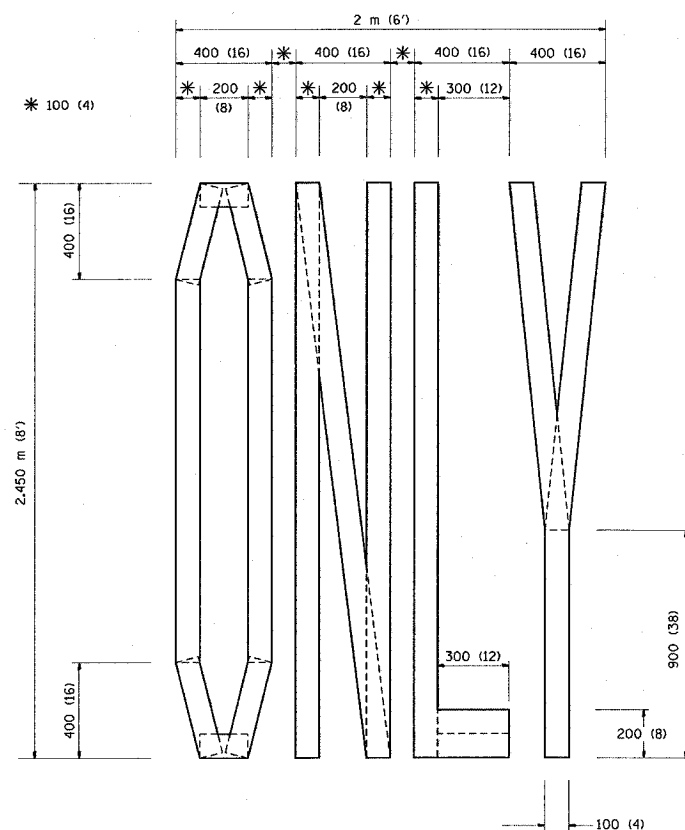
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DATE 3/28/2005

DRAWN BY
CHECKED BY
BD600-06 (BD-24)

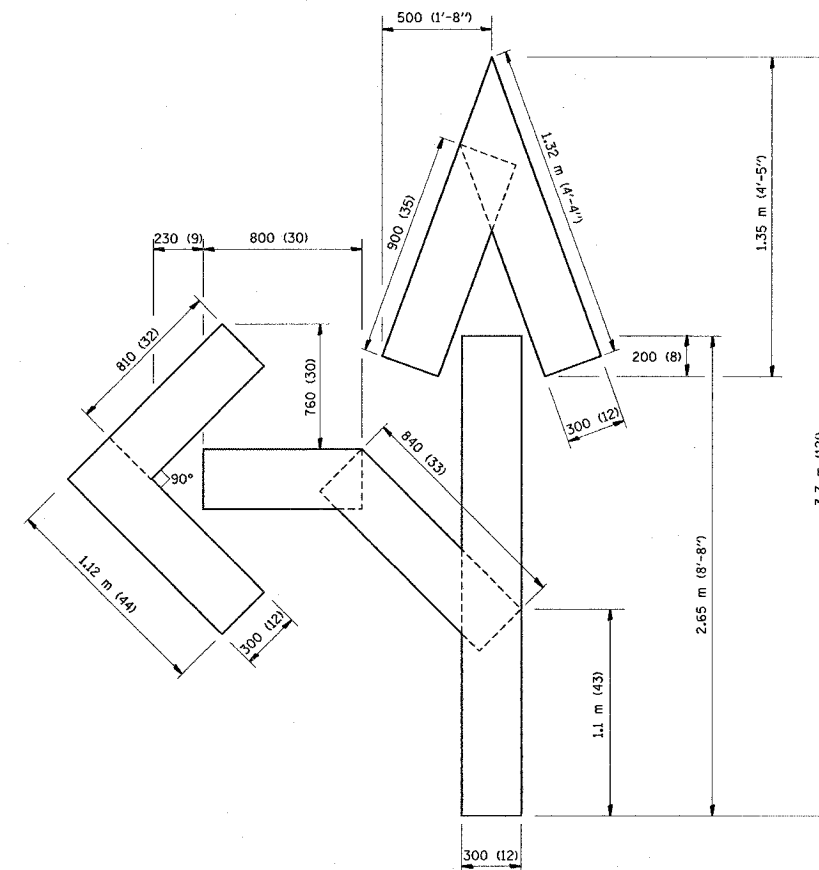
REVISION DATE: 12/06/98

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|---------------------|-----------|----------|------------------|--------------|
| FAU RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 22 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

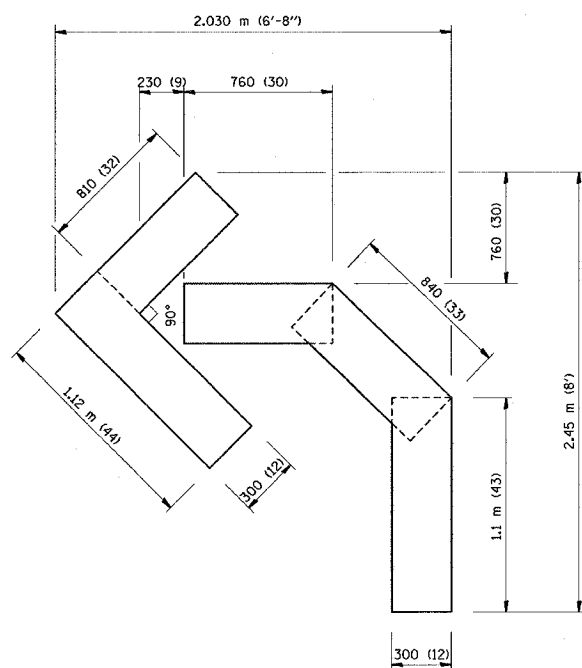
62673



QUANTITY
100 (4) LINE = 19.7 m (64.1 ft.)
1.97 sq. m (21.1 sq. ft.)



QUANTITY
100 (4) LINE = 25.3 m (82.5 ft.)
2.53 sq. m (27.5 sq. ft.)



QUANTITY
100 (4) LINE = 13.9 m (45.5 ft.)
1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/18/94 |
| J. OBERLE | 06/01/96 |
| T. RAMMACHER | 06/05/96 |
| T. RAMMACHER | 11/04/97 |
| T. RAMMACHER | 03/02/98 |
| E. GOMEZ | 08/28/00 |

SCALE: NONE
DATE 3/28/2005

DRAWN BY CADD
CHECKED BY
TC-16

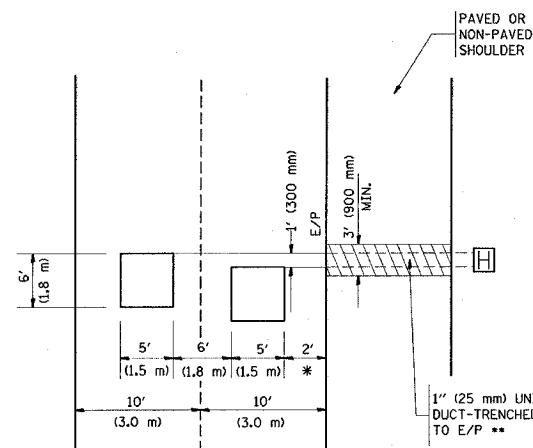
REVISION DATE: 08/28/00

| | | | | |
|-----------------------|-----------|----------|------------------|--------------|
| FAU #1. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3578 | 1314 RS-3 | COOK | 23 | 23 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

62673

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

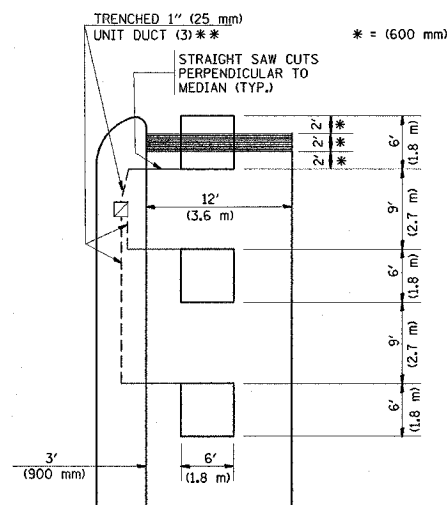


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

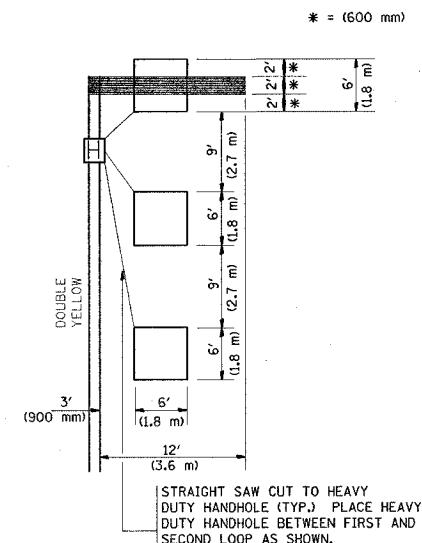
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

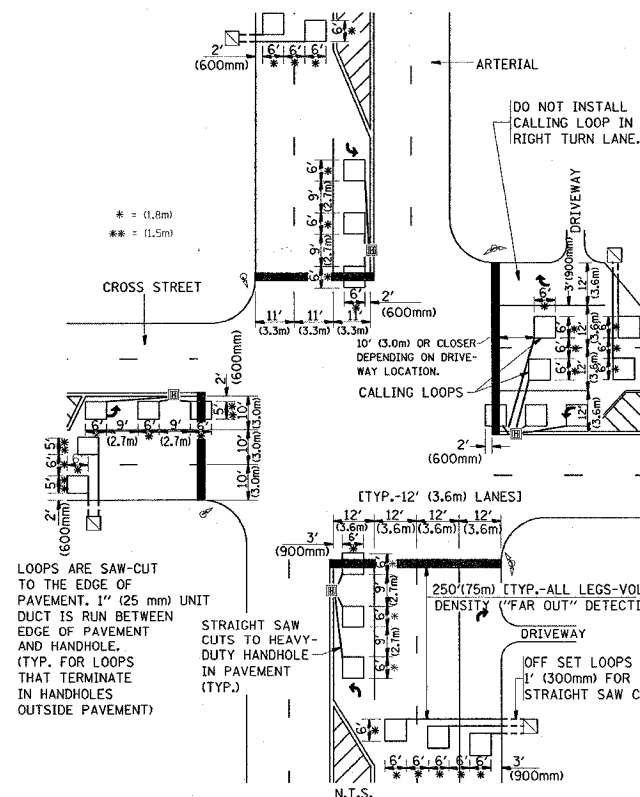
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



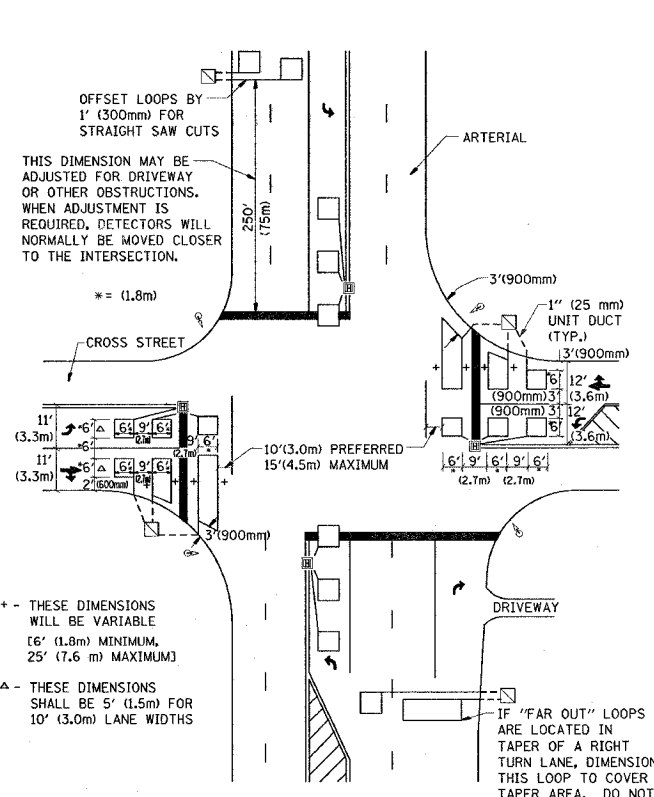
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING

SCALE: NONE
DATE 3/28/2005

DRAWN BY CADD
DESIGNED BY
CHECKED BY R.K.F.

T507

REVISION DATE: