

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

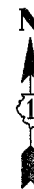
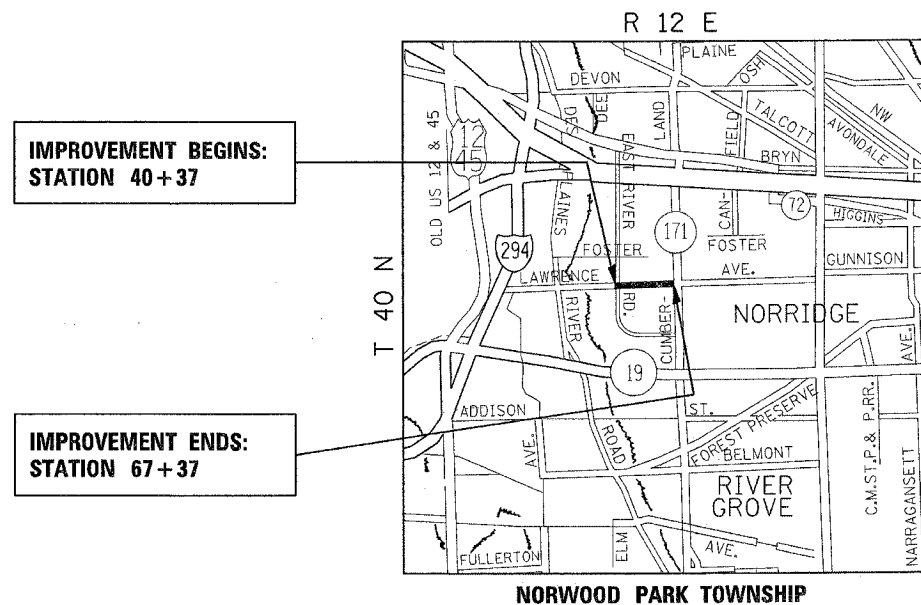
PROPOSED HIGHWAY PLANS

FAU ROUTE 1362: LAWRENCE AVENUE
SECTION: 3200 RS-2
EAST RIVER ROAD TO CUMBERLAND AVENUE
RESURFACING (MAINTENANCE)
COOK COUNTY
C-91-185-05

FOR INDEX OF SHEETS, SEE SHEET NO. 2

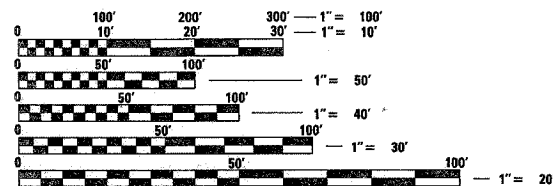
IMPROVEMENT IS LOCATED IN THE VILLAGE OF NORRIDGE AND THE CITY OF CHICAGO

D-91-185-05



TRAFFIC DATA

2002 ADT = 20,100
POSTED SPEED LIMIT = 35 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-392-0123

MAP SCALE : [Symbol] 1.0 MILE

GROSS & NET LENGTH OF IMPROVEMENT = 2700 FEET = 0.51 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 6, 20 05

Dina O'Keefe, IAP
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 1, 20 05
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

July 1, 20 05
Victor Maden
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER: KEN ENG / JENPAI CHANG (847) 705-4432

CONTRACT NO. 62946

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	2
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES & CHICAGO NOTES
3	SUMMARY OF QUANTITIES
4	EXISTING AND PROPOSED TYPICAL SECTION
5	ROADWAY AND PAVEMENT MARKING PLANS
6-7	DETECTOR LOOP REPLACEMENT PLANS
8	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
9	PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT
10	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
11	BUTT JOINT AND BITUMINOUS TAPER DETAILS
12	CATCH BASIN, INLET AND MANHOLE DETAIL - CITY OF CHICAGO
13	METHOD OF FLAGGING
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
15	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
16	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
17	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
18-19	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS
20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
21	TEMPORARY INFORMATION SIGNING DETAIL
22	PAVEMENT MARKING LETTERS & SYMBOLS FOR TRAFFIC STAGING

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF NORRIDGE.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MR. SCOTT KUZNICKI, AREA TRAFFIC FIELD ENGINEER, AT (708) 524-2145 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PAVEMENT MARKINGS

3 METERS (10 FEET) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATED PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURES.

CHICAGO NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "CUAN" (CHICAGO UTILITY ALERT NETWORK), AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS STANDARDS.

IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWERS STRUCTURES AND / OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF SEWERS AT 312-747-7892 OR 312-747-7893.

CLOSED LIDS SHALL BE PLACED ON ALL MANHOLES EXCEPT AT INTERSECTIONS PERFORATED LIDS SHALL BE PLACED ON ALL CATCH BASINS.

BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED FROM THE DEPARTMENT OF SEWERS AT SUITE 410, 333 SOUTH STATE STREET, CHICAGO, IL, 60604-3971. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACING ANY BENCH MONUMENT DAMAGED OR DESTROYED DURING CONSTRUCTION.

ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND GRATES OR LIDS ON SEWER STRUCTURES SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND GRATES OR LIDS. OLD FRAMES AND GRATES OR LIDS SHALL BE DELIVERED TO THE DEPARTMENT OF SEWERS AT 39TH STREET AND ASHLAND AVENUE.

CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER AND/OR DEPARTMENT OF SEWERS.

CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 75 MM (3").

PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED SHALL BE WITH CLASS SI CONCRETE.

BACKFILL MATERIAL UNDER SIDEWALKS SHALL BE FA-2 (2").

SIDEWALK ACCESSIBILITY RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-04	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-01	CLASS C AND D PATCHES
604001-02	FRAME AND LIDS, TYPE 1
604086-01	FRAME AND GRATE, TYPE 23
606001-02	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701606-04	URBAN LANE CLOSURE, MULTILANE, 2W, WITH MOUNTALBE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
702001-05	TRAFFIC CONTROL DEVICES
886001	DETECTOR LOOP INSTALLATION
886006	TYPICAL LAYOUT FOR DETECTOR LOOP

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		INDEX OF SHEETS, STATE STANDARDS, GENERAL & CHICAGO NOTES

SCALE: VERT. DATE
 HORIZ. DRAWN BY
 CHECKED BY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		I000 STATE				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	8	8				
40600300	AGGREGATE (PRIME COAT)	TON	37.5	37.5				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2	2				
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	192	192				
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	446	446				
42101300	PROTECTIVE COAT	SQ YD	135	135				
44000116	BITUMINOUS REMOVAL OVER PATCHES 4"	SQ YD	1220	1220				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	600	600				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	266	266				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	266	266				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	355	355				
55039700	STORM SEWERS TO BE CLEANED	FOOT	500	500				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	5				
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	25	25				
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	32	32				
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	40	40				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5				
67100100	MOBILIZATION	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	985	985				
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	19060	19060				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2375	2375				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1435	1435				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		I000 STATE				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	165	165				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	9635	9635				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19060	19060				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2375	2375				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1435	1435				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	165	165				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	230	230				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	230	230				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	570	570				
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4				
X4066424	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N50	TON	1560	1560				
X4067100	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	780	780				
X4409410	BITUMINOUS SURFACE REMOVAL 2 1/4"	SQ YD	18524	18524				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	57	57				

* SPECIALTY ITEMS

REVISIONS	
NAME	DATE

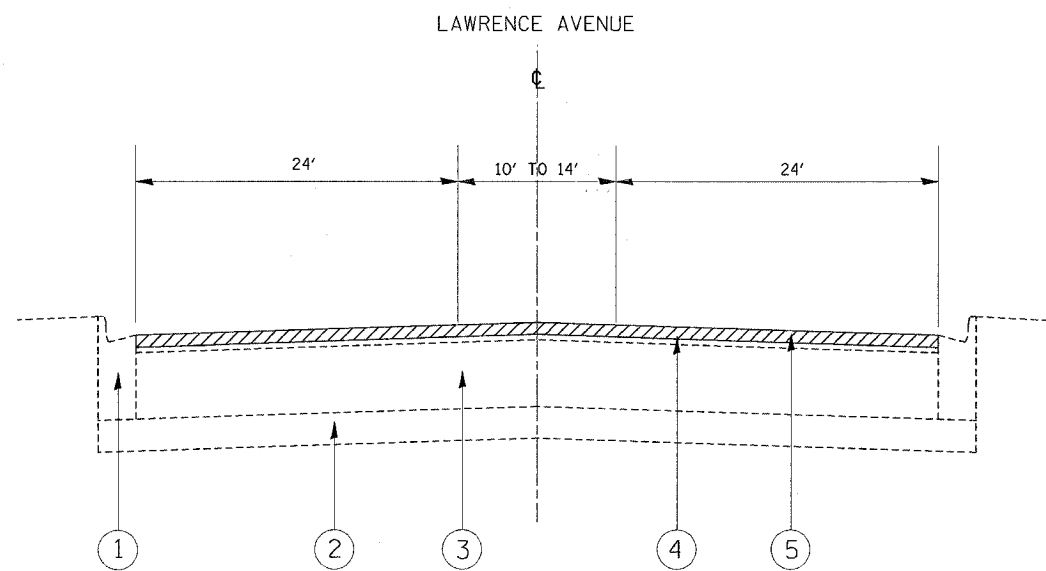
ILLINOIS DEPARTMENT OF TRANSPORTATION
 LAWRENCE AVENUE
 SUMMARY OF QUANTITIES

6/8/2005 6:40:39 AM C:\projects\62946\summary\summary.plt

*REF-
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	4
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS HIGHWAY PROJECT		

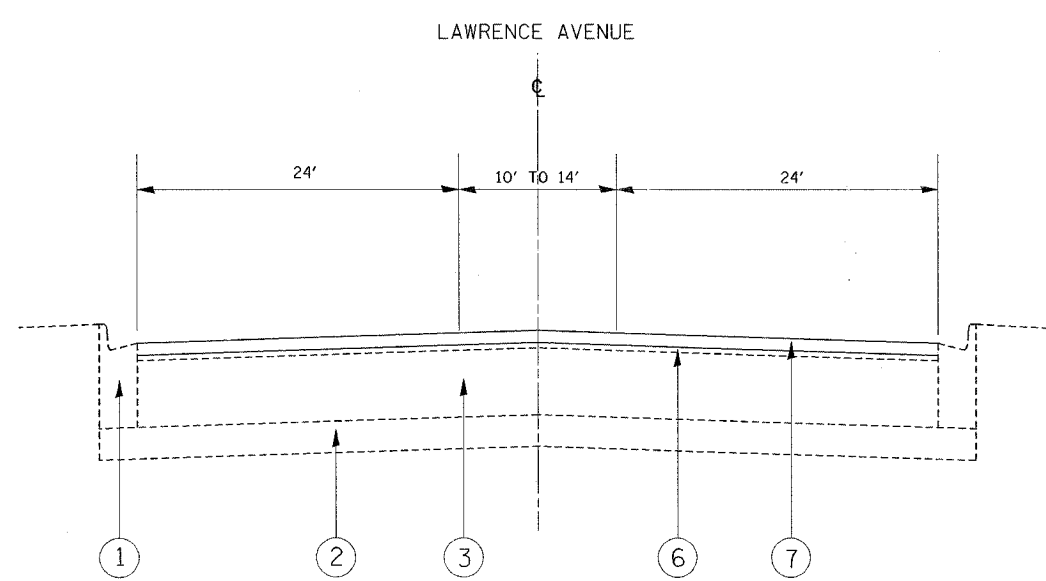
CONTRACT #62946



EXISTING TYPICAL SECTION
LAWRENCE AVENUE
STA. 40+37 TO STA. 67+37

LEGEND:

- ① EXISTING B-6.12 CURB & GUTTER
- ② EXISTING SUB-BASE GRANULAR MATERIAL, 6" (±)
- ③ EXISTING P.C.C PAVEMENT, 9" (±)
- ④ EXISTING BITUMINOUS OVERLAY, 4" (±)
- ⑤ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/4"
- ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 3/4"
- ⑦ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N50, 1 1/2"



PROPOSED TYPICAL SECTION
LAWRENCE AVENUE
STA. 40+37 TO STA. 67+37

BITUMINOUS MIXTURE REQUIREMENTS

MIXTURE USE	AC/PG	MAX RAP. (%)	AIR VOIDS (%)
BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N50	PG 64-22	15%	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50	SBS/SBR PG 76-28	0%	2.5% @ 50 GYR.
BIT. REPLACEMENT OVER PATCHES BINDER IL-19.0 MM	PG 64-22	15%	4% @ 70 GYR.
CLASS D PATCH BINDER IL-19.0 MM	PG 64-22	15%	4% @ 70 GYR.

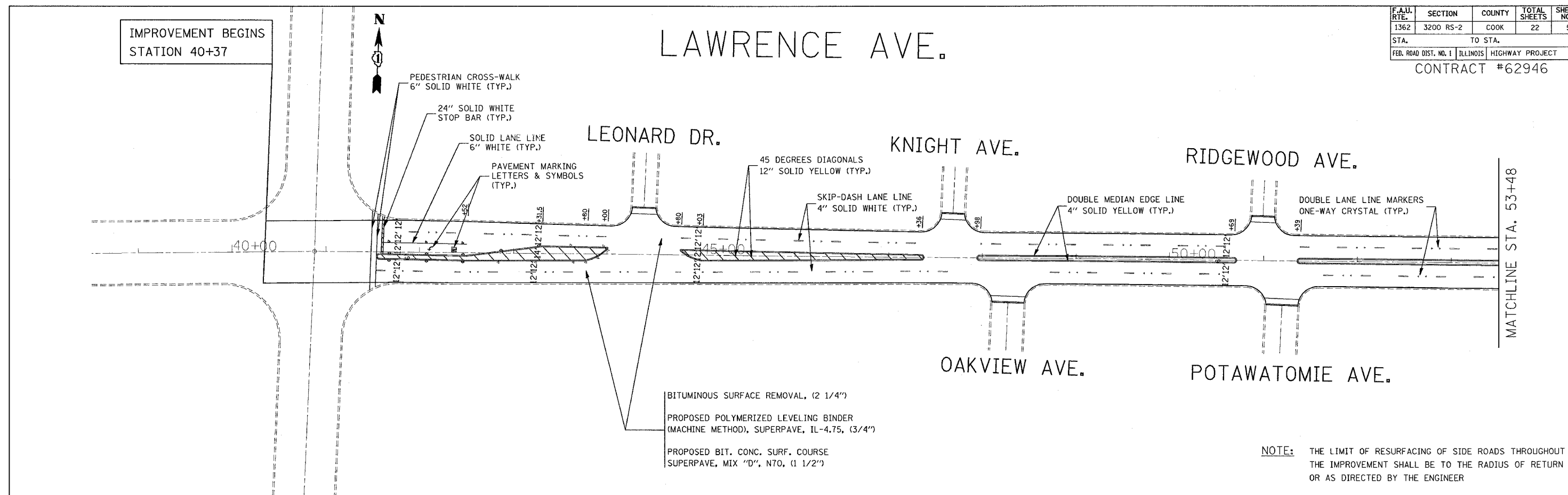
THE UNIT WEIGHT FOR ALL BITUMINOUS SURFACE MIXTURE QUANTITIES IS 112 LBS / SQ. YD. / IN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
LAWRENCE AVENUE
EAST RIVER RD. TO CUMBERLAND AVE.
TYPICAL SECTIONS
SCALE: VERT. NONE
HORIZ. DATE 6/8/2005
DRAWN BY:
CHECKED BY:

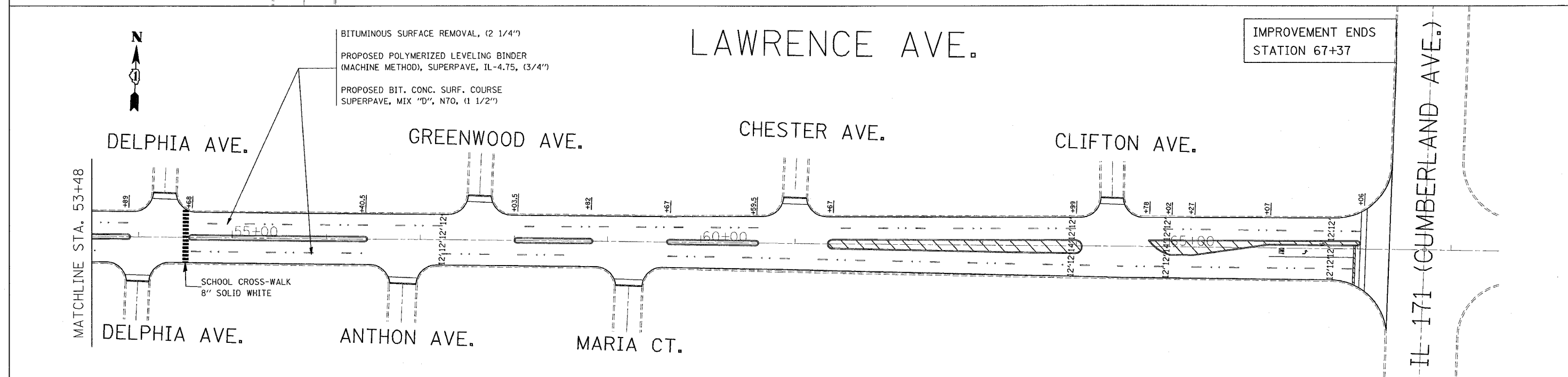
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	5
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	HIGHWAY PROJECT		
CONTRACT #62946				



BITUMINOUS SURFACE REMOVAL, (2 1/4")
 PROPOSED POLYMERIZED LEVELING BINDER
 (MACHINE METHOD), SUPERPAVE, IL-4.75, (3/4")
 PROPOSED BIT. CONC. SURF. COURSE
 SUPERPAVE, MIX "D", N70, (1 1/2")

NOTE: THE LIMIT OF RESURFACING OF SIDE ROADS THROUGHOUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER



BITUMINOUS SURFACE REMOVAL, (2 1/4")
 PROPOSED POLYMERIZED LEVELING BINDER
 (MACHINE METHOD), SUPERPAVE, IL-4.75, (3/4")
 PROPOSED BIT. CONC. SURF. COURSE
 SUPERPAVE, MIX "D", N70, (1 1/2")

NOTES: ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13)
 ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL," (TC-11)

REVISIONS	
NAME	DATE

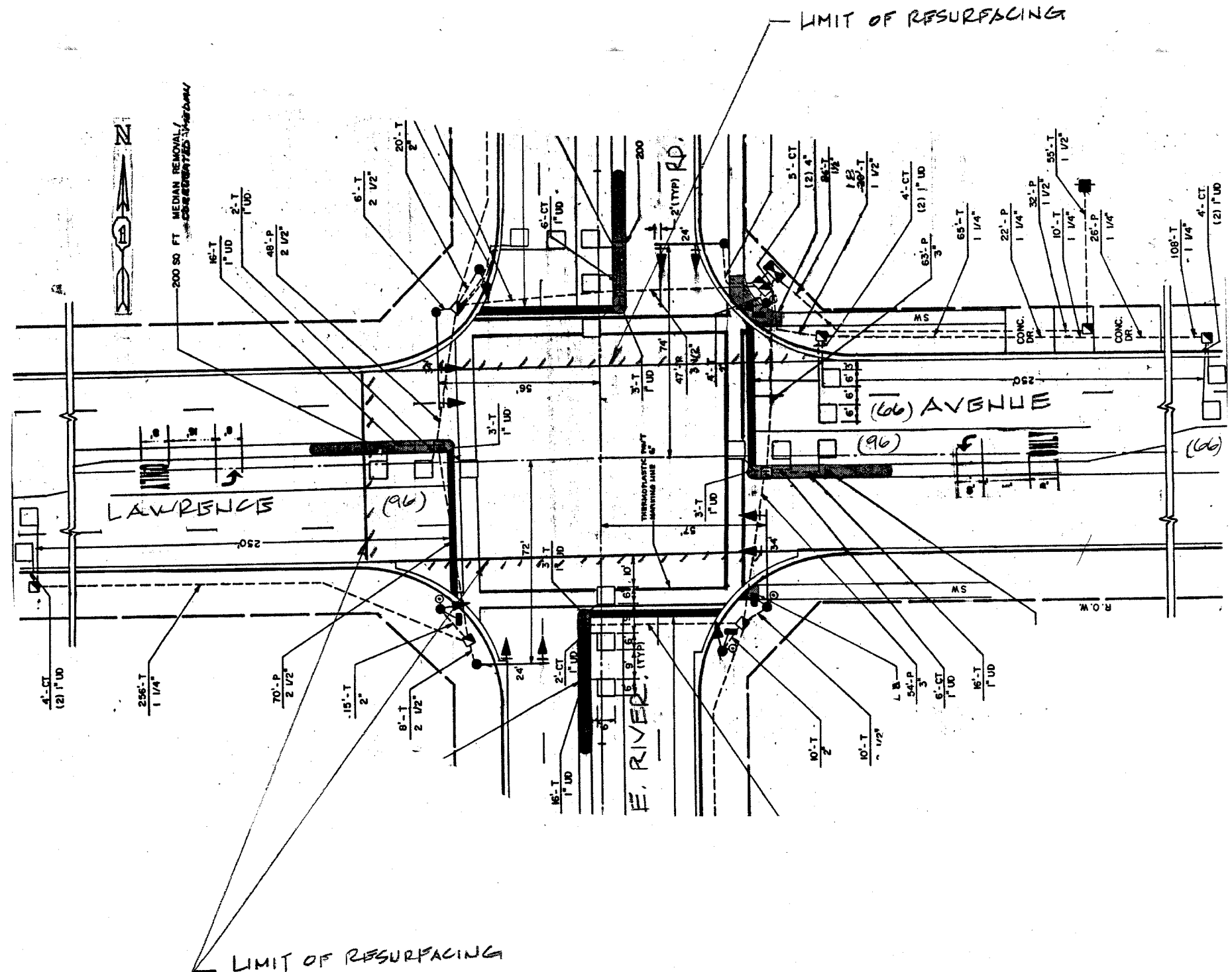
ILLINOIS DEPARTMENT OF TRANSPORTATION
 LAWRENCE AVENUE
 ROADWAY AND PAVEMENT
 MARKING PLAN
 SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 6/8/2005
 DRAWN BY
 CHECKED BY

6/8/2005
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F.A.D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200-RS-2	COOK	22	6
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

62946



TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
SIGNAL HEAD	[Symbol]	[Symbol]
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]
SIGNAL POST	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, STEEL	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]
DOUBLE HANDHOLE	[Symbol]	[Symbol]
G.S. CONDUIT IN TRENCH OR PUSHED	[Symbol]	[Symbol]
PEDESTRIAN PUSHBUTTON DETECTOR	[Symbol]	[Symbol]
DETECTOR LOOP	[Symbol]	[Symbol]
CONCRETE JUNCTION BOX	[Symbol]	[Symbol]
CAST IRON JUNCTION BOX	[Symbol]	[Symbol]
COMMON TRENCH	[Symbol]	[Symbol]

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

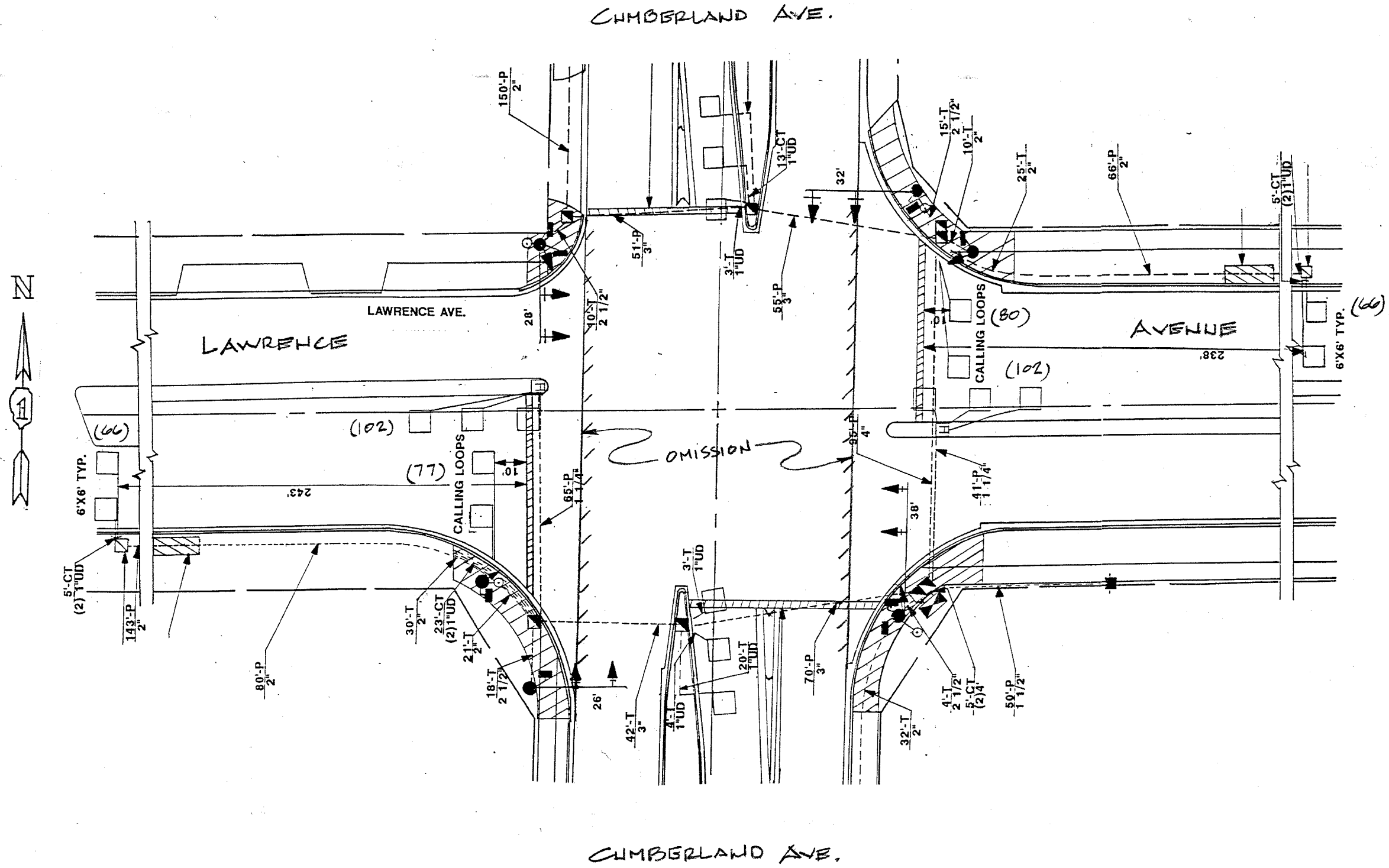
CODE NO.	QUANTITY	UNIT	ITEM
86600600	324	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 LAWRENCE AV. @ E. RIVER RD.
 SCALE: 1" = 20'
 DATE: MAY, 05
 DRAWN BY: J.E.
 DESIGNED BY: J.E.
 CHECKED BY: J.E.

F. A. D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	7
STA.		TO STA.		
FED. ROAD DIST NO. 1		ILLINOIS	FED. AID PROJECT	

#62946



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	493	Foot	Detector Loop Replacement

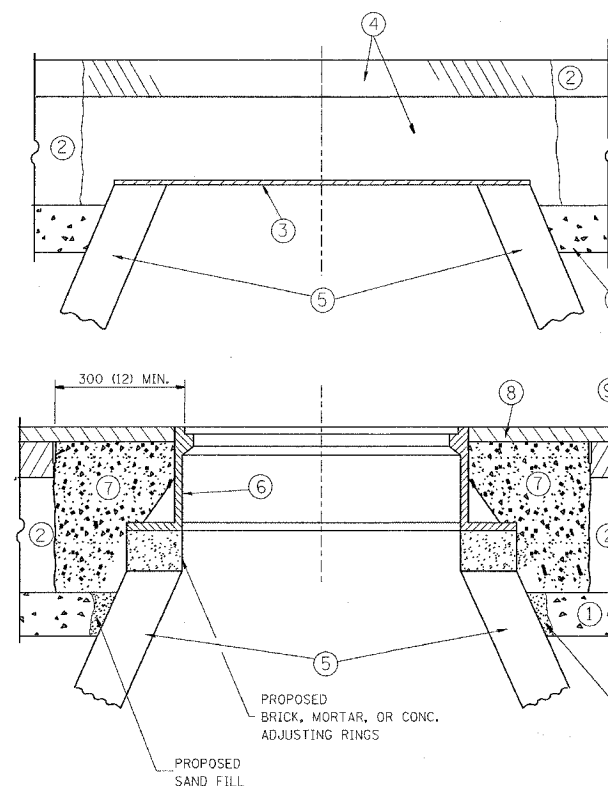
NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
LAWRENCE AVENUE
@
CUMBERLAND AVENUE
SCALE: 1" = 20'
DATE: MAY 05
DRAWN BY: JTE
DESIGNED BY: JTE
CHECKED BY: JTE

REVISIONS	
NAME	DATE

P.L.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 AS-2		22	8
STA.	TO STA.			
FED. ROAD DIST. NO.	ALLEGIS	FED. AID PROJECT		

#62946



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR
FRAMES AND LIDS ADJUSTMENT
WITH MILLING

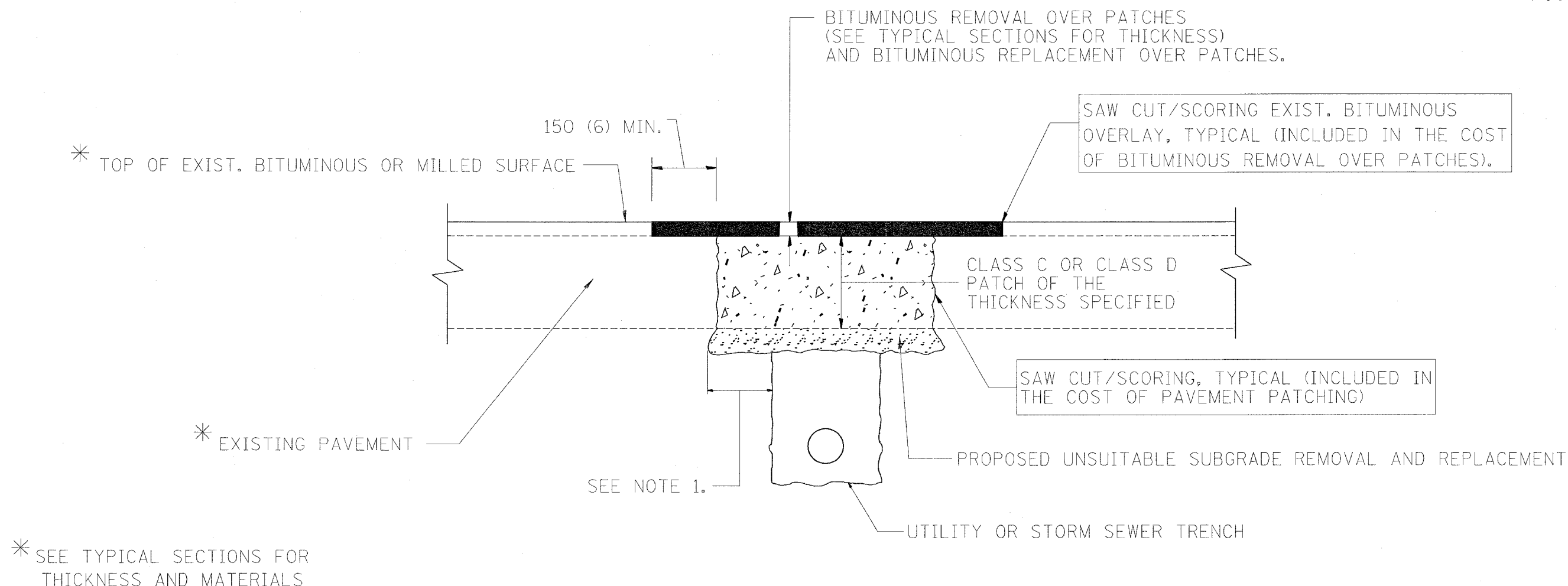
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE
DATE: 5/18/2005

DRAWN BY
CHECKED BY
BD600-03 (BD-8)
REVISION DATE: 05/17/04

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	9
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

#62946



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

SCALE: VERT. DATE 5/18/2005

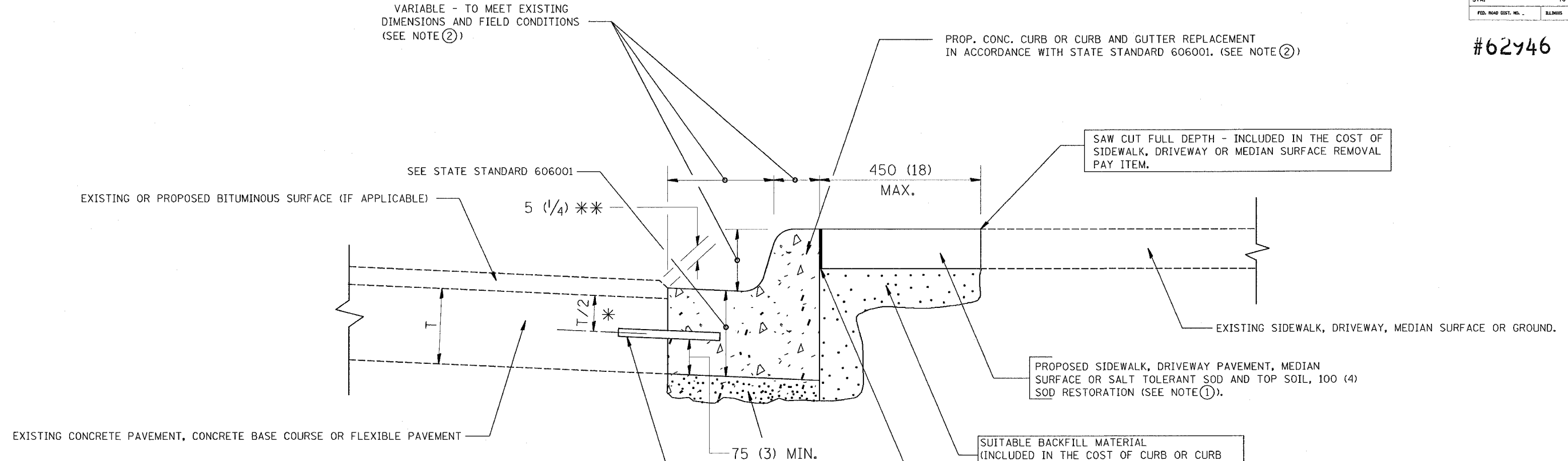
DRAWN BY CHECKED BY

BD400-04 (8D-22)

REVISION DATE: 04/27/98

F. & U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 AS-2	COOK	22	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

#62746



- * 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CURB OR
CURB AND GUTTER
REMOVAL AND REPLACEMENT**

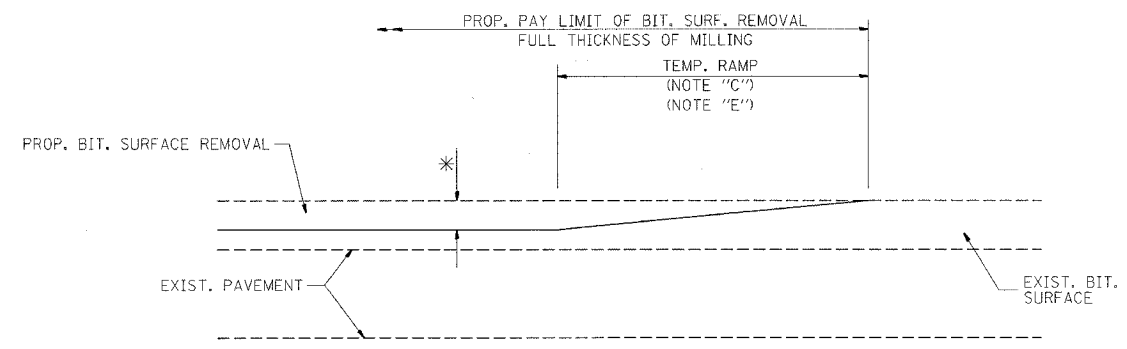
SCALE: NONE
DATE: 5/18/2005
DRAWN BY:
CHECKED BY:
BD600-06 (80-24)

F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 AS-2	COOK	22	11
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

#62946

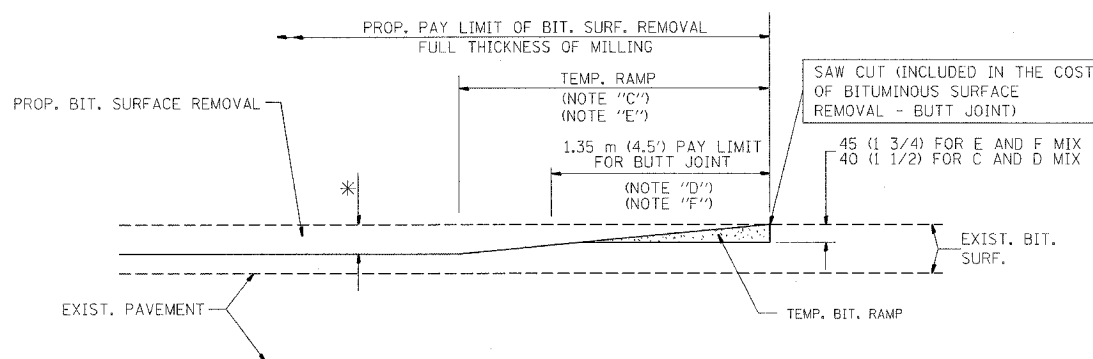
SAW CUT (INCLUDED IN THE COST OF BITUMINOUS SURFACE REMOVAL - BUTT JOINT)

45 (1 3/4) FOR E AND F MIX
40 (1 1/2) FOR C AND D MIX



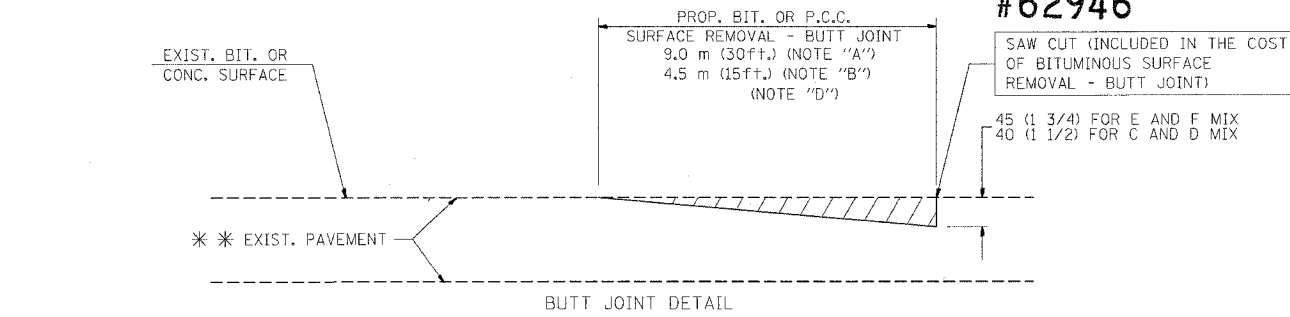
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

OPTION 1

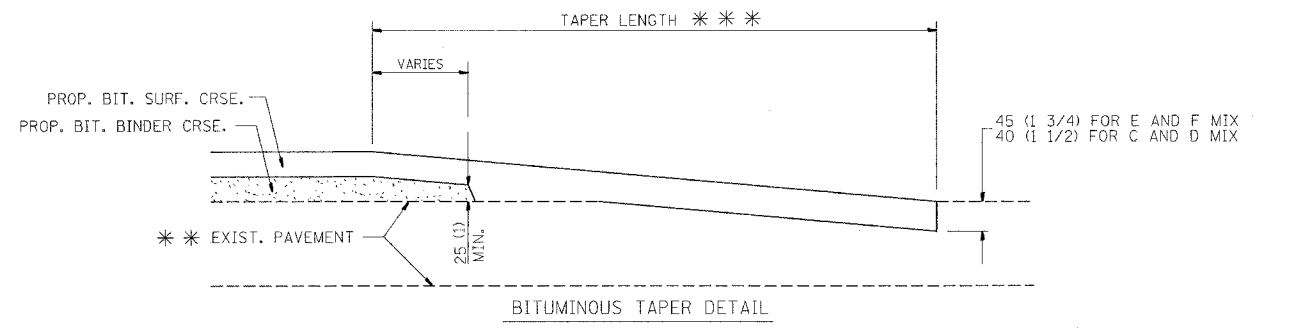


BITUMINOUS CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



BITUMINOUS TAPER DETAIL

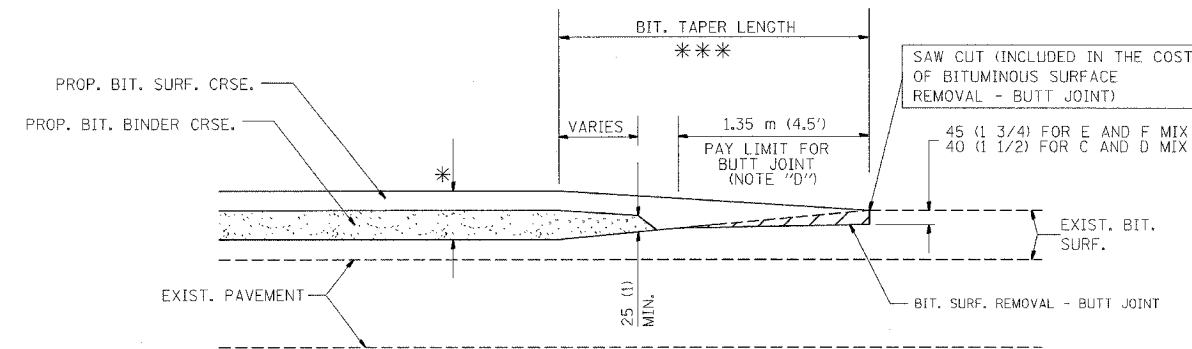
TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR RESURFACING ONLY

** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
- G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR MILLING AND RESURFACING

BASIS OF PAYMENT:
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER
DETAILS

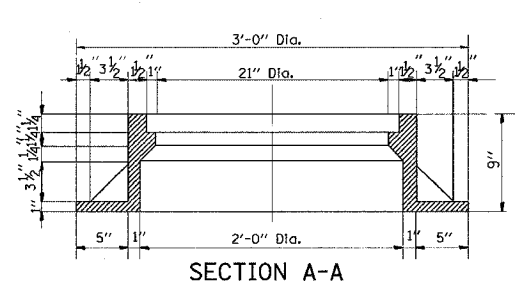
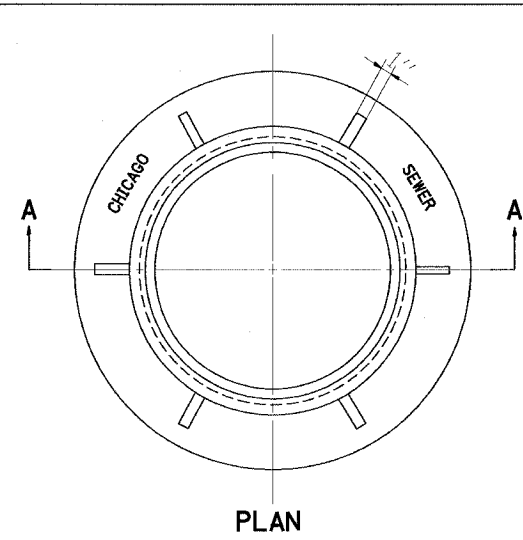
SCALE: NONE
DATE PLOTTED: 5/18/2005

DRAWN BY
CHECKED BY
BD400-05 (V1=BD32)

REVISION DATE: 04/06/01

F.A.L.U. RTL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	3200 RS-Z	COOK	22	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

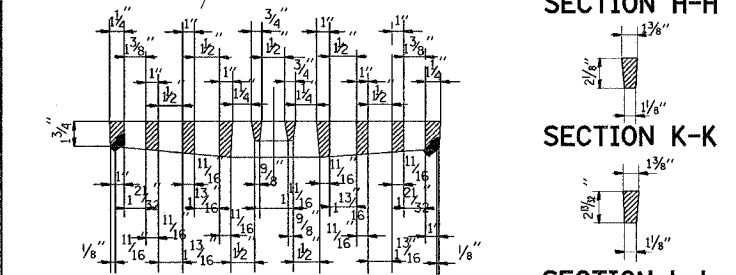
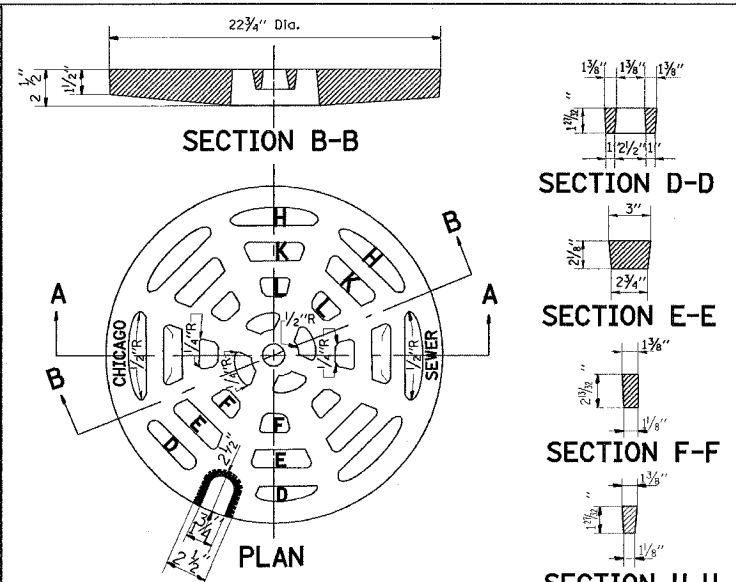
#62946



NOTE: Metal Plates Must Be Furnished For Perforated Lids On Manholes

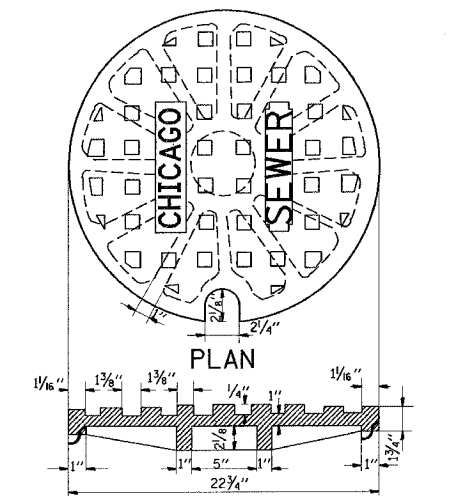
CHICAGO STANDARD MANHOLE FRAME

Scale: 1/2"=1'-0"
Material: Cast Iron



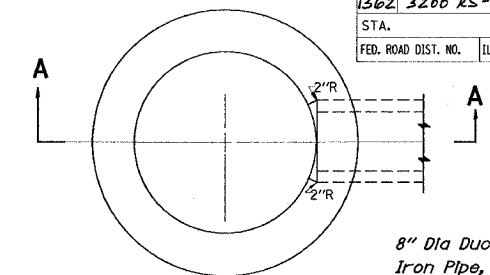
PERFORATED LID FOR CATCH BASINS & MANHOLES

Scale: 2"=1'-0"
Material: Cast Iron

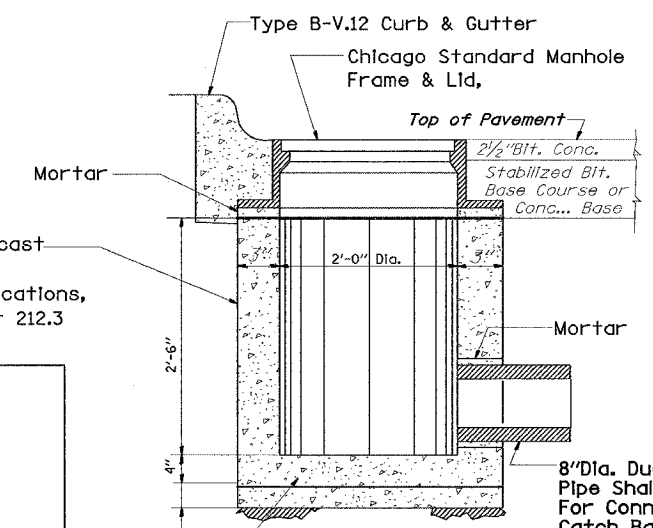


SOLID LID FOR MANHOLES

Scale: NONE
Material: Cast Iron

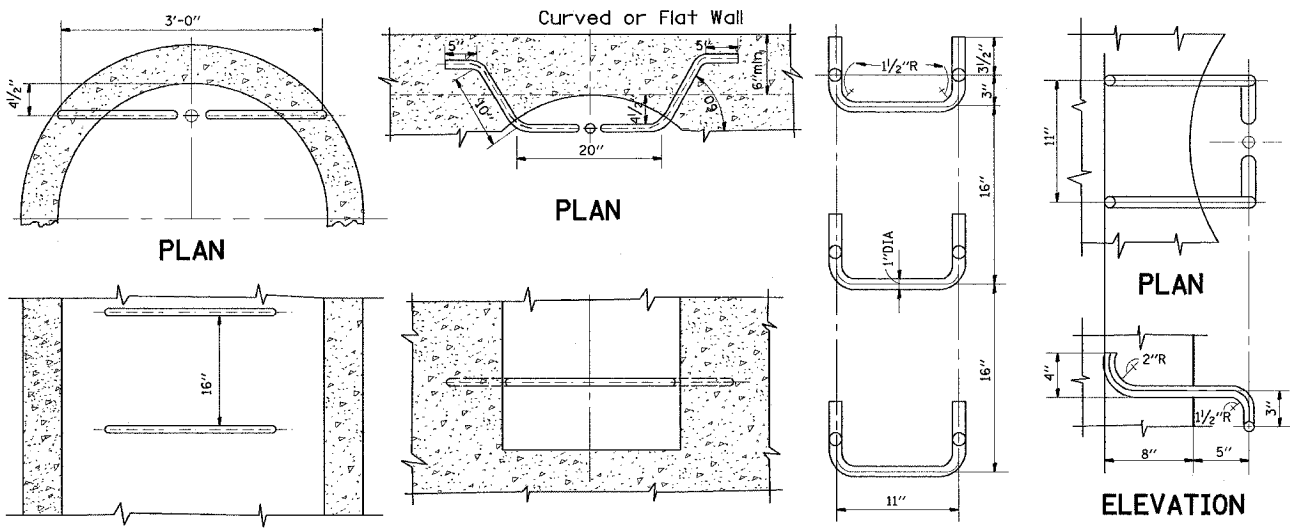


8" Dia Ductile Iron Pipe, Part of Item 76



24" Dia. Precast Conc... Ring
See Specifications, Part 2, Art 212.3

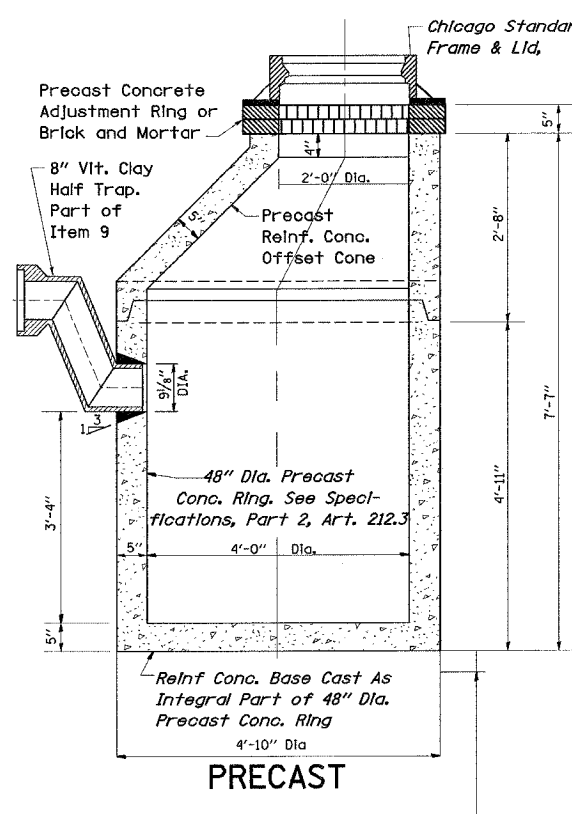
8" Dia. Ductile Iron Pipe Shall Be Used For Connection to Catch Basin Pipe To Be Laid On A Minimum Grade of 1%



STANDARD LADDER RUNGS

HANDHOLD-TYPE Z RUNG
Scale: 1/2"=1'-0"

All Ladder Rungs Shall Be Aluminum or Galvanized Wrought Iron As Specified in Specifications, Part 2, Article 214.2. Rungs Shall Be 1" Diameter or of A Shape Having An Equivalent Cross-Sectional Area



Note: 6" Minimum Granular Embedment Under All Catch Basins

STANDARD CATCH BASINS

Scale: 7/8"=1'-0"
Item 9

STANDARD INLETS

Scale 1"=1'-0"
Item 12
This Inlet Detail Is Sometimes Referred To As "Chicago Standard Inlet, Type A"

NOTE: INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS

REVISIONS	
NAME	DATE
M. GOMEZ	01/25/01

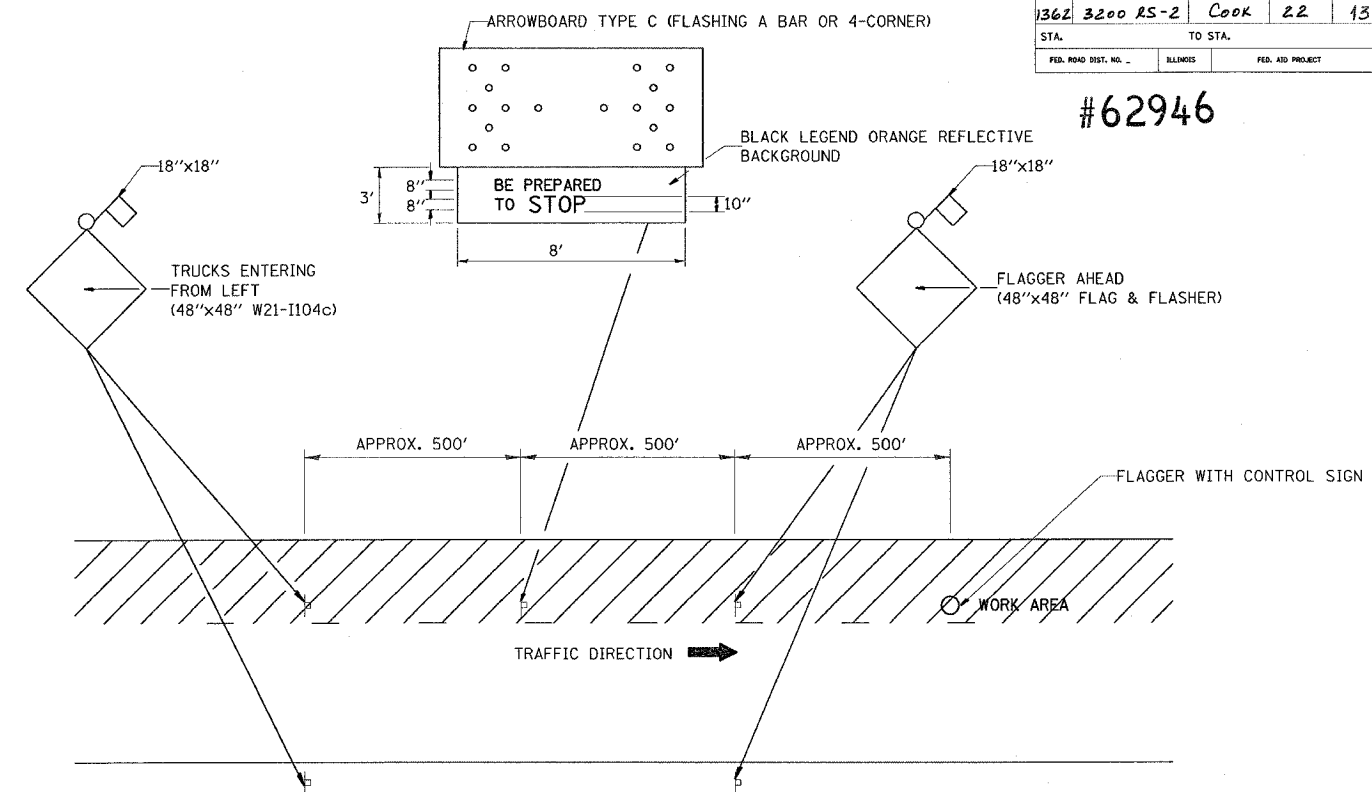
CITY OF CHICAGO
DEPARTMENT OF SEWERS
ENGINEERING DIVISION

ILLINOIS DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO
CATCH BASIN, INLET AND
MANHOLE DETAILS

SCALE: VERT. DATE 5/18/2005
DRAWN BY CHECKED BY
BD600-13 (BD47)
REVISION DATE: 01/25/01

F. & U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
362	3200 RS-2	Cook	22	13
STA.	TO STA.			
FED. ROAD DIST. NO.	BLINDS	FED. AID PROJECT		

#62946



METHOD OF FLAGGING

NOTE:

1. SIGNS SHALL BE MOUNTED AT A MINIMUM CLEARANCE HEIGHT OF 5 FEET
2. ALL SIGNS SHALL BE REMOVED WHEN THE FLAGGING OPERATION CEASES.
3. THIS CASE ALSO APPLIES WHEN THE WORK ZONE IS ON THE RIGHT. UNDER THESE CONDITIONS "TRUCKS ENTERING FROM RIGHT" SIGNS SHALL BE SUBSTITUTED FOR "TRUCKS ENTERING FROM LEFT" SIGNS. ALSO THE ARROWBOARD AND "BE PREPARED TO STOP" SIGNS SHALL BE RELOCATED TO THE RIGHT SIDE OF THE ROAD.
4. WORK ZONE ACCESS POINTS SHOULD BE A MINIMUM OF ONE HALF MILE APART. MEDIAN WORK ZONE ACCESS POINTS SHOULD NOT BE LOCATED OPPOSITE OF EACH OTHER.
5. NIGHTTIME FLAGGING OPERATIONS: THE FLAG STATION SHALL BE LIGHTED WITH ADDITIONAL LIGHTS OTHER THAN STREET LIGHTS. THE FLAGGER CONTROL SIGN AND THE FLAGGER'S VEST SHALL BE REFLECTORIZED. IN ADDITION, THE FLAGGER SHALL HAVE A FLASHLIGHT OR LIGHTED WAND.

REVISIONS	
NAME	DATE
RAY RITCHIE	5/10/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

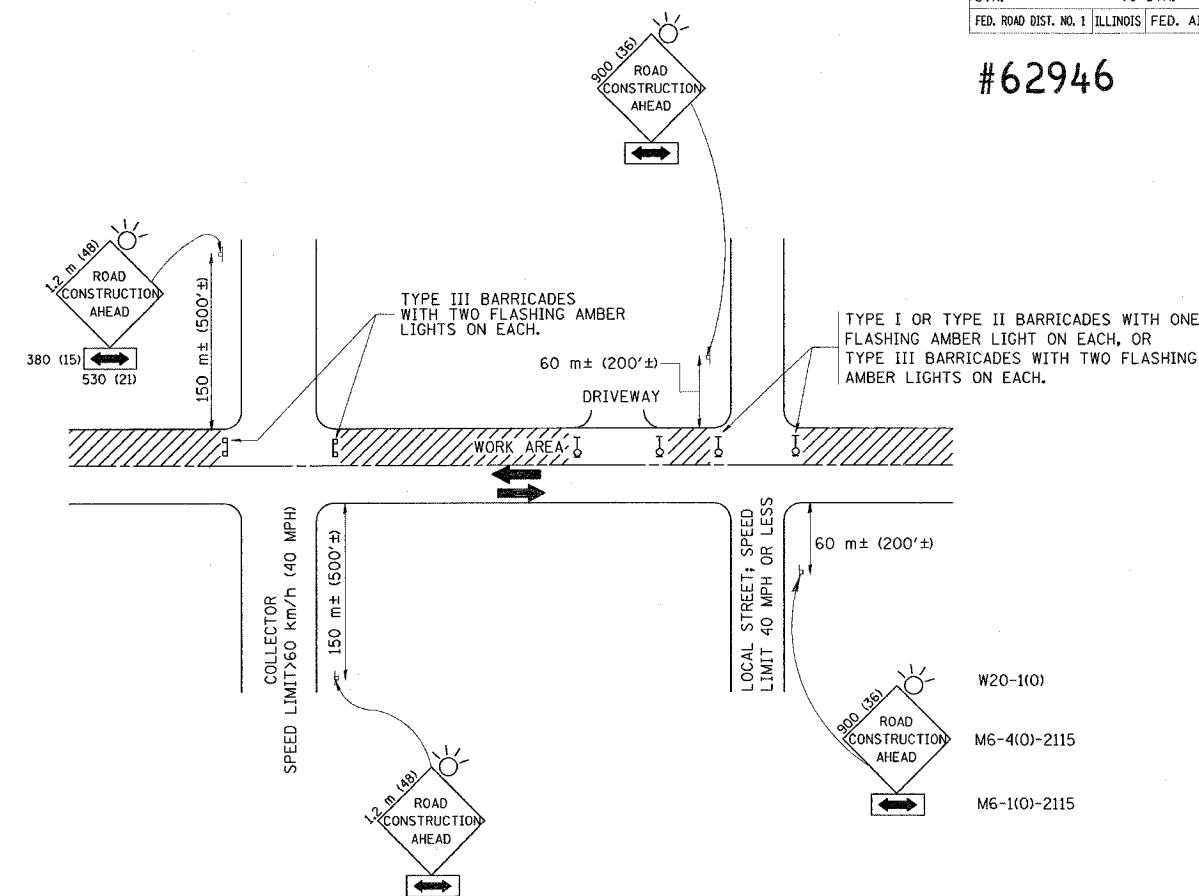
METHOD OF FLAGGING

SCALE: NOT TO SCALE
DATE 5/18/2005

DRAWN BY C.A.D.
CHECKED BY
BM-14
REVISION DATE: 05/10/00

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	Cook	22	14
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

#62946



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

SCALE: VERT.
 HORIZ.
 DATE 5/18/2005

DRAWN BY
 CHECKED BY
 TC-10

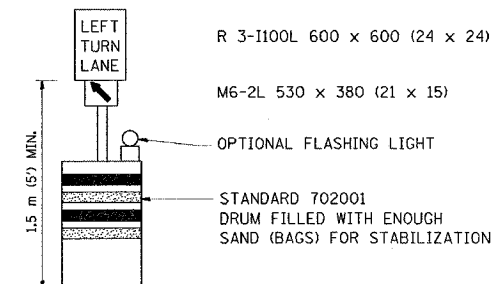
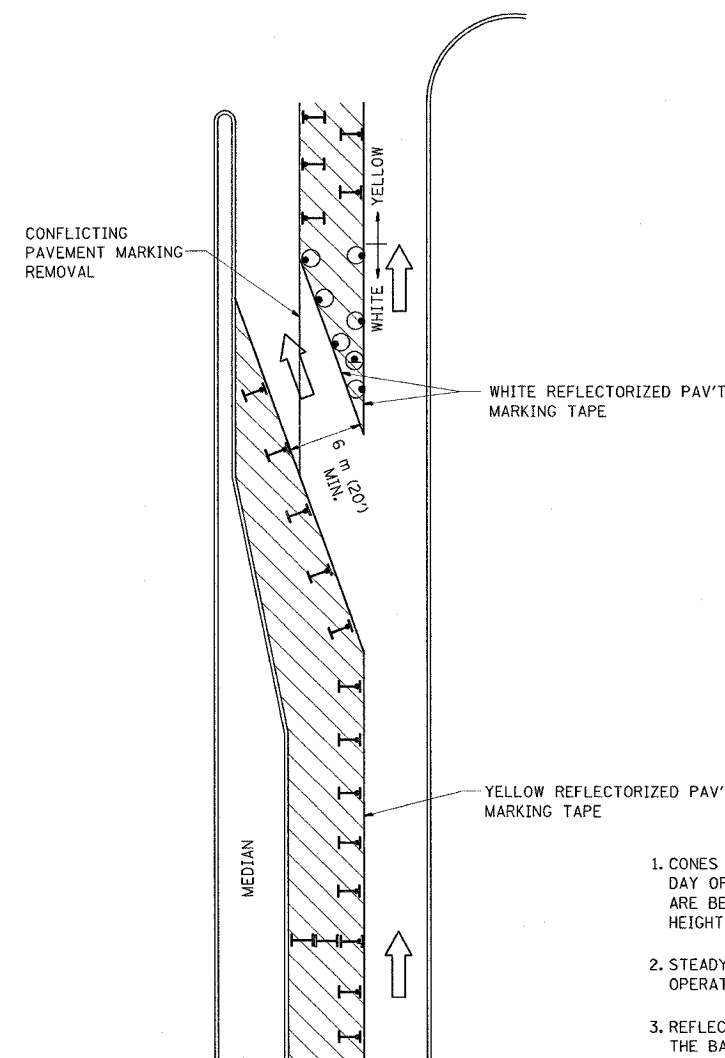
REVISION DATE: 01/06/00

5/18/2005 05:18/2005

5/18/2005
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F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	15
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

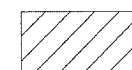





#62946



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

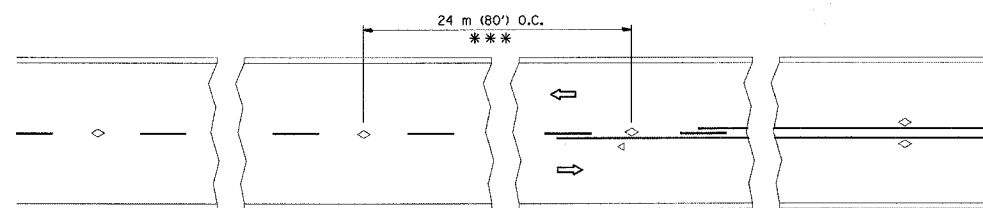
REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

SCALE: NONE
 DATE: 5/18/2005

DRAWN BY
 CHECKED BY LHA
 TC-14

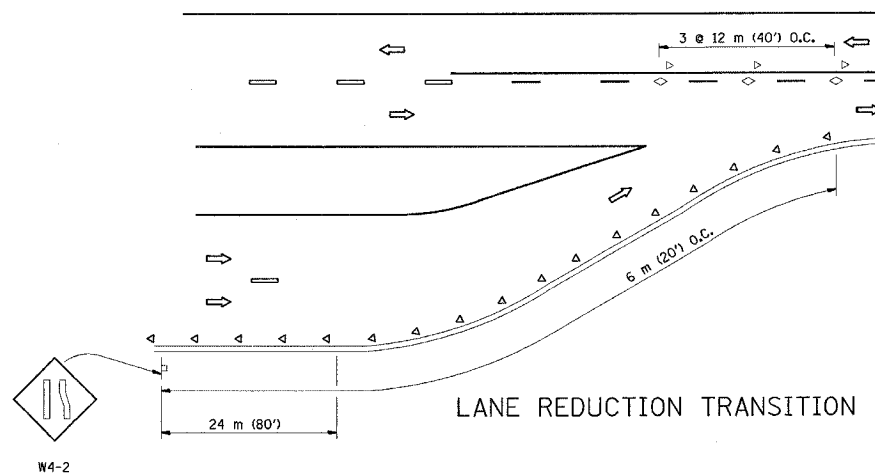
REVISION DATE: 01/06/00

#62946

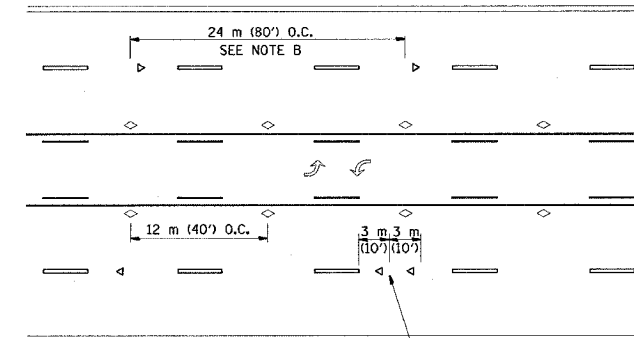


*** REDUCE TO 12 m (40') O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 70 km/h (45 M.P.H.) OR LESS.

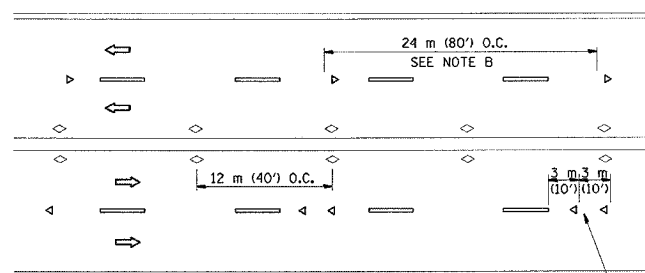
TWO-LANE/TWO-WAY



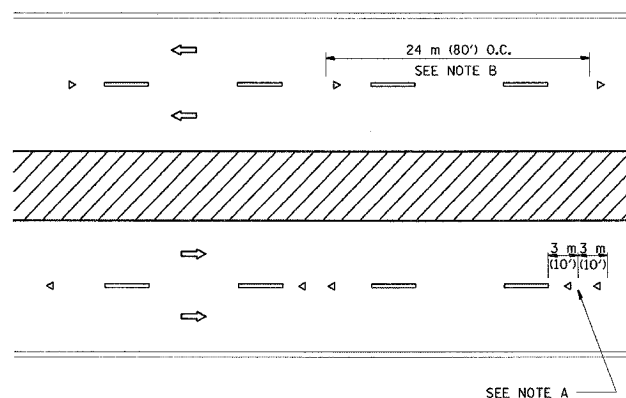
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

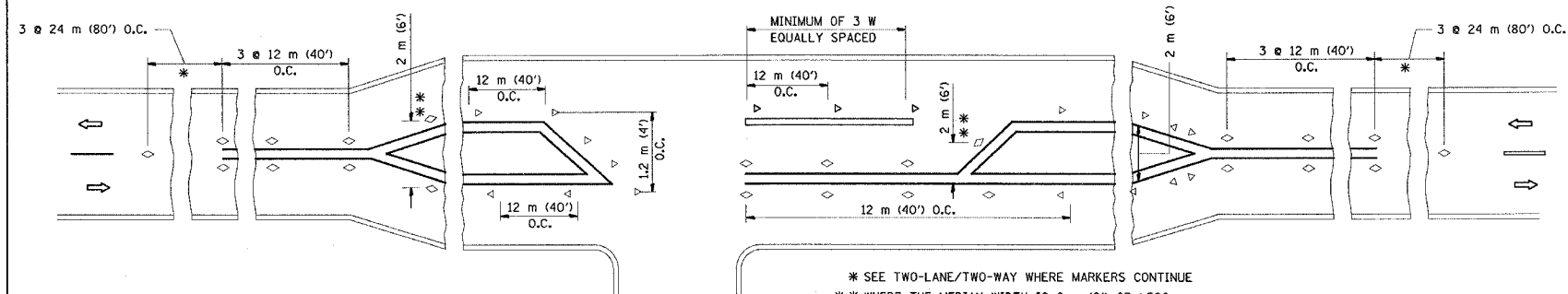
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◊ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.



LEFT TURN

- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

All dimensions are in millimeters (Inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS
(SNOW-PLOW RESISTANT)

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

SCALE: NONE
DATE: 5/18/2005

DRAWN BY CADD
CHECKED BY

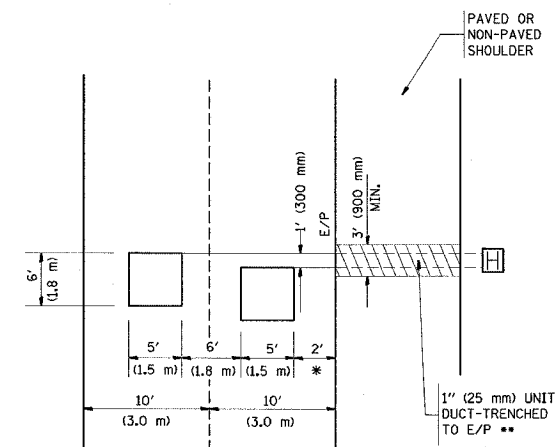
TC-11
REVISION DATE: 01/06/00

F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	Cook	22	17
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

#62946

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

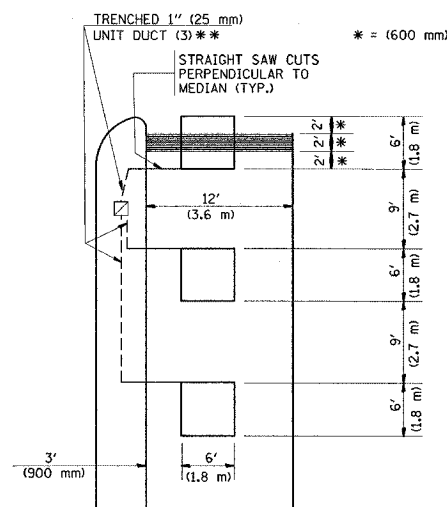


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

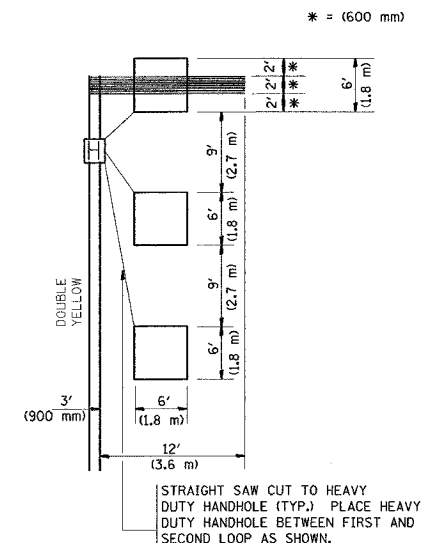
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

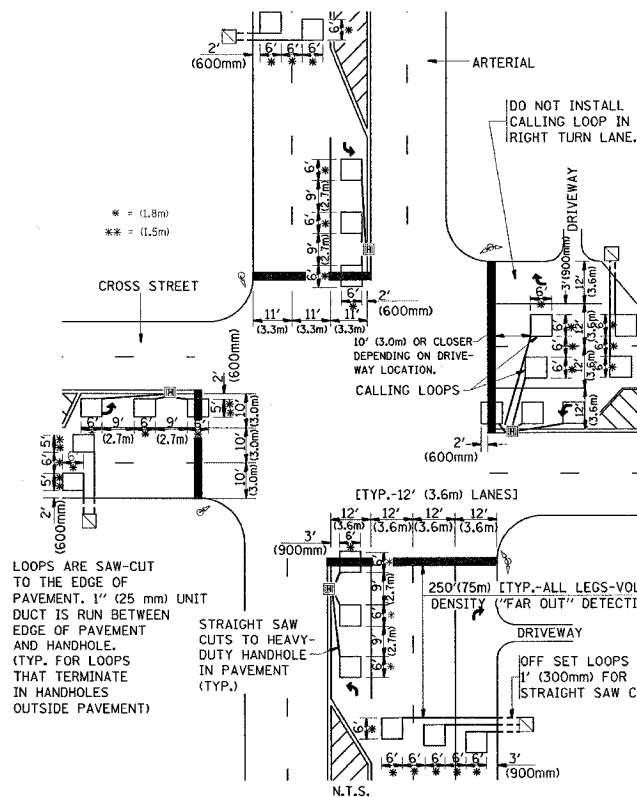
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



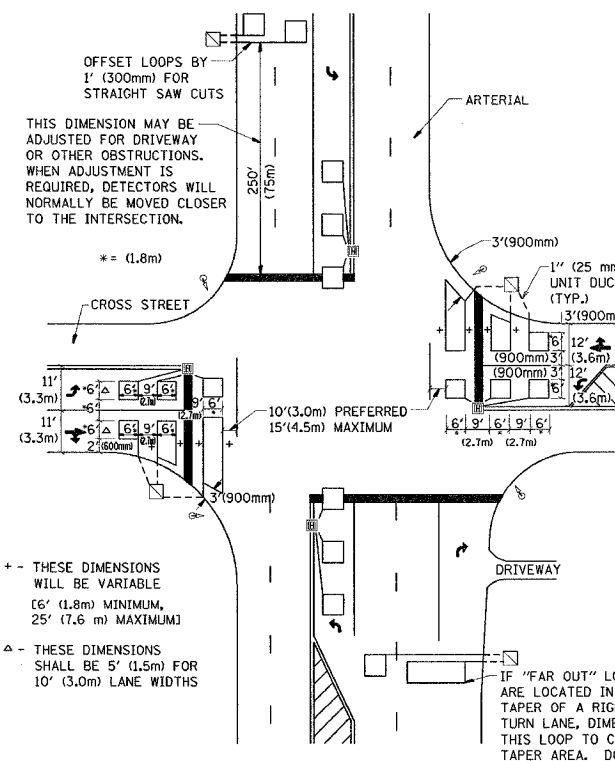
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

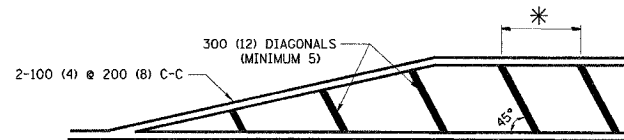
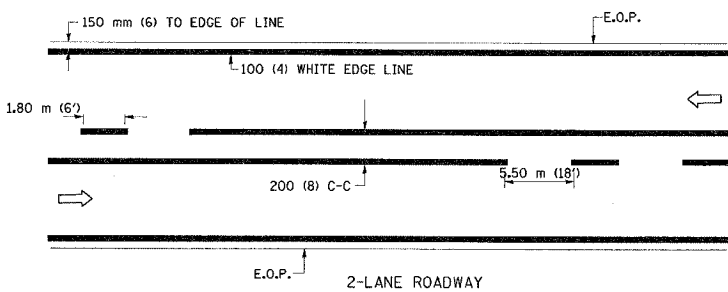
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING

SCALE: NONE
DATE 5/18/2005

DRAWN BY CADD
DESIGNED BY
CHECKED BY R.K.F.
TSOT

REVISION DATE:

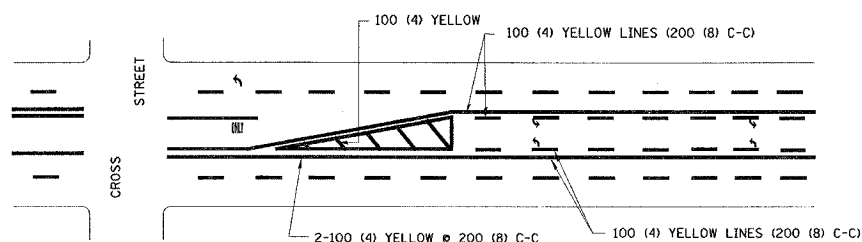
#62946



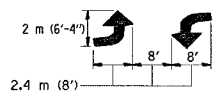
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 6.1 m (20') C-C

PAINTED MEDIANS

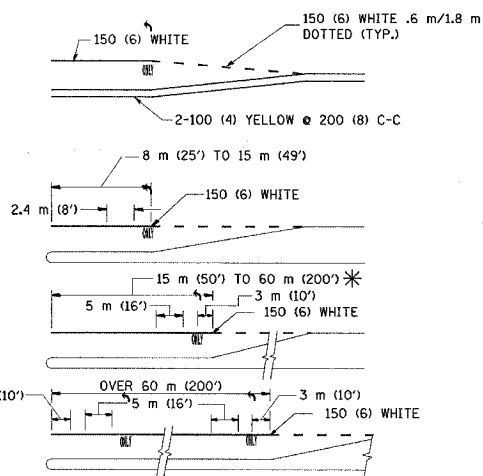


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

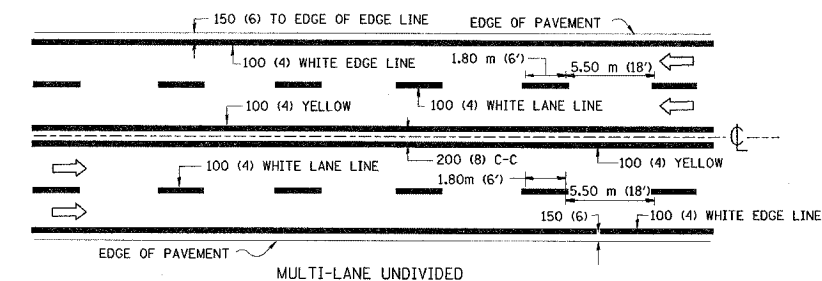


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED. AREA = 1.47 m² (15.8 SQ. FT.) ONLY AREA = 2.13 m² (22.9 SQ. FT.)

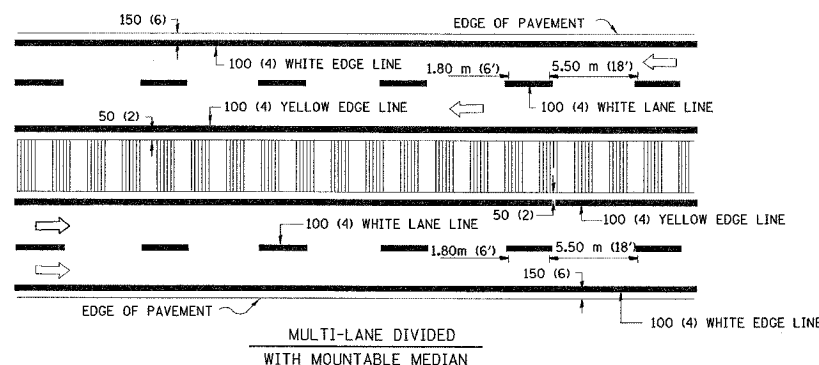
* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



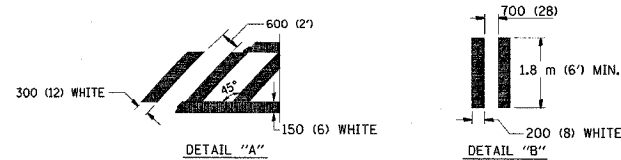
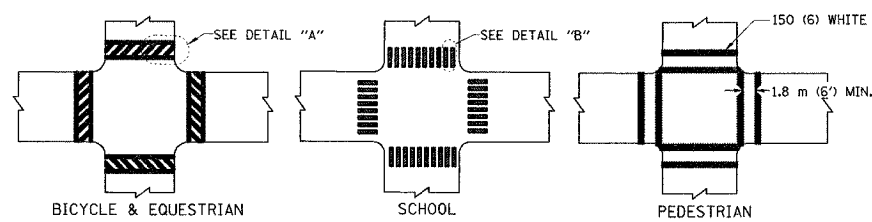
MULTI-LANE UNDIVIDED



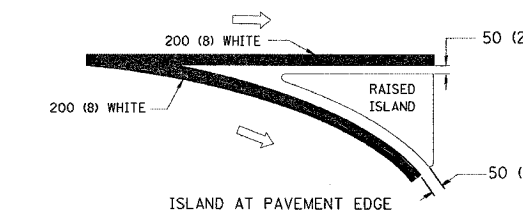
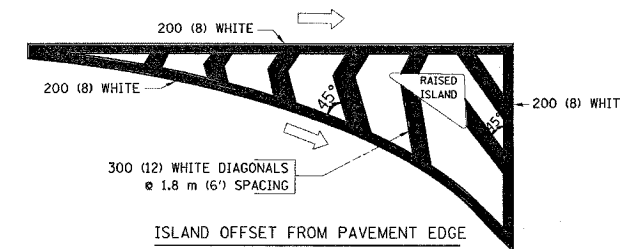
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	1.80 m (6') LINE WITH 5.50 m (18') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	200 (8) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	200 (8) C-C
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	1.80 m (6') LINE WITH 5.50 m (18') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	1.8 m (6') LINE WITH 5.50 m (18') SPACE FOR SKIP-DASH; 200 (8) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 200 (8) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 700 (2'-4") APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	200 (8) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 6.1 m (20') (LESS THAN 50 km/h (30 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT
MARKINGS

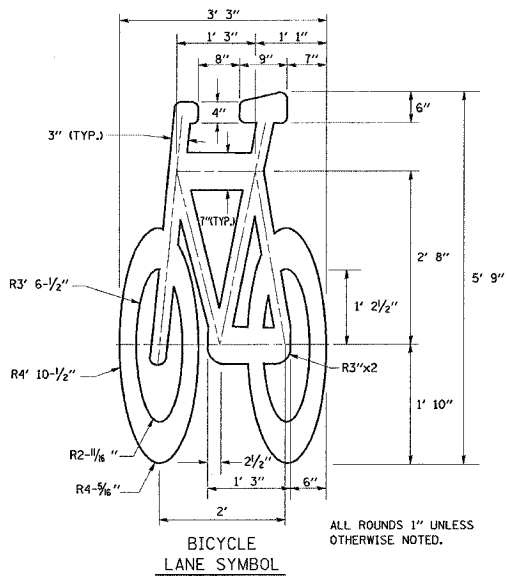
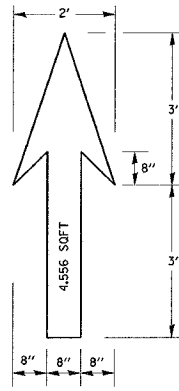
REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

SCALE: NONE
DATE: 5/18/2005

DRAWN BY: CADD
CHECKED BY: TC-24

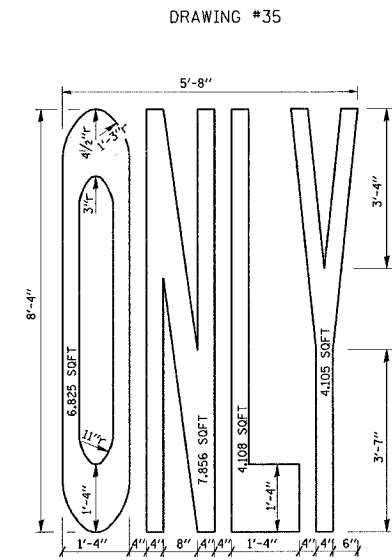
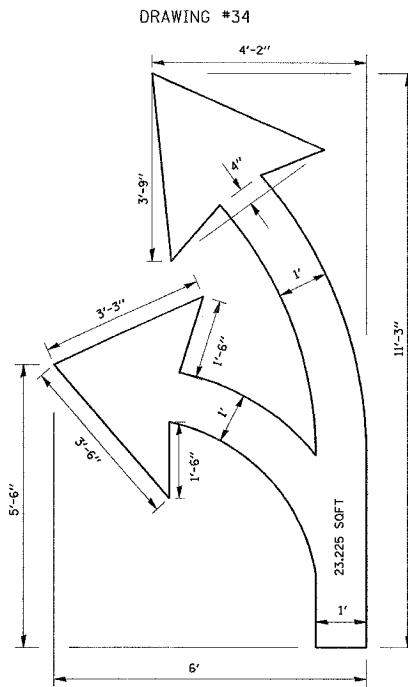
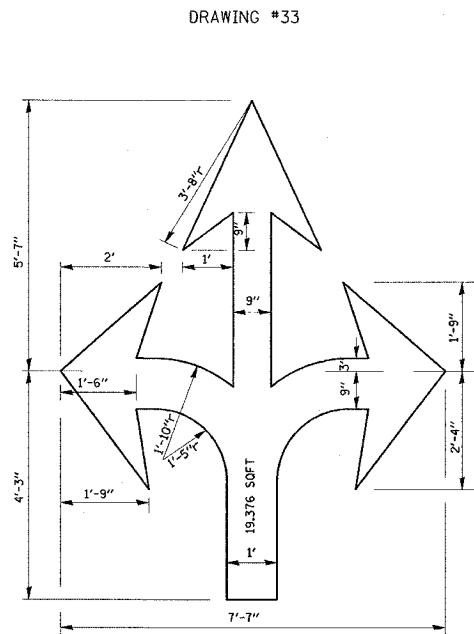
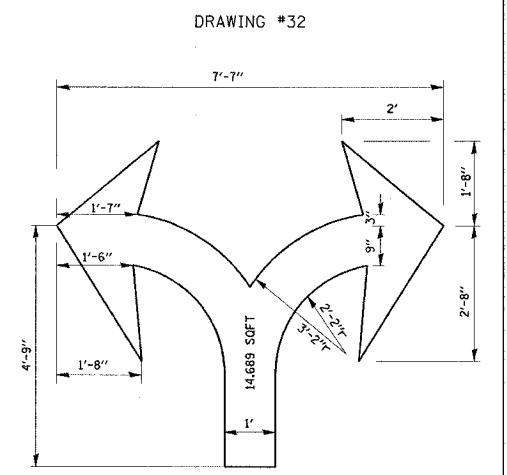
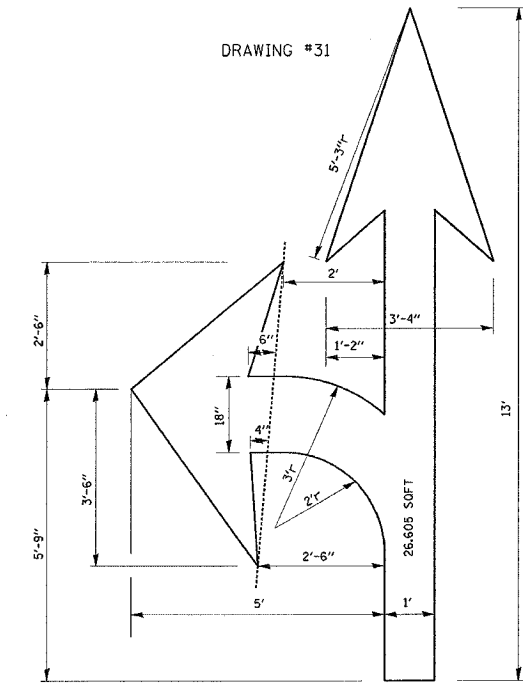
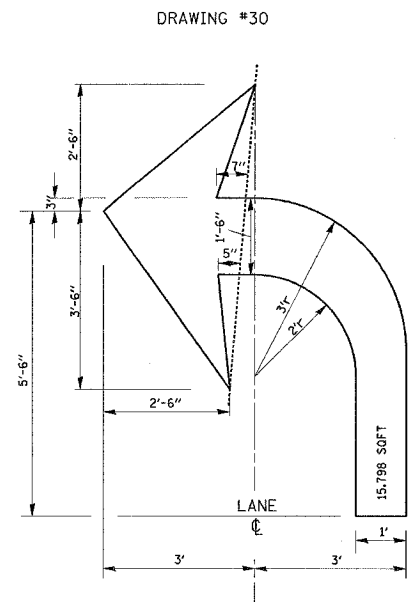
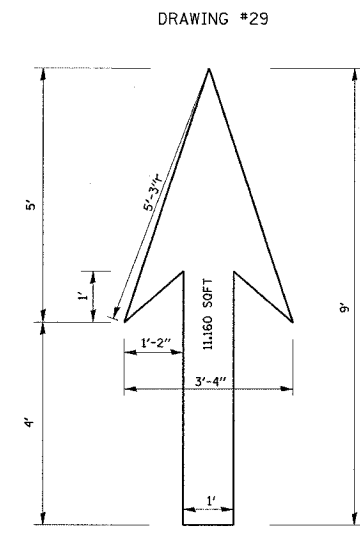
REVISION DATE: 12/07/00

#62946



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE: ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS.

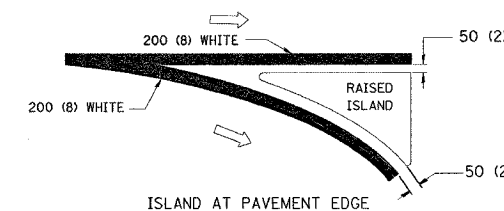
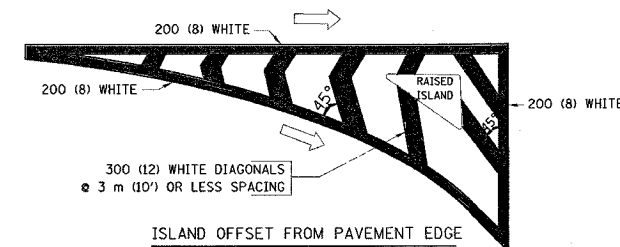
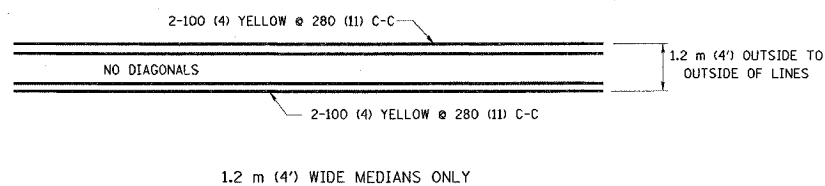
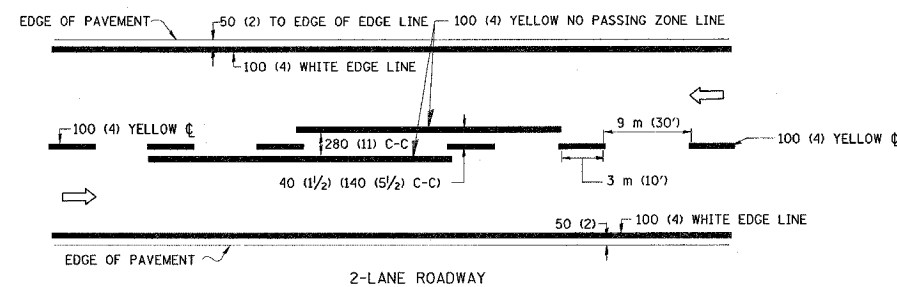
SCALE: NONE
 DATE 5/18/2005

DRAWN BY
 CHECKED BY
 TC-24

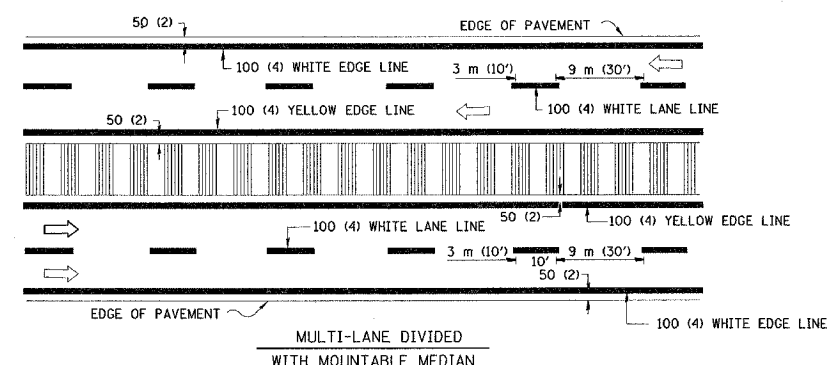
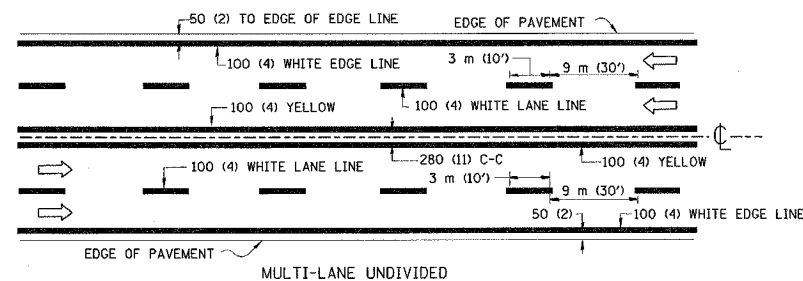
REVISION DATE: 02/25/04

P.A.M. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	COOK	22	20
STA.	TO STA.			
FED. ROAD DIST. NO.	ALIGN.	FED. AID PROJECT		

#62946

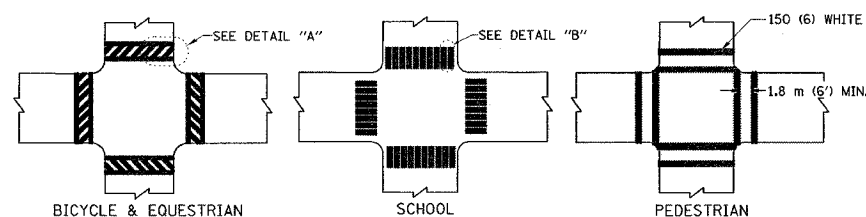


TYPICAL ISLAND MARKING

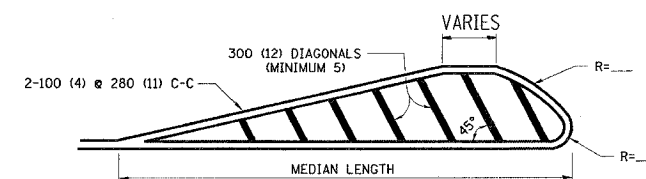


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



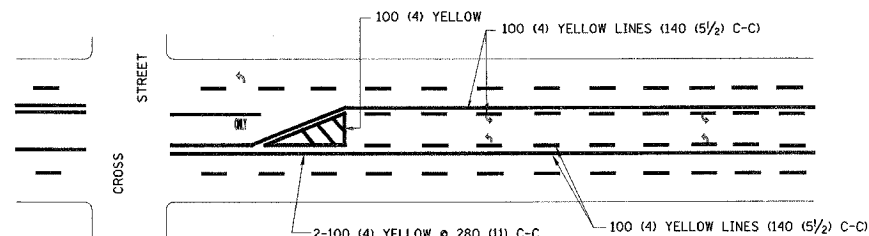
TYPICAL CROSSWALK MARKING



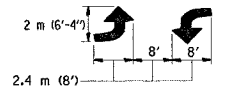
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

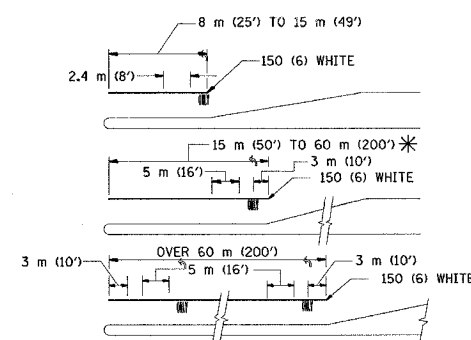


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
AREA = 1.5 m² (15.6 SQ. FT.) ONLY AREA = 1.9 m² (20.8 SQ. FT.)

* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

SCALE: NONE
DATE 5/18/2005

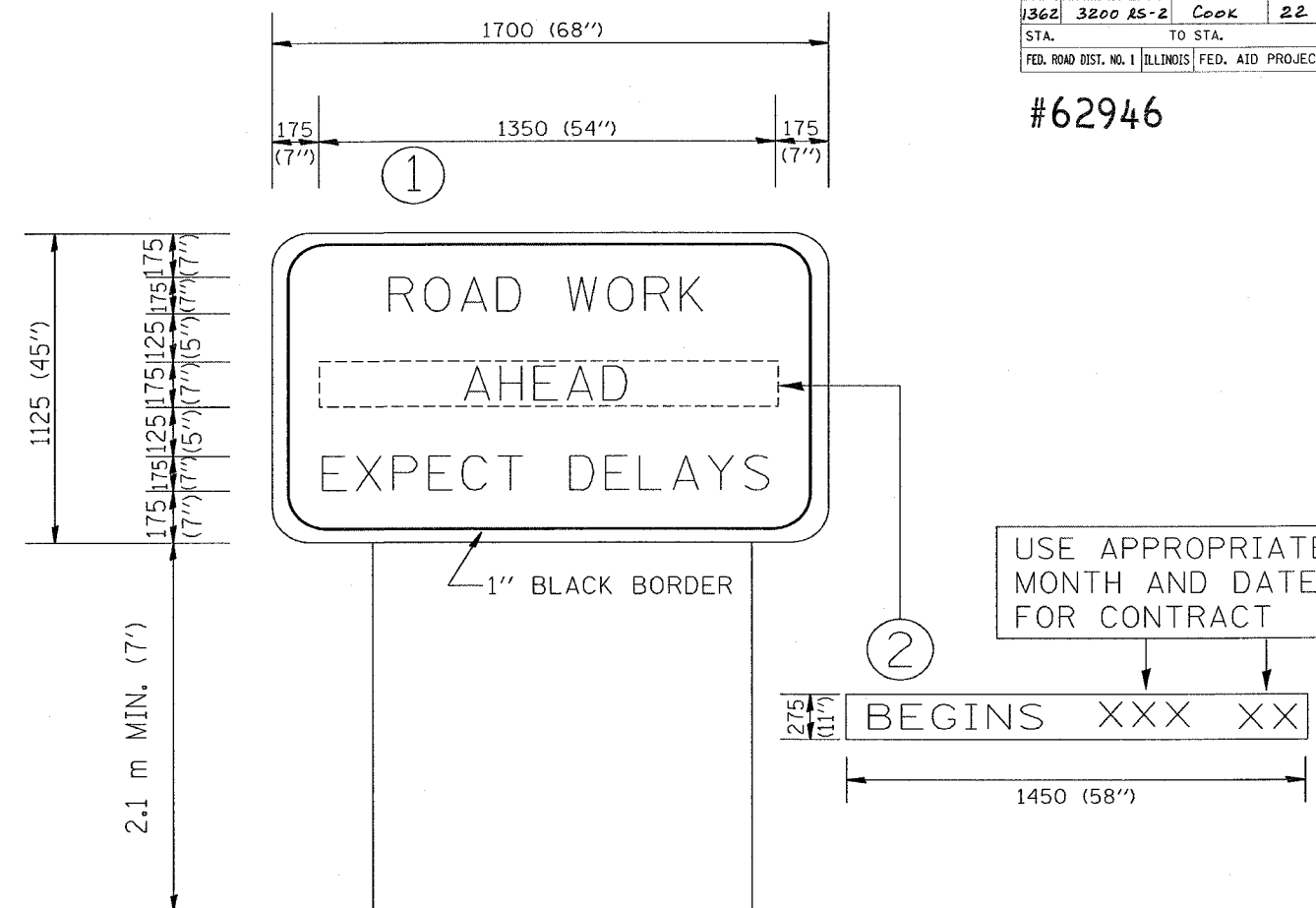
DRAWN BY CADD
CHECKED BY

TC-13

REVISION DATE: 01/06/00

F.A.W. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 RS-2	Cook	22	21
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

#62946



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

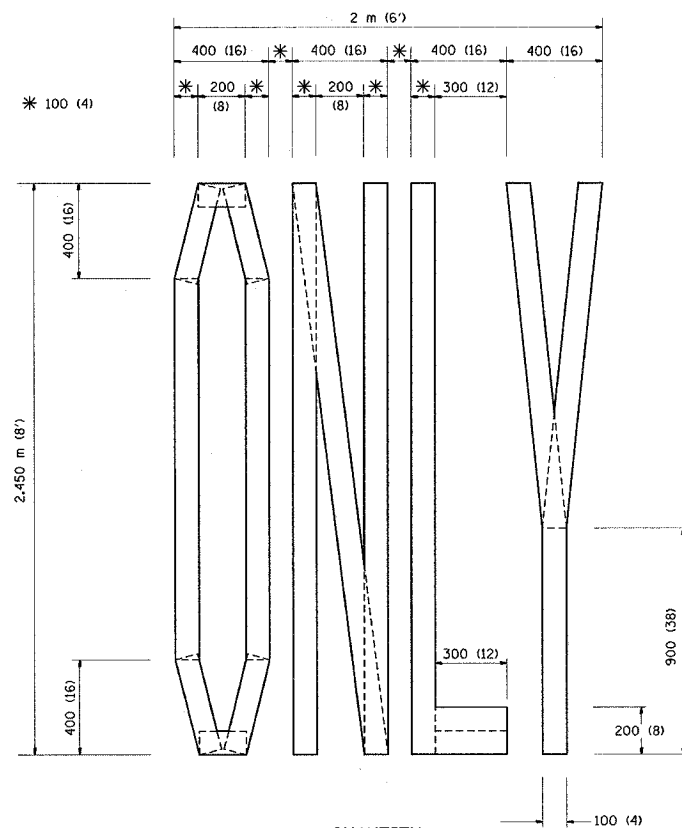
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
R. MIRS	9-15-97	TEMPORARY INFORMATION SIGNING
R. MIRS	2-11-97	
T. RAMMACHER	2-2-99	

SCALE: DATE 5/18/2005 DRAWN BY: BUR. OF DESIGN CHECKED BY

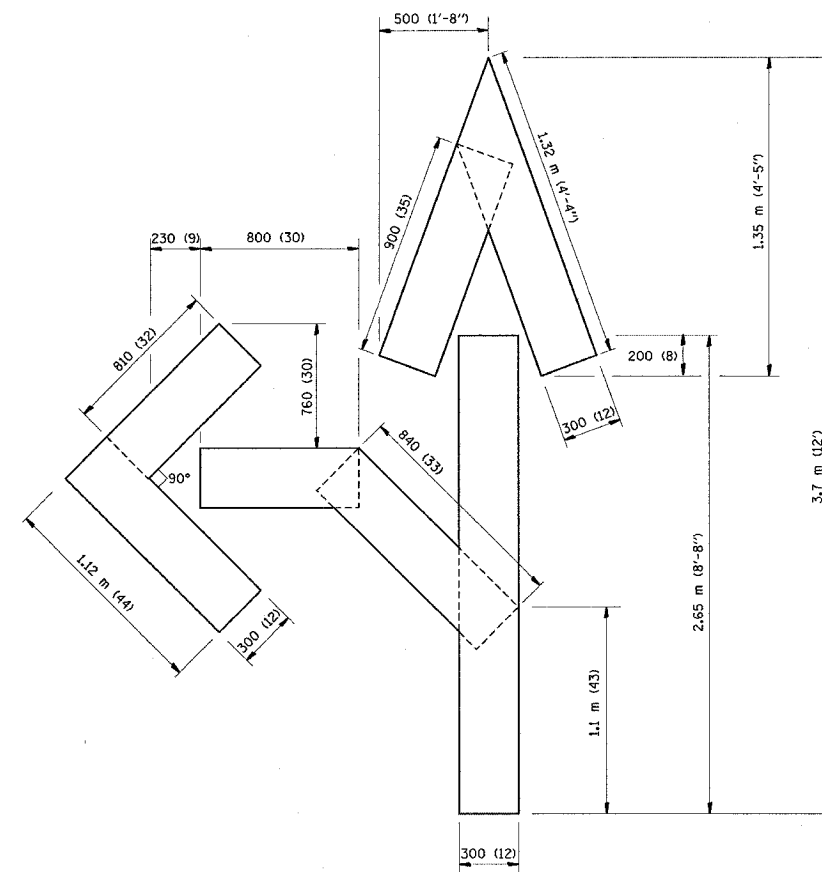
TC22 REVISION DATE: 02/02/99

F. A. U. RFL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1362	3200 AS-2	COOK	22	22
STA.		TO STA.		
FED. ROAD DIST. NO.		BLENDIS	FED. AID PROJECT	

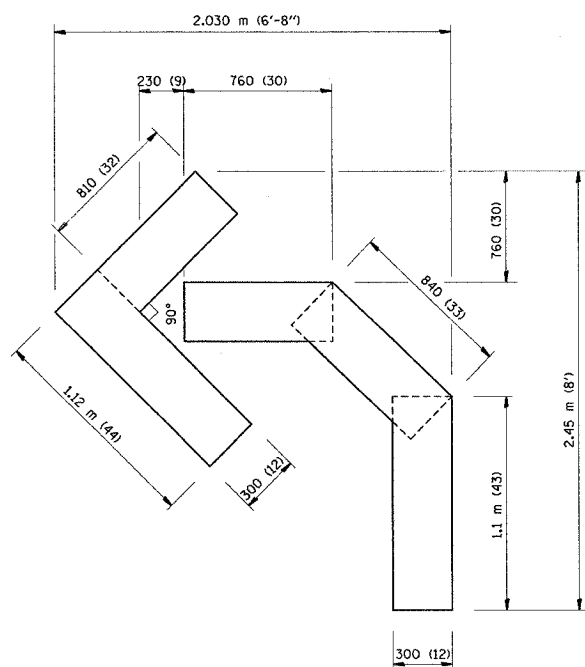
#62946



QUANTITY
100 (4) LINE = 19.7 m (64.1 ft.)
1.97 sq. m (21.1 sq. ft.)



QUANTITY
100 (4) LINE = 25.3 m (82.5 ft.)
2.53 sq. m (27.5 sq. ft.)



QUANTITY
100 (4) LINE = 13.9 m (45.5 ft.)
1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (Inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE
DATE 6/8/2005

DRAWN BY CADD
CHECKED BY
TC-16

REVISION DATE: 08/28/00