

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	1

D-7 JOINT REPAIR 2005-1

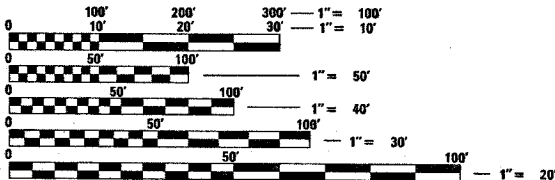
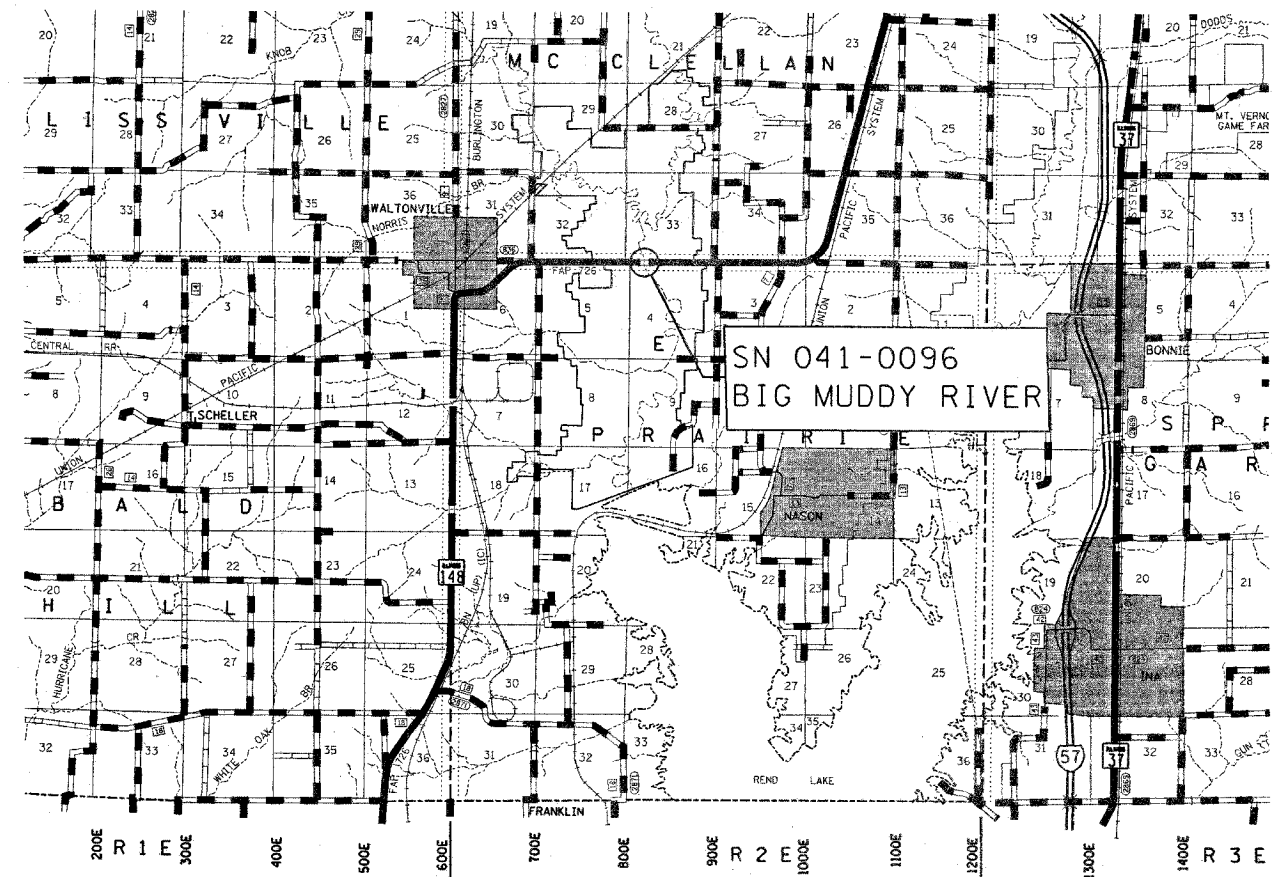
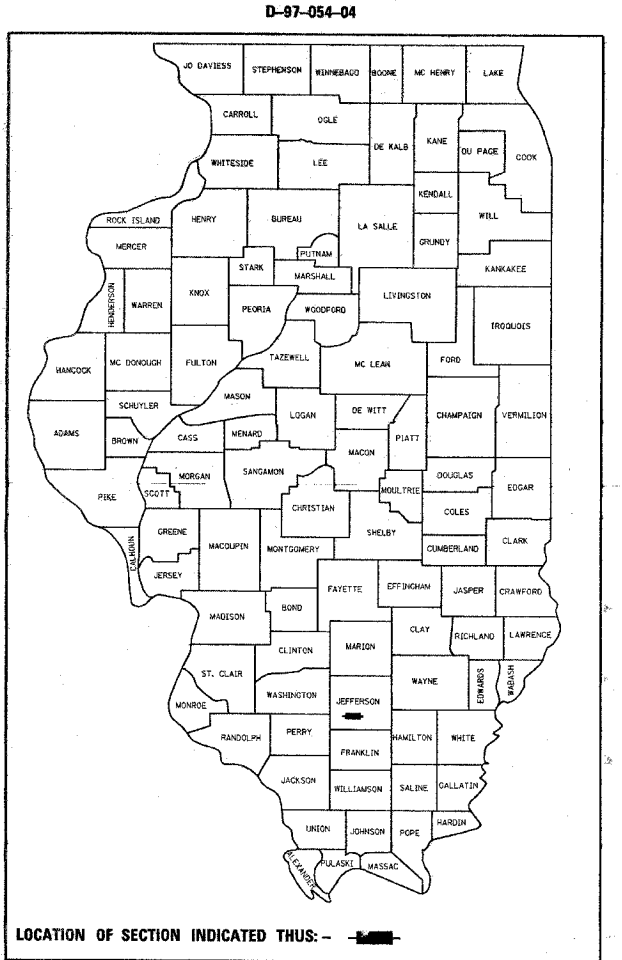
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

FAP ROUTE 726 (IL 148)  
D-7 JOINT REPAIR 2005-1  
JEFFERSON COUNTY

C-97-094-04

SEE SHEET 2 FOR INDEX OF SHEETS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED MARCH 29 2005

*Christine M. Reed*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION FOUR ENGINEER

May 13, 2005  
*Mike Kane*  
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2005  
*Victor Madson*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

PROJECT ENGINEER : MIKE ALLEN  
SQUAD LEADER :  
DESIGNER : ALEC RING  
TELEPHONE : 217/342-3951 EX 316

CONTRACT NO. 74073

2004 ADT = 2650

GROSS LENGTH = 646.7 FT  
NET LENGTH = 646.7 FT

steffenmk  
3/29/2005  
c:\projects\74073d\05404pa.dgn

GEN

CONTRACT NO. 74073

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	2
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\* D-7 JOINT REPAIRS 2005-1

**GENERAL NOTES**

This section shall be constructed in accordance with the plans; the STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, adopted January 1, 2002; the SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, adopted January 1, 2005; and the SPECIAL PROVISIONS included in the proposal.

This project is located south of Mt. Vernon on IL. Route 148 in Jefferson County. The project includes the following structure: Structure Number 041-0096 over the Big Muddy River.

The work included in this project consists of the removal and replacement of the existing neoprene expansion joints, pavement striping, and all other work necessary to complete this section.

The reflective lenses of all raised reflective markers located between the stop bars and the end of the approach pavements shall be removed prior to stage I construction. This work will be paid for at the contract unit price for RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL. Replacement of the bi-directional amber reflectors at the completion of stage 2 will be paid for at the contract unit price for REPLACEMENT REFLECTOR.

The quantities of paint pavement marking line were computed as follows:

144	Feet (yellow)
1798	Feet (white)
1942	Feet (Total)

**INDEX OF SHEETS**

SHEET NO.      DESCRIPTION

- 1      COVERSHEET
- 2      GENERAL NOTES & INDEX OF SHEETS
- 3      SUMMARY OF QUANTITY
- 4      QUANTITY SCHEDULES
- 5      GENERAL PLAN AND ELEVATION
- 6      STAGE CONSTRUCTION DETAILS
- 7-8    EXPANSION JOINT DETAILS
- 9      NEOPRENE EXPANSION JOINTS
- 10     BAR SPLICER ASSEMBLY DETAIL

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 10:

- 000001-04      STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001      AREAS OF REINFORCEMENT BARS
- 001006      DECIMAL OF AN INCH AND OF A FOOT
- 701006-02      OFF-ROAD OPERATIONS, 2L 2W, 15' TO 24" AWAY, SPEEDS > 45 MPH
- 701011-01      OFF-ROAD MOVING OPERATIONS, 2L 2W, DAY ONLY, FOR SPEEDS > 45 MPH
- 701201-02      LANE CLOSURE, 2L 2W, DAY ONLY, ON-ROAD TO 600 MM OFF-ROAD, SPEEDS > 45 MPH
- 701301-02      LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
- 701311-02      LANE CLOSURE, 2L 2W, MOVING DAY ONLY OPERATIONS
- 701316-03      LANE CLOSURE, 2L 2W, BRIDGE REPAIR, FOR SPEEDS > 45 MPH
- 701326-02      LANE CLOSURE, 2L 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
- 702001-05      TRAFFIC CONTROL DEVICES
- 720001      SIGN PANEL MOUNTING DETAILS
- 720006      SIGN PANEL ERECTION DETAILS
- 780001-01      TYPICAL PAVEMENT MARKINGS
- 781001-02      TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS
- 886001      DETECTOR LOOP INSTALLATIONS
- 886006      TYPICAL LAYOUT FOR DETECTION LOOPS

PLOT DATE = 3/29/2005  
FILE NAME = #FILE#  
PLOT SCALE = #SCALE#  
REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

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DATE      HORIZ.

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	•	JEFFERSON	10	3
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\* D-7 JOINT REPAIRS 2005-1

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		SFTY-2A		
50102400	CONCRETE REMOVAL	CU YD	34.1	34.1		
50300155	NEOPRENE EXPANSION JOINT 2 1/2"	FOOT	68	68		
50300160	NEOPRENE EXPANSION JOINT 4"	FOOT	34	34		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	34.3	34.3		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	6296	6296		
51500200	RELOCATING NAME PLATES	EACH	1	1		
67100100	MOBILIZATION	L SUM	1	1		
70100100	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316	EACH	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	4	4		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	57	57		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5940	5940		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1386	1386		
*78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1942	1942		
*78100300	REPLACEMENT REFLECTOR	EACH	8	8		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	637	637		
X0322050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	8	8		
Z0002600	BAR SPLICERS	EACH	40	40		

\*SPECIALTY ITEMS

REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUMMARY OF QUANTITIES**  
 SCALE: VERT.    DRAWN BY  
 DATE    HORIZ.    CHECKED BY

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CONTRACT NO. 74073

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	4
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-7 JOINT REPAIRS 2005-1				

BRIDGE REPAIR SCHEDULE

	LENGTH	CONCRETE REMOVAL		REINFORCEMENT BARS, EPOXY COATED	BAR SPLICERS	NEOPRENE EXPANSION JOINT 2 1/2"	NEOPRENE EXPANSION JOINT 4"
041-0096	FEET	CU FT	CU FT	POUND	EACH	FEET	FEET
Stage I	646.7	17.07	17.14	3148	40	34	17
Stage II		17.07	17.14	3148		34	17
TOTAL	646.7	34.1	34.3	6296	40	68	34

PAVEMENT MARKING SCHEDULE

	PAINT PAVEMENT MARKING - LINE 4"	PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 4"	SHORT TERM PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	RAISED REFLECTOR PAVEMENT MARKER, REFLECTOR REMOVAL	REPLACEMENT REFLECTOR
041-0096	FEET	SQ FT	FEET	FEET	SQ FT	EACH	EACH
TOTAL	1942	637	5940	57	1386	8	8

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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**QUANTITY SCHEDULES**

SCALE: VERT. HORIZ.  
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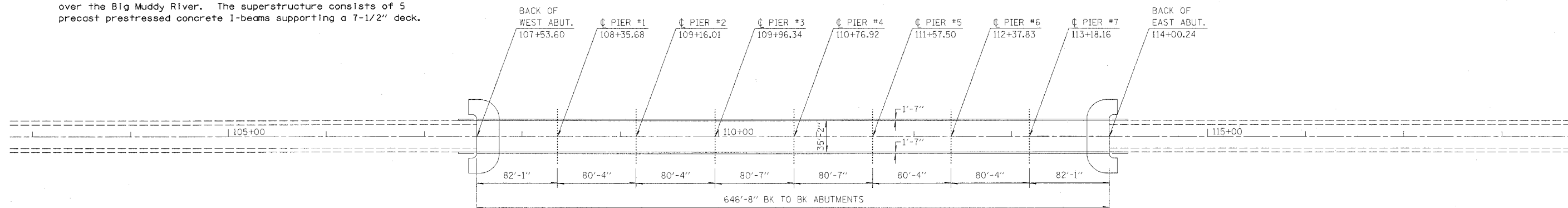
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



D-T JOINT REPAIRS 2005-1

Structure 041-0096 was constructed in 1980 as an 8 span bridge over the Big Muddy River. The superstructure consists of 5 precast prestressed concrete I-beams supporting a 7-1/2" deck.



PLAN

GENERAL NOTES

Prior to pouring the new concrete deck, all loose rust, loose mill scale, and other loose potentially detrimental foreign material shall be removed from the surfaces of the beams or girders in contact with concrete. The cost of this work will be included in the pay item covering removal of the existing concrete. All heavy rust and other tightly adhered potentially detrimental foreign matter shall be removed from the surfaces of the beams or girders in contact with concrete. Tightly adhered paint may remain unless otherwise noted. This removal shall be accomplished by methods that will not damage the steel. The cost of the work will be paid for according to Article 109.04 of the Standard Specifications.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beams.

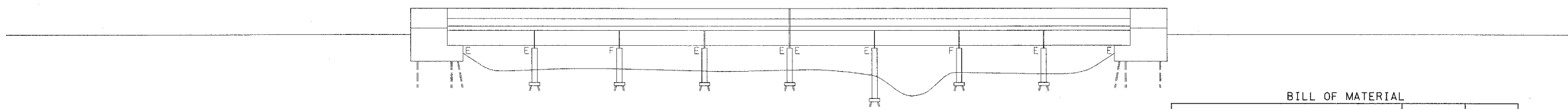
The removal and reinstallation of the first section of guardrail at each abutment to allow construction of the neoprene expansion joint shall be included in the contract unit price for concrete superstructure.

Reinforcement bars shall conform to the requirements of AASHTO M-31, OR M322 Grade 60.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work; however, the contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars shall be cleaned and <sup>STRAIGHTENED</sup> incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system (cost included in CONCRETE REMOVAL).

Joint openings shall be adjusted according to Article 503.10(c) of the STANDARD SPECIFICATIONS when the deck is poured at an ambient temperature other than 50 degrees Fahrenheit.



ELEVATION

BILL OF MATERIAL

Concrete Removal	CU YD	34.1
Neoprene Expansion Joint 2 1/2"	FOOT	68
Neoprene Expansion Joint 4"	FOOT	34
Concrete Superstructure	CU YD	34.1
Reinforcement Bars, Epoxy Coated	POUND	6296
Relocating Name Plates	EACH	1
Bar Splicers	EACH	40

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL PLAN  
AND ELEVATION  
STR #041-0096**

SCALE: VERT.  
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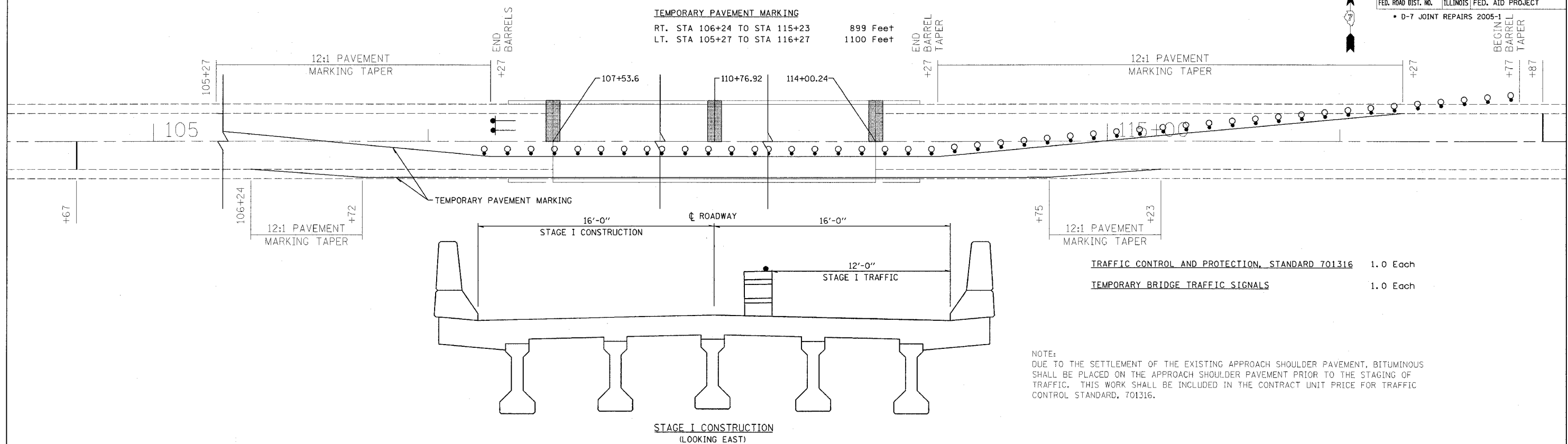
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STAGE

CONTRACT NO. 74073

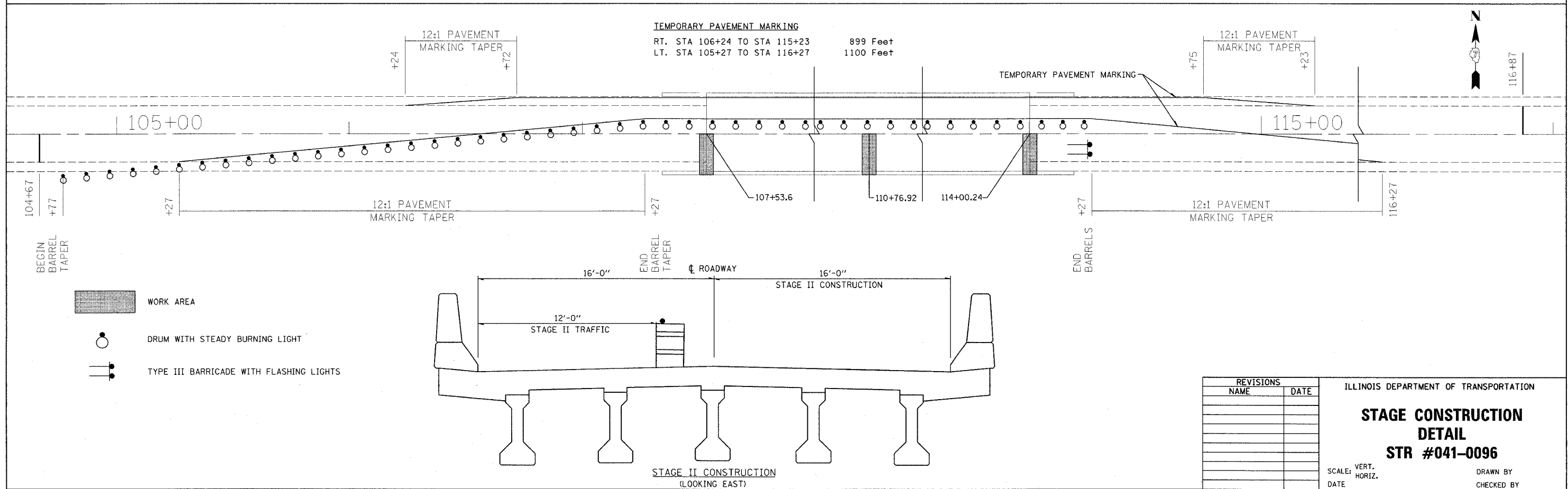
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	6
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

D-7 JOINT REPAIRS 2005-1



- TRAFFIC CONTROL AND PROTECTION, STANDARD 701316 1.0 Each
- TEMPORARY BRIDGE TRAFFIC SIGNALS 1.0 Each

NOTE:  
DUE TO THE SETTLEMENT OF THE EXISTING APPROACH SHOULDER PAVEMENT, BITUMINOUS SHALL BE PLACED ON THE APPROACH SHOULDER PAVEMENT PRIOR TO THE STAGING OF TRAFFIC. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL STANDARD, 701316.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**STAGE CONSTRUCTION  
DETAIL  
STR #041-0096**

SCALE: VERT.  
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FILE NAME = 05404pa.dgn  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

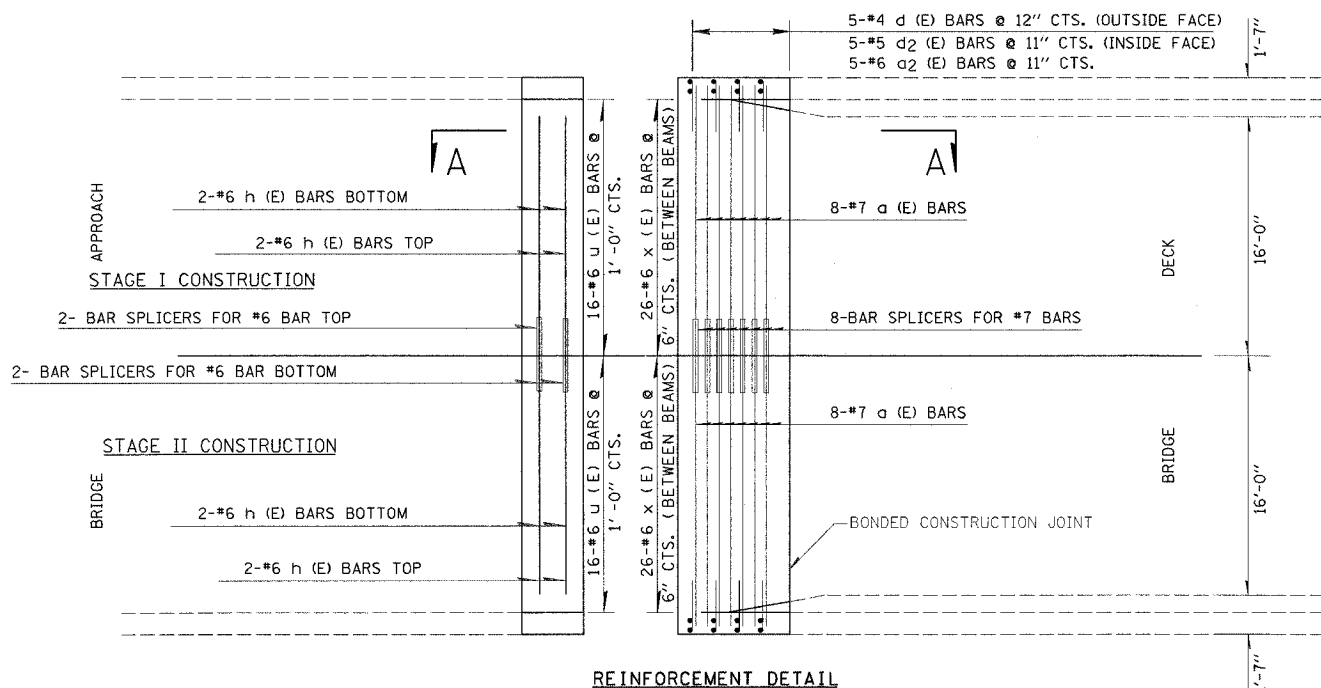
D-7 JOINT REPAIRS 2005-1

BAR LIST - PER ABUTMENTS - STR #041-0096

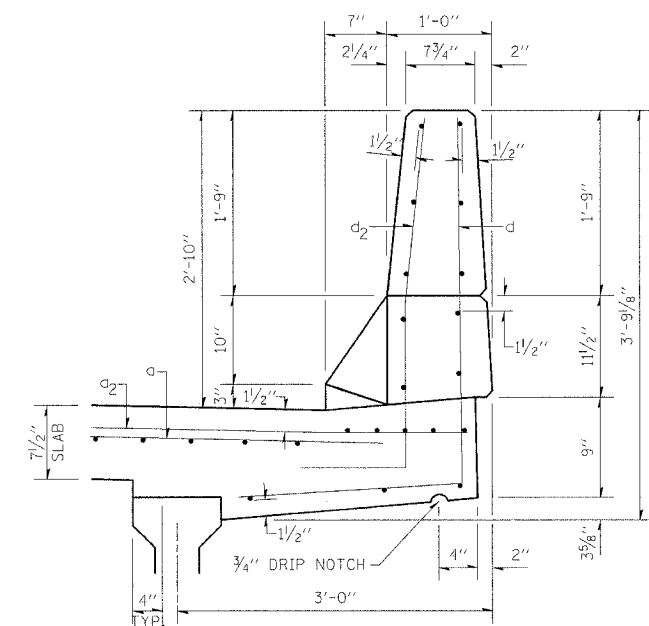
BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE
	STAGE I	STAGE II				
a (E)	8	8	16	#7	15'-1"	—
a1 (E)	10	10	20	#6	6'-6"	—
a2 (E)	5	5	10	#6	4'-0"	—
d (E)	5	5	10	#4	4'-10"	J
d2 (E)	5	5	10	#5	4'-1"	f
h (E)	4	4	8	#6	15'-0"	—
x (E)	26	26	52	#6	6'-11"	—
u (E)	16	16	32	#6	2'-8"	□

CONCRETE REMOVAL (LIMITS ARE FROM OUT TO OUT OF DECK)

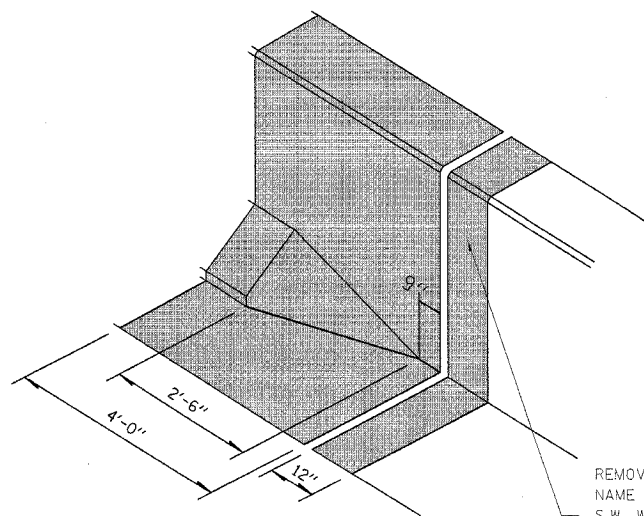
REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED



REINFORCEMENT DETAIL EAST ABUTMENT (WEST ABUTMENT SIMILAR)

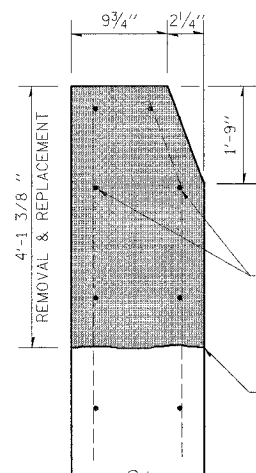


PARAPET DETAIL

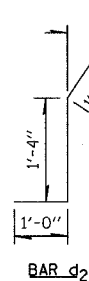


TYPICAL CONCRETE REPLACEMENT AT EACH ABUTMENT

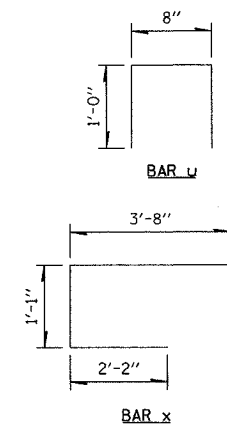
REMOVE AND REINSTALL NAME PLATE LOCATED ON S.W. WING STR. 041-0096, PAID FOR AS RELOCATING NAME PLATE 1.0 EACH



WINGWALL DETAIL

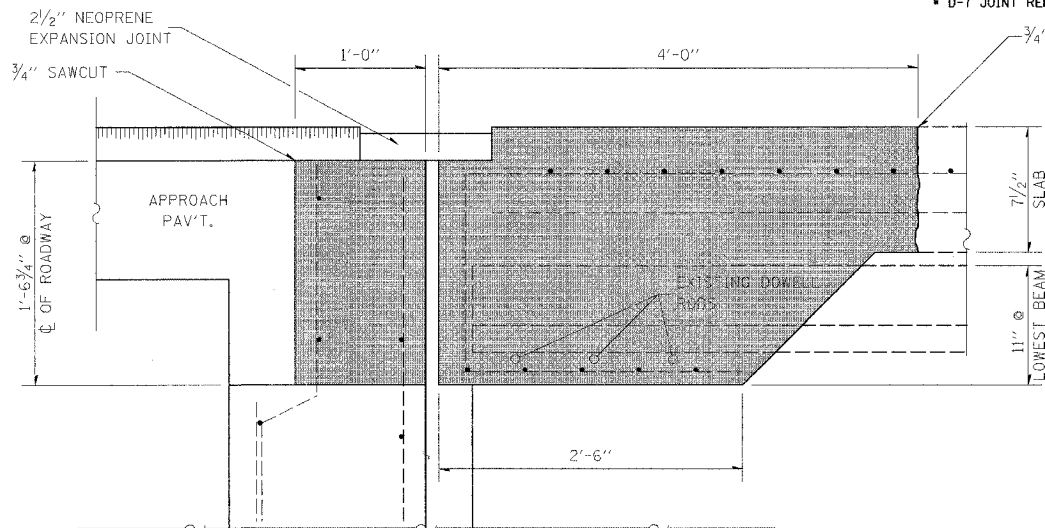


BAR d2



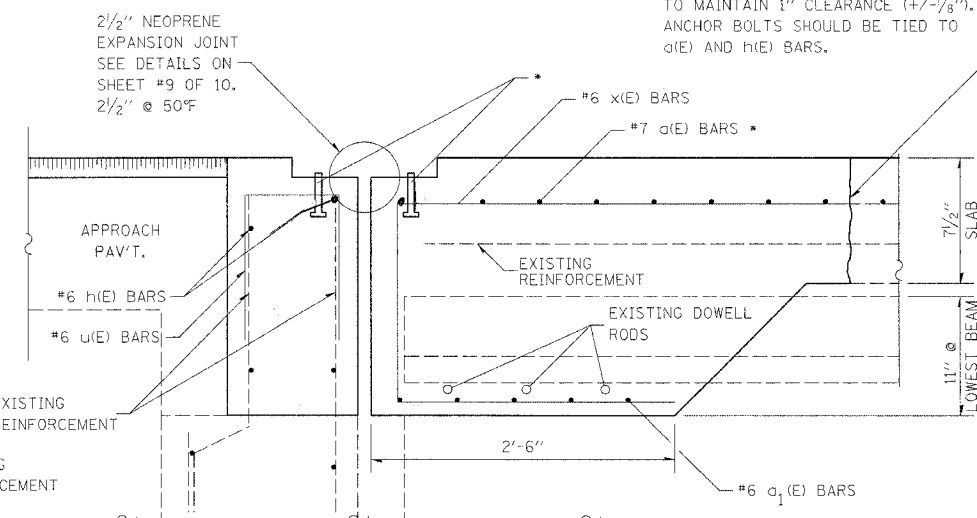
BAR x

BAR d



EXISTING EXPANSION JOINTS AT ABUTMENTS

PLACE a1(E) AND h(E) BARS IN BACK OF ANCHOR BOLT AS SHOWN IF REQUIRED TO MAINTAIN 1" CLEARANCE (+/- 1/8"). ANCHOR BOLTS SHOULD BE TIED TO a1(E) AND h(E) BARS.



PROPOSED EXPANSION JOINTS AT ABUTMENTS

NOTE: EXISTING REINFORCEMENT BARS AND DOWELL RODS ARE TO BE CLEANED, STRIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM (COST INCLUDED IN CONCRETE REMOVAL).

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXPANSION JOINT DETAILS**  
**EAST & WEST ABUTMENTS**  
**STR #041-0096**

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726		JEFFERSON	10	8
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

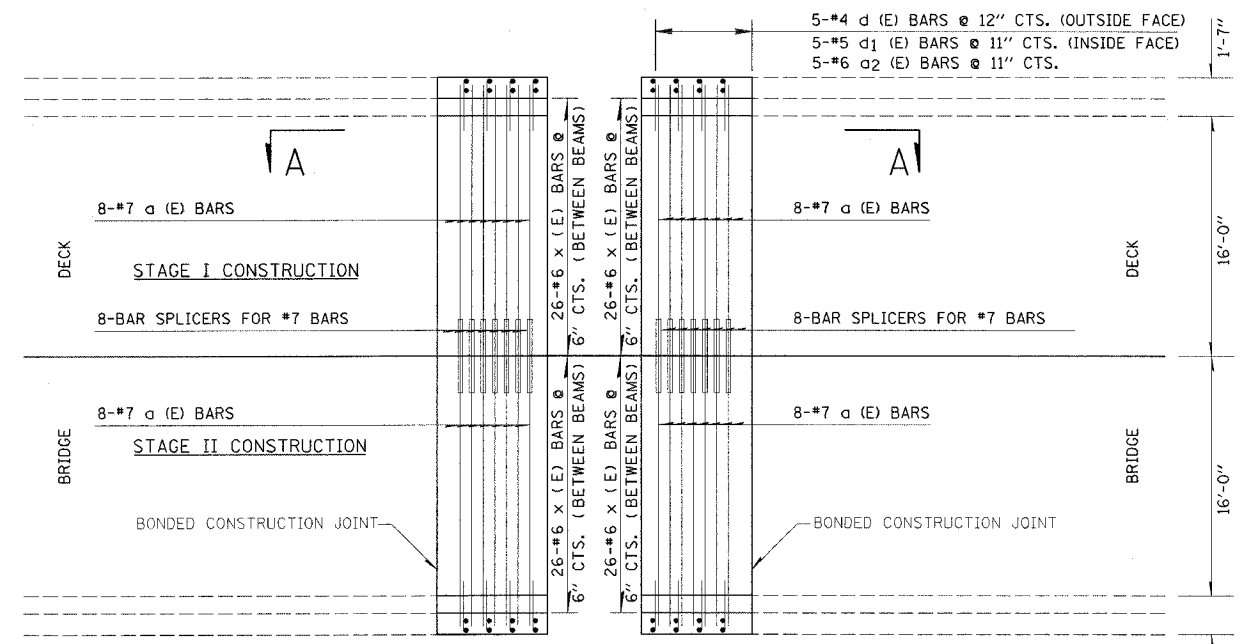
D-7 JOINT REPAIRS 2005-1

BAR LIST - PIER 4 - STR #041-0096

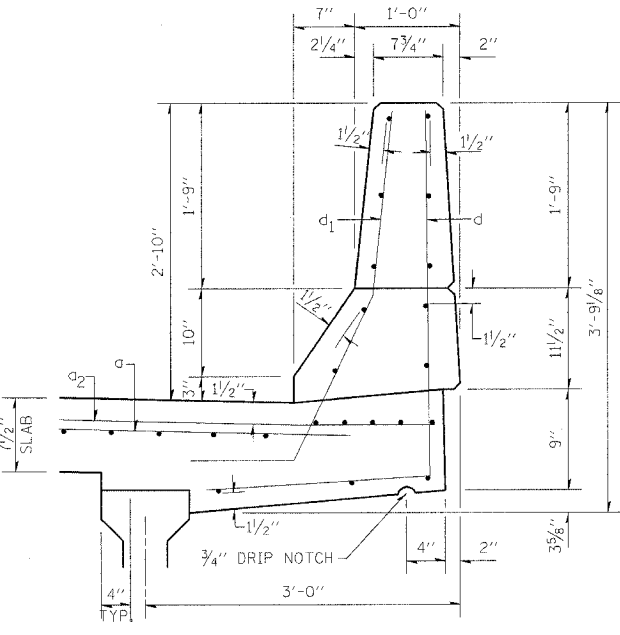
BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE
	STAGE I	STAGE II				
a (E)	16	16	32	#7	15'-1"	—
a1 (E)	20	20	40	#6	6'-6"	—
a2 (E)	10	10	20	#6	4'-0"	—
d (E)	10	10	20	#4	4'-10"	J
d1 (E)	10	10	20	#5	4'-1"	J
x (E)	52	52	104	#6	6'-11"	—

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED

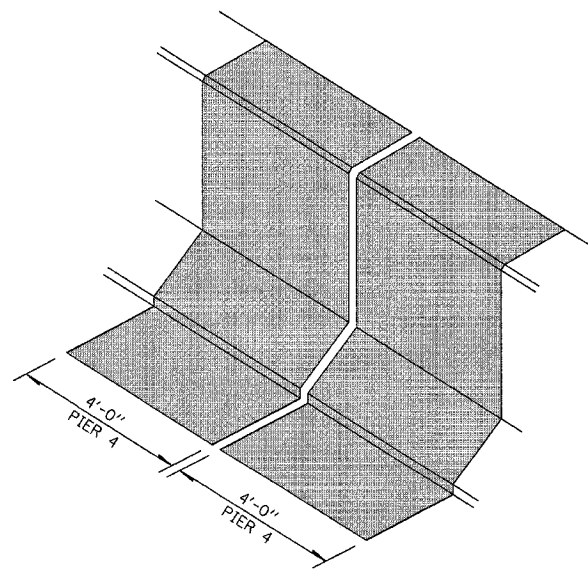
CONCRETE REMOVAL (LIMITS ARE FROM OUT TO OUT OF DECK)



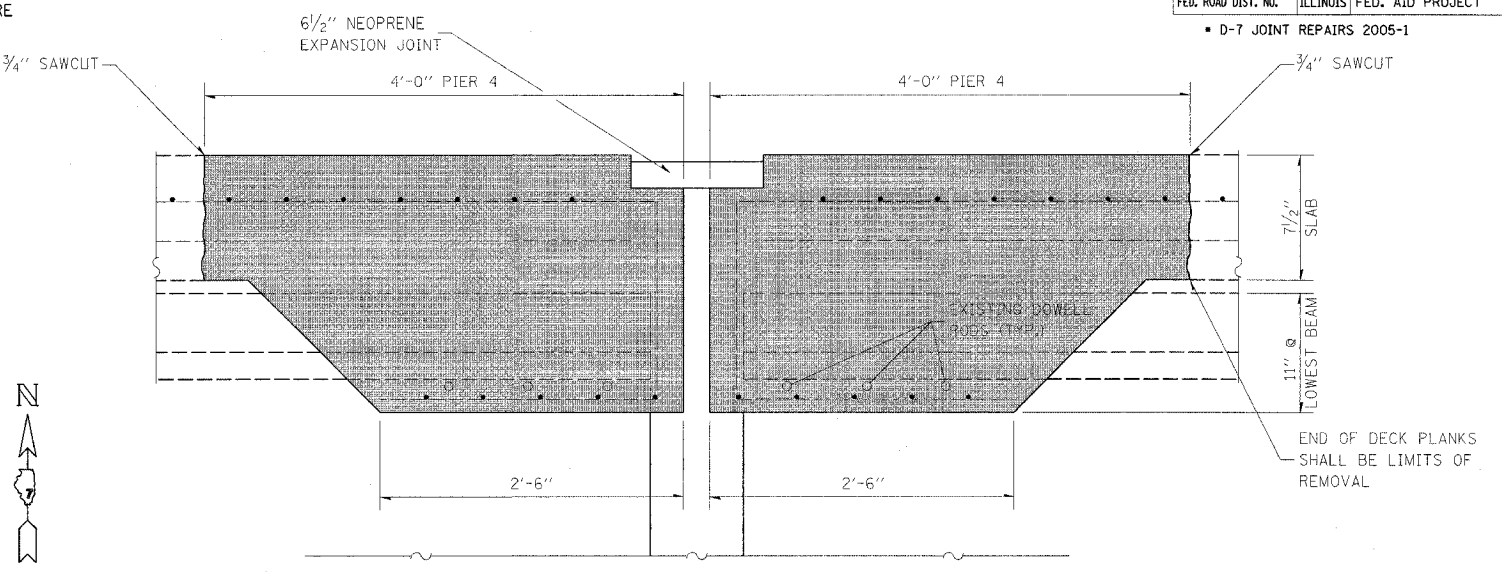
REINFORCEMENT DETAIL PIER 4



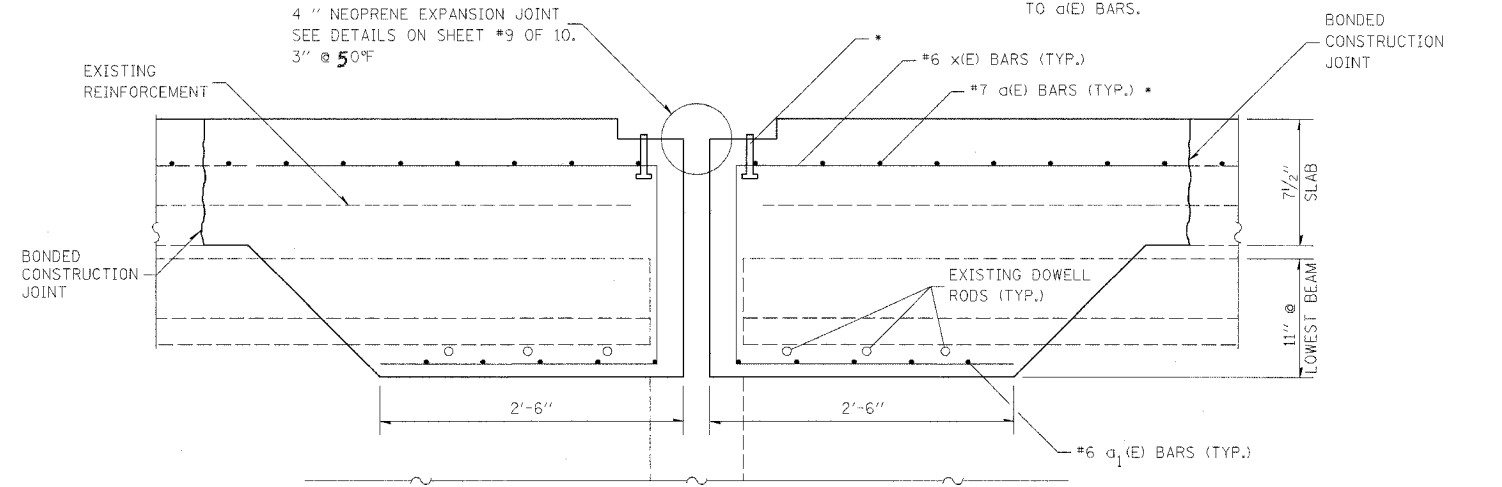
PARAPET DETAIL



TYPICAL CONCRETE REPLACEMENT PIER 4

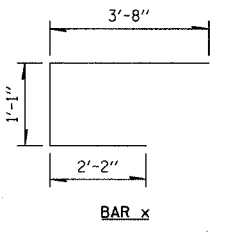
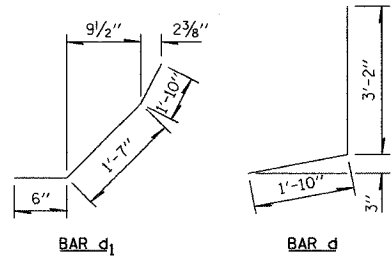


EXISTING EXPANSION JOINTS AT PIER 4



PROPOSED EXPANSION JOINTS AT PIER 4

NOTE: EXISTING REINFORCEMENT BARS AND DOWELL RODS ARE TO BE CLEANED, STRIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM (COST INCLUDED IN CONCRETE REMOVAL).



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXPANSION JOINT DETAILS**  
**PIER 4**  
**STR #041-0096**

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

D-7 JOINT REPAIRS 2005-1

Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1 1/2" Min.
2 1/2"	2 1/2"	1 3/4" Min.
4"	3"	2 1/2" Min.

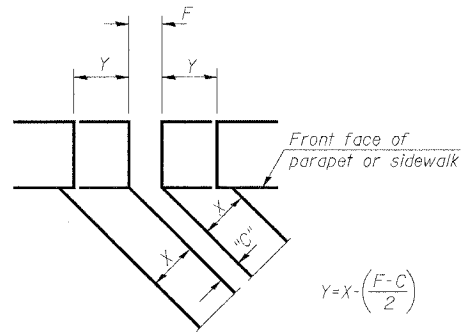
**INSTALLATION NOTES**

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

Note A:  
Maximum spacing of anchor bolts shall be 12" centers.

**SKREW LIMITATIONS**

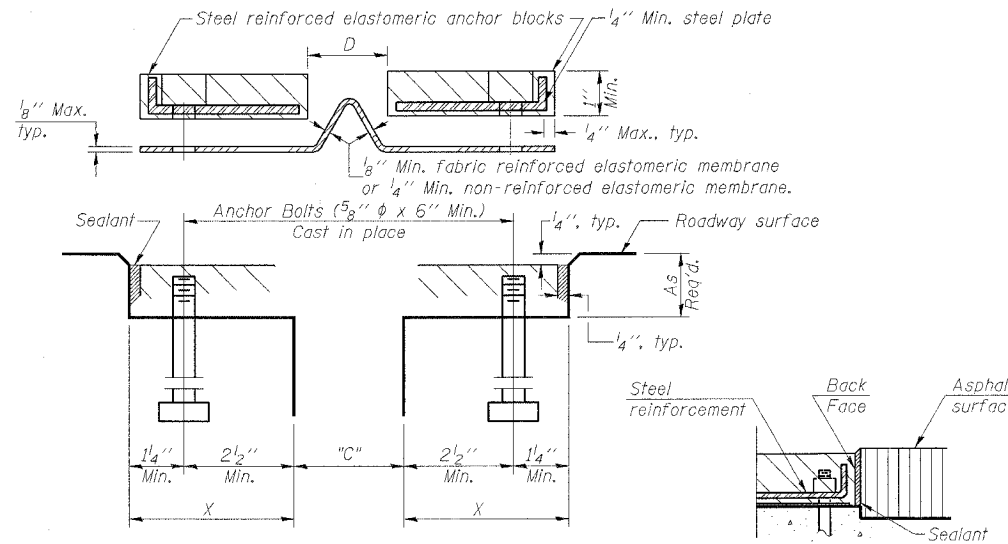
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1/2" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



**FORMING BLOCKOUT SKETCH**

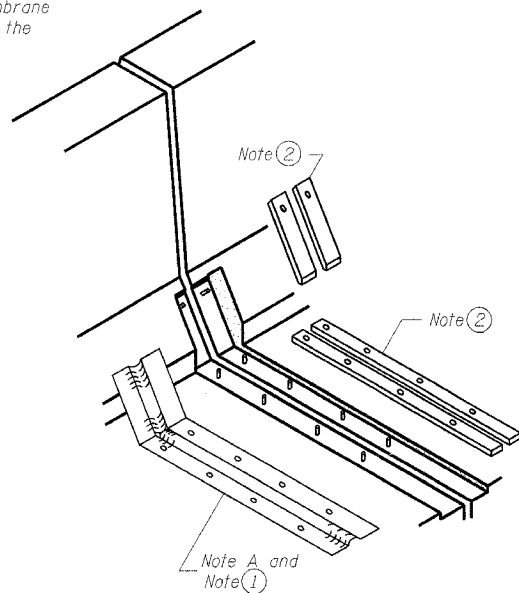
$$Y = X \cdot \left( \frac{F - C}{2} \right)$$

For dimension "F" see sheet # 7-8

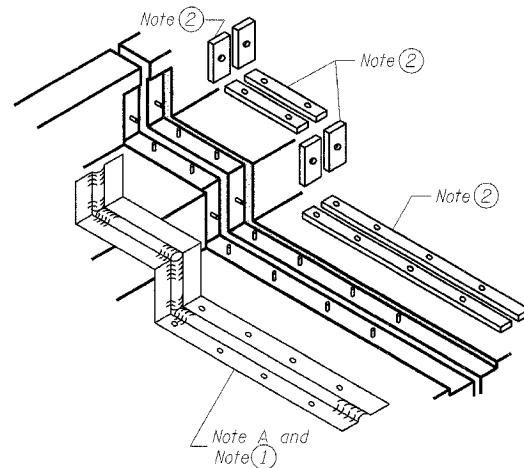


**CROSS SECTION**

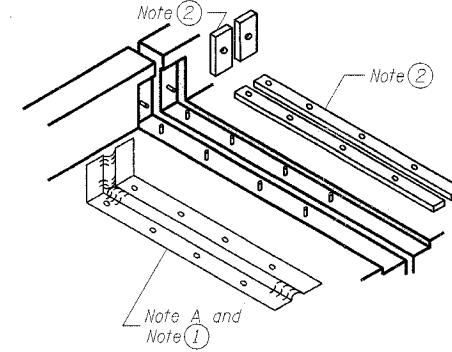
**ANCHOR BLOCK WITH ASPHALT SURFACE**



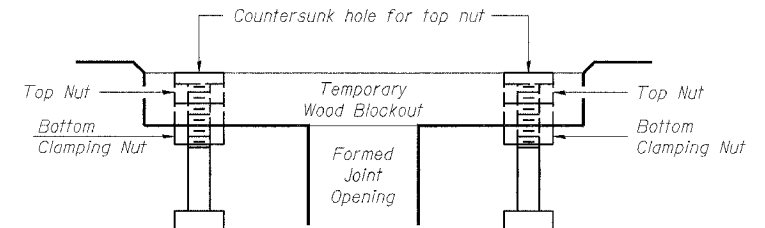
**AT PARAPET**



**AT SIDEWALK OR MEDIAN**

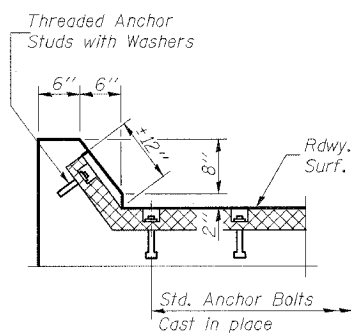


**AT WALL**

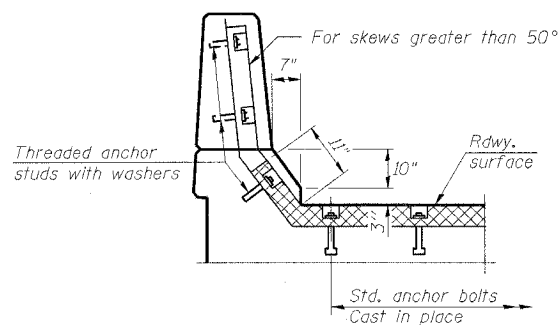


**RECOMMENDED BLOCKOUT DETAIL**

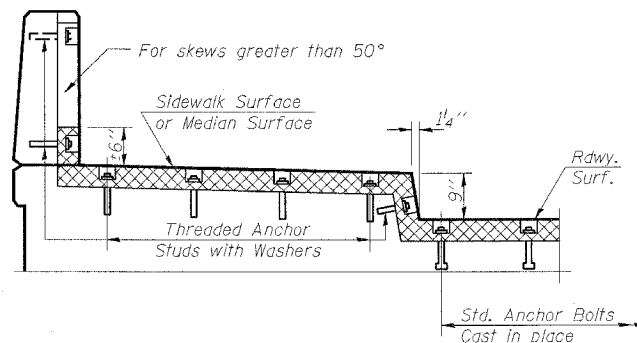
Note:  
Stud needs to be threaded lower to allow for use of clamping nut.



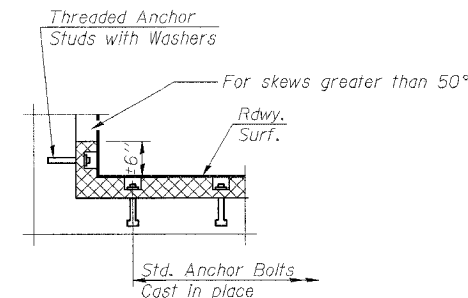
**AT CURB**



**AT PARAPET**



**AT SIDEWALK OR MEDIAN TYPICAL END TREATMENTS**



**AT WALL**

**CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS**  
**STR #041-0096**

SCALE: VERT.  
HORIZ.  
DATE

DRAWN BY  
CHECKED BY

steffemk  
 3/29/2005  
 c:\projects\74073\05404pa.dgn  
 EJC

PLOT DATE = 3/29/2005  
 FILE NAME = #FILE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#

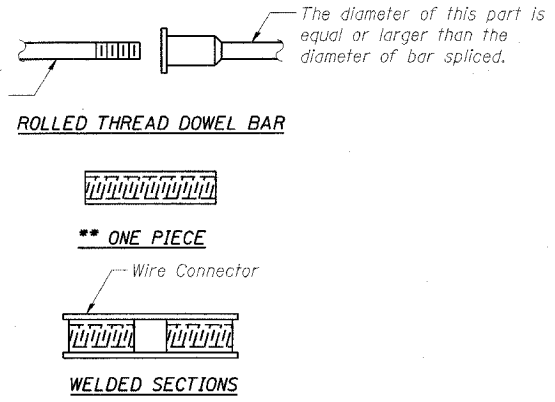
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	10
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

• D-T JOINT REPAIRS 2005-1

steffennk  
 3/29/2005  
 c:\projects\74073\d05404p.dgn  
 BDET 06

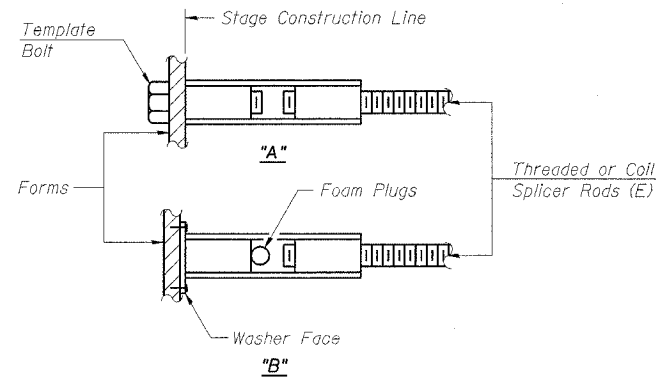
The diameter of this part is the same as the diameter of the bar spliced.

The diameter of this part is equal or larger than the diameter of bar spliced.



**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\* Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



**INSTALLATION AND SETTING METHODS**

"A": Set bar splicer assembly by means of a template bolt.  
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E): Indicates epoxy coating.

**NOTES**

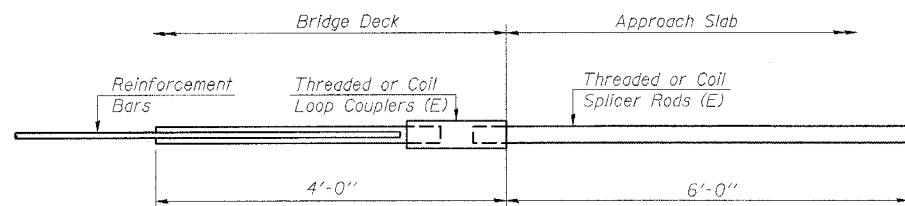
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity =  $1.25 \times f_y \times A_t$   
 (Tension in kips)
- ② Minimum \*Pull-out Strength =  $1.25 \times f_{sallow} \times A_t$   
 (Tension in kips)

Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $f_{sallow}$  = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
 \* = 28 day concrete

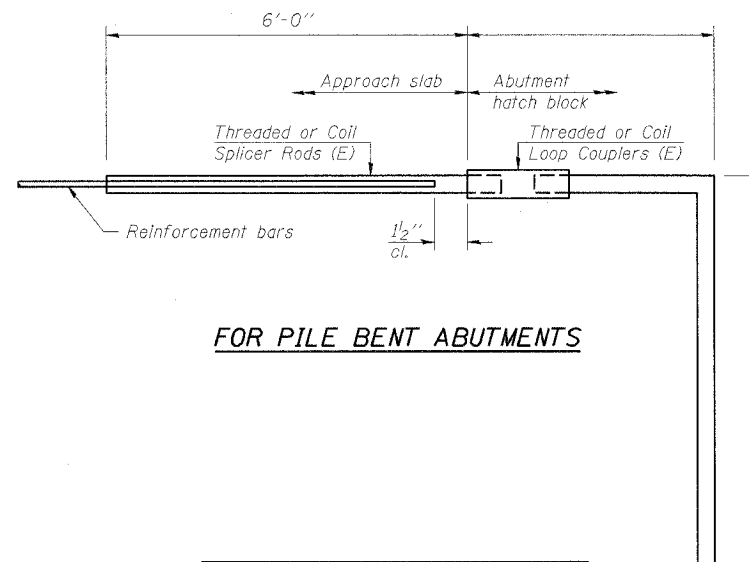
BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."



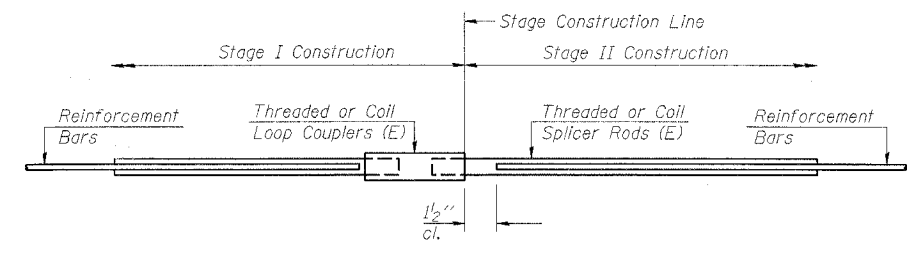
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



**FOR PILE BENT ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



**STANDARD**

Bar Size	No. Assemblies Required	Location
#6	8	E. & W. ABUTMENTS
#7	16	ABUTMENTS
#7	16	PIER 4

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BAR SPLICER ASSEMBLY DETAILS STR #041-0096**

SCALE: VERT. HORIZ. DATE DRAWN BY CHECKED BY