

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	1

** D7 SIGNAL MODERNIZATION 2006-3

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

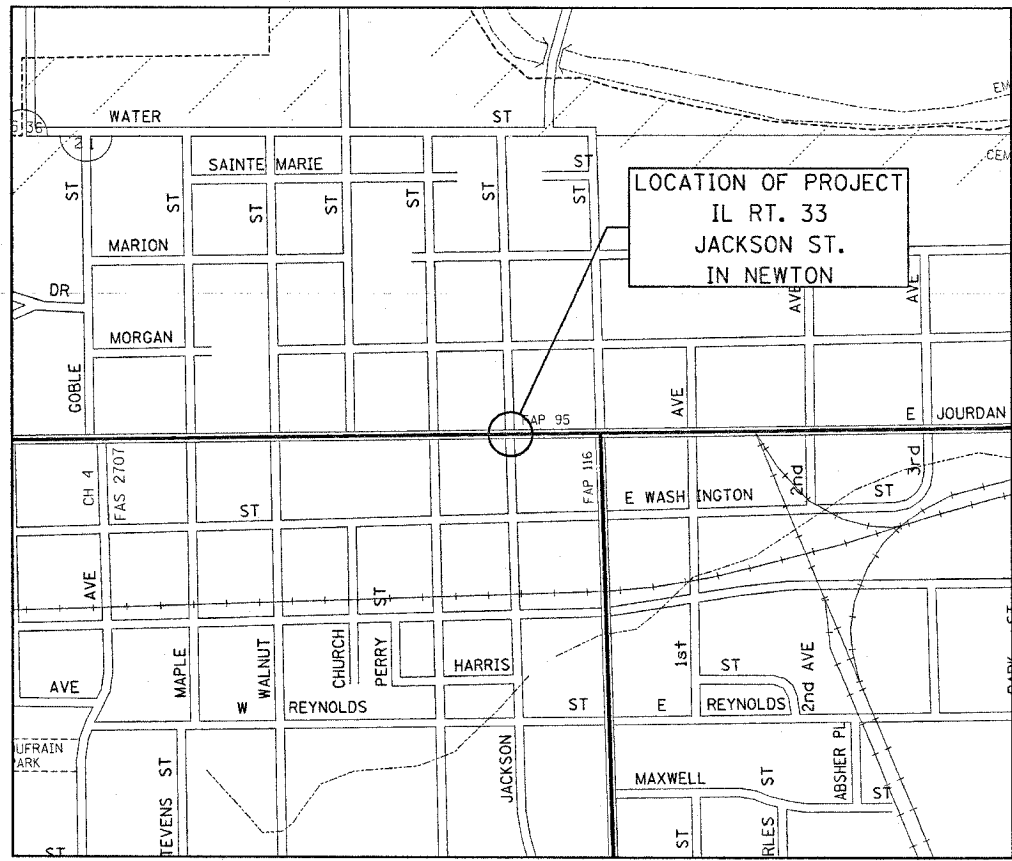
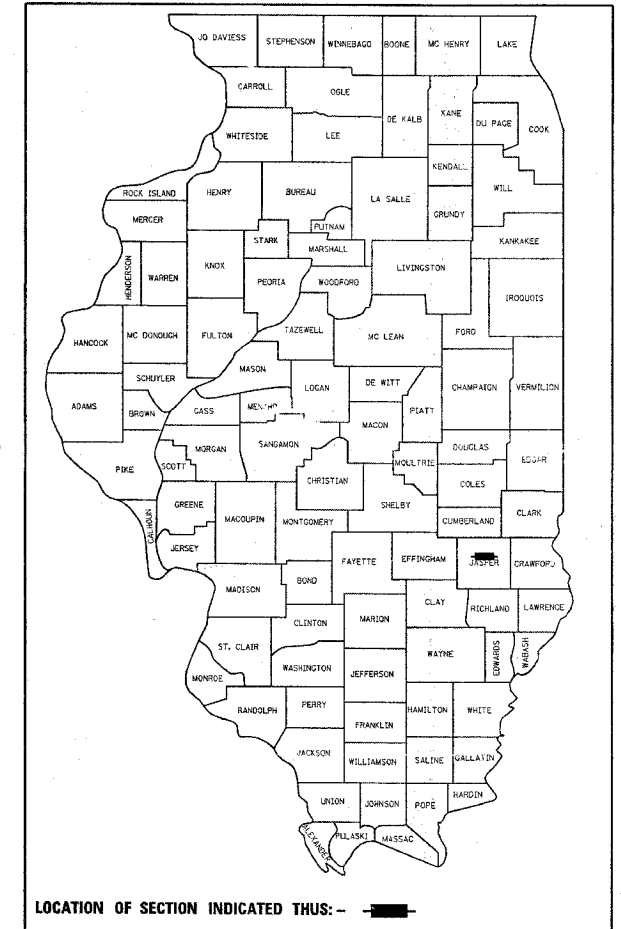
**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 95 (IL ROUTE 33)
D7 SIGNAL MODERNIZATION 2006-3
PROJECT HS-0095(050)
JASPER COUNTY

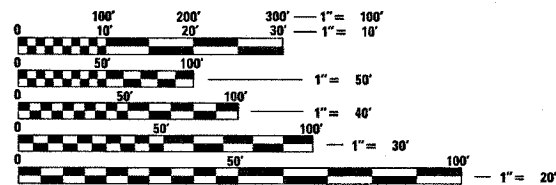
C-97-049-05

FOR INDEX OF SHEETS, SEE SHEET NO. 2

D-97-018-05



NEWTON



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

PROJECT ENGINEER : ROB MACKLIN
SQUAD LEADER :
DESIGNER : LYNN MCCLELLAN
TELEPHONE : 217/342-3951 EX 245

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 17, 2005
Christ H. Reed
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 1, 2005
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

July 1, 2005
Victor Maden
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PLOT DATE : 5/19/2005
FILE NAME : c:\proj\jaspers\74090\050\050.dgn
USER NAME : staff\mink

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	2
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
** D7 SIGNAL MODERNIZATION 2006-3				

GENERAL NOTES

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2002; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED MARCH 1, 2005; AND "THE SPECIAL PROVISIONS" INCLUDED IN THE PROPOSAL.

THIS PROJECT IS LOCATED AT THE INTERSECTION OF IL ROUTE 33 AND JACKSON STREET IN THE CITY OF NEWTON, JASPER COUNTY. THE WORK INCLUDED IN THIS PROJECT CONSISTS OF THE REMOVAL OF 12 TRAFFIC SIGNAL HEADS, THE REPLACEMENT OF 11 TRAFFIC SIGNAL HEADS AND THE INSTALLATION OF THREE NEW TRAFFIC SIGNAL HEADS FOR A TOTAL OF 14 TRAFFIC SIGNAL HEADS; ALL NEW PEDESTRIAN SIGNAL HEADS AND ALL NEW PEDESTRIAN PUSH BUTTONS. TWO OF THE NEW TRAFFIC SIGNAL HEADS WILL BE INSTALLED ON A NEW MAST ARM. ALSO INCLUDED IS ANY OTHER WORK NECESSARY TO COMPLETE THE PROJECT.

THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. CALL J.U.L.I.E. AT 1-800-892-0123.

THE TRAFFIC OPERATIONS UNIT CHIEF AT THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE NOTIFIED AT 217-342-8284 AT LEAST 72 HOURS PRIOR TO TURNING ON ANY FLASHER OR CONTROLLER UNIT.

THE LOCATION OF ALL TRAFFIC COMPONENTS AS SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THE FINAL LOCATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE SIZE OF THE CABLE SUPPLIED SHALL BE EQUAL TO OR GREATER THAN THE SIZE OF THE CABLE REQUIRED TO CARRY THE LOAD BETWEEN THE CONTROLLER AND THE SERVICE INSTALLATION.

THE CONTRACTOR SHALL ARRANGE FOR A FACTORY OR SUPPLIER REPRESENTATIVE TO BE PRESENT AT THE INTERSECTION WHEN THE SIGNALS ARE TURNED ON. THE REPRESENTATIVE SHALL MAKE CERTAIN THAT ALL EQUIPMENT OPERATES TO THE SATISFACTION OF THE ENGINEER.

ALL HARDWARE SHALL BE TIGHTENED AND WELL SECURED. CABLES SHALL BE NEATLY WOUND IN HANDHOLES. CABLES SHALL BE NEATLY TRAINED IN THE CONTROLLER CABINET.

ALL THREADS OF BOLTS USED IN ASSEMBLY OF TRAFFIC SIGNAL COMPONENTS SHALL BE COATED WITH A NON-LEAD BASED ANTI-SEIZE COMPOUND, SIMILAR TO LEAD PLATE, PRIOR TO ASSEMBLY.

NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR PLACING CONDUIT AT GREATER THAN 2 FOOT MINIMUM DEPTH TO AVOID OBSTACLES SUCH AS UNDERGROUND UTILITIES.

THE CONTRACTOR IS RESPONSIBLE FOR UNCOVERING OR HAND DIGGING AROUND UTILITIES AS NECESSARY. THE COST OF THIS WORK IS TO BE INCLUDED WITH THE TRENCH AND BACKFILL FOR ELECTRICAL WORK PAY ITEM.

THE NUMBER OF CONDUCTORS FOR ELECTRIC CABLES AS SHOWN ON THE PLANS SHALL BE THE MINIMUM NUMBER OF CONDUCTORS FURNISHED FOR EACH LOCATION. THE CONTRACTOR MAY SUBSTITUTE AN ELECTRIC CABLE WITH MORE CONDUCTORS THAN SPECIFIED BUT NO ADDITIONAL COMPENSATION WILL BE MADE FOR THE EXTRA CONDUCTORS.

ALL NEW TRAFFIC AND PEDESTRIAN SIGNAL WIRING SHALL EXTEND FROM CONTROLLER TO SIGNAL. SPLICES IN HANDHOLES WILL NOT BE ALLOWED.

ALL MAST ARM MOUNTED SIGNAL HEADS ON AN INDIVIDUAL MAST ARM SHALL BE MOUNTED SO THAT THE RED INDICATIONS ARE LEVEL WITH EACH OTHER.

THE EXISTING TRAFFIC SIGNAL HEADS SHALL REMAIN IN OPERATION DURING THE CONSTRUCTION OF THE PROPOSED TRAFFIC SIGNALS.

IT IS PERMISSIBLE TO PLACE THE INTERSECTION INTO ALL RED FLASH TO FACILITATE THE INSTALLATION OF THE LED TRAFFIC SIGNAL HEADS AS DIRECTED BY THE ENGINEER.

ALL TRAFFIC SIGNAL MAST ARM ASSEMBLIES (STANDARD, COMBINATION, OR DUAL) MUST BE DESIGNED FOR THE LOADINGS SHOWN ON THE HIGHWAY STANDARDS OR THESE SIGNAL PLANS, WHICHEVER IS GREATER.

THERE ARE 8 EXISTING LOAD SWITCHES THAT WILL BE REMOVED AND REPLACED.

THIS WORK SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTION OF SECTION 895 OF THE STANDARD SPECIFICATIONS. ALL 12 EXISTING TRAFFIC SIGNAL HEADS SHALL BECOME THE PROPERTY OF THE CITY OF NEWTON. THE CONTRACTOR SHALL SAVE HARMLESS AND DELIVER THE EQUIPMENT TO:

CITY OF NEWTON:
 RAYMOND REYNOLDS
 ELECTRICAL DEPARTMENT
 108 N. VAN BUREN
 NEWTON, IL 62448
 618-783-3139 OR 618-783-8451

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, AND GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 EXISTING INSTALLATION AND REMOVAL ITEMS
- 5 PROPOSED INSTALLATION
- 6 BORING LOG

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 6:

- 000001-04 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 701006-02 OFF-RD OPERATIONS, 2L, 2W, 15' TO 2' FROM PAVEMENT
- 701501-03 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701701-04 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-03 LANE CLOSURE MULTILANE 1W OR 2W OR SIDEWALK CLOSURE
- 702001-05 TRAFFIC CONTROL DEVICES
- 857001 STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
- 877001-02 STEEL MAST ARM ASSEMBLY AND POLE
- 878001-03 CONCRETE FOUNDATION DETAILS
- 880006 TRAFFIC SIGNAL MOUNTING DETAILS

PLOT DATE = 5/12/2006
 PLOT SCALE = 8000:1
 USER NAME = staff@nwk

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		GENERAL NOTES & INDEX OF SHEETS
SCALE:	VERT. DATE	HORIZ. DATE
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
** D7 SIGNAL MODERNIZATION 2006-3				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		YO31-1F 90% FED. 10% STATE	
* 42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	52	52	
* 44000600	SIDEWALK REMOVAL	SQ FT	52	52	
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FOOT	5	5	
81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	5	5	
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	113	113	
87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1	1	
87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	13.5	13.5	
88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	13	13	
88800100	PEDESTRIAN PUSH-BUTTON	EACH	6	6	
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1	1	
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1	1	
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	1	1	
X8801310	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4	4	
X8801415	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3	3	
X8810395	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED	EACH	2	2	
X8810495	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, BRACKET MOUNTED	EACH	2	2	
XX004976	SIGNAL HEAD, POLYCARBONATE, LED, 3-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1	1	

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*SPECIALTY ITEMS

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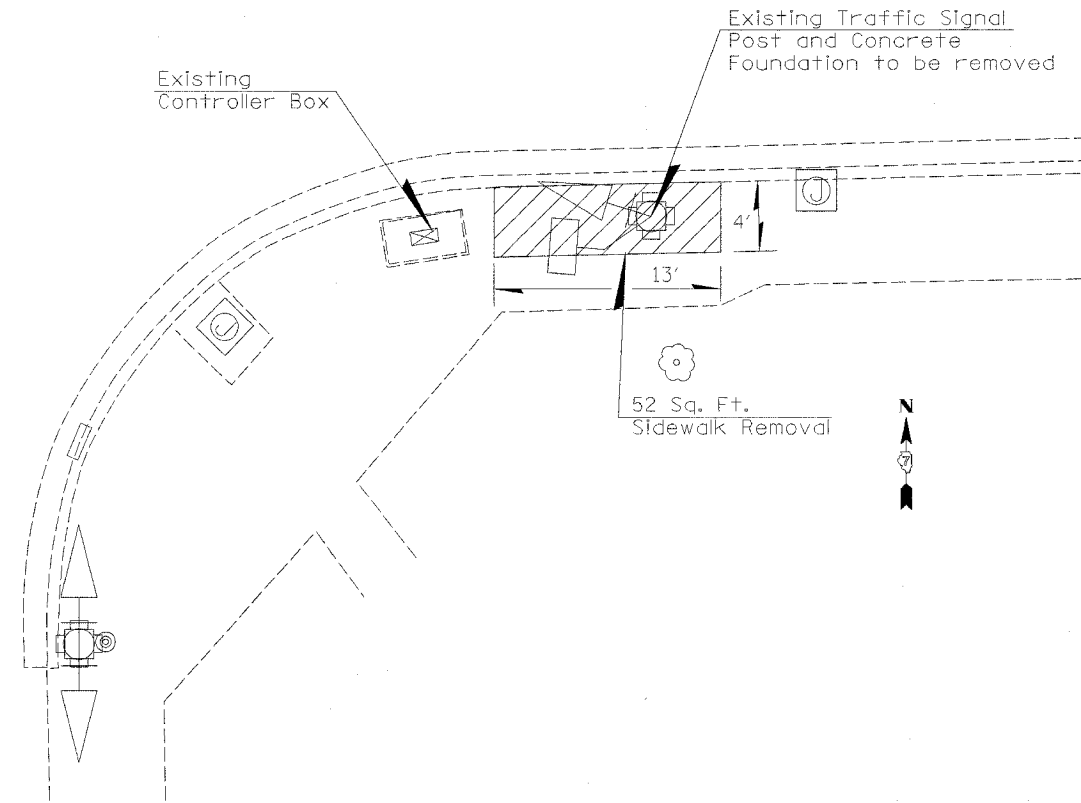
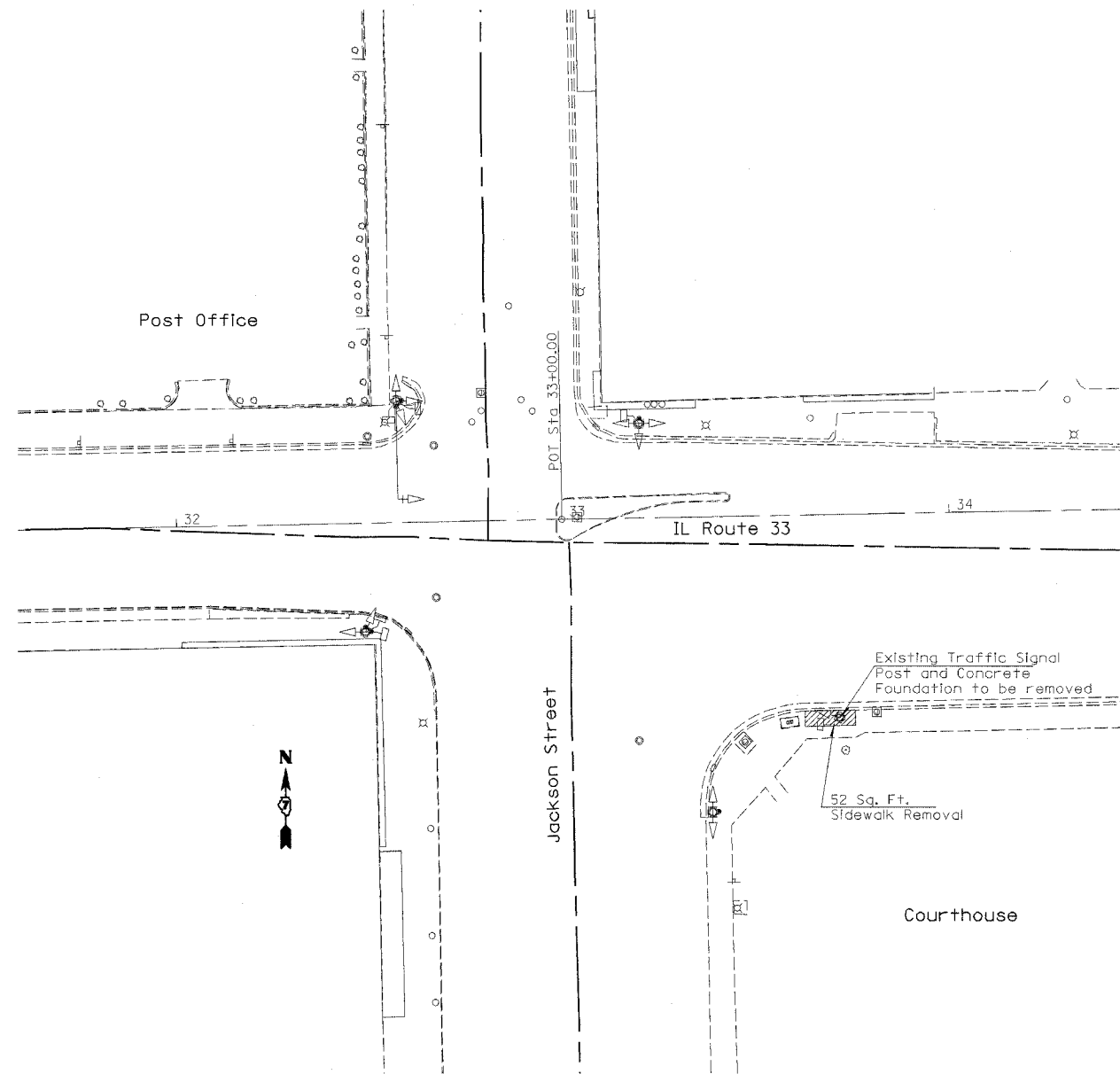
ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
** DT SIGNAL MODERNIZATION 2006-3				



LEGEND

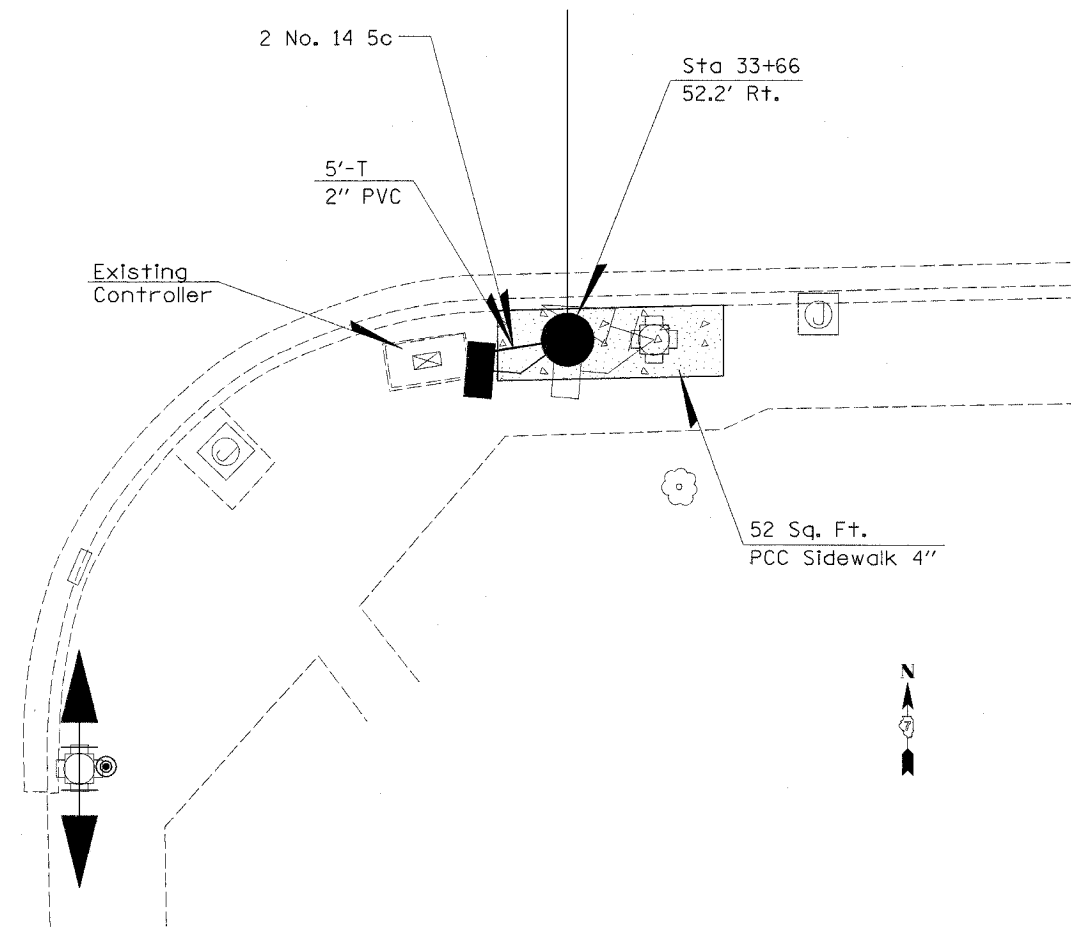
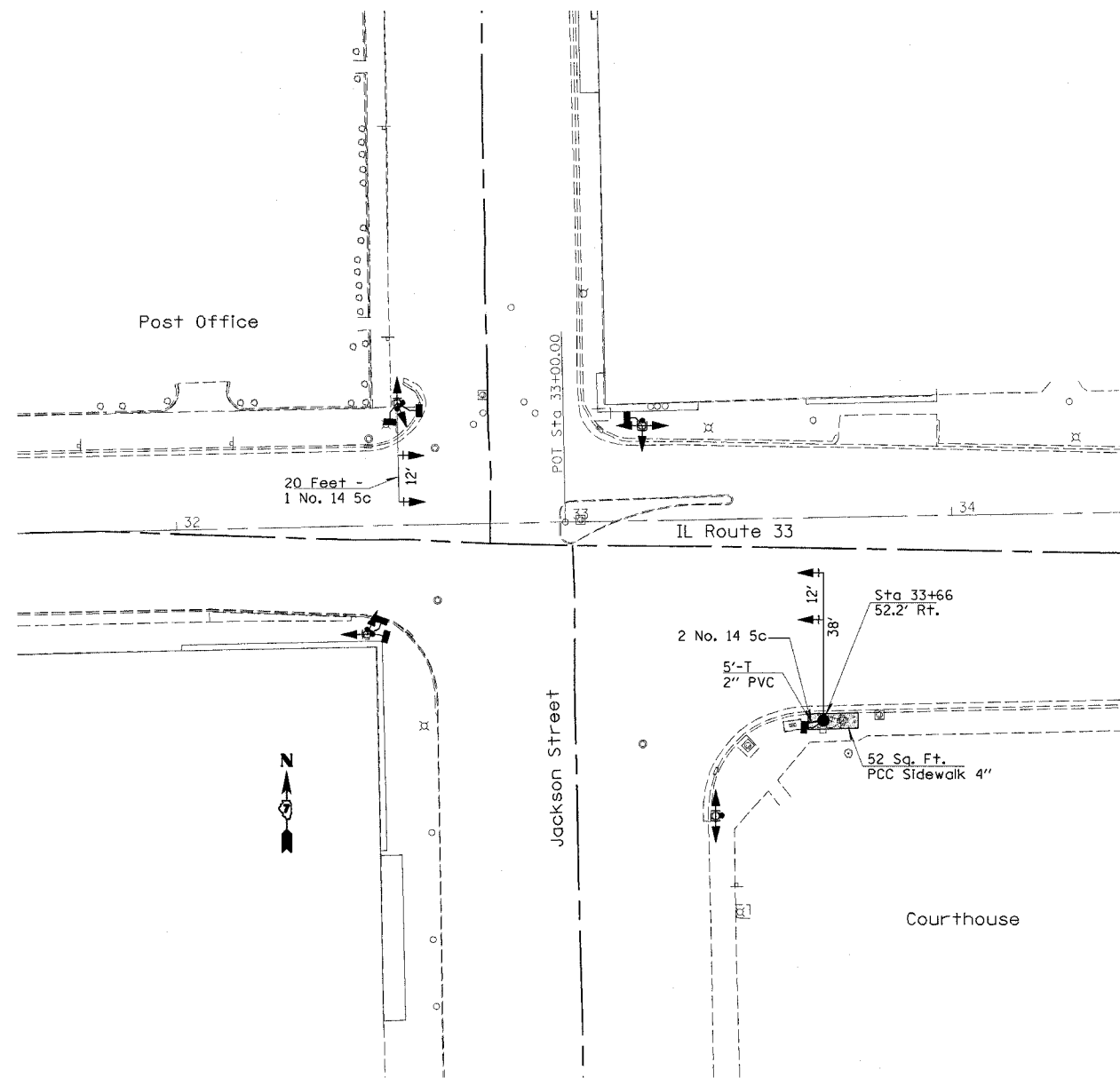
EXISTING	PROPOSED	
		CONTROLLER
		HANDHOLE
		DOUBLE HANDHOLE
		SIGNAL POST
		TRAFFIC SIGNAL WITH BACKPLATE
		PEDESTRIAN SIGNAL HEAD
		SERVICE INSTALLATION
		STEEL MAST ARM ASSEMBLY AND POLE
		DETECTOR LOOP
		JUNCTION BOX
		PEDESTRIAN PUSH BUTTON

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ILLINOIS DEPARTMENT OF TRANSPORTATION
**EXISTING INSTALLATION
 &
 REMOVAL ITEMS**
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
** DT SIGNAL MODERNIZATION 2006-3				



LEGEND

EXISTING	PROPOSED	
		CONTROLLER
		HANDHOLE
		DOUBLE HANDHOLE
		SIGNAL POST
		TRAFFIC SIGNAL WITH BACKPLATE
		PEDESTRIAN SIGNAL HEAD
		SERVICE INSTALLATION
		STEEL MAST ARM ASSEMBLY AND POLE
		DETECTOR LOOP
		JUNCTION BOX
		PEDESTRIAN PUSH BUTTON

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PROPOSED INSTALLATION

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PLOT DATE = 5/10/2005
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	**	JASPER	6	6
STA. TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
** D7 SIGNAL MODERNIZATION 2006-3				



Illinois Department of Transportation
Division of Highways
 District - Materials

SOIL BORING LOG

Date 4/6/05

ROUTE FAP 95 (IL 33) DESCRIPTION TS Fdn: IL 33 and Jackson St., Newton, IL LOGGED BY E. Sandschafer

SECTION D7 Signal Mod. 2006-3 LOCATION 1 - NW 14, 2 - NE 14, SEC. 7, TWP. 6 N, RNG. 9 E, 3 PM

COUNTY Jasper DRILLING METHOD ollow stem auger & split spoon HAMMER TYPE Auto 140#

STRUCT. NO.	Station	BORING NO.	Station	Offset	Ground Surface Elev.	Description	DEPTH (ft)	BLOW COUNT (6")	UNIFORMITY COEFFICIENT (tsf)	MOISTURE CONTENT (%)	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After Hrs.	DEPTH (ft)	BLOW COUNT (6")	UNIFORMITY COEFFICIENT (tsf)	MOISTURE CONTENT (%)	
																					DEPTH (ft)
N/A	gINT file 040-fb03	1 (SE)	33+64	44.00ft Rt	532.36						N/A	N/A		Dry	Dry						
						4.75" asphalt over 7" concrete pavement.	531.36														
						Mixture of Silty Clay and brick fragments.	530.86														
						Medium to stiff, damp, red mottled gray, CLAY.		1													
								4	0.7	19											
								3	B												
								2													
								3													
								5	1.0	22											
								2													
								3													
								5	1.6	18											
								3													
						Several thin Sand partings.		5	0.8	23											
								5	S												
								2													
						Very stiff to hard, moist, brown to gray, CLAY SHALE w/some sand. Sample broken, unable to test for Qu.	519.86	8	+4.5	14											
								17	PP												
								7													
								18													
								28	2.3	16											
								17													
						Sample powdered and broken into small chips, unable to test for Qu.		50		10											
								504"													
								20	42												

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SOIL BORING LOG

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