

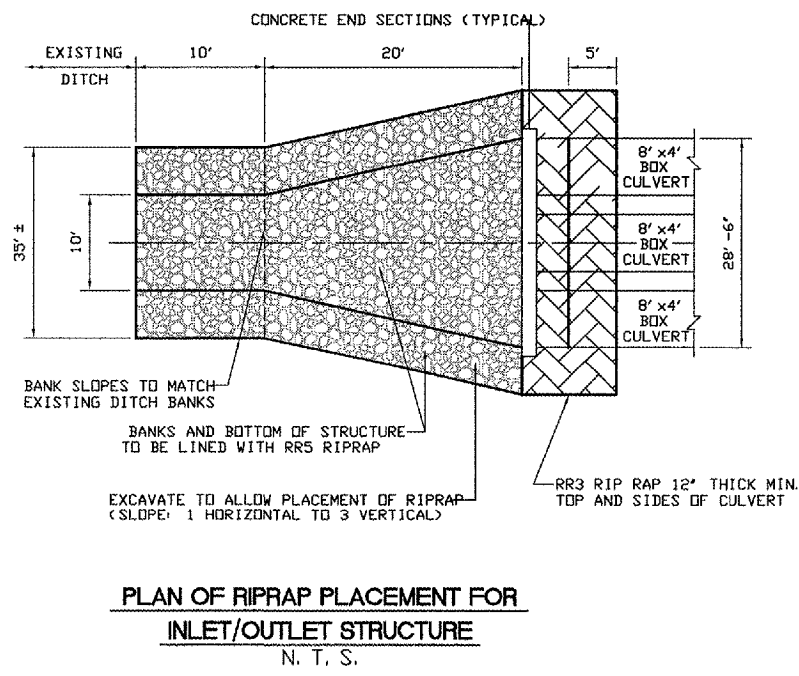
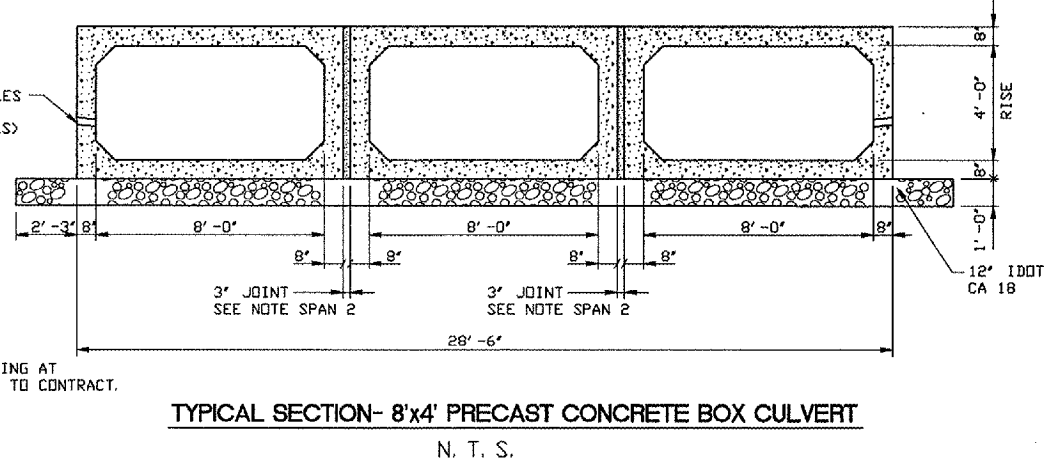
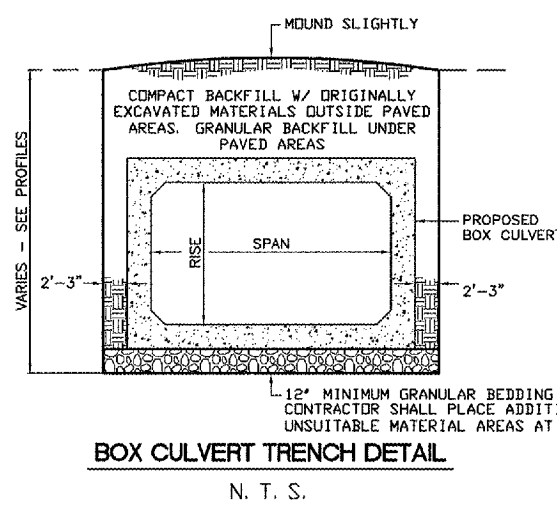
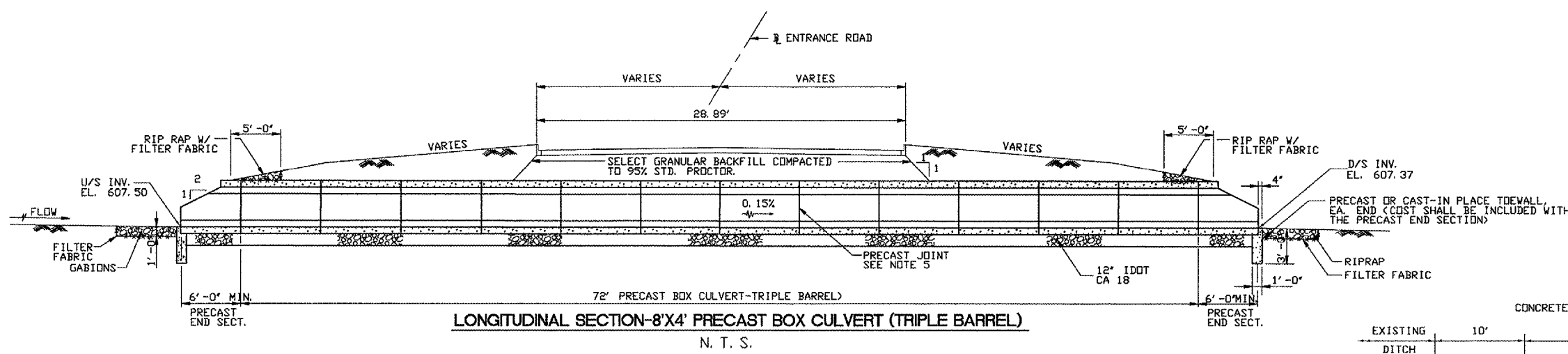
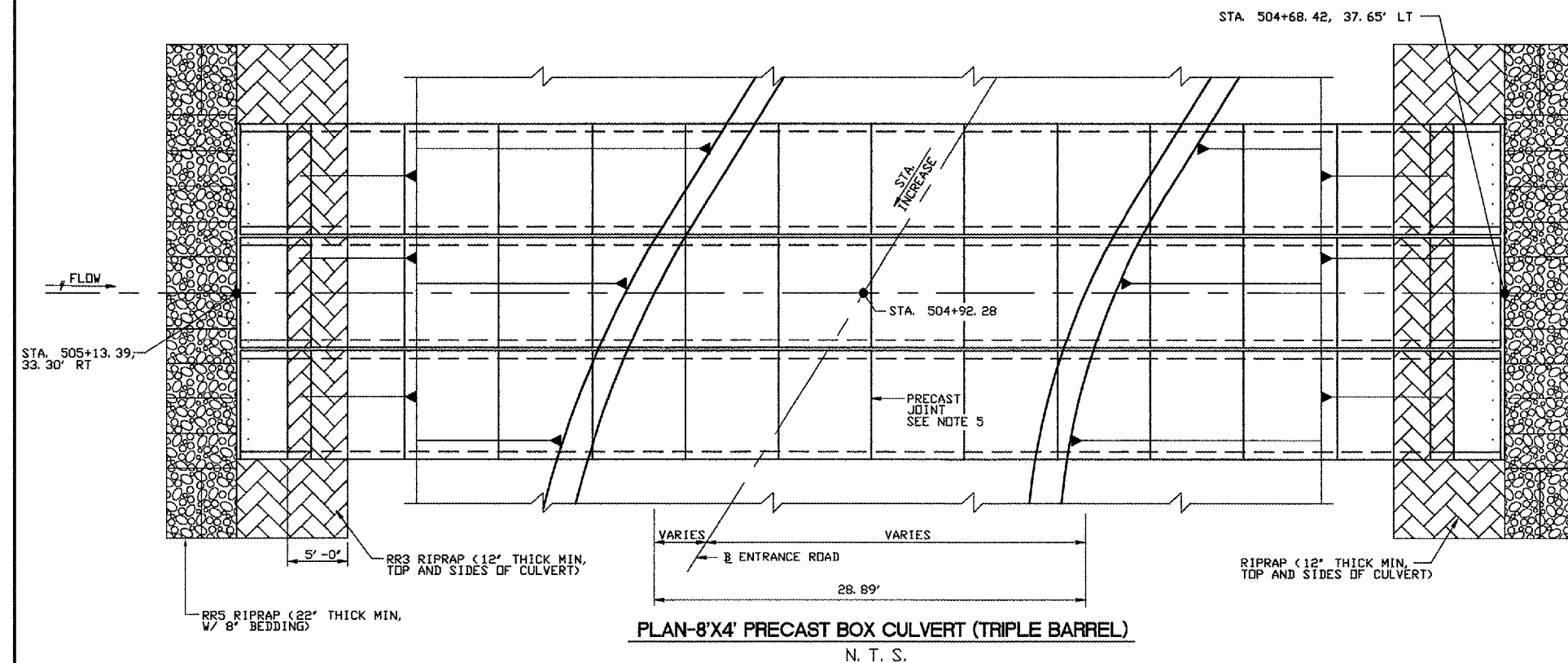
NOTES

1. ALL PRECAST BOX CULVERTS SHALL BE DESIGNED IN ACCORDANCE WITH AASHTO M273, HS-20 LOADING.
2. PRIOR TO BACKFILLING, FILL SOLID THE 3" JOINT BETWEEN BARRELS WITH CONCRETE (610) USING A MAXIMUM SIZE AGGREGATE OF 3/8 INCH.
3. GROUT ALL LIFTING HOLES BEFORE BACKFILLING, ALLOW GROUT TO ACHIEVE MINIMUM STRENGTH BEFORE BACKFILLING.
4. DURING BACKFILL PLACEMENT, DO NOT PERMIT A DIFFERENCE IN FILL ELEVATION ON THE WALLS OF THE CULVERT IN EXCESS OF 2 FEET. DURING COMPACTION, DO NOT ALLOW WHEELS OF ROLLERS TO COME CLOSER THAN ONE FOOT TO THE FACE OF THE STRUCTURE.
5. JOINT MATERIAL BETWEEN PRECAST SECTIONS SHALL CONFORM TO SECTION 1056 OR SECTION 1055 OF IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2002.
6. ALL BOX CULVERT END SECTIONS SHALL HAVE A PRECAST OR CAST IN PLACE TOEWALL.
7. ANY ACCUMULATED DEBRIS WITHIN THE PROJECT AS A RESULT OF THE IMPROVEMENT SHALL BE REMOVED AND DISPOSED OF SATISFACTORILY BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF DRAINAGE AT ALL TIMES DURING THE CONSTRUCTION OF THE CULVERT. THE METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE COST FOR THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE JOINTS SHALL BE EXTERNALLY SEALED ON ALL FOUR SIDES USING EITHER 13 INCH WIDE EXTERNAL SEALING BANDS CONFORMING TO SECTION 1057.01 OR 24" WIDE NONWOVEN GEOTECHNICAL FABRIC MEETING THE REQUIREMENTS OF SECTION 1080.01, IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, EXCEPT THE MINIMUM WEIGHT SHALL BE 4 OZ/SY. THE SEAL OR FABRIC SHALL BE CENTERED OVER THE JOINT AND SECURED TO REMAIN IN PLACE DURING THE BACKFILLING OPERATION.

REVISIONS

NUMBER	BY	DATE
1	ARM	6/10/05

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).



LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS
NORTH QUADRANT SITEWORK - PHASE 1
AND TAXIWAY G2 EXTENSION
PRECAST BOX CULVERT DETAILS

Copyright: CMT, Inc.
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 CONSULTING ENGINEERS
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DESIGN BY:	DKP
DRAWN BY:	JRD
CHECKED BY:	ARM
APPROVED BY:	
DATE:	03/04/05
JOB No:	03297-02
IL PROJECT: IGQ-3329	
A.I.P. PROJECT: 3-17-0121-B21	
SHEET 21 OF 50 SHEETS	