

PALWAUKEE MUNICIPAL AIRPORT COMMISSION WHEELING/PROSPECT HEIGHTS, ILLINOIS



FINAL CONSTRUCTION PLANS FOR

PALWAUKEE MUNICIPAL AIRPORT CONSTRUCT TAXIWAY CHARLIE FROM RUNWAY 24 END TO TAXIWAY KILO AND EAST QUADRANT APRON - PHASE 1

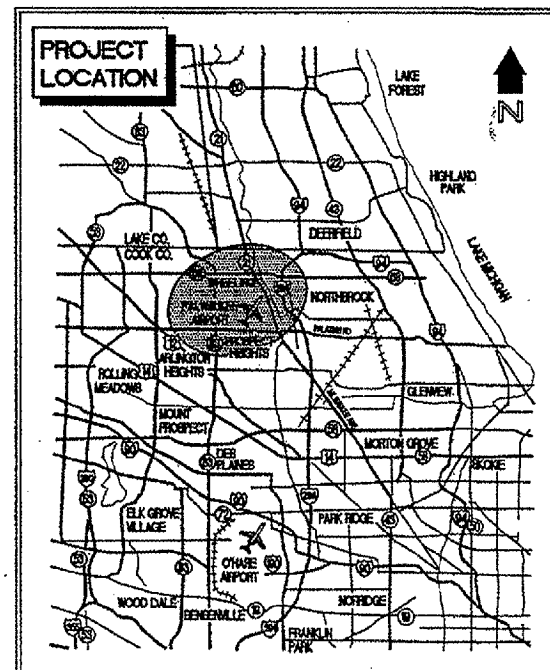
ILLINOIS PROJECT: PWK-3481
A.I.P. PROJECT: 3-17-0018-B
DATE: JUNE 24, 2005

PALWAUKEE MUNICIPAL AIRPORT	
TOWNSHIP: 42 NORTH	WHEELING TOWNSHIP
RANGE: 11 EAST	(SECTION: 13)
COOK COUNTY	

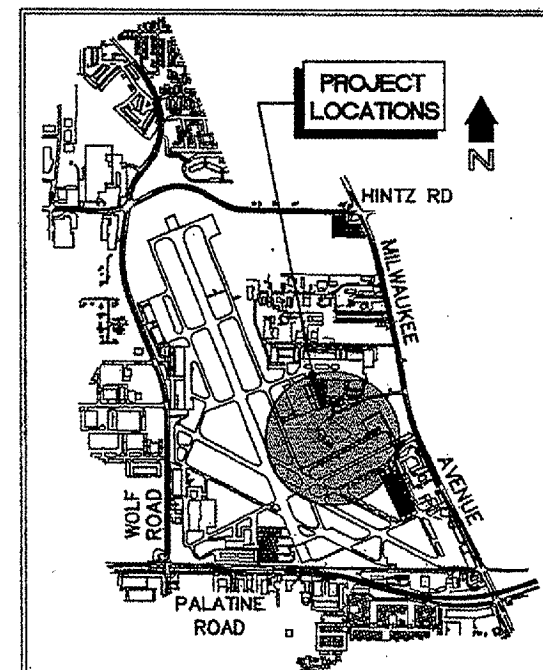
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CRAWFORD MURPHY & TILLY, INC. CONSULTING ENGINEERS	
SUBMITTED BY:	EXPIRES: 11/30/05
DATE: 6/30/05	

PALWAUKEE MUNICIPAL AIRPORT	
APPROVED:	AIRPORT MANAGER
DATE: _____	



LOCATION MAP



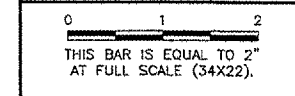
SITE PLAN

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SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	RECORD QUANTITY
AR108108	1/C #8 SKV UG CABLE	LF	8,840	
AR109210	VAULT MODIFICATIONS	LS	1	
AR109321	10 KW REGULATOR, STYLE 1	EACH	1	
AR110202	2" PVC DUCT, DIRECT BURY	LF	5,515	
AR110214	4" STEEL DUCT, DIRECT BURY	LF	160	
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	246	
AR110550	SPLIT DUCT	LF	375	
AR110610	ELECTRICAL HANDHOLE	EACH	2	
AR110714	ELECTRICAL MANHOLE 4"	EACH	7	
AR110907	REMOVE ELECTRICAL MANHOLE	EACH	1	
AR125100	ELEVATED RETROREFLECTIVE MARKER	EACH	12	
AR125415	MITL - BASE MOUNTED	EACH	59	
AR125441	TAXI GUIDANCE SIGN, 1 CHARACTER	EACH	2	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	1	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	1	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	2	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	1	
AR125447	TAXI GUIDANCE SIGN, 7 CHARACTER	EACH	1	
AR125449	TAXI GUIDANCE SIGN, 9 CHARACTER	EACH	1	
AR125461	TAXI GUIDANCE SIGN, SPECIAL	EACH	1	
AR125470	MODIFY EXISTING SIGN PANEL	EACH	2	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	10	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR152540	SOIL STABILIZATION FABRIC	SY	19,960	
AR156510	SILT FENCE	LF	3,750	
AR156511	DITCH CHECK	EACH	7	
AR156520	INLET PROTECTION	EACH	16	
AR156540	RIP RAP	SY	50	
AR162960	RELOCATE CLASS E FENCE	LF	109	
AR163000	TEMPORARY CONSTRUCTION FENCE	LF	550	
AR201610	BITUMINOUS BASE COURSE	TON	190	
AR208515	POROUS GRANULAR EMBANKMENT	CY	1,800	
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	17,755	
AR209607	CRUSHED AGG. BASE COURSE - 7"	SY	1,625	
AR401610	BITUMINOUS SURFACE COURSE	TON	190	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	430	
AR501510	10" PCC PAVEMENT	SY	17,275	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	SY	35	
AR602510	BITUMINOUS PRIME COAT	GAL	480	
AR603510	BITUMINOUS TACK COAT	GAL	120	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	9,875	
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	11,850	
AR620900	PAVEMENT MARKING REMOVAL	SF	1,350	
AR701518	18" RCP, CLASS IV	LF	330	
AR701524	24" RCP, CLASS IV	LF	72	
AR701548	48" RCP, CLASS IV	LF	133	
AR701900	REMOVE PIPE	LF	250	
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	LF	4,100	
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	4	
AR705900	REMOVE UNDERDRAIN	LF	560	
AR705901	REMOVE UNDERDRAIN HEADWALL	EACH	1	
AR705905	REMOVE COLLECTION STRUCTURE	EACH	1	
AR751560	MANHOLE 6"	EACH	4	
AR751903	REMOVE MANHOLE	EACH	2	
AR751943	ADJUST MANHOLE	EACH	1	
AR751983	RECONSTRUCT MANHOLE	EACH	2	
AR752418	PRECAST REINFORCED CONC. FES 18"	EACH	1	
AR752518	GRATING FOR CONC. FES 18"	EACH	1	
AR752906	REMOVE INLET BOX	EACH	1	
AR800001	TYPE 1 INLET	EACH	1	
AR800028	SLOPE BOX INLET 18"	EACH	1	
AR800053	SOIL GUARD	SY	4,905	
AR800103	EMBANKMENT FILL	CY	14,000	
AR800104	SHOULDER FILL	CY	19,400	
AR901510	SEEDING	ACRE	13	
AR905510	TOPSOILING (FROM ON SITE)	CY	3,400	
AR908510	MULCHING	ACRE	12	

REVISIONS		
NUMBER	BY	DATE



**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

SUMMARY OF QUANTITIES

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DESIGN BY:	MJK
DRAWN BY:	JRO
CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	2 OF 38 SHEETS



REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

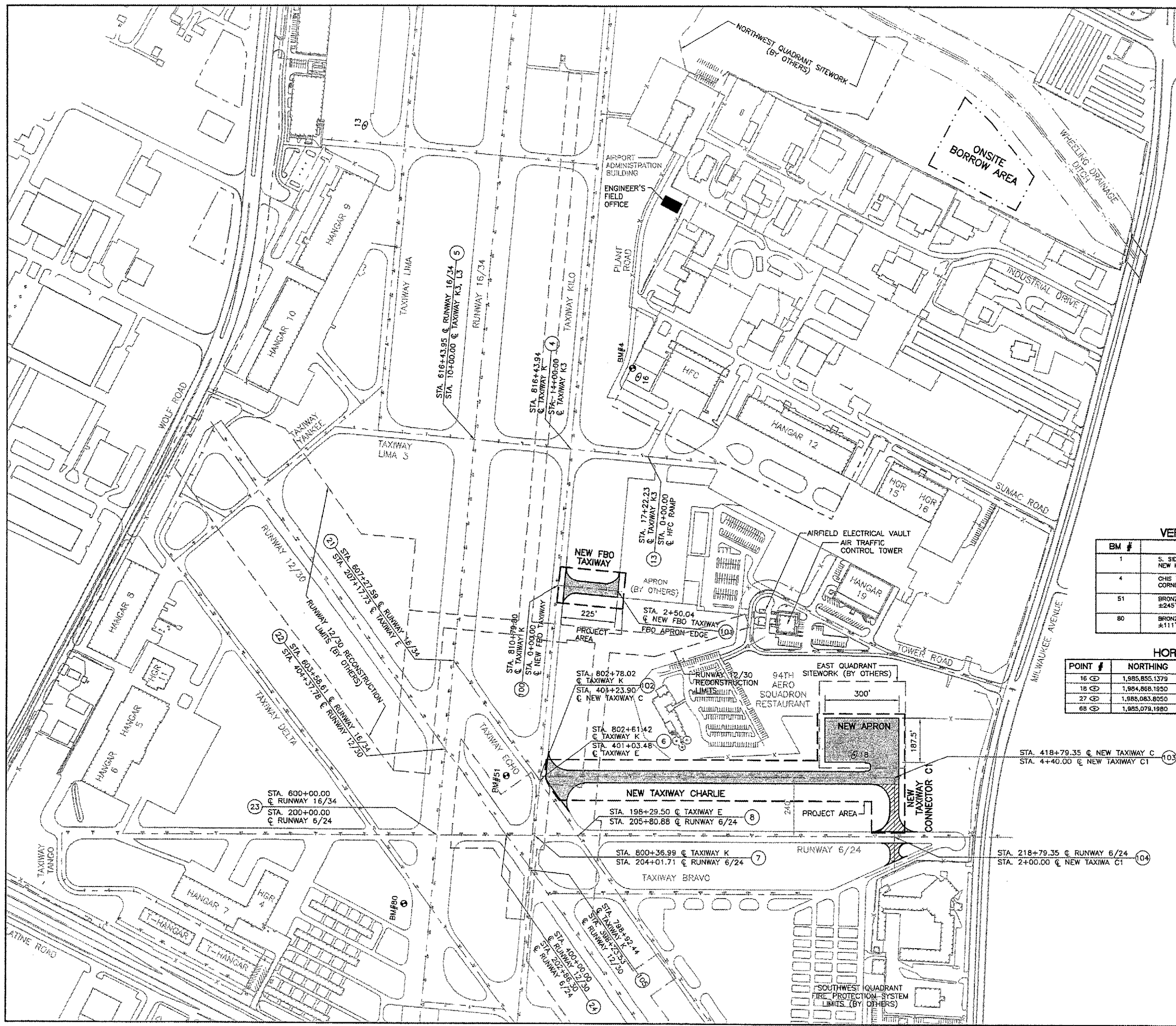
- LEGEND**
- NEW BITUMINOUS PAVEMENT
 - NEW 10" PCC PAVEMENT
 - AIRPORT PROPERTY LINE
 - EXISTING FENCE LINE
 - PROPOSED BORROW SITE
 - PROPOSED PROJECT LIMITS
 - PROJECT LIMITS (BY OTHERS)

VERTICAL CONTROL TABLE

BM #	LOCATION	ELEVATION
1	S. SIDE HEADWALL S. OF SW CORNER OF NEW HINTZ ROAD BRIDGE, CHIS "J"	638.62
4	CHIS "J" W. SIDE LIGHT POLE BASE AT SW CORNER PARKING LOT.	641.59
51	BRONZE DISC ±150' N. OF RUNWAY 12/30 ±245' N.E. OF RUNWAY 16/34	641.51
80	BRONZE DISC ±74' S.W. OF TAXIWAY D ±111' W. OF RUNWAY 16/34	642.62

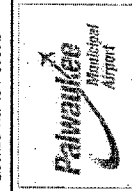
HORIZONTAL CONTROL TABLE

POINT #	NORTHING	EASTING	DESCRIPTION
16	1,985,855.1379	617,111.5731	CHS "X" ON B.O.C.
18	1,984,868.1950	618,606.0940	IR ON BERM ALONG 94TH ENTRANCE DRIVE
27	1,988,083.8050	616,435.7710	CHS "X" B.O.C. S. SIDE OF NEW HINTZ ROAD
68	1,985,078.1980	615,836.4740	PK IN ASPHALT SERVICE ROAD TO RUNWAY 12

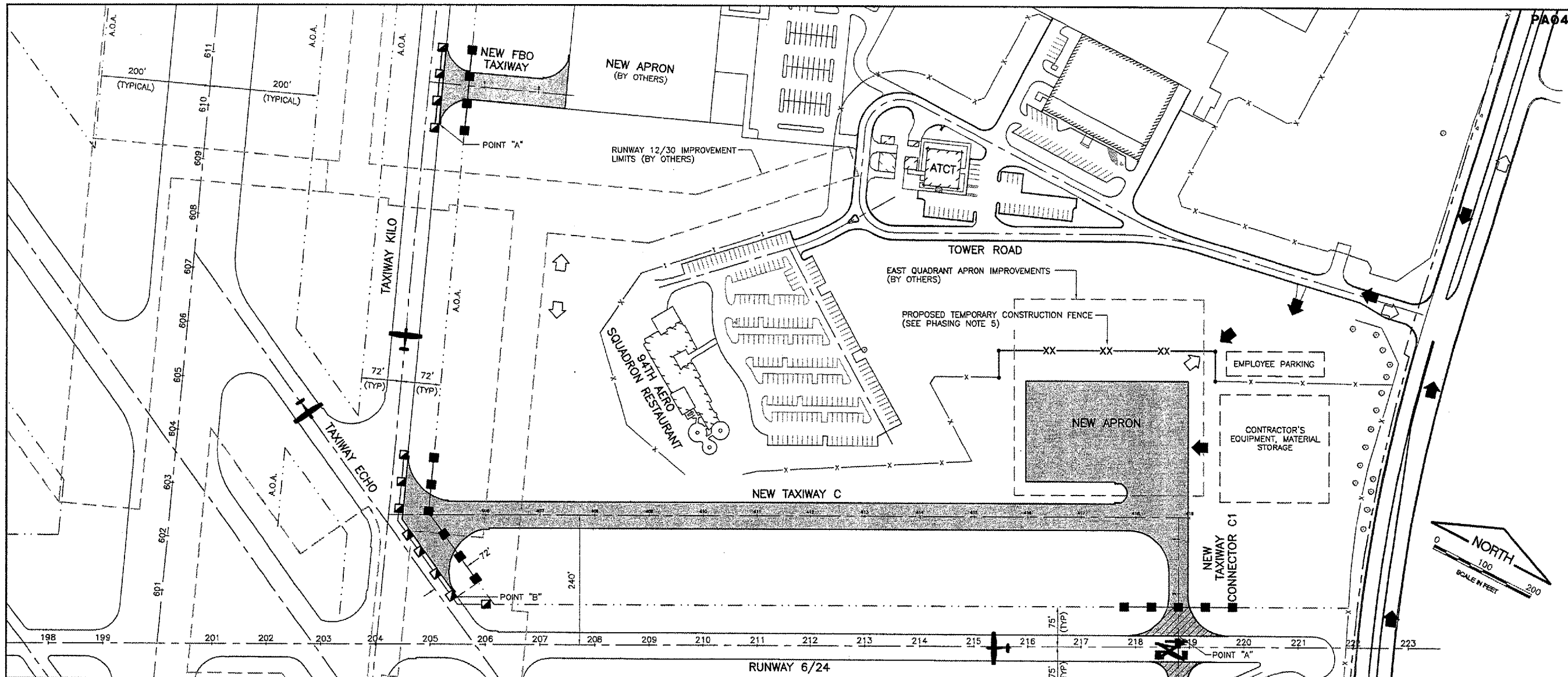


**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**
**CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 SITE PLAN/PROJECT CONTROL PLAN**

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 ILLINOIS PROJECT: PWK-3481
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 SHEET 3 OF 38 SHEETS

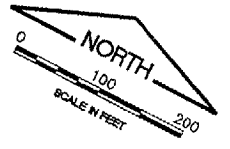


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 LAYOUT: Layout1
 UPDATE BY: johse
 SURVEY BOOK #
 DATE: Fri 7/1/05 3:28pm
 XREF DWG: tbcint.dwg
 tb.dwg
 txy-base.dwg

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
**SEQUENCE OF CONSTRUCTION
 PER AC 150/5370-2**



PHASING NOTES (ALL PHASES)

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE RUNWAY SAFETY AREA (200 FEET FROM CENTERLINE) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES, THE MAXIMUM PAVEMENT DROPOFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT. TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MIGHT BE REQUIRED TO MEET CRITERIA.
3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
4. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE ACTING AIRPORT MANAGER AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATE OF THE CLOSURE.
5. THE CONTRACTOR SHALL SECURE THE AIRFIELD AT THE END OF EACH WORKING DAY BY PLACING TEMPORARY CONSTRUCTION FENCE AS SHOWN. TEMPORARY CONSTRUCTION FENCE SHALL ONLY BE PAID FOR ONCE REGARDLESS OF HOW MANY TIMES IT IS TAKEN DOWN OR PUT UP TO ALLOW FOR CONSTRUCTION OPERATIONS. WITH PRIOR APPROVAL OF THE ENGINEER AND AIRPORT, THE CONTRACTOR MAY PLACE THE CONSTRUCTION FENCE AT AN ALTERNATIVE LOCATION TO SECURE THE AIRFIELD.

LEGEND

- PHASE 1 WORK
- PHASE 2 WORK
- AIRCRAFT MOVEMENT AREA
- BARRICADE WITH FLASHING LIGHTS AND SIGNS PHASE 1A ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- BARRICADE WITH FLASHING LIGHTS AND SIGNS PHASE 1B ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- BARRICADE WITH FLASHING LIGHTS AND SIGNS PHASE 2 ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- AIR OPERATIONS AREA (A.O.A.)
ACTIVE RUNWAYS 200' CENTERLINE TO A.O.A.
ACTIVE TAXIWAYS 72' CENTERLINE TO A.O.A.
- TEMPORARY CLOSED RUNWAY MARKER DURING PHASE 2. (BOTH RUNWAY ENDS)
- CONTRACTOR'S ACCESS/HAUL ROAD DURING PHASE 1.
- CONTRACTOR'S ACCESS/HAUL ROAD (BY OTHERS).
- PROPOSED TEMPORARY CONSTRUCTION FENCE
- WORK LIMITS (BY OTHERS)

LIQUIDATED DAMAGES (ALL PHASES)

1. A MAXIMUM OF 21 CALENDAR DAYS FOR THE CLOSURE OF RUNWAY 6/24 WILL BE ALLOWED.
2. WORK IN THE AIRCRAFT RUNWAY OPERATIONS AREA OF RUNWAY 6/24 AND TAXIWAY KILO SHALL BE EXPEDITED. THE CONTRACTOR SHALL ENSURE THAT TAXIWAY KILO BE OPEN TO AIRPORT TRAFFIC AT THE CLOSE OF EACH DAY.

WORK AREA	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
PHASE 1A • WORK OUTSIDE RUNWAY 6/24, TAXIWAY B, E AND K AIR OPERATIONS AREA	NO RESTRICTIONS	• RUNWAY 6/24 OPEN • TAXIWAY B, E AND K OPEN
PHASE 1B • WORK OUTSIDE RUNWAY 6/24 AND TAXIWAY B AIR OPERATIONS AREA • WORK WITHIN TAXIWAY E AND K AIR OPERATIONS AREA	NO RESTRICTIONS	• TAXIWAY 6/24 OPEN • TAXIWAY B OPEN • TAXIWAY E AND K TEMPORARILY CLOSED DURING WORK IN AIR OPERATIONS AREA
PHASE 2 • WORK OUTSIDE TAXIWAY E AND K AIR OPERATIONS AREA • WORK WITHIN RUNWAY 6/24 AND TAXIWAY B AIR OPERATIONS AREA	WORK WITHIN RUNWAY 6/24 AIR OPERATIONS AREA TO BE COMPLETED WITHIN 21 CALENDAR DAYS	• RUNWAY 6/24 CLOSED • TAXIWAY B CLOSED • TAXIWAY E AND K OPEN

CLOSEST CONSTRUCTION POINTS TO RUNWAY CENTERLINE
POINT "A" 21.27' OFFSET CENTERLINE RUNWAY 6/24 LATITUDE: 42°06'55.31" (NAD83) LONGITUDE: 87°54'04.53" (NAD83) ELEVATION: 642.1
POINT "B" 88.71' OFFSET CENTERLINE RUNWAY 6/24 LATITUDE: 42°06'47.75" (NAD83) LONGITUDE 87°53'58.94" (NAD83) ELEVATION: 639.1
POINT "C" 425' OFFSET CENTERLINE RUNWAY 16/34 LATITUDE: 42°06'53.06" (NAD83) LONGITUDE 87°53'42.77" (NAD83) ELEVATION: 637.8

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Palwaukee
 Municipal Airport

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 DRAWN BY: JRO
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 DATE: 06/24/05
 JOB No: 04290-03
 ILLINOIS PROJECT: PWK-3481
 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 4 OF 38 SHEETS

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2C (LATEST EDITION) SAFETY DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (24" x 24") BETWEEN EACH SET OF BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO DOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS. EQUIPMENT OR MATERIAL SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. IN CONJUNCTION WITH DOT TYPE II BARRICADES, THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE BARREL TYPE BARRICADES.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL, THIS SHEET.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- PALWAUKEE MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 72' FROM ACTIVE TAXIWAYS AND 200' FROM ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

NOTE - ALL PHASES
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

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UPDATE BY: johse
SURVEY BOOK #
DATE: Wed 6/29/05 4:50pm
XREF DWG: tbcint.dwg
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txy-base.dwg

DESIGN AIRCRAFT APPROACH CATEGORY: C
DESIGN AIRPORT GROUP: III

GROUND CONTROL FREQUENCY: 121.7
AIR CONTROL FREQUENCY: 119.9
MAXIMUM ANTICIPATED HEIGHT
OF CONSTRUCTION EQUIPMENT: 25'

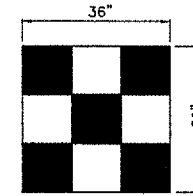
IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE AN ON-SITE CONCRETE BATCH PLANT, LOCATION TO BE COORDINATED WITH RESIDENT ENGINEER AND ACTING AIRPORT MANAGER TO ALLOW FOR APPROPRIATE AIRSPACE CLEARANCE. THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

RUNWAY 24 THRESHOLD

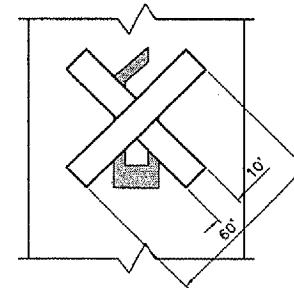
LATITUDE: 42°06'48.49" (NAD83)
LONGITUDE: 87°53'54.42" (NAD83)

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS/SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT.

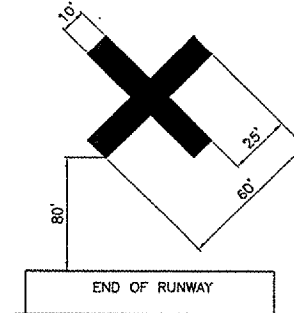
- RECONSTRUCT RUNWAY 12/30
- CONSTRUCT S.W. QUADRANT FIRE PROTECTION SYSTEM
- CONSTRUCT N.E. QUADRANT SITEWORK AND EAST QUADRANT APRON SITEWORK



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE



OFF PAVEMENT CLOSED RUNWAY MARKER DETAIL
NO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (A.O.A.)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. THE CONTRACTOR WILL BE LIMITED TO 21 DAILY CLOSURES ON RUNWAY 6/24.

TAXIWAYS:

ANY WORK WITHIN 72' OF THE TAXIWAY CENTERLINE WILL REQUIRE A TAXIWAY CLOSURE. CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS AS DETERMINED BY THE AIRPORT MANAGER. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

REVISIONS

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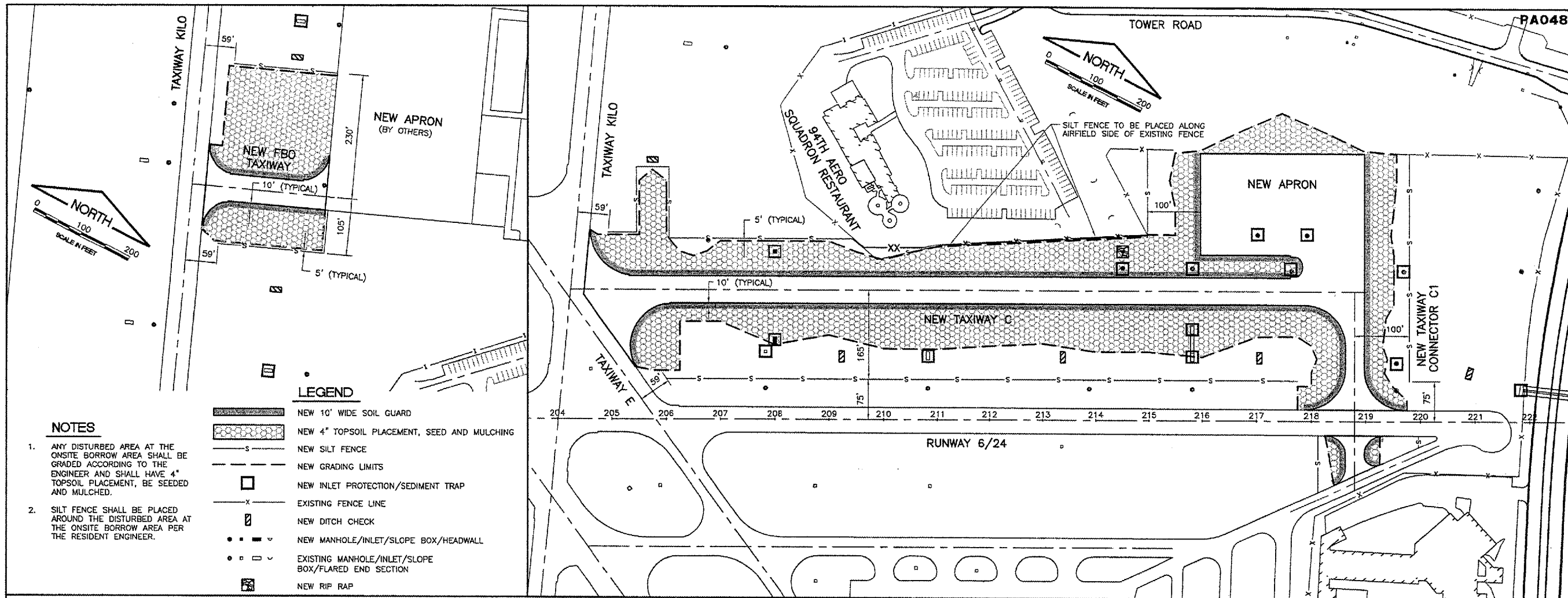
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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 SEQUENCE OF CONSTRUCTION
 GENERAL NOTES AND DETAILS

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DRAWN BY:	JRL
CHECKED BY:	
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DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-34B1
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	5 OF 38 SHEETS



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 UPDATE BY: johse
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**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
STORMWATER POLLUTION PREVENTION PLAN

NOTES

- ANY DISTURBED AREA AT THE ONSITE BORROW AREA SHALL BE GRADED ACCORDING TO THE ENGINEER AND SHALL HAVE 4" TOPSOIL PLACEMENT, BE SEEDED AND MULCHED.
- SILT FENCE SHALL BE PLACED AROUND THE DISTURBED AREA AT THE ONSITE BORROW AREA PER THE RESIDENT ENGINEER.

LEGEND

- NEW 10' WIDE SOIL GUARD
- NEW 4" TOPSOIL PLACEMENT, SEED AND MULCHING
- NEW SILT FENCE
- NEW GRADING LIMITS
- NEW INLET PROTECTION/SEDIMENT TRAP
- EXISTING FENCE LINE
- NEW DITCH CHECK
- NEW MANHOLE/INLET/SLOPE BOX/HEADWALL
- EXISTING MANHOLE/INLET/SLOPE BOX/FLARED END SECTION
- NEW RIP RAP

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING TAXIWAY CHARLIE AND EAST QUADRANT APRON AT THE PALWAUKEE MUNICIPAL AIRPORT. THE PROJECT INCLUDES EARTH EXCAVATION, EMBANKMENT, STORM SEWERS, MANHOLES, INLETS VARIOUS PAVEMENT ITEMS, ELECTRICAL WORK AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

- EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED DRAINAGE AND PAVEMENT IMPROVEMENTS.
- STORM SEWERS, MANHOLES, INLETS AND CULVERT INSTALLATION.
- PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS PERIMETER SILT FENCE, TEMPORARY DITCH CHECKS AND INLET PROTECTION.
- PAVEMENT CONSTRUCTION.
- FINAL GRADING, ELECTRICAL INSTALLATION AND OTHER MISCELLANEOUS ITEMS.
- PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING, MULCHING AND EROSION CONTROL BLANKET.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 17 ACRES OF WHICH 13 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM.

CONTROLS--EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1.THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1.WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

2.EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.

3.AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.
- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

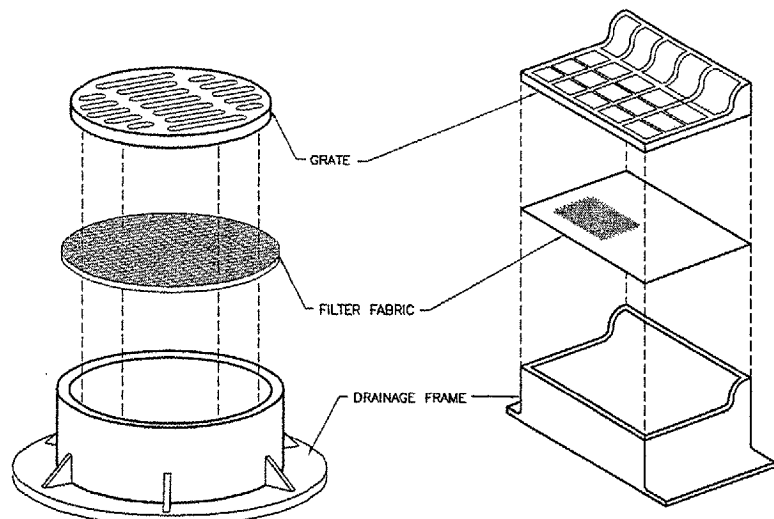
- TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.
- ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

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APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
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A.I.P. PROJECT: 3-17-001B-BXX	
SHEET 6 OF 38 SHEETS	

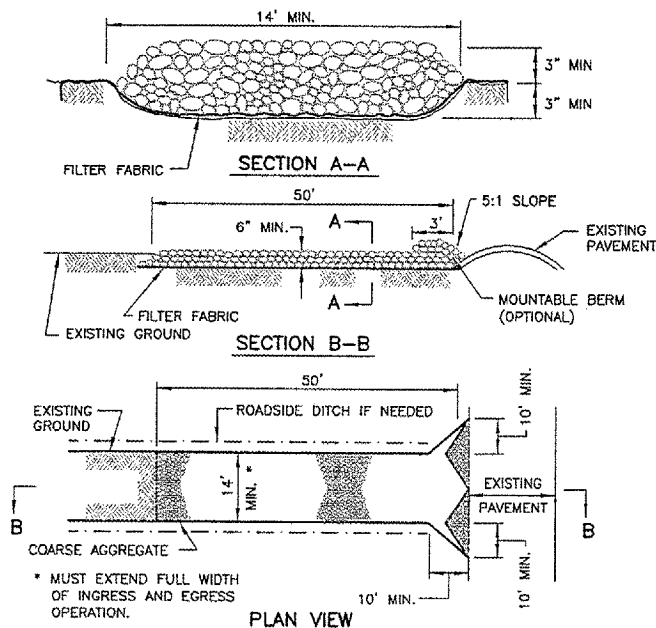


NOTES:

1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO INLET PROTECTION.

DRAINAGE STRUCTURE FILTER WRAP

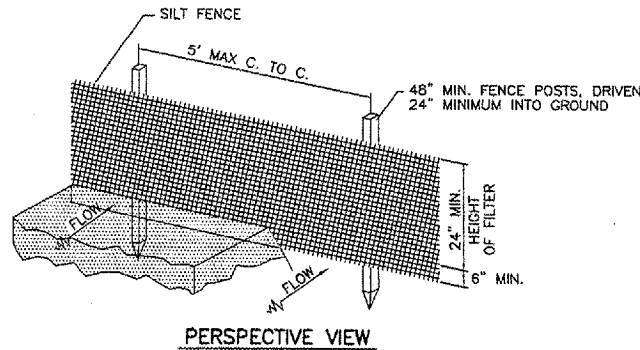
N.T.S.



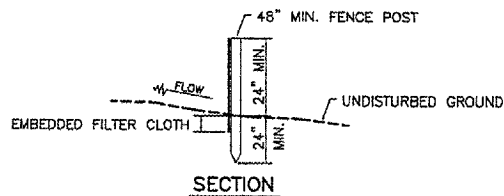
1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND REQUIRING SPECIAL PROVISIONS.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC, DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.

STABILIZED CONSTRUCTION ENTRANCE

FROM NRCS STANDARD DRAWING NO. IL-630



PERSPECTIVE VIEW



SECTION

CONSTRUCTION NOTES FOR SILT FENCE

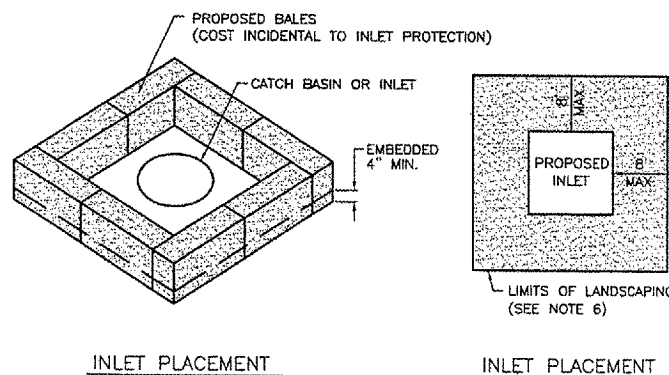
1. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MIN. AND FOLDED.
2. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
3. SILT FENCE SHALL BE INSTALLED PER LANDSCAPING/EROSION CONTROL PLAN OR AS DIRECTED BY THE ENGINEER.

EROSION CONTROL FABRIC FENCE DETAIL

N.T.S.

NOTES

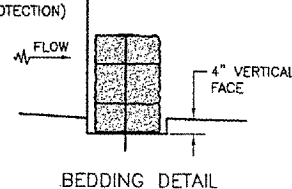
1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR INLET PROTECTION/DITCH CHECK.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO INLET PROTECTION.



INLET PLACEMENT

INLET PLACEMENT

PROPOSED EROSION CONTROL FENCING (COST INCIDENTAL TO INLET PROTECTION)



BEDDING DETAIL

STORM INLET SEDIMENT TRAP DETAIL - TURF AREAS

N.T.S.



**ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
NOTICE OF INTENT (NOI)
GENERAL PERMIT TO DISCHARGE STORM SEWER
CONSTRUCTION SITE ACTIVITIES**

PA048

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XREF DWG:
DATE: Fri 5/21/04 1:49pm

IMPORTANT: FORM MUST BE TYPED, TO ENABLE AUTOMATED OPTICAL PROCESSING.
SUBMIT ORIGINAL - DO NOT SUBMIT PHOTOCOPY

OWNER INFORMATION

NAME: PALWAUKEE MUNICIPAL AIRPORT
ADDRESS: 1020 SOUTH PLANT ROAD
CITY: WHEELING ST. IL ZIP: 60090
CONTACT PERSON: DENNIS ROULEAU
TELEPHONE NUMBER: 847 537-2580

CONTRACTOR INFORMATION

NAME: _____
ADDRESS: _____
CITY: _____

CONSTRUCTION SITE INFORMATION

SELECT ONE AND TYPE: EXISTING SITE NEW SITE CHANGE OF INFORMATION

FACILITY NAME: PALWAUKEE MUNICIPAL AIRPORT
ADDRESS: 1020 SOUTH PLANT ROAD
CITY: WHEELING ST. IL ZIP: 60090
COUNTY: COOK SECTION: 13 TOWNSHIP: 42 NORTH RANGE: 11 EAST
START DATE: MM/DD/YY END DATE: MM/DD/YY
TOTAL SIZE OF CONSTRUCTION SITE IN ACRES: 16.9 ACRES

TYPE OF CONSTRUCTION

RESIDENTIAL COMMERCIAL INDUSTRIAL RECONSTRUCTION TRANSPORTATION OTHER

RECEIVING WATER INFORMATION

DOES YOUR STORM WATER DISCHARGE DIRECTLY TO: (SELECT ONE AND TYPE "X")
 WATER OF THE STATE OR STORM SEWER SYSTEM WHEELING/PROSPECT HTS.

NAME OF CLOSEST RECEIVING WATER (IF KNOWN): DES PLAINES RIVER
DOES THE QUANTITATIVE DATA CURRENTLY EXIST WHICH DESCRIBES THE CONCENTRATION OF POLLUTANTS IN THE STORM WATER DISCHARGE? YES NO

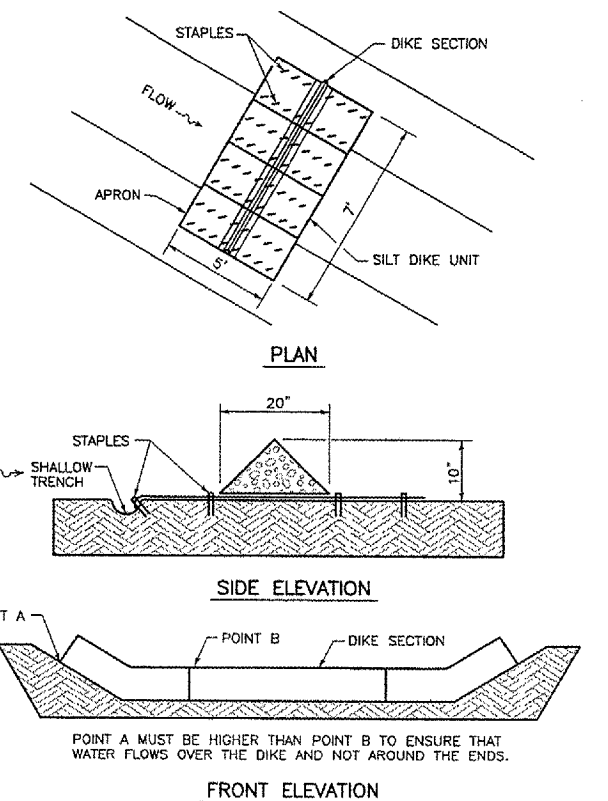
I certify under penalty of law that the applicant and all applicants were prepared under my direction and supervision to ensure that the information submitted is true, accurate, and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment. In addition, I certify that the provisions of the permit, including the development and implementation of a Storm Water Pollution Prevention Plan and a Sedimentation Program Plan, will be accepted and followed.

OWNER SIGNATURE: _____ DATE: _____

MAIL COMPLETED FORM TO: ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, DIVISION OF WATER POLLUTION CONTROL, ATTN: PERMIT SECTION, 2200 CHURCHILL ROAD, POST OFFICE BOX 19276, SPRINGFIELD, IL 62784-9276

FOR OFFICE USE ONLY
LOG _____
PERMIT _____
DATE: _____

This Agency is authorized to require this information under Illinois Revised Statute, 1981, Chapter 111 1/2, section 1039. Information is required under that Section. Failure to do so may prevent this form from being processed and could result in your application being denied. This form has been approved by the Permit Management Center.



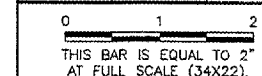
FRONT ELEVATION

URETHANE FOAM/GEOTEXTILE DITCH CHECK

NOT TO SCALE
FROM IDOT STANDARD 280001-02

REVISIONS

NUMBER	BY	DATE



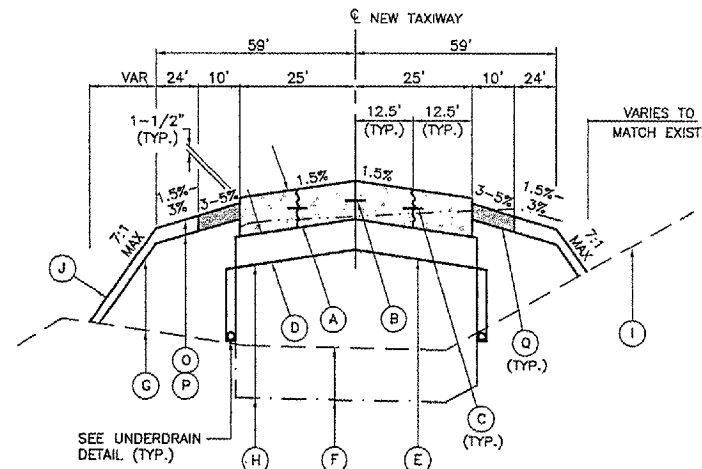
**PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS**

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

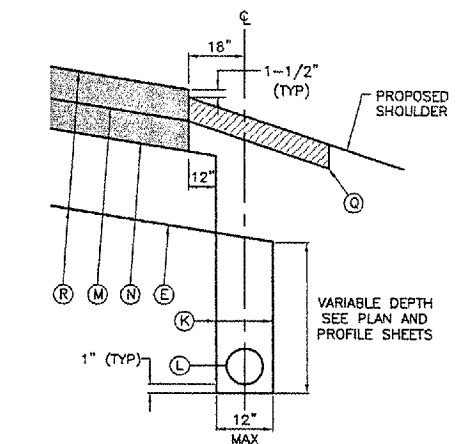
**STORM WATER POLLUTION PREVENTION
PLAN NOTES AND DETAILS**

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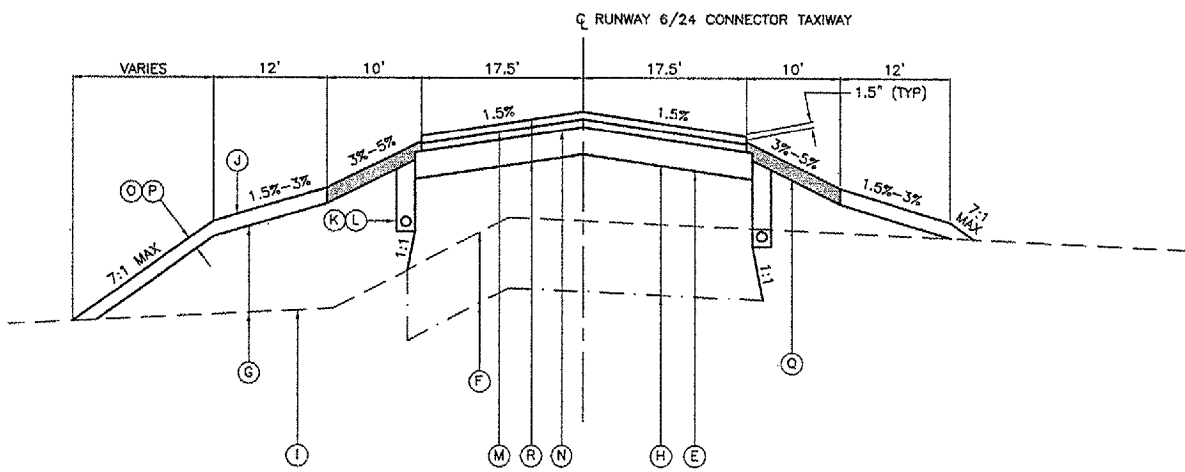
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APPROVED BY:
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JOB No: 04290-03
ILLINOIS PROJECT: PWK-3481
A.I.P. PROJECT: 3-17-0018-BXX
SHEET 7 OF 38 SHEETS



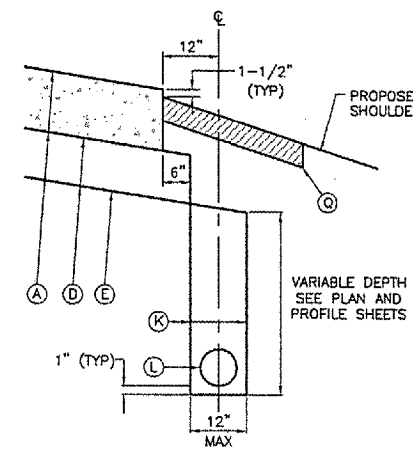
NEW TAXIWAY C AND FBO TAXIWAY TYPICAL SECTION A-A
 NOT TO SCALE



UNDERDRAIN DETAIL
 EDGE OF BITUMINOUS PAVEMENT AREAS
 NOT TO SCALE



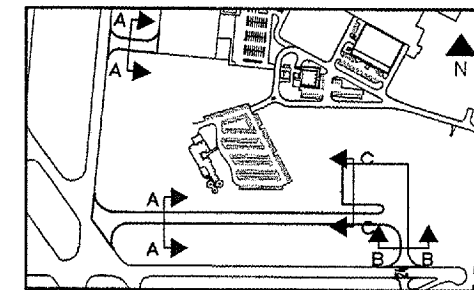
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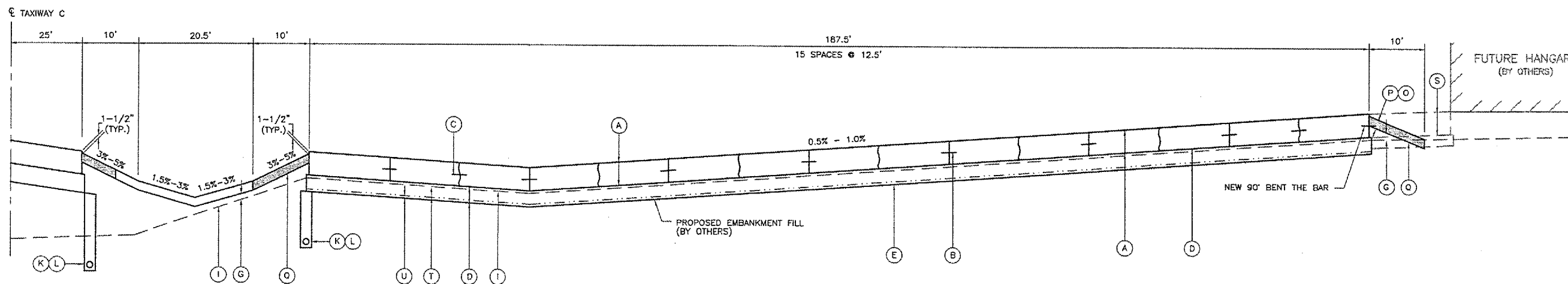
UNDERDRAIN DETAIL
 EDGE OF P.C.C. PAVEMENT AREAS
 NOT TO SCALE

LEGEND

- (A) NEW 10" P.C. CONCRETE PAVEMENT (501)
- (B) NEW DOWEL BAR (501)
- (C) NEW TIE BAR (501)
- (D) NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
- (E) NEW SOIL STABILIZATION FABRIC (152)
- (F) PROPOSED 6" AVERAGE TOPSOIL STRIPPING (152)
- (G) PROPOSED SHOULDER FILL (152)
- (H) PROPOSED EMBANKMENT FILL (152)
- (I) EXISTING GROUND LINE
- (J) PROPOSED GROUNDLINE
- (K) PROPOSED POROUS BACKFILL (705)
- (L) NEW 6" CPPUP WITH SOCK (705)
- (M) PROPOSED TACK COAT (603)
- (N) PROPOSED PRIME COAT (602)
- (O) PROPOSED TOPSOIL PLACEMENT (4" MINIMUM)(905)
- (P) PROPOSED SEEDING AND MULCHING (901 AND 908)
- (Q) NEW 10" WIDE SOIL GUARD (800)
- (R) NEW 2" BITUMINOUS SURFACE COURSE (401)
 NEW 2" BITUMINOUS BASE COURSE (201)
 NEW 7" CRUSHED AGGREGATE BASE COURSE (209)
- (S) FUTURE APRON PAVEMENT (BY OTHERS)
- (T) PROPOSED 4" AVERAGE TOPSOIL STRIPPING (152)
- (U) PROPOSED 1" AVERAGE UNCLASSIFIED EXCAVATION (152)

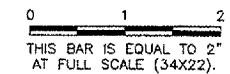


KEY MAP



NEW EAST QUADRANT APRON TYPICAL SECTION C-C
 NOT TO SCALE

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NUMBER	BY	DATE

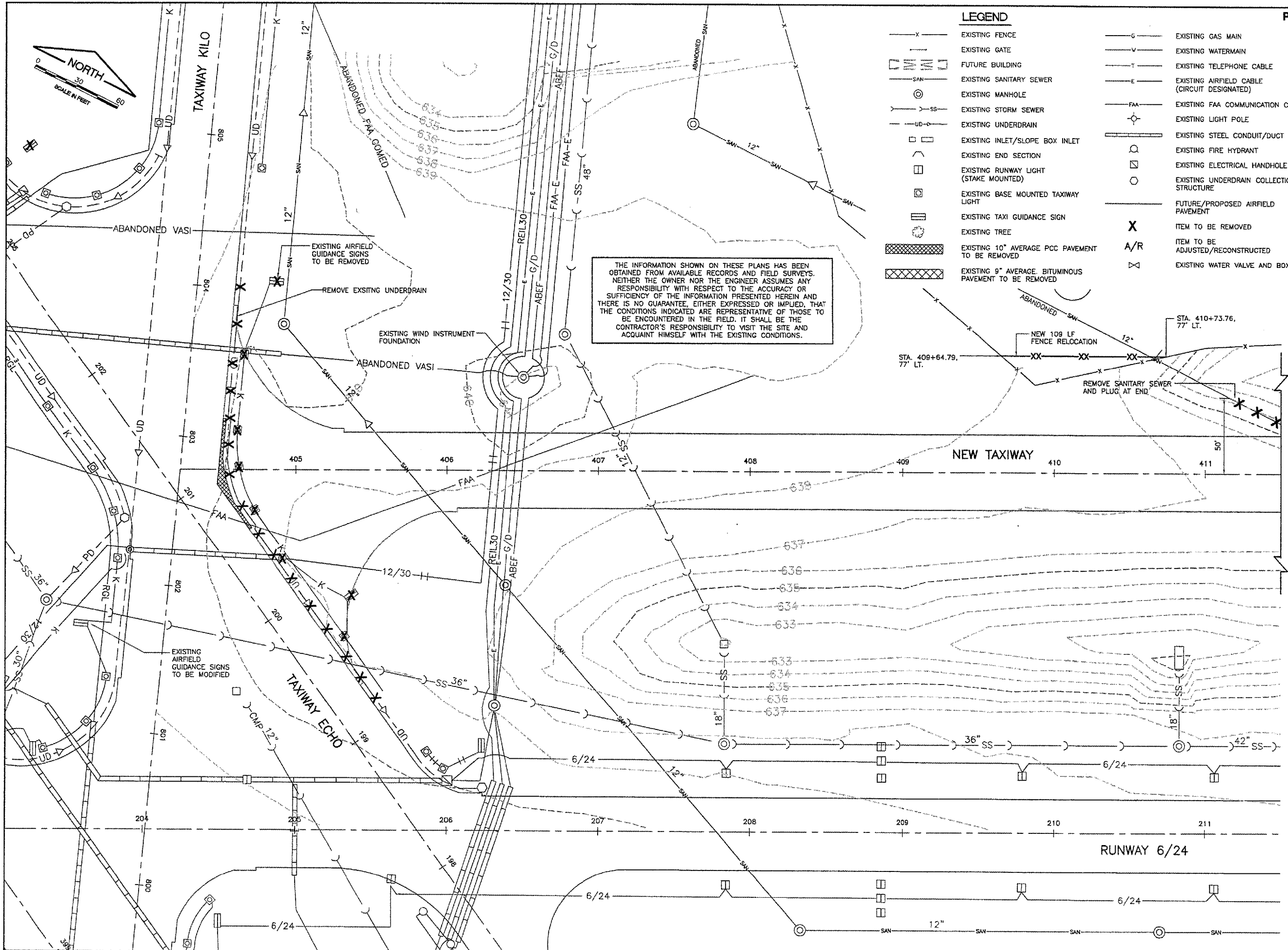


PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 TYPICAL SECTIONS

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JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	8 OF 38 SHEETS



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LEGEND

- X EXISTING FENCE
- EXISTING GATE
- [] FUTURE BUILDING
- SAN — EXISTING SANITARY SEWER
- ⊙ EXISTING MANHOLE
- SS — EXISTING STORM SEWER
- UD — EXISTING UNDERDRAIN
- [] EXISTING INLET/SLOPE BOX INLET
- EXISTING END SECTION
- [] EXISTING RUNWAY LIGHT (STAKE MOUNTED)
- [] EXISTING BASE MOUNTED TAXIWAY LIGHT
- [] EXISTING TAXI GUIDANCE SIGN
- ⊙ EXISTING TREE
- [] EXISTING 10" AVERAGE PCC PAVEMENT TO BE REMOVED
- [] EXISTING 9" AVERAGE BITUMINOUS PAVEMENT TO BE REMOVED
- G — EXISTING GAS MAIN
- V — EXISTING WATERMAIN
- T — EXISTING TELEPHONE CABLE
- E — EXISTING AIRFIELD CABLE (CIRCUIT DESIGNATED)
- FAA — EXISTING FAA COMMUNICATION CABLES
- ⊙ EXISTING LIGHT POLE
- [] EXISTING STEEL CONDUIT/DUCT BANK
- ⊙ EXISTING FIRE HYDRANT
- [] EXISTING ELECTRICAL HANDHOLE
- ⊙ EXISTING UNDERDRAIN COLLECTION STRUCTURE
- FUTURE/PROPOSED AIRFIELD PAVEMENT
- X ITEM TO BE REMOVED
- A/R ITEM TO BE ADJUSTED/RECONSTRUCTED
- ⊗ EXISTING WATER VALVE AND BOX

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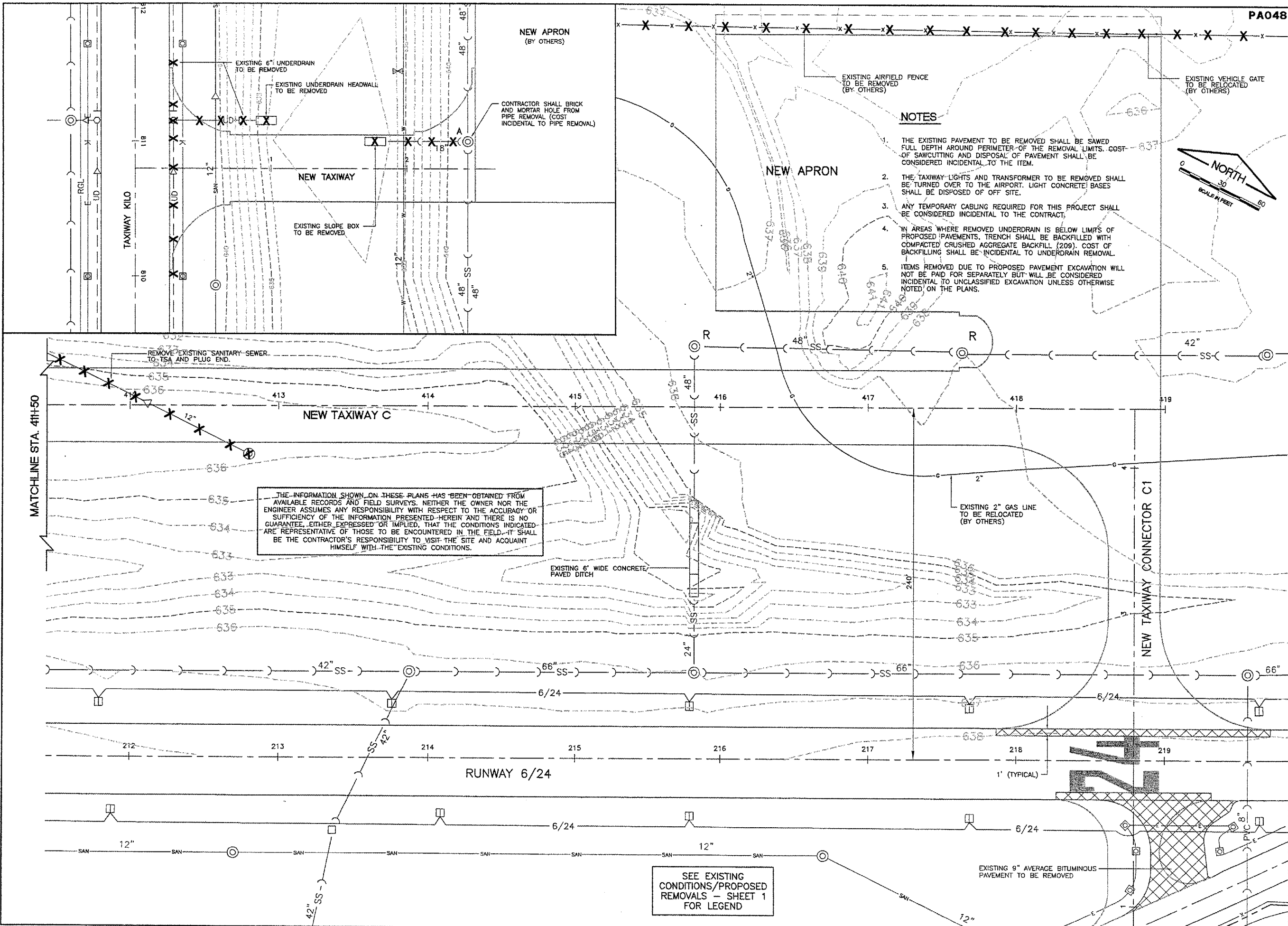
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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
EXISTING CONDITIONS/
PROPOSED REMOVALS - SHEET 1

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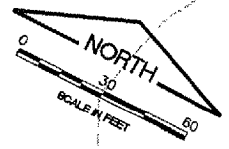
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 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 9 OF 38 SHEETS



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NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWS CUT FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. THE TAXIWAY LIGHTS AND TRANSFORMER TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. LIGHT CONCRETE BASES SHALL BE DISPOSED OF OFF SITE.
3. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. IN AREAS WHERE REMOVED UNDERDRAIN IS BELOW LIMITS OF PROPOSED PAVEMENTS, TRENCH SHALL BE BACKFILLED WITH COMPACTED CRUSHED AGGREGATE BACKFILL (209). COST OF BACKFILLING SHALL BE INCIDENTAL TO UNDERDRAIN REMOVAL.
5. ITEMS REMOVED DUE TO PROPOSED PAVEMENT EXCAVATION WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION UNLESS OTHERWISE NOTED ON THE PLANS.

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SEE EXISTING CONDITIONS/PROPOSED REMOVALS - SHEET 1 FOR LEGEND

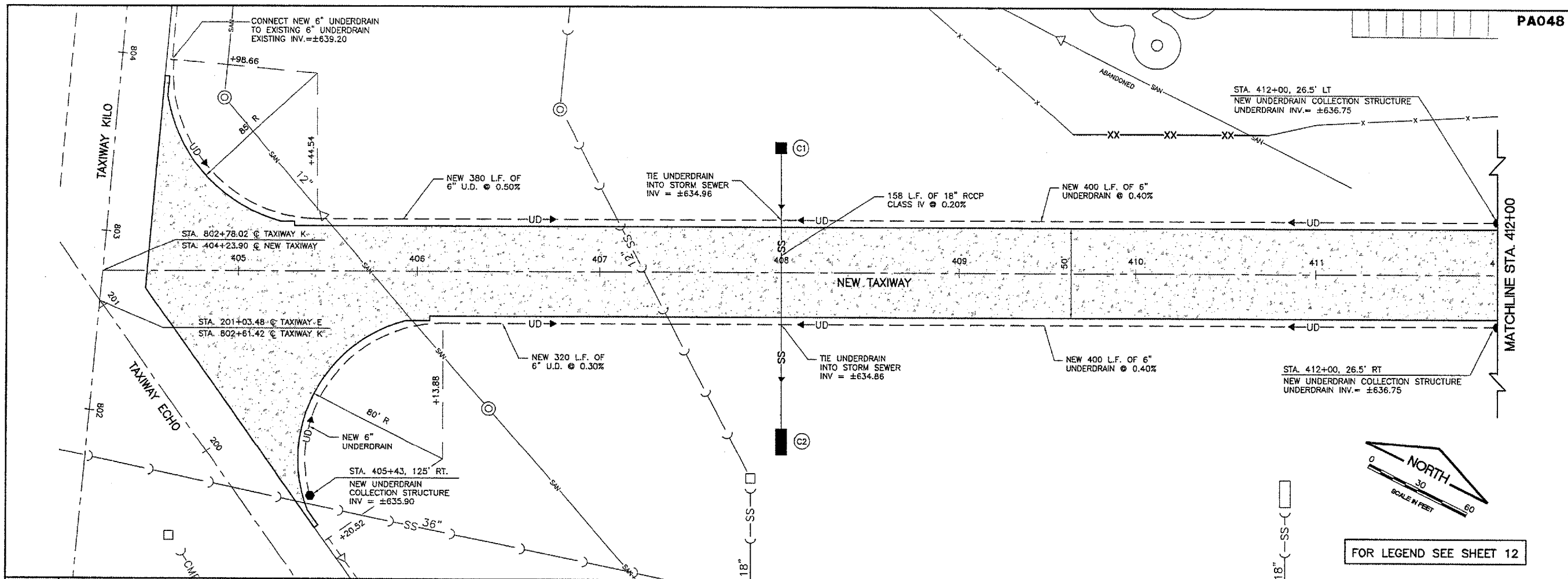
**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**

**CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 EXISTING CONDITIONS/
 PROPOSED REMOVALS - SHEET 2**

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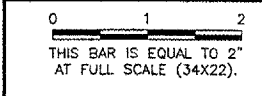
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A.I.P. PROJECT:	3-17-0018-BXX
SHEET	10 OF 38 SHEETS



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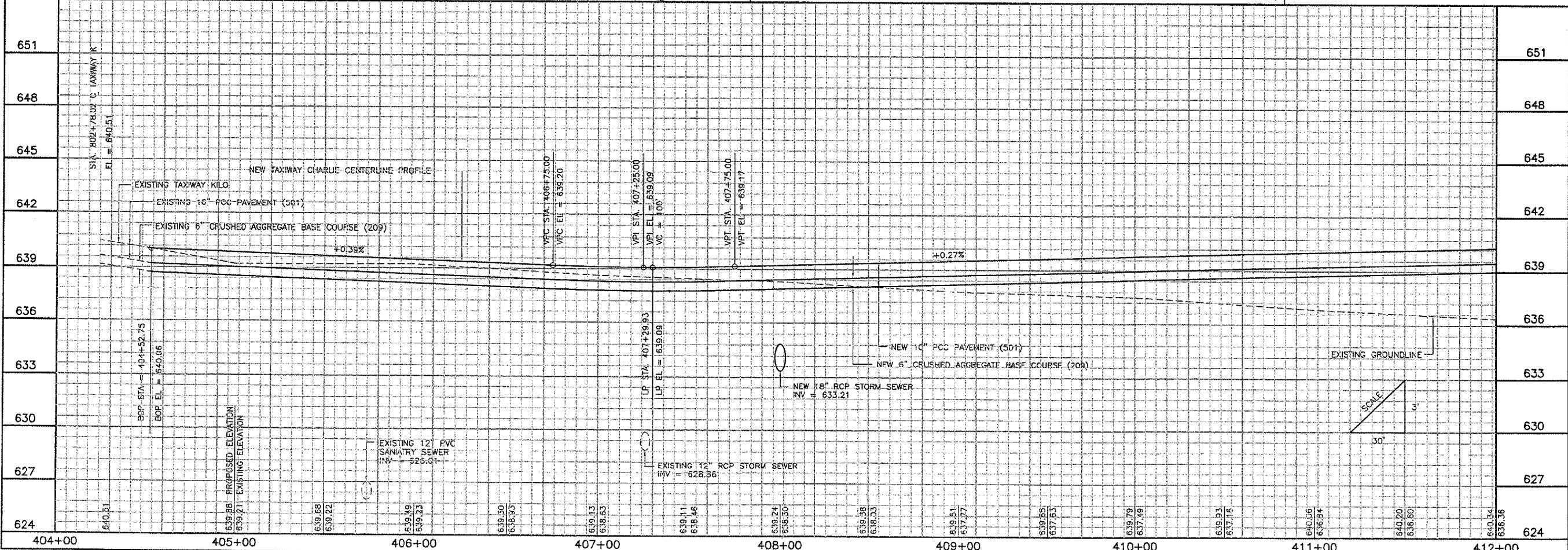
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**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**

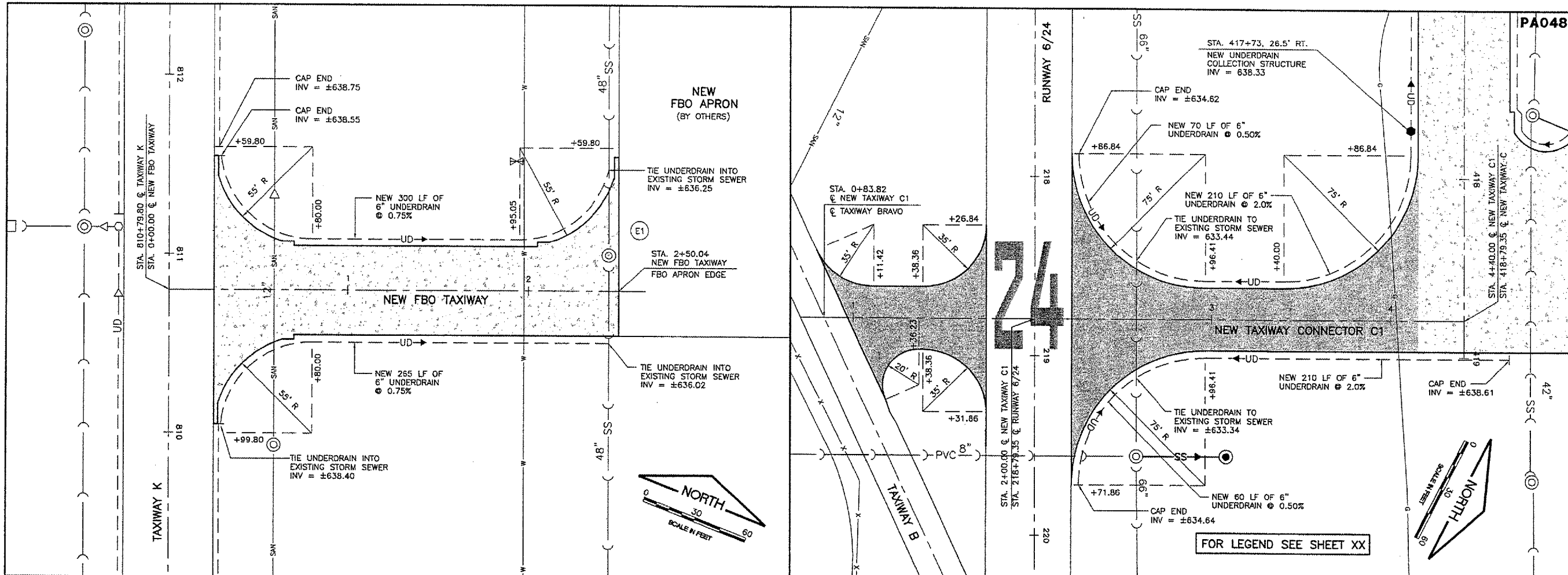
**CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 PLAN AND PROFILE
 STA. 404+00 TO STA. 412+00**



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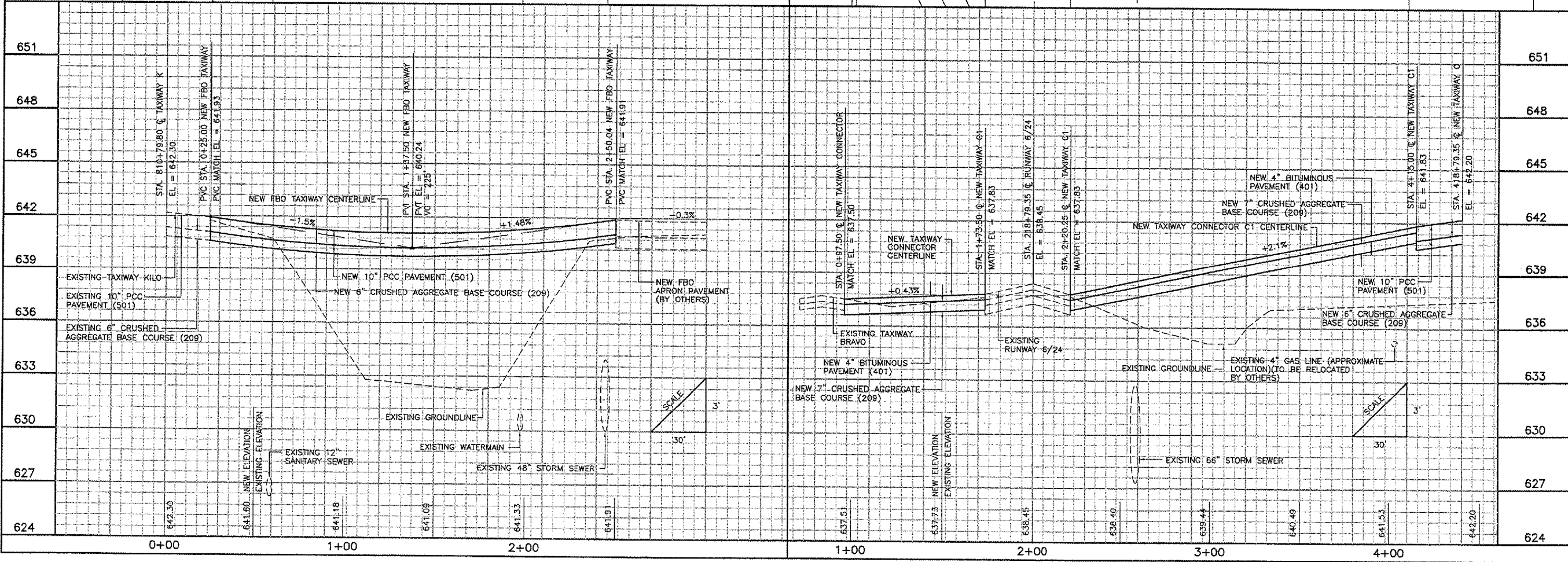
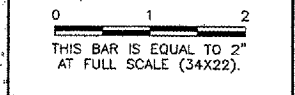
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ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	11 OF 38 SHEETS



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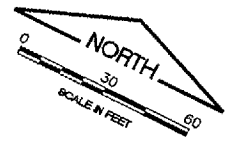
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PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
PLAN AND PROFILE
TAXIWAY CONNECTORS

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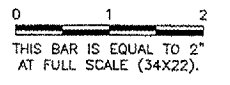
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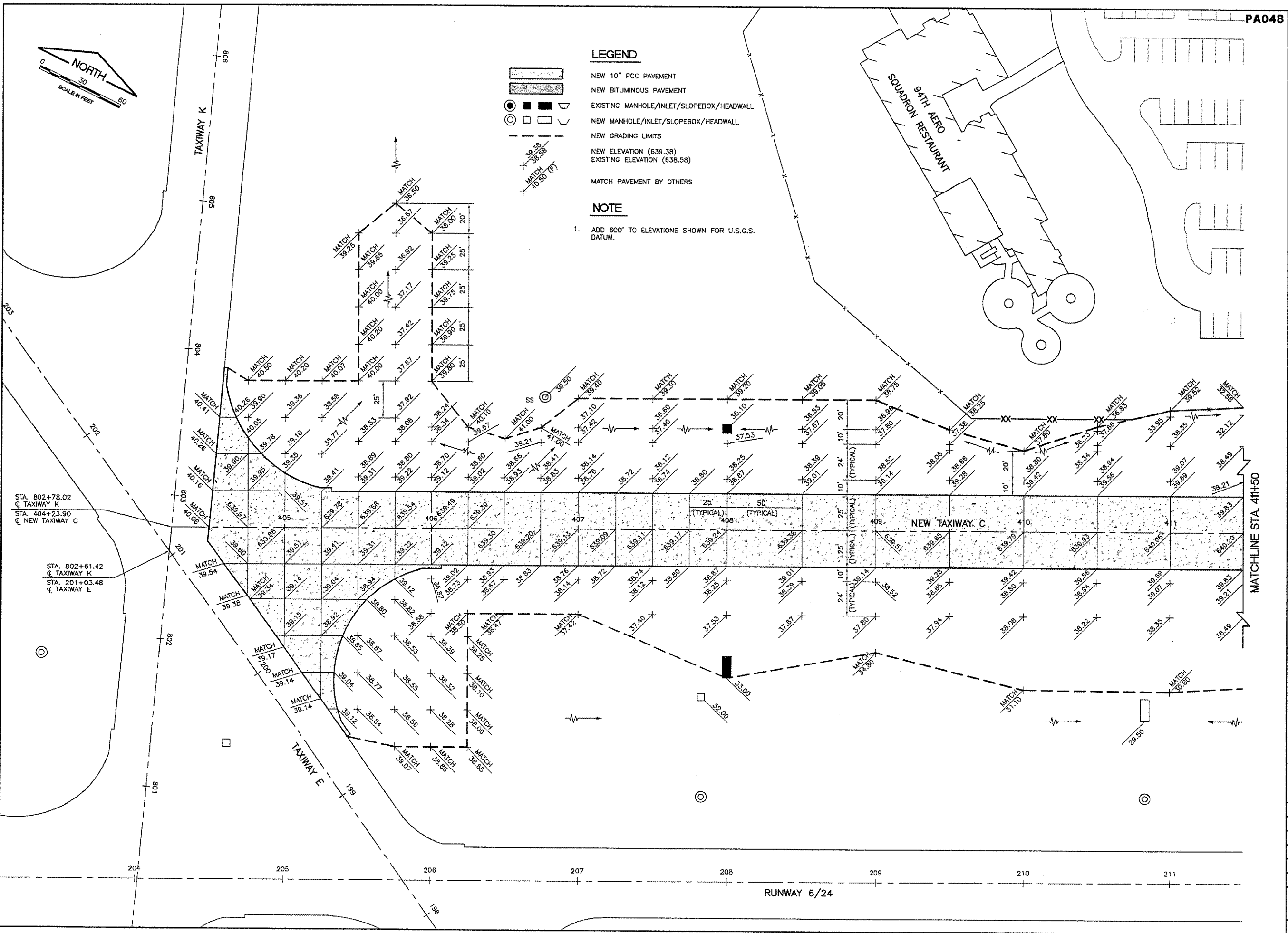


LEGEND

- NEW 10" PCC PAVEMENT
- NEW BITUMINOUS PAVEMENT
- EXISTING MANHOLE/INLET/SLOPEBOX/HEADWALL
- NEW MANHOLE/INLET/SLOPEBOX/HEADWALL
- NEW GRADING LIMITS
- NEW ELEVATION (639.38)
- EXISTING ELEVATION (638.58)
- MATCH PAVEMENT BY OTHERS

NOTE

1. ADD 600' TO ELEVATIONS SHOWN FOR U.S.G.S. DATUM.



**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 GRADING PLAN -
 SHEET 1**

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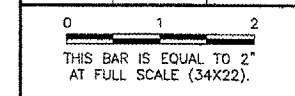
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SHEET 14 OF 38 SHEETS	

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REVISIONS		
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PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

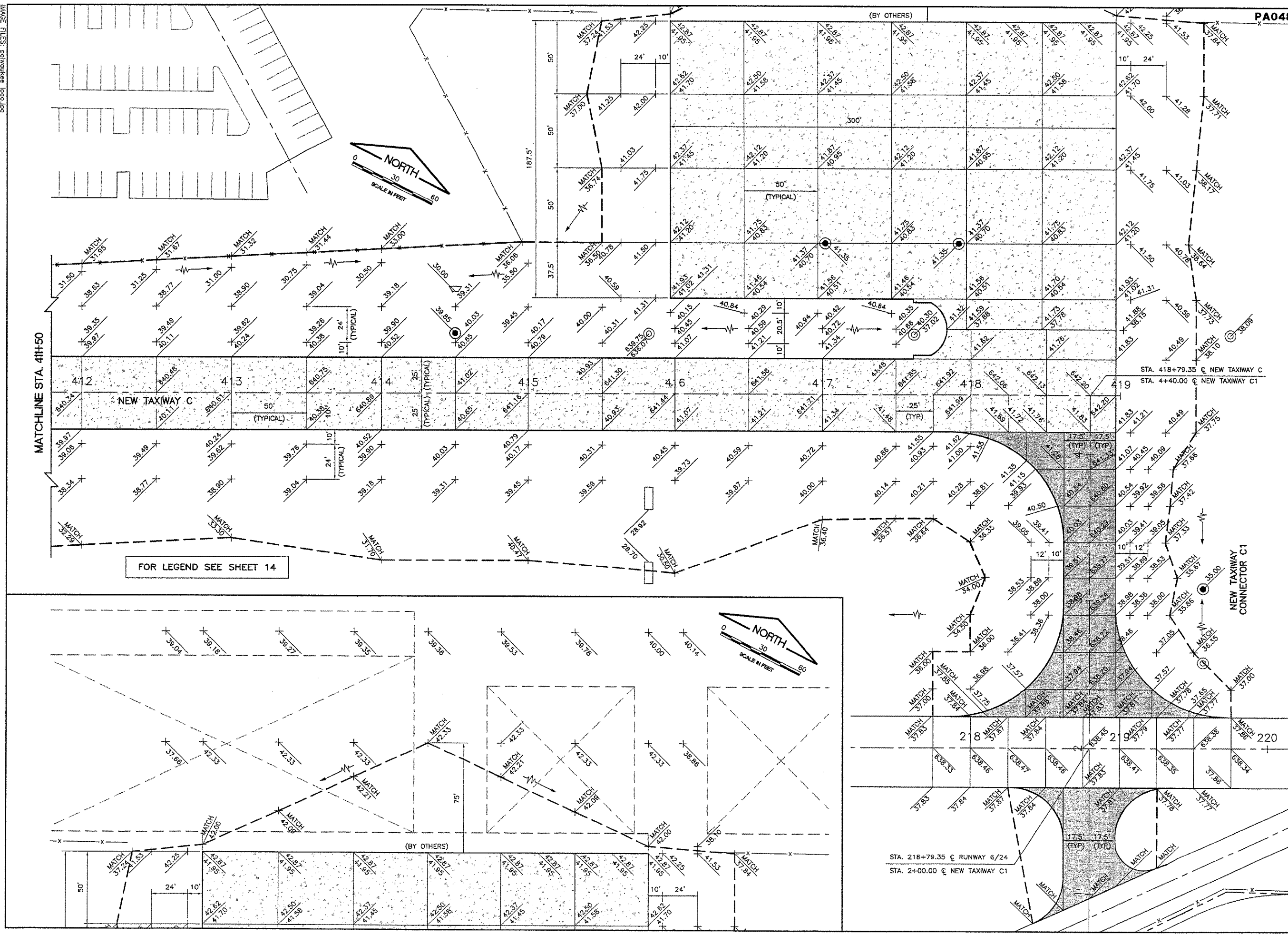
GRADING PLAN - SHEET 2

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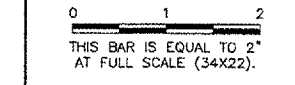
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Palwaukee Municipal Airport

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DRAWN BY: JRO
CHECKED BY:
APPROVED BY:
DATE: 06/24/05
JOB No: 04290-03
ILLINOIS PROJECT: PWK-3481
A.I.P. PROJECT: 3-17-0018-BXX
SHEET 15 OF 38 SHEETS



REVISIONS		
NUMBER	BY	DATE

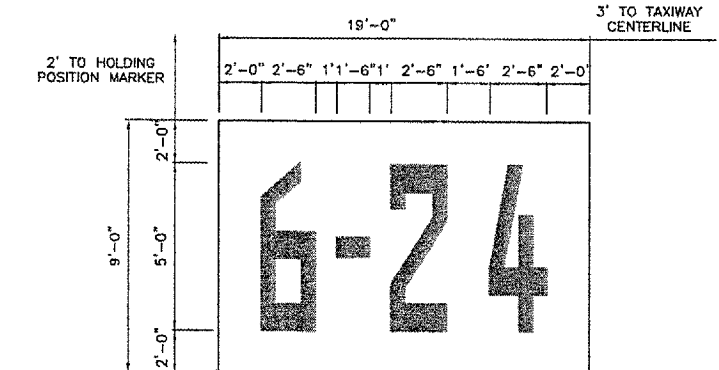


LEGEND

- E — EXISTING ELECTRICAL CIRCUIT
- K — EXISTING TAXIWAY KILO CIRCUIT
- 6/24 — EXISTING RUNWAY 6/24 CIRCUIT
- 12/30 — EXISTING RUNWAY 6/24 CIRCUIT
- 16/34 — EXISTING RUNWAY 16/34 CIRCUIT
- DLMT — EXISTING TAXIWAY DELTA, LIMA AND YANKEE CIRCUIT
- RGL — EXISTING RUNWAY 16/34 GUARD LIGHTS
- RDR — EXISTING RUNWAY DISTANCE REMAINING CIRCUIT
- LAHSO — EXISTING RUNWAY LAND AND HOLD SHORT OPERATIONS CIRCUIT
- FAA — EXISTING FAA COMMUNICATION CABLE
- REIL16/34 — EXISTING RUNWAY 16/34 END IDENTIFICATION LIGHT CIRCUIT
- REIL12 — EXISTING RUNWAY 12 END IDENTIFICATION LIGHT CIRCUIT
- REIL30 — EXISTING RUNWAY 30 END IDENTIFICATION LIGHT CIRCUIT
- (with circle) EXISTING BASE MOUNTED, MEDIUM INTENSITY TAXIWAY LIGHT
- ▬ (with circle) EXISTING AIRFIELD GUIDANCE SIGN
- ▬ (with square) EXISTING DUCT
- (with circle) EXISTING ELECTRICAL HANDHOLE
- (with circle) EXISTING ELECTRICAL MANHOLE
- (with square) EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHTS

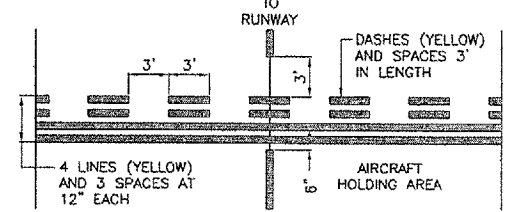
- ▬ (with square) NEW SPLIT DUCT
- ▬ (with circle) NEW 4-WAY CONCRETE ENCASED DUCT
- ▬ (with square) NEW GRS CONDUIT
- (with circle) NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- (with circle) NEW ELECTRICAL MANHOLE
- ▬ (with square) NEW AIRFIELD GUIDANCE SIGN
- K — NEW TAXIWAY KILO 1/C #8 5KV CABLE CIRCUIT IN 2" PVC DUCT (NUMBER OF TICK MARKS INDICATE NUMBER OF CABLES)
- C — NEW TAXIWAY C 1/C #8 5KV CABLE CIRCUIT IN 2" PVC DUCT (NUMBER OF TICK MARKS INDICATE NUMBER OF CABLES)
- ⬆ NEW RETROREFLECTIVE MARKER
- (with circle) NEW ELECTRICAL HANDHOLE

FOR AIRFIELD SIGN SCHEDULE AND NOTES SEE SHEET 27



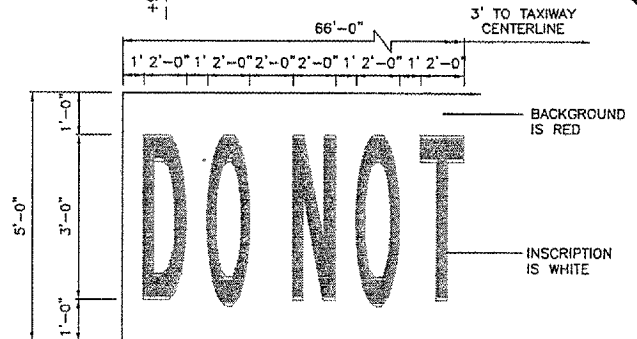
SURFACE PAINTED HOLDING POSITION SIGN
NO SCALE

- ALL SURFACE PAINTED HOLDING POSITION SIGNS SHALL HAVE A RED BACKGROUND WITH A WHITE INSCRIPTION, AND WILL BE OUTLINED WITH A 6" BLACK BORDER.
- 6 AND 24 MAY BE SWITCHED DEPENDING ON WHICH TAXIWAY IS BEING MARKED. SEE PLAN SHEET FOR ORIENTATION OF NUMERALS.

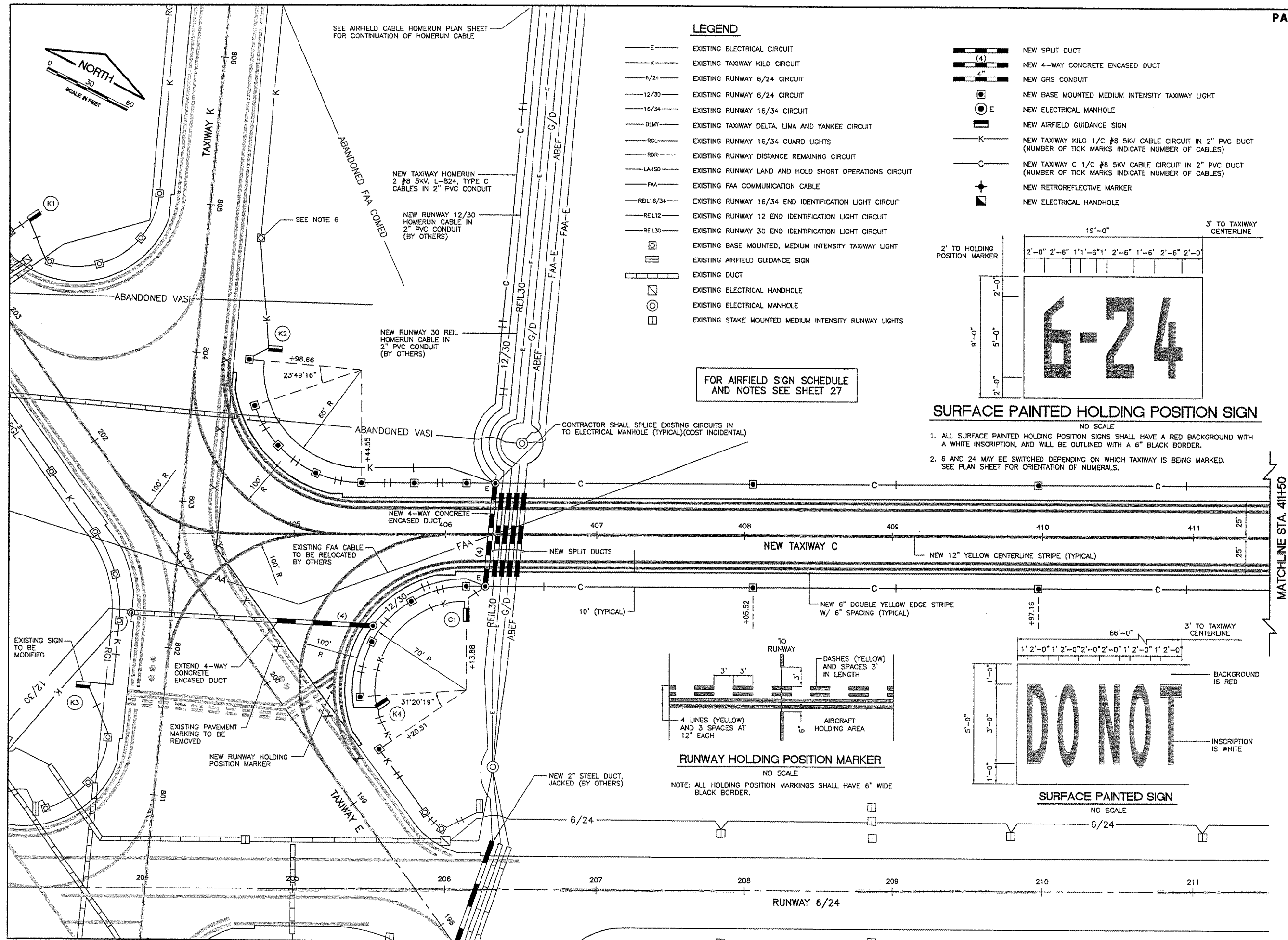


RUNWAY HOLDING POSITION MARKER
NO SCALE

NOTE: ALL HOLDING POSITION MARKINGS SHALL HAVE 6" WIDE BLACK BORDER.



SURFACE PAINTED SIGN
NO SCALE



PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
LIGHTING AND PAVEMENT MARKING PLAN -
SHEET 1

CMT
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 CONSULTING ENGINEERS
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DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	17 OF 38 SHEETS

IMAGE FILES: palwaukee_logop.jpg

PA048

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LAYOUT: Layout1
UPDATE BY: johse
SURVEY BOOK #
DATE: Fri 7/1/05 4:30pm
XREF DWG: tbcint.dwg
tb.dwg
ty-base.dwg

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).


**PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS**

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

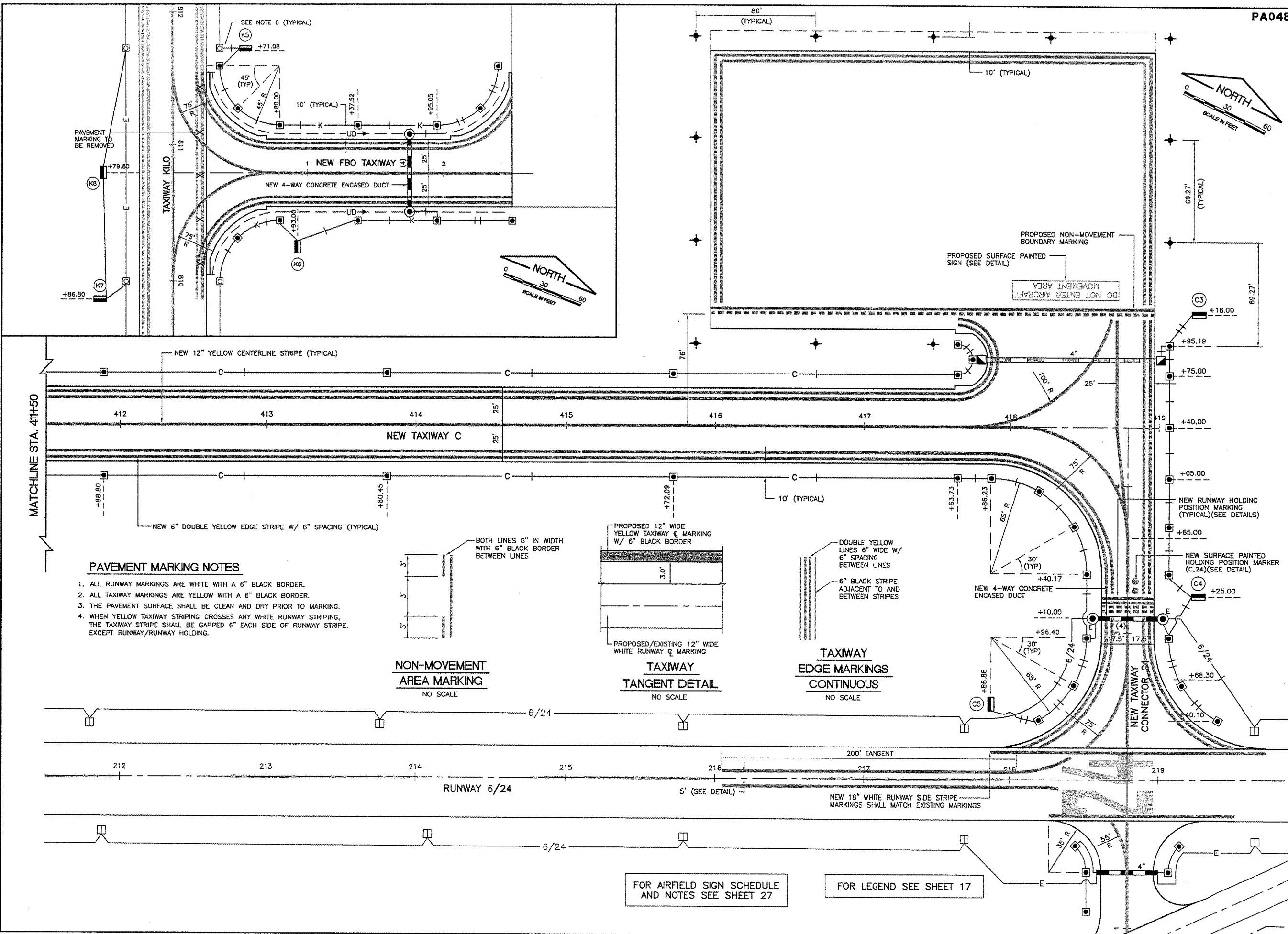
LIGHTING AND PAVEMENT MARKING PLAN - SHEET 2

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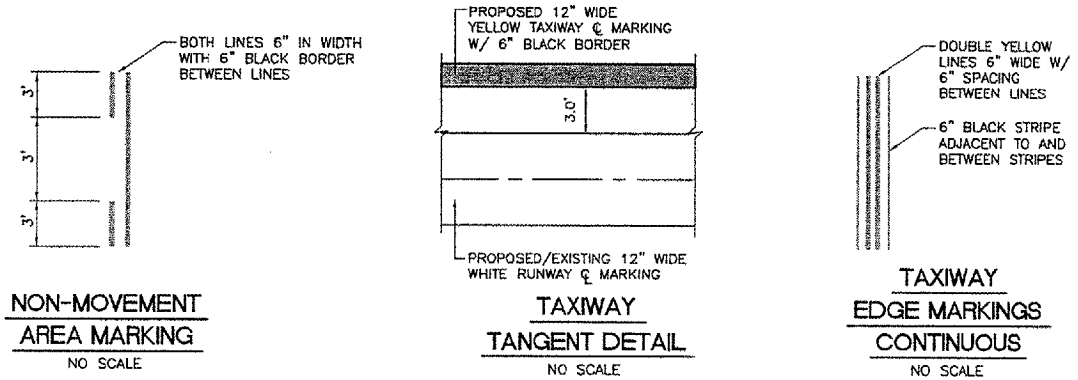
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SHEET 18 OF 38 SHEETS	



- PAVEMENT MARKING NOTES**
1. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER.
 2. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER.
 3. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
 4. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE, EXCEPT RUNWAY/RUNWAY HOLDING.



FOR AIRFIELD SIGN SCHEDULE AND NOTES SEE SHEET 27

FOR LEGEND SEE SHEET 17


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 SURVEY BOOK #
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REVISIONS		
NUMBER	BY	DATE

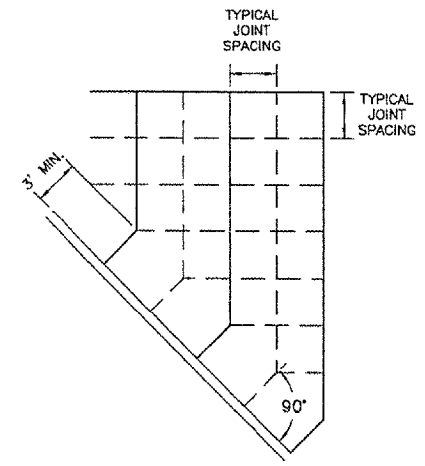
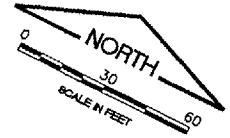
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 PAVEMENT JOINTING PLAN -
 SHEET 1**

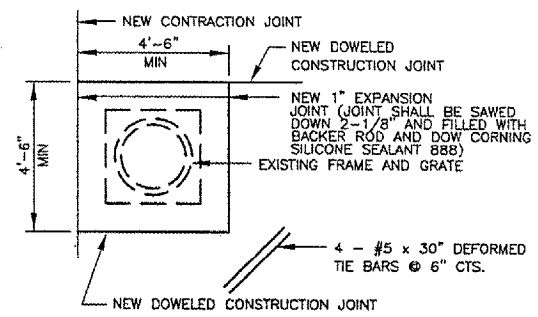
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 DATE: 06/24/05
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 ILLINOIS PROJECT: PWK-3481
 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 19 OF 38 SHEETS

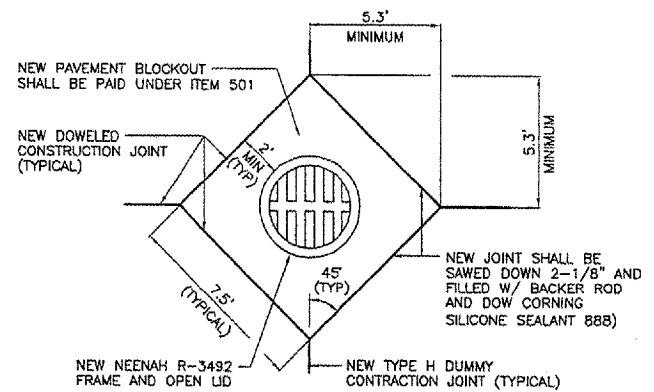


JOINTING AT SKEWED EDGE
 NOT TO SCALE



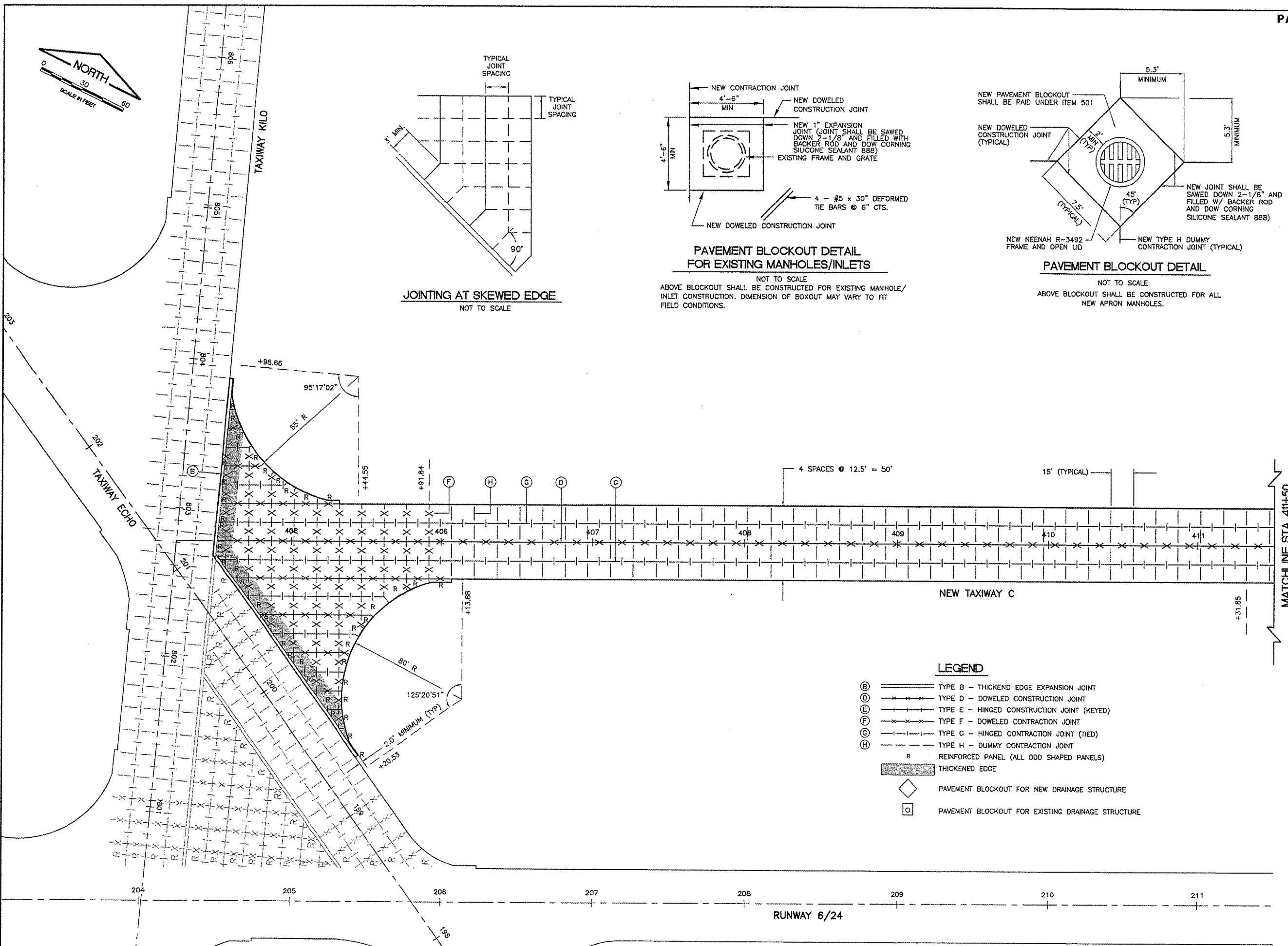
**PAVEMENT BLOCKOUT DETAIL
 FOR EXISTING MANHOLES/INLETS**

NOT TO SCALE
 ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/
 INLET CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT
 FIELD CONDITIONS.



PAVEMENT BLOCKOUT DETAIL

NOT TO SCALE
 ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR ALL
 NEW APRON MANHOLES.

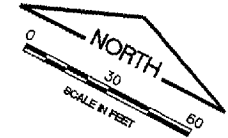


- LEGEND**
- (B) TYPE B - THICKENED EDGE EXPANSION JOINT
 - (D) TYPE D - DOWELED CONSTRUCTION JOINT
 - (E) TYPE E - HINGED CONSTRUCTION JOINT (KEYED)
 - (F) TYPE F - DOWELED CONSTRUCTION JOINT
 - (G) TYPE G - HINGED CONSTRUCTION JOINT (TIED)
 - (H) TYPE H - DUMMY CONSTRUCTION JOINT
 - R REINFORCED PANEL (ALL ODD SHAPED PANELS)
 - THICKENED EDGE
 - ◇ PAVEMENT BLOCKOUT FOR NEW DRAINAGE STRUCTURE
 - PAVEMENT BLOCKOUT FOR EXISTING DRAINAGE STRUCTURE

IMAGE FILES: palwaukee_togo.jpg

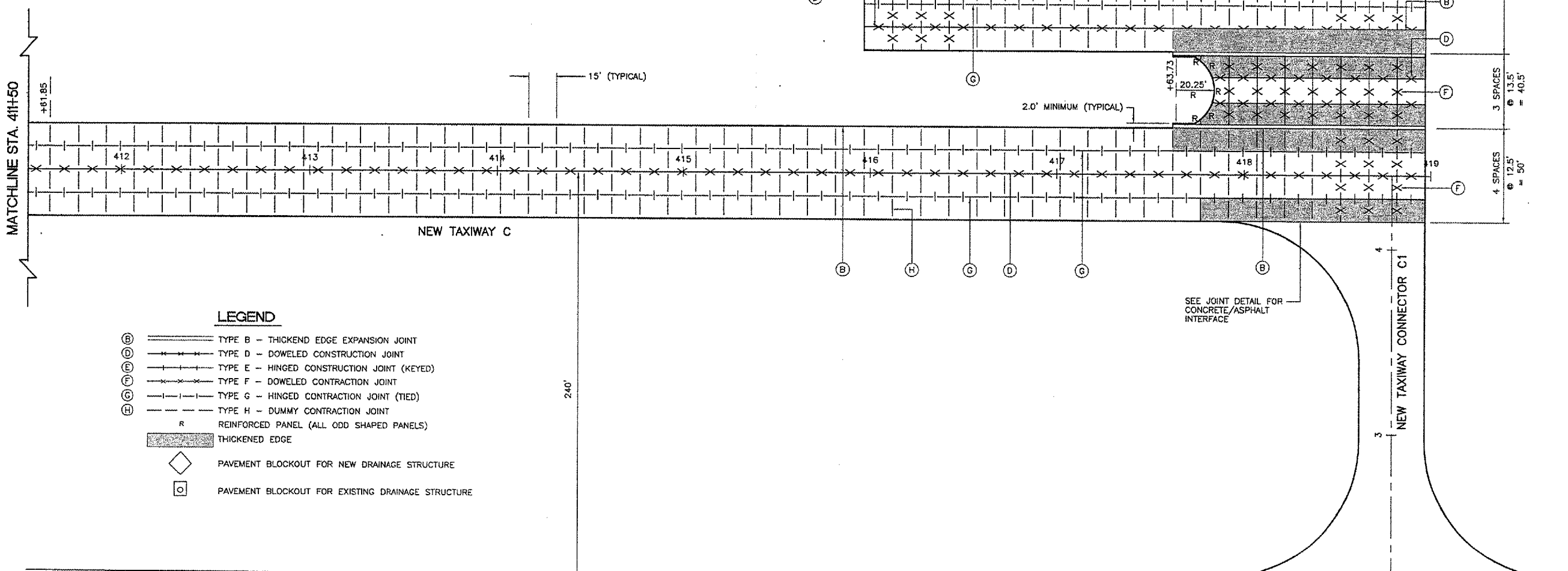
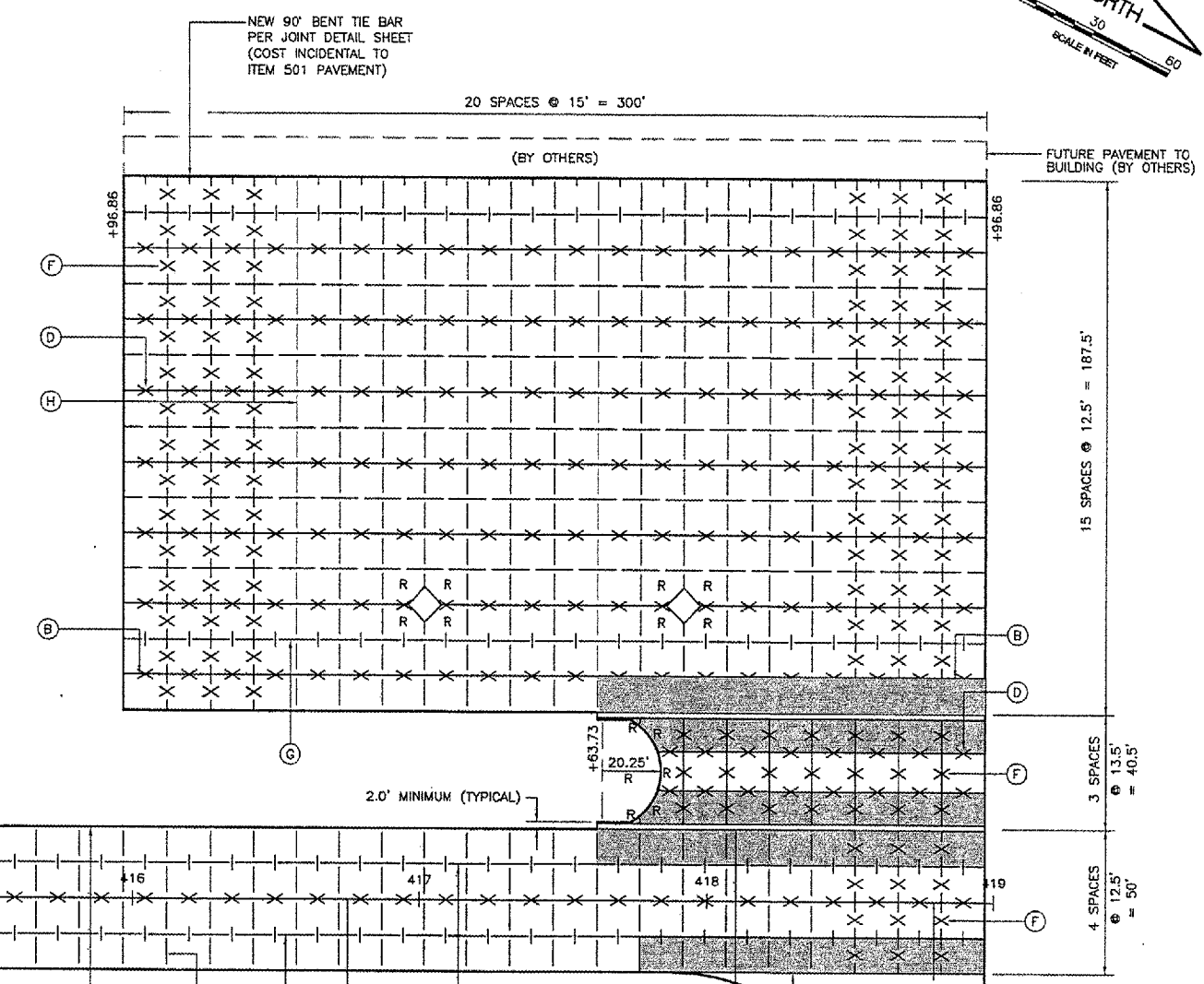
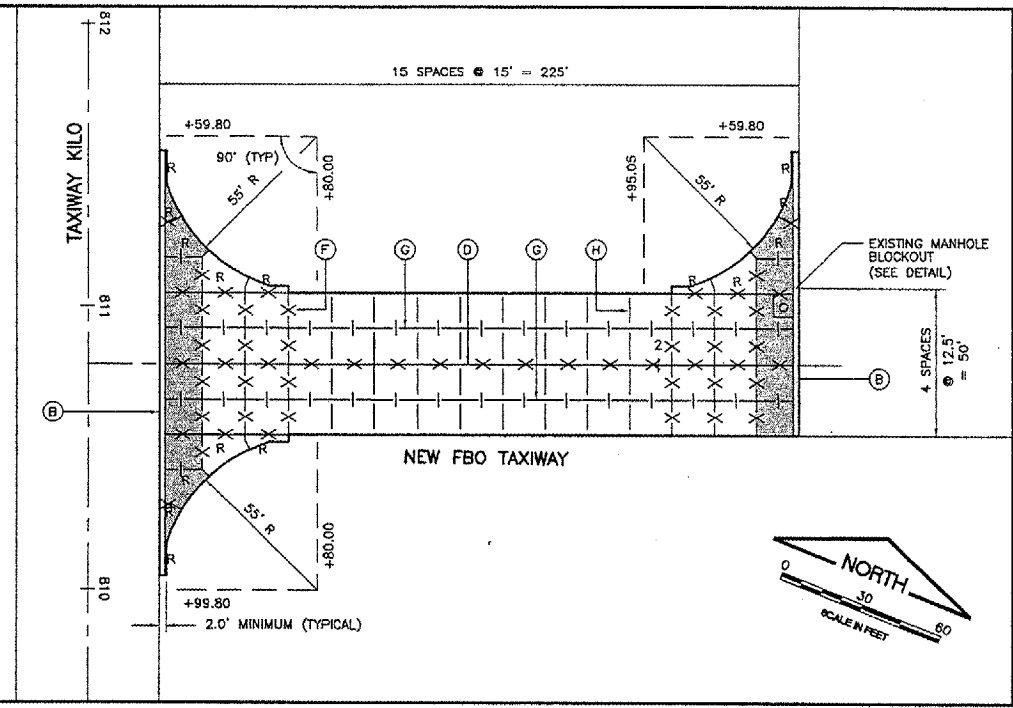
PA048

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UPDATE BY: johse
SURVEY BOOK #
DATE: Thu 6/30/05 3:43pm
XREF DWG: tbcint.dwg
tb.dwg
txy-base.dwg



REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).



- LEGEND**
- (B) TYPE B - THICKEND EDGE EXPANSION JOINT
 - (D) TYPE D - DOWELED CONSTRUCTION JOINT
 - (E) TYPE E - HINGED CONSTRUCTION JOINT (KEYED)
 - (F) TYPE F - DOWELED CONTRACTION JOINT
 - (G) TYPE G - HINGED CONTRACTION JOINT (TIED)
 - (H) TYPE H - DUMMY CONTRACTION JOINT
 - R REINFORCED PANEL (ALL ODD SHAPED PANELS)
 - THICKENED EDGE
 - PAVEMENT BLOCKOUT FOR NEW DRAINAGE STRUCTURE
 - PAVEMENT BLOCKOUT FOR EXISTING DRAINAGE STRUCTURE

PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
PAVEMENT JOINTING PLAN - SHEET 2

CMT
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DRAWN BY: JRO
CHECKED BY:
APPROVED BY:

DATE: 06/24/05
JOB No: 04290-03

ILLINOIS PROJECT: PWK-3481
A.I.P. PROJECT: 3-17-0018-BXX

SHEET 20 OF 38 SHEETS

24

JOINT NOTES

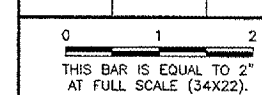
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 UPDATE BY: johse
 SURVEY BOOK #
 XREF DWG:
 XREF DWG:
 DATE: Tue 7/5/05 1:38pm

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY OR MECHANICALLY INSTALL PER ARTICLE 420.10 JOINTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES OR MECHANICAL METHOD SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- WHEN TIE BARS ARE USED FOR SLIP-FORM LONGITUDINAL CONSTRUCTION JOINTS, ONE END OF THE TIE BAR SHALL BE BENT AND INSERTED INTO THE KEYWAY OF THE JOINT. THE BENT BARS SHALL BE STRAIGHTENED TO RIGHT ANGLES WITH LONGITUDINAL JOINT AND SHALL EXTEND INTO THE ADJACENT LANE PARALLEL TO THE PAVEMENT SURFACE.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. THE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.

REVISIONS

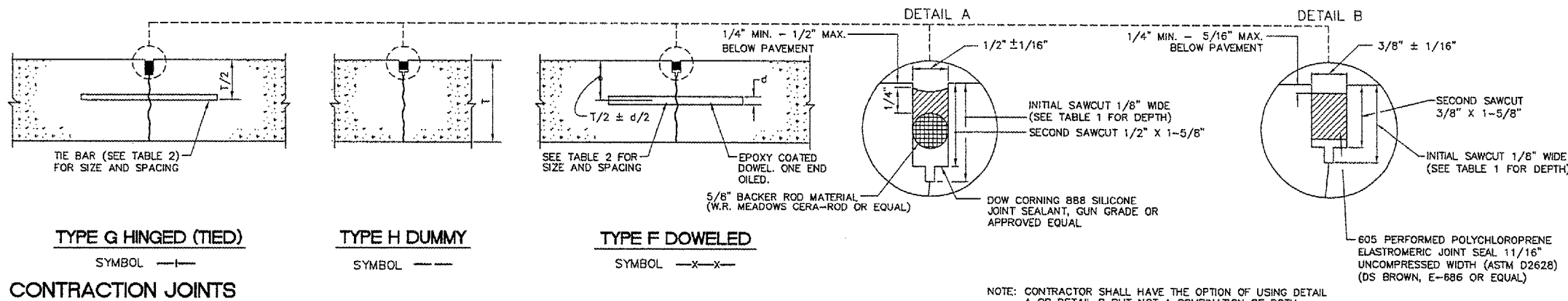
NUMBER	BY	DATE



PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
PAVEMENT JOINTING DETAILS

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 DRAWN BY: JRO
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 DATE: 06/24/05
 JOB No: 04290-03
 ILLINOIS PROJECT: PWK-3481
 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 21 OF 38 SHEETS

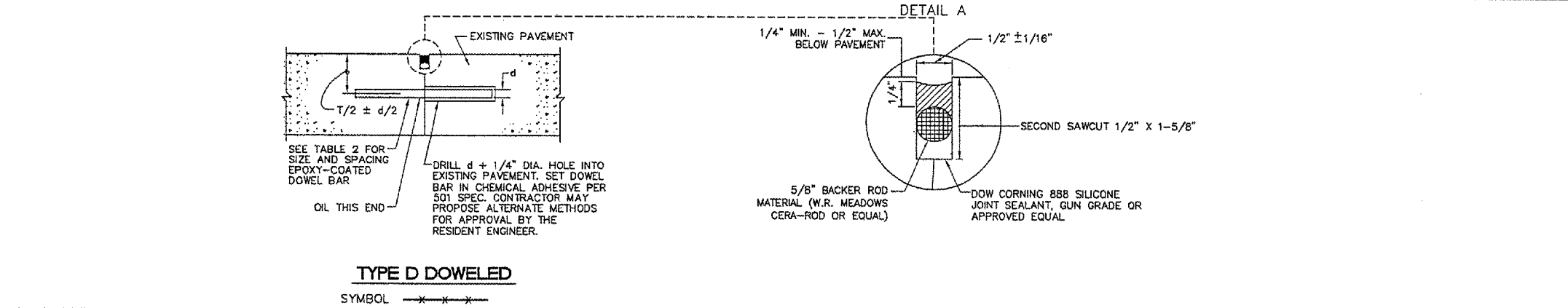


TYPE G HINGED (TIED)
 SYMBOL —|—

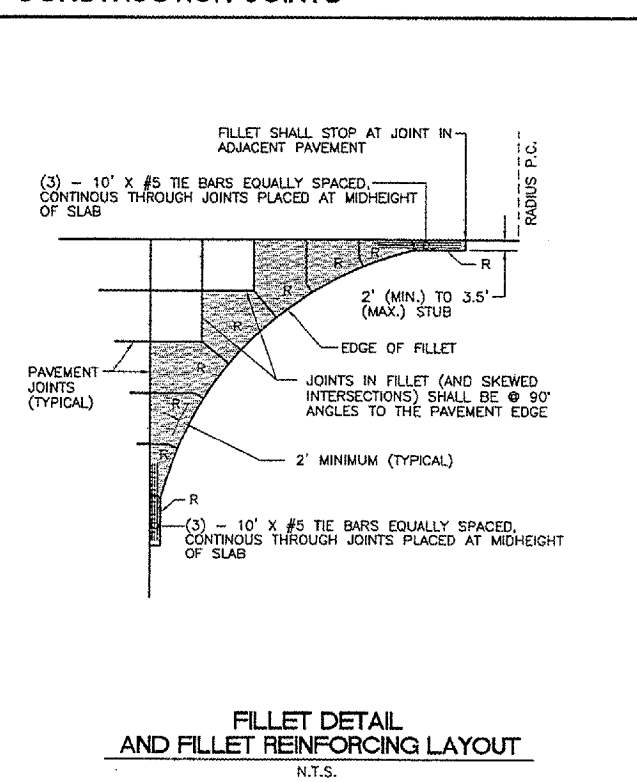
TYPE H DUMMY
 SYMBOL —|—

TYPE F DOWELED
 SYMBOL —x—x—

NOTE: CONTRACTOR SHALL HAVE THE OPTION OF USING DETAIL A OR DETAIL B BUT NOT A COMBINATION OF BOTH



TYPE D DOWELED
 SYMBOL —x—x—



FILLET DETAIL AND FILLET REINFORCING LAYOUT
 N.T.S.

Ⓢ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH DEFORMED WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

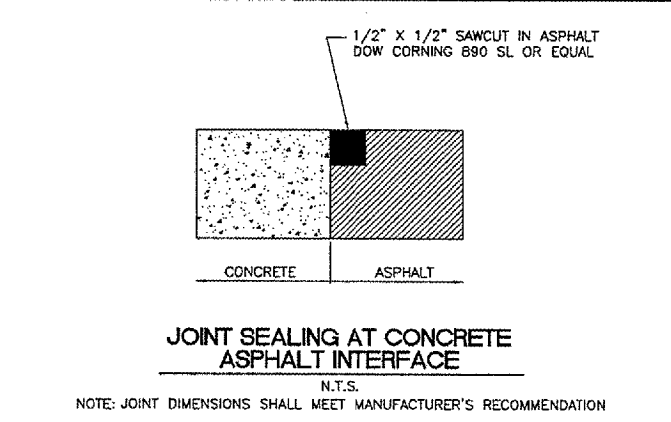


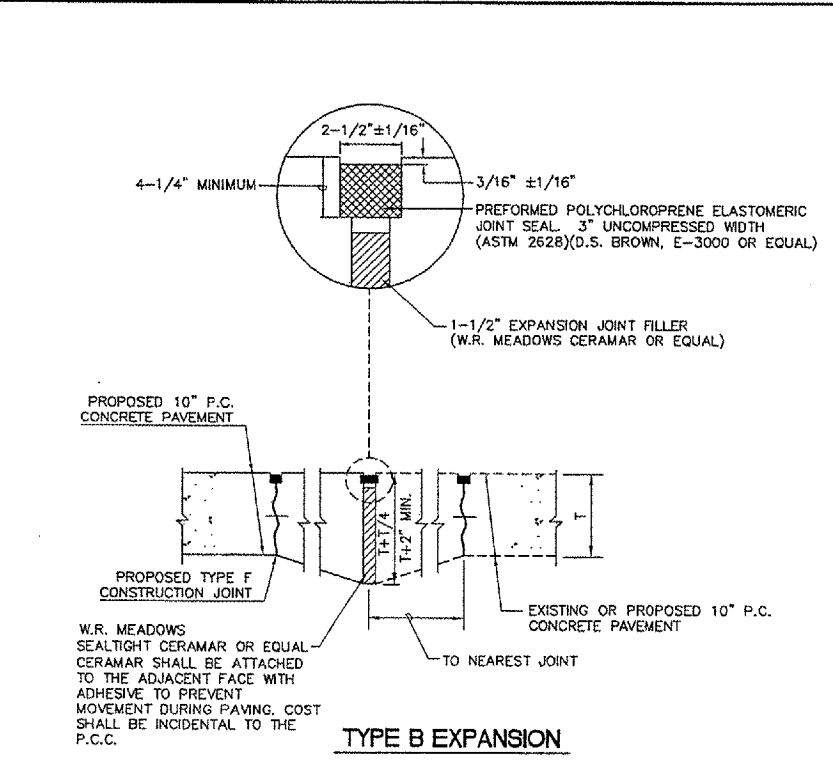
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
10	2.5"

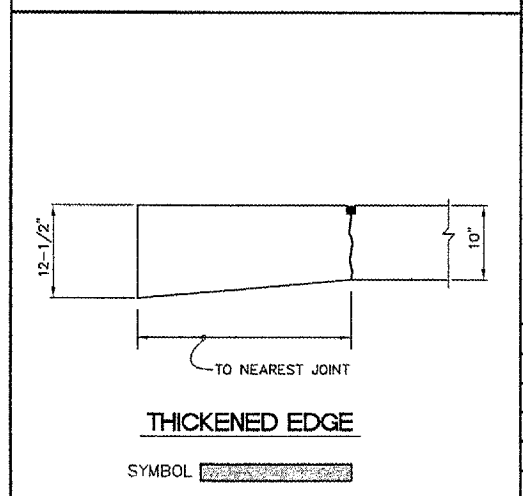
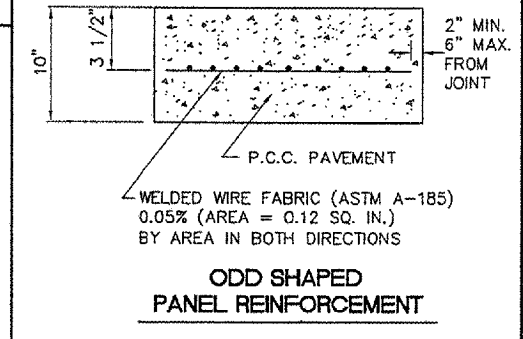
TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS	
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH
10	1"	19"	12"	#5	30"

DIMENSION TABLES

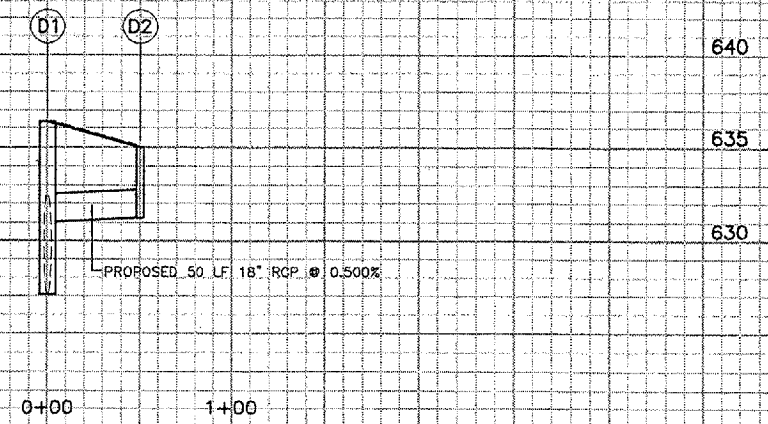
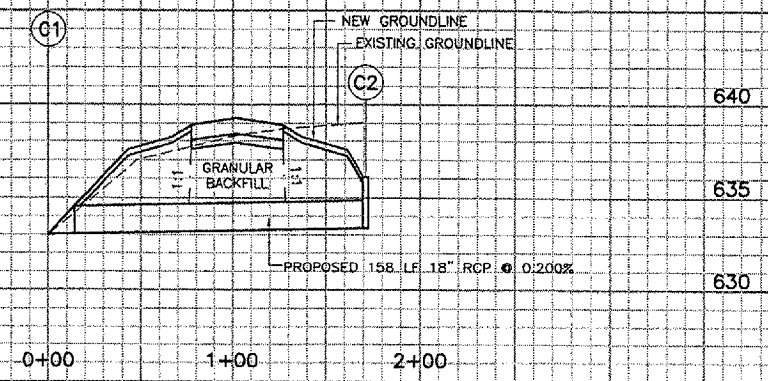
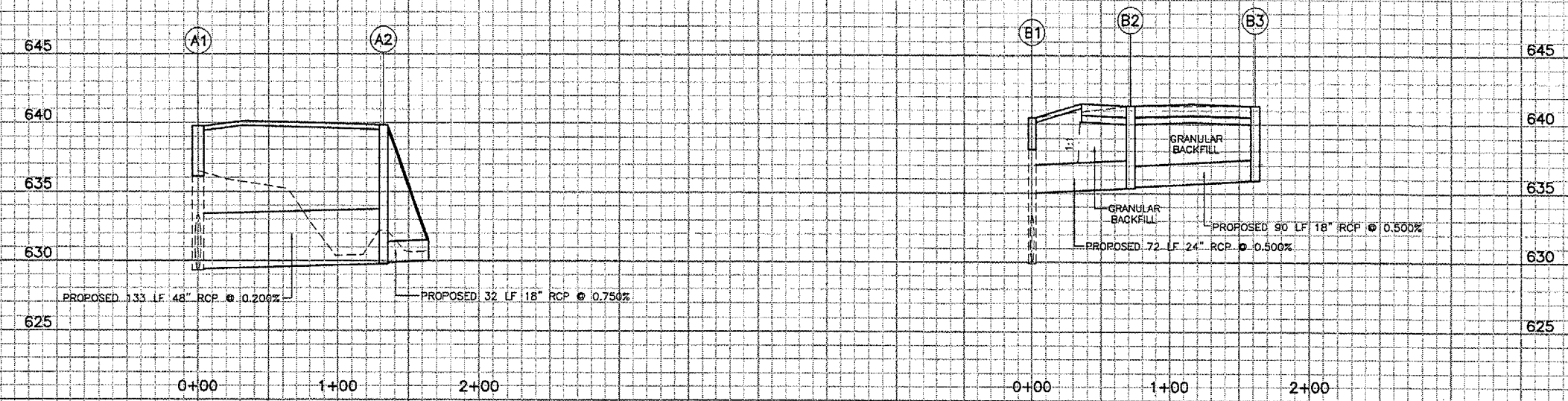
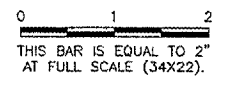


EXPANSION JOINTS



THICKENED EDGE
 SYMBOL ———

REVISIONS		
NUMBER	BY	DATE



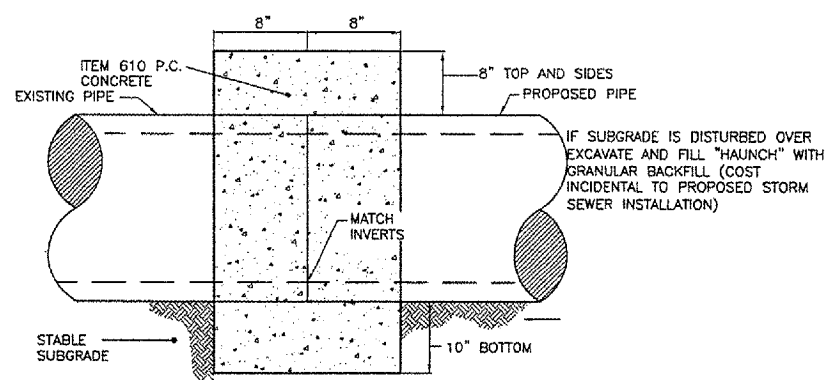
STORM SEWER SCHEDULE				
NUMBER	LOCATION	DESCRIPTION	RIM	INVERT
A1	415+82, 41.5' LT. C NEW TAXIWAY C	EXISTING 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID TO BE RECONSTRUCTED	NEW=639.75 EXISTING=636.07	EXISTING 48" (S) = 629.29 EXISTING 48" (E) = 629.29 NEW 48" (W) = 629.48
A2	414+50, 41.5' LT. C NEW TAXIWAY C	NEW 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID	NEW=639.85	NEW 48" (E) = 629.66 NEW 18" (N) = 629.76
A3	414+50, 73.5' LT. C NEW TAXIWAY C	NEW 18" FLARED END SECTION W/ GRATE	-	NEW 18" (N) = 630.00
B1	417+63.58, 37.9' LT. C NEW TAXIWAY C	EXISTING 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID TO BE RECONSTRUCTED	NEW=640.40 EXISTING=637.02	EXISTING 48" (W) = 629.56 EXISTING 42" (E) = 629.80 NEW 24" (N) = 635.00
B2	417+91.86, 103' LT. C NEW TAXIWAY C	NEW 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID	NEW=641.35	NEW 24" (S) = 635.34 NEW 18" (W) = 635.44
B3	417+01.86, 103' LT. C NEW TAXIWAY C	NEW 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID	NEW=641.35	NEW 18" (E) = 635.89
C1	408+00, 69' LT. C NEW TAXIWAY C	NEW TYPE 1 INLET W/ FRAME AND GRATE	NEW=636.10	NEW 18" (S) = 633.34
C2	408+00, 103' RT. C NEW TAXIWAY C	NEW 18" SLOPE BOX W/ GRATE	-	NEW 18" (N) = 633.00
D1	2+58.07, 77' RT. C NEW TAXIWAY CONNECTOR C1	EXISTING 9' MANHOLE W/ TYPE 1 FRAME AND CLOSED LID	EXISTING=636.35	EXISTING 66" (E) = 627.08 EXISTING 66" (W) = 627.15 EXISTING 8" PVC (S) = 630.54 NEW 18" (N) = 631.00
D2	3+07.75, 77' RT. C NEW TAXIWAY CONNECTOR C1	NEW 6' MANHOLE W/ TYPE 1 FRAME AND OPEN LID	NEW =635.00	NEW 18" (S) = 631.25
E1	2+44.57, 20.2' LT. C NEW FBO TAXIWAY	EXISTING 6' MANHOLE W/ TYPE 1 FRAME AND CLOSED LID TO BE ADJUSTED	NEW=641.78 EXISTING=640.80	EXISTING 48" (N) = 631.63 EXISTING 48" (S) = 631.63 EXISTING 18" (W) = TO BE BRICK AND MORTARED

PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
STORM SEWER PROFILES AND
DRAINAGE SCHEDULE

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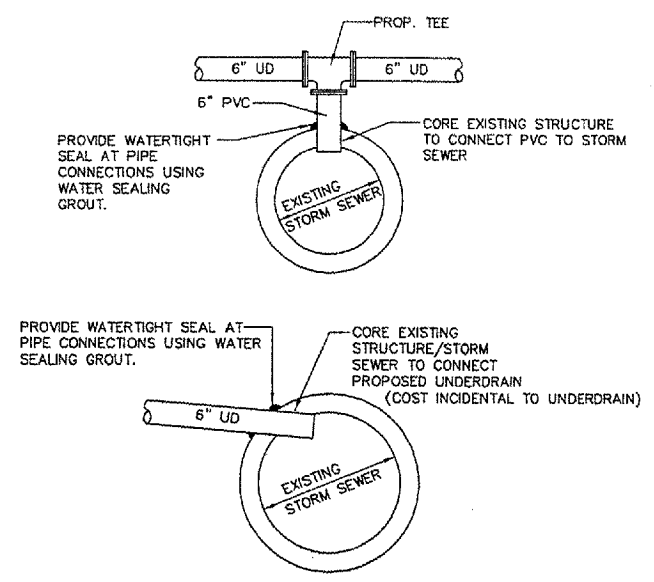
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DRAWN BY:	JRO
CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	22 OF 38 SHEETS



CONCRETE COLLAR - STORM SEWER
 NOT TO SCALE

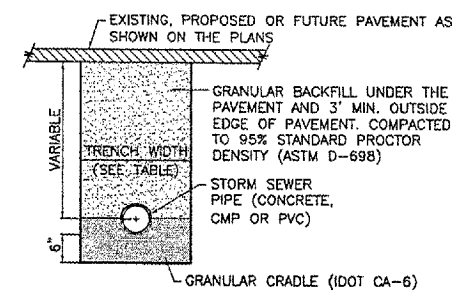
NOTE: COST INCIDENTAL TO INSTALLATION OF PROPOSED PIPE



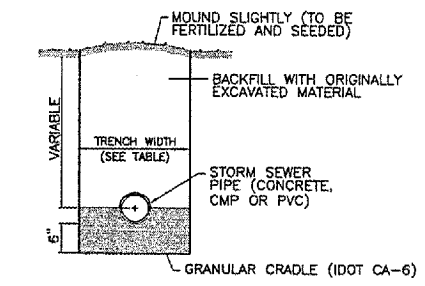
UNDERDRAIN CONNECTION DETAILS
 NOT TO SCALE

UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO PROPOSED STRUCTURES AND STORM SEWERS / EXISTING STRUCTURES AND STORM SEWERS, SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"
66	9'-5"
72	10'-0"
78	10'-7"
84	11'-2"
90	11'-9"
96	12'-4"
102	12'-11"
108	13'-6"

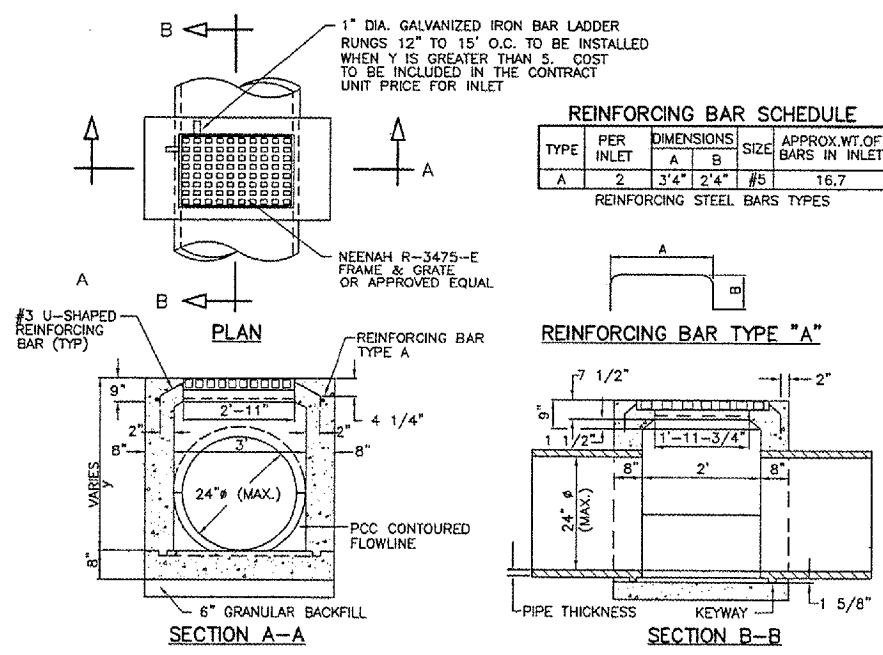
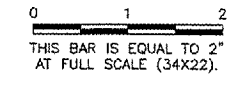


ALL PAVED AREAS



NON-PAVED AREAS

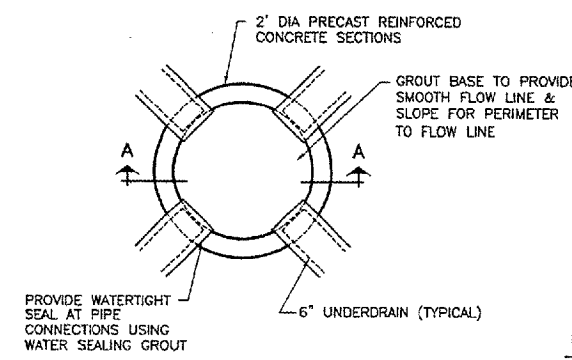
REVISIONS		
NUMBER	BY	DATE



TYPE 1 INLET
 NOT TO SCALE

NOTES

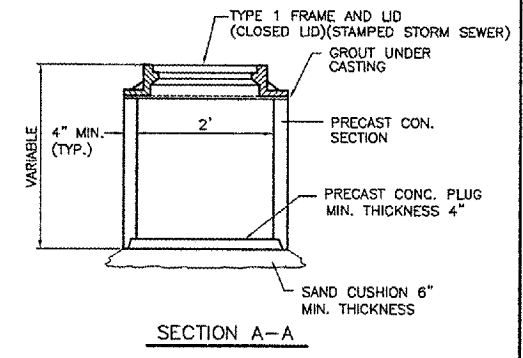
- 1/2" CHAMFER TO BE USED ON ALL EXPOSED CORNERS ON INLETS. BARS TO BE INSTALLED 2" FROM FACE OF WALL.
- INLET TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE FOR INLET SHALL INCLUDE THE GRATE AND FRAME AS SPECIFIED.



PLAN VIEW

STORM SEWER/UNDERDRAIN NOTES

- CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
- ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
- INSTALL PROPOSED ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COST INCIDENTAL.
- UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
- PRIOR TO ORDERING AND INSTALLING ALL FIELD TILE REPLACEMENT PIPE, THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND INVERTS OF EXISTING FIELD TILE CONNECTIONS. ADJUSTMENTS SHALL BE MADE AS NECESSARY AT NO ADDITIONAL COST TO THE CONTRACT.
- CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER PIPE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.
- BRICK AND MORTARING OF UNDERDRAIN/STORM SEWER STRUCTURES AFTER PIPE REMOVAL SHALL BE INCIDENTAL TO THE REMOVAL PAY ITEM.



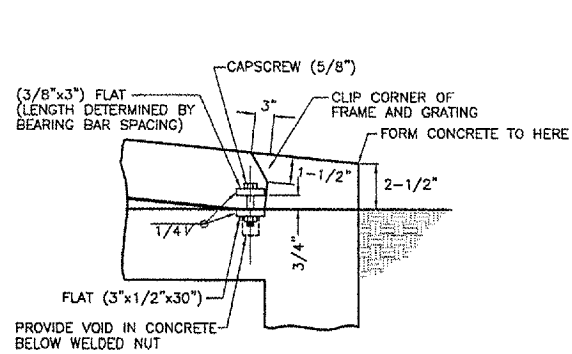
SECTION A-A

UNDERDRAIN COLLECTION STRUCTURE DETAIL
 NOT TO SCALE

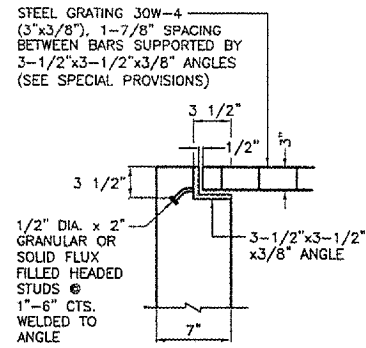
PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
DRAINAGE DETAILS

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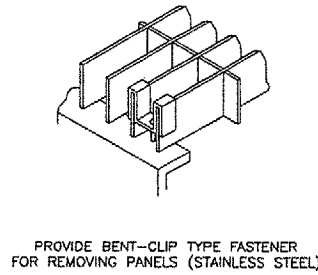
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CHECKED BY:	
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JOB No:	04290-03
ILLINOIS PROJECT: PWK-3481 A.I.P. PROJECT: 3-17-0018-BXX	
SHEET 23 OF 36 SHEETS	



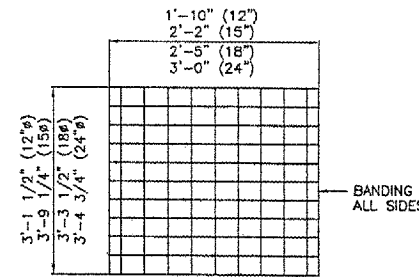
DETAIL A
NO SCALE



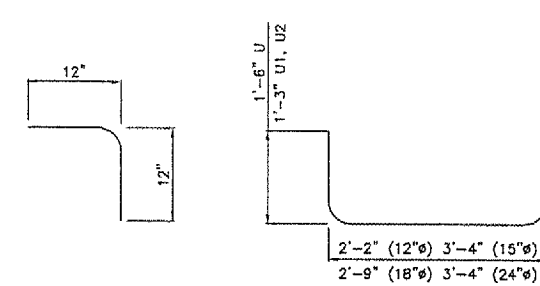
SECTION-B
NO SCALE



PROVIDE BENT-CLIP TYPE FASTENER FOR REMOVING PANELS (STAINLESS STEEL)
SADDLE CLIP
NO SCALE



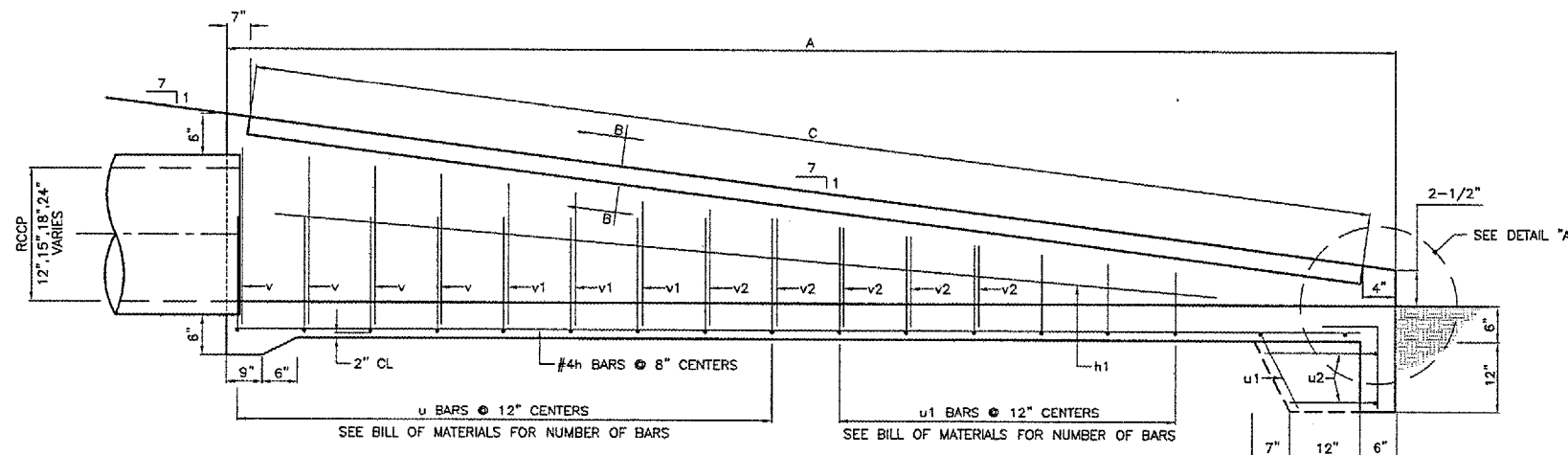
GRATING DETAILS - PLAN VIEW
NO SCALE
ONE SECTION OF GRATING DETAILED.
TOTAL OF 3 SECTIONS REQUIRED FOR 12" & 15".
TOTAL OF 4 SECTIONS REQUIRED FOR 18"
TOTAL OF 5 SECTIONS REQUIRED FOR 24".
SEE SPECIAL PROVISIONS FOR FURTHER DETAILS.



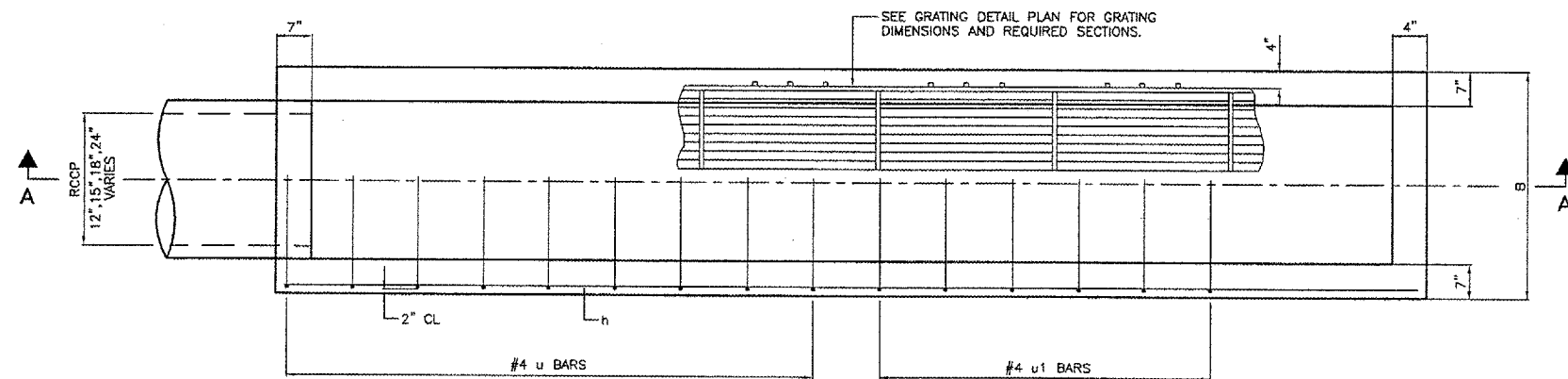
BAR L NO SCALE
BAR U, U1 AND U2 NO SCALE

BILL OF MATERIALS
INLET BOX

TYPE	BAR	QUANTITY	SIZE	LENGTH
12"	h	4	#4	9'-11"
15"	h	4	#4	11'-7"
18"	h	6	#4	13'-8"
24"	h	6	#4	17'-6"
12"	h1	2	#4	8'-0"
15"	h1	2	#4	10'-0"
18"	h1	2	#4	12'-0"
24"	h1	2	#4	15'-0"
12"	L	4	#4	2'-0"
15"	L	4	#4	2'-0"
18"	L	4	#4	2'-0"
24"	L	4	#4	2'-0"
12"	U	4	#4	5'-2"
15"	U	6	#4	5'-6"
18"	U	8	#4	5'-9"
24"	U	12	#4	6'-4"
12"	U1	4	#4	4'-8"
15"	U1	3	#4	5'-0"
18"	U1	4	#4	5'-3"
24"	U1	4	#4	5'-10"
12"	U2	2	#4	4'-8"
15"	U2	2	#4	5'-0"
18"	U2	2	#4	5'-3"
24"	U2	2	#4	5'-10"
12"	V	4	#4	1'-4"
15"	V	4	#4	1'-8"
18"	V	6	#4	1'-10"
24"	V	6	#4	2'-5"
12"	V1	-	#4	-
15"	V1	4	#4	1'-4"
18"	V1	6	#4	1'-5"
24"	V1	6	#4	2'-0"
12"	V2	-	#4	-
15"	V2	-	#4	-
18"	V2	-	#4	-
24"	V2	8	#4	1'-6"
CONCRETE STRUCTURES				
12"		CU.YD.	2	
15"		CU.YD.	2	
18"		CU.YD.	3	
24"		CU.YD.	3	
REINFORCEMENT BARS				
12"		POUND	85	
15"		POUND	100	
18"		POUND	145	
24"		POUND	200	
GRATING				
12"		SG.FT.	18	
15"		SG.FT.	25	
18"		SG.FT.	32	
24"		SG.FT.	51	



SECTION A-A
NO SCALE



PLAN
NO SCALE

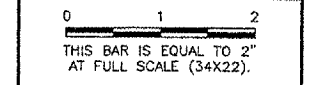
MANUFACTURED BY MC CARR CONCRETE PRODUCTS OR EQUAL.

TABLE OF DIMENSIONS

DIMENSION	12"φ	15"φ	18"φ	24"φ
A	10'-3"	12'-1"	14'-0"	17'-10"
B	2'-6"	2'-10"	3'-1"	3'-8"
C	9'-5"	11'-4"	13'-3"	17'-1"

REVISIONS

NUMBER	BY	DATE



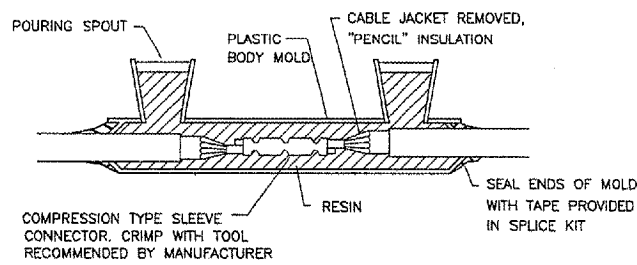
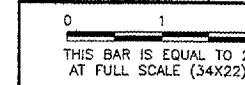
PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
SLOPE BOX INLET DETAILS

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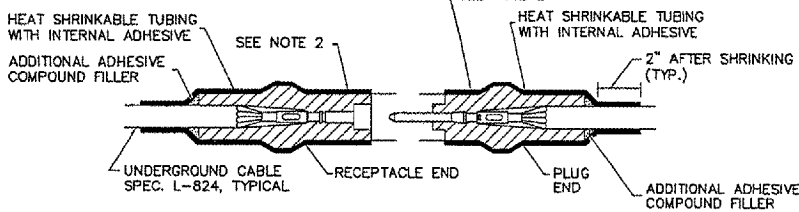
DESIGN BY: ATI
 DRAWN BY: JRO
 CHECKED BY:
 APPROVED BY:
 DATE: 06/24/05
 JOB No: 04290-03
 ILLINOIS PROJECT: PWK-3481
 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 24 OF 38 SHEETS

REVISIONS		
NUMBER	BY	DATE



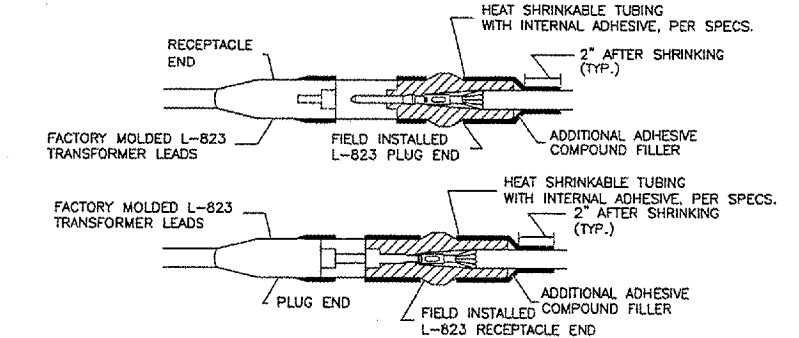
TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
 N.T.S.



TYPE B - CABLE SPLICE

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
 N.T.S.

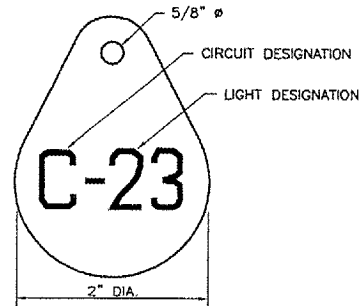


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
 N.T.S.

NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

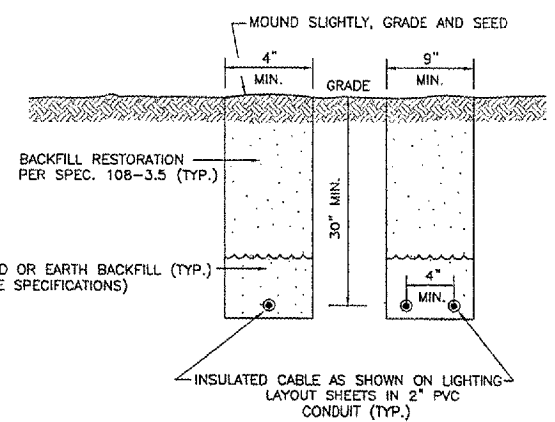


LIGHT IDENTIFICATION DETAIL

NOT TO SCALE

NOTES:

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

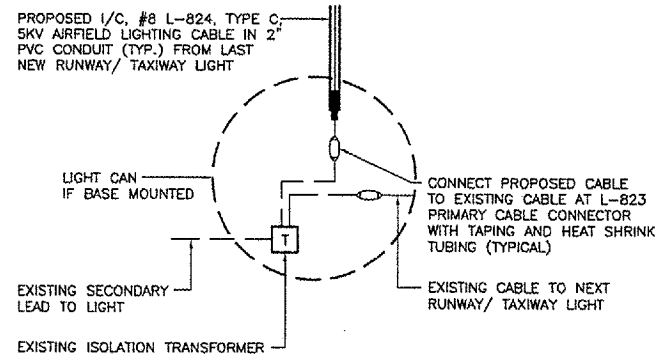


TURF AREA CABLE TRENCH DETAIL

NOT TO SCALE

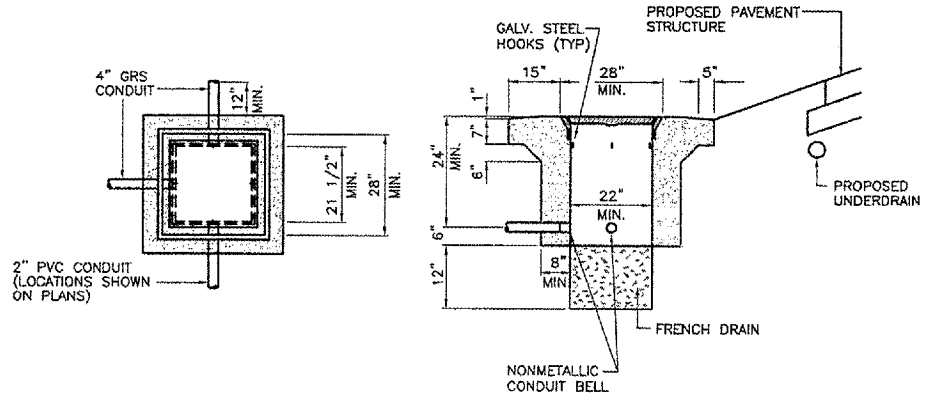
NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.



RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE

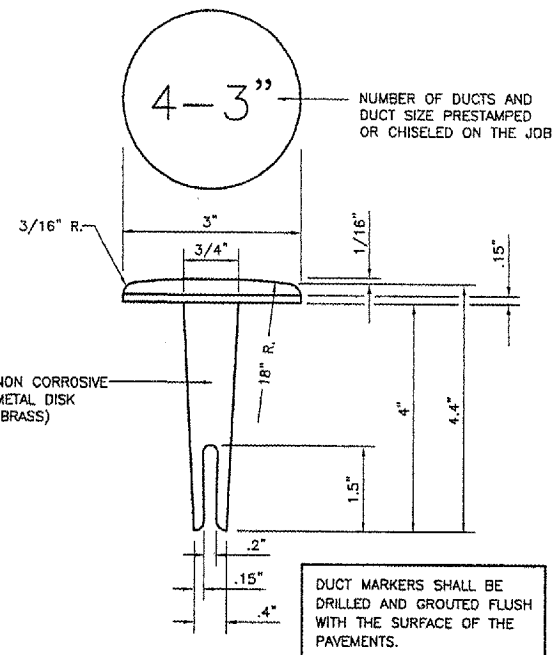


CONCRETE HEAVY DUTY ELECTRICAL HANDHOLE DETAIL
 IDOT STANDARD NO. 841001

NOT TO SCALE

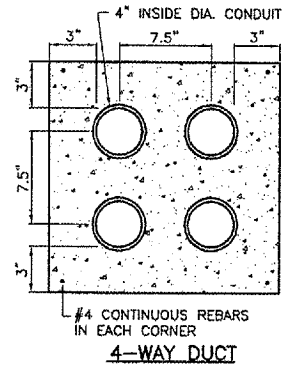
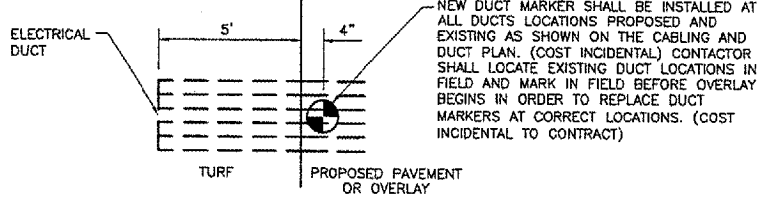
NOTE

- CONDUIT ENTRANCES AND NUMBER OF CONDUITS SHALL BE AS SHOWN ON SITE PLAN VIEW.
- FRAME AND COVER SHALL BE NEENAH R6662-HH OR EQUAL.
- COVER SHALL BE STAMPED "ELECTRICAL".



DUCT MARKER DETAIL

NOT TO SCALE

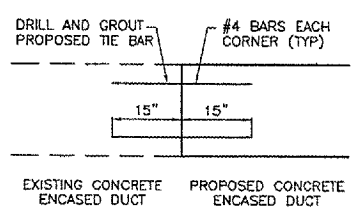


CONCRETE ENCASED DUCT BANK

NOT TO SCALE

NOTES:

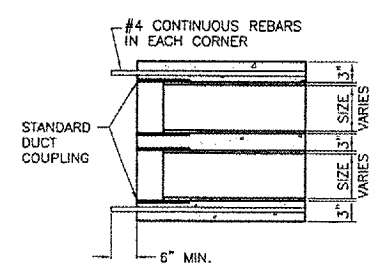
- DIMENSIONS ARE MINIMUM.
- CONCRETE SHALL CONFORM TO ITEM 610.
- ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
- TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.
- 4" SPLIT DUCT SHALL BE CONCRETE ENCASED WITH 3" MIN. CONCRETE SURROUNDING 4" CONDUIT. COST INCIDENTAL TO SPLIT DUCT.



EXTENSION OF EXISTING DUCT

NO SCALE

NOTE: COST OF CONNECTION SHALL BE CONSIDERED INCIDENTAL TO PROPOSED DUCT.



CONCRETE ENCASED DUCT END DETAIL

NO SCALE

**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 ELECTRICAL DETAILS 1**

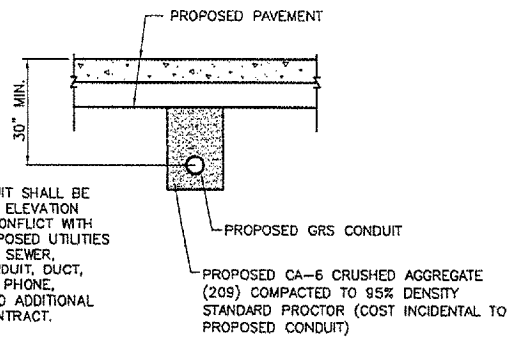
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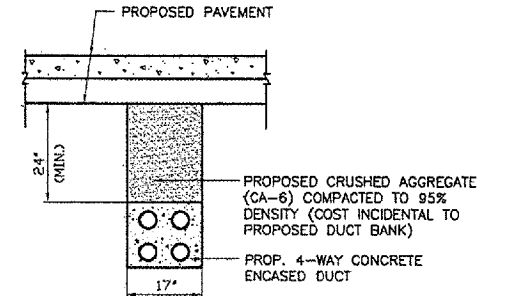
DESIGN BY:	ARM
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CHECKED BY:	
APPROVED BY:	
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ILLINOIS PROJECT:	PWK-3481
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SHEET	25 OF 38 SHEETS

GENERAL NOTES

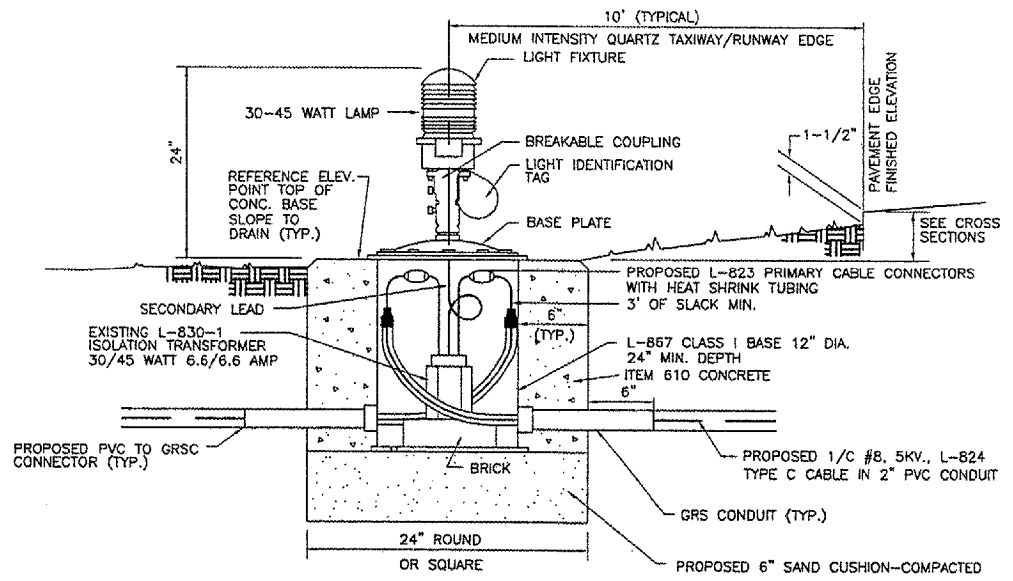
1. THE CONCRETE BASE FOR BASE MOUNTED LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE, SLOPE TO DRAIN (610 CONCRETE).
2. TRANSFORMER HOLDER MAY BE ANY COMMERCIALY AVAILABLE BRICK.
3. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
4. ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
5. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
6. ALL LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE ASSISTANT MANAGER THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
7. TAXIWAY LIGHTS SHALL HAVE A BLUE LENS. RUNWAY LIGHTS SHALL HAVE A CLEAR LENS OR 180° CLEAR/YELLOW SPLIT LENS AND THRESHOLD LIGHTS SHALL HAVE A 180° RED/GREEN SPLIT LENS.
8. DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.



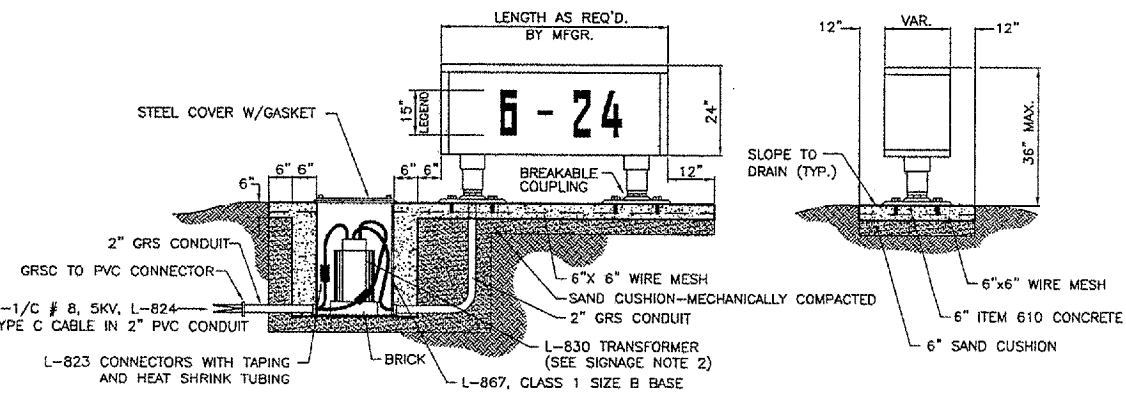
GRS CONDUIT UNDER PROPOSED PAVEMENT DETAIL
NOT TO SCALE



CONC ENCASED (4-WAY) ELECTRICAL DUCT BACKFILL
NO SCALE



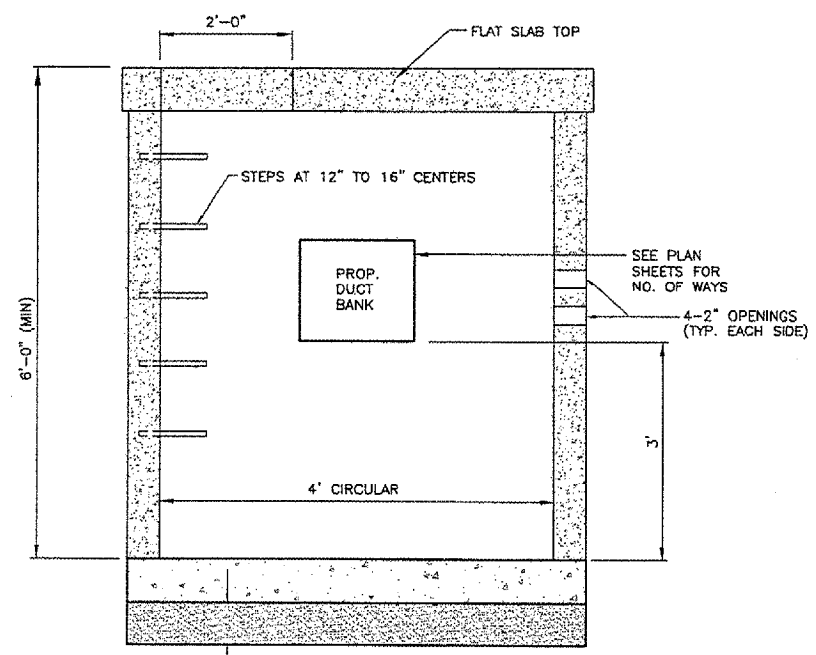
PROPOSED BASE MOUNTED MEDIUM INTENSITY LIGHT
NOT TO SCALE



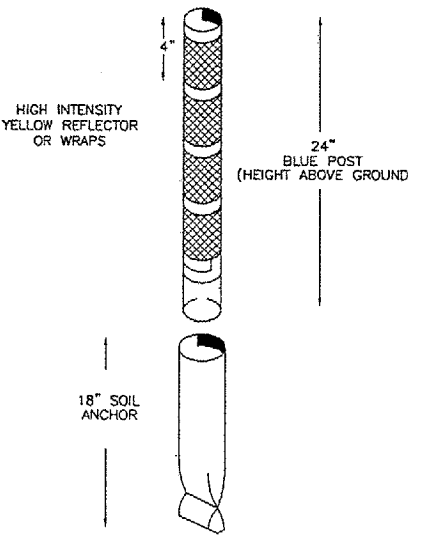
PROPOSED AIRFIELD SIGN L-858
NOT TO SCALE

SIGNAGE NOTES

1. ALL SIGNS ARE 2-SIDED SIGNS. STYLE 2 AND CLASS 2 AS MANUFACTURED BY SIEMENS OR APPROVED EQUAL.
2. TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER.
3. LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
4. THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
5. CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION. PROPOSED SIGNS SHALL BE LUMACURVE, INC. OR EQUAL.



ELEVATION ELECTRICAL MANHOLE
NOT TO SCALE



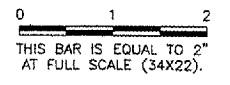
TAXIWAY RETROREFLECTIVE MARKER DETAIL
NOT TO SCALE

NOTES

1. ELECTRICAL MANHOLE (4' DIA.) SHALL BE PER IDOT STANDARD 602406 AS MODIFIED BY DETAIL ON THE PLANS.
2. ECCENTRIC AND CONCENTRIC CONE TOPS SHALL NOT BE USED. PRECAST REINFORCED CONCRETE FLAT SLAB TOP SHALL BE USED PER IDOT STANDARD 602601 AND 602406.
3. USE TYPE 1 FRAME AND LID MARKED "ELECTRIC" PER IDOT STANDARD 604001
4. PROPOSED ELECTRICAL MANHOLE SHALL ACCOMMODATE 4-2" OPENINGS ON EACH SIDE OF THE MANHOLE.

REVISIONS

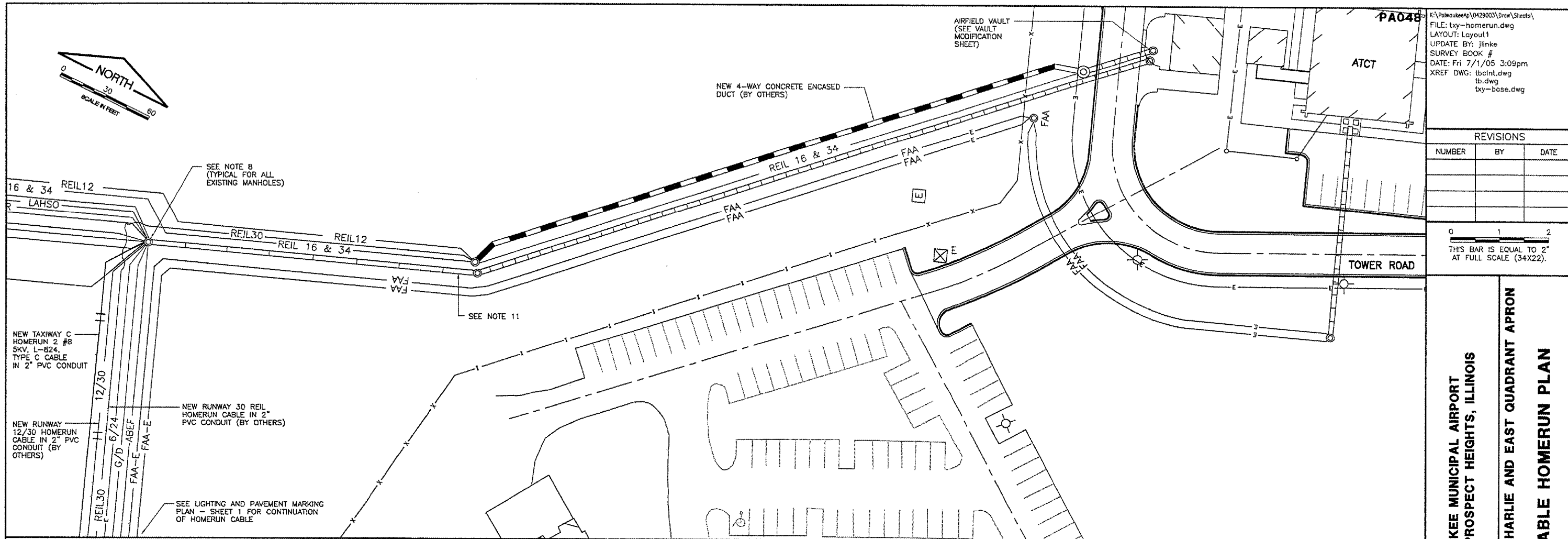
NUMBER	BY	DATE



**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 ELECTRICAL DETAILS 2**

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CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	26 OF 38 SHEETS



PA048
 K:\Palauke\042903\Draw\Sheets\FILE: tx-homerun.dwg
 LAYOUT: Layout1
 UPDATE BY: jlinke
 SURVEY BOOK #
 DATE: Fri 7/1/05 3:09pm
 XREF DWG: tbcint.dwg
 tb.dwg
 tx-base.dwg

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**PALAUKE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 AIRFIELD CABLE HOMERUN PLAN**

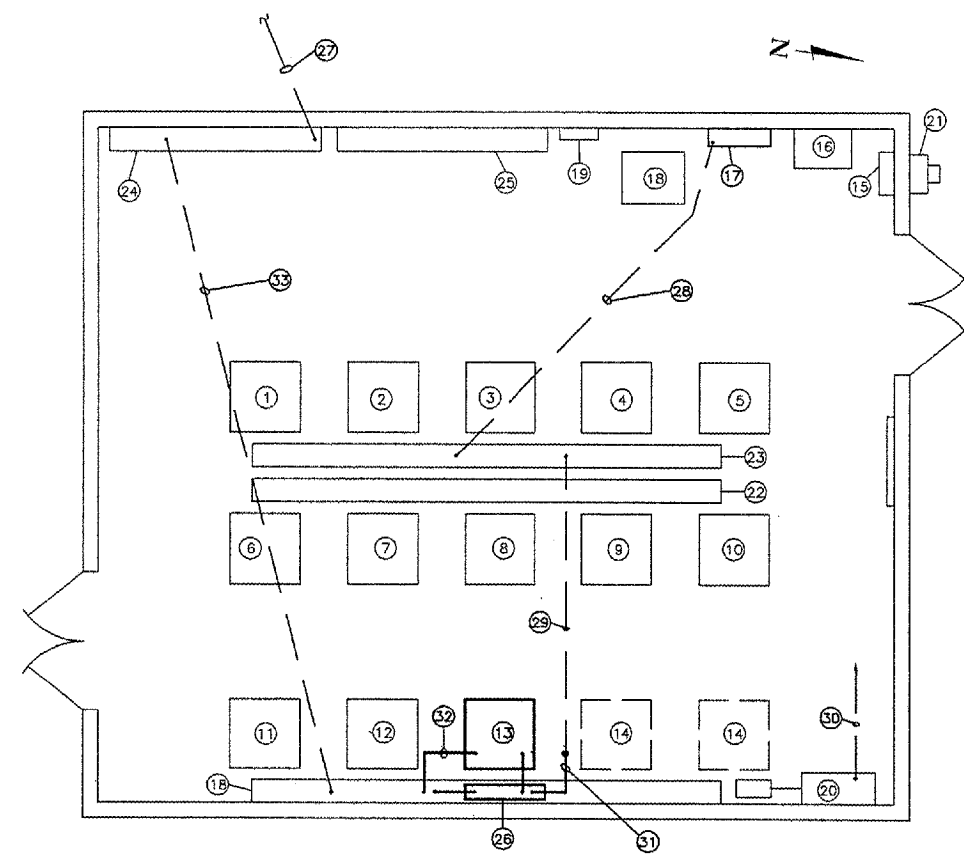
AIRFIELD SIGNAGE SCHEDULE					
SIGN NUMBER	PROPOSED SIGN FACE	EXISTING SIGN LEGEND	PROPOSED SIGN LEGEND	PROPOSED SIGN TYPE	PROPOSED SIGN LOCATION
K1 (SIGN REMOVAL/REPLACEMENT)	NW SE			3,2,3,3 2	STA. 203+46.58, 50' RT. @ TAXIWAY E
K2 (SIGN REMOVAL/REPLACEMENT)	N S			3,3,2 3	STA. 804+07.35, 50' RT. @ TAXIWAY K
K3 (SIGN MODIFICATION)	N S			2,1,1 2,3	STA. 801+69.87, 50' LT. @ TAXIWAY K
K4	NW SE			1,2 2,3,3	STA. 199+80.53, 77.1' RT. @ TAXIWAY E
K5	N S	N.A.		2 2	STA. 811+72.80, 50' RT. @ TAXIWAY K
K6	W E	N.A.		0 3	STA. 0+93.00, 50' RT. @ A
K7	N S	N.A.		0 2	STA. 809+86.80, 50' LT. @ TAXIWAY K
K8	W E	N.A.		0 3,3	STA. 810+79.80, 50' LT. @ TAXIWAY K
C1	W E	N.A.		2 3,3,2,3,3	STA. 406+13.88, 50' RT. @ TAXIWAY C
C3	N S	N.A.		1 0	STA. 3+16.00, 42.5' RT. @ TAXIWAY C1 EXTENDED
C4	N S	N.A.		1,2 2	STA. 1+25.00, 50' RT. @ TAXIWAY C1
C5	W E	N.A.		3 0	STA. 217+86.87, 46.4' LT. @ RUNWAY 6/24

PROPOSED SIGN TYPE LEGEND
 0 --- BLANK PANEL - BLACK
 1 --- RUNWAY/TAXIWAY HOLDLINE - WHITE LEGEND ON RED BACKGROUND
 2 --- LOCATION SIGN - YELLOW LEGEND ON BLACK BACKGROUND
 3 --- DIRECTION/INFORMATION SIGN - BLACK LEGEND ON YELLOW BACKGROUND
 * --- DO NOT ENTER AIRCRAFT MOVEMENT AREA

- NOTES:**
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
 - AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
 - THE LOCATION OF EXISTING UTILITIES ARE APPROXIMATE, THE CONTRACTOR SHALL OPEN THE ENTIRE TRENCH BETWEEN MANHOLES BEFORE ANY CONDUIT IS LAID TO ASCERTAIN THE EXISTENCE AND POSITION OF ANY OBSTRUCTIONS.
 - CONTRACTOR SHALL COORDINATE THE LOCATION OF THE EXISTING AND PROPOSED UTILITIES PRIOR TO INSTALLATION OF THE PROPOSED UNIT DUCTS, CONDUITS AND DUCT BANKS. ANY DAMAGES TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
 - CONTRACTOR SHALL CONNECT EXISTING CABLE TO PROPOSED CABLE IN SIGN/LIGHT/MANHOLE/HANDHOLE. COST OF CONNECTION INCIDENTAL TO CABLE.
 - NO 90 DEGREE BENDS WILL BE ALLOWED IN CONDUIT.
 - CONTRACTOR SHALL CORE HOLES INTO EXISTING MANHOLES. THE COST OF CORING AND GROUTING SHALL BE INCIDENTAL TO THE INSTALLATION OF THE PROPOSED CABLE.
 - ALL LIGHTS AND SIGNS SHALL HAVE QUARTZ BULBS.
 - AT EXISTING DUCT BANKS THAT FALL WITHIN THE PROPOSED P.C.C. PAVEMENT, THE CONTRACTOR SHALL STAMP THE DUCT LOCATION, AS DIRECTED BY THE ENGINEER, IN THE PROPOSED P.C.C..
 - INSTALL 2-1/C #8, 5KV, L-824 TYPE C CABLE FOR TAXIWAY C CIRCUIT IN EXISTING DUCT BANK TO THE VAULT.

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APPROVED BY:	
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ILLINOIS PROJECT: FWK-3481 A.I.P. PROJECT: 3-17-0018-BXX	
SHEET 27 OF 38 SHEETS	

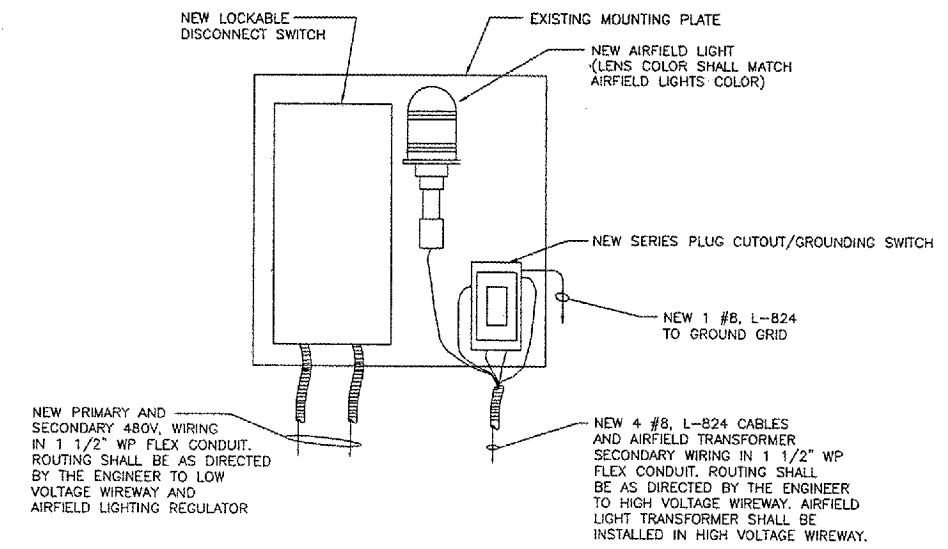


PROP. AIRFIELD ELECT. VAULT PLAN VIEW
 1/4" = 1'-0"

NOTES:
 ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.

PROPOSED VAULT NOMENCLATURE

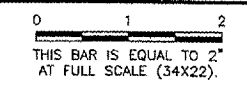
- ① EXISTING 30KW (5-STEP) REGULATOR FOR RUNWAY 16/34.
- ② EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 6/24.
- ③ EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 12/30.
- ④ EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY G,D AND 34 HOLD APRON.
- ⑤ EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RDR.
- ⑥ EXISTING 15KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RGL.
- ⑦ EXISTING 10 KW (3-STEP) REGULATOR FOR RUNWAY 16 APPROACH LIGHTING.
- ⑧ EXISTING 10 KW (3-STEP) REGULATOR FOR HFC APRON LIGHTING.
- ⑨ EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY A,B,E AND F.
- ⑩ EXISTING 20KW (3-STEP) REGULATOR FOR TAXIWAY K.
- ⑪ EXISTING 30 KW (5-STEP) REGULATOR FOR SPARE (STAND-BY) USE.
- ⑫ EXISTING 30KW (3-STEP) REGULATOR FOR TAXIWAY D,L,M,N AND Y.
- ⑬ **NEW 10KW (3-STEP) REGULATOR FOR TAXIWAY C. PROVIDE NEW 30A, 2-POLE DISCONNECT SWITCH.**
- ⑭ FUTURE AIRFIELD LIGHTING REGULATOR.
- ⑮ EXISTING MAIN UTILITY SERVICE CIRCUIT BREAKER DISCONNECT. 800A, 480V, 3-POLE.
- ⑯ EXISTING AUTOMATIC TRANSFER SWITCH. 800A, 480V, 3-POLE.
- ⑰ EXISTING HIGH VOLTAGE POWER DISTRIBUTION PANEL. 480V, 3-PHASE WITH 800AMP MAIN CIRCUIT BREAKER. INSTALL 1-30A, 2-POLE CIRCUIT BREAKER IN EXISTING PANELBOARD.
- ⑱ EXISTING 150KVA, 480V-280Y/120V, 3φ, 4-WIRE TRANSFORMER.
- ⑲ EXISTING LOW VOLTAGE LIGHTING PANEL. 208Y/120V, 3-PHASE WITH 400AMP MAIN CIRCUIT BREAKER.
- ⑳ EXISTING PLC CONTROL CABINET TO BE MODIFIED. SEE VAULT CONTROL DETAILS SHEET.
- ㉑ EXISTING 800AMP CT CABINET.
- ㉒ EXISTING 12"x12"x12' LONG HIGH VOLTAGE WIREWAY.
- ㉓ EXISTING 12"x12"x12' LONG LOW VOLTAGE WIREWAY.
- ㉔ EXISTING 12"x12"x9' LONG HIGH VOLTAGE WIREWAY. HOMERUN CABLES FOR PROPOSED TAXIWAY C CIRCUIT SHALL BE TIED INTO SECONDARY CABLES FROM REGULATOR IN WIREWAY USING L-823 CONNECTORS. CONNECTORS SHALL BE TAPED AND LABELED.
- ㉕ EXISTING 12"x12"x9' LONG LOW VOLTAGE WIREWAY.
- ㉖ **NEW REGULATOR EQUIPMENT MOUNTING PLATE. CONTRACTOR SHALL INSTALL SERIES PLUG CUT-OUT, AIRFIELD LIGHT AND PRIMARY 30A, 2-POLE POWER DISCONNECT FOR TAXIWAY C REGULATOR ON MOUNTING PLATE. SEE MOUNTING PLATE DETAIL THIS SHEET. MATCH WITH EXISTING CONDITIONS.**
- ㉗ **NEW 2-1/C #8 5KV, L-824 AIRFIELD CABLES FOR TAXIWAY C CIRCUIT IN EXISTING 4" CONDUIT TO EXISTING MANHOLE EMH 2.**
- ㉘ **NEW 2 #8 THWN, 1 #10 GND. IN EXISTING 4" GRS CONDUIT FOR TAXIWAY C REGULATOR.**
- ㉙ **NEW 2 #8 THWN, 1 #10 GND. (POWER) AND 4 #12 THWN (CONTROLS) IN EXISTING 1-1/2" GRS CONDUIT.**
- ㉚ **NEW 4 #12 THWN CONTROL WIRES IN EXISTING 4" GRS CONDUIT TO LOW VOLTAGE WIREWAY.**
- ㉛ **NEW 2 #8 THWN, 1 #10 GND. (POWER) AND 4 #12 THWN (CONTROL) IN 1-1/2" WP/FLEX CONDUIT. (PROVIDE 3' MIN. SLACK)**
- ㉜ **NEW 2 #8, 5KV, L-824 AIRFIELD CABLES IN 1" WP/FLEX CONDUIT. (PROVIDE 3' MIN. SLACK)**
- ㉝ **NEW 2 #8, 5KV, L-824, TYPE C CABLES IN EXISTING 4" CONDUIT TO HIGH VOLTAGE WIREWAY.**



EQUIPMENT MOUNTING PLATE DETAIL
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REVISIONS

NUMBER	BY	DATE



**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**
**CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 AIRFIELD ELECTRICAL VAULT MODIFICATIONS**

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SHEET 28 OF 38 SHEETS	

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

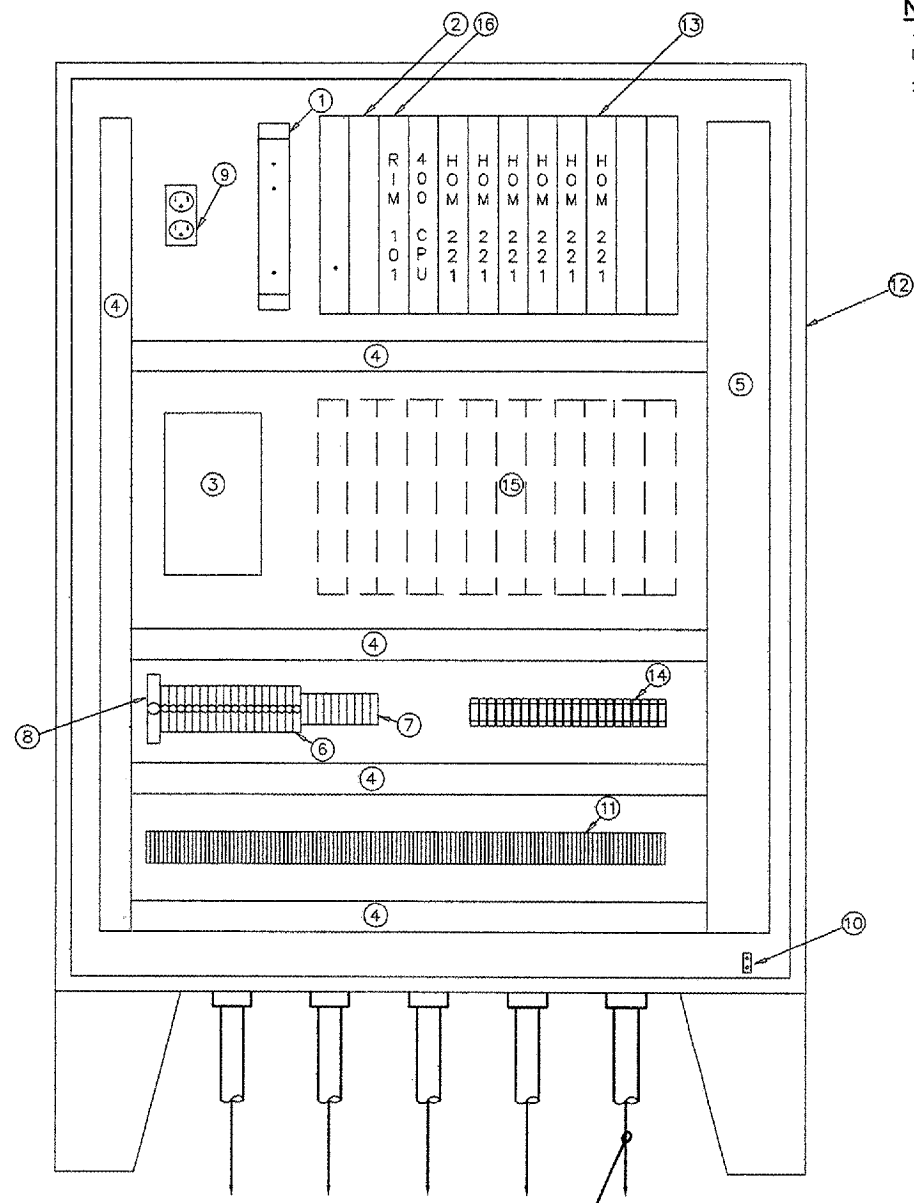
**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 VAULT CONTROL DETAILS - SHEET 1**

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SHEET 29 OF 38 SHEETS	

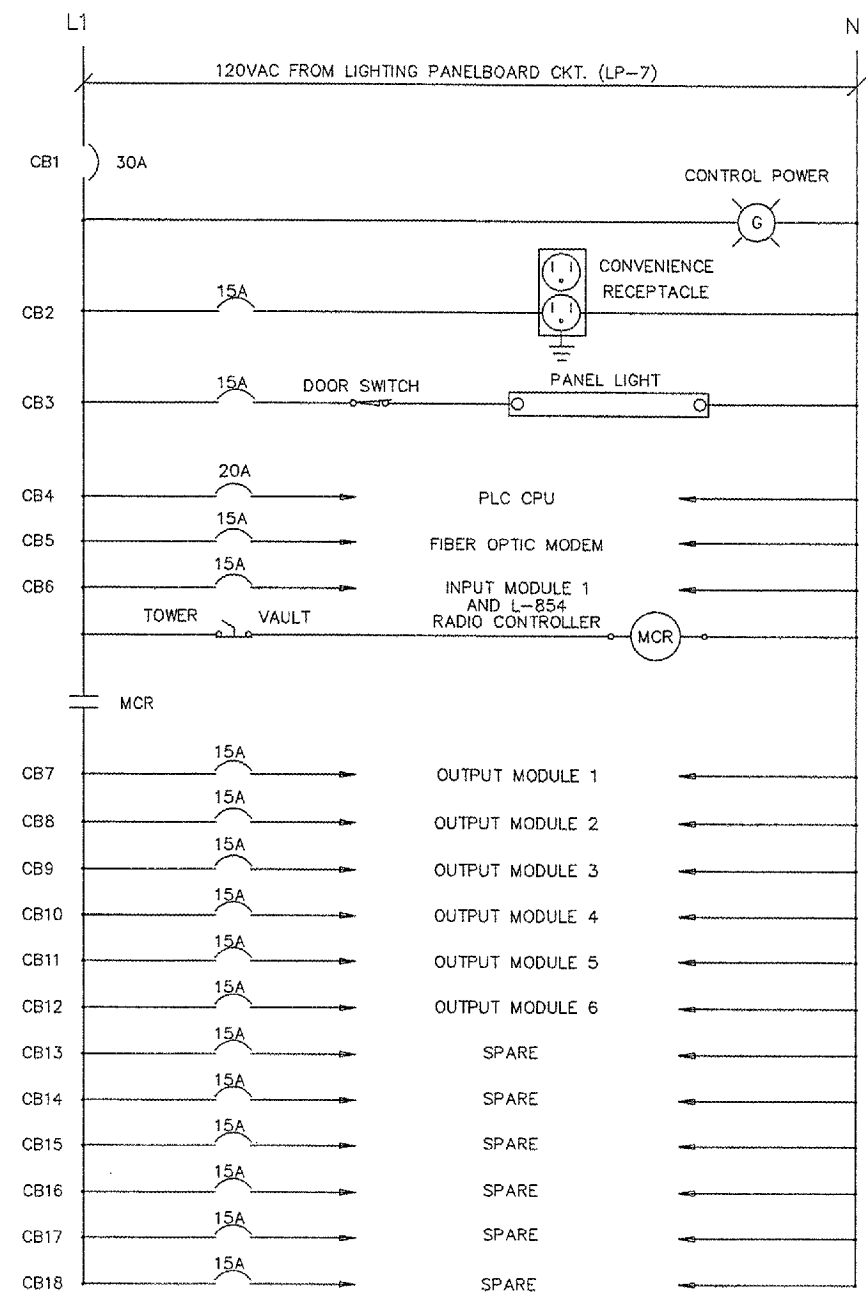
- NOTES:**
1. ALL PROPOSED WORK OF ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
 2. MODIFY EXISTING PLC LOGIC TO ADD PROPOSED 3-STEP REGULATOR.



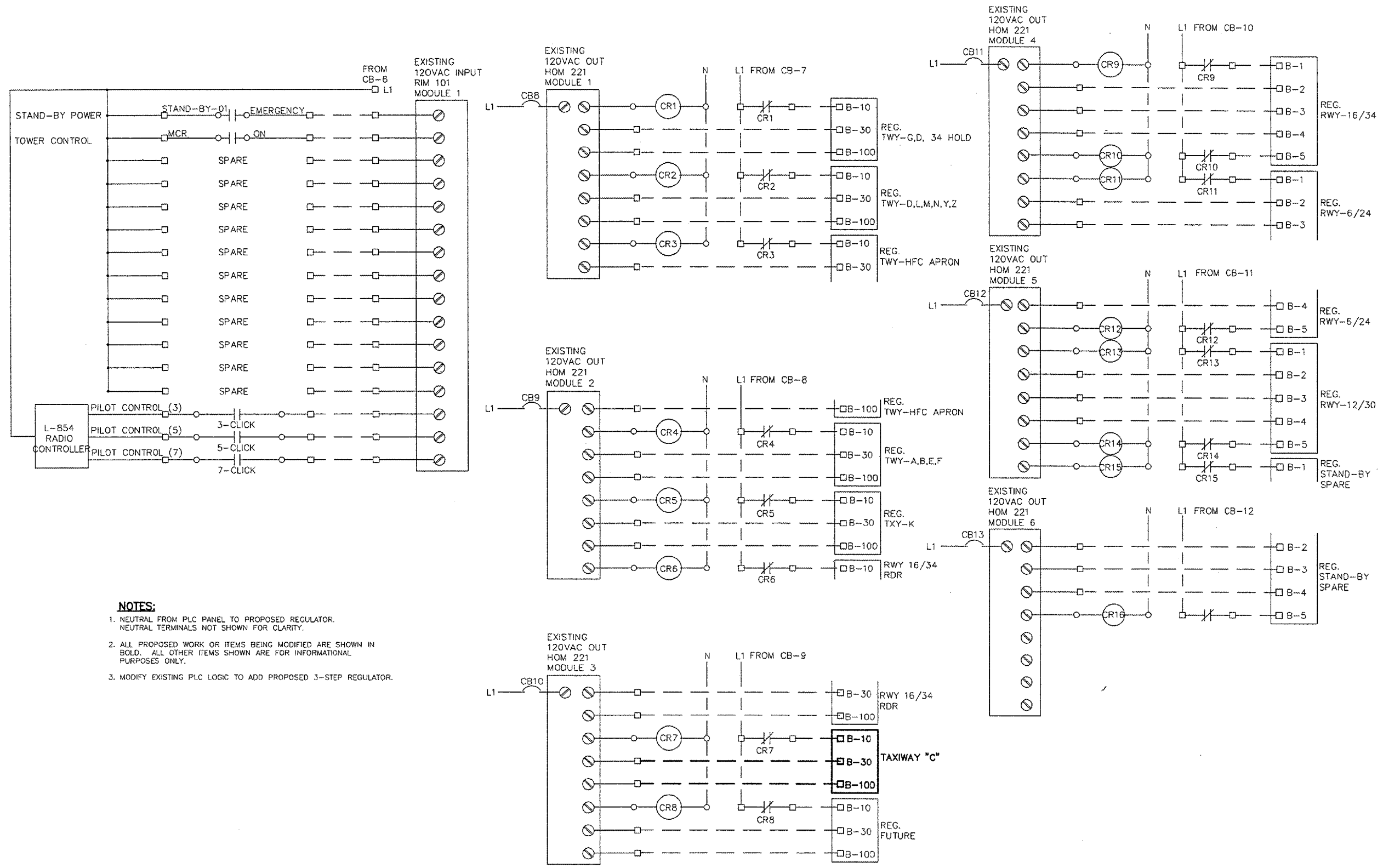
NEW 3 #12 THWN & 1 #12 GND.
 IN EXISTING 4" GRSC TO PROPOSED
 3-STEP REGULATOR FOR TAXIWAY "C"
 CONTROL TERMINALS VIA LOW VOLTAGE
 WIREWAY IN CENTER OF VAULT.

PLC ENCLOSURE DETAIL
 1"=6"

- LEGEND**
- 1 EXISTING FIBER OPTIC MODEM.
 - 2 EXISTING PROGRAMMABLE LOGIC CONTROLLER (PLC).
 - 3 EXISTING PLC POWER SUPPLY.
 - 4 EXISTING 4"x2"W PLASTIC WIRE DUCT.
 - 5 EXISTING 4"x4" PLASTIC WIRE DUCT.
 - 6 EXISTING RAIL MOUNTED CIRCUIT BREAKERS.
 - 7 EXISTING NEUTRAL TERMINALS.
 - 8 EXISTING 30A MAIN CIRCUIT BREAKER.
 - 9 EXISTING CONVENIENCE RECEPTACLE.
 - 10 EXISTING GROUND LUG.
 - 11 EXISTING TERMINALS CONNECT REGULATOR CONTROL WIRES.
 - 12 EXISTING 60"Hx48"Wx12"D NEMA 12 ENCLOSURE
 - 13 (6) EXISTING SQUARE D HOM 221, 120VAC OUTPUT MODULES.
 - 14 (23) EXISTING SINGLE POLE RELAYS OR RELAY TYPE TERMINAL BLOCKS WITH 120VAC COILS & 5A RATED CONTACTS.
 - 15 SPACE FOR FUTURE I/O RACK
 - 16 EXISTING SQUARE D, RIM 101, 120VAC, INPUT MODULE.



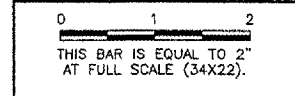
**PLC ENCLOSURE POWER
 SCHEMATIC**



- NOTES:**
1. NEUTRAL FROM PLC PANEL TO PROPOSED REGULATOR. NEUTRAL TERMINALS NOT SHOWN FOR CLARITY.
 2. ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
 3. MODIFY EXISTING PLC LOGIC TO ADD PROPOSED 3-STEP REGULATOR.

REVISIONS

NUMBER	BY	DATE



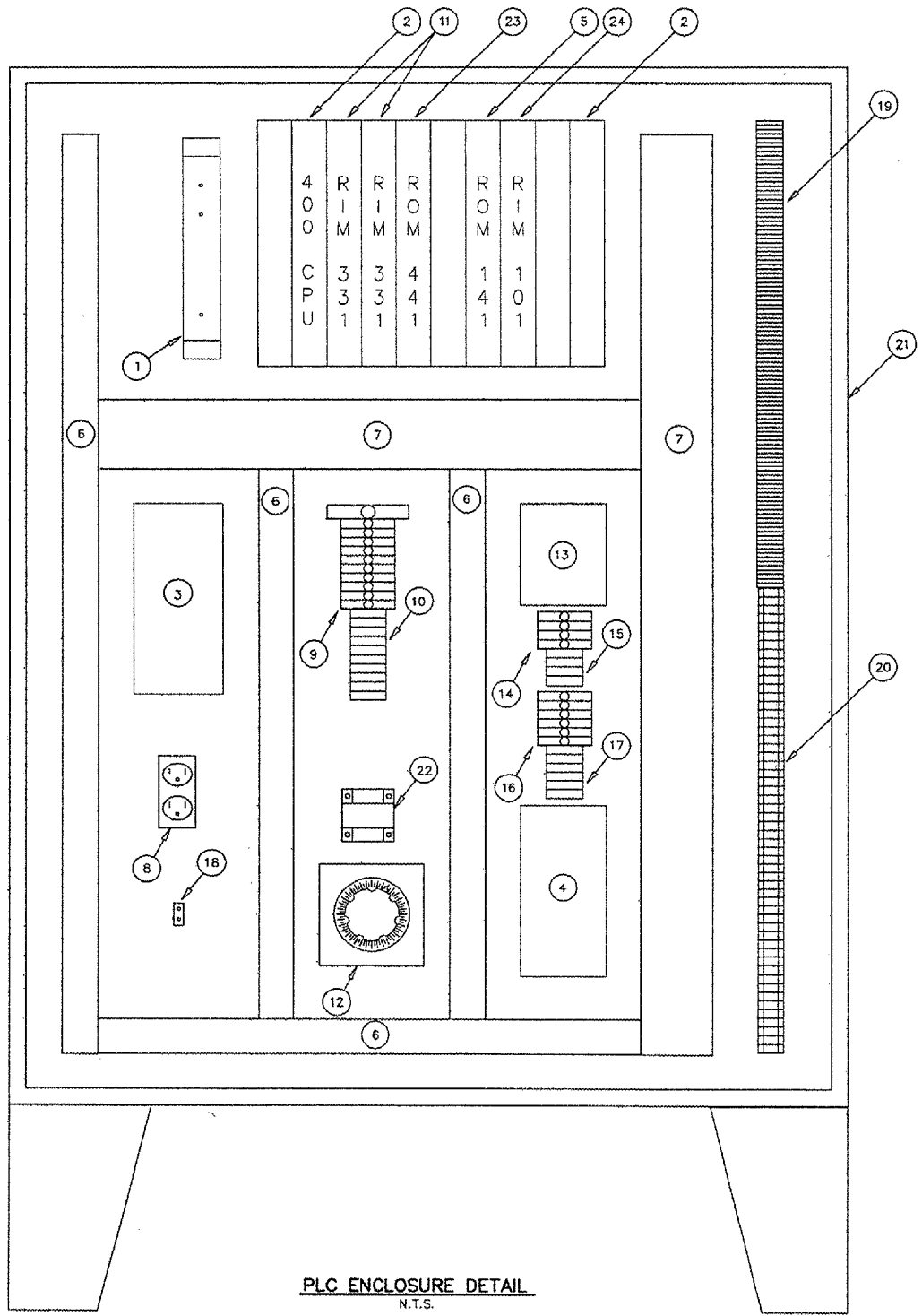
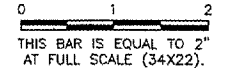
**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 VAULT CONTROL DETAILS - SHEET 2**

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A.I.P. PROJECT:	3-17-0018-BXX
SHEET	30 OF 38 SHEETS

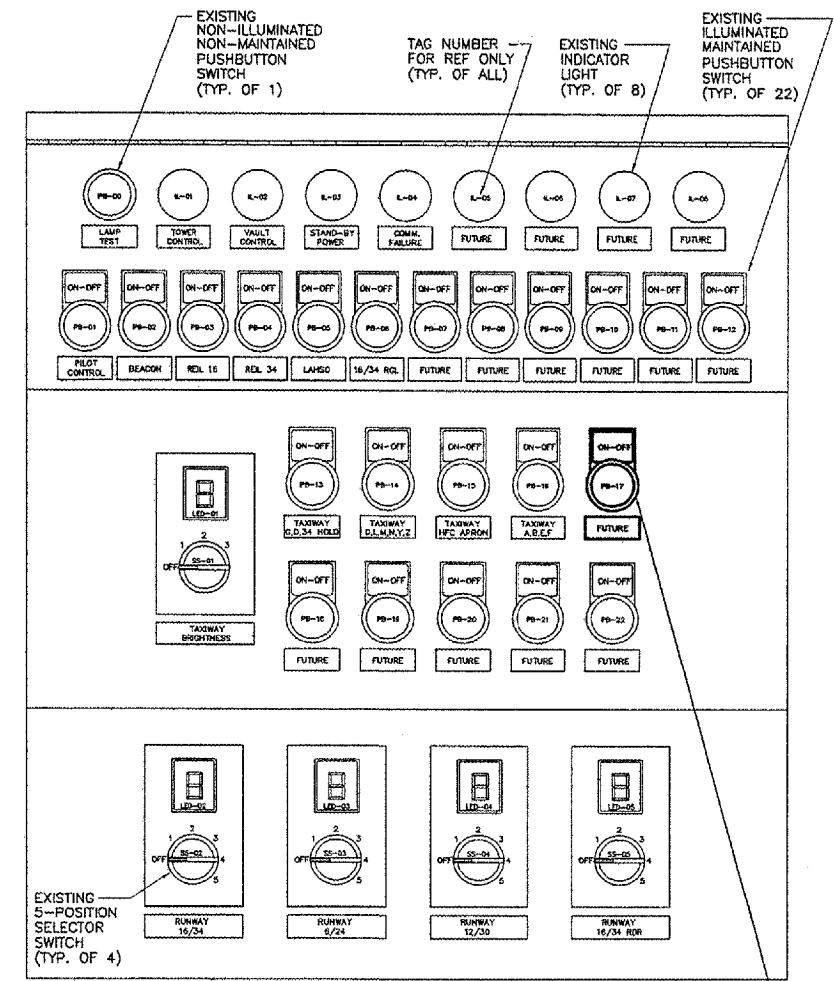
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PLC ENCLOSURE DETAIL
 N.T.S.

- LEGEND**
- 1 EXISTING FIBER OPTIC MODEM.
 - 2 EXISTING PROGRAMMABLE LOGIC CONTROLLER (PLC).
 - 3 EXISTING PLC POWER SUPPLY.
 - 4 EXISTING 24VDC POWER SUPPLY.
 - 5 EXISTING SQUARE D. ROM 141, 5VDC. BCD OUTPUT MODULE.
 - 6 EXISTING 4"Hx2"W PLASTIC WIRE DUCT.
 - 7 EXISTING 4"x4" PLASTIC WIRE DUCT.
 - 8 EXISTING CONVENIENCE RECEPTACLE.
 - 9 EXISTING RAIL MTD AC CKT BKRS CB1 - CB11. VALUES SHOWN ON SCHEMATIC.
 - 10 EXISTING NEUTRAL TERMINALS.
 - 11 EXISTING SQUARE D. RIM 331, 24V DC, INPUT MODULE.
 - 12 EXISTING 0-120VAC VARIABLE XFMR, 230VA, MOUNTED IN 6"x6"x4" NEMA 1, SCREW COVER PULL BOX.
 - 13 EXISTING 5VDC POWER SUPPLY, 2.5A MINIMUM.
 - 14 EXISTING RAIL MTD 5VDC CKT BKRS, CB31 - CB34.
 - 15 EXISTING 5VDC COMMON TERMINALS.
 - 16 EXISTING RAIL MTD 24VDC CKT BKRS, CB21 - CB26.
 - 17 EXISTING 24VDC COMMON TERMINALS.
 - 18 EXISTING GROUND LUG.
 - 19 EXISTING HIGH DENSITY, 600V TERMINAL BLOCKS, TUBULAR SCREW TYPE WITH PRESSURE PLATE.
 - 20 EXISTING SINGLE POLE PLUG IN RELAYS OR RELAY TYPE TERMINAL BLOCKS. 24VDC COILS, 10A RATED CONTACTS.
 - 21 EXISTING ENCLOSURE, 60"Hx48"Wx12"D. NEMA 12, WITH BACK PANEL.
 - 22 EXISTING 120/6VAC, 50VA TRANSFORMER
 - 23 EXISTING SQUARE D. ROM 441, 24V DC, OUTPUT MODULE.
 - 24 EXISTING SQUARE D. RIM 101, 24V DC, INPUT MODULE.

- NOTES:**
- CONTRACTOR SHALL COORDINATE ALL WORK IN THE EXISTING CONTROL TOWER WITH THE AIRWAYS FACILITIES AND THE RESIDENT ENGINEER. CONTRACTOR SHALL GIVE A MINIMUM OF 2 DAYS NOTICE PRIOR TO BEGINNING WORK IN THE TOWER.
 - PROPOSED WORK OF ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
 - MODIFY EXISTING PLC LOGIC TO ADD PROPOSED 3-STEP REGULATOR.



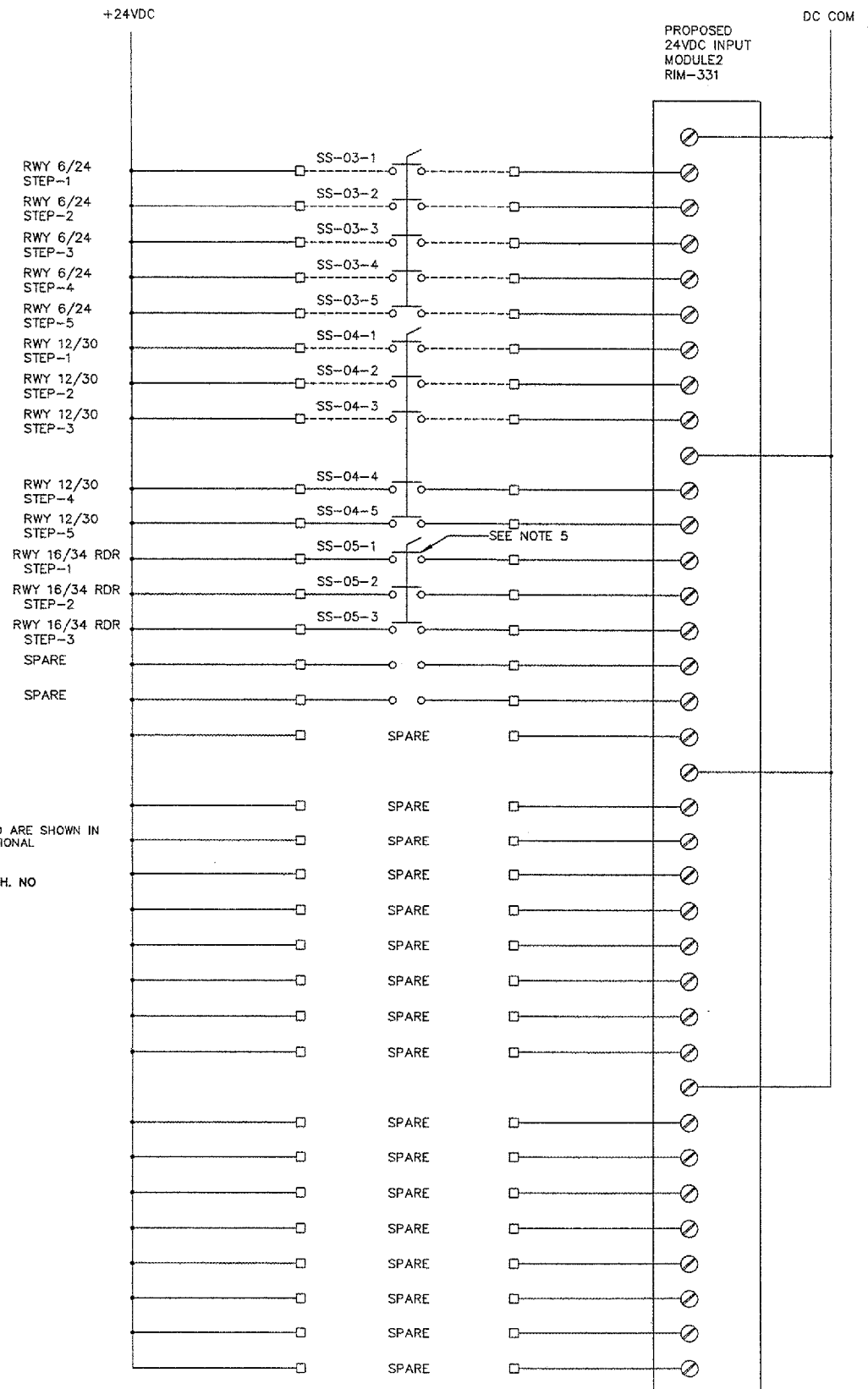
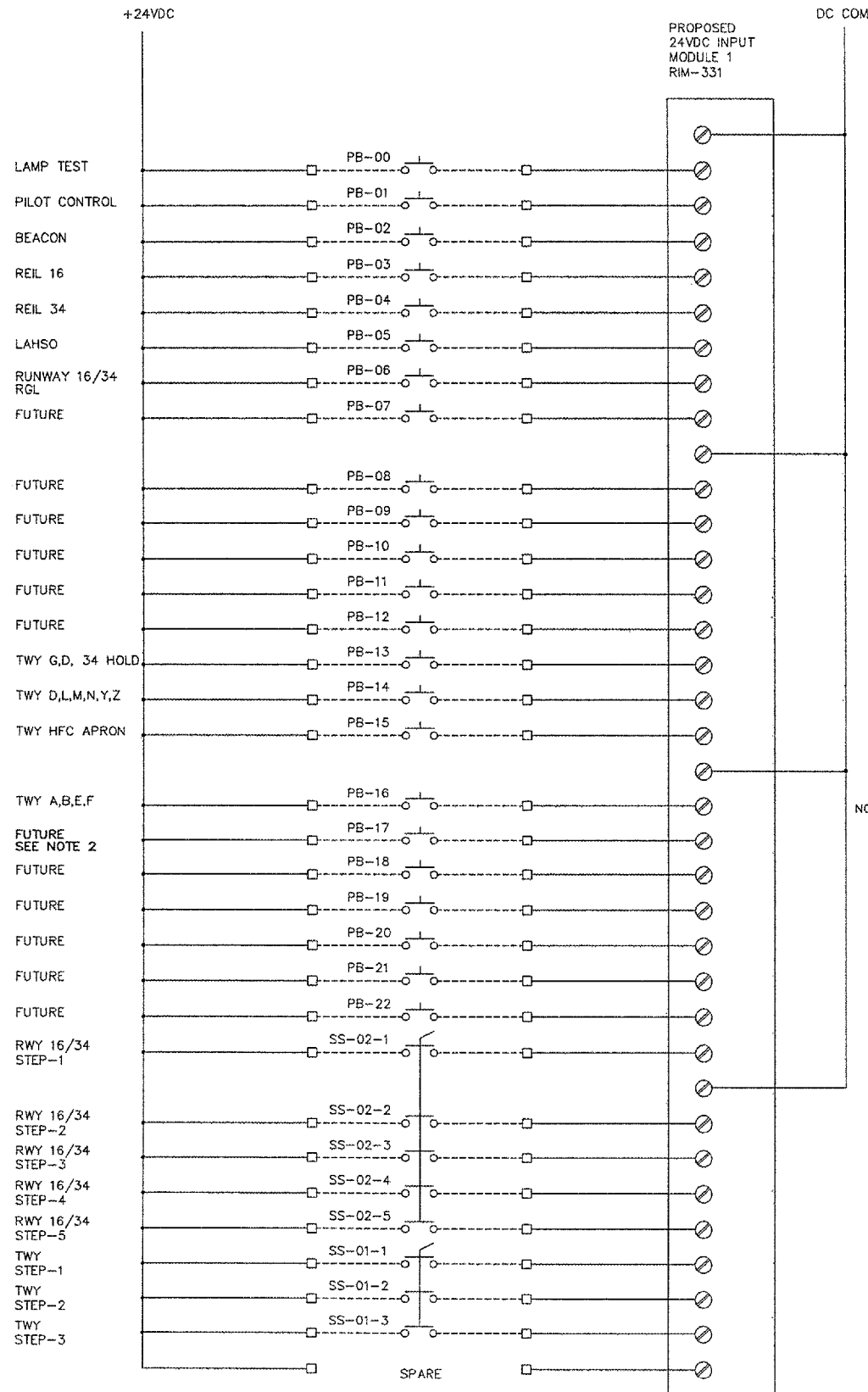
PROPOSED L-821 CONTROL PANEL MODIFICATIONS
 NOT TO SCALE

EXISTING PUSHBUTTON SWITCH TO BE USED AS "TAXIWAY K" SWITCH. REPLACE LABEL TO READ "TAXIWAY K". LABEL SHALL MATCH EXISTING LABELS.

PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 TOWER CONTROL DETAILS I

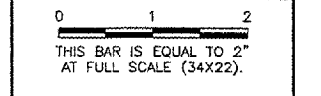
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SHEET 31 OF 38 SHEETS	



NOTES:
 1. ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES.
 2. TO BE USED FOR PROPOSED "TAXIWAY C" SWITCH. NO MODIFICATIONS REQUIRED.

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NUMBER	BY	DATE



**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

TOWER CONTROL DETAILS - SHEET 2

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SHEET 32 OF 38 SHEETS	

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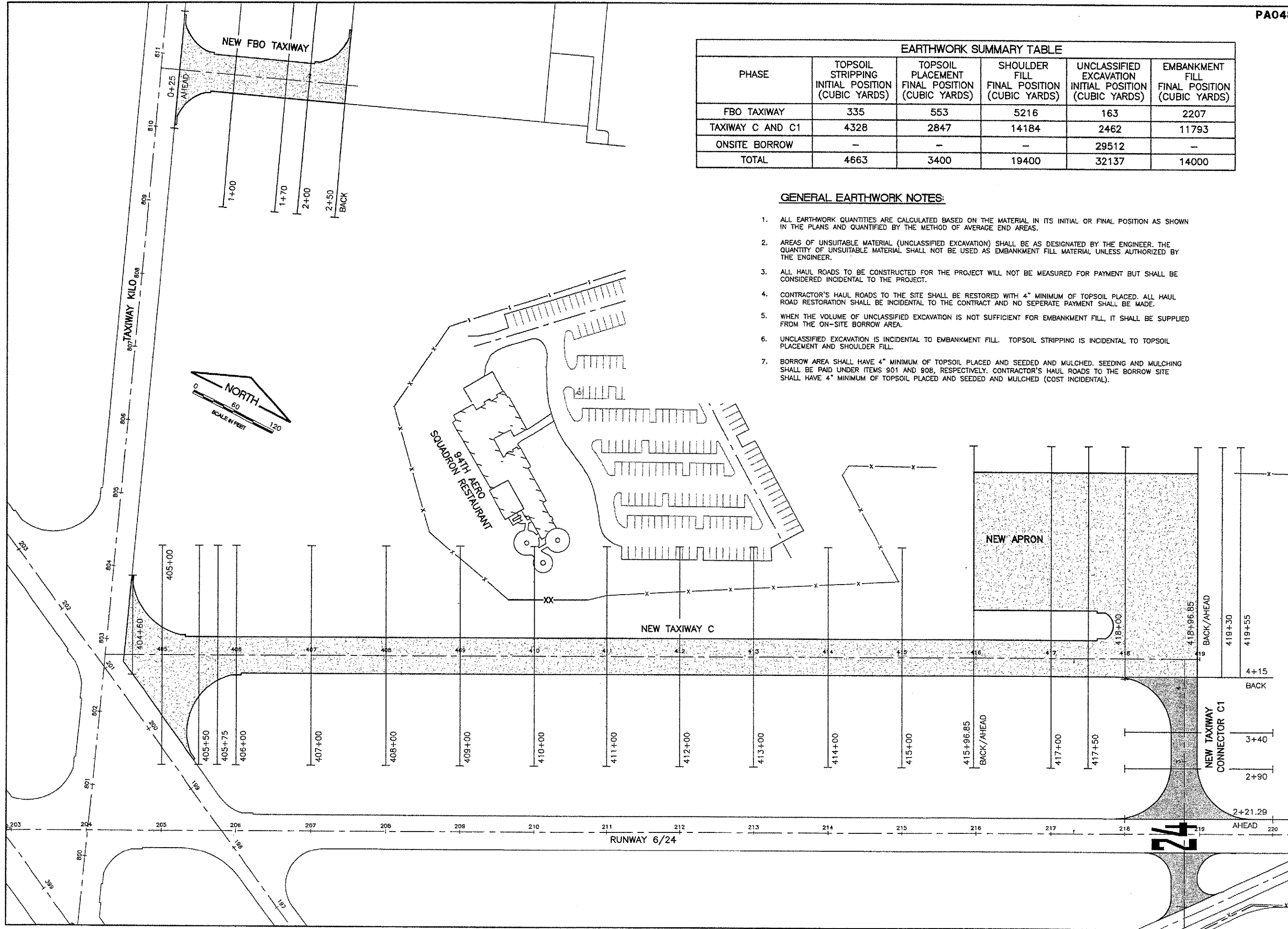
EARTHWORK SUMMARY TABLE					
PHASE	TOPSOIL STRIPPING INITIAL POSITION (CUBIC YARDS)	TOPSOIL PLACEMENT FINAL POSITION (CUBIC YARDS)	SHOULDER FILL FINAL POSITION (CUBIC YARDS)	UNCLASSIFIED EXCAVATION INITIAL POSITION (CUBIC YARDS)	EMBANKMENT FILL FINAL POSITION (CUBIC YARDS)
FBO TAXIWAY	335	553	5216	163	2207
TAXIWAY C AND C1	4328	2847	14184	2462	11793
ONSITE BORROW	-	-	-	29512	-
TOTAL	4663	3400	19400	32137	14000

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

GENERAL EARTHWORK NOTES:

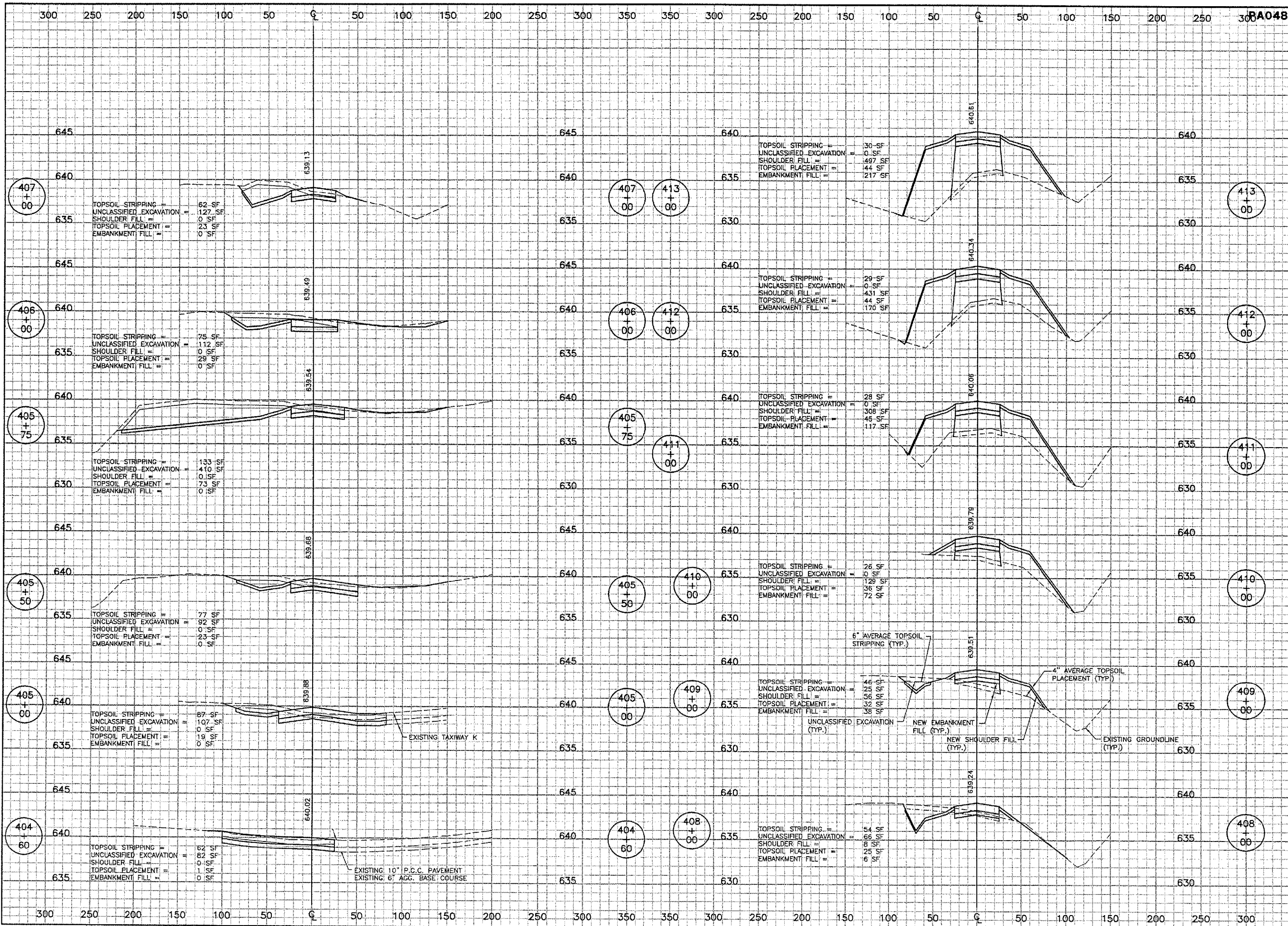
- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
- ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- CONTRACTOR'S HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPERATE PAYMENT SHALL BE MADE.
- WHEN THE VOLUME OF UNCLASSIFIED EXCAVATION IS NOT SUFFICIENT FOR EMBANKMENT FILL, IT SHALL BE SUPPLIED FROM THE ON-SITE BORROW AREA.
- UNCLASSIFIED EXCAVATION IS INCIDENTAL TO EMBANKMENT FILL. TOPSOIL STRIPPING IS INCIDENTAL TO TOPSOIL PLACEMENT AND SHOULDER FILL.
- BORROW AREA SHALL HAVE 4" MINIMUM OF TOPSOIL PLACED AND SEEDED AND MULCHED. SEEDING AND MULCHING SHALL BE PAID UNDER ITEMS 901 AND 908, RESPECTIVELY. CONTRACTOR'S HAUL ROADS TO THE BORROW SITE SHALL HAVE 4" MINIMUM OF TOPSOIL PLACED AND SEEDED AND MULCHED (COST INCIDENTAL).



PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
INDEX TO CROSS SECTIONS / EARTHWORK SUMMARY

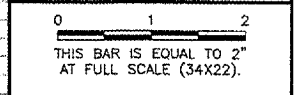
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 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 33 OF 38 SHEETS



300A048
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 SURVEY BOOK #
 XREF DWG:
 XREF DWG:
 DATE: Tue 7/5/05 7:52am

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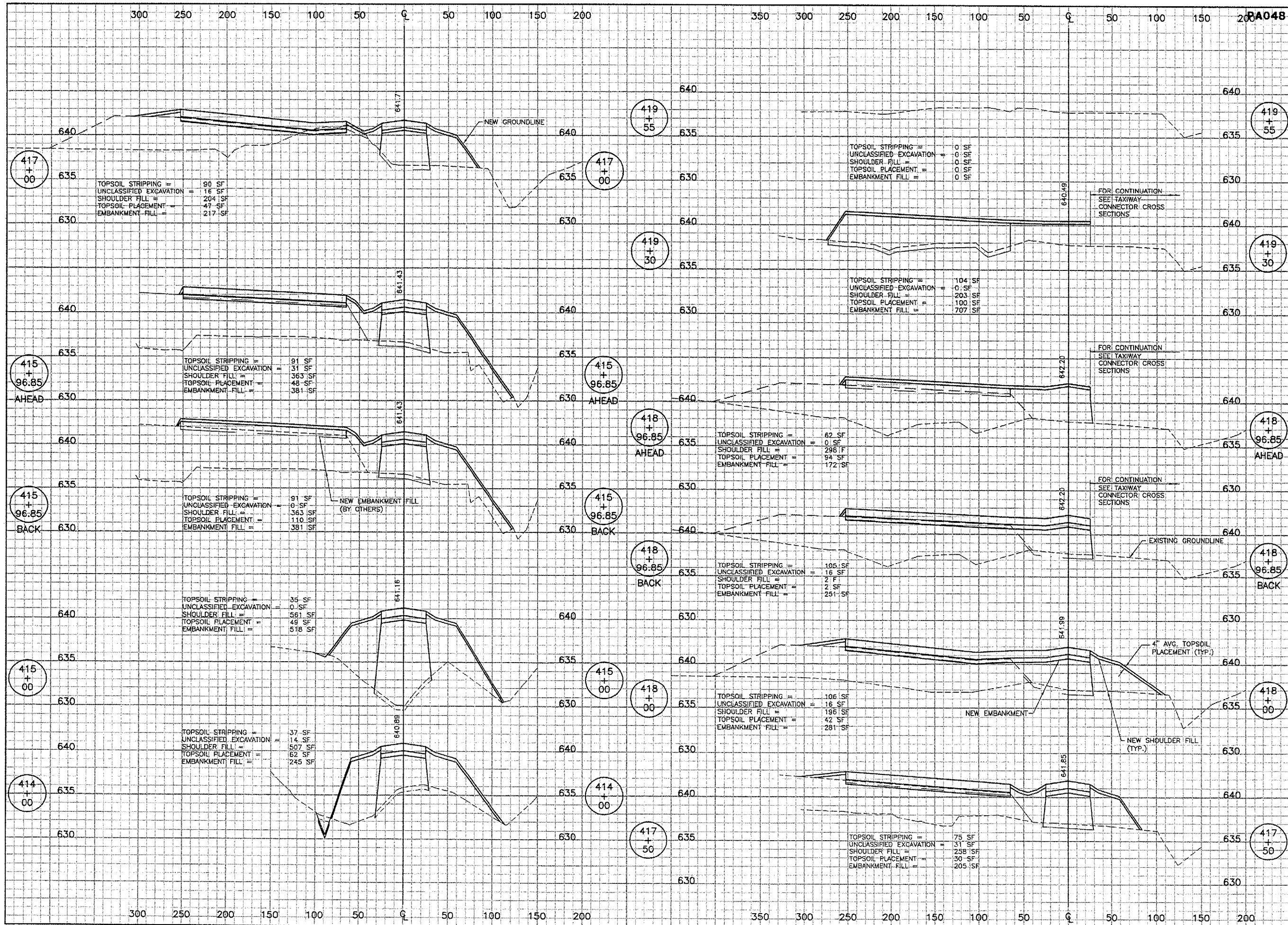
**CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
 CROSS SECTIONS**

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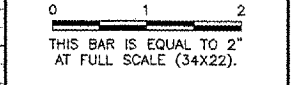
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 CONSULTING ENGINEERS
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DESIGN BY:	
DRAWN BY:	JRL / ARM
CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT: PWK-3481 A.I.P. PROJECT: 3-17-0018-BXX	
SHEET 34 OF 38 SHEETS	



PATH: K:\Palwaukee\0429003\Draw
 FILE: rc004001.dwg
 UPDATE BY: johse
 SURVEY BOOK #
 XREF DWG:
 DATE: Thu 6/16/05 3:24pm

REVISIONS		
NUMBER	BY	DATE



PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
CROSS SECTIONS
STA. 414+00 TO STA. 419+55

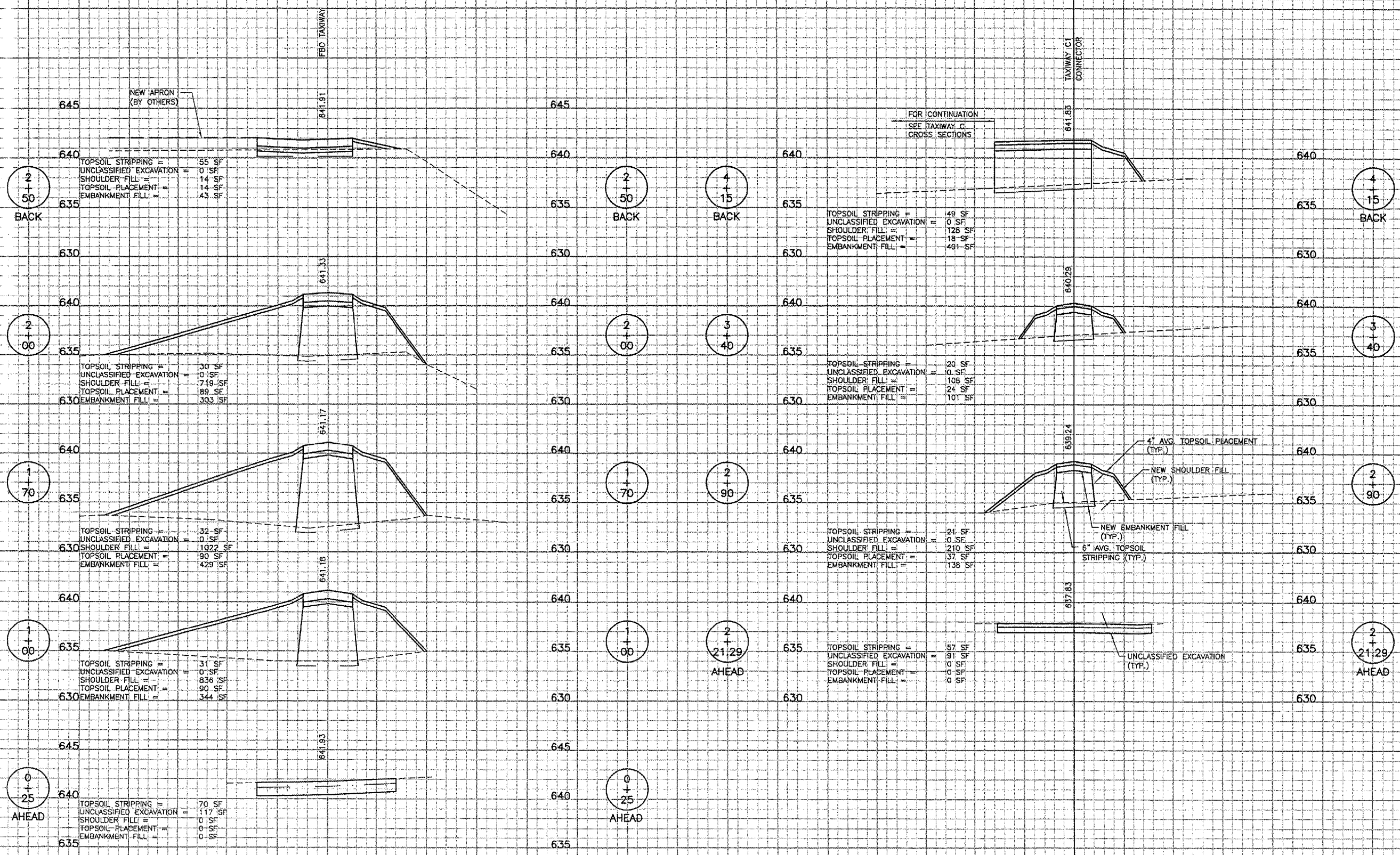
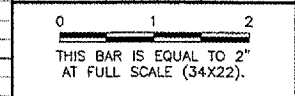
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DESIGN BY:	JRL / ARM
DRAWN BY:	JRO/JLR
CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT: FWK-3481	
A.I.P. PROJECT: 3-17-0018-BXX	
SHEET 35 OF 38 SHEETS	

300 250 200 150 100 50 0 50 100 150 200 250 300 300 250 200 150 100 50 0 50 100 150 200 250 300

PA048
 PATH: K:\Palwaukee\0429003\Draw
 FILE: rc006001.dwg
 UPDATE BY: johse
 SURVEY BOOK #
 XREF DWG:
 XREF DWG:
 DATE: Thu 6/16/05 2:46pm

REVISIONS		
NUMBER	BY	DATE



645
 NEW APRON
 (BY OTHERS)

FOR CONTINUATION
 SEE TAXIWAY C
 CROSS SECTIONS

640
 TOPSOIL STRIPPING = 55 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 14 SF
 TOPSOIL PLACEMENT = 14 SF
 EMBANKMENT FILL = 43 SF

640
 TOPSOIL STRIPPING = 49 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 128 SF
 TOPSOIL PLACEMENT = 18 SF
 EMBANKMENT FILL = 491 SF

640
 TOPSOIL STRIPPING = 30 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 719 SF
 TOPSOIL PLACEMENT = 89 SF
 EMBANKMENT FILL = 303 SF

640
 TOPSOIL STRIPPING = 20 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 108 SF
 TOPSOIL PLACEMENT = 24 SF
 EMBANKMENT FILL = 101 SF

640
 TOPSOIL STRIPPING = 32 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 1022 SF
 TOPSOIL PLACEMENT = 90 SF
 EMBANKMENT FILL = 429 SF

640
 TOPSOIL STRIPPING = 21 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 210 SF
 TOPSOIL PLACEMENT = 37 SF
 EMBANKMENT FILL = 138 SF

4" AVG. TOPSOIL PLACEMENT (TYP.)
 NEW SHOULDER FILL (TYP.)
 NEW EMBANKMENT FILL (TYP.)
 6" AVG. TOPSOIL STRIPPING (TYP.)

640
 TOPSOIL STRIPPING = 31 SF
 UNCLASSIFIED EXCAVATION = 0 SF
 SHOULDER FILL = 836 SF
 TOPSOIL PLACEMENT = 90 SF
 EMBANKMENT FILL = 344 SF

640
 TOPSOIL STRIPPING = 57 SF
 UNCLASSIFIED EXCAVATION = 91 SF
 SHOULDER FILL = 0 SF
 TOPSOIL PLACEMENT = 0 SF
 EMBANKMENT FILL = 0 SF

UNCLASSIFIED EXCAVATION (TYP.)

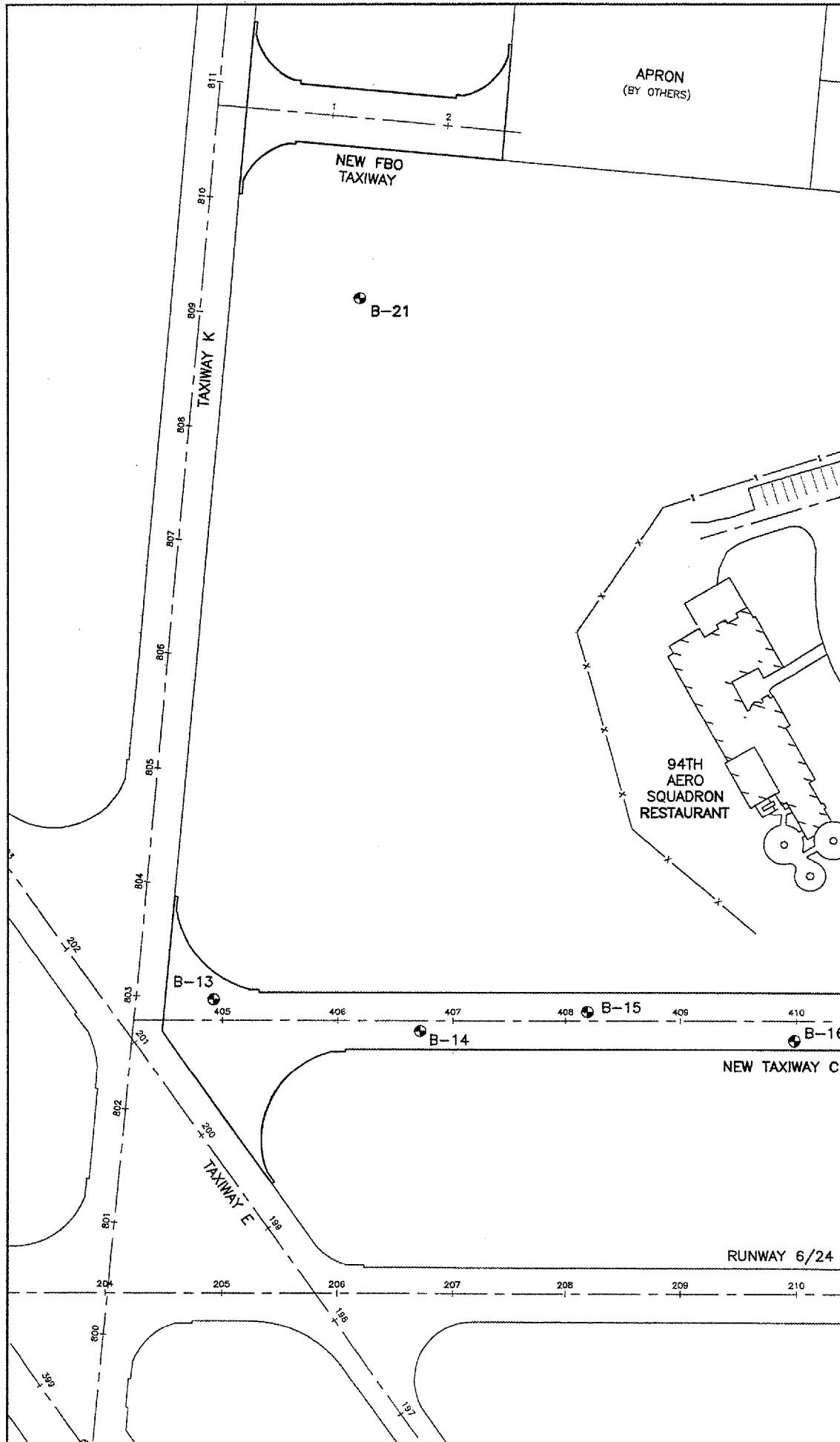
640
 TOPSOIL STRIPPING = 70 SF
 UNCLASSIFIED EXCAVATION = 117 SF
 SHOULDER FILL = 0 SF
 TOPSOIL PLACEMENT = 0 SF
 EMBANKMENT FILL = 0 SF

PALWUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
CROSS SECTIONS
FBO TAXIWAY AND TAXIWAY C1

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DESIGN BY:	JRL / ARM
DRAWN BY:	JRO/JLR
CHECKED BY:	
APPROVED BY:	
DATE:	06/24/05
JOB No:	04290-03
ILLINOIS PROJECT:	PWK-3481
A.I.P. PROJECT:	3-17-0018-BXX
SHEET	36 OF 38 SHEETS

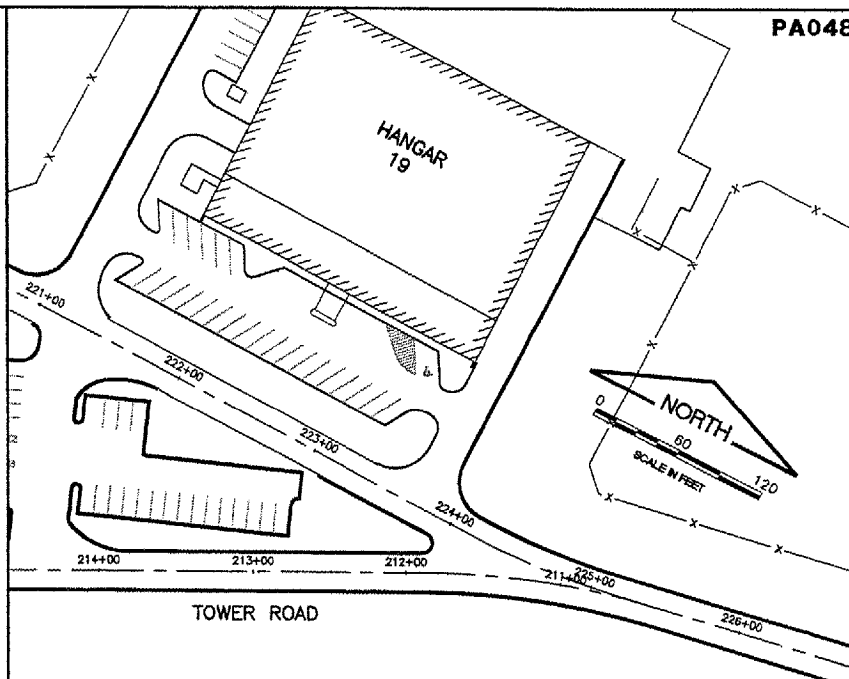
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PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
 BORING 13 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-53,203
 ELEVATIONS GROUND SURFACE 638.3 WATER LEVEL OBSERVATIONS 10.5'
 END OF BORING 624.3 AT END OF BORING 11.0'
 WHILE DRILLING 24 HOURS

DEPTH	ELEV.	SOIL DESCRIPTIONS
0.0	638.0	FILL - Black SILTY CLAY (topsoil)
1.0	637.0	Very tough brown and gray CLAY, moist A-6
2.0	636.0	Tough brown CLAY, very moist A-6
3.0	635.0	Hard brown CLAY LOAM, trace gravel, moist A-6
4.0	634.0	Very tough to hard gray CLAY, occasional silt seams, moist A-6
5.0	633.0	Firm gray medium to fine SAND, trace gravel, very moist A-1-B
15.0	624.0	End of Boring at 15.0'

* Approximate unconfined compressive strength based on measurements with a calibrated pocket penetrometer.



BORING	STATION/OFFSET	NORTHING	EASTING
B-13	204+92.67, 18.75' Lt.	1984241.8914	617550.9678
B-14	206+71.74, 9.28' Rt.	1984298.1179	617723.2754
B-15	208+19.12, 7.91' Lt.	1984380.2691	617846.8451
B-16	209+98.23, 17.5' Rt.	1984438.8421	618017.9959
B-17	212+03.11, 17.5' Lt.	1984562.943	618184.7259
B-18	213+85.33, 0.0' Lt.	1984629.9793	618355.0724
B-19	217+04.93, 9.54' Rt.	1984766.4082	618644.2455
B-20	218+75.29, 111.05' Rt.	1984753.185	618842.1175
B-21	206+20.25, 630.85' Lt.	1984845.2941	617387.1040

*ALL DISTANCES ARE OFFSET FROM THE PROPOSED TAXIWAY CENTERLINE

PA048
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 LAYOUT: Layout1
 UPDATE BY: jlinke
 SURVEY BOOK #
 DATE: Fri 6/17/05 1:40pm
 XREF DWG: tbcInt.dwg
 tb.dwg
 txy-base.dwg

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**PALWAUKEE MUNICIPAL AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS**

CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON

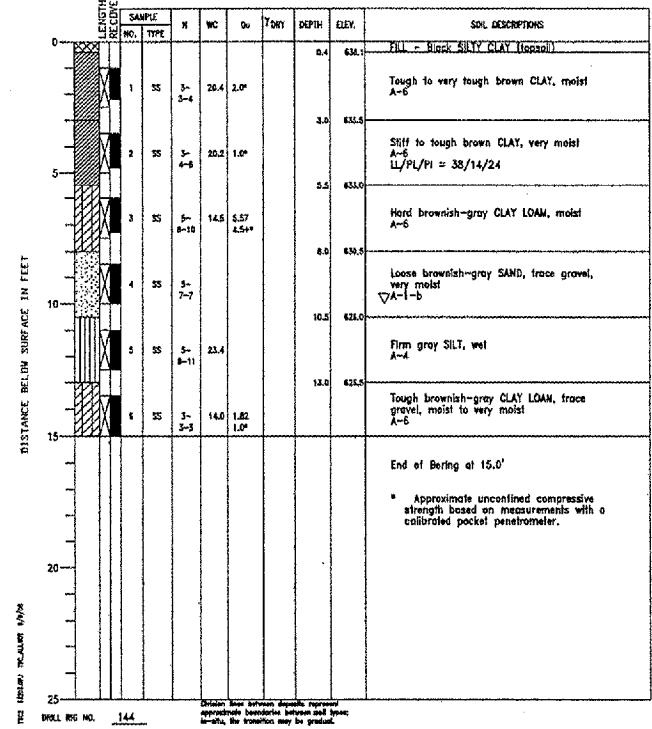
ENGINEERING INFORMATION SHEET 1

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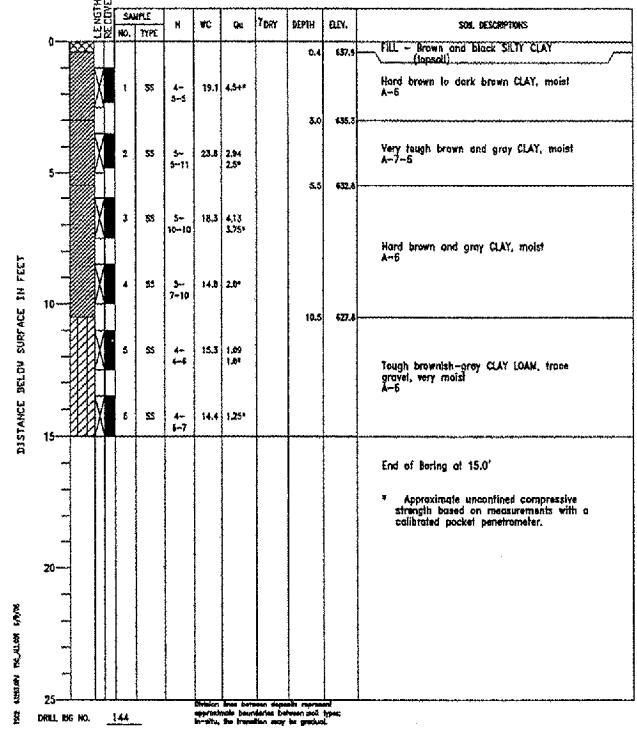
Palwaukee Municipal Airport

DESIGN BY: JRL
 DRAWN BY: JRO
 CHECKED BY:
 APPROVED BY:
 DATE: 06/24/05
 JOB No: 04290-03
 ILLINOIS PROJECT: FWK-3481
 A.I.P. PROJECT: 3-17-0018-BXX
 SHEET 37 OF 38 SHEETS

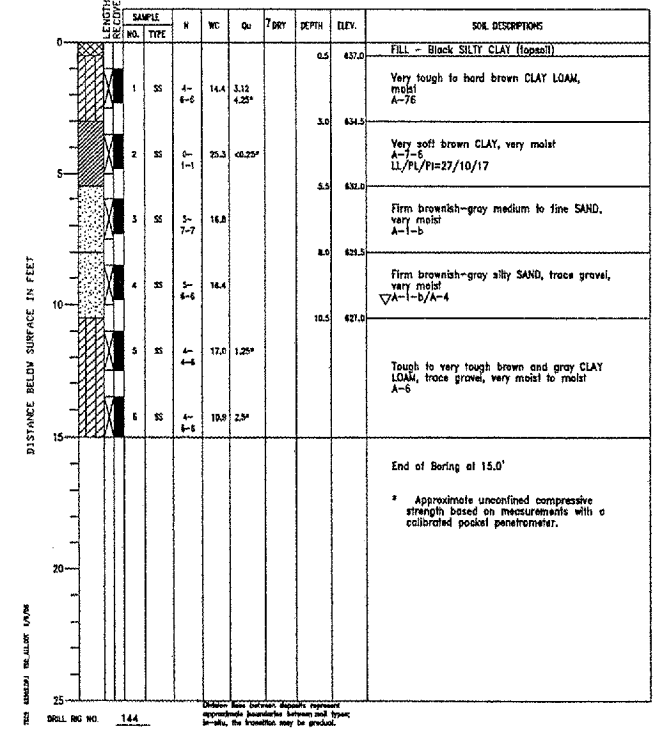
PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 14 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 538.5 WATER LEVEL OBSERVATIONS 10.5'
END OF BORING 623.5 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS



PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 15 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 638.3 WATER LEVEL OBSERVATIONS Dry
END OF BORING 623.5 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS



PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 16 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 637.5 WATER LEVEL OBSERVATIONS 10.5'
END OF BORING 622.5 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS



PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 18 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 656.1 WATER LEVEL OBSERVATIONS 10.5'
END OF BORING 621.1 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS

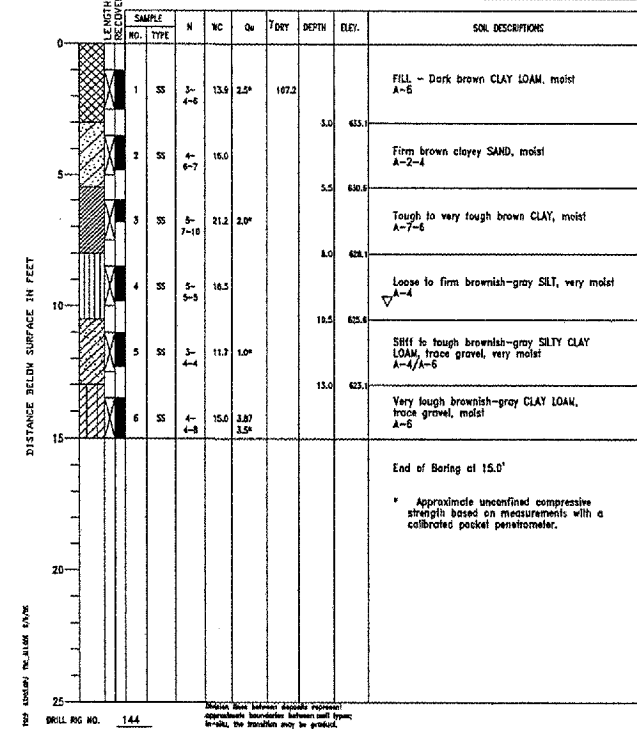
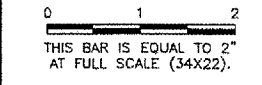
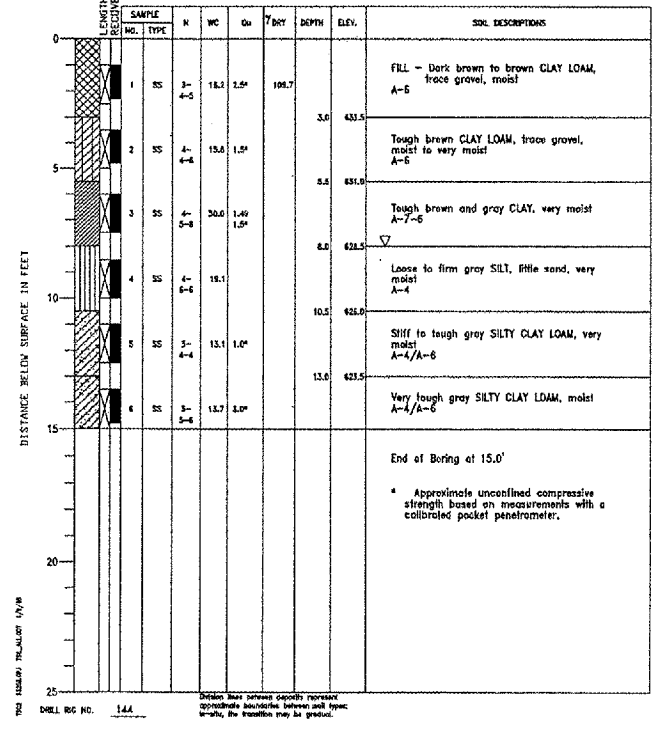


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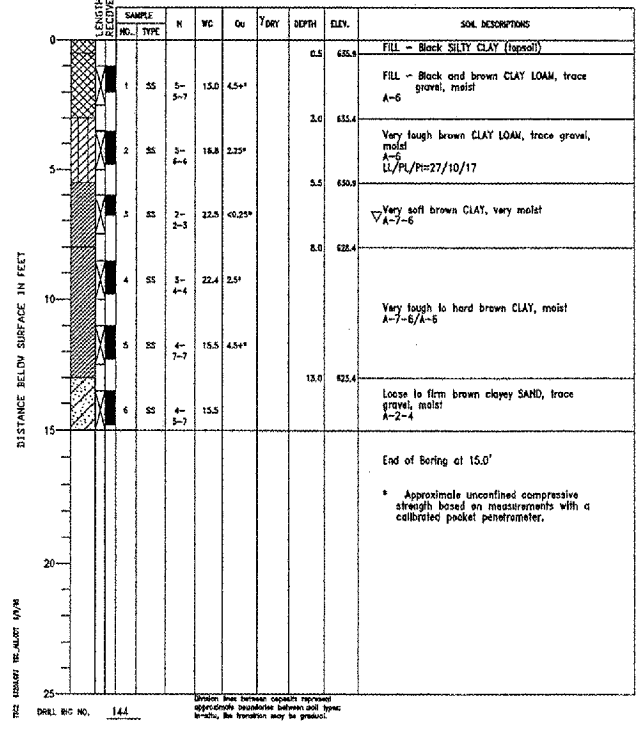


PALWAUKEE MUNICIPAL AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT TAXIWAY CHARLIE AND EAST QUADRANT APRON
ENGINEERING INFORMATION SHEET 2

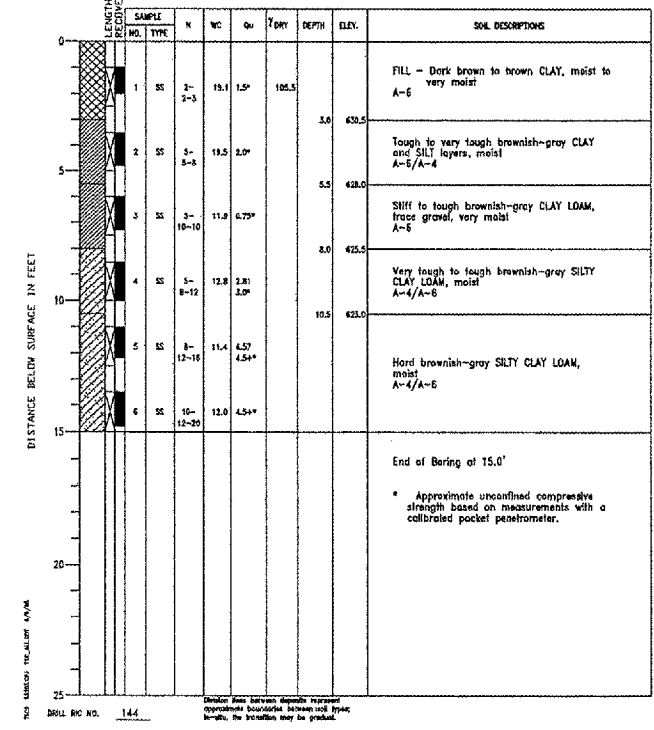
PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 19 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 636.5 WATER LEVEL OBSERVATIONS 8.5'
END OF BORING 621.5 AT END OF BORING 8.0'
WILE DRILLING 24 HOURS



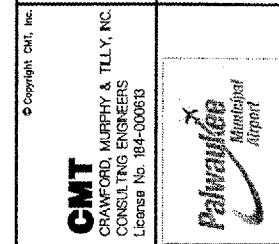
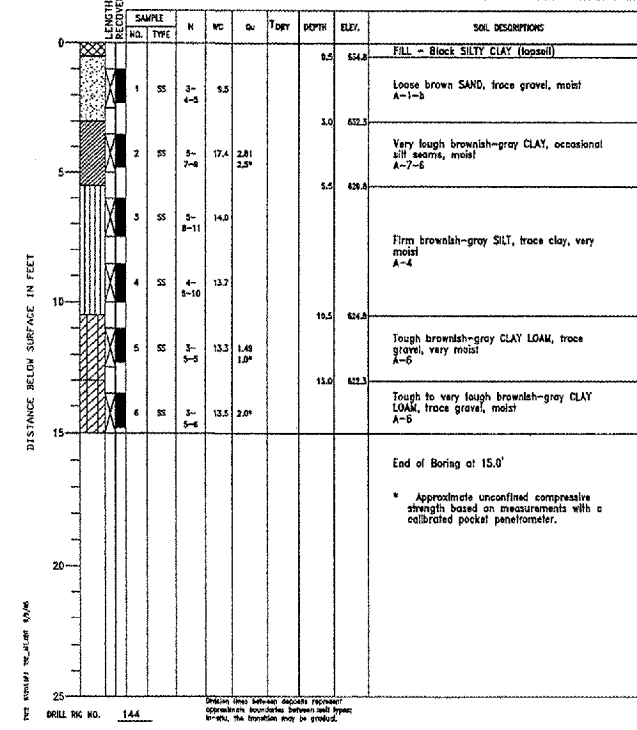
PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 20 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 636.4 WATER LEVEL OBSERVATIONS 8.0'
END OF BORING 621.4 AT END OF BORING 7.0'
WILE DRILLING 24 HOURS



PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 21 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 633.5 WATER LEVEL OBSERVATIONS Dry
END OF BORING 618.5 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS



PROJECT Palwaukee Airport, Parallel Taxiway Runway 6/24, Wheeling, IL
CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois
BORING 17 DATE STARTED 5-16-05 DATE COMPLETED 5-16-05 JOB L-63,203
ELEVATIONS GROUND SURFACE 635.3 WATER LEVEL OBSERVATIONS Dry
END OF BORING 630.3 AT END OF BORING 10.0'
WILE DRILLING 24 HOURS



DESIGN BY: JRL
DRAWN BY:
CHECKED BY:
APPROVED BY:
DATE: 06/24/05
JOB No: 04290-03
ILLINOIS PROJECT: PWK-34B1
A.I.P. PROJECT: 3-17-0018-BXX
SHEET 38 OF 38 SHEETS