

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-1	DUPAGE	33	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60P34		

33 + 1 = 34

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

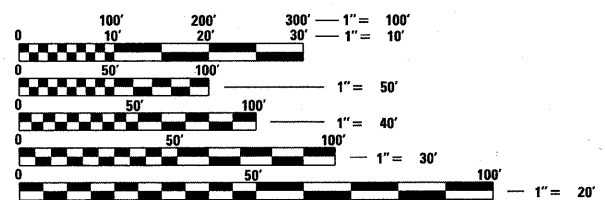
VARIOUS RAMPS
AT VARIOUS DUPAGE COUNTY LOCATIONS
SECTION: 2011-030-1
RESURFACING
DUPAGE COUNTY
C-91-515-11

FOR INDEX OF SHEETS, SEE SHEET NO. 2

D-91-515-11

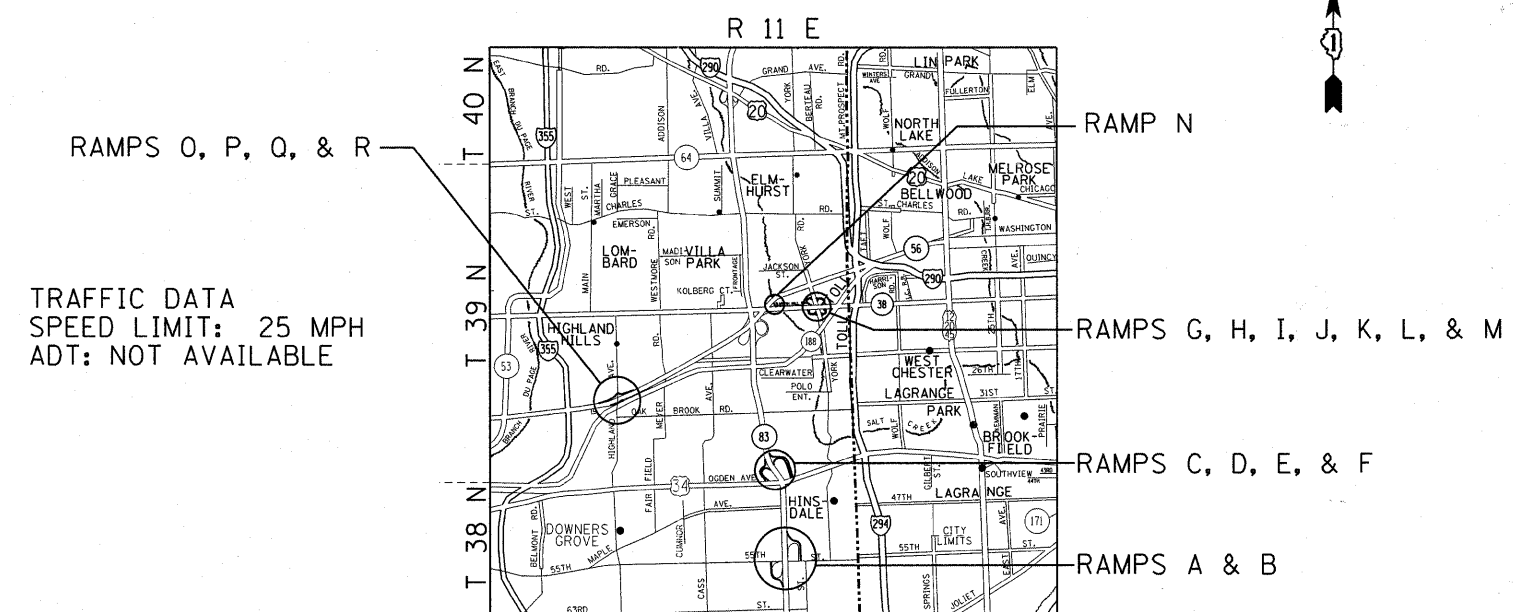


IMPROVEMENT IS LOCATED IN THE CITY OF ELMHURST AND ALSO IN THE VILLAGES OF WESTMONT, LOMBARD, HINDSDALE, DOWNERS GROVE, AND CLARENDON HILLS.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



TRAFFIC DATA
SPEED LIMIT: 25 MPH
ADT: NOT AVAILABLE

ADDISON, YORK & DOWNERS GROVE TOWNSHIPS

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60P34

GROSS LENGTH OF IMPROVEMENT = NET LENGTH IMPROVEMENT = 18,540 FEET (3.5 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 16 20 11

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 1 20 11
Scott E. Stitt P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

July 1 20 11
Christina M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3A-3B	SUMMARY OF QUANTITIES
4-9	TYPICAL SECTIONS
10-20	ROADWAY & PAVEMENT MARKING PLANS
21	DETECTOR LOOP PLANS
22	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
23	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
24	HMA TAPER AT EDGE OF P.C.C. PAVEMENT (BD-33)
25	DETAILS FOR DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL. (BD-34)
26	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
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33	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05)

STATE STANDARDS

000001-00	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606201-02	TYPE B GUTTER INLET, OUTLET & ENTRANCE
630001-09	STEEL PLATE BEAM GUARDRAIL
631031-09	TRAFFIC BARRIER TERMINAL, TYPE 6
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND CITY OF ELMHURST, AND ALSO WITH THE VILLAGES OF HINDSDALE, CLARENDON HILLS, LOMBARD, WESTMONT AND DOWNERS GROVE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL HOT-MIX ASPHALT PAVEMENT PATCHING SHALL BE CLASS D.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

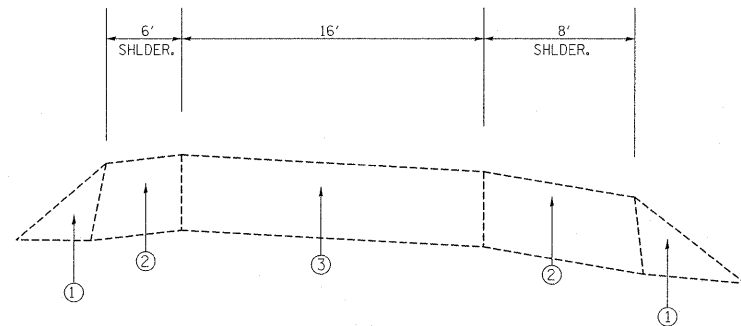
BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VARIOUS AT VARIOUS DUPAGE COUNTY LOCATIONS INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr\pr\work\p\dot\abreuah\0265704\015111\shs-plan.dgn		DRAWN -	REVISED -		SCALE: 1" = 50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT
	PLOT SCALE = 50.7684" / IN.	CHECKED -	REVISED -										
	PLOT DATE = 5/24/2011	DATE -	REVISED -										
											CONTRACT NO. 60P34		

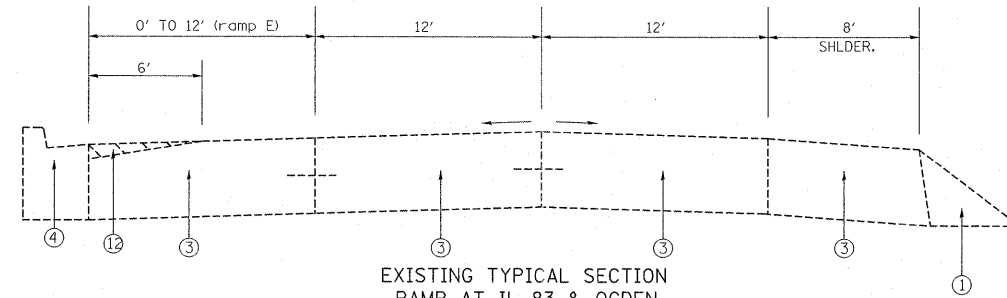
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN 100% STATE					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN 100% STATE						
				0005									0005						
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	27	27						67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
25200110	SODDING, SALT TOLERANT	SO YD	27	27						67100100	MOBILIZATION	L SUM	1	1					
25200200	SUPPLEMENTAL WATERING	UNIT	0.27	0.27						70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	24	24						70300100	SHORT TERM PAVEMENT MARKING	FOOT	5498	5498					
40600300	AGGREGATE (PRIME COAT)	TON	117	117						70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	364	364					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	88	88						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	26925	26925					
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	53	53						70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	998	998					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1						70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2111	2111					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	867	867						70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	308	308					
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	2113	2113						70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	108	108					
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	4663	4663						70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	916	916					
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	3509	3509						78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	364	364					
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	6	6					*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	26925	26925					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1387	1387					*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	998	998					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	2729	2729					*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2111	2111					
42001300	PROTECTIVE COAT	SO YD	190	190					*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	308	308					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	9486	9486					*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	108	108					
44022029	PARTIAL DEPTH REMOVAL 3"	SO YD	922	922					*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	92	92					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	1552	1552					*	78200420	GUARDRAIL MARKERS, TYPE B	EACH	69	69					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	165	165					*	78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	5	5					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	1287	1287						78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	74	74					
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	2158	2158					*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	512	512					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1984	1984					X2020110	GRADING AND SHAPING SHOULDERS	UNIT	248	248						
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1					X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	1187	1187						
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	5	5					X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	362	362						
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	245	245					X4403800	MEDIAN SURFACE REMOVAL	SO FT	490	490						
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	3231.25	3231.25					X4421000	PARTIAL DEPTH PATCHING	TON	155	155						
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	6	6					X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1						
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	5	5					* SPECIALTY ITEMS										
63200310	GUARDRAIL REMOVAL	FOOT	3731	3731															

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	490	490															
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	6	6															
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	11	11															
Z0026346	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1															
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4															

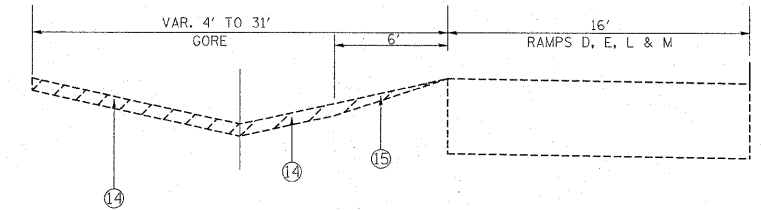
URBAN
100% STATE



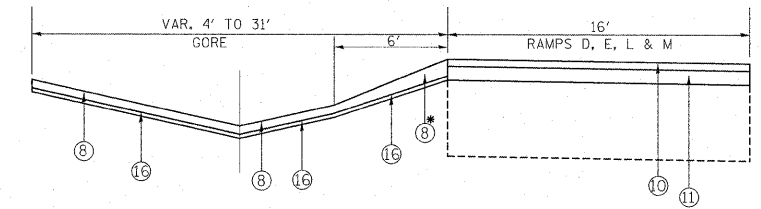
EXISTING TYPICAL SECTION
RAMP AT IL 83 & OGDEN
RAMPS 'C' STA. 15+41 TO STA. 20+66
RAMPS 'E' STA. 11+09 TO STA. 19+56
RAMPS 'D' & 'F'



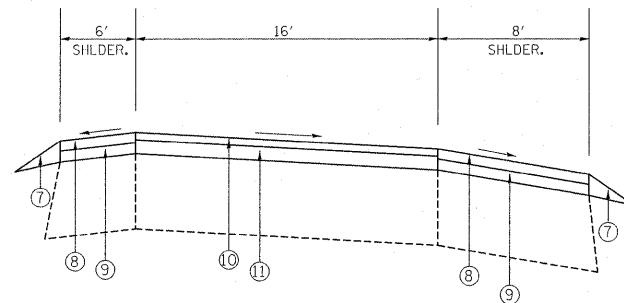
EXISTING TYPICAL SECTION
RAMP AT IL 83 & OGDEN
RAMP 'E' STA. 24+23 TO STA. 28+14
(ADJACENT TO CURB & GUTTER)



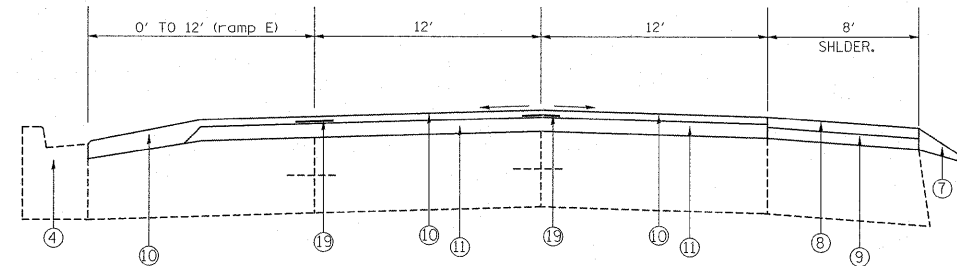
DETAIL A
VARIABLE MILL AT GORE



*SURFACE WILL VARY FROM 1 1/2" TO 3 1/4" FOR A 6' WIDTH
DETAIL B
RESURFACING AT GORE



PROPOSED TYPICAL SECTION
RAMP AT IL 83 & OGDEN
RAMPS 'C' STA. 15+41 TO STA. 20+66
RAMPS 'E' STA. 11+09 TO STA. 19+56
RAMPS 'D' & 'F'



PROPOSED TYPICAL SECTION
RAMP AT IL 83 & OGDEN
RAMP 'E' STA. 24+23 TO STA. 28+14
(ADJACENT TO CURB & GUTTER)

LEGEND:

- ① EXISTING AGGREGATE SHOULDER TYP. B
- ② EXISTING HMA SHOULDER, (VARIABLE DEPTH)
- ③ EXISTING JOINTED REINFORCED PC CONCRETE PAVEMENT, 10"
- ④ EXISTING CURB & GUTTER B 6.12
- ⑤ EXISTING CURB & GUTTER B 6.24
- ⑥ EXISTING CURB & GUTTER M 2.24
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 1 1/2" (SHOULDER/GORE/ HMA RAMP SECTION)
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2" (SHOULDER)
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 1 3/4" (RAMP)
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2 1/4" (RAMP)
- ⑫ PROPOSED PCC SURFACE REMOVAL (VAR. DEPTH), PER STD. BD-33
- ⑬ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" (GORE/HMA RAMP SECTION)
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VAR. DEPTH (0 TO 2 1/4") (GORE)
- ⑯ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4" (GORE/HMA RAMP SECTION)
- ⑰ PROPOSED PARTIAL DEPTH PATCH, (HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 3")
- ⑱ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; ±5"
- ⑲ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT

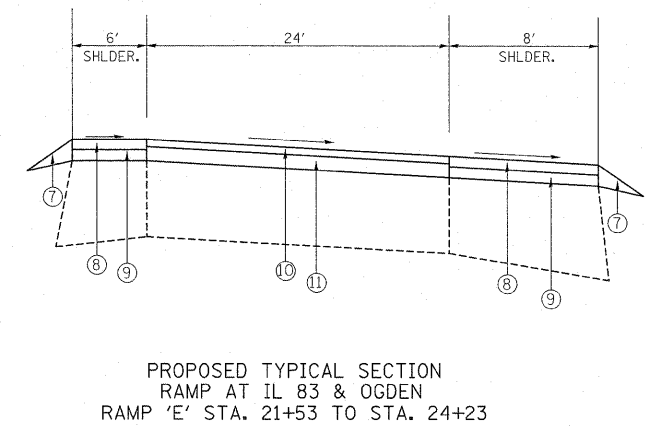
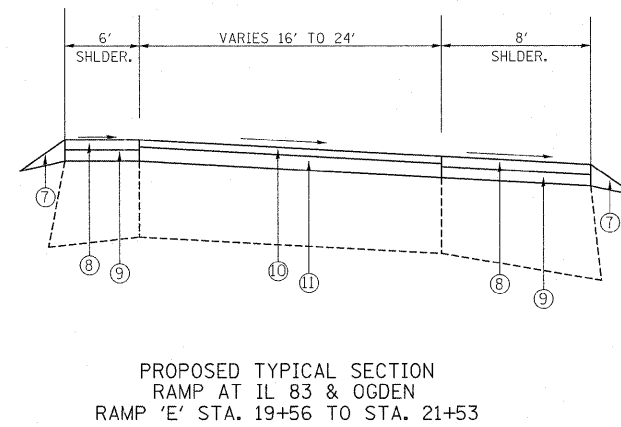
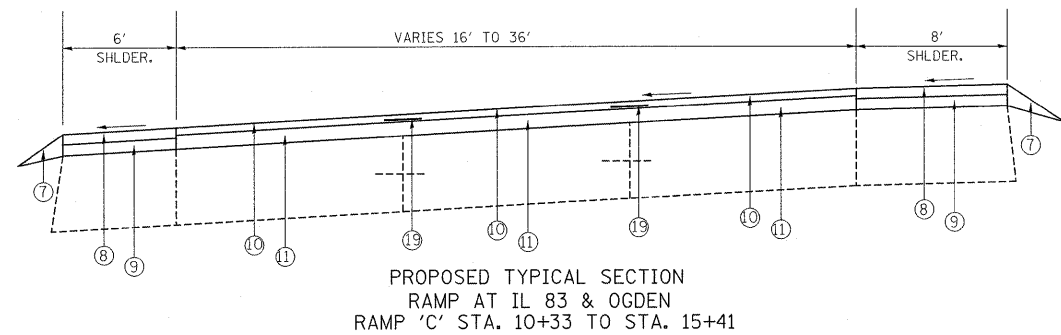
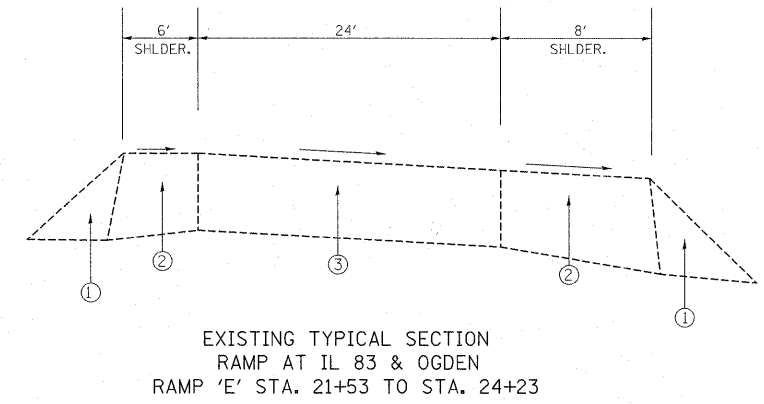
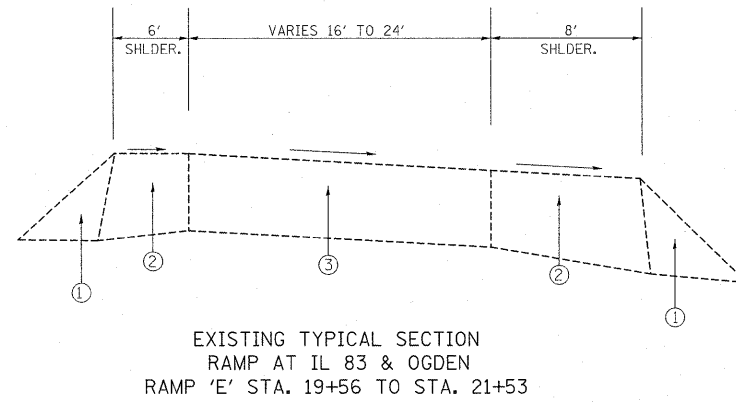
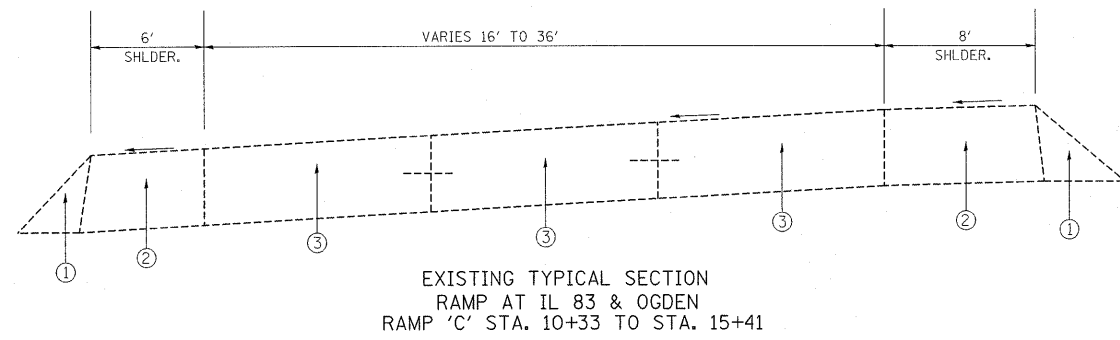
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
RESURFACING (OVER BARE CONCRETE RAMPS)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)	4% @ 90 GYR
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	4% @ 90 GYR
RESURFACING (OVER RAMP SHOULDERS & LEVELING LOW SHOULDERS)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm)	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 GYR
RESURFACING (GORE/HMA RAMP SECTION)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm)	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70	4% @ 70 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR
PARTIAL DEPTH PATCH (FOR LONGITUDINAL JOINTS ON RAMPS O, P, Q, & R)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm)	4% @ 70 GYR
HOT MIX ASPHALT MEDIANS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, (IL 9.5 mm)	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/ SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME =	USER NAME = abreaah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VARIOUS RAMPS AT VARIOUS DUPAGE COUNTY LOCATIONS TYPICAL SECTIONS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pwork\abreaah\d0265704\015111-sht-plan.dgn	11-sht-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	DUPAGE	33	4
		CHECKED -	REVISED -										
		DATE -	REVISED -										

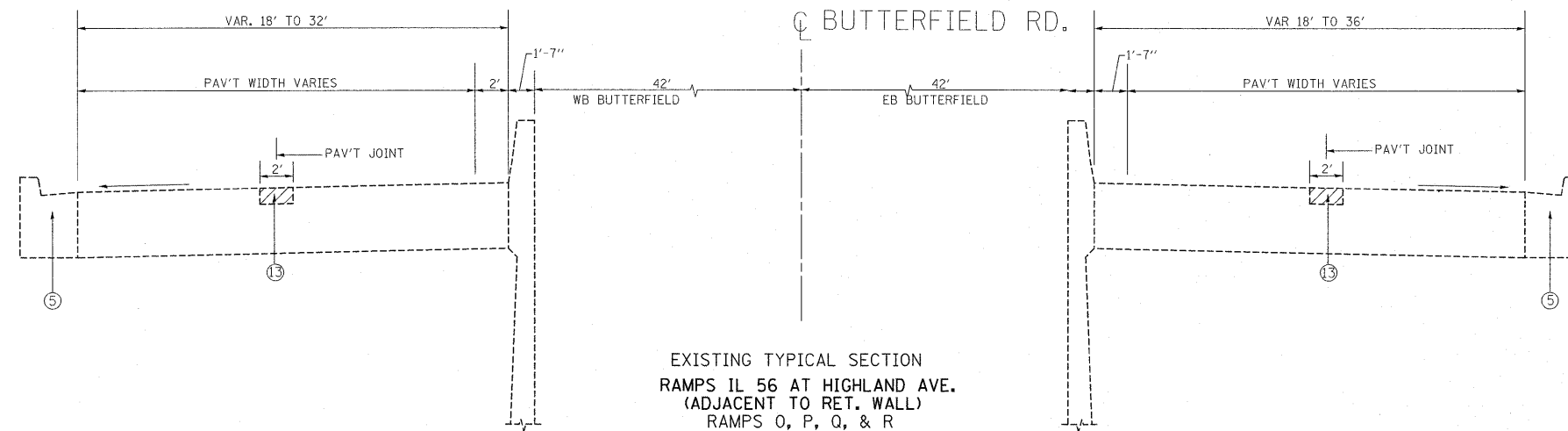
ILLINOIS FED. AID PROJECT CONTRACT NO. 60P34



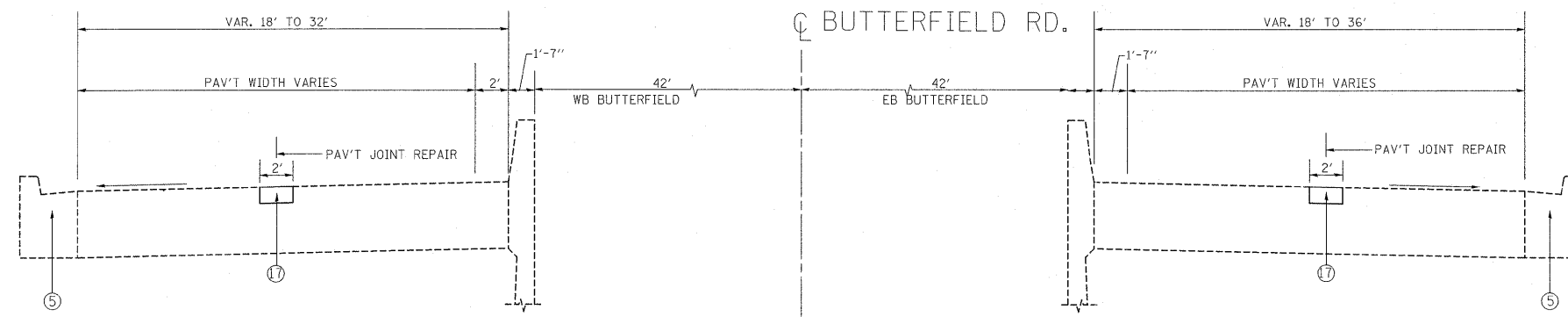
LEGEND:

- ① EXISTING AGGREGATE SHOULDER TYP. B
- ② EXISTING HMA SHOULDER, (VARIABLE DEPTH)
- ③ EXISTING JOINTED REINFORCED PC CONCRETE PAVEMENT, 10"
- ④ EXISTING CURB & GUTTER B 6.12
- ⑤ EXISTING CURB & GUTTER B 6.24
- ⑥ EXISTING CURB & GUTTER M 2.24
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 1 1/2" (SHOULDER/GORE/ HMA RAMP SECTION)
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2" (SHOULDER)
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 1 3/4" (RAMP)
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2 1/4" (RAMP)
- ⑫ PROPOSED PCC SURFACE REMOVAL (VAR. DEPTH), PER STD. BD-33
- ⑬ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" (GORE/HMA RAMP SECTION)
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VAR. DEPTH (0 TO 2 1/4") (GORE)
- ⑯ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4" (GORE/HMA RAMP SECTION)
- ⑰ PROPOSED PARTIAL DEPTH PATCH, (HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 3")
- ⑱ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; ±5"
- ⑲ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT

FILE NAME =	USER NAME = ebraueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VARIOUS RAMPS AT VARIOUS DUPAGE COUNTY LOCATIONS TYPICAL SECTIONS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\pudot\ebraueh\d0265704\015111-sh-t-plan.dgn	11-sh-t-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	DUPAGE	33	5
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
PLOT DATE = 5/23/2011		DATE -	REVISED -		CONTRACT NO. 60P34								



EXISTING TYPICAL SECTION
RAMPS IL 56 AT HIGHLAND AVE.
(ADJACENT TO RET. WALL)
RAMPS O, P, Q, & R



PROPOSED TYPICAL SECTION
RAMPS IL 56 AT HIGHLAND AVE.
RAMPS O, P, Q, & R

LEGEND:

- ① EXISTING AGGREGATE SHOULDER TYP. B
- ② EXISTING HMA SHOULDER, (VARIABLE DEPTH)
- ③ EXISTING JOINTED REINFORCED PC CONCRETE PAVEMENT, 10"
- ④ EXISTING CURB & GUTTER B 6.12
- ⑤ EXISTING CURB & GUTTER B 6.24
- ⑥ EXISTING CURB & GUTTER M 2.24
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 1 1/2" (SHOULDER/GORE/ HMA RAMP SECTION)
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2" (SHOULDER)
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 1 3/4" (RAMP)
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2 1/4" (RAMP)
- ⑫ PROPOSED PCC SURFACE REMOVAL (VAR. DEPTH), PER STD. BD-33
- ⑬ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" (GORE/HMA RAMP SECTION)
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VAR. DEPTH (0 TO 2 1/4") (GORE)
- ⑯ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4" (GORE/HMA RAMP SECTION)
- ⑰ PROPOSED PARTIAL DEPTH PATCH, (HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 3")
- ⑱ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; ±5"
- ⑲ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT

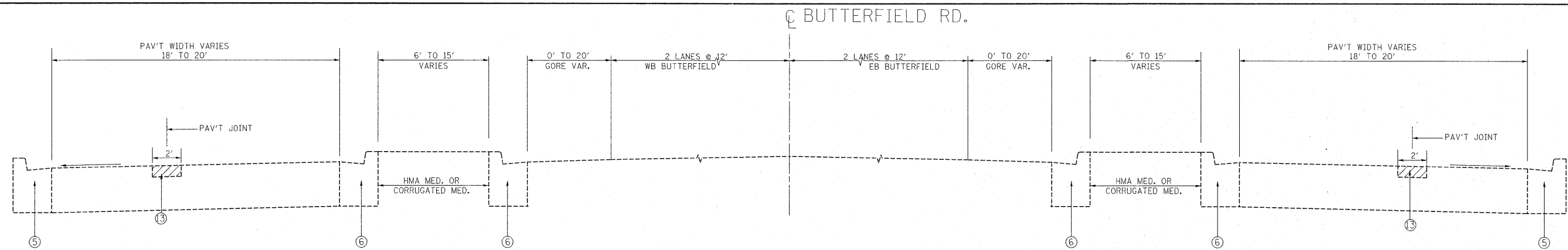
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 5/23/2011		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

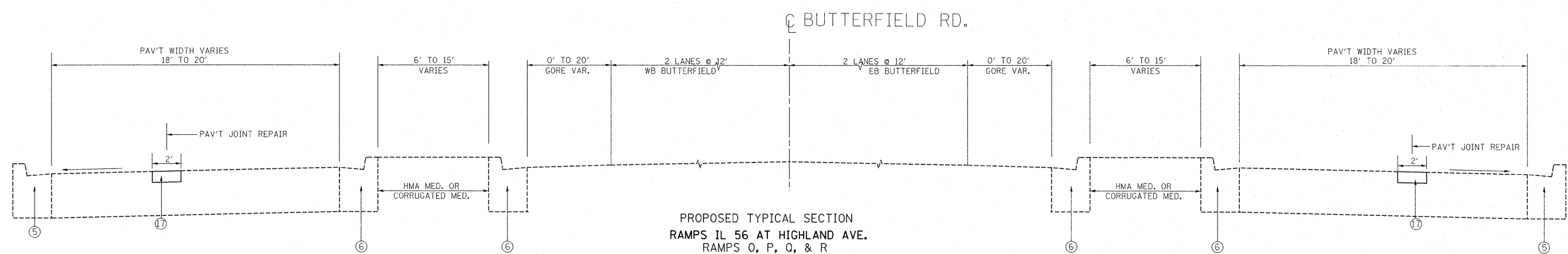
VARIOUS RAMPS AT VARIOUS DUPAGE COUNTY LOCATIONS
TYPICAL SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	33	6
CONTRACT NO. 60P34				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
RAMPS IL 56 AT HIGHLAND AVE.
RAMPS O, P, Q, & R



PROPOSED TYPICAL SECTION
RAMPS IL 56 AT HIGHLAND AVE.
RAMPS O, P, Q, & R

LEGEND:

- ① EXISTING AGGREGATE SHOULDER TYP. B
- ② EXISTING HMA SHOULDER, (VARIABLE DEPTH)
- ③ EXISTING JOINTED REINFORCED PC CONCRETE PAVEMENT, 10"
- ④ EXISTING CURB & GUTTER B 6.12
- ⑤ EXISTING CURB & GUTTER B 6.24
- ⑥ EXISTING CURB & GUTTER M 2.24
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 1 1/2" (SHOULDER/GORE/ HMA RAMP SECTION)
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2" (SHOULDER)
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 1 3/4" (RAMP)
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2 1/4" (RAMP)
- ⑫ PROPOSED PCC SURFACE REMOVAL (VAR. DEPTH), PER STD. BD-33
- ⑬ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" (GORE/HMA RAMP SECTION)
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VAR. DEPTH (0 TO 2 1/4") (GORE)
- ⑯ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4" (GORE/HMA RAMP SECTION)
- ⑰ PROPOSED PARTIAL DEPTH PATCH, (HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 3")
- ⑱ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; ±5"
- ⑲ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT

FILE NAME =	USER NAME = obreuh	DESIGNED -	REVISED -
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 5/23/2011	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**VARIOUS RAMPS AT VARIOUS DUPAGE COUNTY LOCATIONS
TYPICAL SECTIONS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	33	7
CONTRACT NO. 60P34			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

NOTE:
 *REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

(SHOULDER RESURFACING)
 HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N70, 1 1/2"
 HOT-MIX ASPHALT BINDER COURSE, IL-19, N70, 2 1/2"

PROPOSED GUARDRAIL REMOVAL*
 PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE A
 PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT

RAMP TO S.B. IL RTE. 83

RAMP 'A'

RAMP FROM S.B. IL RTE. 83
 TO W.B. 55TH ST.

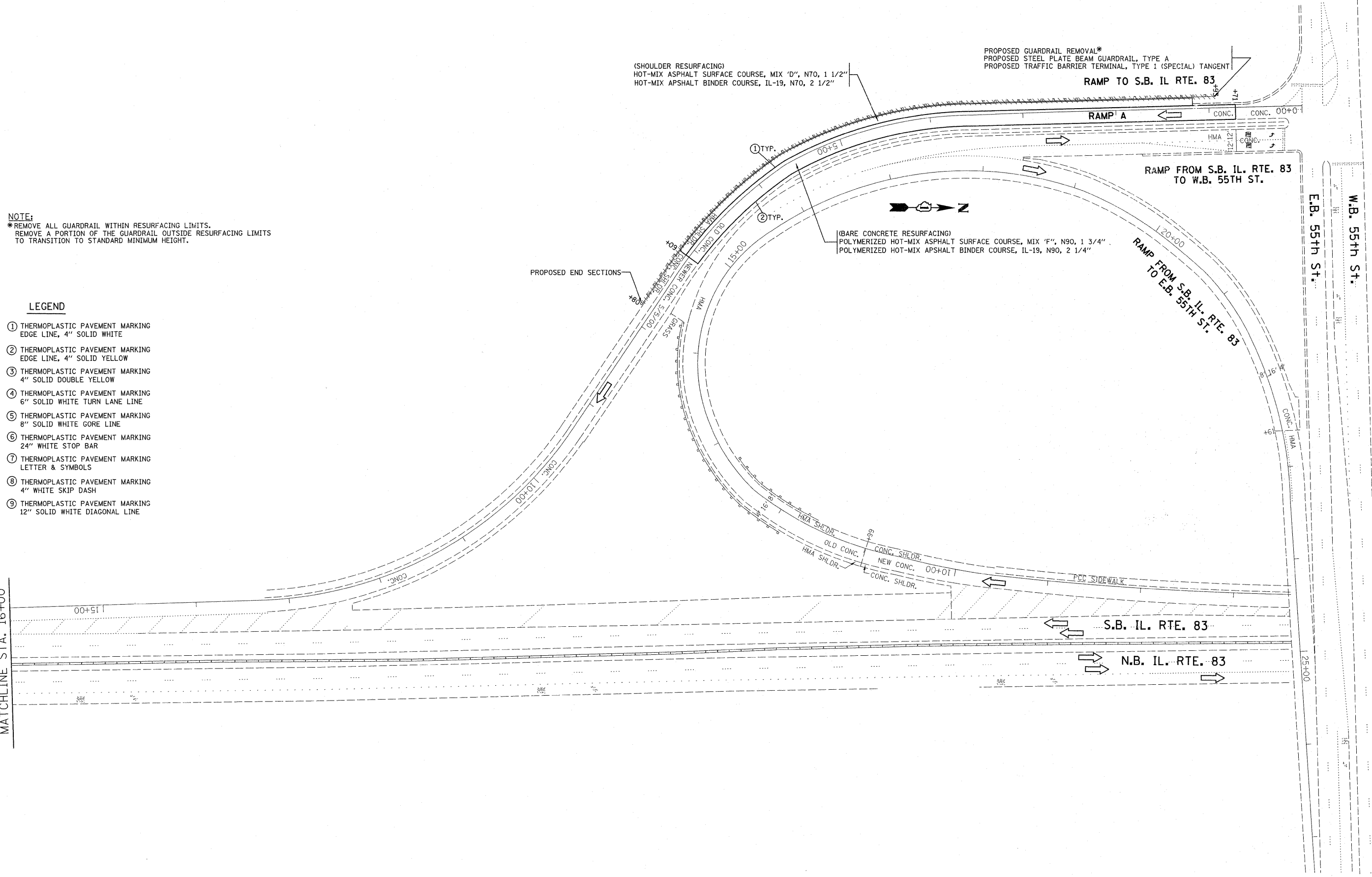
(BARE CONCRETE RESURFACING)
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'F', N90, 1 3/4"
 POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19, N90, 2 1/4"

PROPOSED END SECTIONS

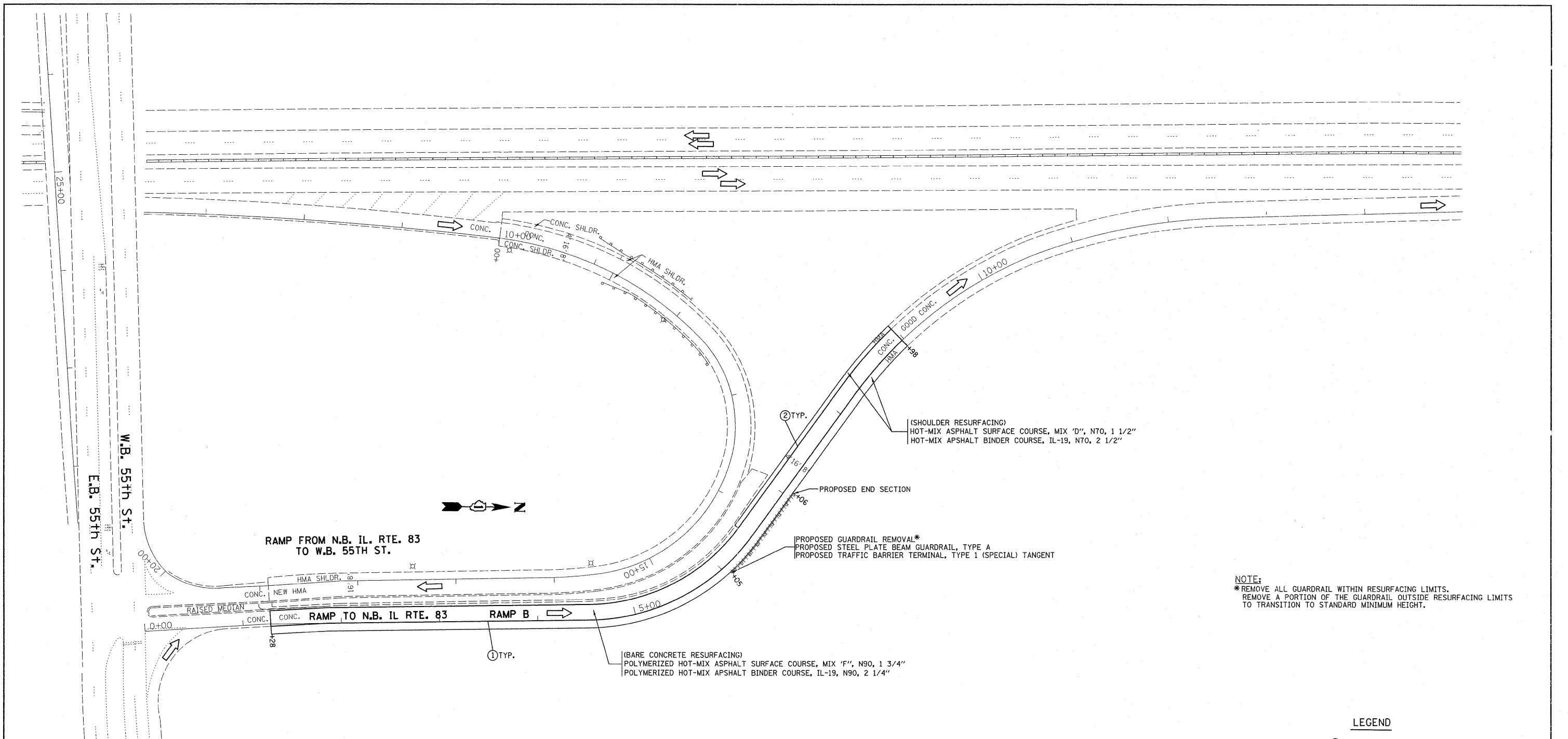


MATCHLINE STA. 16+00

W.B. 55th St.
 E.B. 55th St.



FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS 55th STREET AT IL RTE. 83 (RAMP 'A')				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -									CONTRACT NO. 60P34		
		DATE -	REVISED -									ILLINOIS FED. AID PROJECT		



NOTE:
 *REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

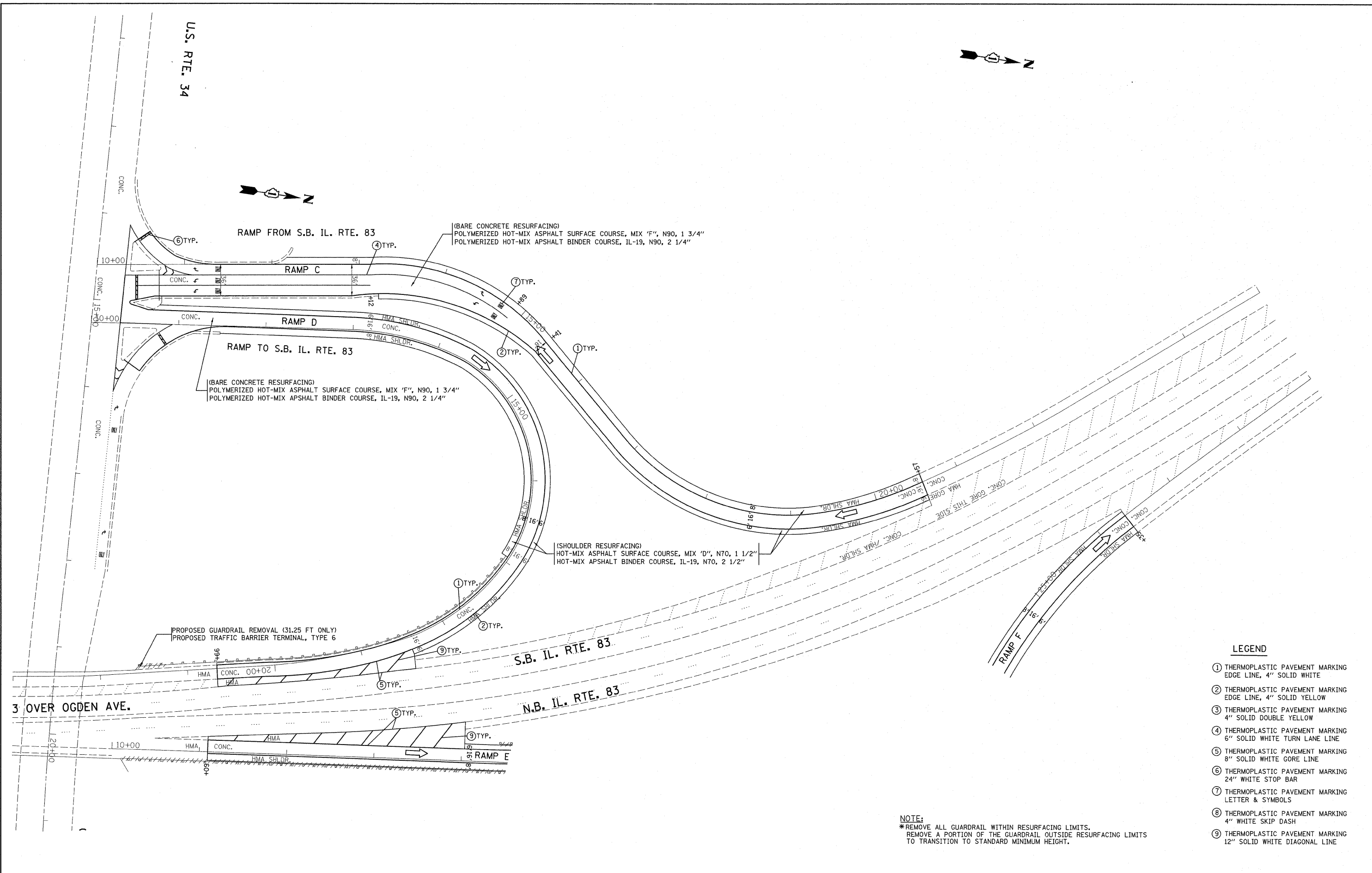
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 5/24/2011	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
 55th STREET AT IL. RTE. 83 (RAMP 'B')**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	33	11
CONTRACT NO. 60P34				
ILLINOIS FED. AID PROJECT				

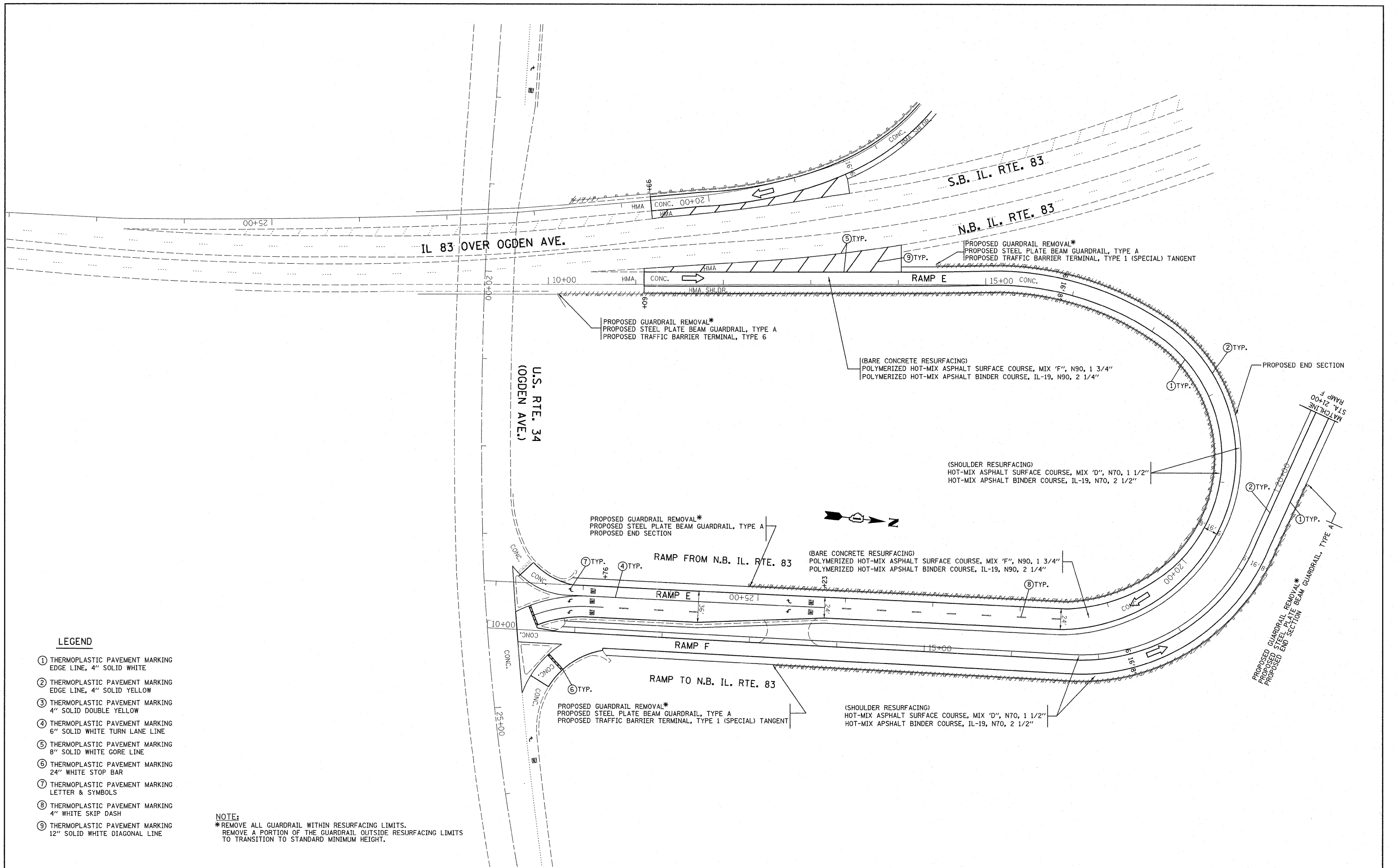


LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

NOTE:
 *REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.

FILE NAME =	USER NAME = abraueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS IL. ROUTE 83 AT US ROUTE 34 (RAMP 'C' & 'D')				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
c:\pwork\pwork\dot\abraueh\0265704\DI519	11-sh-t-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	VAR.	2011-030-I	DUPAGE	37	12
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 60P34										
	PLOT DATE = 5/24/2011	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

NOTE:
 *REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.

FILE NAME =	USER NAME = abreaugh	DESIGNED -	REVISED -
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 5/24/2011		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

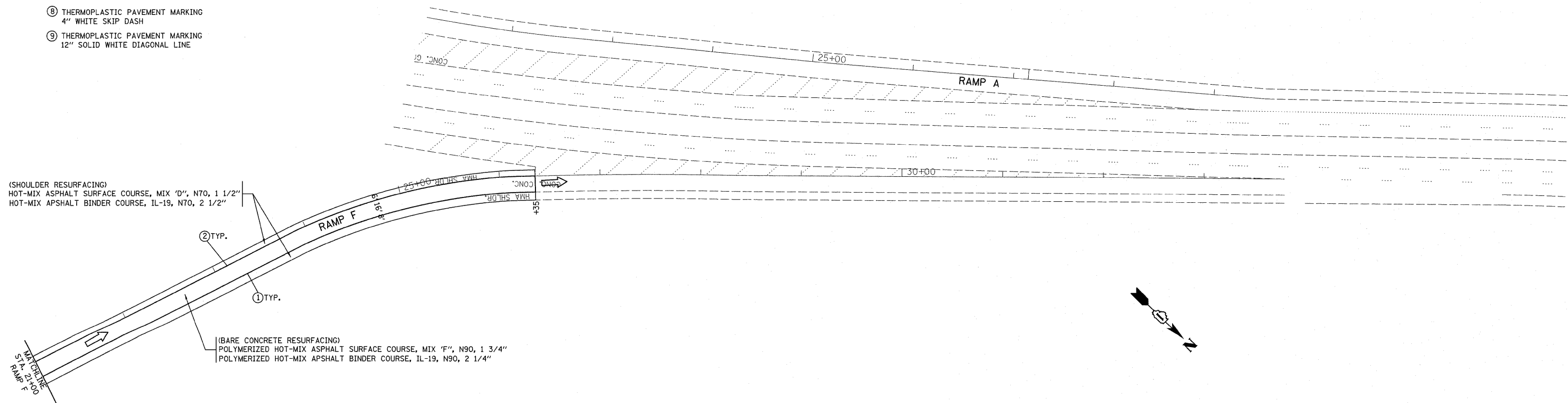
**ROADWAY AND PAVEMENT MARKING PLANS
 IL. ROUTE 83 AT US ROUTE 34 (RAMP 'E' & 'F')**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	38	13
CONTRACT NO. 60P34				
ILLINOIS FED. AID PROJECT				

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE



FILE NAME =	USER NAME = obreuh	DESIGNED -	REVISED -
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	PLOT DATE = 5/24/2011	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
IL. ROUTE 83 AT US ROUTE 34 (RAMP 'F')**

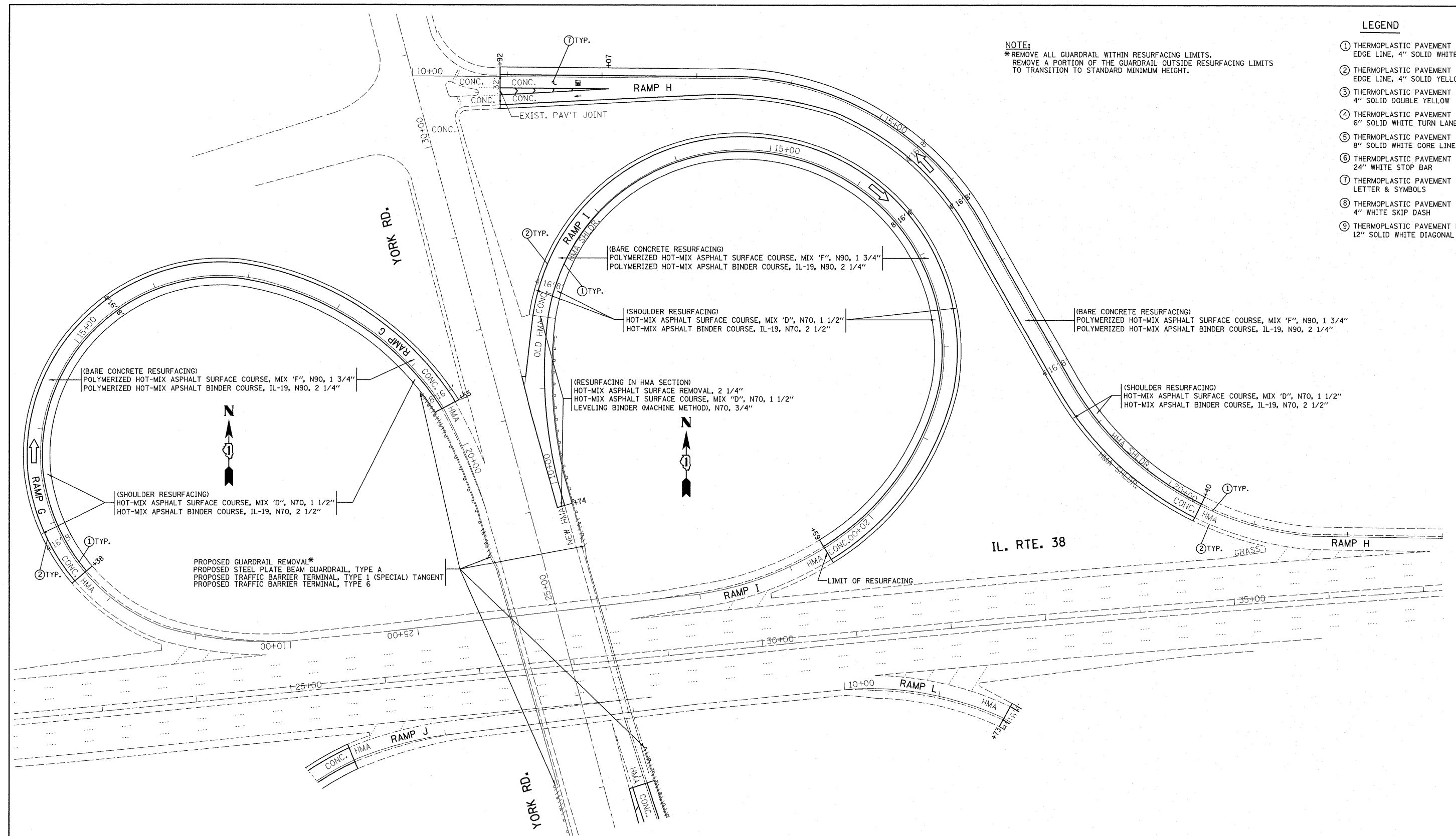
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	33	14
CONTRACT NO. 60P34			ILLINOIS FED. AID PROJECT	

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

NOTE:
 * REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.



PROPOSED GUARDRAIL REMOVAL*
 PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE A
 PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT
 PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 6

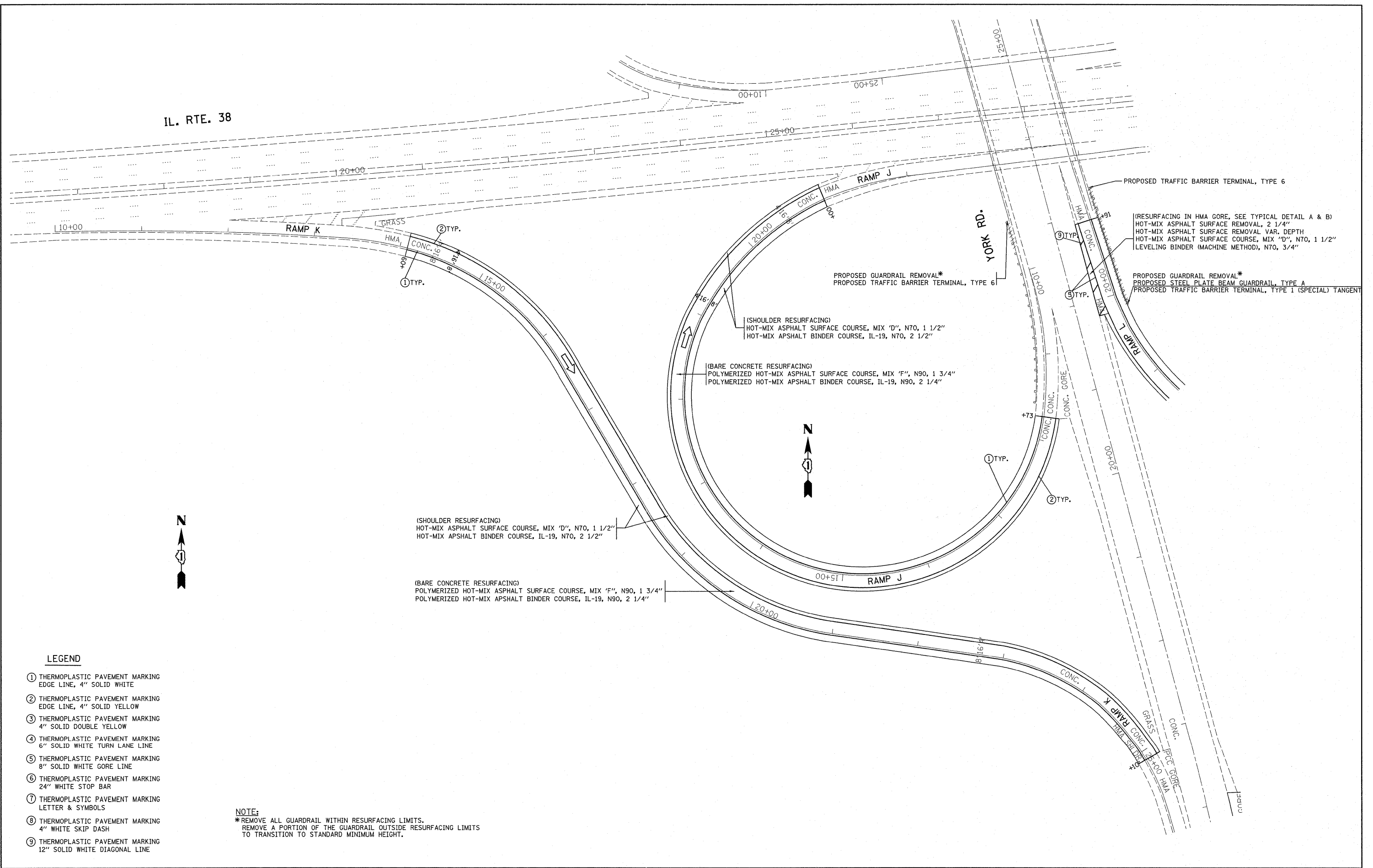
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
 IL. ROUTE 38 AT YORK RD. (RAMP G, H, & I)**

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PLOT DATE = 5/24/2011	DATE -	REVISED -	REVISED -

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	DUPAGE	33	15
CONTRACT NO. 60P34				
ILLINOIS FED. AID PROJECT				



IL. RTE. 38

RAMP K

RAMP J

YORK RD.

PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 6

(RESURFACING IN HMA GORE, SEE TYPICAL DETAIL A & B)
HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
HOT-MIX ASPHALT SURFACE REMOVAL VAR. DEPTH
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
LEVELING BINDER (MACHINE METHOD), N70, 3/4"

PROPOSED GUARDRAIL REMOVAL*
PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 6

PROPOSED GUARDRAIL REMOVAL*
PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE A
PROPOSED TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT

(SHOULDER RESURFACING)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70, 2 1/2"

(BARE CONCRETE RESURFACING)
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19, N90, 2 1/4"

(SHOULDER RESURFACING)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70, 2 1/2"

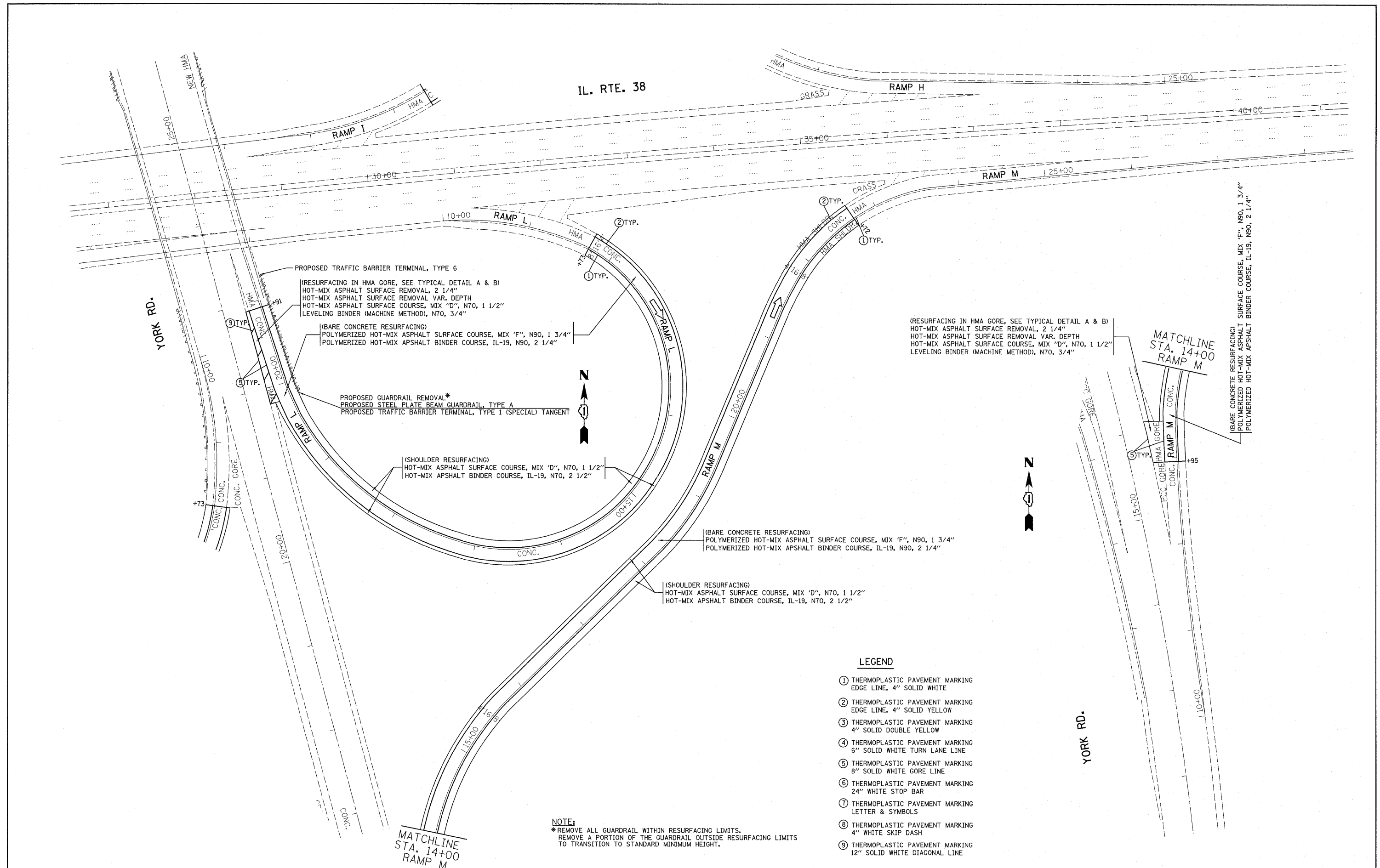
(BARE CONCRETE RESURFACING)
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19, N90, 2 1/4"

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

NOTE:
* REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
TO TRANSITION TO STANDARD MINIMUM HEIGHT.

FILE NAME =	USER NAME = abraueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS IL. ROUTE 38 AT YORK RD. (RAMP J & K)				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\abraueh\d0265704\01511	11-shr-plan.dgn	DRAWN -	REVISED -		SCALE: 1"=50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	DUPAGE	33	16
		CHECKED -	REVISED -		CONTRACT NO. 60P34								
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

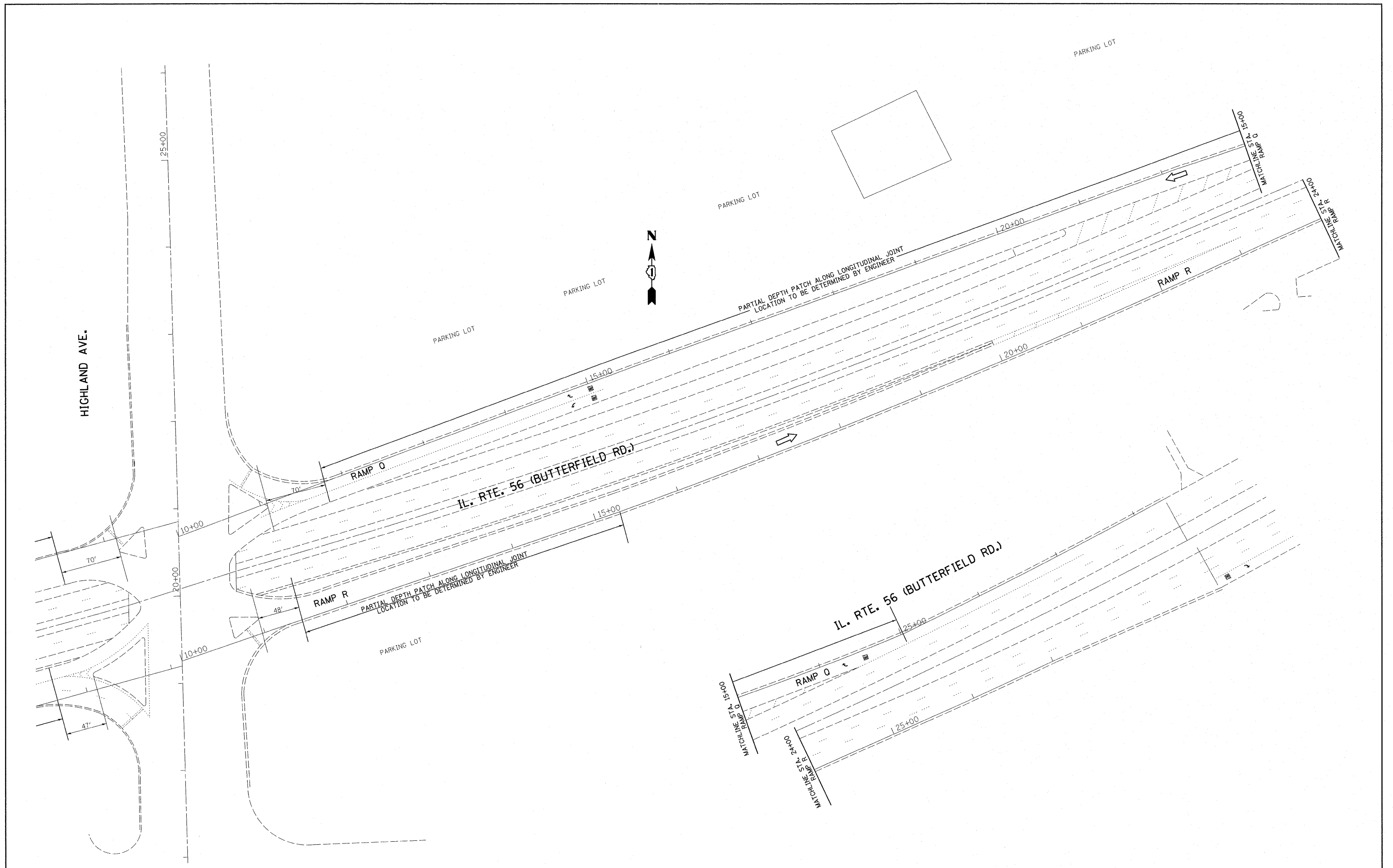


NOTE:
 *REMOVE ALL GUARDRAIL WITHIN RESURFACING LIMITS.
 REMOVE A PORTION OF THE GUARDRAIL OUTSIDE RESURFACING LIMITS
 TO TRANSITION TO STANDARD MINIMUM HEIGHT.

LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

FILE NAME =	USER NAME = ebraueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL. ROUTE 38 AT YORK RD. (RAMP L & M)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
c:\p\work\p1\dot\ebraueh\d0265704\DI51511-sht-plan.dgn		DRAWN -	REVISED -		SCALE: 1"=50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	VAR.	2011-030-I	DUPAGE	33	17
		PLOT SCALE = 50.0000' / IN.	REVISED -												
		PLOT DATE = 5/24/2011	REVISED -												
											CONTRACT NO. 60P34		ILLINOIS FED. AID PROJECT		



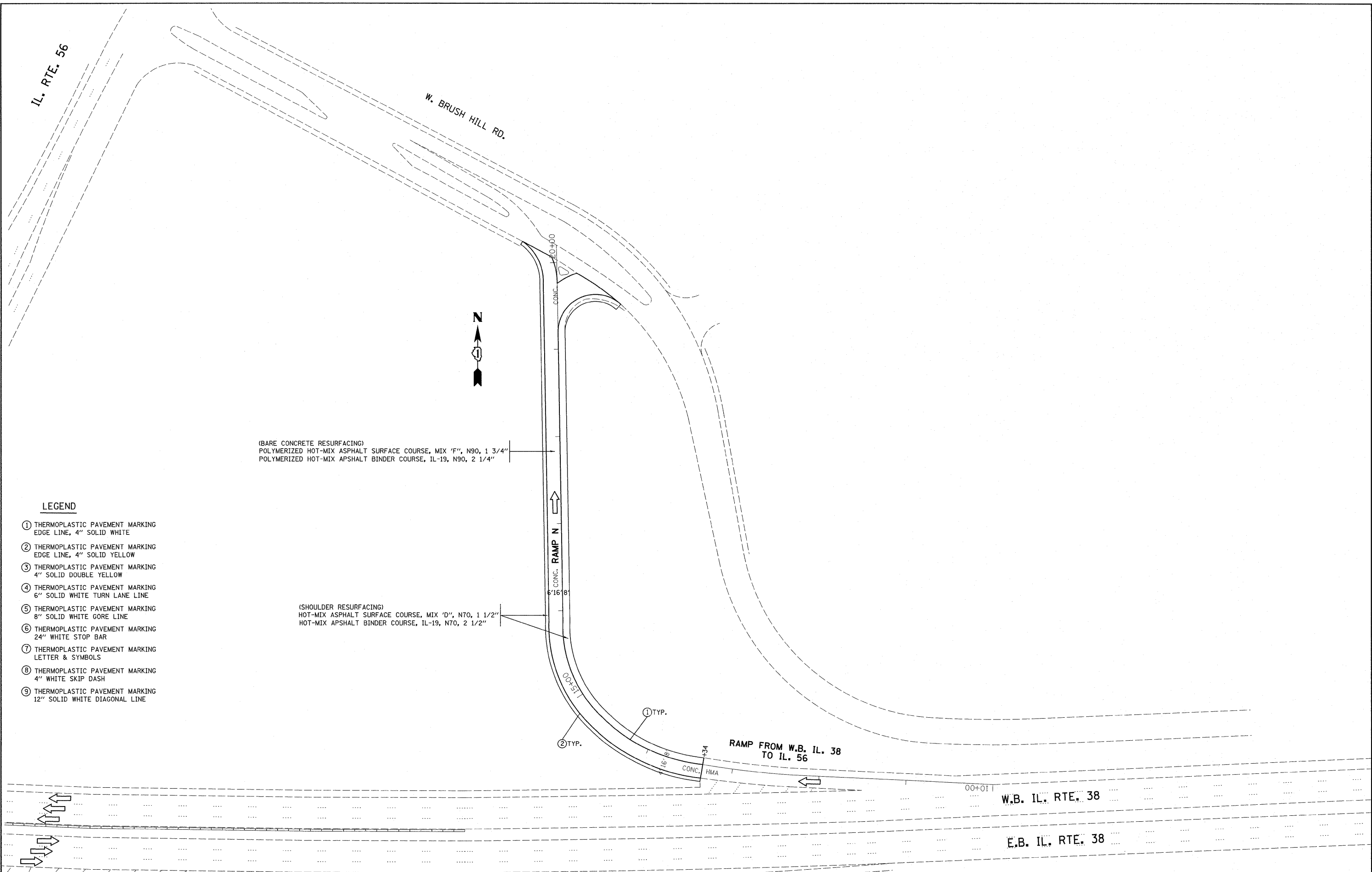
FILE NAME =	USER NAME = abrauh	DESIGNED -	REVISED -
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 5/24/2011		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLANS
IL. ROUTE 56 AT HIGHLAND AVE. (RAMP 'Q' & 'R')**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-1	DUPAGE	33	19
CONTRACT NO. 60P34				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.



(BARE CONCRETE RESURFACING)
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'F', N90, 1 3/4"
 POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19, N90, 2 1/4"

(SHOULDER RESURFACING)
 HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N70, 1 1/2"
 HOT-MIX ASPHALT BINDER COURSE, IL-19, N70, 2 1/2"

LEGEND

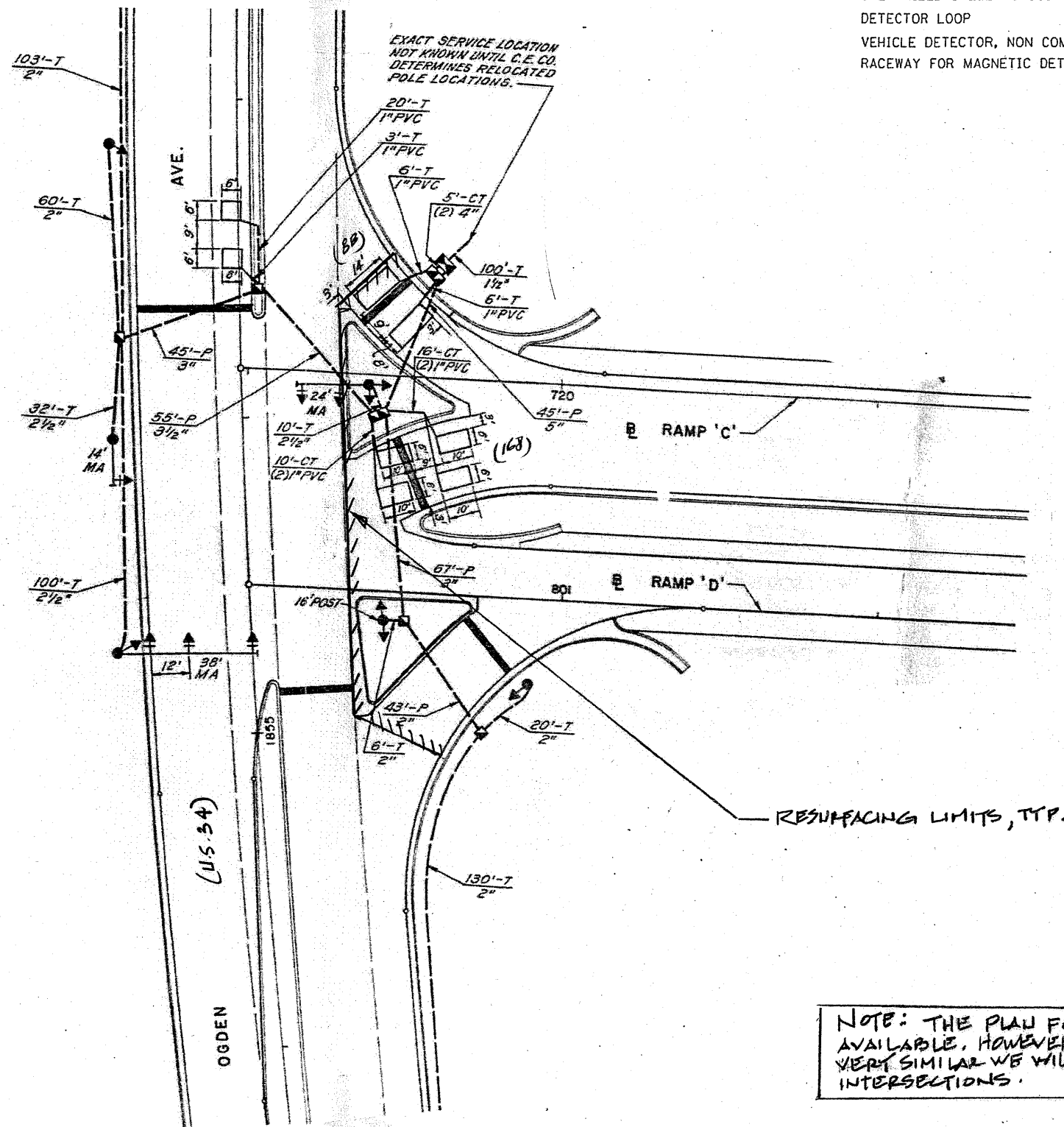
- ① THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING
EDGE LINE, 4" SOLID YELLOW
- ③ THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE TURN LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE GORE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING
LETTER & SYMBOLS
- ⑧ THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP DASH
- ⑨ THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONAL LINE

FILE NAME =	USER NAME = obreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS W.B. IL. RTE. 38 TO IL. RTE. 56 (RAMP 'N')				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pwork\obreuh\d0265704\01518\11-shit-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	DUPAGE	33	20
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 60P34								
	PLOT DATE = 5/24/2011	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	⊕	⊕
SIGNAL HEAD	→	→
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	—	—
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—	—



NOTE: THE PLAN FOR THE WEST RAMPS IS NOT AVAILABLE. HOWEVER, SINCE THE EAST RAMPS ARE VERY SIMILAR WE WILL USE THIS PLAN FOR BOTH INTERSECTIONS.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	256	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = nguyensm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT U.S. RTE. 34 @ ILL. 93 RAMPS C&D, E&F	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pr_work\pwidot\nguyensm\20112618\112618.dgn		DRAWN -	REVISED -			VAR	2011-030-I	DUPAGE	33	21
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		CONTRACT NO. 60P34
PLOT DATE = 12/1/2010		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

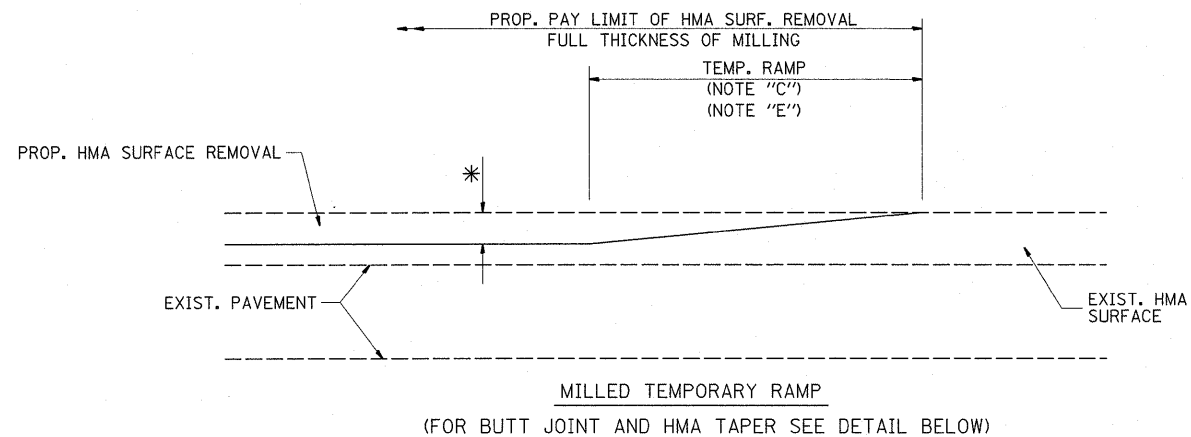
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

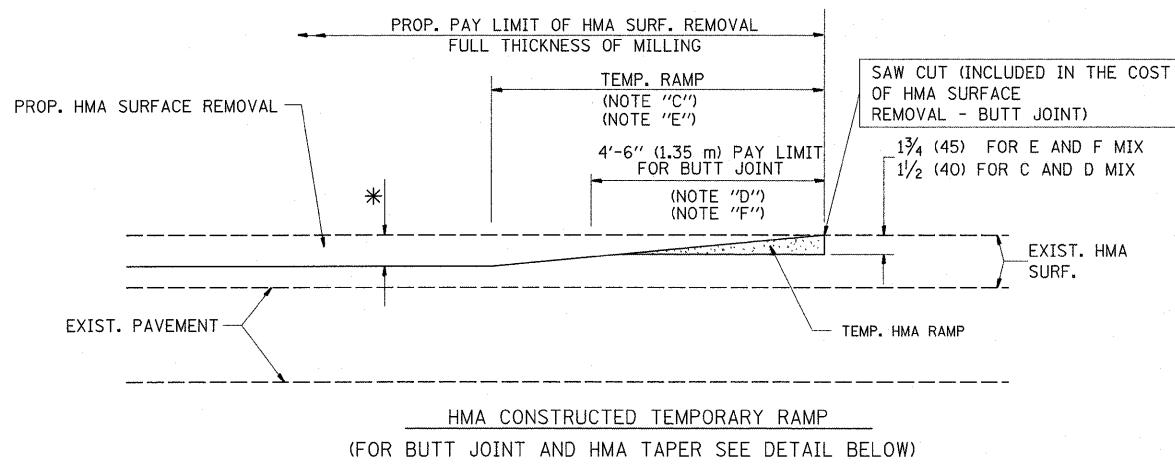
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abreuah	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED - M. GOMEZ 01-22-01					BD600-06 (BD-24)		CONTRACT NO. 60P34			
		DATE - 03-11-94	REVISED - R. BORO 12-15-09					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

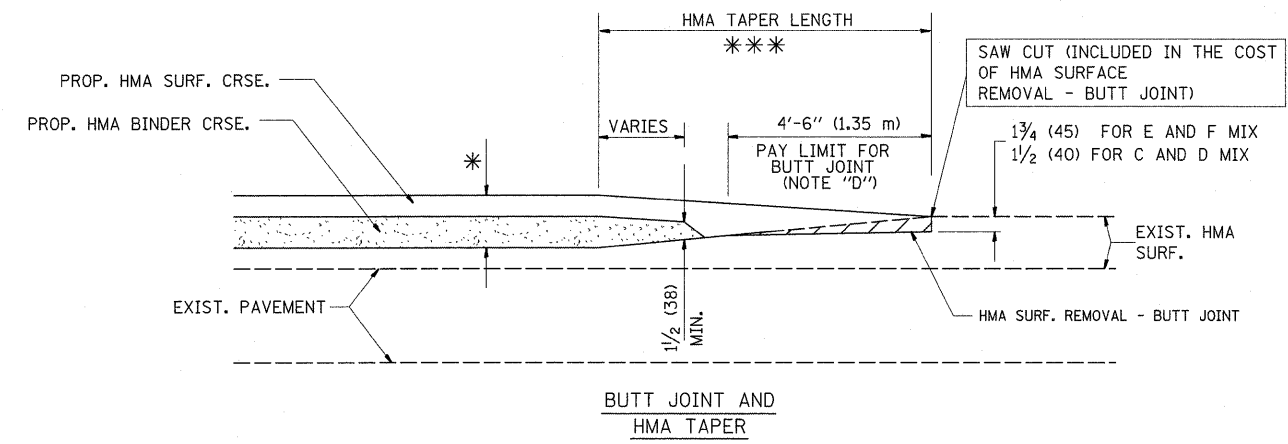


OPTION 1

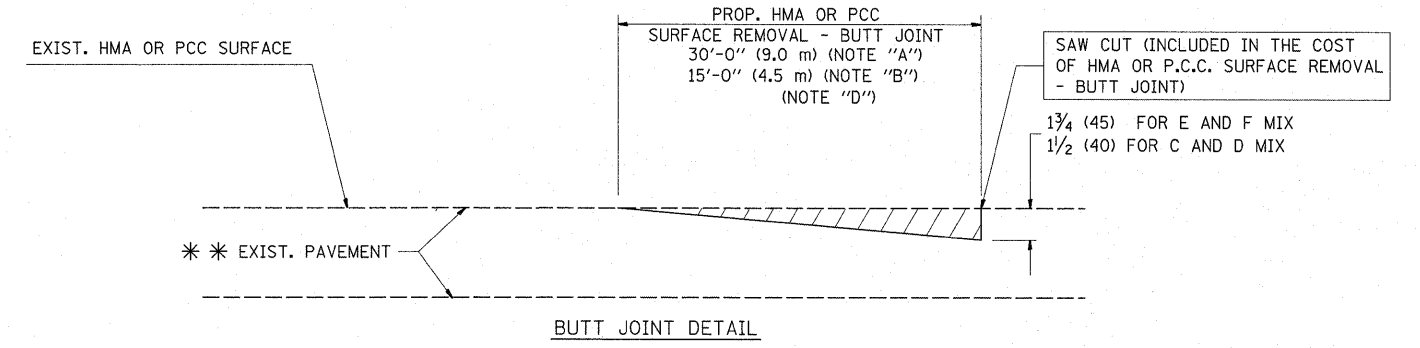


OPTION 2

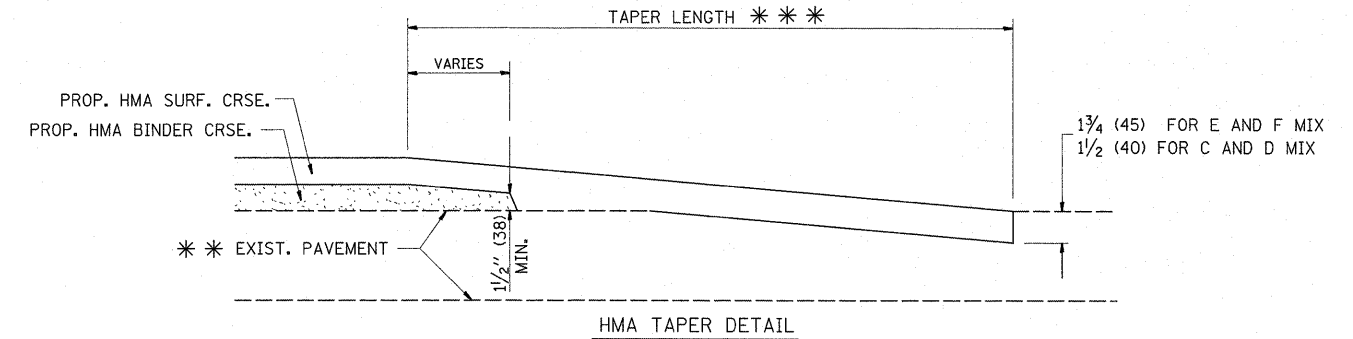
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

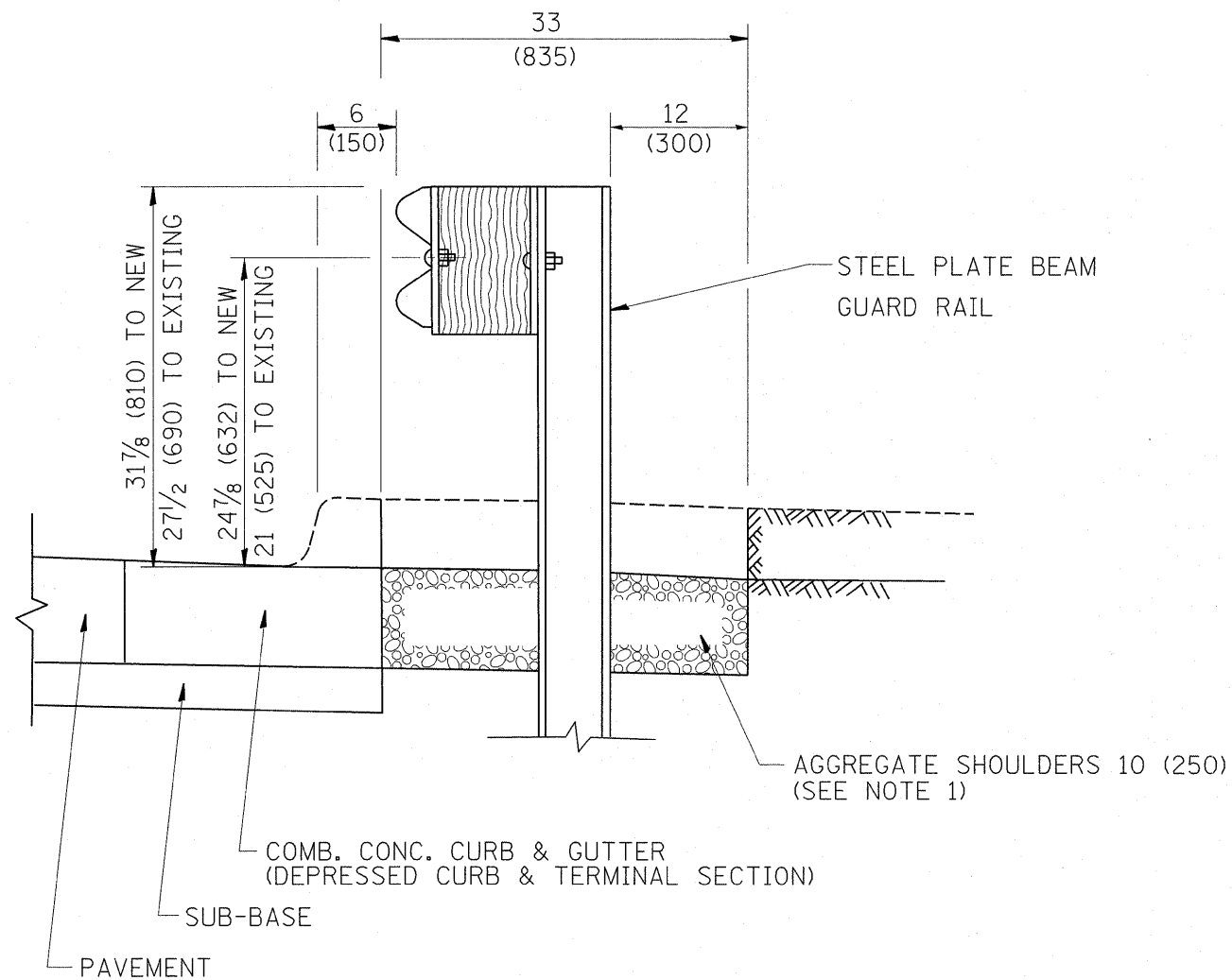
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

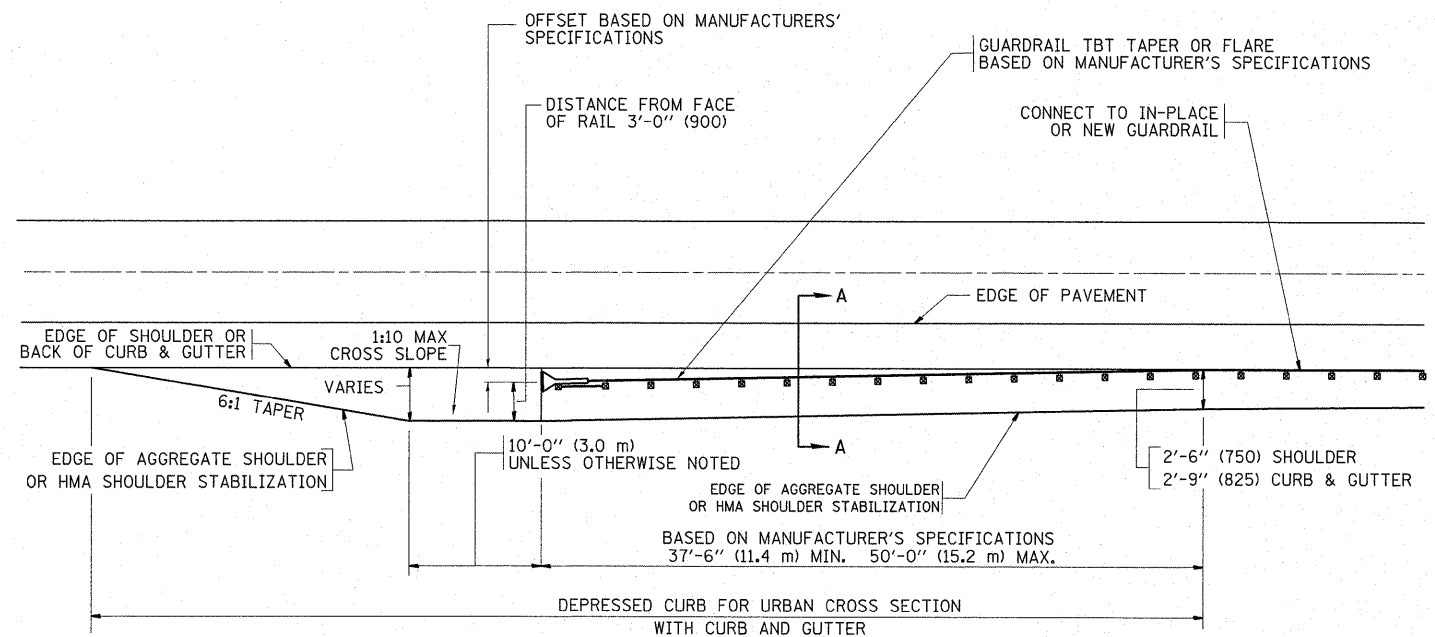
FILE NAME =	USER NAME = ebreuh	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\p\dot\abreuh\d0265784\01st\dtd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		VAR.	2011-030-I	VARIOUS	33	23			
PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01		BD400-05 BD32			CONTRACT NO. 60P34				
PLOT DATE = 5/18/2011		DATE - 06-13-90	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



SECTION A-A

- NOTES: 1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

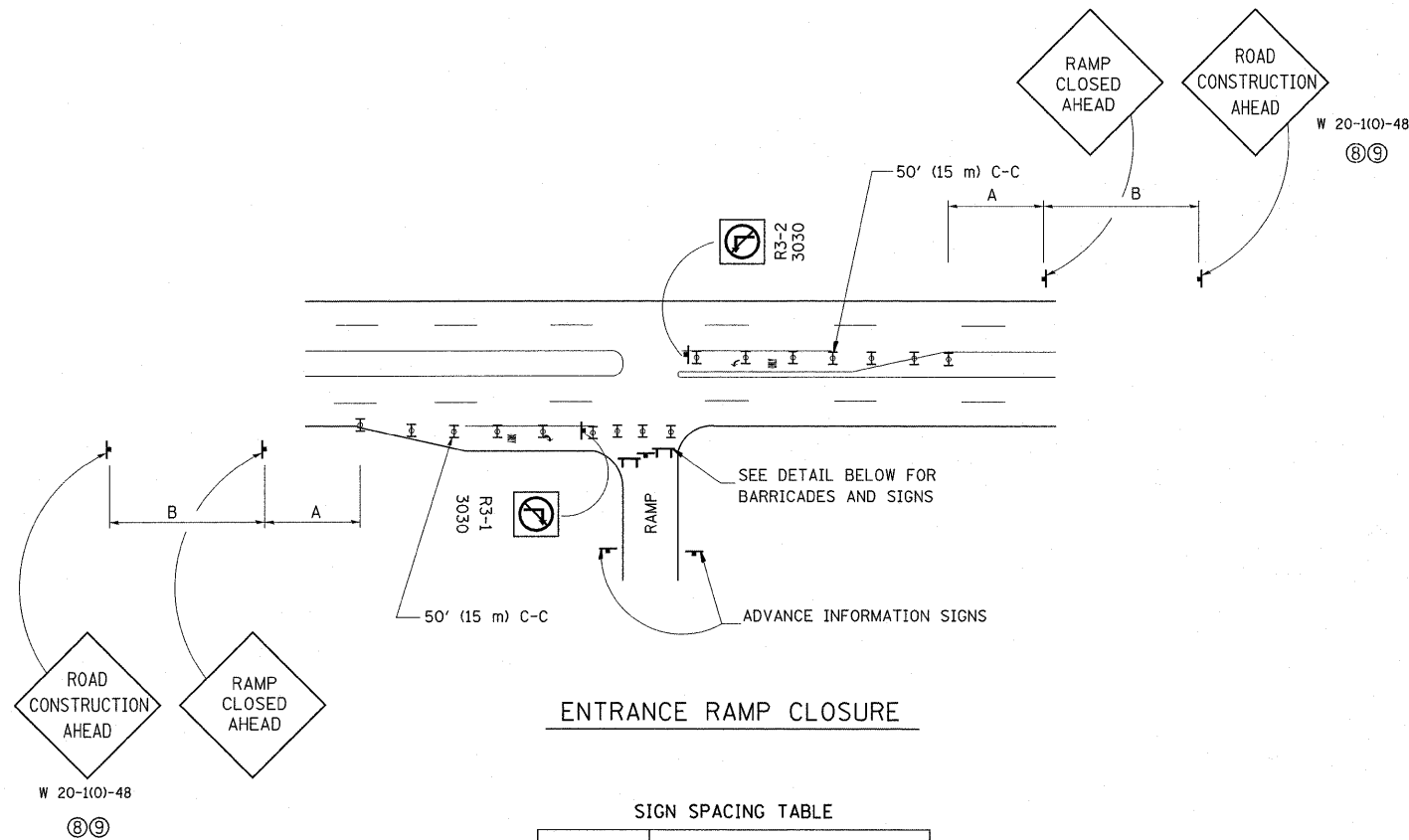
FILE NAME =	USER NAME = abreuah	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00
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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 5/10/2011	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY 1 SPL.**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	VARIOUS	33	25
BD600-10 (BD 34)			CONTRACT NO. 60P34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

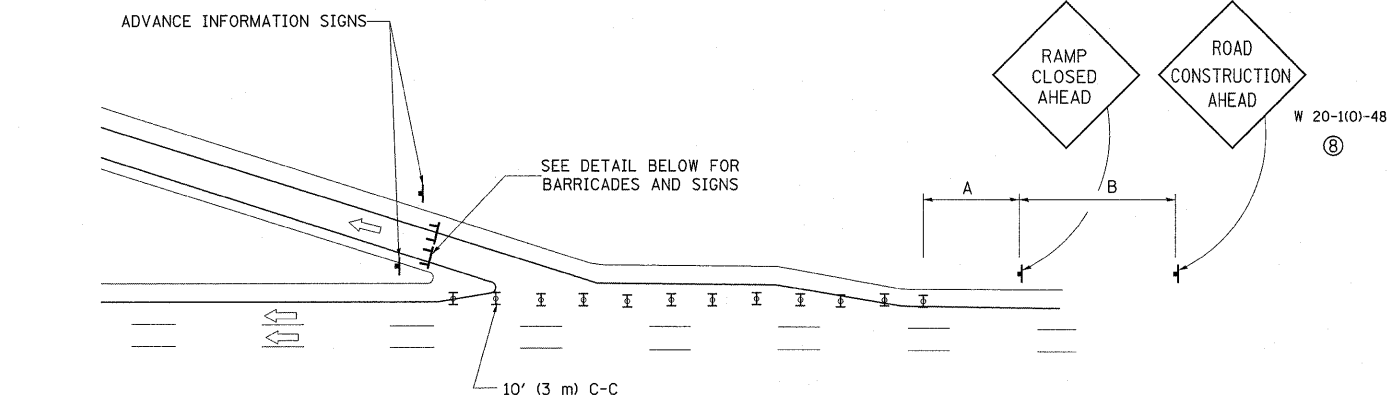


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

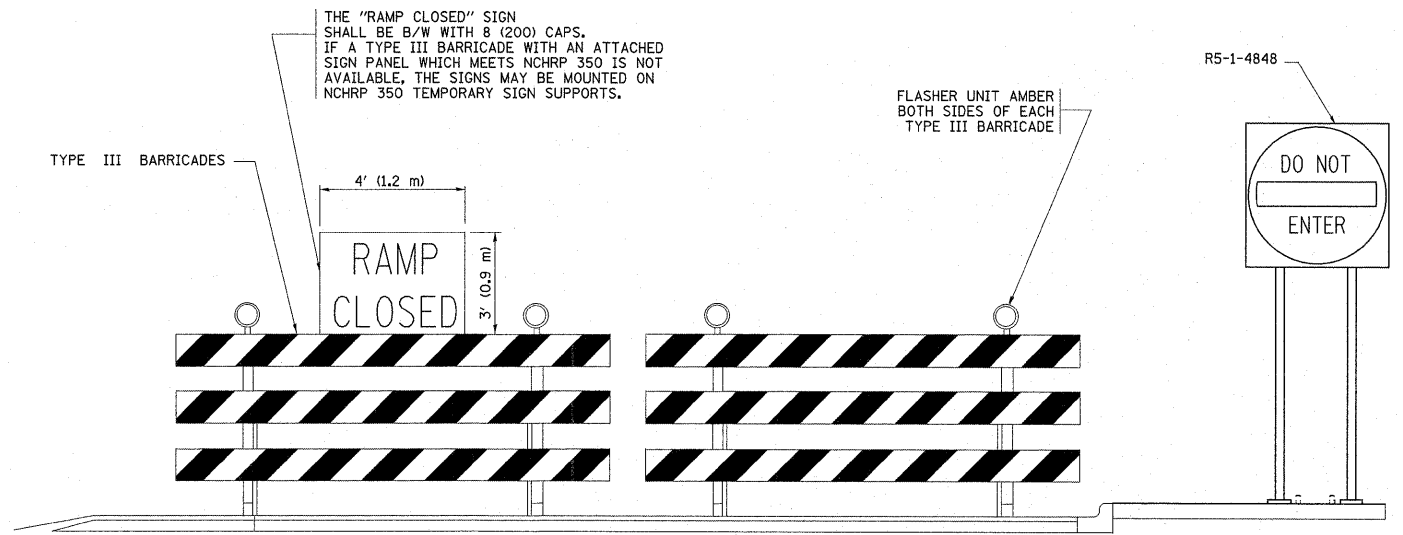
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



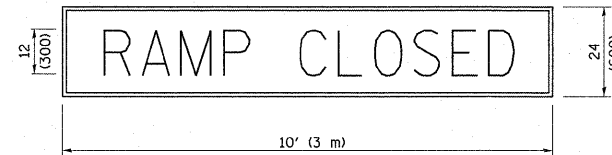
EXIT RAMP CLOSURE

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⌈ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS

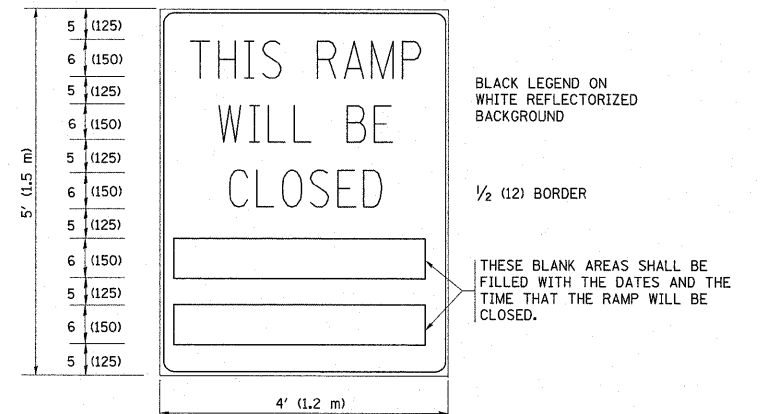
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND

1/2 (12) BORDER

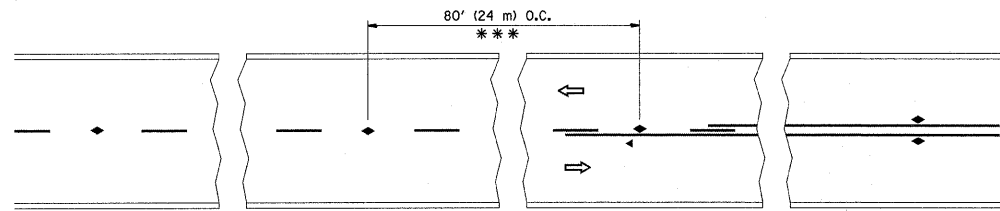
THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

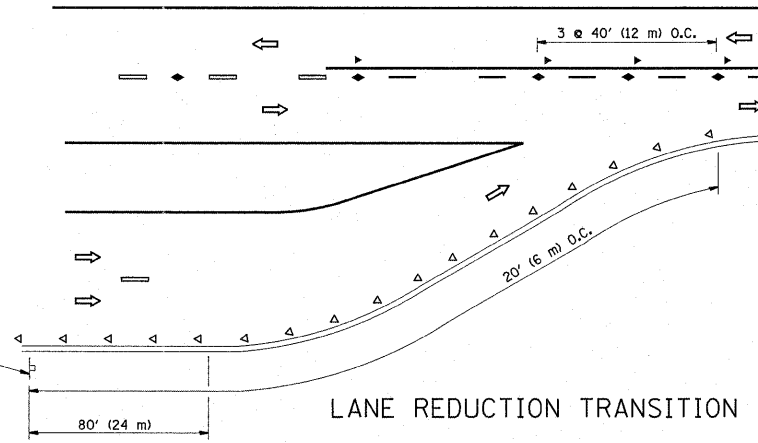
- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

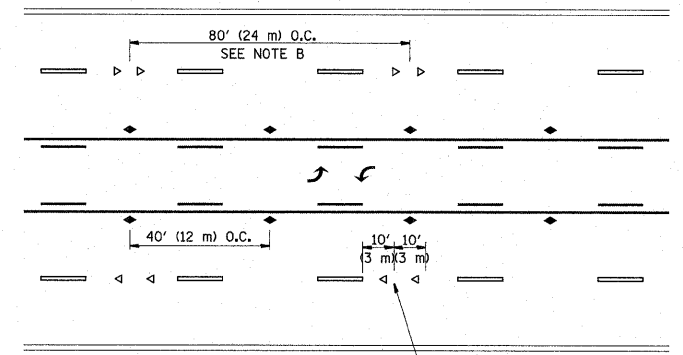


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

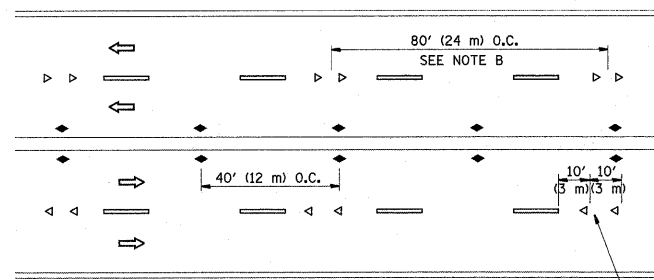
TWO-LANE/TWO-WAY



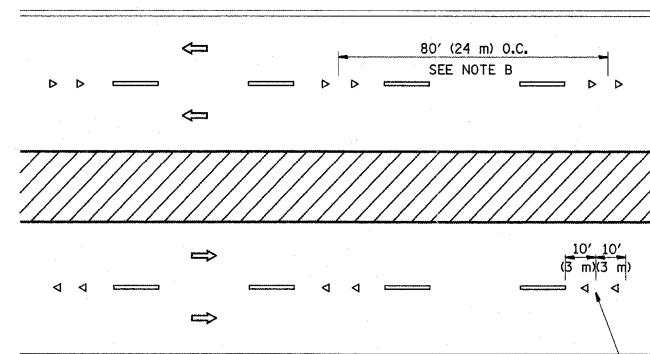
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

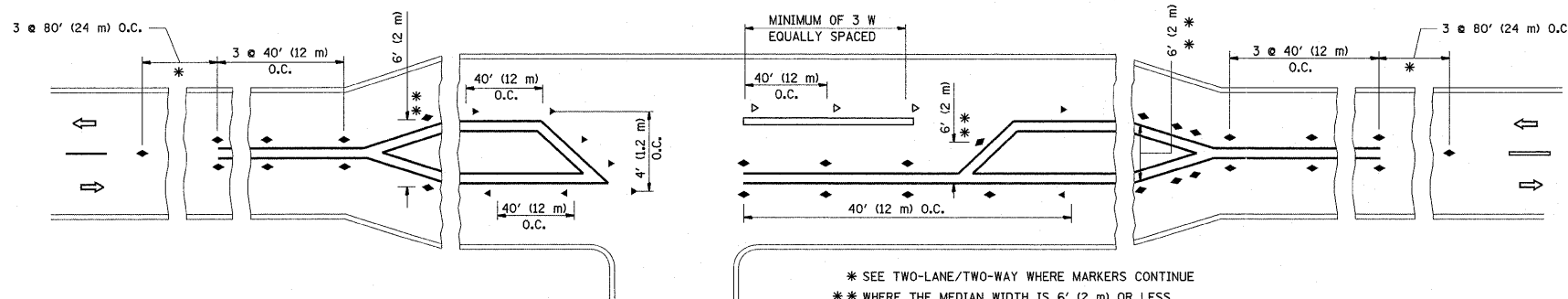
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

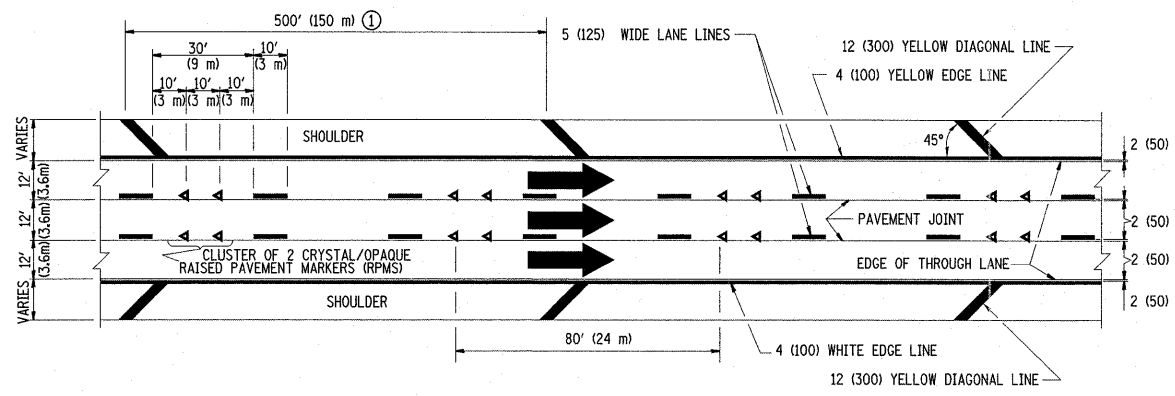
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

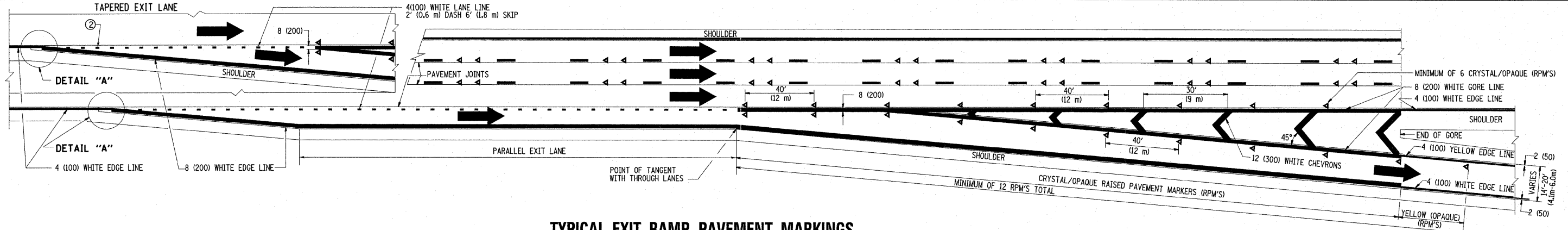
FILE NAME =	USER NAME = abreueh	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\p\dot\abreueh\d0265704\Dist\td.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2011-030-I	VARIOUS	33	27
		CHECKED -	REVISED - T. RAMMACHER 01-06-00						TC-11		CONTRACT NO. 60P34	
		DATE -	REVISED - C. JUCIUS 09-09-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



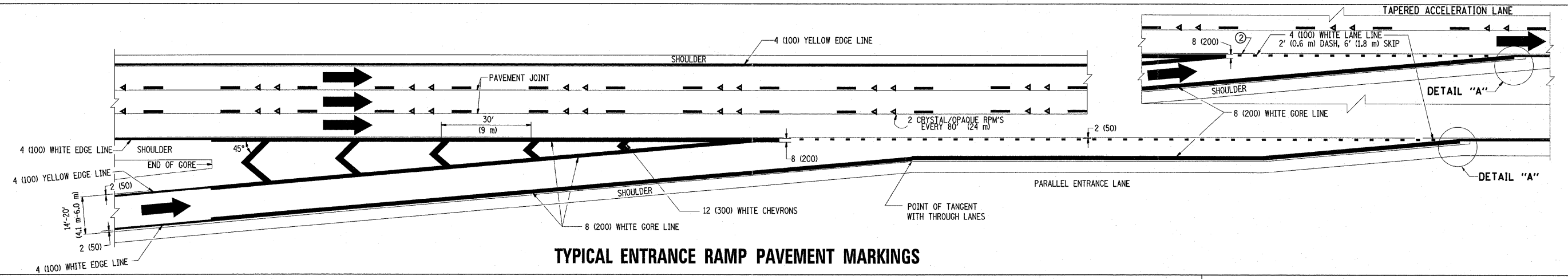
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

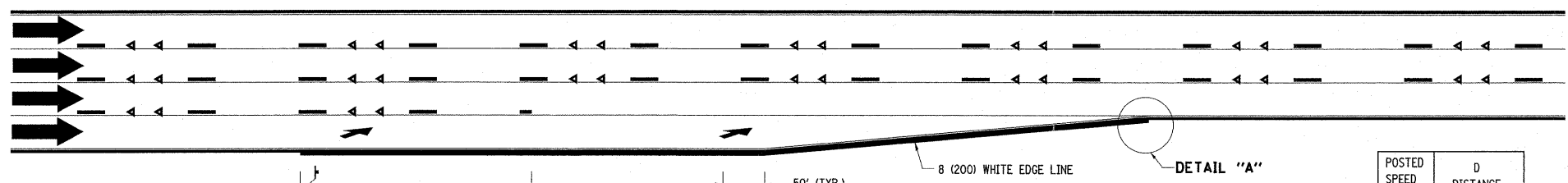
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



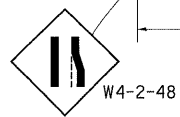
TYPICAL EXIT RAMP PAVEMENT MARKINGS



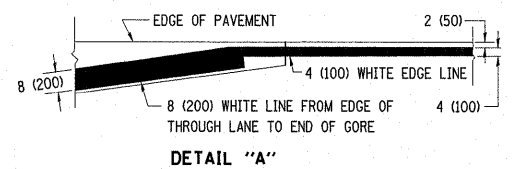
TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



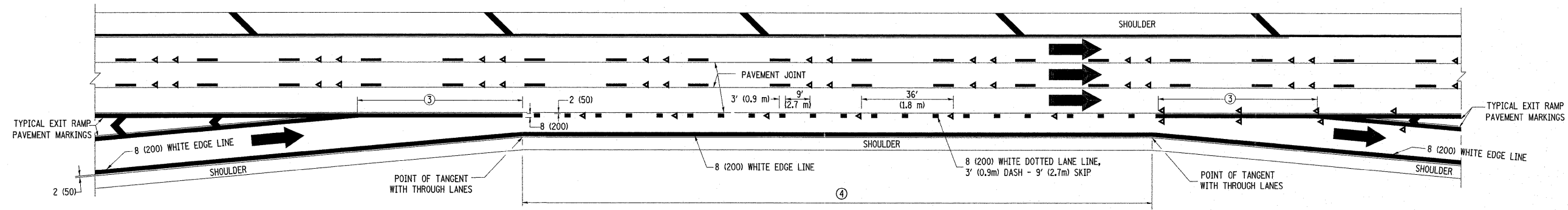
LANE REDUCTION PAVEMENT MARKINGS



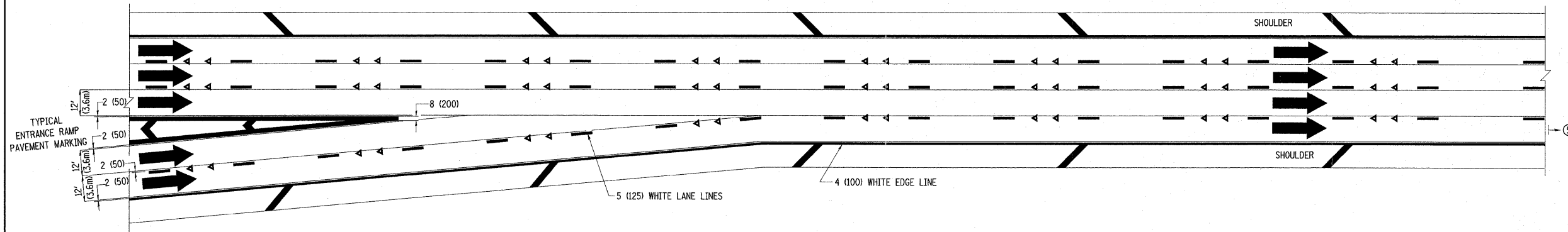
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



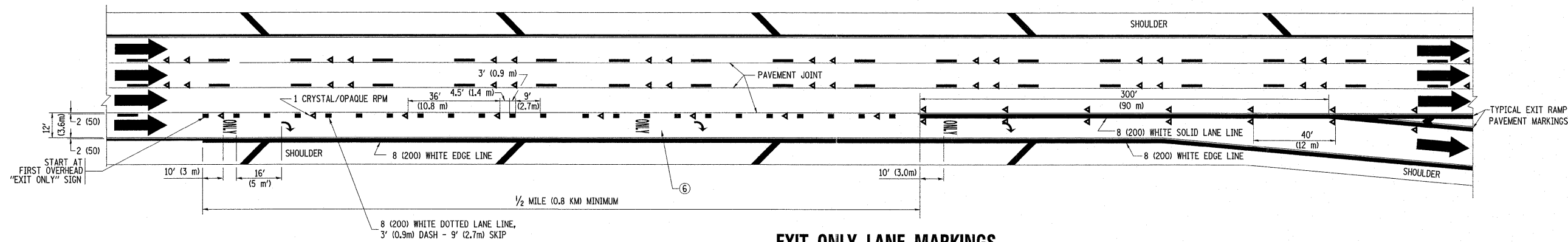
- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



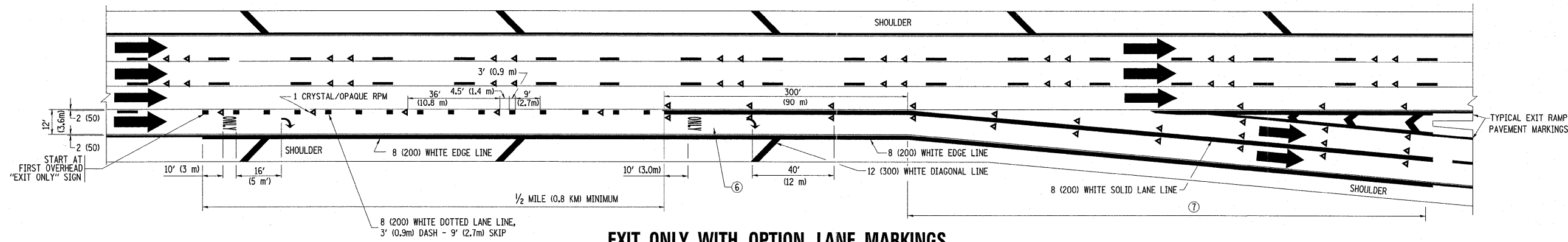
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



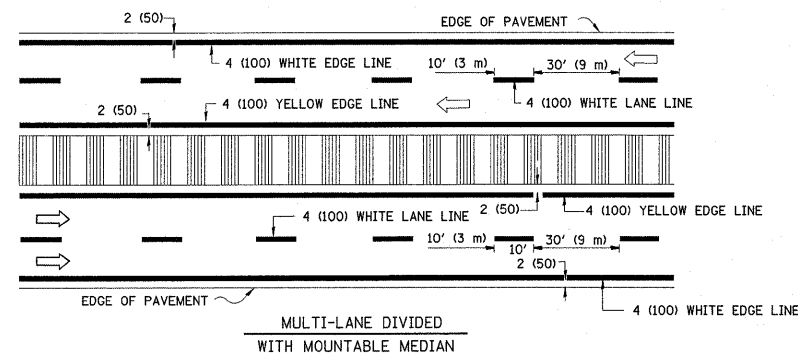
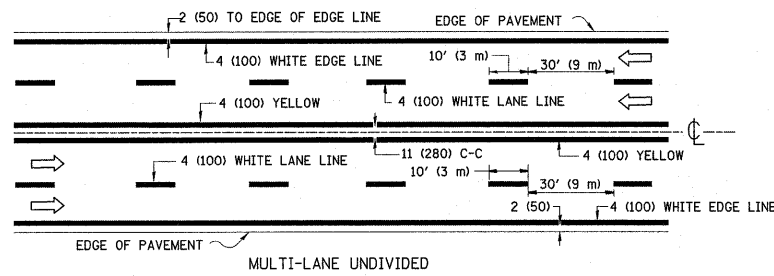
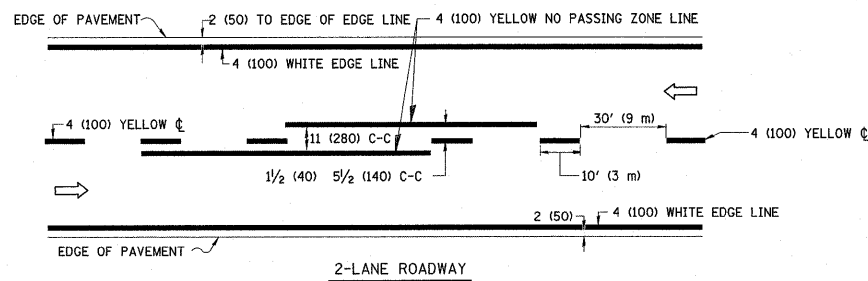
EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

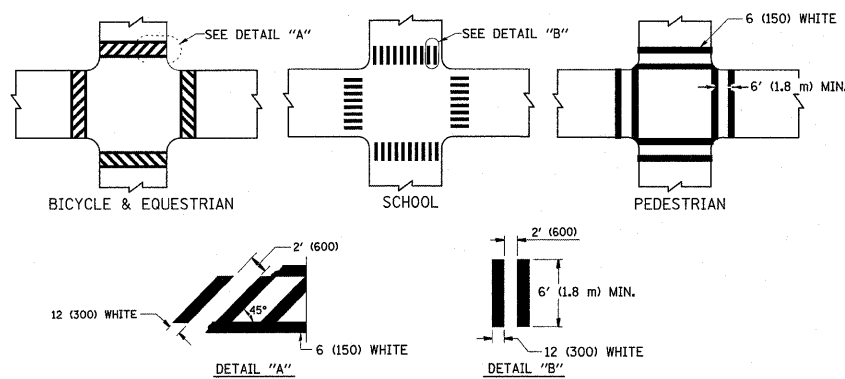
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = abreuh	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
01\pw_work\pilot\abreuh\d0265704\Dist	td.dgn	DRAWN -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	VAR.	2011-030-1	VARIOUS	33	29
		PLOT SCALE = 50,000 ' / IN.	CHECKED - S.P.B. 01-07					TC-12			CONTRACT NO. 60P34		
		PLOT DATE = 5/10/2011	DATE - 01-90		REVISED - S.P.B. 01-10				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

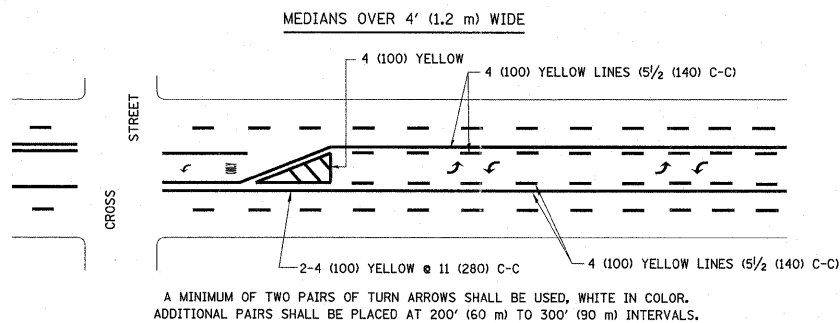
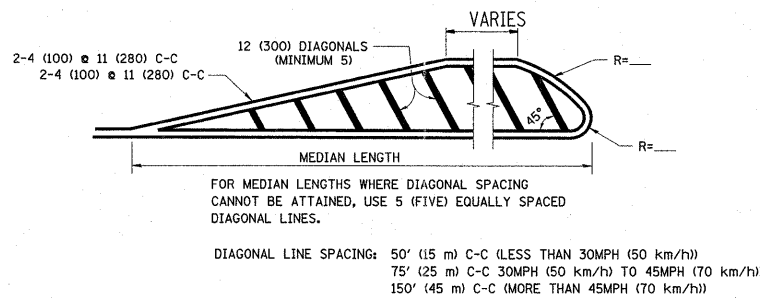
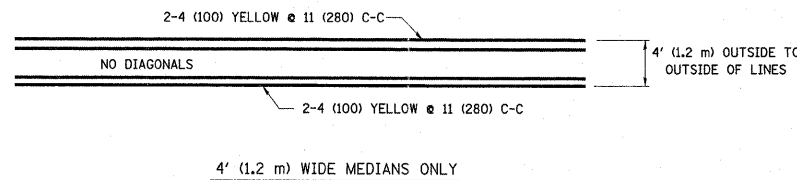


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

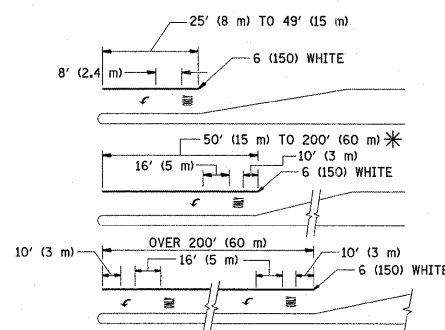
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



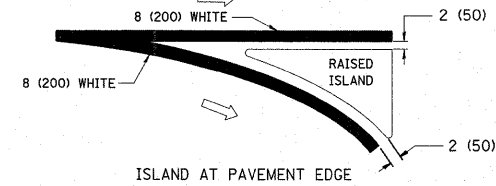
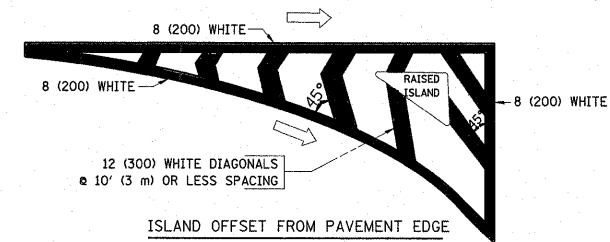
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

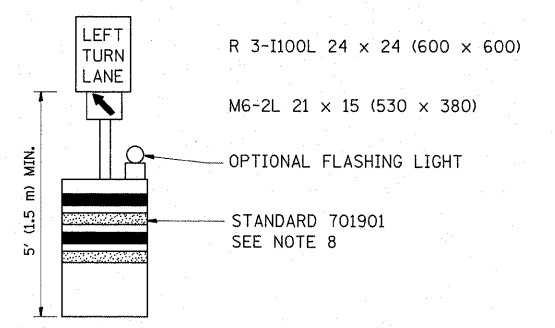
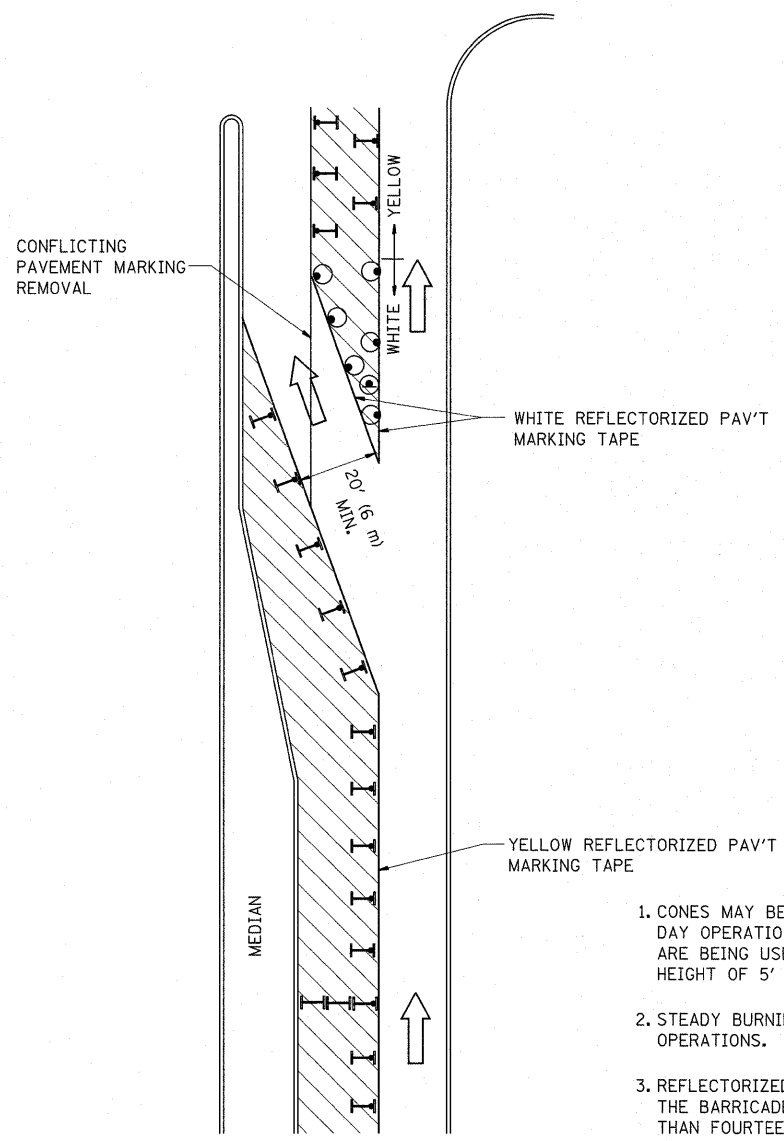
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PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISOR -	REVISOR -
PLOT DATE = 5/10/2011	DATE - 03-19-90	REVISED -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. VAR.	SECTION 2011-030-I	COUNTY VARIOUS	TOTAL SHEETS 33	SHEET NO. 30
TC-13			CONTRACT NO. 60P34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

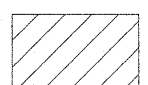
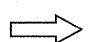






GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

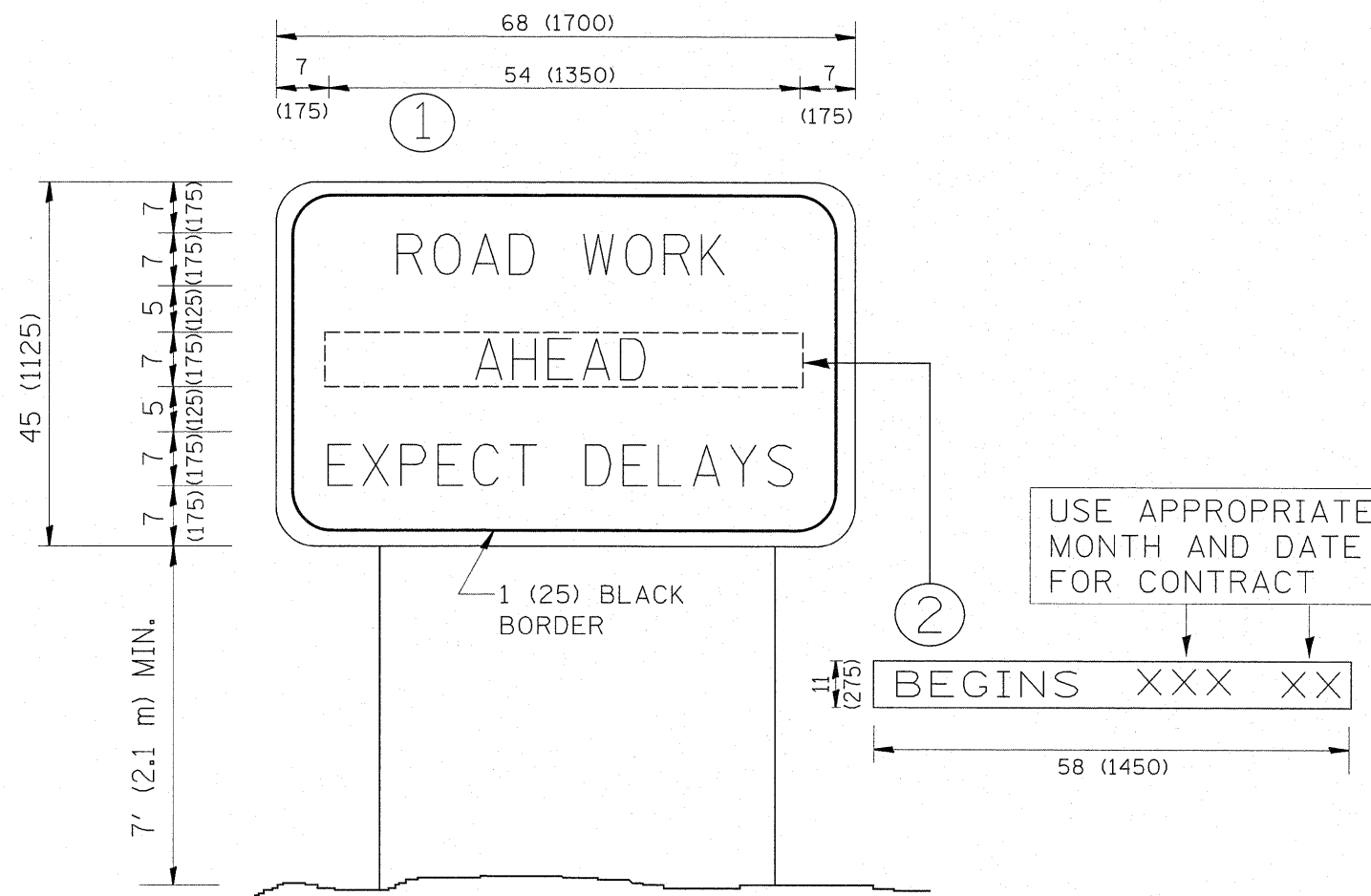
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PLOT SCALE = 50,0000 ' / IN.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 5/10/2011		REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-I	VARIOUS	33	31
TC-14		CONTRACT NO. 60P34		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

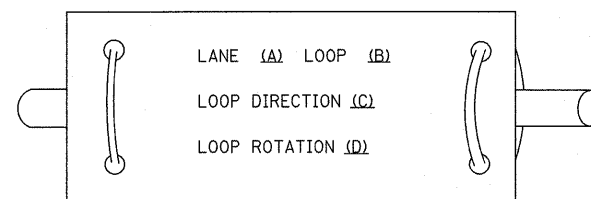
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FILE NAME =	USER NAME = ebreueh	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A. RTE. *	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw_work\pwsdot\ebreueh\d0265704\Dist\td.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	VAR.			2011-030-I	VARIOUS	33	32		
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60P34					
PLOT DATE = 5/10/2011	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		

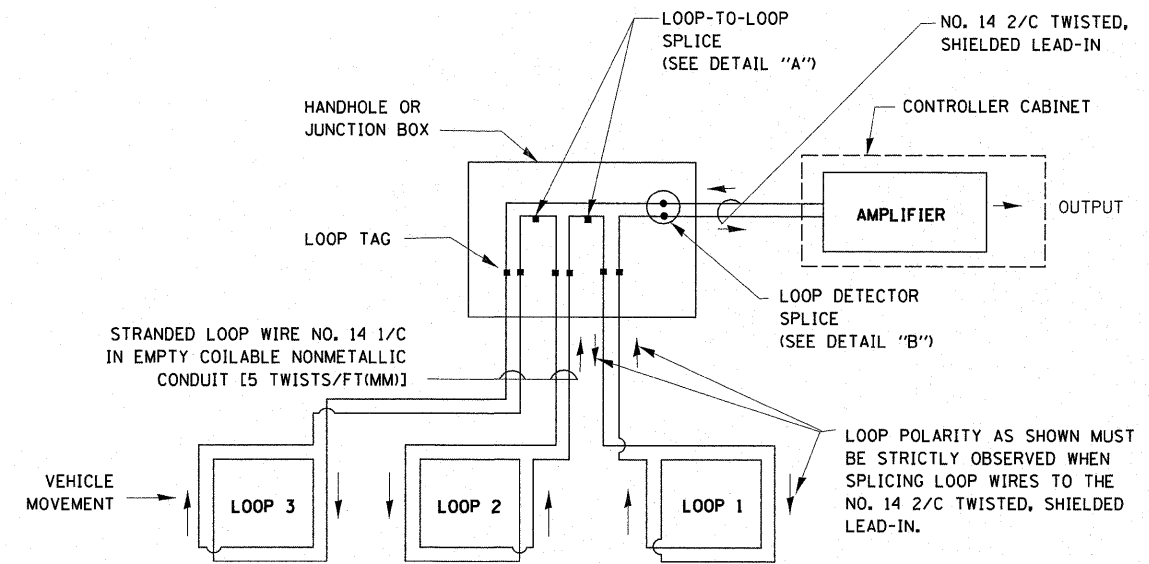
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

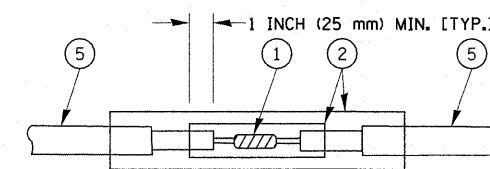


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

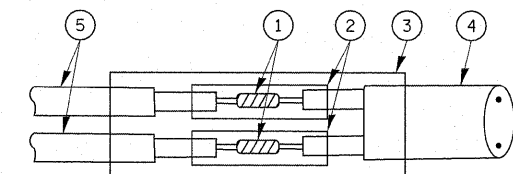


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

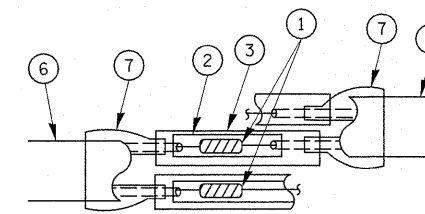


DETAIL "A"
LOOP-TO-LOOP SPLICE

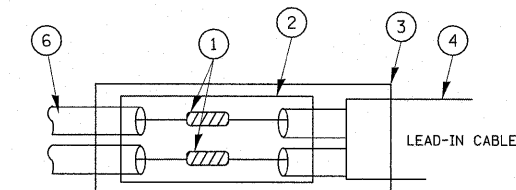


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = abreuh	DESIGNED - DAD	REVISED -
es:\pwwork\pwwork\abreuh\d0265704\Dist1	td.dgn	DRAWN - BCK	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - DAD	REVISIONS -	REVISIONS -
PLOT DATE = 5/10/2011	DATE - 10-28-09	REVISIONS -	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-030-1	VARIOUS	33	33
TS-05			CONTRACT NO. 60P34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				