

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847)705-4406 SCHAUMBURG, IL.

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HIGHWAY STANDARDS
SEE SHEET 2

TRAFFIC DATA

ADT: 16TH STREET
EXISTING: 10,500 (2011)

DESIGN DESIGNATION:

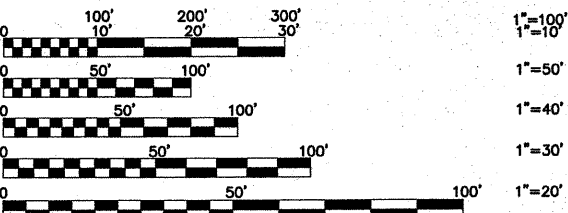
COLLECTOR 0.90 (COMP-20)

POSTED SPEED

30 MPH-16TH STREET (EXISTING)

DESIGN SPEED

30 MPH-16TH STREET (PROPOSED)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULIE JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811

Know what's below. Call before you dig.

Frank Novotny & Associates, Inc.
825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
Civil Engineers Municipal Consultants ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000928

FNA PROJECT NO. 10080 DRAWN/DESIGNED JFP/AMS CHECKED/APPROVED AMS/TPG

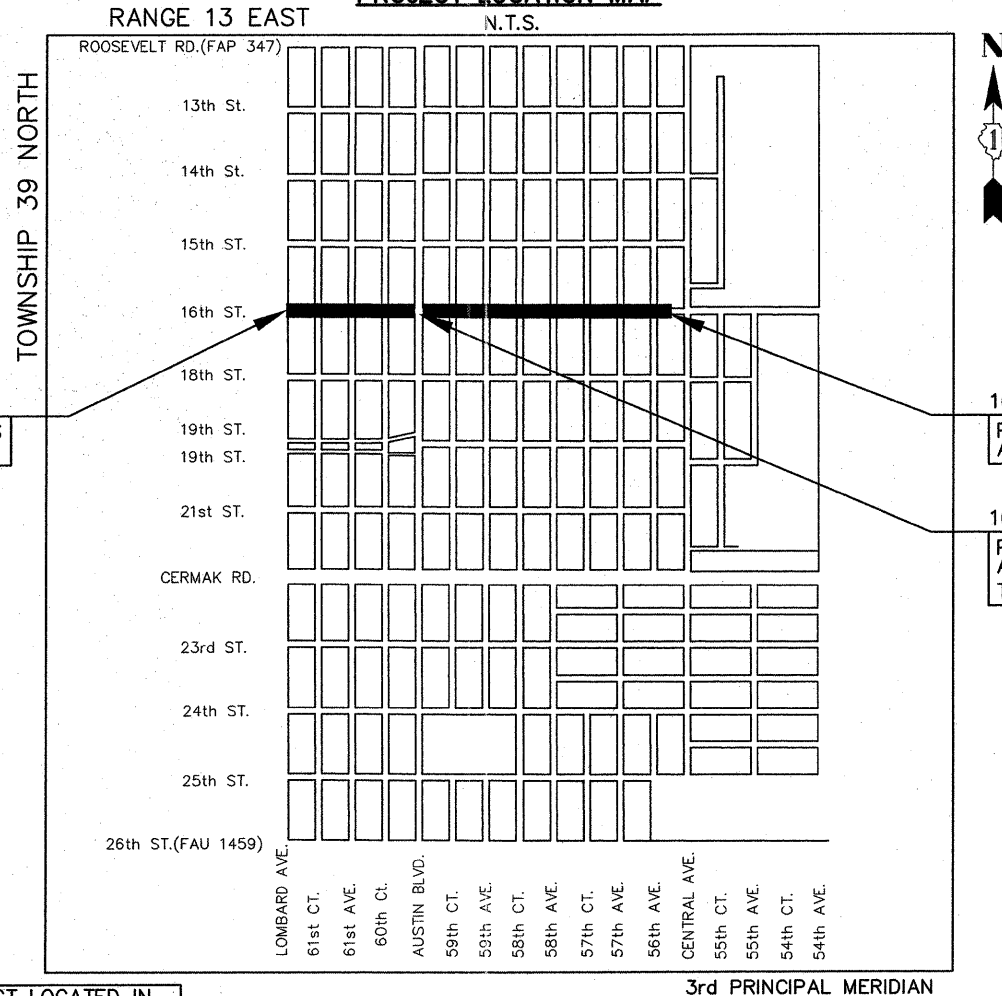
REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	4/7/11	PER I.D.O.T. COMMENTS
2	AMS	4/22/11	PER I.D.O.T. COMMENTS

CONTRACT NO. 63607

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

FAU 1447 (16TH STREET)
LOMBARD AVENUE TO FAU 2798 (CENTRAL AVENUE)
RESURFACING
SECTION 11-00213-00-RS
PROJECT HPP-3045(003)
TOWN OF CICERO
COOK COUNTY
C-91-471-11

PROJECT LOCATION MAP



PROJECT LOCATED IN THE TOWN OF CICERO

— DENOTES LOCATION OF IMPROVEMENT IN CICERO TOWNSHIP

LENGTH OF PROJECT

GROSS LENGTH OF PROJECT 3,863 FEET (0.73 MILES)
NET LENGTH OF PROJECT 3,822 FEET (0.72 MILES)

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	1
F.H.W.A. REG.	ILLINOIS	PROJECT HPP-3045(003)		

CONTRACT NO. 63607



LOCATION OF SECTION INDICATED THUS: ■

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 26 April 20_11

TOWN PRESIDENT Larry Dominick
LARRY DOMINICK, PRESIDENT

PASSED May 24 20_11

Christina
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW MAY 25 20_11

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

Timothy P. Geary
TIMOTHY P. GEARY, P.E.
REG. NO. 62-043796
EXPIRES 11-30-2011
DATE 4-25-11

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

GENERAL CONSTRUCTION NOTES PAVING AND STORM SEWERS

SPECIFICATIONS

THE LATEST EDITIONS OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", PREPARED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" SHALL GOVERN ALL WORK ASSOCIATED WITH THIS PROJECT. THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY GOVERN OTHER WORK ON THIS PROJECT AS INDICATED BY REFERENCE.

CARE IN EXCAVATION

CARE SHALL BE EXERCISED BY THE CONTRACTOR IN CARRYING OUT EARTH AND/OR TRENCHING OPERATIONS SO THAT LOCAL UTILITY SERVICES, WATER VALVES, MANHOLES, CATCH BASINS, INLETS, BUFFALO BOXES, AND OTHER STRUCTURES ARE NOT DAMAGED OR REMOVED. ANY DAMAGE DONE BY THE CONTRACTOR, WHETHER THE STRUCTURE OR SERVICE IS VISIBLE AT THE GROUND SURFACE OR NOT, SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTIFICATION OF PUBLIC UTILITIES

PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OFFICIALS OF THE PUBLIC WORKS DEPARTMENT OF CICERO (TEL 708-656-3600), J.U.L.I.E. AT 1-800-892-0123 OR 811, AND OTHER PUBLIC AND PRIVATE UTILITIES SO THAT ARRANGEMENTS CAN BE MADE TO LOCATE THEIR VARIOUS FACILITIES WITHIN THE LIMITS OF CONSTRUCTION UNDER THIS CONTRACT, AS WELL AS TO PROVIDE ADEQUATE PROTECTION AND INSPECTION THERETO. IT SHALL BE THIS CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES IN THE FIELD.

TRAFFIC CONTROL DEVICES

BARRICADES AND WARNING SIGNS SHALL BE PROVIDED IN ACCORDANCE WITH ARTICLE 107.14 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

PROTECTION OF SIGNS AND PROPERTY

ALL TRAFFIC SIGNS, STREET SIGNS, ETC., THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AND PLACED AT NEW LOCATIONS AS DESIGNATED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE COMBINATION CURB AND GUTTER REMOVAL, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. IN ADDITION, ALL MAIL BOXES THAT INTERFERE WITH CONSTRUCTION SHALL BE SIMILARLY RELOCATED AT NO ADDITIONAL COST IN ACCORDANCE WITH ARTICLES 107.20 AND 107.21 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

SUPERINTENDENCE

SPECIAL ATTENTION IS DRAWN TO ARTICLE 105.06 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" WHICH REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT AND SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVING CONTROL OF ALL THE WORK AS THE AGENT OF THE GENERAL CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.07.

SAWING EXISTING IMPROVEMENTS

ALL PERMANENT TYPE PAVEMENTS OR OTHER PERMANENT IMPROVEMENTS WHICH ABUT THE PROPOSED IMPROVEMENT AND MUST BE REMOVED, SHALL BE SAWED AS DIRECTED PRIOR TO REMOVAL. ALL ITEMS SO REMOVED SHALL BE REPLACED WITH SIMILAR CONSTRUCTION MATERIALS TO THEIR ORIGINAL CONDITION OR BETTER. PAYMENT FOR SAWING SHALL BE INCLUDED IN THE COST FOR THE REMOVAL OF EACH ITEM, AND REPLACEMENT WILL BE PAID FOR UNDER THE RESPECTIVE ITEMS IN THE CONTRACT UNLESS OTHERWISE INDICATED. SAW CUTTING FOR PATCHES WILL BE INCLUDED IN THE COST OF TO THE PATCHING ITEM. EXISTING DRIVEWAY PAVEMENT AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAWCUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND THE EXISTING, AND SUCH COST SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

PROJECT SAFETY

BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR SHALL COMPLY WITH AND OBSERVE THE RULES AND REGULATIONS OF O.S.H.A. AND APPROPRIATE AUTHORITIES REGARDING SAFETY PROVISIONS. THE CONTRACTOR, ENGINEER, AND OWNER SHALL EACH BE RESPONSIBLE FOR THEIR OWN RESPECTIVE AGENTS AND EMPLOYEES.

THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS, OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE DOCUMENTS AND SPECIFICATIONS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

MWRDGC NOTES

METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO
LOCAL SEWER SYSTEMS SECTION

TYPICAL GENERAL NOTES

- The MWRD Local Sewer Systems Section Field Office must be notified at least two (2) working days prior to the commencement of any work (call 708/588-4055).
- Elevation datum is U.S.G.S.
Conversion equation is N/A.
- All floor drains shall discharge to the sanitary sewer system. (NOT APPLICABLE)
- All downspouts and footing drains shall discharge to the storm sewer system. (NOT APPLICABLE)
- All sanitary sewer pipe materials and joints (and storm sewer pipe materials and joints in a combined sewer area) shall conform to:

Pipe Material Spec. Joint Spec.

Vitrified Clay Pipe
VCP (C-700) C-425
VCP (No-Bel)(C-700) C-425
Joint D-1784
Collar

Concrete Pipe (C-14)
RCP (C-76) C-443
ACP (C-428) D-1869

ABS Sewer Pipe
Solid Wall 6" dia. SDR 23.5 D-2751
ABS D-2751

ABS Composite/Truss Pipe
8" - 15" dia. D-2680
ABS D-2680

PVC Gravity Sewer Pipe
6" - 15" dia. SDR 26 D-2241 D-3139
AWWA-C-900 D-3139

18" - 27" dia. F/dy=46 D-3212 or
F-679 D-2855

CISP A-74 C-564
DIP A-21.51 A-21.11

(Note: The District has approved less common pipe materials on a qualified basis in addition to those above. Please contact the District if considering using pipe not listed above.)

- All sanitary sewer construction (and storm sewer construction in combined sewer areas), requires stone bedding with stone 1/4" to 1" in size, with minimum bedding thickness equal to 1/4 the outside diameter of the sewer pipe, but not less than four (4) inches nor more than eight (8) inches. Materials shall be CA-11 or CA-13 and shall be extended at least 12" above the top of the pipe when using PVC.
- "Band-Seal" or similar flexible-type couplings shall be used in the connection of sewer pipe of dissimilar materials.
- When connecting to an existing sewer main by means other than an existing wye, tee, or an existing manhole, one of the following methods shall be used:
 - Circular saw-cut of sewer main by proper tools ("Shower-Tap" machine or similar) and proper installation of hub-wye saddle or hub-tee saddle.
 - Remove an entire section of pipe (breaking only the top of one bell) and replace with a wye or tee branch section.
 - With pipe cutter, neatly and accurately cut out desired length of pipe for insertion of proper fitting, using "Band-Seal" or similar couplings to hold it firmly in place.
- Wherever a sanitary/combined sewer crosses under a water main, the minimum vertical distance from the top of the sewer to the bottom of the water main shall be 18 inches. Furthermore, a minimum horizontal distance of 10 feet between sanitary/combined sewers and water main shall be maintained unless: the sewer is laid in a separate trench, keeping a minimum 18" vertical separation; or the sewer is laid in the same trench with a water main located at the opposite side on a bench of undisturbed earth, keeping a minimum 18" vertical separation. If either the vertical or horizontal distances described above cannot be maintained or the sewer crosses above the water main, the sewer shall be constructed to water main standards.
- All existing septic systems shall be abandoned. Abandoned tanks shall be filled with granular material or removed.
- All sanitary manholes, and also storm manholes in combined sewer areas, shall have a minimum inside diameter of 48 inches, and shall be cast-in-place or pre-cast reinforced concrete. Resilient connectors, conforming to ASTM C-923, shall be used between manhole and pipe(s) for all sanitary and combined sewer structures.

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602701-02	MANHOLE STEPS
604001-03	FRAMES & LIDS-TYPE 1
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
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701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNT DETAILS
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886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

SPECIAL PROJECT NOTES

- ALL SAWCUTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS FOR WHICH THE WORK APPLIES.
- ALL EXISTING FRAMES AND LIDS THAT ARE TO BE REPLACED (AS DIRECTED BY THE ENGINEER), SHALL BE SALVAGED TO THE CONTRACTOR.
- ALL METERS, VALVES, AND BUFFALO BOXES WITHIN SIDEWALK AND DRIVEWAY REMOVAL LIMITS SHALL BE ADJUSTED AS DIRECTED.
- ALL AT&T MANHOLES TO BE ADJUSTED (BY OTHERS).
- MEET EXISTING CURB AND FLOW LINE ELEVATIONS AT SIDE STREET APPROACHES.
- ALL CURBLINE INLETS AND CATCH BASINS ON THIS PROJECT FLOW TO A COMBINED SEWER. ALL WORK SHALL CONFORM TO ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (M.W.R.D.) STANDARDS. THE FINAL OUT FLOWING PIPE FROM ANY INLET OR CATCH BASIN STRUCTURE THAT WILL FLOW TO THE COMBINED SEWER SHALL BE TRAPPED AS DETAILED. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE REPLACEMENT OF THE STRUCTURE.
- AT LOCATIONS WHERE NEW DRAINAGE STRUCTURES ARE TO BE INSTALLED, THE REMOVAL OF THE EXISTING DRAINAGE STRUCTURE SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- ALL PROPOSED STORM SEWER LATERAL RECONSTRUCTION PIPE SHALL BE PVC (SDR 26), ASTM D-2241, 8" DIA. AND SHALL BE INSTALLED WITH INLET AND CATCH BASIN, SPECIALS. NEW 8" LATERAL PIPE SHALL BE CONNECTED TO EXISTING STORM SEWER PIPE USING A "BAND SEAL" CONNECTION.
- DRAINAGE STRUCTURE FABRICATION WILL NOT COMMENCE PRIOR TO FIELD VERIFICATION OF PIPE CONNECTIONS BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.

FILE NAME = FAU 1447 (16TH STREET)-
(FROM LOMBARD AVENUE TO CENTRAL AVENUE)

USER NAME =
PLOT SCALE =
PLOT DATE =

DESIGNED - AMS
DRAWN - JFP
CHECKED - TPG
DATE - 3/11

REVISED - AMS 4-7-11
REVISED - AMS 4-22-11
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF HIGHWAY STANDARDS, GENERAL CONSTRUCTION
NOTES, MWRDGC NOTES,
SPECIAL PROJECT NOTES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

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Civil Engineers
Municipal Consultants
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000228

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	2
CONTRACT NO. 63607				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	HPP-3045(003)	

Specialty Item	Special Provision	Item No	Description	Unit	Construction Code 0005 Quantity
	SP	21101605	TOP SOIL FURNISH AND PLACE, 2"	SQ YD	2,600
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	35
		25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	35
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	35
		25200100	SODDING	SQ YD	2,600
		25200200	SUPPLEMENTAL WATERING	UNIT	40
		28000500	INLET AND PIPE PROTECTION	EACH	60
		28000510	INLET FILTERS	EACH	15
		35300300	PORTLAND CEMENT CONCRETE BASE COURSE 8"	SQ YD	950
	SP	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	140
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,900
		40600300	AGGREGATE (PRIME COAT)	TON	20
		40600535	LEVELING BINDER (HAND METHOD), N/0	TON	100
		40600635	LEVELING BINDER (MACHINE METHOD), N/0	TON	1,120
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	440
		40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	365
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N/0	TON	2,200
		40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	40
		42000300	PORTLAND CEMENT CONCRETE PAVEMENT 8"	SQ YD	400
	SP	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	675
	SP	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	15,900
	SP	42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	3,000
		44000100	PAVEMENT REMOVAL	SQ YD	1,350
		44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	19,000
	SP	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	675
	SP	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	8,500
	SP	44000600	SIDEWALK REMOVAL	SQ FT	18,900
		44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	2,165
		44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	300
		44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	600
		44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	1,000
	SP	44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	19,000
	SP	56500300	DOMESTIC METER VAULTS TO BE ADJUSTED	EACH	3
	SP	56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	7
		60250200	CATCH BASINS TO BE ADJUSTED	EACH	29
		60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	13
		60255500	MANHOLES TO BE ADJUSTED	EACH	1
		60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2
		60260100	INLETS TO BE ADJUSTED	EACH	17
		60265700	VALVE VAULTS TO BE ADJUSTED	EACH	28
		60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	7

Specialty Item	Special Provision	Item No	Description	Unit	Construction Code 0005 Quantity
		60266600	VALVE BOXES TO BE ADJUSTED	EACH	1
		60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	10
		60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	19
	SP	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3
		67100100	MOBILIZATION	L SUM	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD, 701501	L SUM	1
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD, 701701	L SUM	1
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD, 701801	L SUM	1
		70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4,200
		70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	150
		70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6,500
		70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3,800
		70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	320
		70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	400
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,500
*		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	150
*		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6,500
*		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,800
*		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	320
*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	400
*	SP	88600600	DETECTOR LOOP REPLACEMENT	FOOT	450
	SP	X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	1,100
	SP	X0795800	COURSE AGGREGATE	TON	250
	SP	X2020410	EARTH EXCAVATION (SPECIAL)	CY YD	150
	SP	X4400196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	200
	SP	X6022402	MANHOLES, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID, SPECIAL	EACH	1
	SP	X6022805	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID, SPECIAL	EACH	6
	SP	X6022905	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID, SPECIAL	EACH	8
	SP	X6023204	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID, SPECIAL	EACH	13
	SP	X6026628	VALVE BOXES TO BE REPLACED	EACH	1
	SP	X6029600	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID, SPECIAL	EACH	2
	SP	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	18
*	SP	X8140115	HANDHOLE TO BE ADJUSTED	EACH	3
*	SP	X8140215	HEAVY-DUTY HANDHOLE TO BE ADJUSTED	EACH	2
*	SP	Z0001110	GAS VALVE TO BE ADJUSTED	EACH	7
	SP	Z0038116	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 3/4"	SQ YD	1,500
	SP	Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CY YD	20
	SP	Z0076600	TRAINERS	FOUR	500
	SP	XX003313	REMOVE AND REINSTALL BRICK PAVEMENT	SQ FT	180
	SP	XX008464	DOMESTIC WATER SERVICE BOX TO BE ADJUSTED (SPECIAL)	EACH	2
	SP	XX104800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12	FOOT	8,500

△ 0042

FILE NAME = FAU 1447 (16TH STREET)- (FROM LOMBARD AVENUE TO CENTRAL AVENUE)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-7-11
		DRAWN - JFP	REVISED - AMS 4-22-11
	PLOT SCALE =	CHECKED - TPG	REVISED -
	PLOT DATE =	DATE - 3/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
835 Midway Drive • Willmetts, IL • 60577 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000828

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	3
CONTRACT NO. 63607				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)				

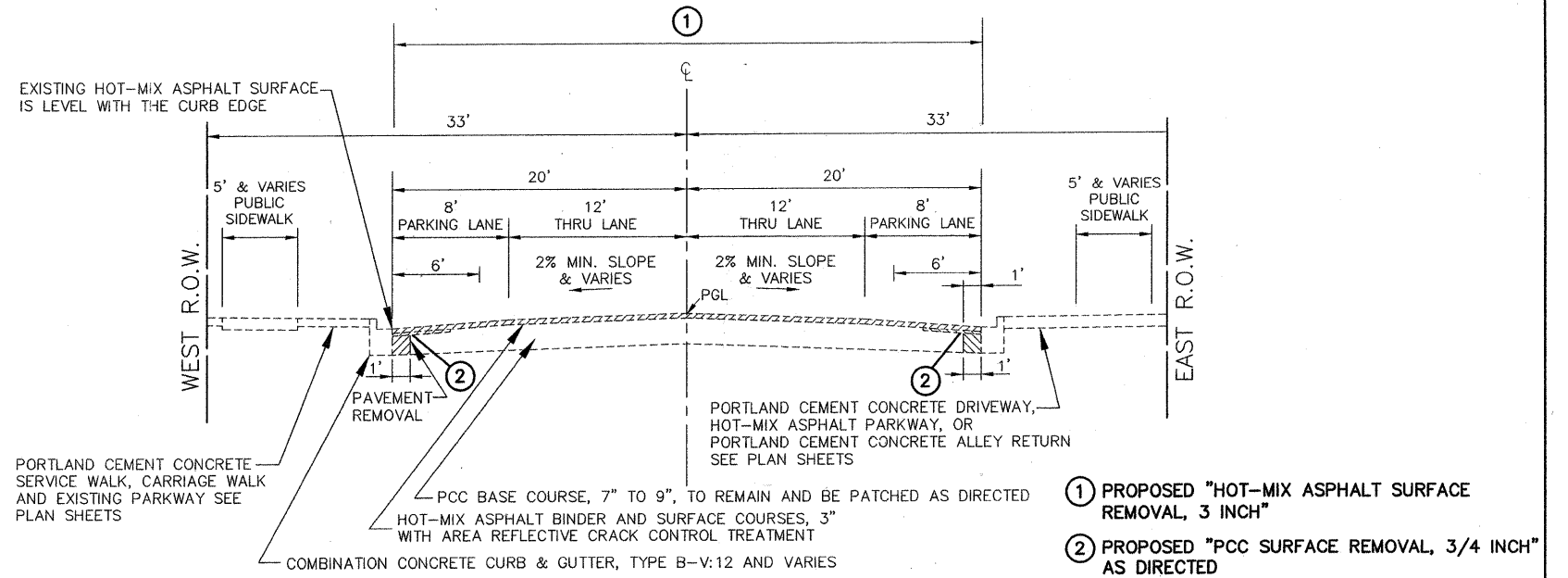
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	PERCENT AIR VOIDS
ROADWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5mm), 2"	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70, (IL-9.5mm), 1"	4% @ 70 GYR
LEVELING BINDER (HAND METHOD), N70, (IL-9.5mm)	4% @ 70 GYR
PARKWAYS	
INCIDENTAL HOT-MIX ASPHALT SURFACING, (HMA SURFACE COURSE, MIX "C", N50, (IL-9.5mm), 2"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES, TYPE II-IV, 9", (HMA BINDER IL-19.0mm) (IN 3 LIFTS)	4% @ 70 GYR
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (HMA BINDER IL-19.0mm)	4% @ 70 GYR

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

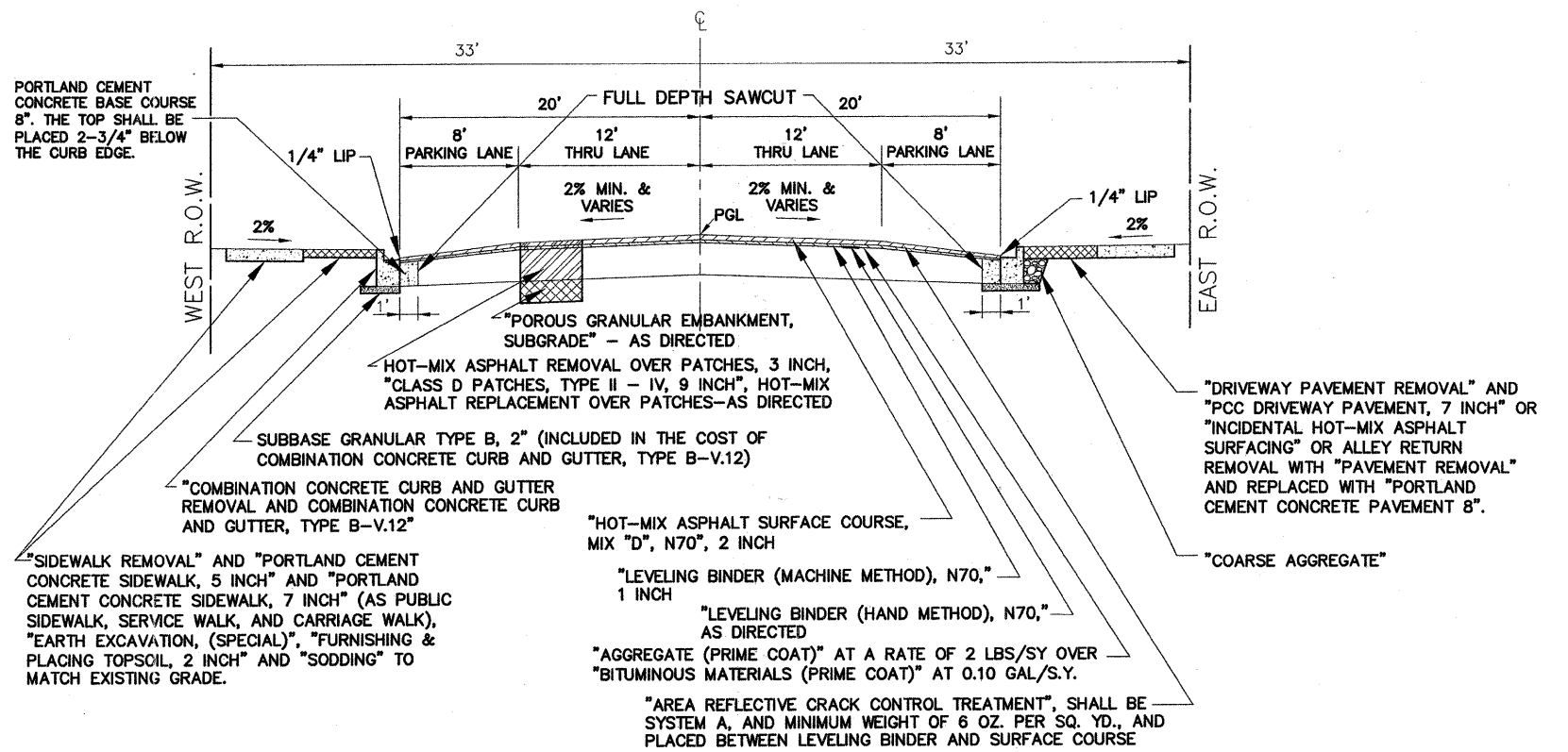
"THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS"

"FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS"



EXISTING TYPICAL SECTION

LOMBARD AVENUE TO CENTRAL AVENUE
STA. 0+00 TO STA. 38+63



PROPOSED TYPICAL SECTION

LOMBARD AVENUE TO CENTRAL AVENUE
STA. 0+00 TO STA. 38+63

16TH STREET

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FILE NAME = FAU 1447 (16TH STREET)- (FROM LOMBARD AVENUE TO CENTRAL AVENUE)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-7-11
RESURFACING	PLOT SCALE =	DRAWN - JFP	REVISED - AMS 4-22-11
	PLOT DATE =	CHECKED - TPG	REVISED -
		DATE - 3/11	REVISED -

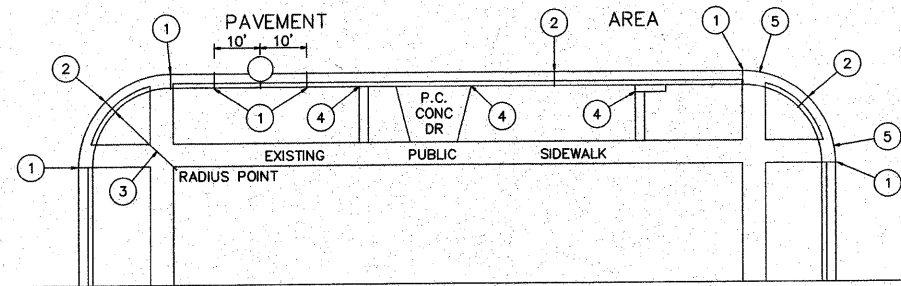
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS,
HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

SCALE: 1"=6' SHEET NO. OF SHEETS STA. TO STA.

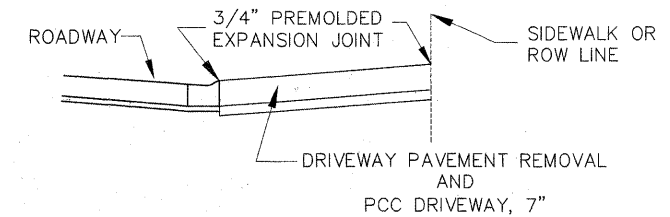
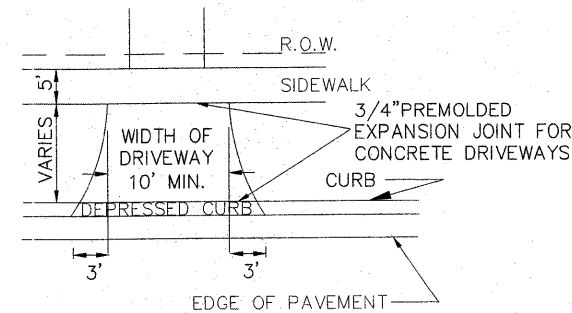
Frank Novotny & Associates, Inc.
Civil Engineer
Municipal Consultants
626 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000628

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	4
CONTRACT NO. 63607				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)				



1. EXPANSION JOINTS AT TANGENT POINTS AND 150 FOOT INTERVALS, CONSISTING OF ONE INCH PREMOLDED JOINT FILLER MATERIAL WITH #8 DOWEL BARS, 18" IN LENGTH, GREASED, PROVIDE EXPANSION CAP ON ONE END. ALSO CONSTRUCT THIS JOINT TEN FEET EACH SIDE OF PROPOSED UNDERGROUND STRUCTURE.
2. CONTRACTION JOINTS AT FIFTEEN FOOT INTERVALS AND AT THE CENTER OF RETURNS.
3. ALL RADII SHALL BE 25 FEET TO THE BACK OF CURB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
4. LONGITUDINAL EXPANSION JOINT CONSISTING OF THREE QUARTER INCH PREMOLDED JOINT FILLER.
5. DEPRESS CURB AT LOCATIONS WHERE PUBLIC WALKS INTERSECT CURB LINE AT STREET INTERSECTIONS, ALLEYS, AND OTHER LOCATIONS AS DIRECTED, FOR THE CONSTRUCTION OF RAMPED SIDEWALKS FOR ACCESS BY THE HANDICAPPED.

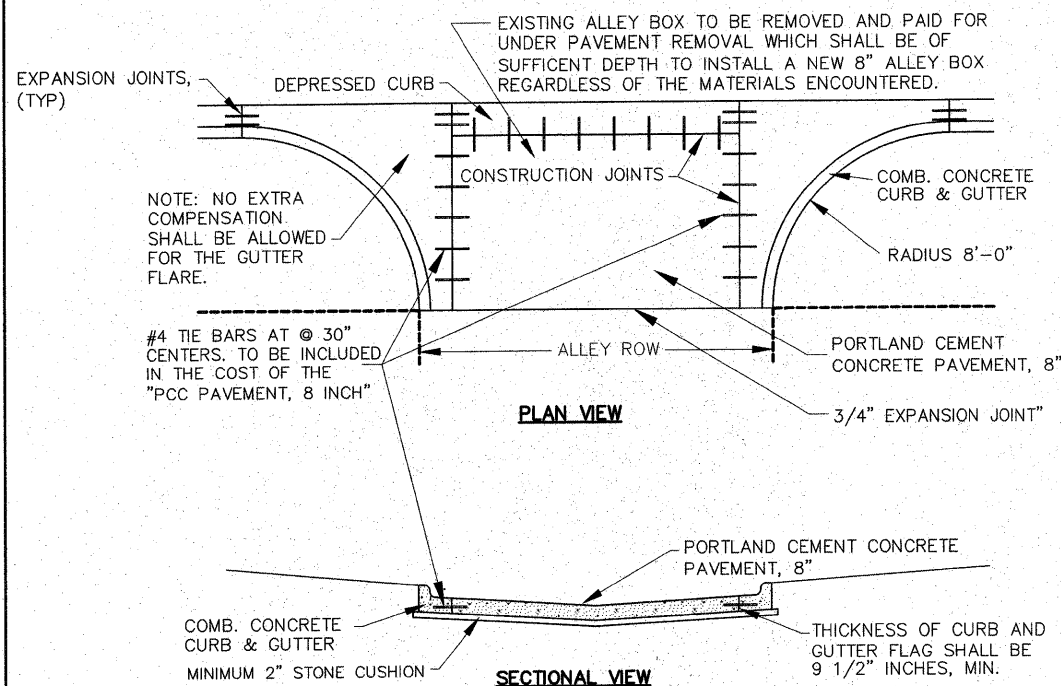
JOINT DETAILS



THICKNESS OF CURB AND GUTTER FLAG SHALL BE 9 1/2 INCHES, MIN.

SAWCUTTING AND THE 3/4" PREMOLDED EXPANSION JOINT WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF PCC DRIVEWAY PAVEMENT, 7"

TYPICAL DRIVEWAY DETAIL



TYPICAL ALLEY RETURN DETAIL

		EARTH EXCAVATION (SPECIAL)	
		SCHEDULE	
STATION TO	STATION	SOUTH PARKWAY	NORTH PARKWAY
0+20	1+30	5 CY	
1+80	3+15		5 CY
2+15	3+20	4 CY	
3+50	4+90		5 CY
3+50	4+95	7 CY	
6+90	8+10	5 CY	5 CY
8+45	9+85	5 CY	
8+65	9+70		4 CY
10+30	11+30	4 CY	
10+35	11+40		4 CY
11+80	13+00	5 CY	
13+80	14+77	4 CY	4 CY
15+50	16+45		3 CY
16+93	18+10		5 CY
16+93	19+75	6 CY	
21+00	22+18	4 CY	
23+60	26+45	6 CY	
23+60	24+85		4 CY
27+00	27+80	3 CY	
28+00	29+05	4 CY	
29+35	29+80	3 CY	
27+10	28+10		4 CY
29+20	29+80		3 CY
30+28	31+45		4 CY
30+42	31+50	4 CY	
32+05	33+25	4 CY	
32+07	33+14		5 CY
33+76	34+60	3 CY	
34+77	35+53	3 CY	
35+70	36+70	4 CY	
33+73	35+06		5 CY
35+47	36+63		4 CY
37+00	38+15	4 CY	
37+12	38+35		4 CY
		87 CY	63 CY
		TOTAL 150 CY	

FILE NAME = FAU 1447 (16TH STREET)- (FROM LOMBARD AVENUE TO CENTRAL AVENUE)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-7-11
RESURFACING	PLOT SCALE =	DRAWN - JFP	REVISED -
	PLOT DATE =	CHECKED - TPG	REVISED -
		DATE - 3/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SPECIAL PROJECT DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

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Municipal Consultants
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ILLINOIS PROFESSIONAL DESIGN PERM NO. 184-00028

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 5
CONTRACT NO. 63607			ILLINOIS FED. AID PROJECT HPP-3045(003)	

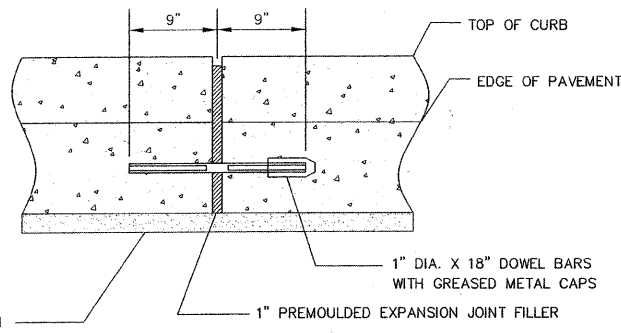
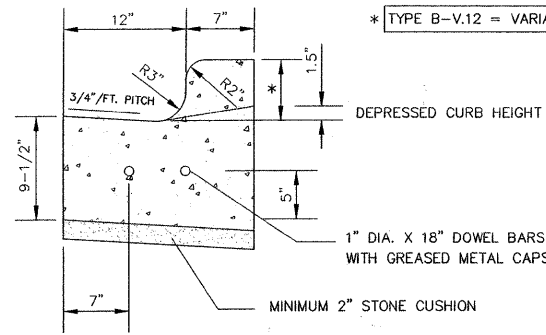
NOTES:

1" PREFORMED EXPANSION MOLDING SHALL BE PLACED WITH TWO SMOOTH 1" DIA. DOWEL BARS WITH GREASED CAPS AT ALL POINTS OF CURVATURE AND CORNERS.

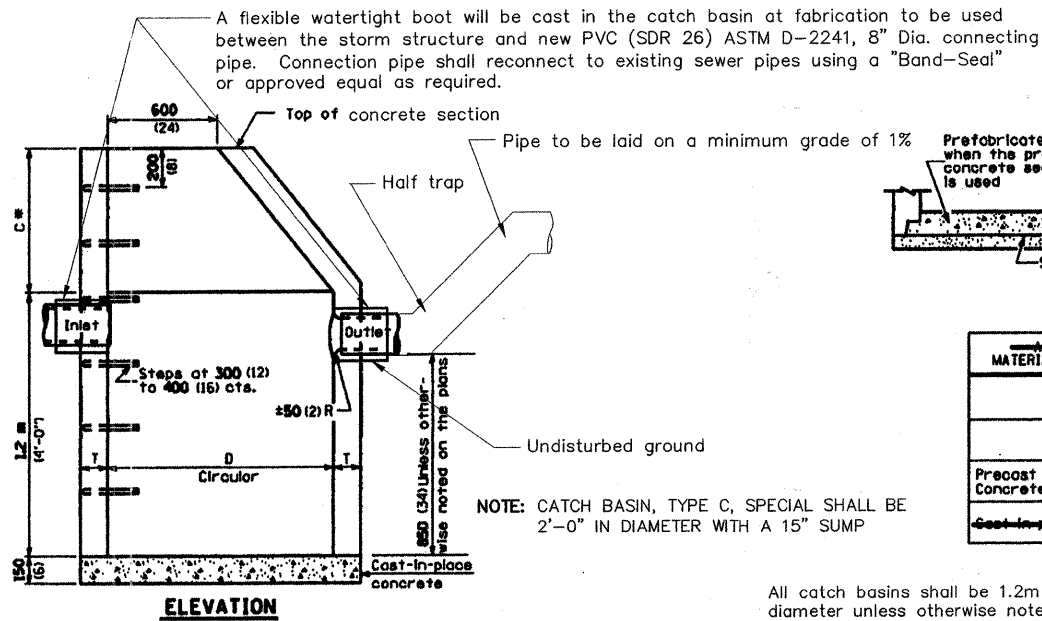
CONTRACTION JOINTS SHALL BE SAW CUT OR TOOLED TO A DEPTH OF 2" @ 15' MINIMUM SPACING. SAW CUT CONTRACTION JOINTS SHALL BE DONE WITHIN 24 HR.. ALL CONTRACTION JOINTS SHALL BE SEALED WITH AN IDOT APPROVED JOINT SEALANT.

AN IDOT APPROVED CURING COMPOUND SHALL BE USED ON ALL PROPOSED CONCRETE CURB AND GUTTER.

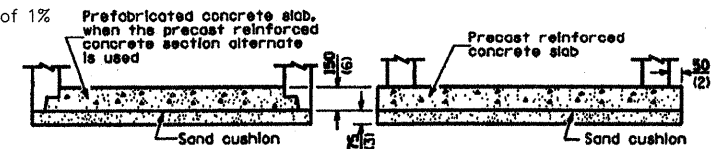
3/4" TIE ANCHOR BARS SHALL BE INSTALLED IN THE ENDS OF EXISTING CURB AND GUTTER TO TIE THE NEW CURB TO THE EXISTING. TIE ANCHOR BARS WILL NOT BE PAID FOR SEPARATELY AND INCLUDED IN THE COST OF THE COMBINATION CONCRETE CURB & GUTTER.



EXPANSION JOINT DETAIL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12



ELEVATION
CATCH BASIN, TYPE A, SPECIAL



ALTERNATE BOTTOM SLAB

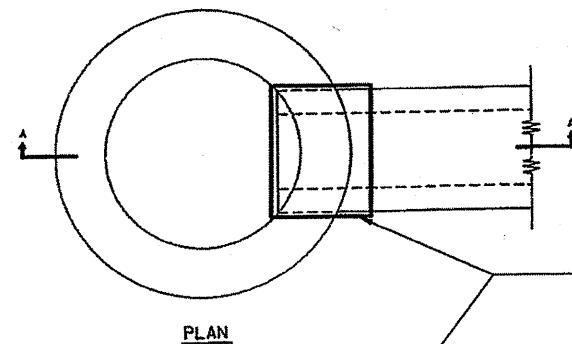
ALTERNATE MATERIALS FOR WALLS	D	C	T (min.)
Precast Reinforced Concrete Section	1.2 m (4'-0")	750 (30)	100 (4)
Cast-in-place Concrete	1.5 m (5'-0")	115 m (3'-9")	125 (5)

GENERAL NOTES

All catch basins shall be 1.2m (4'-0") in diameter unless otherwise noted on the plans.
*Dimension C for precast reinforced concrete section may vary from the dimension given to plus 150mm (6").

See Standard IDOT 602601 for optional precast reinforced concrete flat slab top.
See Standard IDOT 602701 for details of steps.
All dimensions are in millimeters (inches) unless otherwise shown.

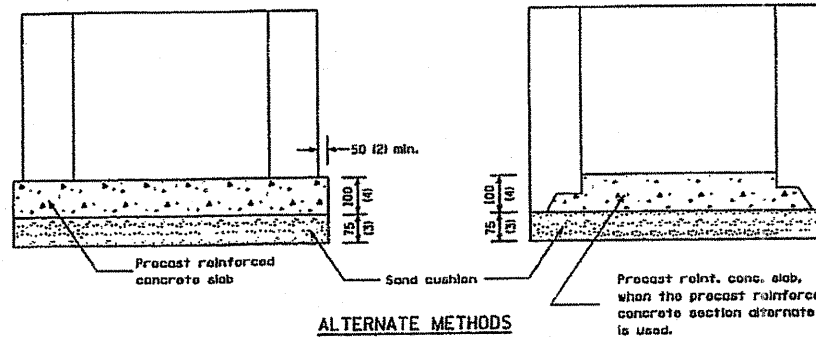
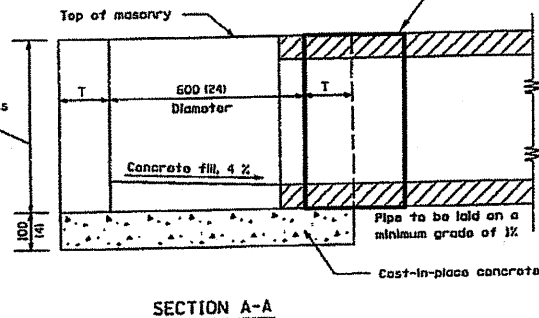
CATCH BASIN, TYPE C, SPECIAL



TOP OF PRECAST STRUCTURE

ALTERNATE MATERIALS FOR WALLS	T
BRIER MASONRY	200 (8)
CAST-IN-PLACE CONCRETE	150 (6)
CONCRETE MASONRY UNIT	125 (5)
PRECAST REINFORCED CONCRETE SECTION	75 (3)

A flexible watertight boot will be cast in the inlet at fabrication to be used between the storm structure and new PVC (SDR 26) ASTM D-2241, 8" Dia. connecting pipe. Connection pipe shall reconnect to existing sewer pipes using a "Band-Seal" or approved equal as required.

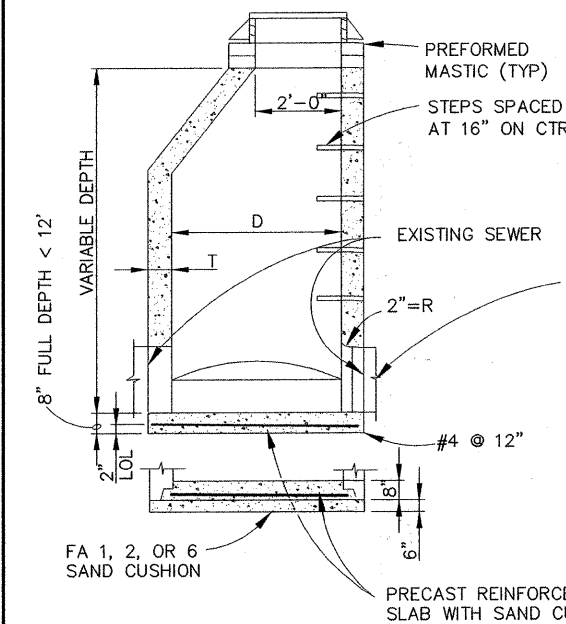


SECTION A-A
INLET, TYPE A, SPECIAL

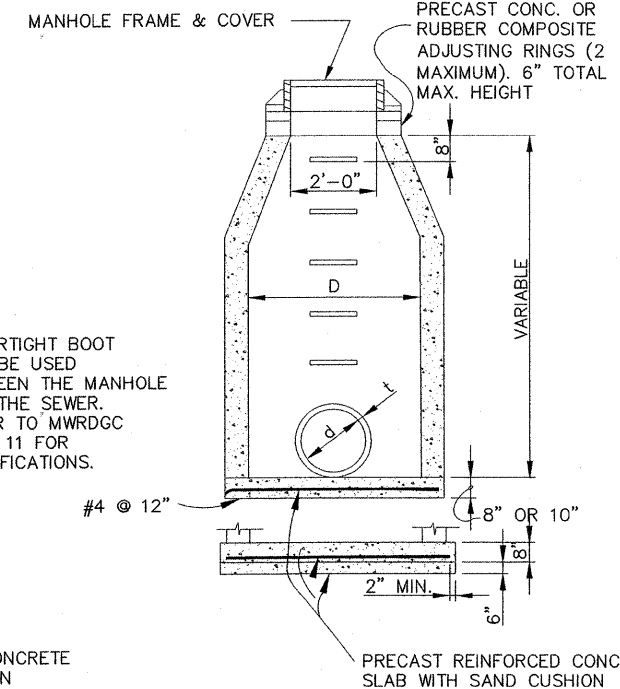
All dimensions are in millimeters (inches) unless otherwise shown.

NOTES:

PROVIDE CA-6 AGGREGATE BACKFILL AROUND MANHOLE TO SUBGRADE ELEVATION IN PAVED AREAS. LIFT HOLES ON MANHOLES TO BE SEALED WITH CONCRETE PLUG AND MASTIC



SECTION-ECCENTRIC



SECTION-CONCENTRIC

FOR DIAMETER, D= 4 ft. THICKNESS, T= 5 in.
5 ft. 6 in.
6 ft. 6 in.

MANHOLE, TYPE A, SPECIAL

FILE NAME = FAU 1447 (16TH STREET)-
(FROM LOMBARD AVENUE TO CENTRAL AVENUE)
RESURFACING

USER NAME =
DESIGNED - AMS
DRAWN - JFP
CHECKED - TPG
DATE - 3/11

REVISD - AMS 4-7-11
REVISD -
REVISD -
REVISD -

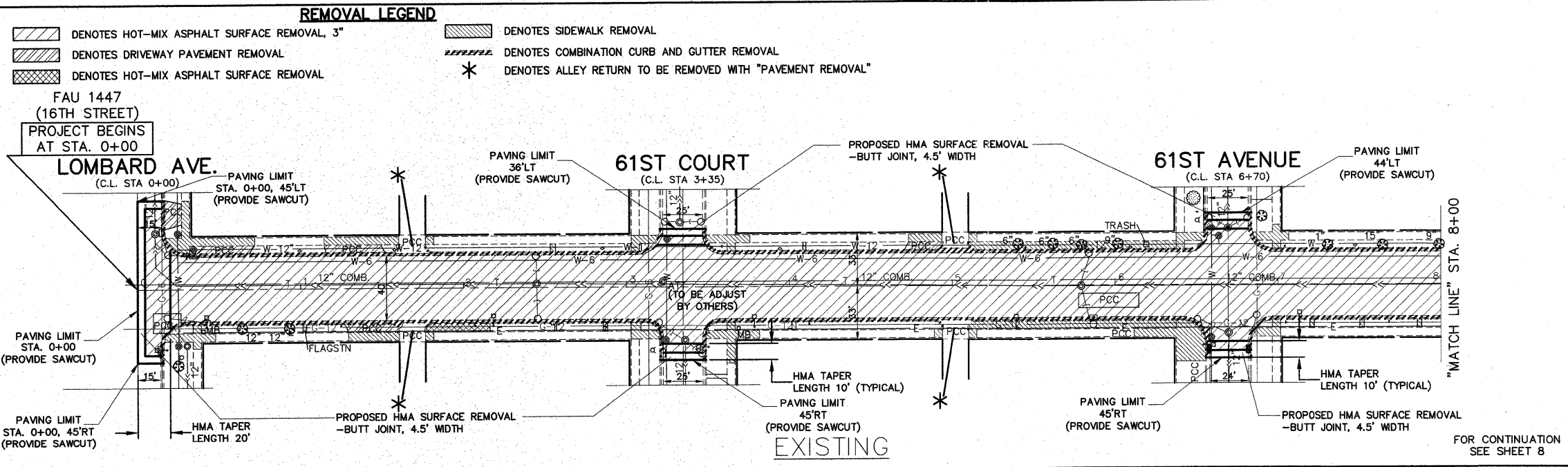
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SPECIAL PROJECT DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-000938

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	6
CONTRACT NO. 63607				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)				



BENCHMARKS
(U.S.G.S. DATUM)

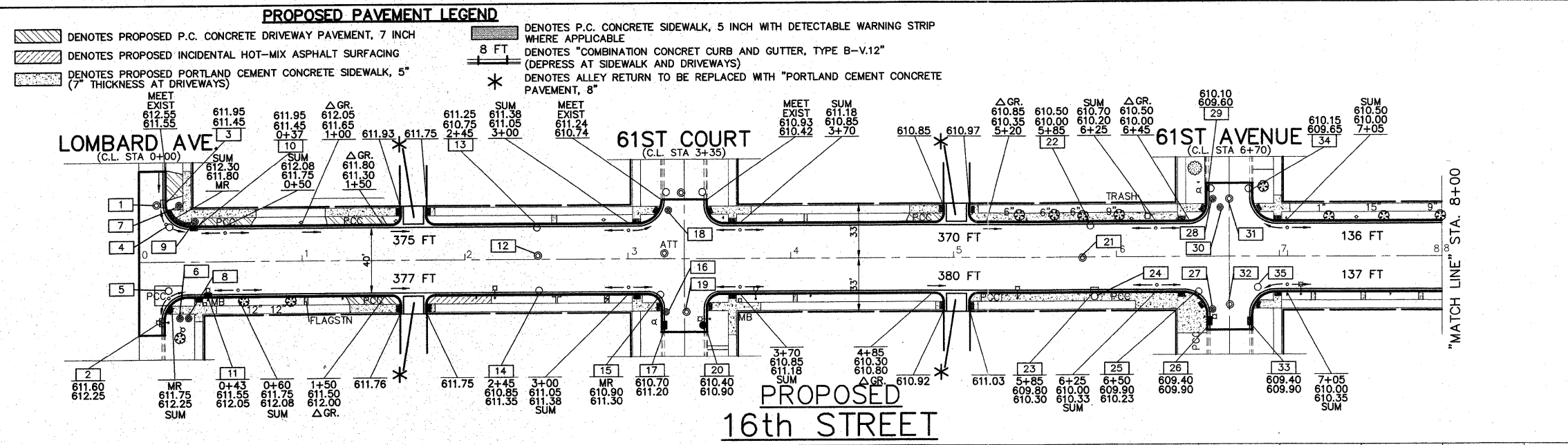
NE BOLT ON FIRE HYDRANT AT SE CORNER OF 16TH STREET AND LOMBARD AVENUE
ELEV.=613.77

NE BOLT ON FIRE HYDRANT AT SW CORNER OF 16TH STREET AND 61ST COURT
ELEV.=612.99

HORZ. 1"=40'
VERT. 1"=2'

STRUCTURE SCHEDULE

1 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL RIM=612.05	20 PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=610.40 INV(N)=606.40
2 INLET TO BE ADJUSTED RIM=611.60	21 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL RIM=610.70
3 INLET TO BE ADJUSTED RIM=611.45	22 CATCH BASIN TO BE ADJUSTED RIM=610.00
4 CATCH BASIN TO BE RECONSTRUCTED RIM=611.90 INV(NE)=608.89 INV(W)=607.75	23 INLET TO BE ADJUSTED RIM=609.80
5 CATCH BASIN TO BE ADJUSTED RIM=612.05	24 CATCH BASIN TO BE ADJUSTED RIM=610.40
6 VALVE VAULT, NO WORK	25 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=609.70 INV(S)=606.90 INV(SE)=606.20
7 VALVE VAULT TO BE ADJUSTED, NEW TYPE 1 FR. & C.L. RIM=612.30	26 PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=609.40 INV(N)=607.00
8 VALVE VAULT, NO WORK	27 VALVE VAULT TO BE ADJUSTED RIM=609.55
9 VALVE VAULT TO BE ADJUSTED, NEW TYPE 1 FR. & C.L. RIM=612.10	28 VALVE VAULT TO BE RECONSTRUCTED RIM=609.65
10 PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=611.45 INV(W)=609.00	29 CATCH BASIN TO BE ADJUSTED RIM=609.60
11 PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=611.55 INV(W)=609.10	30 VALVE VAULT TO BE RECONSTRUCTED RIM=609.60
12 MANHOLE TO BE RECONSTRUCTED, NEW TYPE 1 FR. & C.L. RIM=611.65 INV(N/S)=607.14 INV(E/W)=606.90	31 PROPOSED MANHOLE, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=610.00 INV(NW)=604.85 INV(NE)=604.90 INV(N)=603.01
13 CATCH BASIN TO BE ADJUSTED RIM=610.75	32 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & C.L. RIM=610.15
14 CATCH BASIN TO BE ADJUSTED RIM=610.85	33 PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=609.40 INV(N)=607.20
15 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=610.90 INV(S)=608.15 INV(SE)=607.13	34 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=609.65 INV(SW)=605.15
16 VALVE VAULT TO BE RECONSTRUCTED RIM=610.80	35 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=610.70 INV(S)=606.90 INV(SW)=606.30
17 PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=610.70 INV(N)=608.50	
18 VALVE VAULT TO BE ADJUSTED RIM=611.00	
19 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM=611.06	



PROPOSED 16th STREET

EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING ELEVATION	PROPOSED ELEVATION	STATION
612	612.47	0	612	612.47	1	612	612.47	2	612	612.47	3	612	612.47	4	612	612.47	5
610	612.43	0	610	612.43	1	610	612.43	2	610	612.43	3	610	612.43	4	610	612.43	5
608	612.27	0	608	612.27	1	608	612.27	2	608	612.27	3	608	612.27	4	608	612.27	5
	612.10	0		612.10	1		612.10	2		612.10	3		612.10	4		612.10	5
	611.91	0		611.91	1		611.91	2		611.91	3		611.91	4		611.91	5
	611.75	0		611.75	1		611.75	2		611.75	3		611.75	4		611.75	5
	611.63	0		611.63	1		611.63	2		611.63	3		611.63	4		611.63	5
	611.36	0		611.36	1		611.36	2		611.36	3		611.36	4		611.36	5
	611.29	0		611.29	1		611.29	2		611.29	3		611.29	4		611.29	5
	611.02	0		611.02	1		611.02	2		611.02	3		611.02	4		611.02	5
	610.84	0		610.84	1		610.84	2		610.84	3		610.84	4		610.84	5
	610.70	0		610.70	1		610.70	2		610.70	3		610.70	4		610.70	5
	610.73	0		610.73	1		610.73	2		610.73	3		610.73	4		610.73	5
	610.50	0		610.50	1		610.50	2		610.50	3		610.50	4		610.50	5
	610.41	0		610.41	1		610.41	2		610.41	3		610.41	4		610.41	5

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4



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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

FILE NAME = FAU 1447 (16TH STREET)	USER NAME =	DESIGNED -- AMS	REVISED -- AMS 4-7-11
LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)		DRAWN -- JFP-JEP	REVISED -- AMS 4-22-11
10080 HPP RESURFACING		CHECKED -- TPC	REVISED --
		DATE -- 3/11	REVISED --

FILE NAME = FAU 1447 (16TH STREET)	USER NAME =	DESIGNED -- AMS	REVISED -- AMS 4-7-11
LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)		DRAWN -- JFP-JEP	REVISED -- AMS 4-22-11
10080 HPP RESURFACING		CHECKED -- TPC	REVISED --
		DATE -- 3/11	REVISED --

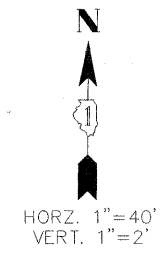
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
16th STREET - (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED SHEET NO. OF SHEETS STA. 0+00 TO STA. 8+00

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 7
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63607	
			HPP-3045(003)	

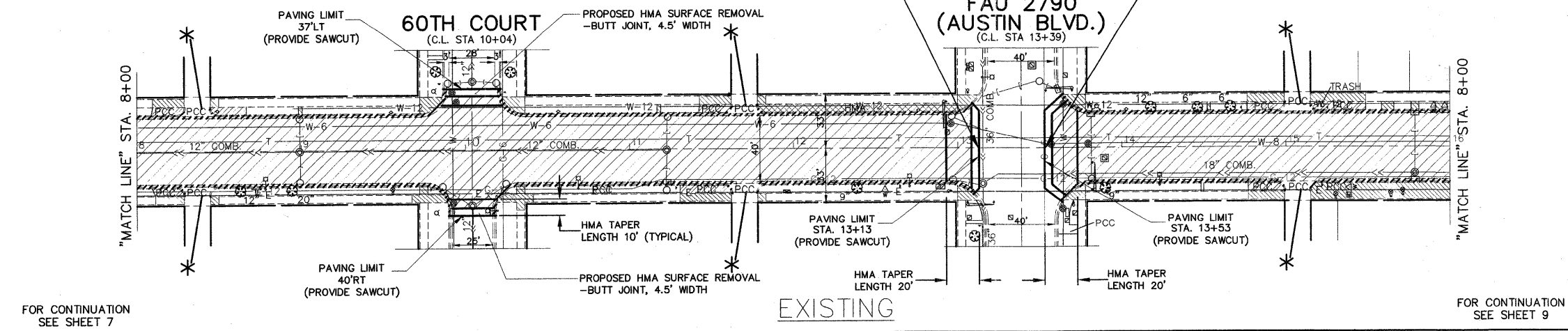
- REMOVAL LEGEND**
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - DENOTES DRIVEWAY PAVEMENT REMOVAL
 - DENOTES HOT-MIX ASPHALT SURFACE REMOVAL
 - DENOTES SIDEWALK REMOVAL
 - DENOTES COMBINATION CURB AND GUTTER REMOVAL
 - DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL"

BENCHMARKS
(U.S.G.S. DATUM)
NE BOLT ON FIRE HYDRANT AT NE CORNER OF 16TH STREET AND AUSTIN BLVD.
ELEV.=611.36

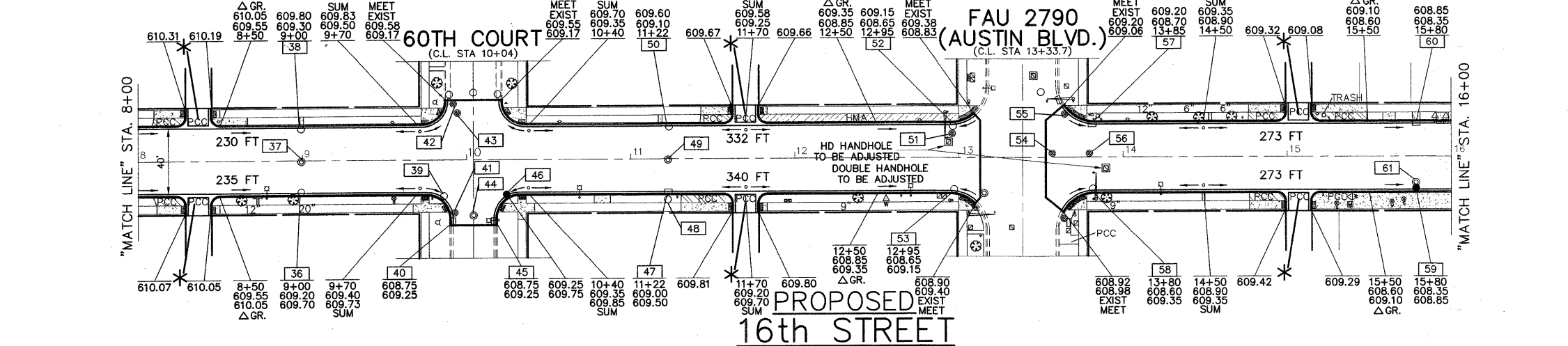


STRUCTURE SCHEDULE

- 36 CATCH BASIN TO BE RECONSTRUCTED, NEW TYPE 1 FR. & C.L. RIM=604.00 INV(NE)=598.01
- 37 MANHOLE TO BE RECONSTRUCTED, NEW TYPE 1 FR. & C.L. RIM=610.20 INV(N/S)=605.21 INV(E/W)=605.15
- 38 CATCH BASIN TO BE ADJUSTED RIM=609.36
- 39 CATCH BASIN TO BE RECONSTRUCTED RIM=609.20 INV(S)=606.50 INV(SE)=606.18
- 40 INLET TO BE ADJUSTED RIM=608.75
- 41 VALVE VAULT TO BE ADJUSTED RIM=608.30
- 42 VALVE VAULT TO BE ADJUSTED RIM=609.25
- 43 VALVE VAULT TO BE ADJUSTED RIM=609.30
- 44 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & C.L. RIM=609.65
- 45 INLET TO BE ADJUSTED RIM=608.75
- 46 PROPOSED CATCH BASIN, TYPE A, 4' DIA. TYPE 1 FR. & C.L., SPECIAL RIM=609.25 INV(S)=606.26 INV(SW)=605.25
- 47 INLET TO BE ADJUSTED RIM=609.00
- 48 CATCH BASIN TO BE ADJUSTED RIM=609.50
- 49 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM=609.90
- 50 CATCH BASIN TO BE ADJUSTED RIM=609.10
- 51 VALVE VAULT TO BE ADJUSTED RIM=609.10
- 52 CATCH BASIN TO BE ADJUSTED RIM=608.65
- 53 CATCH BASIN TO BE ADJUSTED RIM=608.65
- 54 VALVE VAULT TO BE ADJUSTED RIM=609.60
- 55 VALVE VAULT TO BE ADJUSTED RIM=609.00
- 56 VALVE VAULT TO BE ADJUSTED RIM=609.65
- 57 INLET TO BE ADJUSTED RIM=608.70
- 58 CATCH BASIN TO BE ADJUSTED RIM=608.60
- 59 PROPOSED CATCH BASIN, TYPE C, NEW TYPE 1 FR. & O.L., SPECIAL RIM=608.35 INV(N)=605.00
- 60 CATCH BASIN TO BE ADJUSTED RIM=608.35
- 61 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & C.L. RIM=608.75



- PROPOSED PAVEMENT LEGEND**
- DENOTES PROPOSED P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH
 - DENOTES PROPOSED INCIDENTAL HOT-MIX ASPHALT SURFACING
 - DENOTES PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
 - DENOTES P.C. CONCRETE SIDEWALK, 5 INCH WITH DETECTABLE WARNING STRIP WHERE APPLICABLE
 - DENOTES "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12" (DEPRESS AT SIDEWALK AND DRIVEWAYS)
 - DENOTES ALLEY RETURN TO BE REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8"



EXISTING @ ELEVATION	EXISTING @ PROFILE		PROPOSED @ PROFILE		EXISTING @ ELEVATION
	PROPOSED @ ELEVATION	STATION	PROPOSED @ ELEVATION	STATION	
610.41	610.41	8	610.34	8+50	610
610.34	610.34	9	610.20	9+00	608
610.20	610.20	10+04	610.15	10+40	606
610.00	610.00	11	610.00	11+22	
609.90	609.90	12	609.84	12+50	
609.84	609.84	13+13	609.78	13+53	
609.78	609.78	14	609.59	14+50	
609.59	609.59	15	609.46	15+50	
609.46	609.46	16	609.37	16+00	

NOTE:
ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4



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825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0192
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-00028

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
16th STREET- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 8+00 TO STA. 16+00

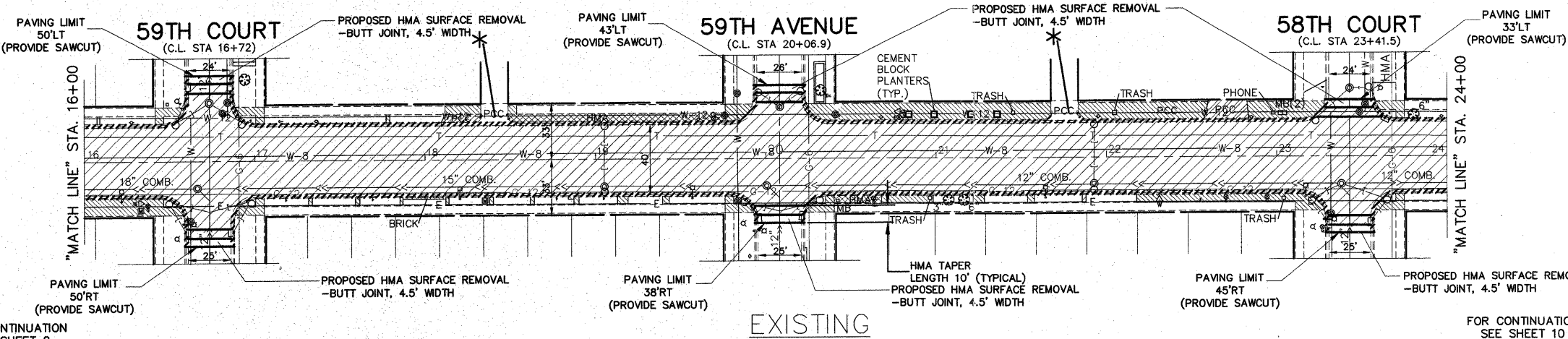
FAU R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	8
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			HPP-3045(003)	

FILE NAME = FAU 1447 (16TH STREET) LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)
10080 HPP RESURFACING

USER NAME =
DESIGNED - AMS
DRAWN - JFP-JEP
CHECKED - TPG
DATE - 3/11

REVISED - AMS 4-7-11
REVISED - AMS 4-22-11
REVISED -
REVISED -

- REMOVAL LEGEND**
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - DENOTES DRIVEWAY PAVEMENT REMOVAL
 - DENOTES HOT-MIX ASPHALT SURFACE REMOVAL
 - DENOTES SIDEWALK REMOVAL
 - DENOTES COMBINATION CURB AND GUTTER REMOVAL
 - DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL"



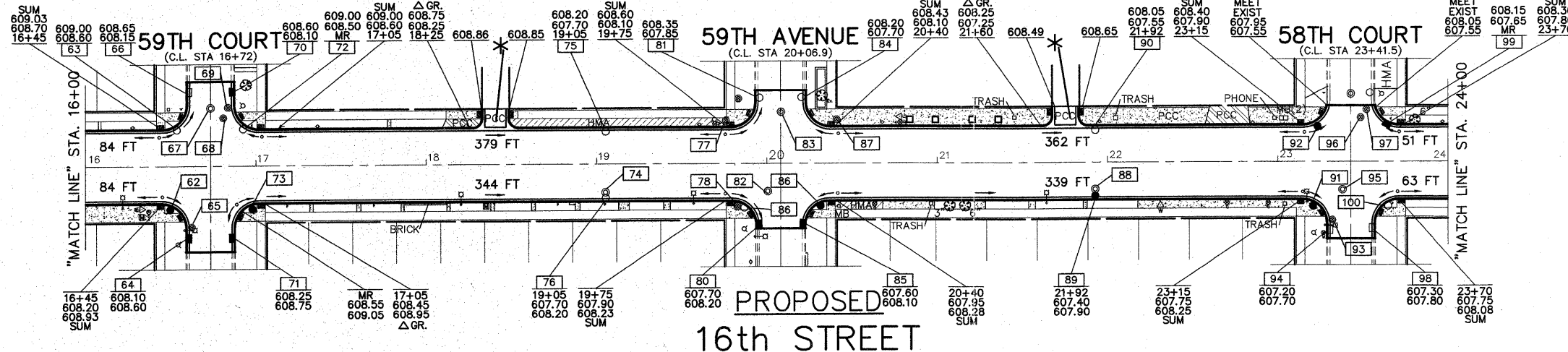
BENCHMARKS
(U.S.G.S. DATUM)
SW BOLT ON FIRE HYDRANT AT NW CORNER
OF 16TH STREET AND 59TH AVENUE
ELEV.=610.28

HORZ. 1"=40'
VERT. 1"=2'

STRUCTURE SCHEDULE

- [62] PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.80 INV(SE)=605.00 INV(E)=604.50
- [63] CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=608.80 INV(NE)=604.35
- [64] PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=608.10 INV(NW)=605.70
- [65] VALVE VAULT TO BE RECONSTRUCTED RIM=608.20
- [66] INLET TO BE ADJUSTED RIM=618.15
- [67] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.95
- [68] VALVE VAULT TO BE RECONSTRUCTED RIM=608.60
- [69] VALVE VAULT TO BE ADJUSTED RIM=608.30
- [70] PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=608.10 INV(SE)=605.70
- [71] PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=608.25 INV(NE)=605.85
- [72] PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.50 INV(N)=605.30 INV(NW)=605.05
- [73] PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=609.05 INV(SW)=605.25 INV(W)=605.05
- [74] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL RIM= 608.30
- [75] CATCH BASIN TO BE ADJUSTED RIM=607.70
- [76] CATCH BASIN TO BE ADJUSTED RIM=607.70
- [77] VALVE VAULT TO BE ADJUSTED NEW TYPE 1 FR. & C.L. RIM=608.20
- [78] VALVE VAULT TO BE RECONSTRUCTED NEW TYPE 1 FR. & C.L. RIM=608.10
- [79] CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=608.10 INV(S)=603.86 INV(SE)=603.60
- [80] PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=607.70 INV(NW)=605.20
- [81] CATCH BASIN TO BE ADJUSTED RIM=607.85
- [82] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.20
- [83] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.35
- [84] CATCH BASIN TO BE ADJUSTED RIM=607.70
- [85] PROPOSED INLET TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=607.60 INV(NE)=605.10
- [86] PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.00 INV(SW)=604.70 INV(W)=604.30
- [87] VALVE VAULT TO BE ADJUSTED NEW TYPE 1 FR. & C.L. RIM=608.45
- [88] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 607.75
- [89] PROPOSED CATCH BASIN, TYPE C, TYPE 1 FR. & O.L., SPECIAL RIM=607.40 INV(N)=604.40
- [90] CATCH BASIN TO BE ADJUSTED RIM=607.55
- [91] PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.30 INV(SE)=604.20 INV(E)=604.00
- [92] PROPOSED CATCH BASIN TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=607.90 INV(NE)=604.40
- [93] VALVE VAULT TO BE ADJUSTED RIM=607.25
- [94] INLET TO BE ADJUSTED RIM=607.20
- [95] MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.05
- [96] VALVE VAULT TO BE ADJUSTED NEW TYPE 1 FR. & C.L. RIM=607.95
- [97] VALVE VAULT TO BE ADJUSTED RIM=607.75
- [98] INLET TO BE ADJUSTED RIM=607.30
- [99] PROPOSED INLET, TYPE A, TYPE 1 FR. & O.L., SPECIAL RIM=607.65 INV(N)=605.45
- [100] CATCH BASIN TO BE ADJUSTED RIM=608.10

- PROPOSED PAVEMENT LEGEND**
- DENOTES PROPOSED P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH
 - DENOTES PROPOSED INCIDENTAL HOT-MIX ASPHALT SURFACING
 - DENOTES PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
 - DENOTES P.C. CONCRETE SIDEWALK, 5 INCH WITH DETECTABLE WARNING STRIP WHERE APPLICABLE
 - DENOTES "COMBINATION CONCRET CURB AND GUTTER, TYPE B-V.12" (DEPRESS AT SIDEWALK AND DRIVEWAYS)
 - DENOTES ALLEY RETURN TO BE REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8"



EXISTING ELEVATION	PROPOSED ELEVATION	STATION	EXISTING Q. PROFILE	PROPOSED Q. PROFILE
610	609.28	16		
608	609.26	+72		
606	609.14	17		
	609.02	18		
	608.79	19		
	608.73	20+06.9		
	608.71	21		
	608.75	22		
	608.65	23		
	608.52	24		
	608.47			
	608.35			
	608.34			
	608.53			
	608.50			
	608.50			

NOTE:
ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION
OF NEW PAVEMENT WORK
SEE SHEET 4



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ILLINOIS PROFESSIONAL DESIGN PERM NO. 134-000828

FILE NAME = FAU 1447 (16TH STREET) LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-7-11
10080 HPP RESURFACING	PLOT SCALE =	DRAWN - JFP-JEP	REVISED - AMS 4-22-11
	PLOT DATE =	CHECKED - TPG	REVISED -
		DATE - 3/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

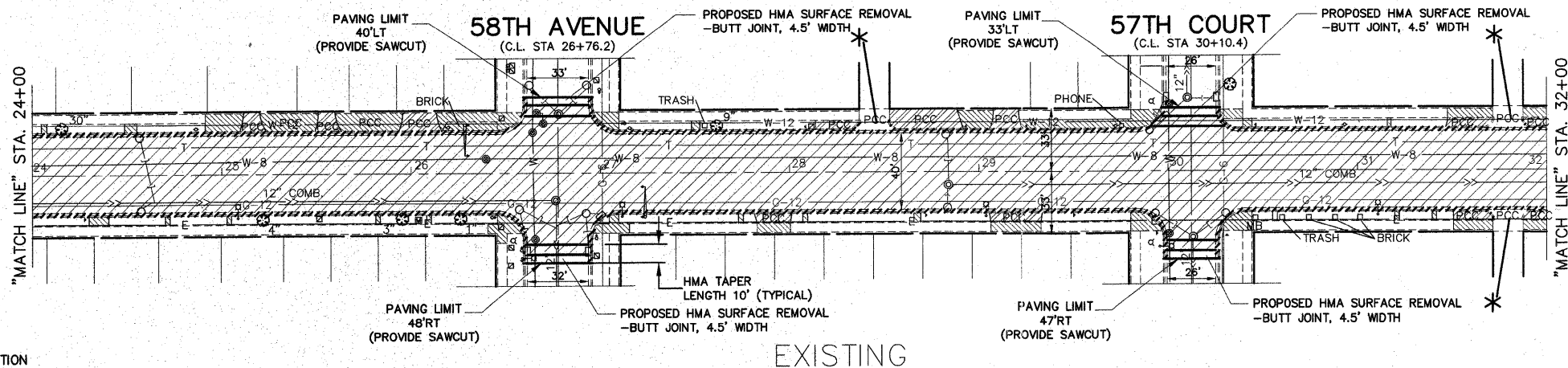
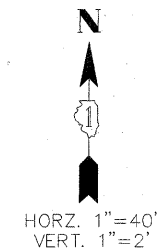
PLAN & PROFILE:
16th STREET - (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED SHEET NO. OF SHEETS STA. 16+00 TO STA. 24+00

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 9
CONTRACT NO. 63607			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)	

- REMOVAL LEGEND**
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - DENOTES DRIVEWAY PAVEMENT REMOVAL
 - DENOTES HOT-MIX ASPHALT SURFACE REMOVAL
 - DENOTES SIDEWALK REMOVAL
 - DENOTES COMBINATION CURB AND GUTTER REMOVAL
 - DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL"

BENCHMARKS

(U.S.G.S. DATUM)
SSW BOLT ON FIRE HYDRANT AT NW CORNER OF 16TH STREET AND 57TH COURT
ELEV.=609.31



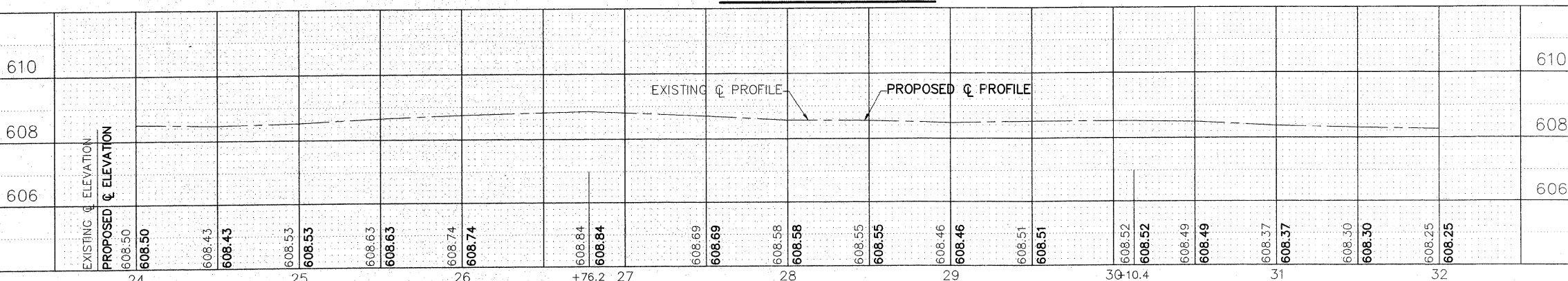
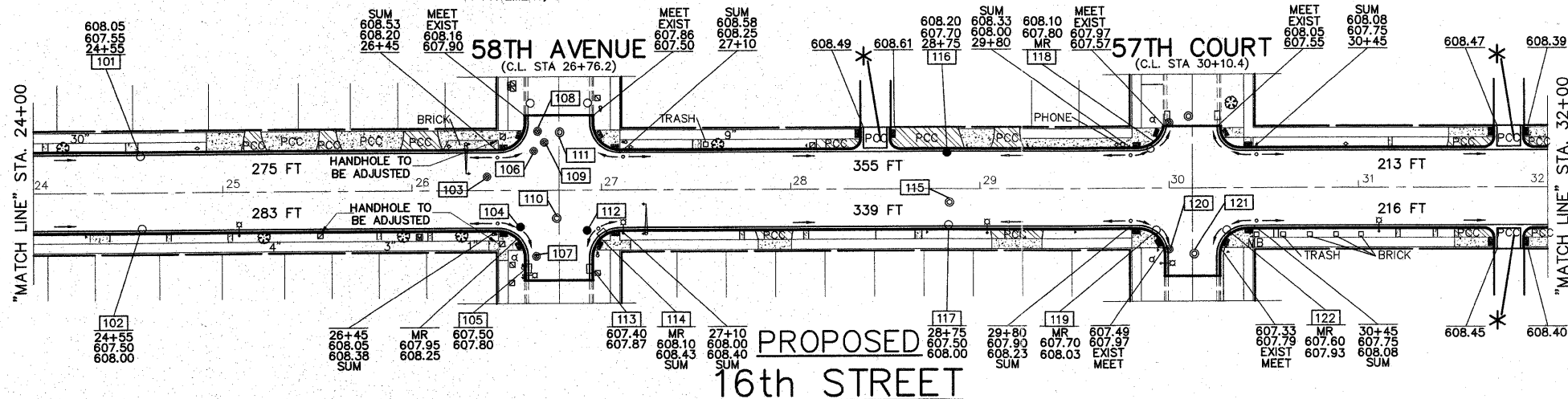
FOR CONTINUATION SEE SHEET 9

FOR CONTINUATION SEE SHEET 11

STRUCTURE SCHEDULE

- 101 CATCH BASIN TO BE ADJUSTED RIM=607.55
- 102 CATCH BASIN TO BE ADJUSTED RIM=607.50
- 103 VALVE VAULT TO BE ADJUSTED RIM=608.65
- 104 PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.45 INV(S)=605.00 INV(SE)=604.25
- 105 INLET TO BE ADJUSTED RIM=607.50
- 106 VALVE VAULT TO BE ADJUSTED RIM=608.30
- 107 VALVE VAULT TO BE ADJUSTED NEW TYPE 1 FR. & C.L. RIM=608.05
- 108 VALVE VAULT TO BE ADJUSTED RIM=608.15
- 109 VALVE VAULT TO BE ADJUSTED RIM=608.20
- 110 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.40
- 111 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL RIM= 608.45
- 112 PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=608.20 INV(S)=604.90 INV(W)=604.50
- 113 INLET TO BE ADJUSTED RIM=607.40
- 114 INLET TO BE ADJUSTED RIM=608.10
- 115 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL NEW TYPE 1 FR. & O.L. RIM= 608.50
- 116 PROPOSED CATCH BASIN, TYPE A, 4' DIA., TYPE 1 FR. & C.L., SPECIAL RIM=607.70 INV(S)=603.20
- 117 CATCH BASIN TO BE ADJUSTED RIM=607.50
- 118 CATCH BASIN TO BE ADJUSTED RIM=607.80
- 119 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=607.70 INV(SE)=603.50
- 120 VALVE VAULT TO BE ADJUSTED NEW TYPE 1 FR. & C.L. RIM=607.60
- 121 MANHOLE, FRAME AND LID TO BE ADJUSTED SPECIAL RIM= 607.80
- 122 CATCH BASIN TO BE RECONSTRUCTED NEW TYPE 1 FR. & O.L. RIM=607.60 INV(SW)=603.40

- PROPOSED PAVEMENT LEGEND**
- DENOTES PROPOSED P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH
 - DENOTES PROPOSED INCIDENTAL HOT-MIX ASPHALT SURFACING
 - DENOTES PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
 - DENOTES P.C. CONCRETE SIDEWALK, 5 INCH WITH DETECTABLE WARNING STRIP WHERE APPLICABLE
 - DENOTES "COMBINATION CONCRET CURB AND GUTTER, TYPE B-V.12" (DEPRESS AT SIDEWALK AND DRIVEWAYS)
 - DENOTES ALLEY RETURN TO BE REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8"

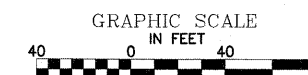


NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4



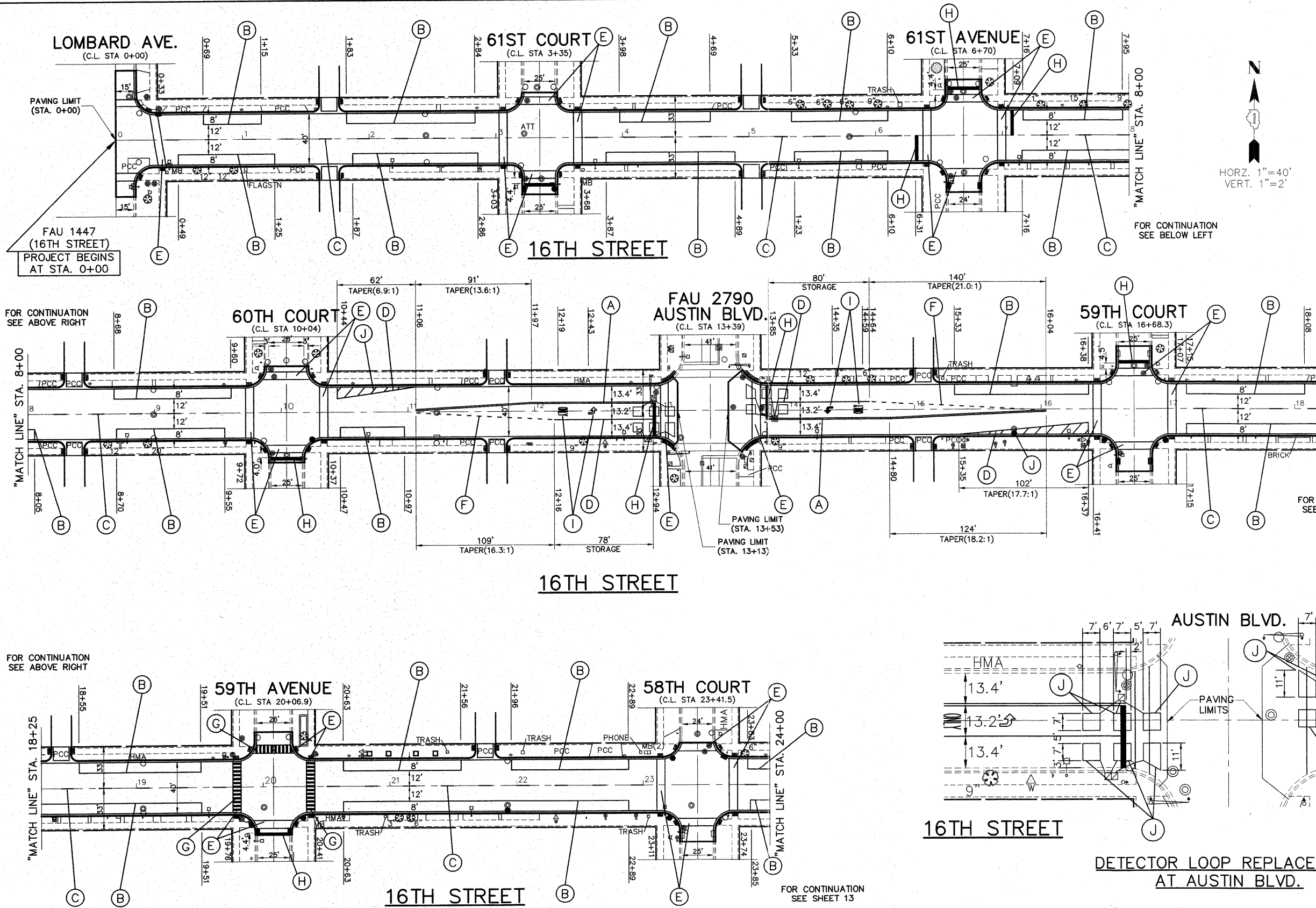
Frank Novotny & Associates, Inc.
825 Midway Drive • Willowbrook, IL • 60187 • Telephone: (630) 887-8840 • Fax: (630) 887-4132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-000288

FILE NAME = FAU 1447 (16TH STREET) LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)	USER NAME =	DESIGNED -- AMS	REVISED -- AMS 4-7-11
10080 HPP RESURFACING	PLOT SCALE =	DRAWN -- JFP-JEP	REVISED -- AMS 4-22-11
	PLOT DATE =	CHECKED -- TPG	REVISED --
		DATE -- 3/11	REVISED --

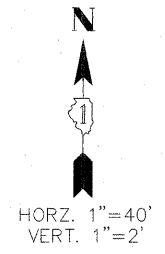
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
16th STREET- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 24+00 TO STA. 32+00

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 10
CONTRACT NO. 63607			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)	



- THERMOPLASTIC STRIPING CODE**
- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C
 - (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" - (PARKING LINE)
 - (C) PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" - 10' DASH, 30' SKIP (LANE LINE)
 - (D) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
 - (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK LINE)
 - (F) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
 - (G) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12" (PEDESTRIAN SCHOOL CROSSING)
 - (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
 - (I) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
 - (J) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12" (45° DIAGONAL, 8' APART)
- DETECTOR LOOPS**
- (K) PROPOSED DETECTOR LOOP REPLACEMENT

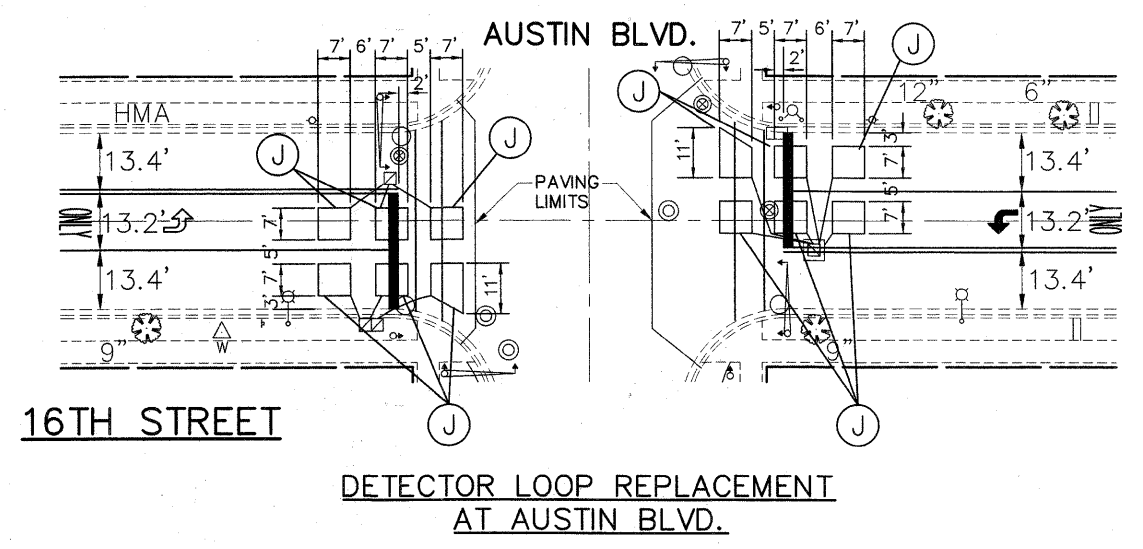


NOTE:
ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8" IN HEIGHT.

NOTE:
PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C (NOTE: (A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE:
SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTE:
PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



Frank Novotny & Associates, Inc.
Civil Engineer
Illinois Professional Design Firm No. 184-000828

FILE NAME = FAU 1447 (16TH STREET) LOMBARD AVE. TO FAU 2790 (CENTRAL AVE.)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-22-11
10080 HPP RESURFACING	PLOT SCALE =	DRAWN - JFP-JEP	REVISED -
	PLOT DATE =	CHECKED - TPG	REVISED -
		DATE - 3/11	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN & PROFILE:
16th STREET - (EXISTING AND PROPOSED ROADWAY PAVEMENT MARKING)

SCALE: AS NOTED SHEET NO. OF SHEETS STA. 0+00 TO STA. 24+00

F.A.U. RT. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 12
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63607	
HPP-3045(003)				

THERMOPLASTIC STRIPING CODE

- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C
- (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" - (PARKING LINE)
- (C) PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" - 10' DASH, 30' SKIP (LANE LINE)
- (D) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
- (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK LINE)
- (F) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
- (G) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12" (PEDESTRIAN SCHOOL CROSSING)
- (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
- (I) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
- (J) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12" (45° DIAGONAL, 8' APART)

DETECTOR LOOPS

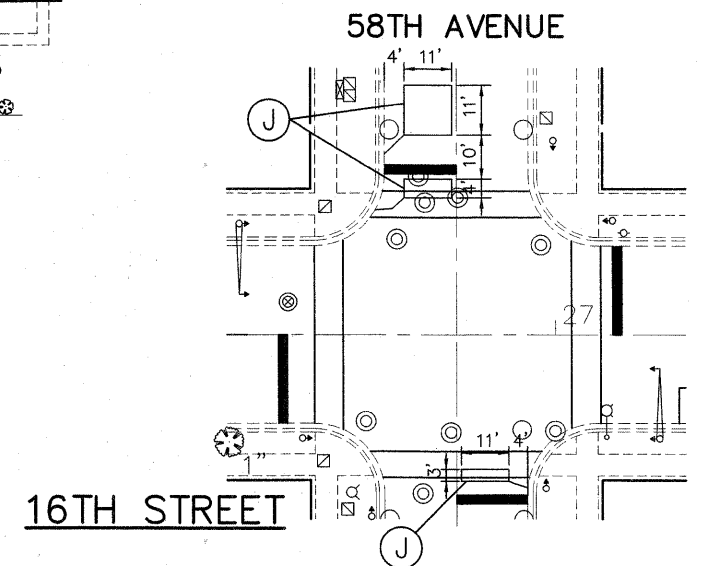
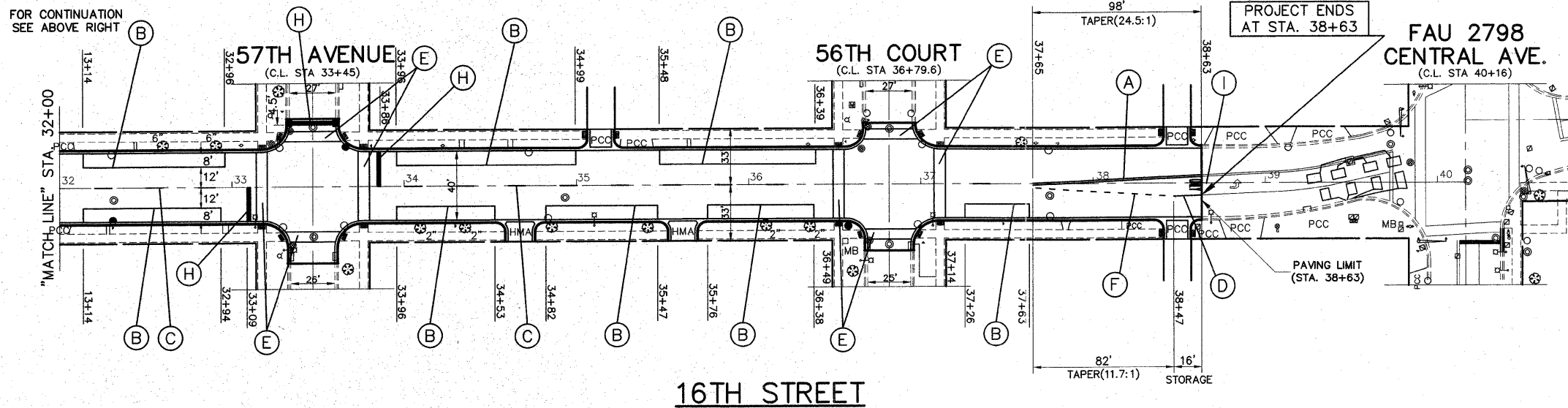
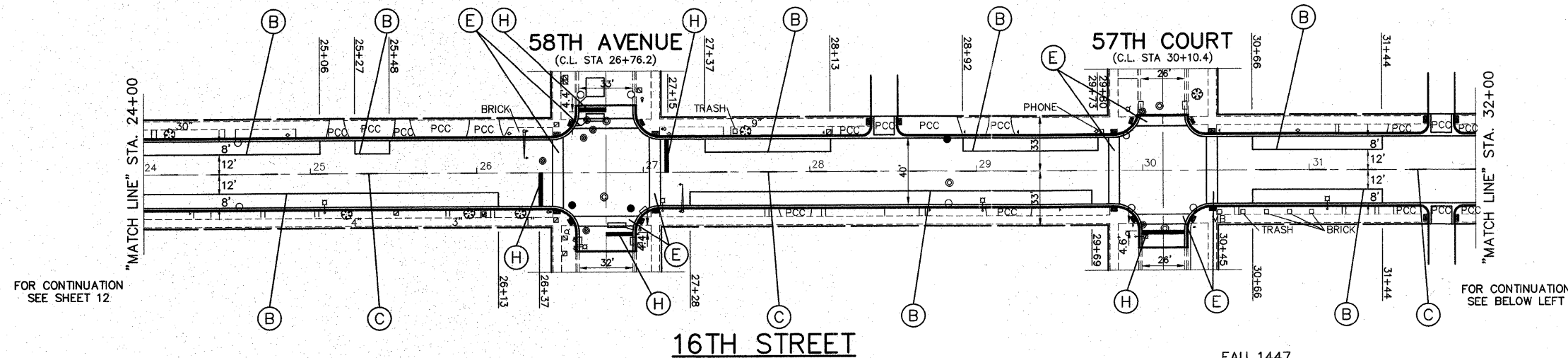
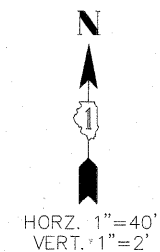
- (K) PROPOSED DETECTOR LOOP REPLACEMENT

NOTE: ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8" IN HEIGHT.

NOTE: PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C (NOTE: (A) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE: SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

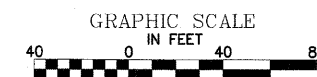
NOTE: PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.



DETECTOR LOOP REPLACEMENT AT 58TH AVENUE

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



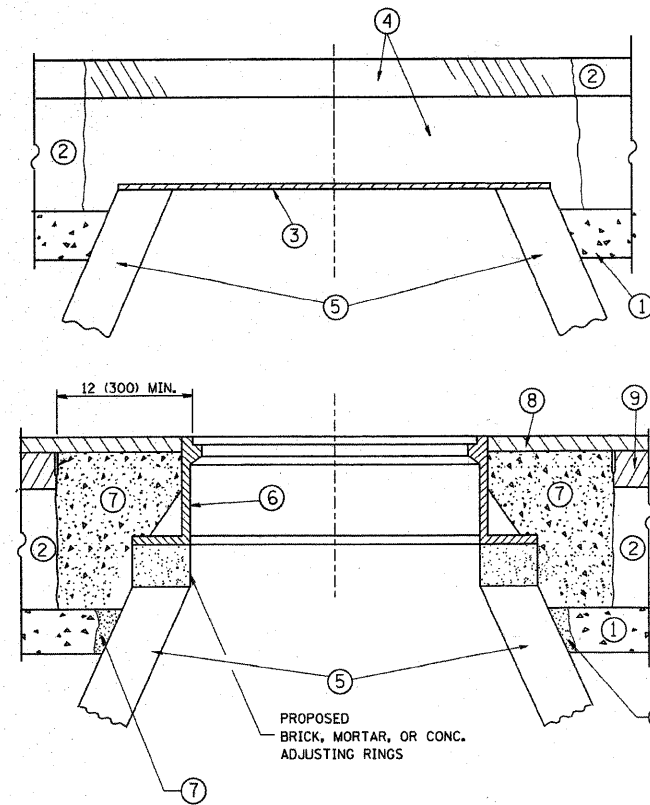
Frank Novotny & Associates, Inc.
 826 Midway Drive • Willowbrook, IL • 60227 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-000288

FILE NAME = FAU 1447 (16TH STREET) LOMBARD AVE. TO FAU 2798 (CENTRAL AVE.)	USER NAME =	DESIGNED - AMS	REVISED - AMS 4-22-11
10080 HPP RESURFACING	PLOT SCALE =	DRAWN - JFP-JEP	REVISED -
	PLOT DATE =	CHECKED - TPG	REVISED -
		DATE - 3/11	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
16th STREET - (EXISTING AND PROPOSED ROADWAY PAVEMENT MARKING)
 SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 24+00 TO STA. 38+63

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 13
CONTRACT NO. 63607				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-3045(003)				



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

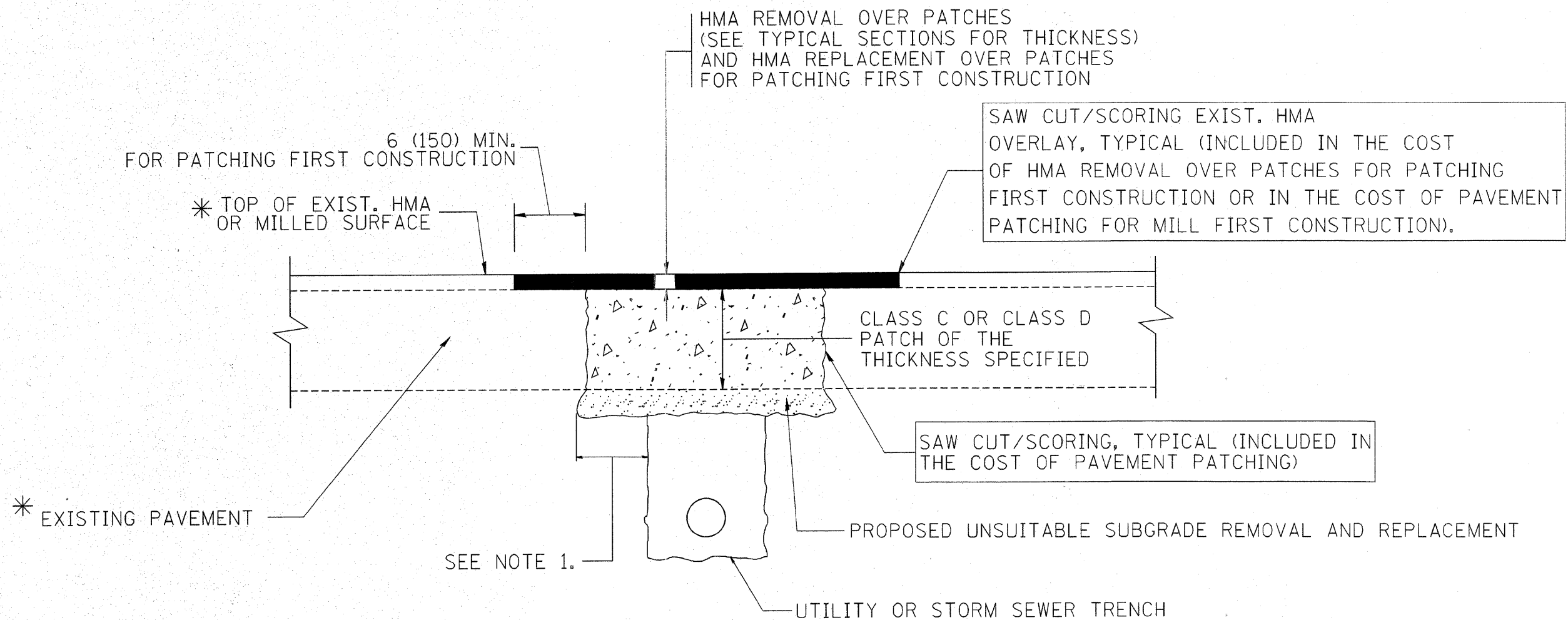
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	PLOT DATE = 3/18/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	14
BD600-03 (BD-9)		CONTRACT NO. 63607		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

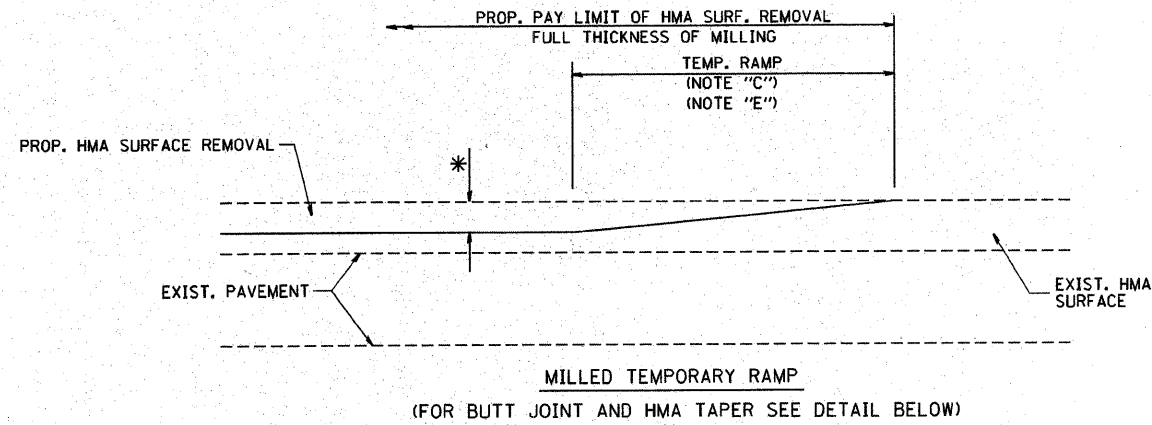
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

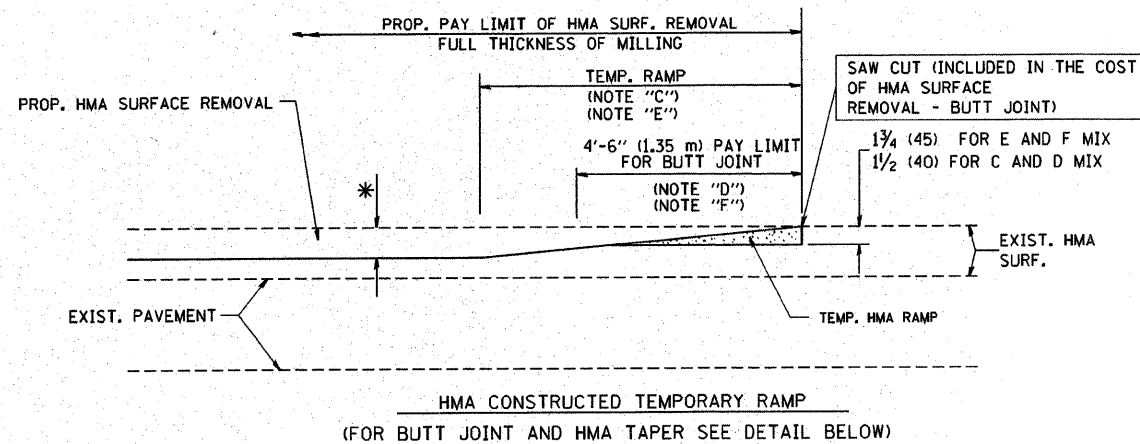
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

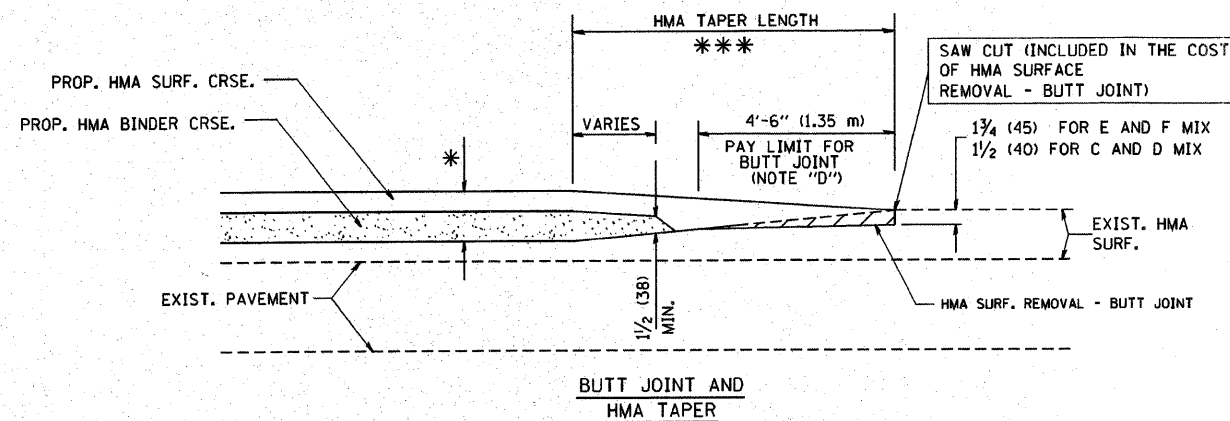
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		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63607		
		PLOT SCALE = 50,000' / IN.	CHECKED -		REVISED - R. BORO 09-04-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)						
		PLOT DATE = 10/27/2008	DATE = 10-25-94		REVISED - K. ENG 10-27-08							



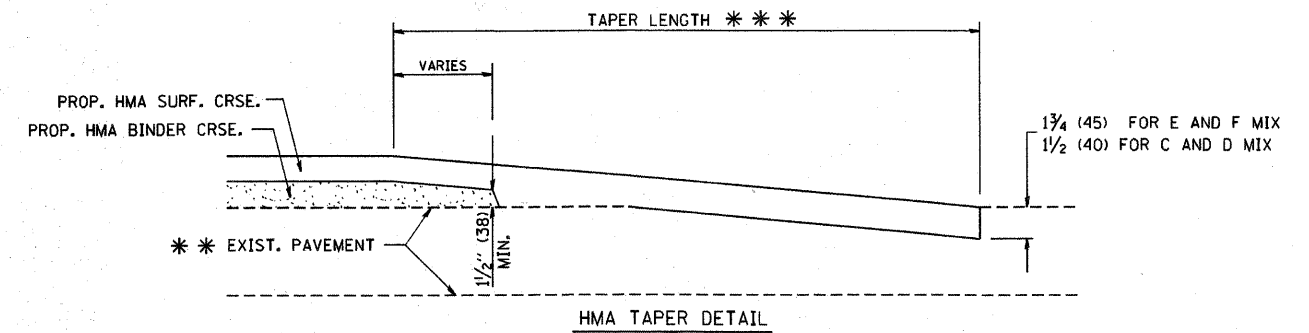
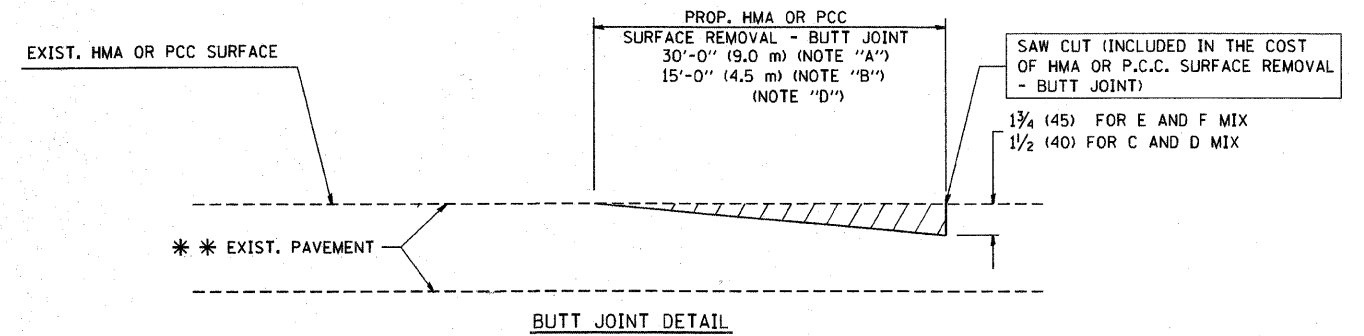
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

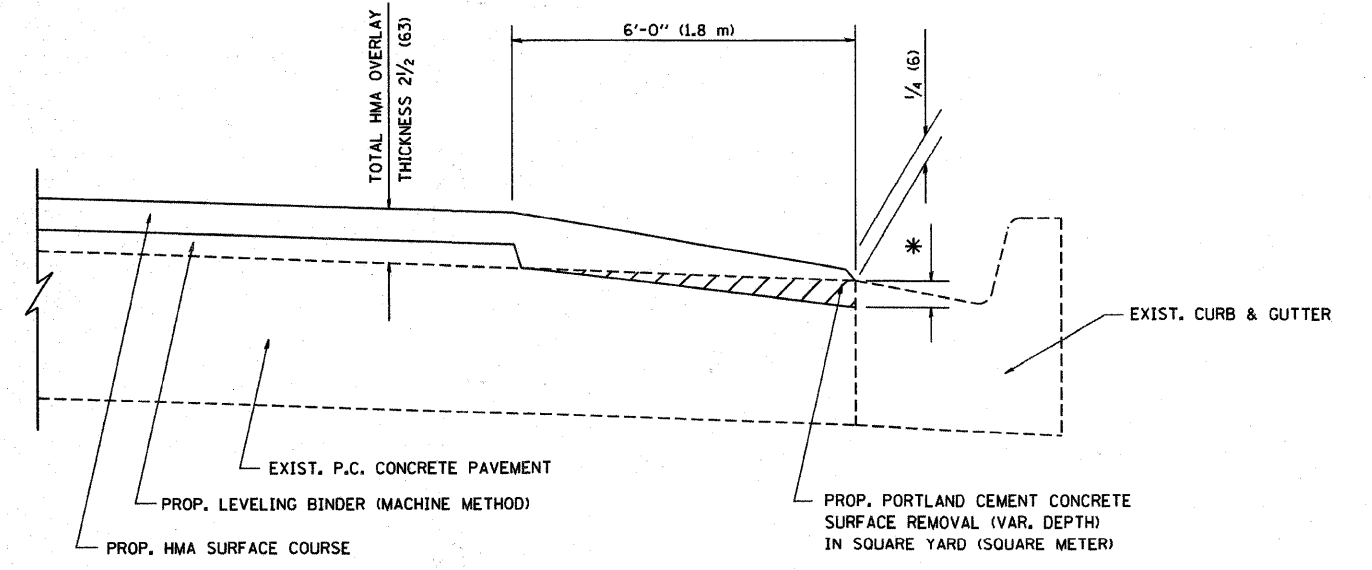
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2000	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	16
BD400-05 BD32		CONTRACT NO. 63607		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				



**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

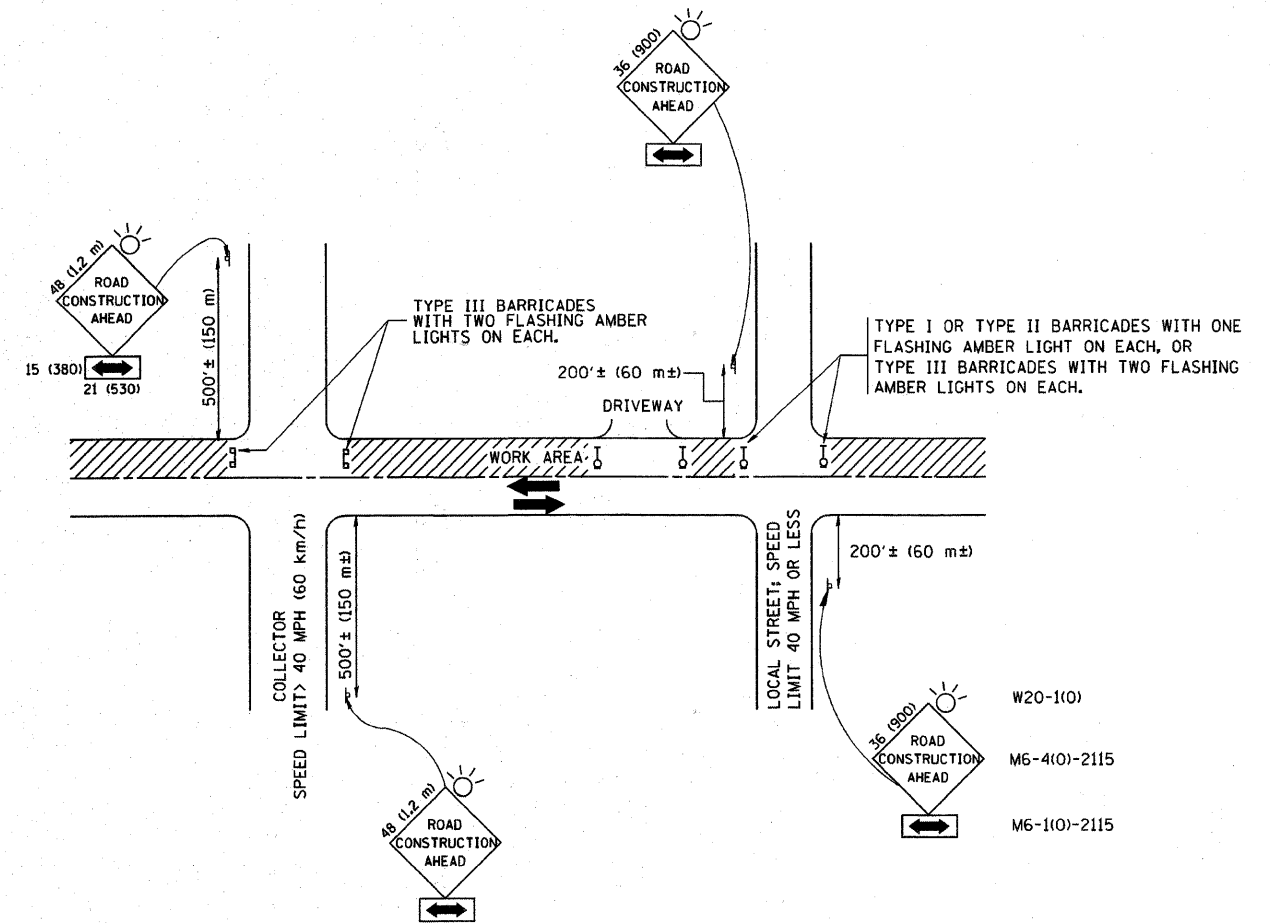
HMA SURFACE MIX	LEVELING BINDER		* MILLING AT GUTTER FLAG
	THICKNESS	THICKNESS	
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
F	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 1/4/2000	DATE - 09-10-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT		F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 17
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63607		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)						



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

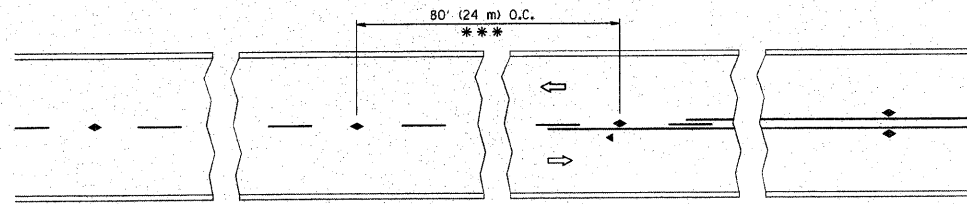
All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
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	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

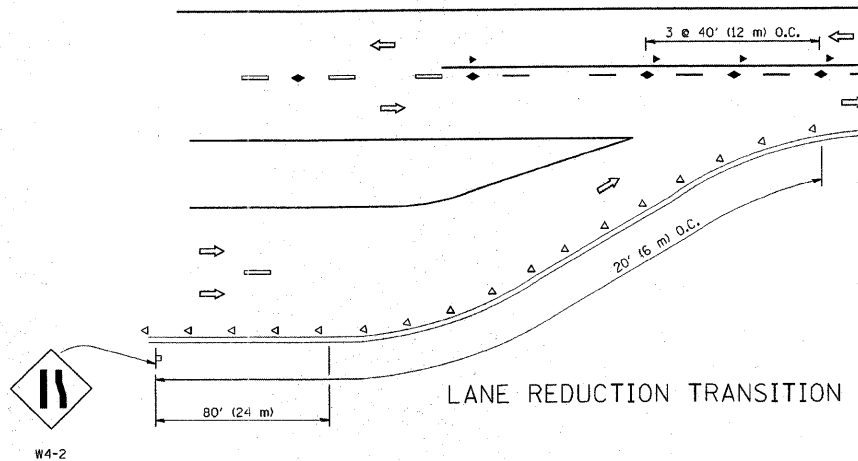
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	18
TC-10			CONTRACT NO. 63607	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				

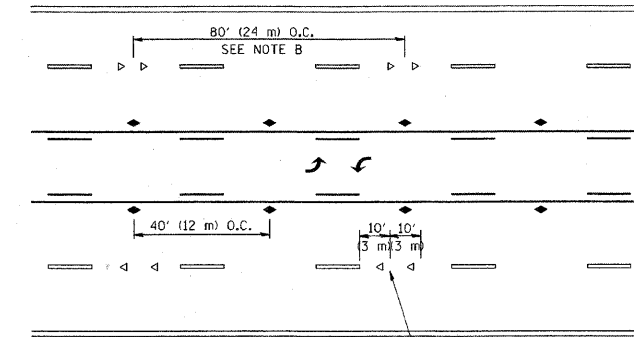


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

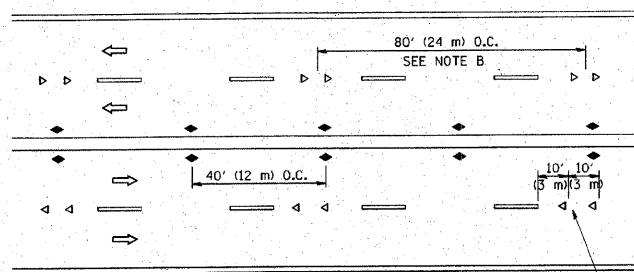
TWO-LANE/TWO-WAY



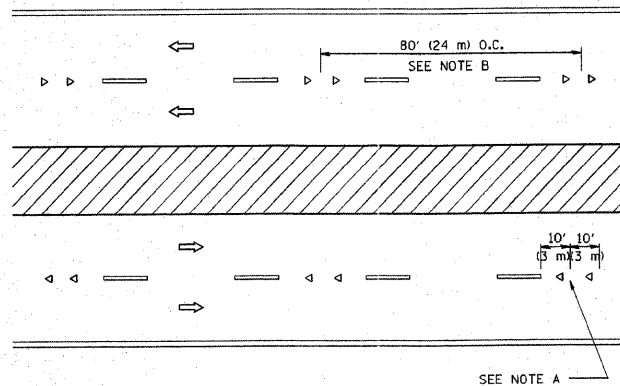
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

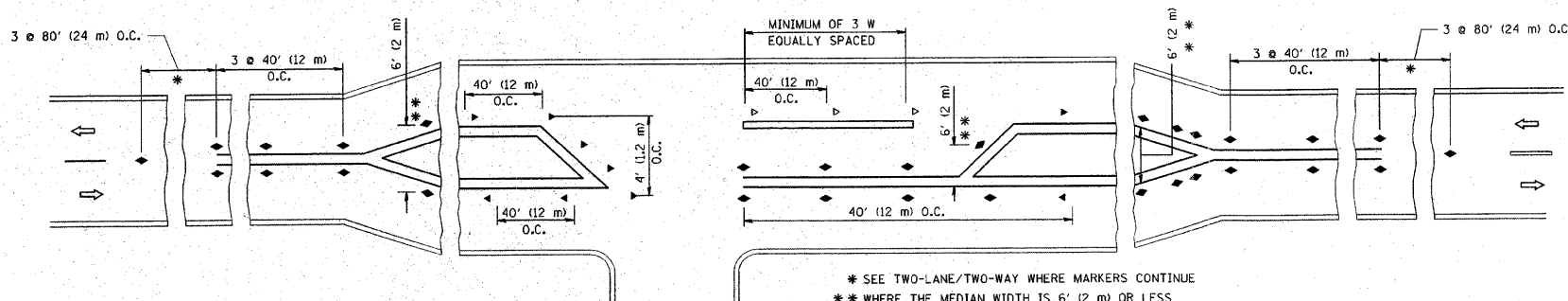
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

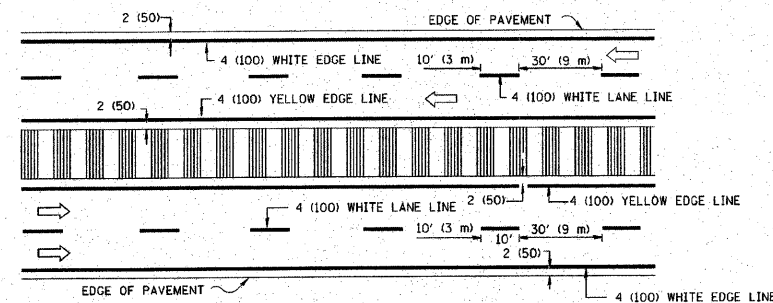
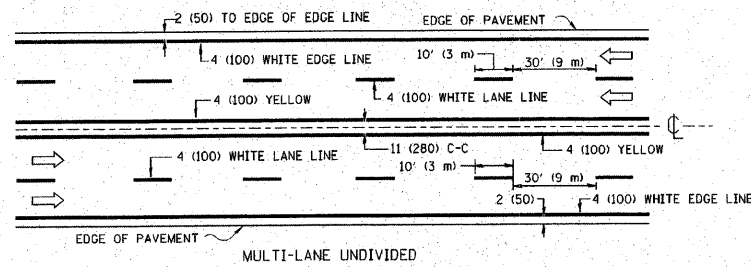
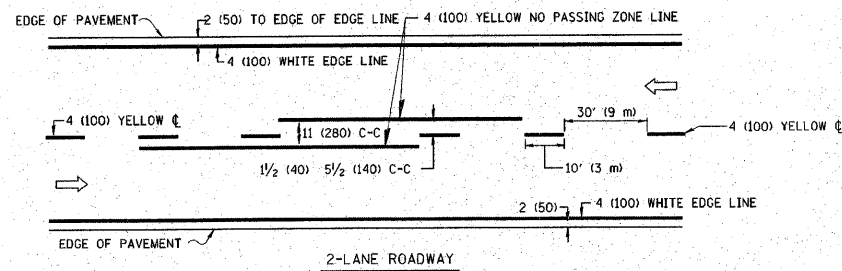
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	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

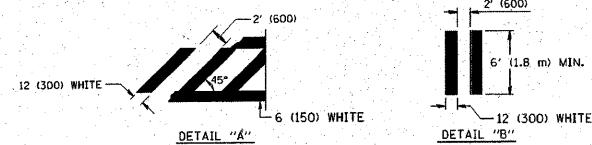
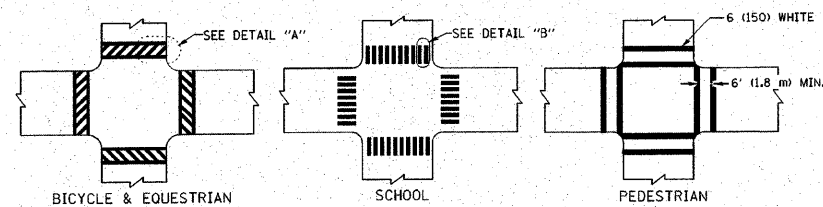
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	19
TC-11			CONTRACT NO. 63607	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				

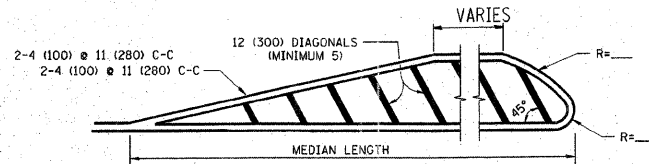
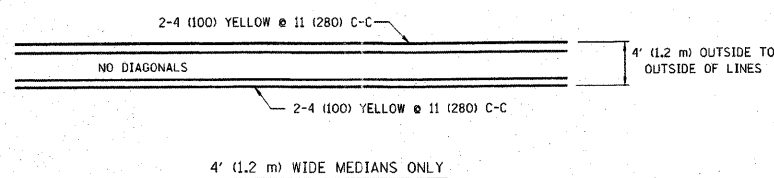


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

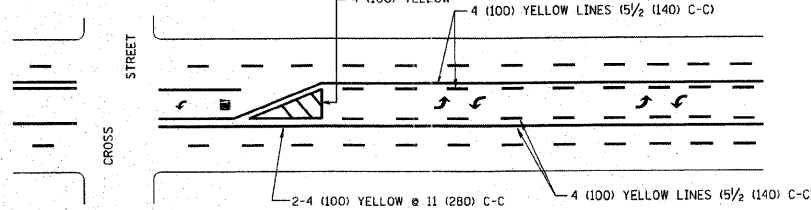


TYPICAL CROSSWALK MARKING

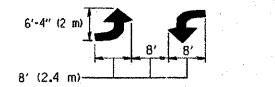


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

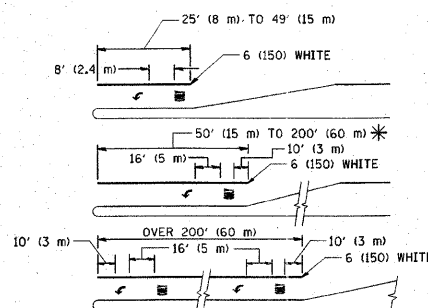


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

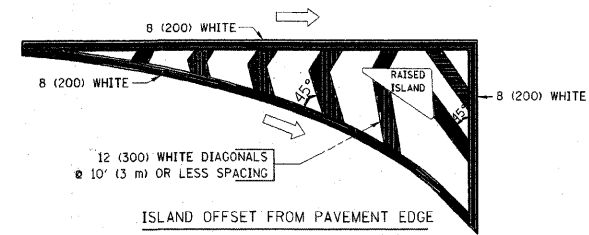


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²); ONLY AREA = 20.8 SQ. FT. (1.9 m²)

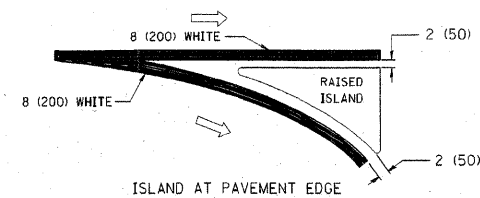
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 130 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

	LARGE SIZE	SMALL SIZE
THROUGH ARROW	1.07 (11.5)	0.60 (6.5)
LEFT OR RIGHT ARROW	1.47 (15.6)	0.60 (6.5)
COMBINATION LEFT (RIGHT) AND THROUGH ARROW	2.42 (26.0)	1.37 (14.7)
RAILROAD "R" 1.8m (6ft.)	0.33 (3.6)	—
RAILROAD "X" 6.1m (20ft.)	5.02(54.0)	—
HANDICAPPED SYMBOL	0.56 (6.0)	—

All dimensions are in inches (millimeters) unless otherwise shown.

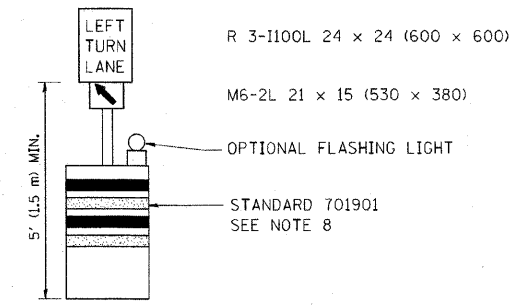
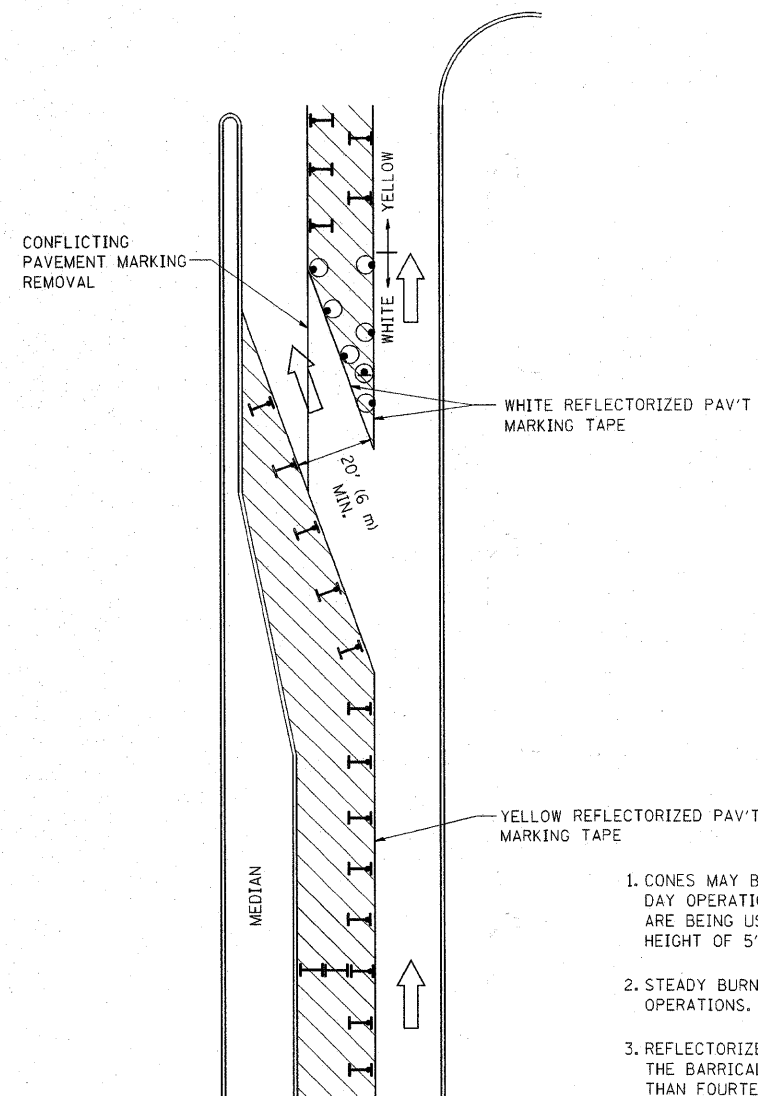
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PLOT DATE = 9/9/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	20
TC-13		CONTRACT NO. 63607		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				

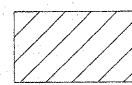







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

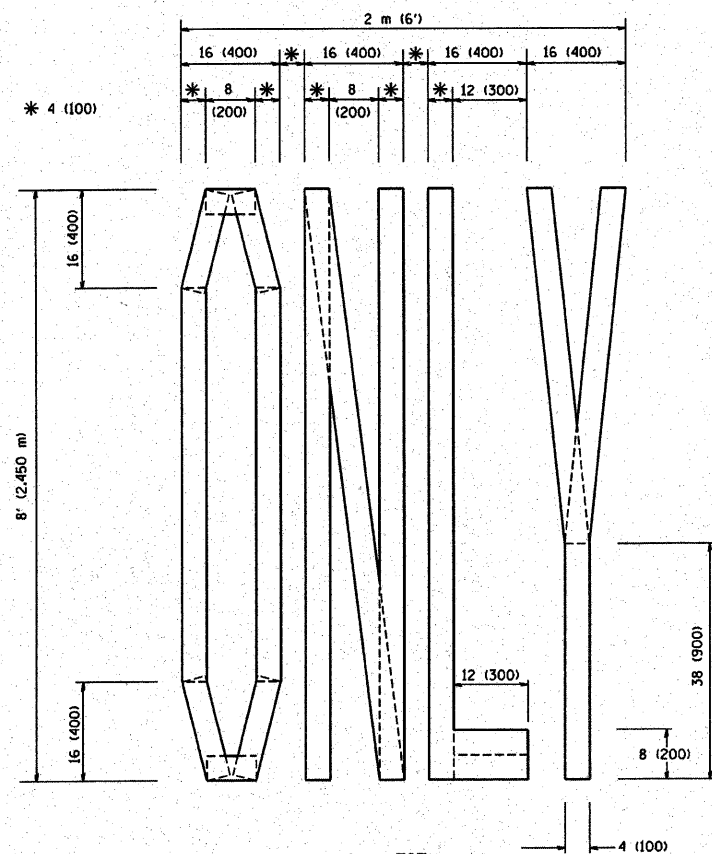
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

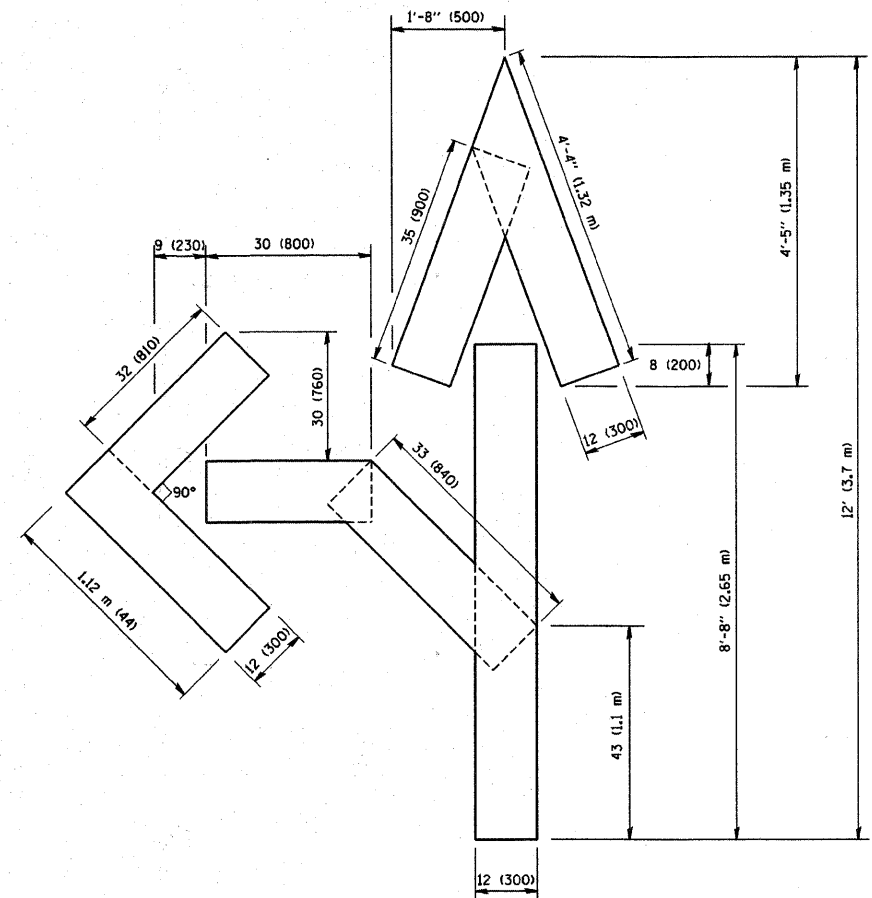
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

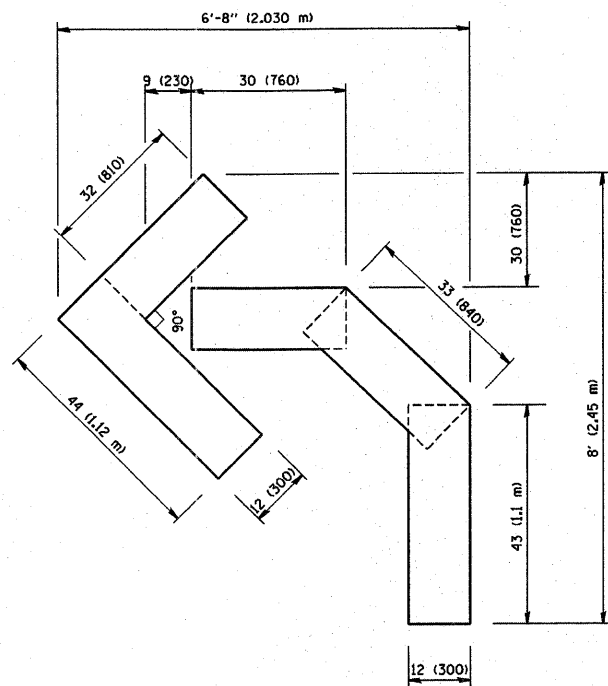
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	21
TC-14			CONTRACT NO. 63607	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T, RAMMACHER 11-04-97
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

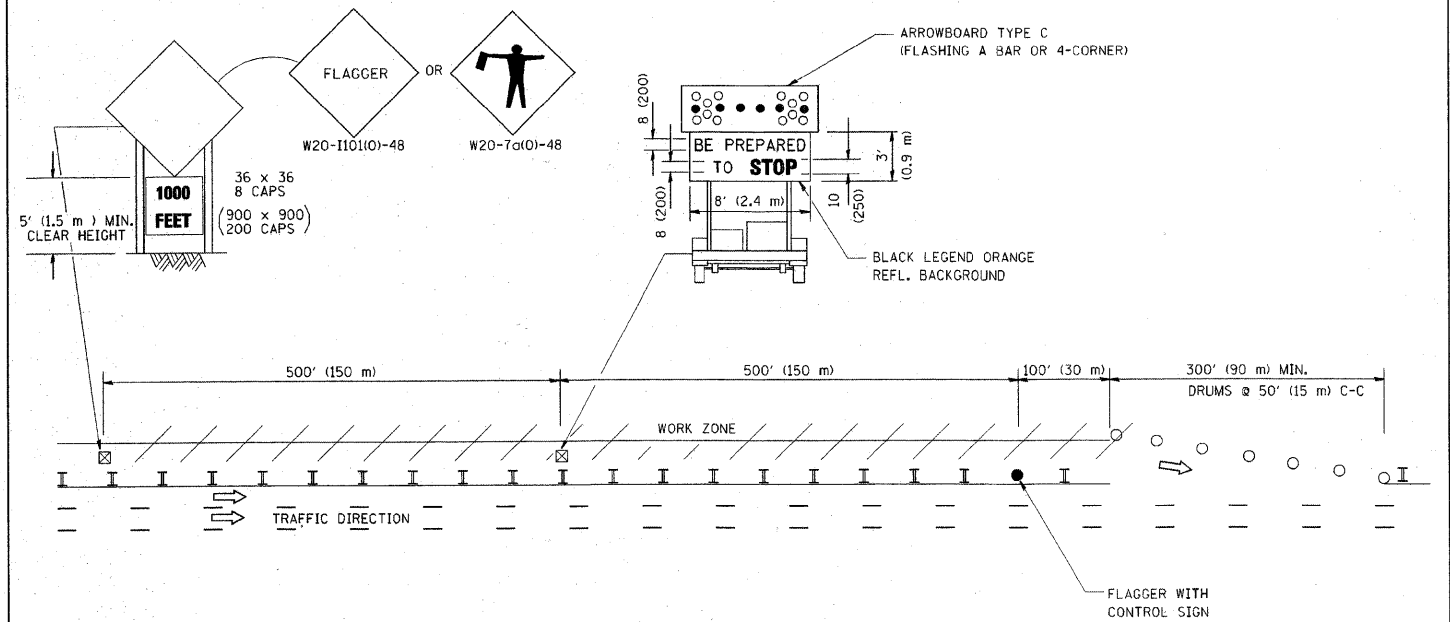
PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

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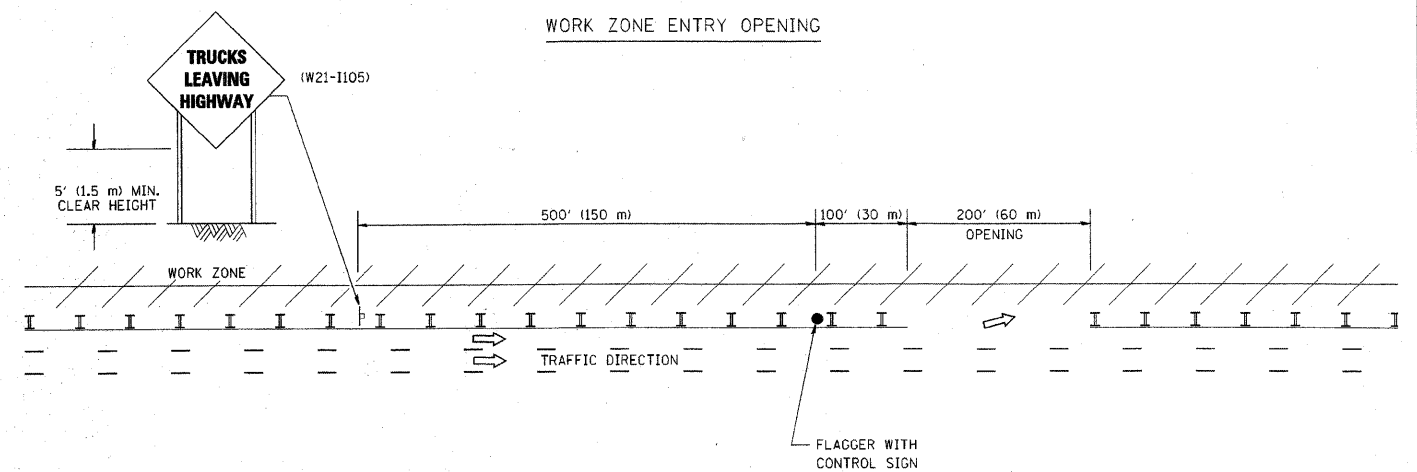
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TC-16			CONTRACT NO. 63607	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

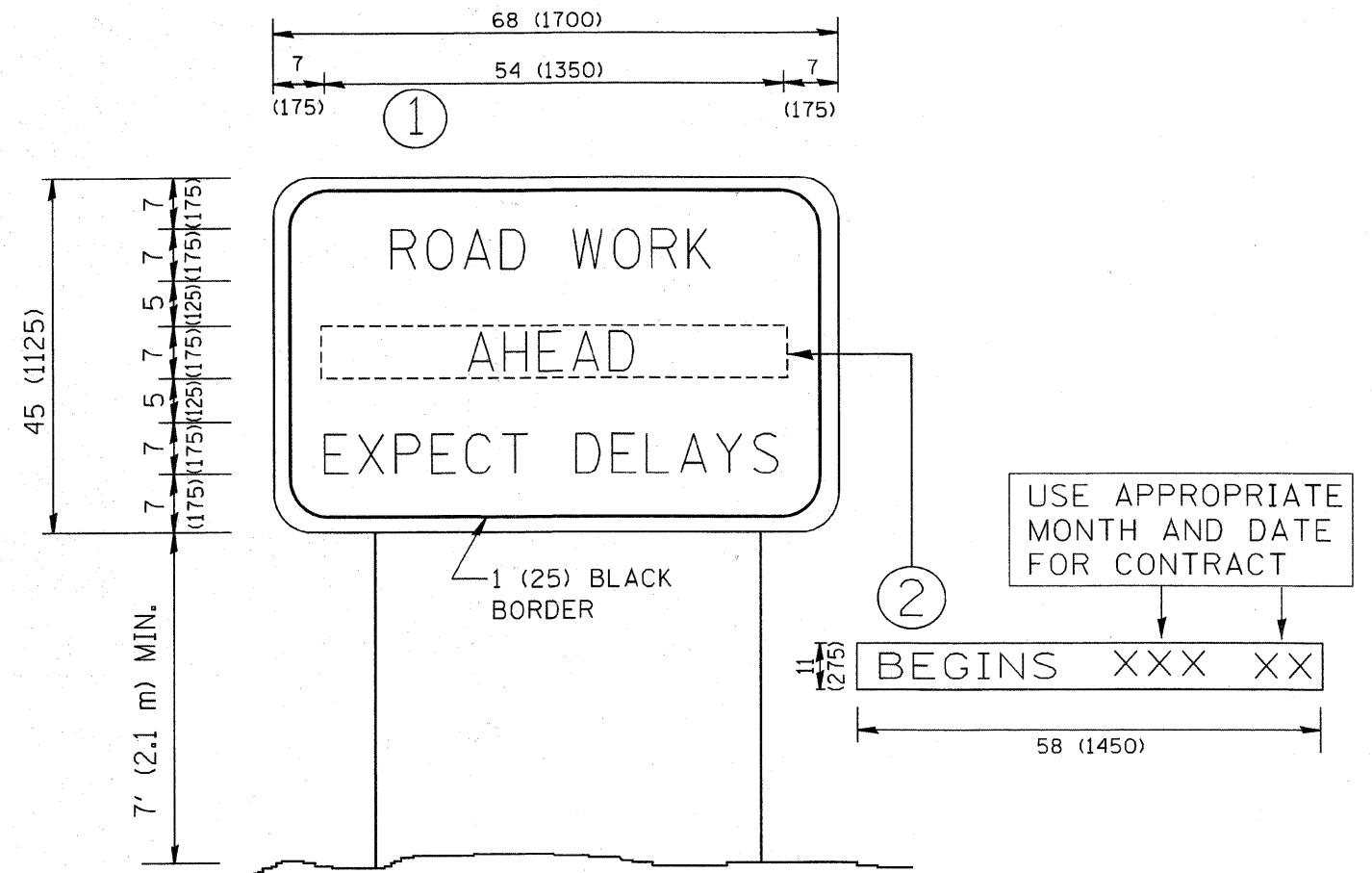
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		PLOT DATE = 1/26/2010	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1447	SECTION 11-00213-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 23
TC-18		CONTRACT NO. 63607		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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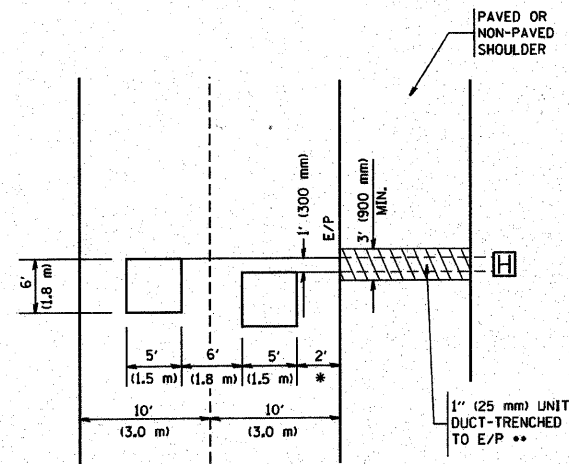
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1447	11-00213-00-RS	COOK	25	24
TC-22			CONTRACT NO. 63607	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

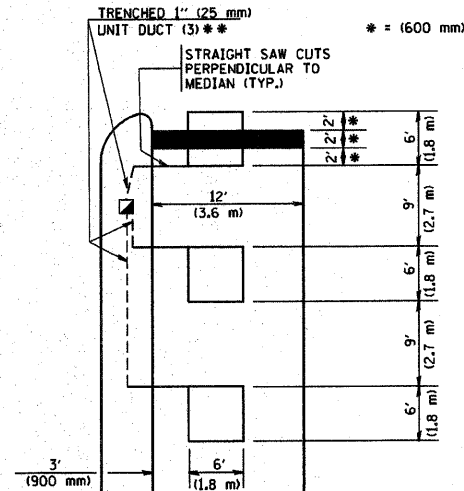


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

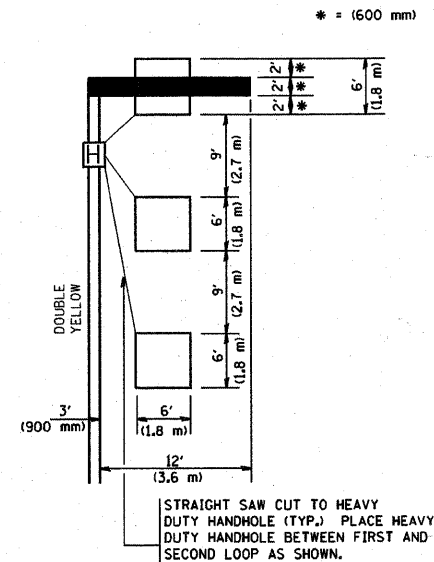
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

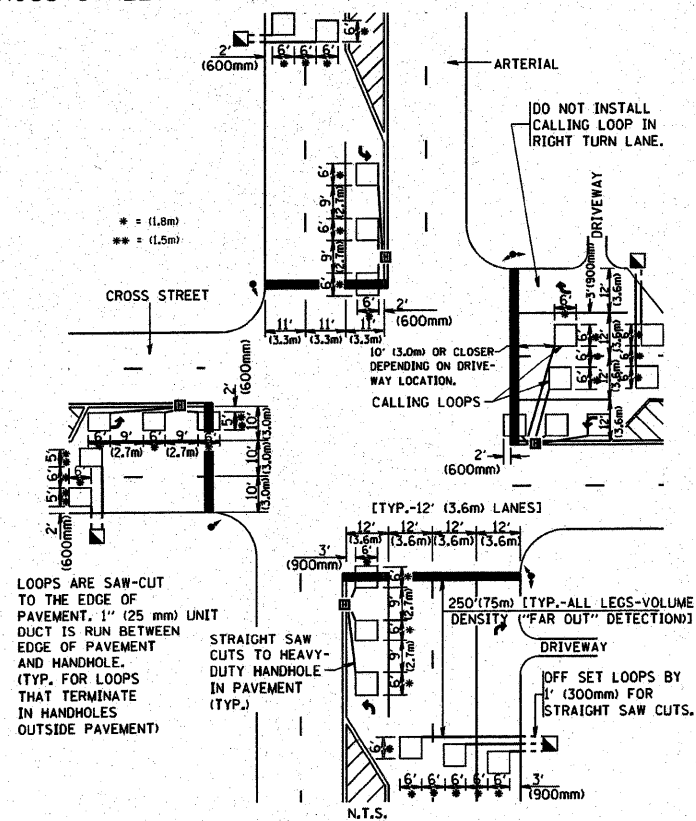
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



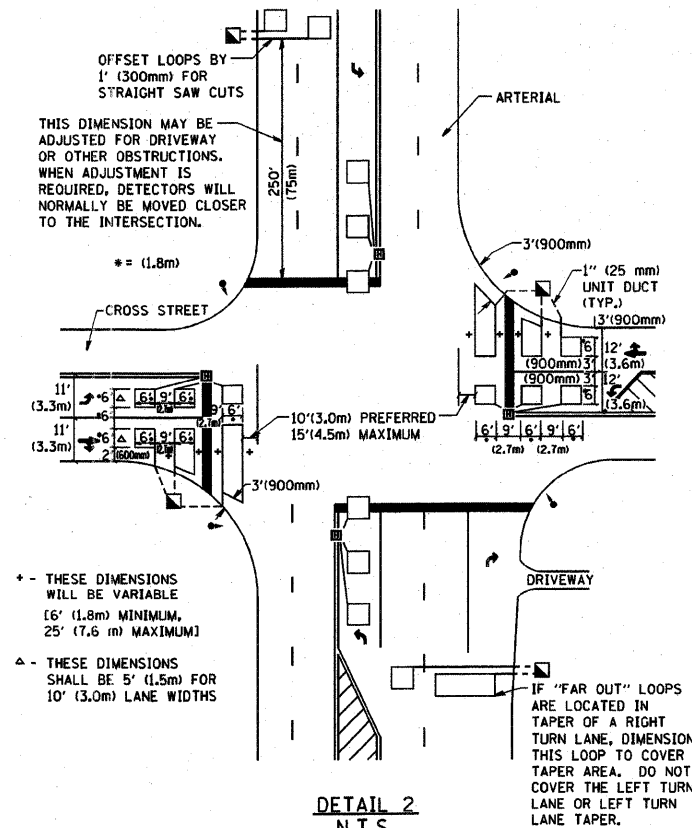
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	DATE -	REVISED -			1447	11-00213-00-RS	COOK	25	25
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -			TS-07		CONTRACT NO. 63607		
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-3045(003)