


# VILLAGE OF LANSING LANSING, ILLINOIS

## CONSTRUCTION PLANS FOR LANSING MUNICIPAL AIRPORT


### REHABILITATE RUNWAY 18/36, TAXIWAY B AND TAXIWAY G

ILLINOIS PROJECT: IGQ-4941  
 SBGP PROJECT: 3-17-SBGP-XXX


JUNE 10, 2022


 **Lansing Municipal Airport**

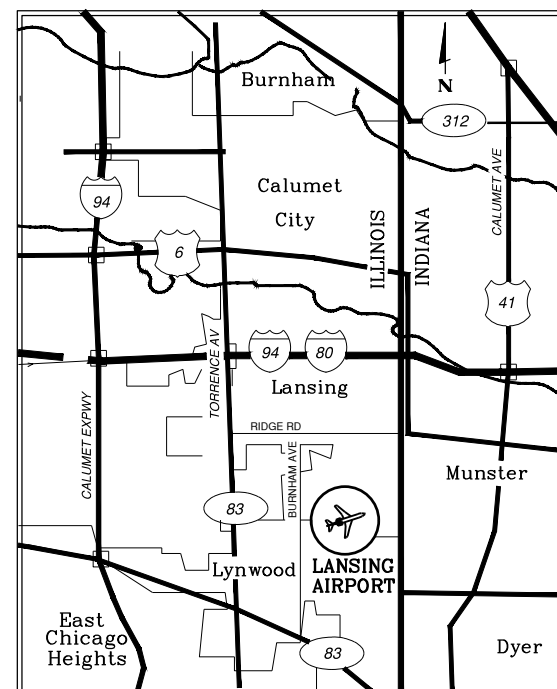
APPROVED BY   
 DANIEL PODGORSKI  
 TITLE CITY ADMINISTRATOR  
 DATE JUNE 7 20 22

 **CMT**  
 CRAWFORD MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS

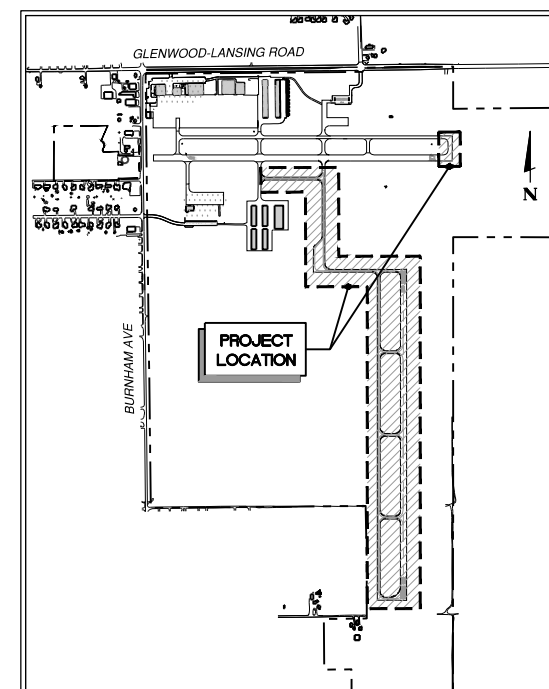
20092279.00

  
 LICENSE EXPIRATION DATE: 11-30-2023


SUBMITTED BY   
 ANTONIO R. MARIN, P.E.  
 DATE JUNE 10 20 22



LOCATION MAP



SITE PLAN

 Know what's below.  
 Call before you dig.

J.U.L.I.E.  
 JOINT UTILITY LOCATING  
 INFORMATION FOR EXCAVATORS  
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

<b>DESIGN INFORMATION</b>	
APPROACH CATEGORY B DESIGN GROUP II	
<b>LANSING MUNICIPAL AIRPORT</b>	
TOWNSHIP: 35 NORTH RANGE: 15 EAST COOK COUNTY	BLOOM TOWNSHIP (SECTION: 8 AND 17) OPPOSITE GLENWOOD-LANSING ROAD
<b>UNICOM RADIO FREQUENCY - 122.7</b>	



CONSULTANTS

**INDEX TO SHEETS**

SHEET NUMBER	SHEET DESIGNATOR	SHEET TITLE
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3	CS100	SITE PLAN - PROJECT CONTROL
4	GC100	CONSTRUCTION ACTIVITY PLAN - 1
5	GC101	CONSTRUCTION ACTIVITY PLAN - 2
6	GC102	CONSTRUCTION ACTIVITY PLAN - 3
7	GC103	CONSTRUCTION ACTIVITY PLAN GENERAL NOTES AND DETAILS
8	ED100	EXISTING CONDITIONS/REMOVALS - 1
9	ED101	EXISTING CONDITIONS/REMOVALS - 2
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14	GC300	TYPICAL SECTIONS
15	CS200	PLAN AND PROFILE RUNWAY 18-36 - 1
16	CS201	PLAN AND PROFILE RUNWAY 18-36 - 2
17	CS202	PLAN AND PROFILE RUNWAY 18-36 - 3
18	CS203	PLAN AND PROFILE TAXIWAY G
19	CS204	PLAN AND PROFILE TAXIWAY D
20	CS205	RUNWAY AND TAXIWAY GRADE TABLES - 1
21	CS206	RUNWAY AND TAXIWAY GRADE TABLES - 2
22	CM100	PAVEMENT MARKING PLAN - 1
23	CM101	PAVEMENT MARKING PLAN - 2
24	CM102	PAVEMENT MARKING PLAN - 3
25	CM103	PAVEMENT MARKING PLAN - 4
26	GC115	PAVEMENT MARKING DETAILS
27	GV100	GROOVING PLAN AND MISCELLANEOUS DETAILS
28	GC500	TAXIWAY A IMPROVEMENTS - ADDITIVE ALTERNATE NO. 1

**SUMMARY OF QUANTITIES**

SUMMARY OF QUANTITIES (BASE BID)				
ITEM	DESCRIPTION	UNIT	PLAN QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	23,300	
AR201663	SAND MIX CRACK REPAIR	FOOT	1,000	
AR401610	BITUMINOUS SURFACE COURSE	TON	8,200	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	24,350	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	65,394	
AR401655	BUTT JOINT CONSTRUCTION	SQ YD	852	
AR401662	SURFACE COURSE CRACK REPAIR	SQ YD	1,300	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SQ YD	485	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	144	
AR403673	REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	1,300	
AR603510	BITUMINOUS TACK COAT	GALLON	8,300	
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	22,500	
AR620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	16,200	
AR705945	ADJUST COLLECTION STRUCTURE	EACH	6	
AR901510	SEEDING	ACRE	0.50	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	0.50	

SUMMARY OF QUANTITIES (ADDITIVE ALTERNATE NO. 1)				
ITEM	DESCRIPTION	UNIT	PLAN QUANTITY	RECORD QUANTITY
AS150520	MOBILIZATION	L SUM	1	
AS152410	UNCLASSIFIED EXCAVATION	CU YD	440	
AS152540	SOIL STABILIZATION FABRIC	SQ YD	830	
AS208515	POROUS GRANULAR EMBANKMENT	CU YD	280	
AS209607	CRUSHED AGGREGATE BASE COURSE - 7"	SQ YD	830	
AS401610	BITUMINOUS SURFACE COURSE	TON	200	
AS401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	800	
AS602510	BITUMINOUS PRIME COAT	GAL	250	
AS603510	BITUMINOUS TACK COAT	GAL	50	
AS620520	PAVEMENT MARKING - WATERBORNE	SQ FT	300	
AS620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	475	
AS705524	4" PERFORATED UNDERDRAIN W/ SOCK	FOOT	290	
AS705544	4" NON PERFORATED UNDERDRAIN	FOOT	56	
AS705900	REMOVE UNDERDRAIN	FOOT	350	
AS705945	ADJUST COLLECTION STRUCTURE	EACH	2	

FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-XXX

IL PROJ. NO. IGQ-4941

CMT PROJECT NO: 200922-79-00

CAD DWG FILE: SUMMARY OF QUANTITIES.DWG

DESIGNED BY: ARM

DRAWN BY: JRO

CHECKED BY:

APPROVED BY:

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SHEET TITLE

INDEX TO SHEETS/  
SUMMARY OF  
QUANTITIES

GI101  
SHEET 2 OF 28



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FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

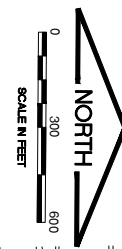
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AIP PROJ. NO. 3-17-SBGP-XXX
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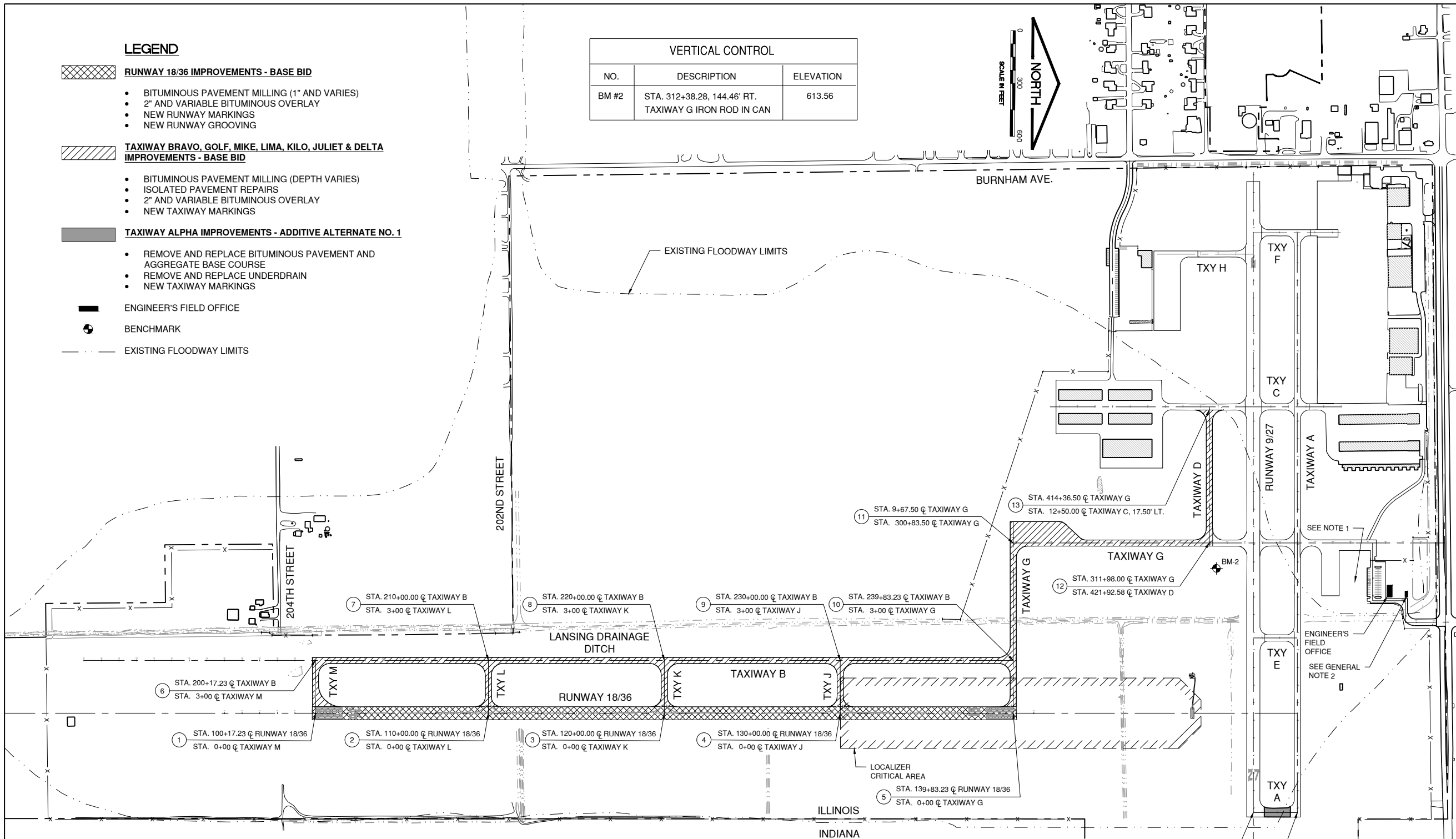
SHEET TITLE

SITE PLAN - PROJECT  
CONTROL

VERTICAL CONTROL		
NO.	DESCRIPTION	ELEVATION
BM #2	STA. 312+38.28, 144.46' RT. TAXIWAY G IRON ROD IN CAN	613.56



- LEGEND**
- RUNWAY 18/36 IMPROVEMENTS - BASE BID**
    - BITUMINOUS PAVEMENT MILLING (1" AND VARIES)
    - 2" AND VARIABLE BITUMINOUS OVERLAY
    - NEW RUNWAY MARKINGS
    - NEW RUNWAY GROOVING
  - TAXIWAY BRAVO, GOLF, MIKE, LIMA, KILO, JULIET & DELTA IMPROVEMENTS - BASE BID**
    - BITUMINOUS PAVEMENT MILLING (DEPTH VARIES)
    - ISOLATED PAVEMENT REPAIRS
    - 2" AND VARIABLE BITUMINOUS OVERLAY
    - NEW TAXIWAY MARKINGS
  - TAXIWAY ALPHA IMPROVEMENTS - ADDITIVE ALTERNATE NO. 1**
    - REMOVE AND REPLACE BITUMINOUS PAVEMENT AND AGGREGATE BASE COURSE
    - REMOVE AND REPLACE UNDERDRAIN
    - NEW TAXIWAY MARKINGS
  - ENGINEER'S FIELD OFFICE
  - BENCHMARK
  - EXISTING FLOODWAY LIMITS



**HORIZONTAL CONTROL (NAD 27)**

PT	DESCRIPTION	NORTHING	EASTING
1	RUNWAY 18/36 AND TAXIWAY M	1770576.574	720618.747
2	RUNWAY 18/36 AND TAXIWAY L	1771559.051	720612.071
3	RUNWAY 18/36 AND TAXIWAY K	1772559.028	720605.276
4	RUNWAY 18/36 AND TAXIWAY J	1773559.005	720598.481
5	RUNWAY 18/36 AND TAXIWAY G	1774542.482	720591.798
6	TAXIWAY B AND TAXIWAY M	1770574.536	720318.754
7	TAXIWAY B AND TAXIWAY L	1771557.013	720312.078
8	TAXIWAY B AND TAXIWAY K	1772556.990	720305.282
9	TAXIWAY B AND TAXIWAY J	1773556.967	720298.487
10	TAXIWAY B AND TAXIWAY G	1774540.444	720291.804
11	TAXIWAY G AND TAXIWAY G	1774535.908	719624.320
12	TAXIWAY G AND TAXIWAY D	1775650.381	719616.747
13	TAXIWAY G AND TAXIWAY C	1775645.243	718860.666
14	RUNWAY 9/27 AND TAXIWAY A END CONNECTOR	1775911.154	721147.622
15	TAXIWAY A AND TAXIWAY A END CONNECTOR	1776161.148	721145.882

**GENERAL NOTES**

- AT THE DISCRETION OF THE AIRPORT, CONTRACTOR MAY STOCKPILE ON SITE A LIMITED AMOUNT OF THE HMA MILLINGS FOR LATER USE BY AIRPORT.
- CONTRACTOR SHALL POWER ENGINEER'S FIELD OFFICE FROM THE EXISTING PARKING LOT ELECTRICAL ENCLOSURE. ALL TOOLS, MATERIALS, EQUIPMENT, MANPOWER AND MISCELLANEOUS ITEMS TO PROVIDE POWER TO THE FIELD OFFICE IS INCIDENTAL TO THE FIELD OFFICE PAY ITEM.
- PRIME CONTRACTOR SHALL BE REQUIRED TO SUBMIT A VILLAGE OF LANSING BUILDING PERMIT APPLICATION. ALL CONTRACTORS WILL BE REQUIRED TO BE REGISTERED WITH THE VILLAGE OF LANSING AS PART OF THE PERMIT APPLICATION. THE VILLAGE OF LANSING WILL WAIVE THE BUILDING PERMIT FEE, BUT CONTRACTORS ARE STILL RESPONSIBLE FOR THE CONTRACTOR REGISTRATION FEES. (COST INCIDENTAL TO CONTRACT)**
- THE EXISTING PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACT.
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.



CONSULTANTS

FINAL  
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VILLAGE OF LANSING



Lansing  
Municipal Airport  
LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

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SHEET TITLE  
**CONSTRUCTION ACTIVITY  
PLAN - 1**

**LEGEND**

- WORK AREA 1A WORK LIMITS
  - WORK AREA 1B WORK LIMITS
  - TEMPORARY BARRICADE LINE
  - TEMPORARY BARRICADE LINE (WORK AREA 1B ONLY)
  - RUNWAY CLOSED MARKER
  - TAXIWAY CLOSED MARKER
  - CONTRACTOR'S HAUL ROUTE
  - CONTRACTOR FLAGGER/ESCORT
  - CONTRACTOR'S EMPLOYEE PARKING
  - CONTRACTOR'S EQUIPMENT STORAGE
  - AIRCRAFT MOVEMENT AREA
  - AOA
- AIR OPERATIONS AREA (A.O.A.)  
ACTIVE RUNWAY 18/36 125' CENTERLINE TO A.O.A.  
ACTIVE RUNWAY 9/27 125' CENTERLINE TO A.O.A.  
ACTIVE TAXIWAY 65.5' CENTERLINE TO A.O.A.  
ACTIVE TAXILANE 57.5' CENTERLINE TO A.O.A.

**NOTES**

- WORK AREA 1A AND 1B TO OCCUR CONCURRENTLY.
- WORK AREA 1A BARRICADES TO REMAIN IN PLACE WHEN CONTRACTOR WORKING WITHIN PHASE 1B LIMITS.
- SEE CONSTRUCTION ACTIVITY PLAN - 2 FOR GENERAL NOTES.
- CONTRACTOR SHALL NOTIFY THE AIRPORT AND RESIDENT ENGINEER A MINIMUM OF 5 CALENDAR DAYS IN ADVANCE OF THE ANTICIPATED WORK ON TAXIWAY C ADJACENT TO TAXIWAY D IN ORDER TO NOTIFY AIRFIELD TENANTS.

**PHASE 1 WORK**

- HMA PAVEMENT PATCHING
- HMA PAVEMENT MILLING
- HMA PAVEMENT PAVING
- TURF SHOULDER RESTORATION
- RUNWAY PAVEMENT MARKING (1 COAT AND NO BLACK BORDER)
- TAXIWAY PAVEMENT MARKING (1 COAT AND NO BLACK BORDER)

**GENERAL NOTES**

- ONCE CONSTRUCTION COMMENCES, THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER, A MINIMUM OF 7 CALENDAR DAYS PRIOR, TO THE INITIATION OF ANY WORK WHICH REQUIRES CLOSURE OF ACTIVE AIRFIELD PAVEMENT.
- ALL WORK AREAS SHALL BE SWEEPED AND CLEANED TO THE SATISFACTION OF THE AIRPORT PRIOR TO RE-OPENING PAVEMENT TO AIRCRAFT OPERATIONS.
- CONTRACTOR SHALL MAINTAIN AIRFIELD GATES CLOSED WHEN NOT IN USE. IF THE CONTRACTOR REQUIRES THE GATE TO REMAIN OPEN FOR LONG DURATIONS, A GATE GUARD SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- THE AIRPORT SHALL TURN OFF THE LOCALIZER WHEN RUNWAY 18/36 IS CLOSED.
- ALL WORK ON TAXIWAY M AND TAXIWAY G (BETWEEN TAXIWAY B AND RUNWAY 18/36) SHALL NOT BEGIN UNTIL WORK ON RUNWAY 18/36 (PHASE 1) IS SUBSTANTIALLY COMPLETED TO LIMIT CONSTRUCTION TRAFFIC LOADS ON THESE PAVEMENTS.

**SUGGESTED SEQUENCE OF CONSTRUCTION**

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 7 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREAS 1A AND 1B
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
  - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
  - COVER AND/OR DE-ENERGIZE SIGNS AND AIRFIELD LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS. INSTALL TAXIWAY CABLE JUMPERS AS REQUIRED.
  - COMPLETE PHASE 1 WORK
  - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
  - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
A	RUNWAY 9/27	41°32'13.82"	89°18'09.94"	614	639
B	RUNWAY 9/27	41°32'17.18"	89°18'10.03"	614	639
C	RUNWAY 9/27	41°32'11.35"	89°18'13.35"	614	639
D	RUNWAY 9/27	41°32'10.68"	89°18'19.14"	614	639
E	RUNWAY 9/27	41°32'17.32"	89°17'59.85"	614	639
F	RUNWAY 9/27	41°32'16.07"	89°18'00.74"	614	639
G	RUNWAY 9/27	41°32'15.96"	89°18'09.08"	616	641

PHASE 1	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 1A	52 CALENDAR DAYS	<ul style="list-style-type: none"> <li>RUNWAY 9/27 OPEN; RUNWAY 18/36 CLOSED; TAXIWAYS B, D, G, J, K, L AND M CLOSED; ALL OTHER TAXIWAYS OPEN</li> <li>RUNWAY 18/36, TAXIWAYS B, C (SOUTH OF RUNWAY 9/27), D, G (SOUTH OF RUNWAY 9/27), J, K, L AND M LIGHTING CIRCUIT WILL BE DE-ENERGIZED FOR THE DURATION OF THE PHASE</li> </ul>
WORK AREA 1B	THREE - 4 HOUR CLOSURES	<ul style="list-style-type: none"> <li>RUNWAY 9/27 OPEN; RUNWAY 18/36 CLOSED; TAXIWAYS B, C, D, G, J, K, L AND M CLOSED; ALL OTHER TAXIWAYS OPEN</li> <li><b>NO ACCESS TO WEST HANGAR AREA. ALL WORK TO BE EXPEDITED.</b></li> </ul>

202ND STREET

LANSING DRAINAGE DITCH

TAXIWAY B  
VOV  
AOA  
TAXIWAY J  
VOV  
AOA  
TAXIWAY K  
VOV  
AOA  
TAXIWAY L  
VOV  
AOA  
TAXIWAY M  
VOV  
AOA

RUNWAY 18/36

TEMPORARY CLOSED RUNWAY MARKER (TYPICAL)

LOCALIZER CRITICAL AREA

SEE NOTE 5

ILLINOIS  
INDIANA

BURNHAM AVE.

AIRPORT  
ADMINISTRATION  
BUILDING

CONTRACTOR'S  
EMPLOYEE  
PARKING

CONTRACTOR'S  
EQUIPMENT  
STORAGE

TXY H

POINT  
"D"

POINT  
"C"

POINT  
"B"

POINT  
"A"

POINT  
"G"

POINT  
"F"

POINT  
"E"

TAXIWAY D

TAXIWAY C

TAXIWAY G

TAXIWAY A

TAXIWAY B

TAXIWAY C

TAXIWAY D

TAXIWAY E

TAXIWAY F

TAXIWAY G

TAXIWAY H

TAXIWAY I

TAXIWAY J

TAXIWAY K

TAXIWAY L

TAXIWAY M

TAXIWAY N

TAXIWAY O

TAXIWAY P

TAXIWAY Q

TAXIWAY R

TAXIWAY S

TAXIWAY T

TAXIWAY U

TAXIWAY V

TAXIWAY W

TAXIWAY X

TAXIWAY Y

TAXIWAY Z

TAXIWAY AA

TAXIWAY AB

TAXIWAY AC

TAXIWAY AD

TAXIWAY AE

TAXIWAY AF

TAXIWAY AG

TAXIWAY AH

TAXIWAY AI

TAXIWAY AJ

TAXIWAY AK

TAXIWAY AL



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OWNER

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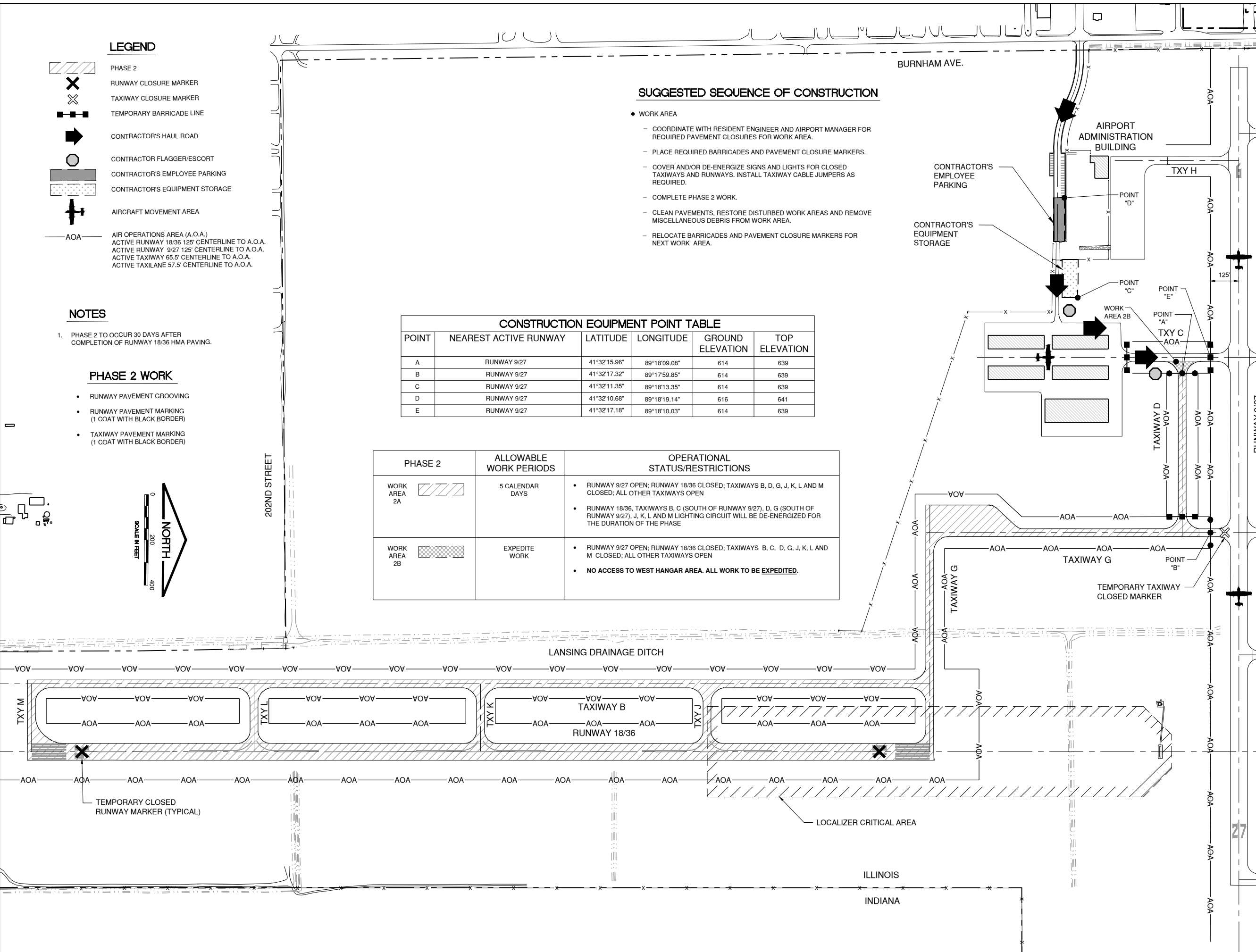


Lansing  
Municipal Airport

LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

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CMT PROJECT NO: 200922-79-00		
CAD DWG FILE: GC101.DWG		
DESIGNED BY: ARM		
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APPROVED BY:		
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SHEET TITLE  
**CONSTRUCTION ACTIVITY  
PLAN - 2**



**SUGGESTED SEQUENCE OF CONSTRUCTION**

- WORK AREA
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
  - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
  - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS. INSTALL TAXIWAY CABLE JUMPERS AS REQUIRED.
  - COMPLETE PHASE 2 WORK.
  - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
  - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
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C	RUNWAY 9/27	41°32'11.35"	89°18'13.35"	614	639
D	RUNWAY 9/27	41°32'10.68"	89°18'19.14"	616	641
E	RUNWAY 9/27	41°32'17.18"	89°18'10.03"	614	639

PHASE 2	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 2A	5 CALENDAR DAYS	<ul style="list-style-type: none"> <li>• RUNWAY 9/27 OPEN; RUNWAY 18/36 CLOSED; TAXIWAYS B, D, G, J, K, L AND M CLOSED; ALL OTHER TAXIWAYS OPEN</li> <li>• RUNWAY 18/36, TAXIWAYS B, C (SOUTH OF RUNWAY 9/27), D, G (SOUTH OF RUNWAY 9/27), J, K, L AND M LIGHTING CIRCUIT WILL BE DE-ENERGIZED FOR THE DURATION OF THE PHASE</li> </ul>
WORK AREA 2B	EXPEDITE WORK	<ul style="list-style-type: none"> <li>• RUNWAY 9/27 OPEN; RUNWAY 18/36 CLOSED; TAXIWAYS B, C, D, G, J, K, L AND M CLOSED; ALL OTHER TAXIWAYS OPEN</li> <li>• NO ACCESS TO WEST HANGAR AREA. ALL WORK TO BE EXPEDITED.</li> </ul>

**LEGEND**

- PHASE 2
  - RUNWAY CLOSURE MARKER
  - TAXIWAY CLOSURE MARKER
  - TEMPORARY BARRICADE LINE
  - CONTRACTOR'S HAUL ROAD
  - CONTRACTOR FLAGGER/ESCORT
  - CONTRACTOR'S EMPLOYEE PARKING
  - CONTRACTOR'S EQUIPMENT STORAGE
  - AIRCRAFT MOVEMENT AREA
  - AOA
- AIR OPERATIONS AREA (A.O.A.)  
 ACTIVE RUNWAY 18/36 125' CENTERLINE TO A.O.A.  
 ACTIVE RUNWAY 9/27 125' CENTERLINE TO A.O.A.  
 ACTIVE TAXIWAY 65.5' CENTERLINE TO A.O.A.  
 ACTIVE TAXILANE 57.5' CENTERLINE TO A.O.A.

**NOTES**

- PHASE 2 TO OCCUR 30 DAYS AFTER COMPLETION OF RUNWAY 18/36 HMA PAVING.

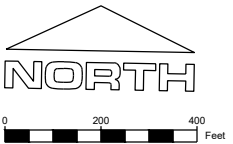
**PHASE 2 WORK**

- RUNWAY PAVEMENT GROOVING
- RUNWAY PAVEMENT MARKING (1 COAT WITH BLACK BORDER)
- TAXIWAY PAVEMENT MARKING (1 COAT WITH BLACK BORDER)





License No. 184-000613  
CONSULTANTS



FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK	DATE	DESCRIPTION
AIP PROJ. NO. 3-17-SBGP-XXX		
IL PROJ. NO. IGQ-4941		
CMT PROJECT NO: 200922-79-00		
CAD DWG FILE: GC102A.DWG		
DESIGNED BY: MH		
DRAWN BY: JRO		
CHECKED BY: CHK		
APPROVED BY: APR		
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SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN - 3**

GC102  
SHEET 6 OF 28

**SUGGESTED SEQUENCE OF CONSTRUCTION**

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 7 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
  - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
  - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS. INSTALL TAXIWAY CABLE JUMPERS AS REQUIRED.
  - COMPLETE PAVEMENT IMPROVEMENTS.
  - REMOVE TEMPORARY COVERS AND RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
  - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
  - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.
- RESTORE STAGING AREAS.

**CONSTRUCTION EQUIPMENT POINT TABLE**

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
A	RUNWAY 9/27	41°32'16.78"	89°17'59.84"	614	639
B	RUNWAY 9/27	41°32'24.97"	89°17'57.10"	613	638
C	RUNWAY 9/27	41°32'20.90"	89°18'09.20"	614	639
D	RUNWAY 9/27	41°32'19.03"	89°17'52.79"	616	641
E	RUNWAY 9/27	41°32'17.98"	89°17'47.04"	614	639
F	RUNWAY 9/27	41°32'22.43"	89°17'57.08"	614	639

**NOTES**

1. PHASE 3 WORK CANNOT BE COMPLETED CONCURRENTLY WITH PHASE 1 AND 2.
2. SEE CONSTRUCTION ACTIVITY PLAN - 1 FOR GENERAL NOTES.

**PHASE 3 WORK**

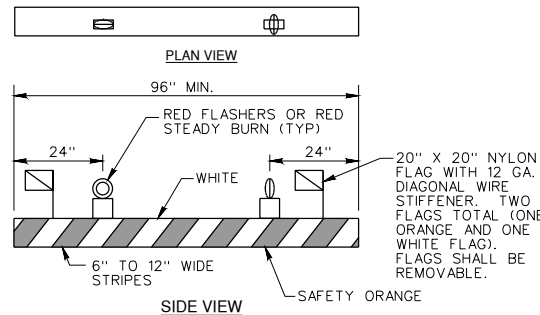
- HMA PAVEMENT REMOVAL
- UNCLASSIFIED EXCAVATION
- AGGREGATE SUBGRADE/BASE COURSE PLACEMENT
- HMA PAVEMENT PAVING
- RESTORE DISTURBED TURF SHOULDERS
- TAXIWAY PAVEMENT MARKINGS  
(2 COATS WITH BLACK BORDER: FIRST COAT HALF-RATE WITHOUT BEADS AND NO BLACK BORDER. 30 DAY CURE. SECOND COAT FULL-RATE WITH BEADS AND BLACK BORDER)

**LEGEND**

- TEMPORARY BARRICADE LINE
- CONTRACTOR'S EQUIPMENT/MATERIAL STORAGE
- AIR OPERATIONS AREA (A.O.A.)  
ACTIVE RUNWAY 18/36 125' CENTERLINE TO A.O.A.  
ACTIVE RUNWAY 9/27 125' CENTERLINE TO A.O.A.  
ACTIVE TAXIWAY 65.5' CENTERLINE TO A.O.A.  
ACTIVE TAXILANE 57.5' CENTERLINE TO A.O.A.
- HAUL ROUTE
- CLOSED RUNWAY MARKER
- TEMPORARY STOP SIGN AND CONTRACTOR CROSSING GUARD/FLAGGER
- AIRCRAFT MOVEMENT AREA

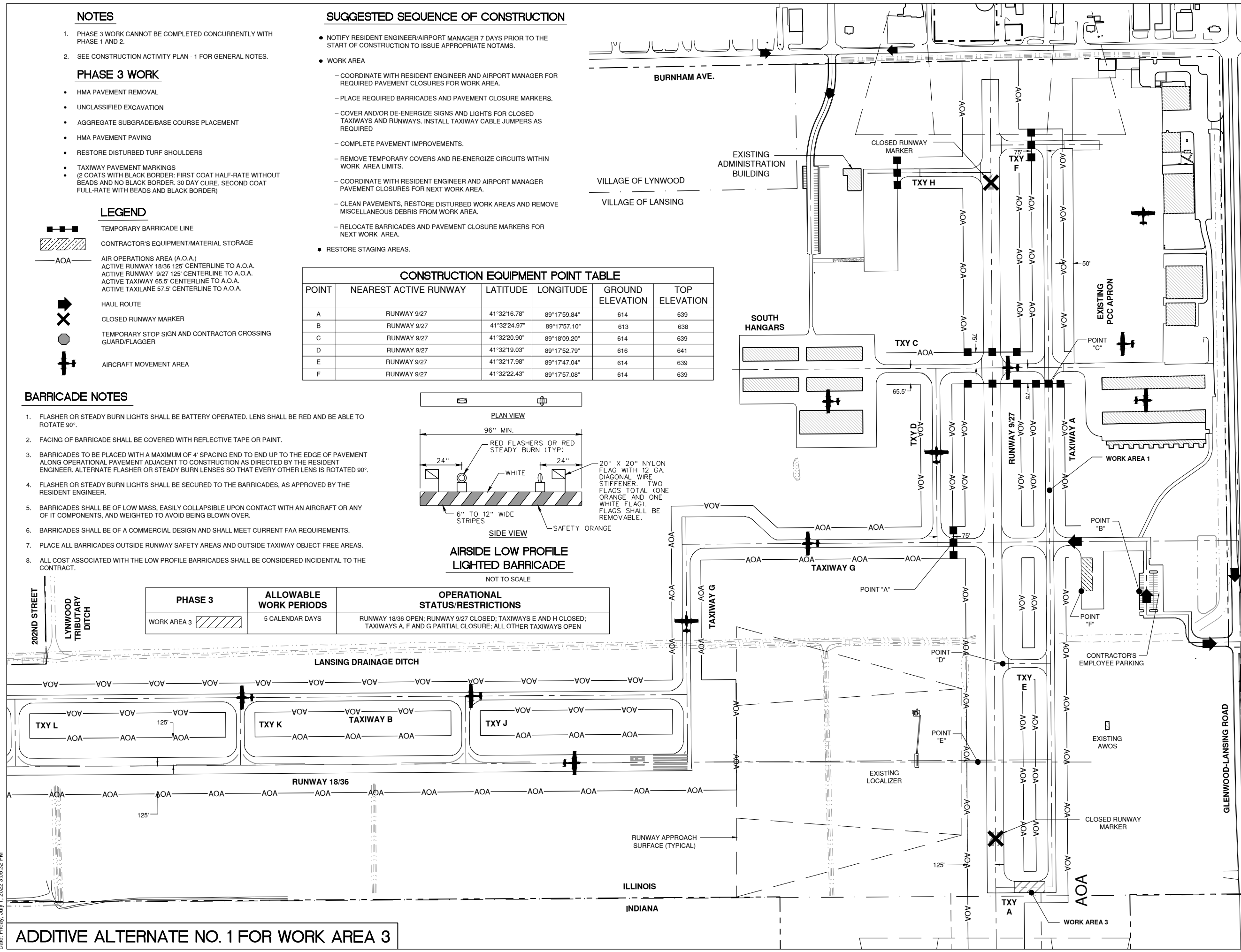
**BARRICADE NOTES**

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**AIRSIDE LOW PROFILE LIGHTED BARRICADE**  
NOT TO SCALE

PHASE 3	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 3	5 CALENDAR DAYS	RUNWAY 18/36 OPEN; RUNWAY 9/27 CLOSED; TAXIWAYS E AND H CLOSED; TAXIWAYS A, F AND G PARTIAL CLOSURE; ALL OTHER TAXIWAYS OPEN



**ADDITIVE ALTERNATE NO. 1 FOR WORK AREA 3**

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FINAL
JUNE 10, 2022
REHABILITATE RUNWAY 18-36, TAXIWAY B AND TAXIWAY G



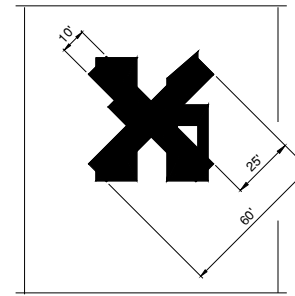
VILLAGE OF LANSING
Lansing Municipal Airport
LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

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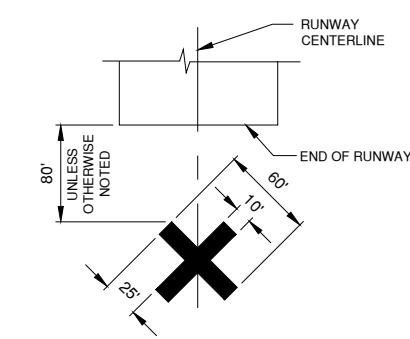
SHEET TITLE
CONSTRUCTION ACTIVITY PLAN
GENERAL NOTES AND DETAILS
GC103
SHEET 7 OF 28

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'

DESIGN AIRCRAFT APPROACH CATAGORY: B
DESIGN AIRPORT GROUP: II
RUNWAY 18/36 SAFETY AREA TOTAL WIDTH = 150'
RUNWAY 9/27 SAFETY AREA TOTAL WIDTH = 150'
TAXIWAY CENTERLINE TO OBJECT SEPARATION = 65.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION = 57.5'



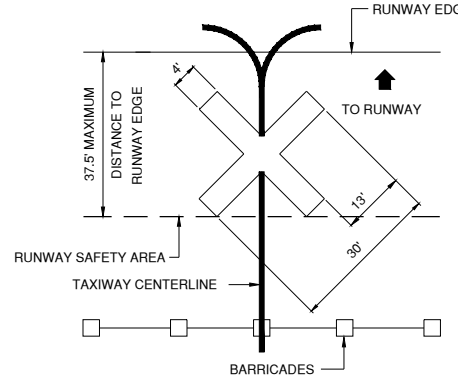
ON PAVEMENT
CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE



OFF PAVEMENT
CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

- 1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT...



CLOSED TAXIWAY MARKER DETAIL
NOT TO SCALE

CLOSED TAXIWAY MARKER DETAIL NOTES

- 1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR TAXIWAY CLOSURES EXCEEDING 72 HOURS.

AIRFIELD LIGHTS AND SIGNS NOTES

- 1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE. TAXIWAY CABLE JUMPERS MAY BE USED TO ISOLATE TAXIWAY CIRCUITS (COST INCIDENTAL).
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

- 23. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS.
24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY.
25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
26. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 65.5' FROM ACTIVE TAXIWAYS, 57.5' FROM ACTIVE TAXILANES, 125' FROM ACTIVE RUNWAY 18/36 AND 125' FROM ACTIVE RUNWAY 9/27 UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES.
29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED.
30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT...

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE AIR FIELD SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE...

ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER...

WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS...

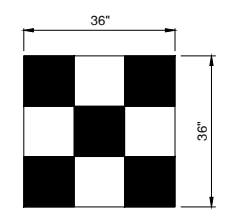
LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY AIRCRAFT OPERATION AREA AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED...

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA...



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL ORANGE AND WHITE CHECKERED FLAG
NOT TO SCALE

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS...

- REHABILITATE LIGHTING FOR RUNWAY 18-36 AND PARALLEL TAXIWAY

GENERAL NOTES

- 1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. FOR WORK ON AIRPORT PROPERTY: PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, TAXIWAY AND RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A TRUCK IN THE DUMP POSITION WHICH HAS A MAXIMUM HEIGHT OF 25 FEET.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. LANSING MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE CONSTRUCTION ACTIVITY.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS.
22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR...

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CONSULTANTS

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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

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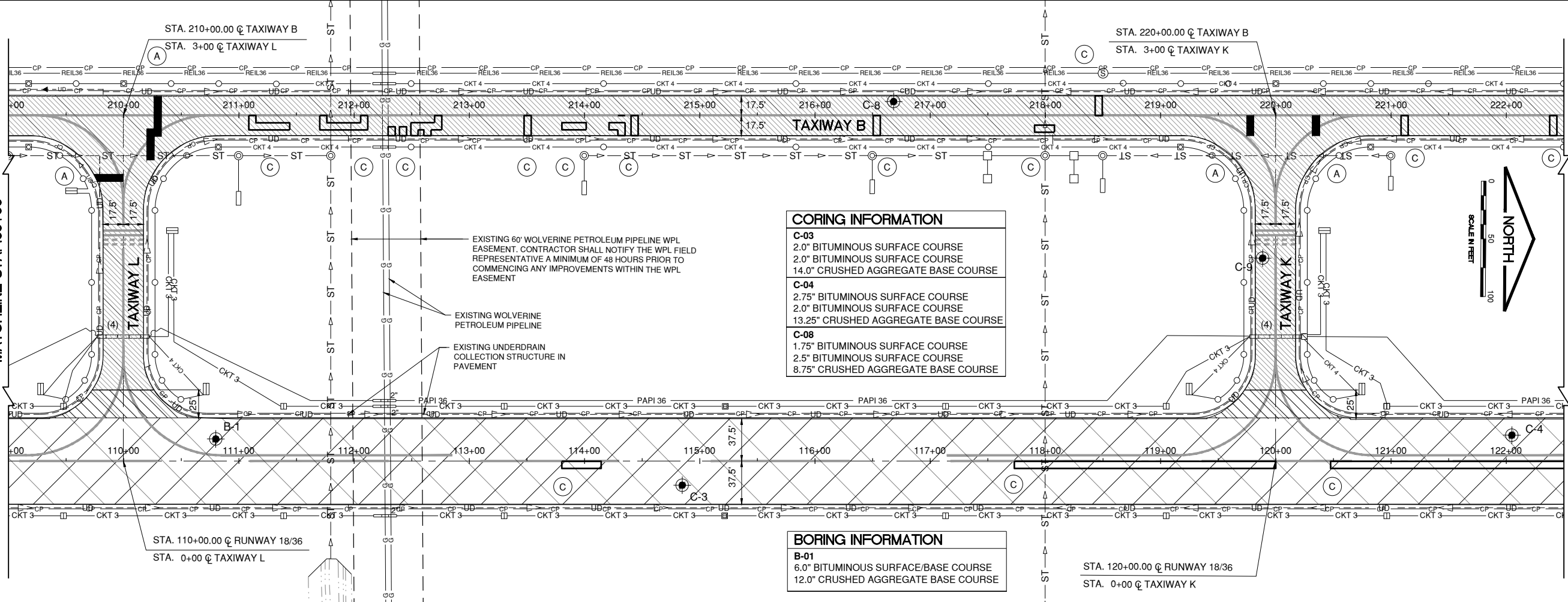
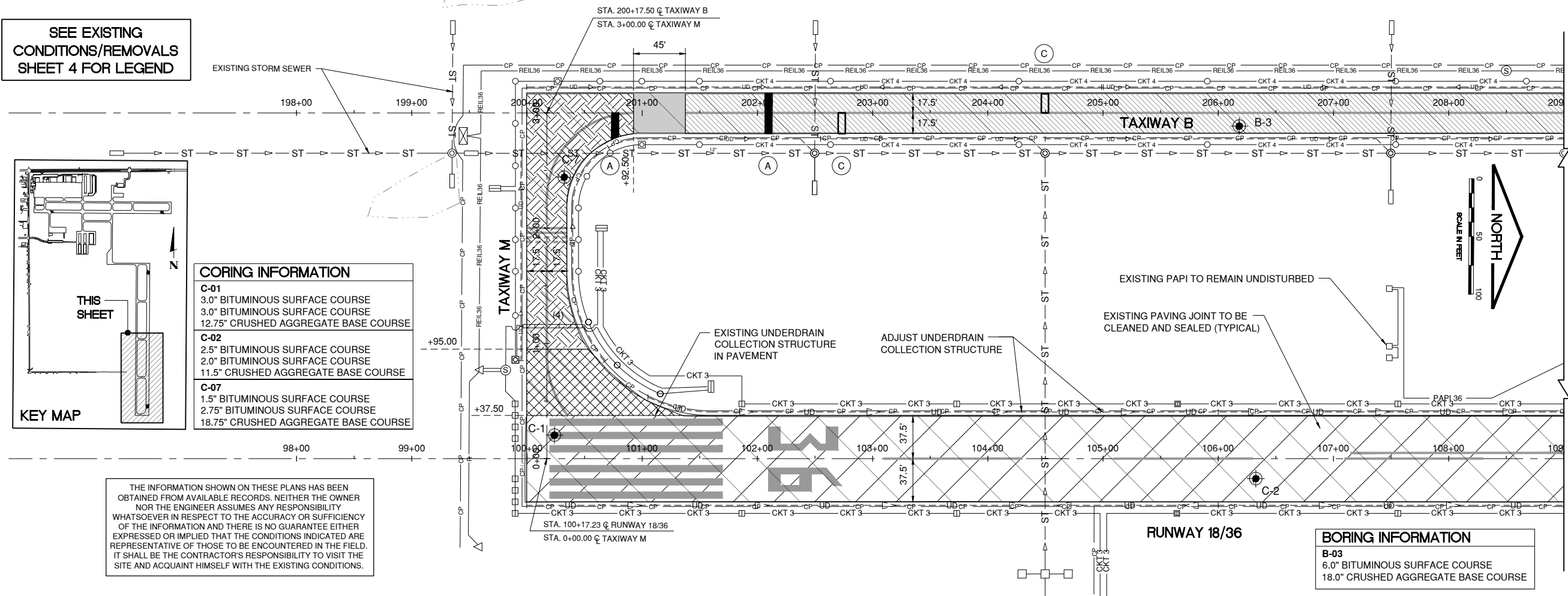
LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK DATE DESCRIPTION

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IL PROJ. NO. IGQ-4941
CMT PROJECT NO: 200922-79-00
CAD DWG FILE: ED100.DWG
DESIGNED BY: GH
DRAWN BY: JRO
CHECKED BY: DKP
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**EXISTING  
CONDITIONS /  
REMOVALS - 1**

ED100  
SHEET 8 OF 28



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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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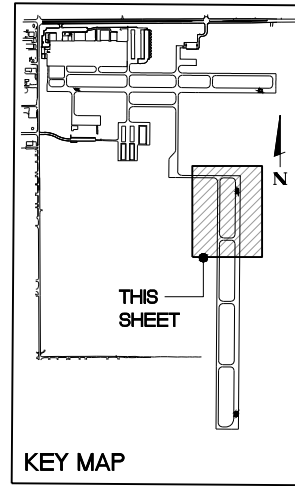
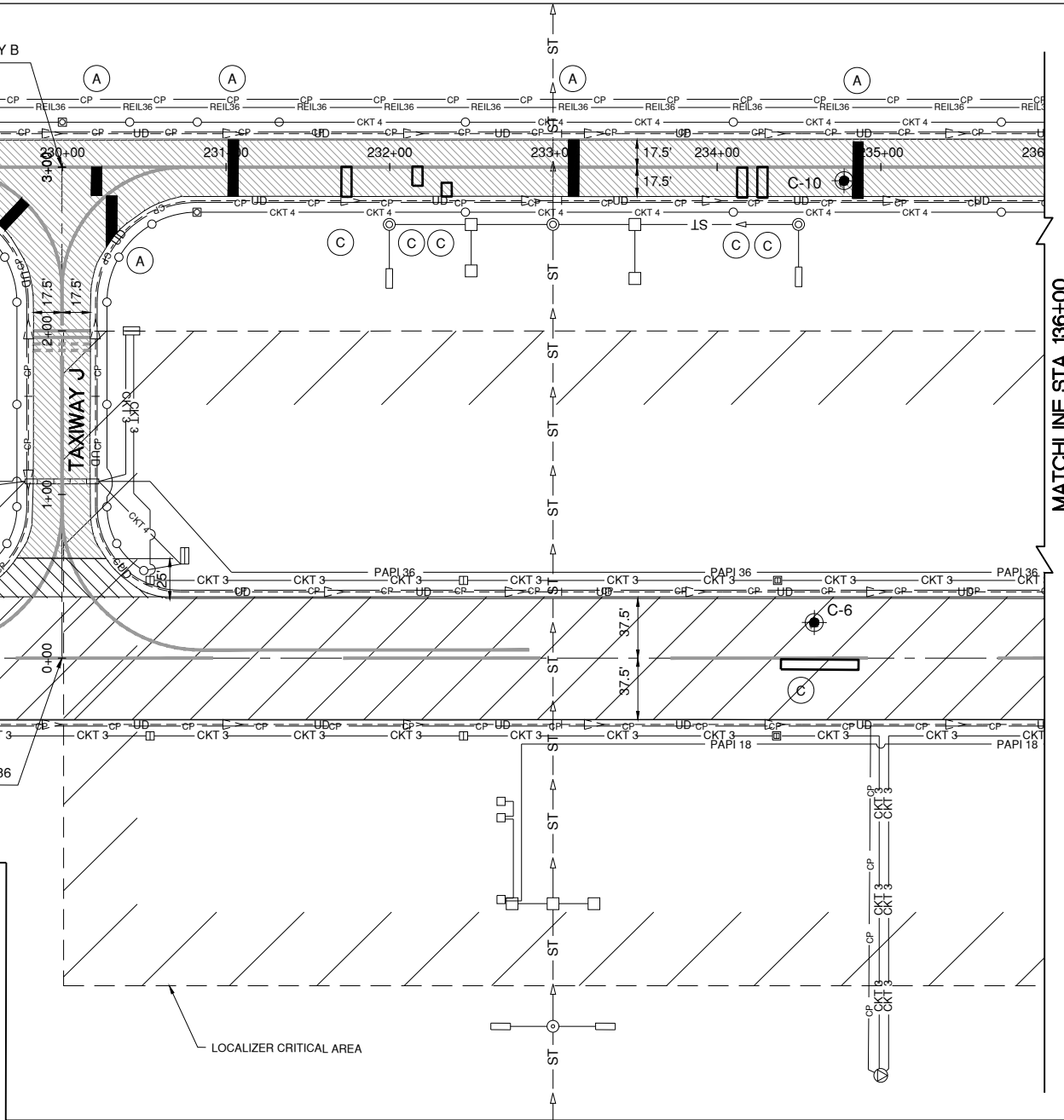
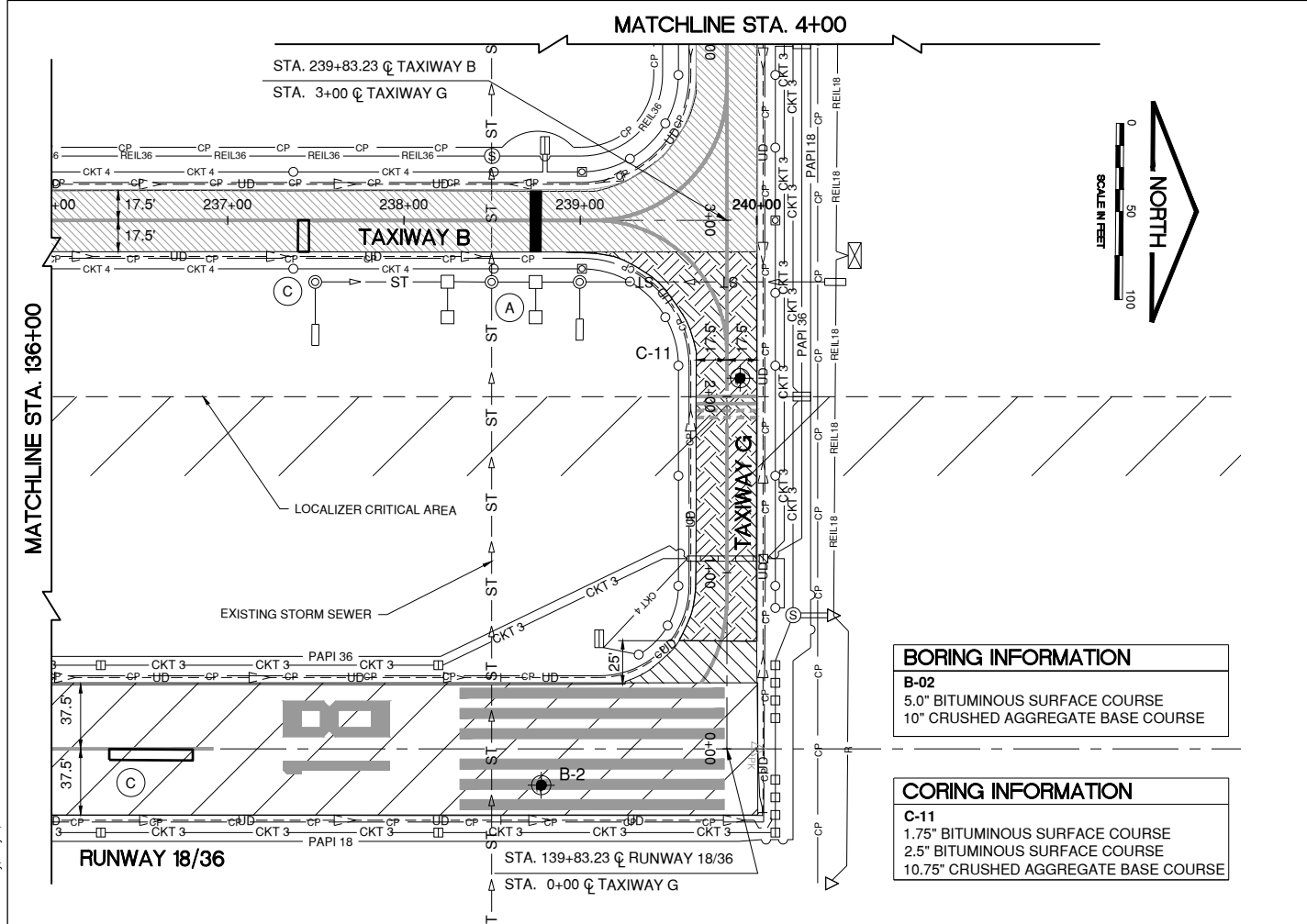
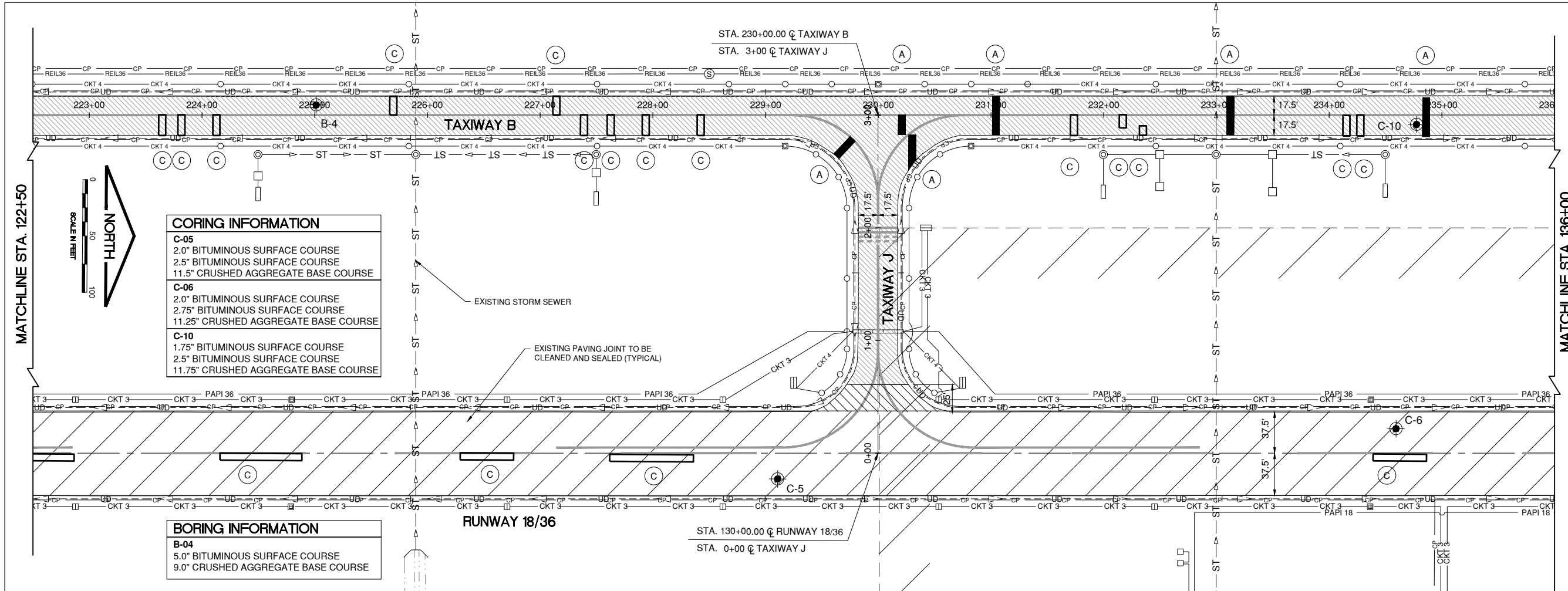


LANSING MUNICIPAL AIRPORT  
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EXISTING  
CONDITIONS /  
REMOVALS - 2

ED101  
SHEET 9 OF 28



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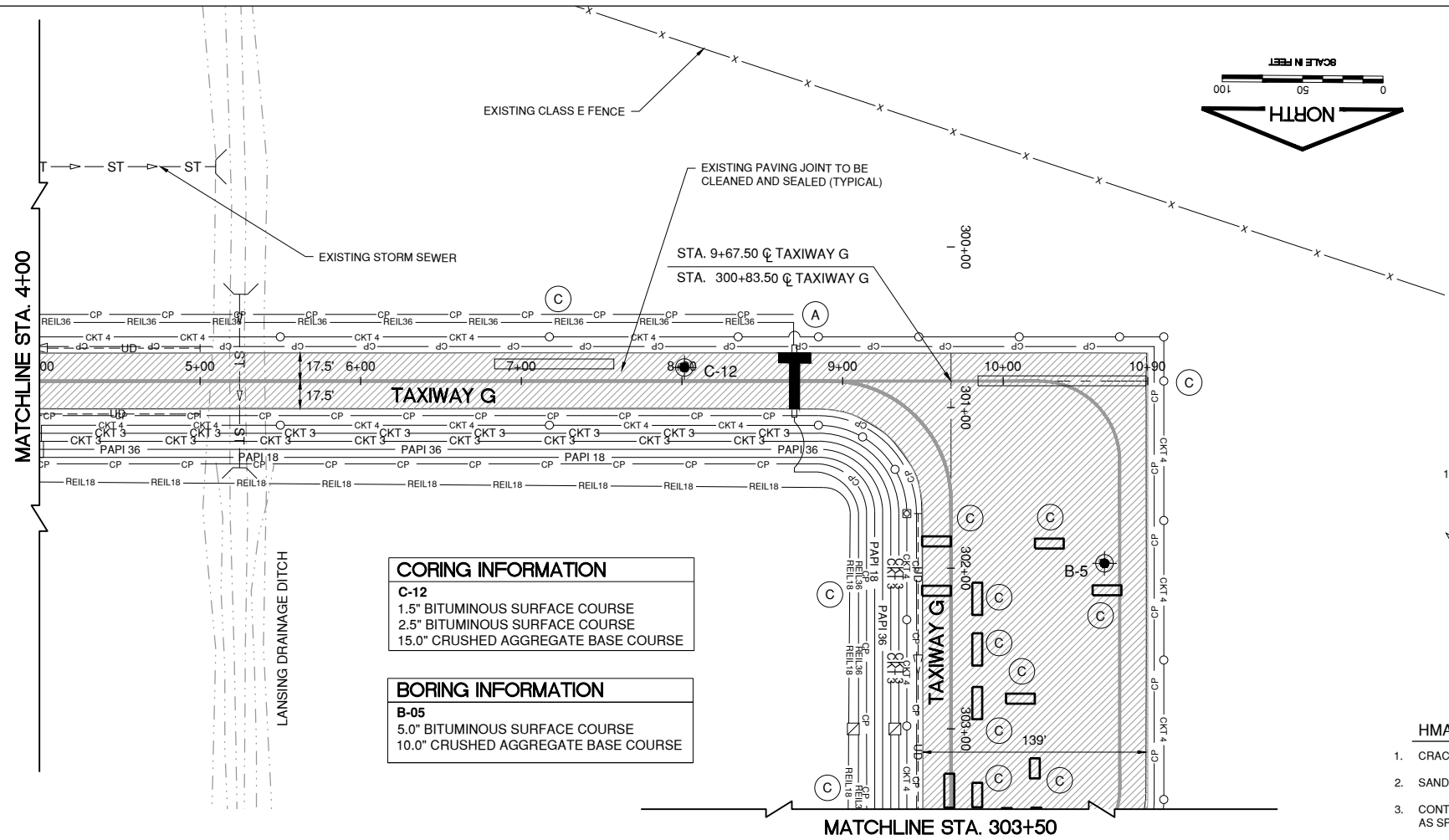
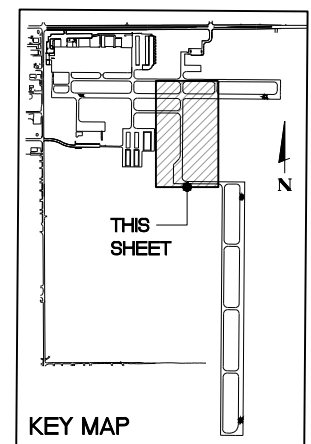
SEE EXISTING  
CONDITIONS/REMOVALS  
SHEET 4 FOR LEGEND

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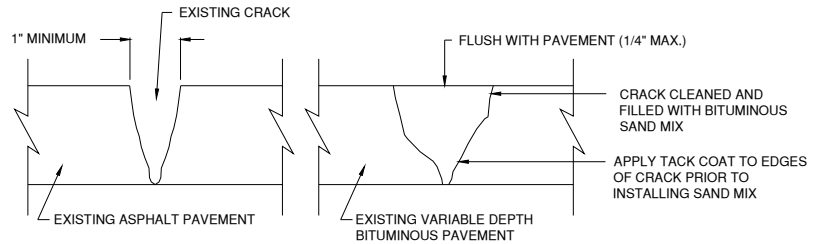
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SEE EXISTING CONDITIONS/REMOVALS SHEET 4 FOR LEGEND



**CORING INFORMATION**  
**C-12**  
 1.5" BITUMINOUS SURFACE COURSE  
 2.5" BITUMINOUS SURFACE COURSE  
 15.0" CRUSHED AGGREGATE BASE COURSE

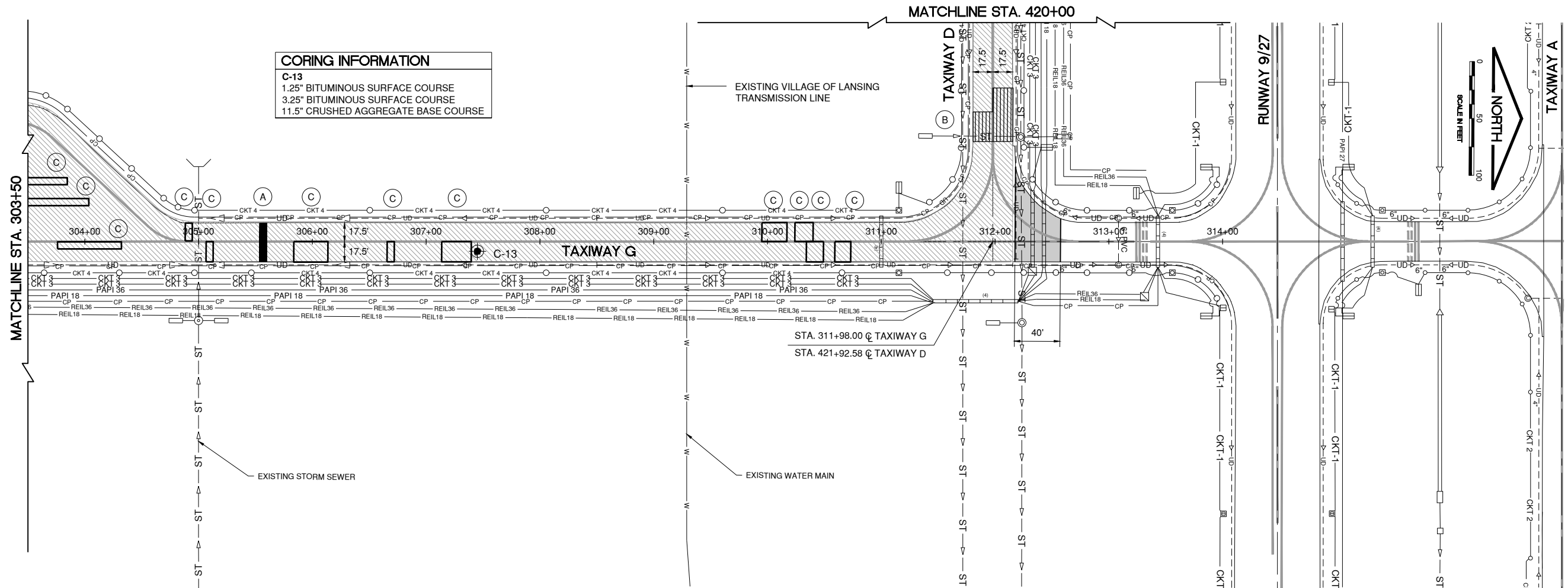
**BORING INFORMATION**  
**B-05**  
 5.0" BITUMINOUS SURFACE COURSE  
 10.0" CRUSHED AGGREGATE BASE COURSE



HMA SAND MIX CRACK REPAIR DETAIL (AR201663)

- HMA SAND MIX CRACK REPAIR NOTES**
1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
  2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE OR AS DESIGNATED BY THE RESIDENT ENGINEER.
  3. CONTRACTOR SHALL ROUTE, CRACK PLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING

FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



**CORING INFORMATION**  
**C-13**  
 1.25" BITUMINOUS SURFACE COURSE  
 3.25" BITUMINOUS SURFACE COURSE  
 11.5" CRUSHED AGGREGATE BASE COURSE



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EXISTING  
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ED102  
SHEET 10 OF 28

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CONSULTANTS

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VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

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IL PROJ. NO. IGQ-4941
CMT PROJECT NO: 200922-79-00
CAD DWG FILE: ED103.DWG
DESIGNED BY: GH
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SHEET TITLE  
**EXISTING  
CONDITIONS /  
REMOVALS - 4**  
ED103  
SHEET 11 OF 28

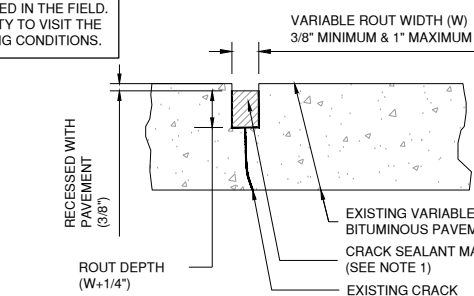
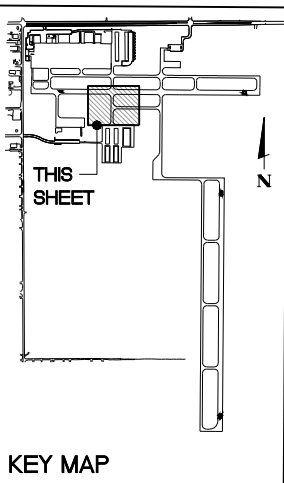
**LEGEND**

	EXISTING PAVEMENT		EXISTING ELECTRIC LINE
	BITUMINOUS PAVEMENT TO BE MILLED (1/2")		EXISTING STAKE/BASE MOUNTED MRL
	BITUMINOUS PAVEMENT TO BE MILLED (1" AND VARIES)		EXISTING STAKE/BASE MOUNTED MITL
	BITUMINOUS PAVEMENT TO BE VARIABLY MILLED (1" - 2")		EXISTING WIND CONE
	BITUMINOUS PAVEMENT TO BE MILLED (2" AVG.)		EXISTING REIL
	BITUMINOUS PAVEMENT TO BE VARIABLY MILLED (0" - 1")		EXISTING STORM SEWER
	BITUMINOUS PAVEMENT BUTT JOINT CONSTRUCTION		EXISTING DRAINAGE STRUCTURE
	BITUMINOUS PAVEMENT TO BE REMOVED		EXISTING STORM SEWER END TREATMENT
	REMOVE AND REPLACE HMA PAVEMENT - TYPE A		EXISTING CONCRETE ENCASED DUCT BANK (#OF WAYS INDICATED)
	REMOVE AND REPLACE HMA PAVEMENT - TYPE B		EXISTING GRS CONDUIT (SIZE INDICATED)
	SURFACE COURSE CRACK REPAIR		EXISTING RETROREFLECTIVE MARKER
	EXISTING AIRFIELD LIGHTING CIRCUITS		EXISTING ELECTRICAL SPLICE CAN
	EXISTING REIL AUXILIARY UNIT CONTROL CABLE		EXISTING CLASS E FENCE
	EXISTING PAPI CIRCUITS		EXISTING WATER MAIN
	EXISTING WINDCONE CIRCUIT		EXISTING WATER VALVE
	EXISTING RUNWAY 36 REIL CIRCUIT		EXISTING FIRE HYDRANT
	EXISTING RUNWAY 18 REIL CIRCUIT		EXISTING HANDHOLE
	EXISTING AIRFIELD GUIDANCE SIGNAGE		EXISTING WINDSOCK
	EXISTING 2-BOX PAPI (L-881)		EXISTING UNDERDRAIN
			EXISTING GAS/PETROLEUM LINE
			EXISTING COUNTERPOISE WIRE
			PAVEMENT CORE

**GENERAL NOTES**

- THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT SHALL IDENTIFY ALL CRACKS TO BE REPAIRED AND OR SEALED. CONTRACTOR SHALL ADJUST THEIR OPERATIONS AS NECESSARY FOR THE IDENTIFIED WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE.
- CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY UNLESS OTHERWISE NOTED ON PLANS.
- PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION.
- PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS FOUND TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR.
- EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- DRAINAGE COLLECTION STRUCTURES TO BE ADJUSTED SHALL BE DETERMINED IN THE FIELD AND THE NEW RIM ELEVATION SHALL MATCH THE NEW GRADING ELEVATIONS.

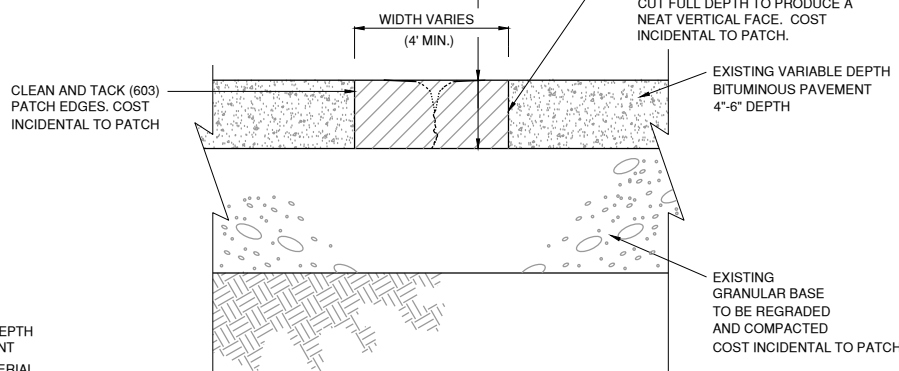
THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



**CLEAN AND SEAL BITUMINOUS CRACKS (AR201661)**

**CRACK SEALING NOTES:**

- THE CRACK ROUTING DIMENSIONS WILL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL.
- CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.
- THE CRACK SEALANT MATERIAL SHALL BE RECESSED WITH THE PAVEMENT AS NOTED IN THE DETAIL. AT THE DISCRETION OF THE RESIDENT ENGINEER, SHOULD ANY AREAS BE IDENTIFIED AS NOT BEING RECESSED, THE IDENTIFIED PORTION OF SEALANT SHALL BE REMOVED, THE CRACK CLEANED AND RESEALED AT NO ADDITIONAL COST TO THE PROJECT.



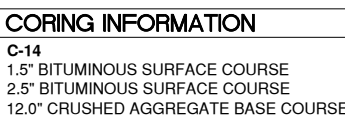
**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A (AR401915)**

NOT TO SCALE

- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- BITUMINOUS PAVEMENT PLACEMENT SHALL BE CONSTRUCTED IN TWO (2) LIFTS (3" MAX. LIFT). TACK COAT SHALL BE APPLIED ON THE SIDES OF THE PATCH. THIS COST SHALL BE INCIDENTAL TO ITEM - REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A.
- REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A SHALL OCCUR PRIOR TO PAVEMENT MILLING OPERATIONS.

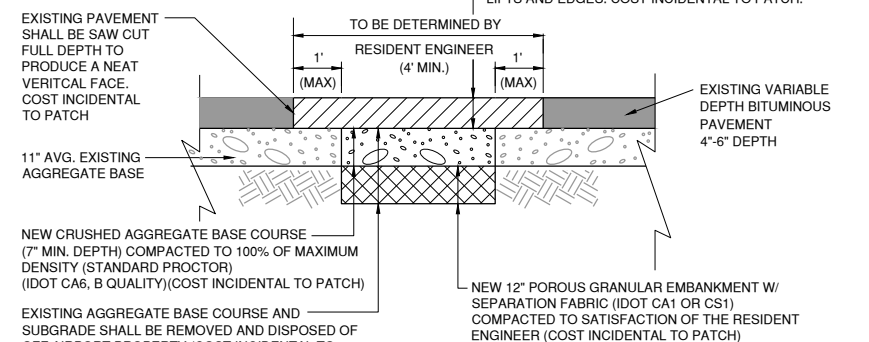
**SURFACE COURSE CRACK REPAIR NOTES**

- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- BITUMINOUS PAVEMENT PLACEMENT SHALL BE CONSTRUCTED IN ONE (1) LIFT. TACK COAT SHALL BE APPLIED ON THE SIDES OF THE PATCH. THIS COST SHALL BE INCIDENTAL TO ITEM - SURFACE COURSE CRACK REPAIRS.
- SURFACE COURSE CRACK REPAIRS SHALL OCCUR PRIOR TO PAVEMENT MILLING OPERATIONS.



**SURFACE COURSE CRACK REPAIR (AR401662)**

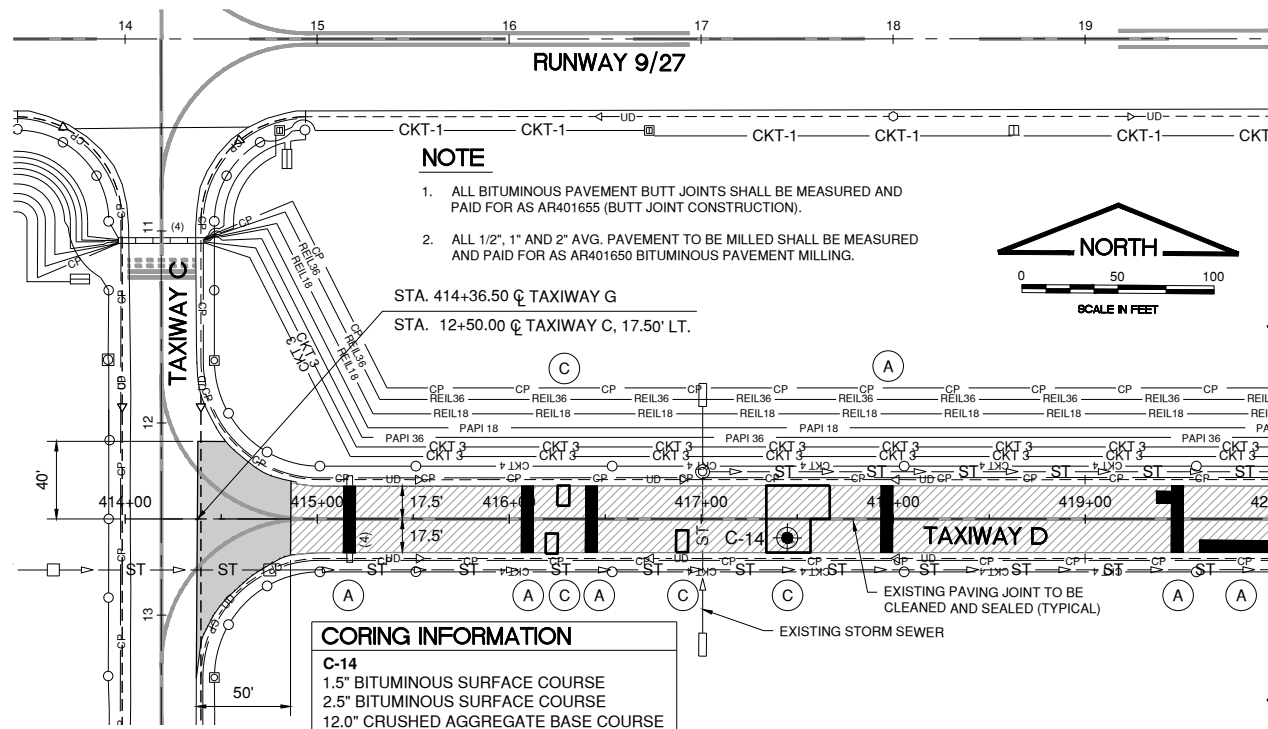
NOT TO SCALE



**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B (AR401916)**

NOT TO SCALE

- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREA IN THE FIELD DURING CONSTRUCTION.
- BITUMINOUS PAVEMENT PLACEMENT SHALL BE CONSTRUCTED IN MULTIPLE LIFTS. TACK COAT SHALL BE APPLIED ON THE SIDES OF THE PATCH, THIS COST SHALL BE INCIDENTAL TO ITEM - REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B.
- NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR ALL REQUIRED EXCAVATION, EXCAVATION DISPOSAL, FURNISHING AGGREGATE BASE COURSE, POROUS GRANULAR EMBANKMENT, SEPARATION FABRIC AND TACK OR FOR PREPARING AND PLACING THESE MATERIALS, AND ALL LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS NOTED IN THE DETAIL. ALL COSTS SHALL BE INCIDENTAL TO THE TYPE B PATCH.
- REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B SHALL OCCUR PRIOR TO PAVEMENT MILLING OPERATIONS.





CONSULTANTS

FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK	DATE	DESCRIPTION
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IL PROJ. NO. IGQ-4941		
CMT PROJECT NO: 200922-79-00		
CAD DWG FILE: L100.DWG		
DESIGNED BY: ARM		
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SHEET TITLE  
**STORM WATER POLLUTION  
PREVENTION PLAN**

L100  
SHEET 12 OF 28

**LEGEND**

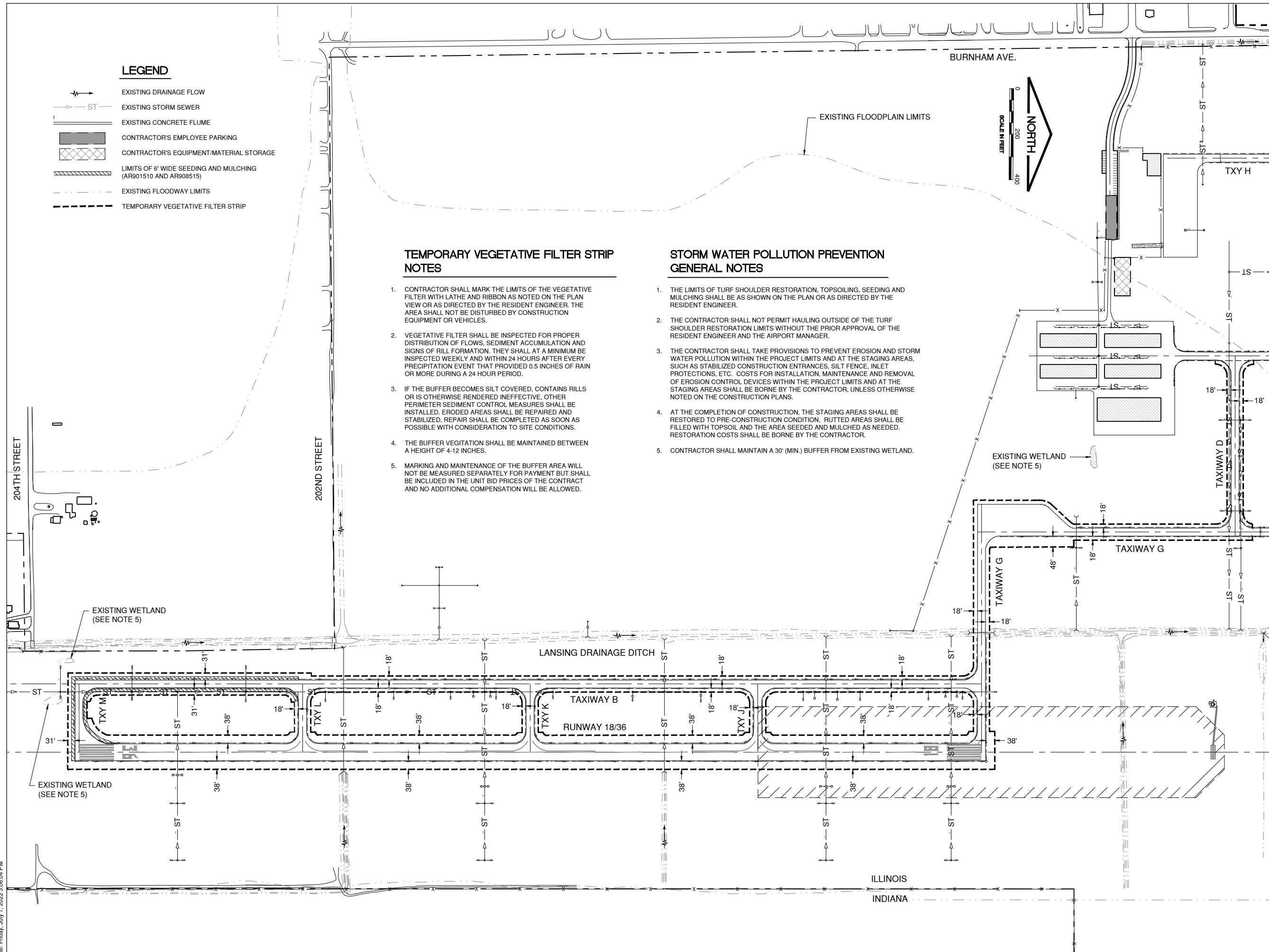
- EXISTING DRAINAGE FLOW
- EXISTING STORM SEWER
- EXISTING CONCRETE FLUME
- CONTRACTOR'S EMPLOYEE PARKING
- CONTRACTOR'S EQUIPMENT/MATERIAL STORAGE
- LIMITS OF 6' WIDE SEEDING AND MULCHING (AR901510 AND AR908515)
- EXISTING FLOODWAY LIMITS
- TEMPORARY VEGETATIVE FILTER STRIP

**TEMPORARY VEGETATIVE FILTER STRIP NOTES**

- CONTRACTOR SHALL MARK THE LIMITS OF THE VEGETATIVE FILTER WITH LATHE AND RIBBON AS NOTED ON THE PLAN VIEW OR AS DIRECTED BY THE RESIDENT ENGINEER. THE AREA SHALL NOT BE DISTURBED BY CONSTRUCTION EQUIPMENT OR VEHICLES.
- VEGETATIVE FILTER SHALL BE INSPECTED FOR PROPER DISTRIBUTION OF FLOWS, SEDIMENT ACCUMULATION AND SIGNS OF RILL FORMATION. THEY SHALL AT A MINIMUM BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PROVIDED 0.5 INCHES OF RAIN OR MORE DURING A 24 HOUR PERIOD.
- IF THE BUFFER BECOMES SILT COVERED, CONTAINS RILLS OR IS OTHERWISE RENDERED INEFFECTIVE, OTHER PERIMETER SEDIMENT CONTROL MEASURES SHALL BE INSTALLED. ERODED AREAS SHALL BE REPAIRED AND STABILIZED. REPAIR SHALL BE COMPLETED AS SOON AS POSSIBLE WITH CONSIDERATION TO SITE CONDITIONS.
- THE BUFFER VEGETATION SHALL BE MAINTAINED BETWEEN A HEIGHT OF 4-12 INCHES.
- MARKING AND MAINTENANCE OF THE BUFFER AREA WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**STORM WATER POLLUTION PREVENTION GENERAL NOTES**

- THE LIMITS OF TURF SHOULDER RESTORATION, TOPSOILING, SEEDING AND MULCHING SHALL BE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE TURF SHOULDER RESTORATION LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR, UNLESS OTHERWISE NOTED ON THE CONSTRUCTION PLANS.
- AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED WITH TOPSOIL AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.
- CONTRACTOR SHALL MAINTAIN A 30' (MIN.) BUFFER FROM EXISTING WETLAND.



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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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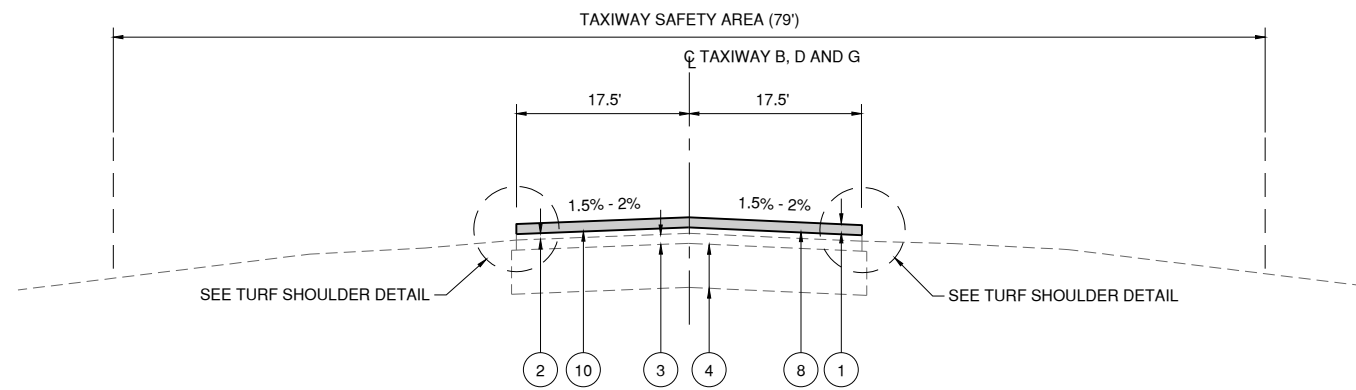


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LANSING, ILLINOIS

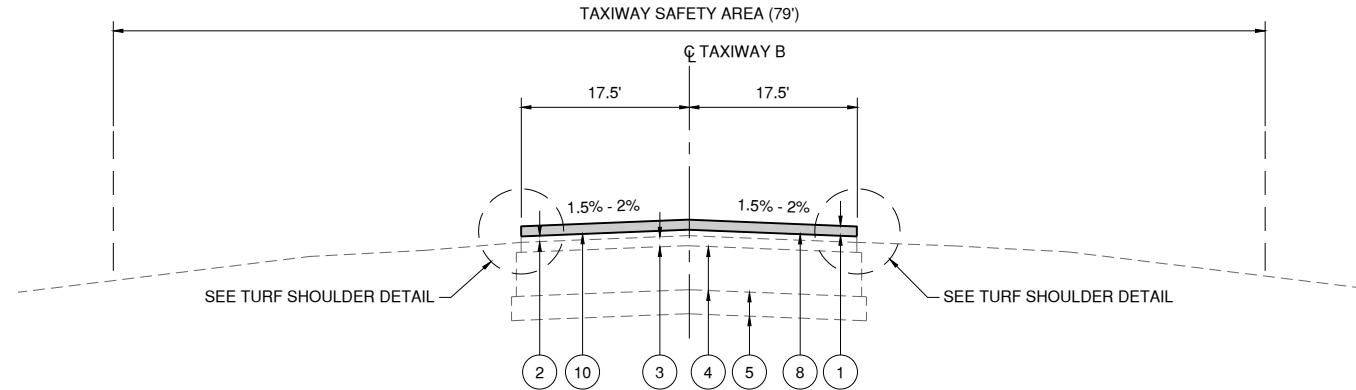
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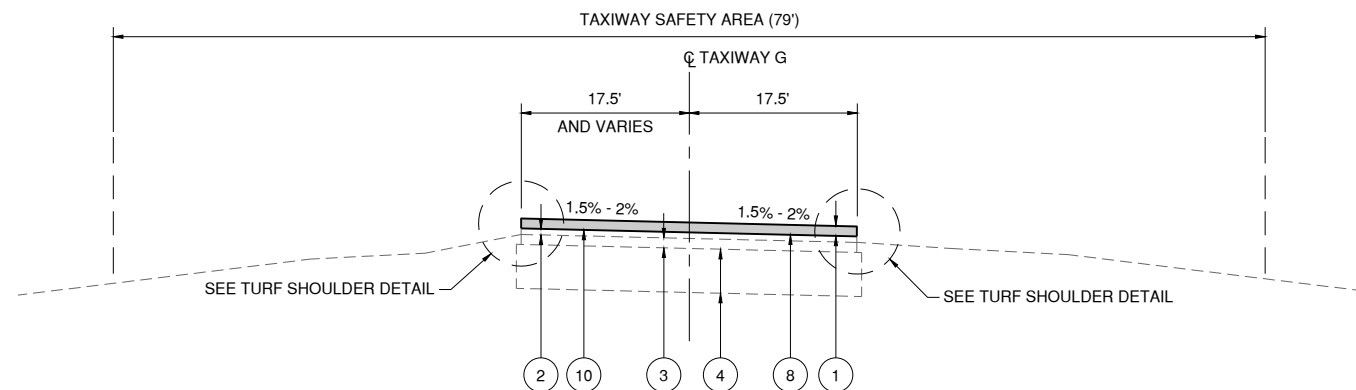
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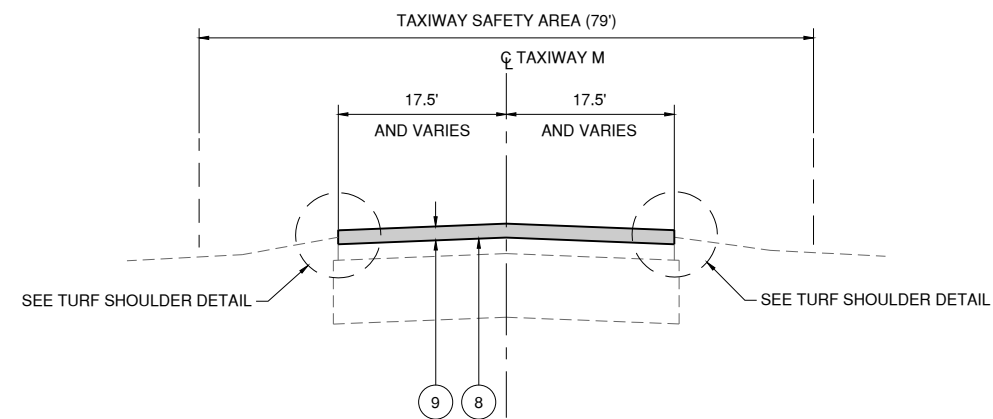
**TAXIWAY B, G AND D TYPICAL SECTION A-A**  
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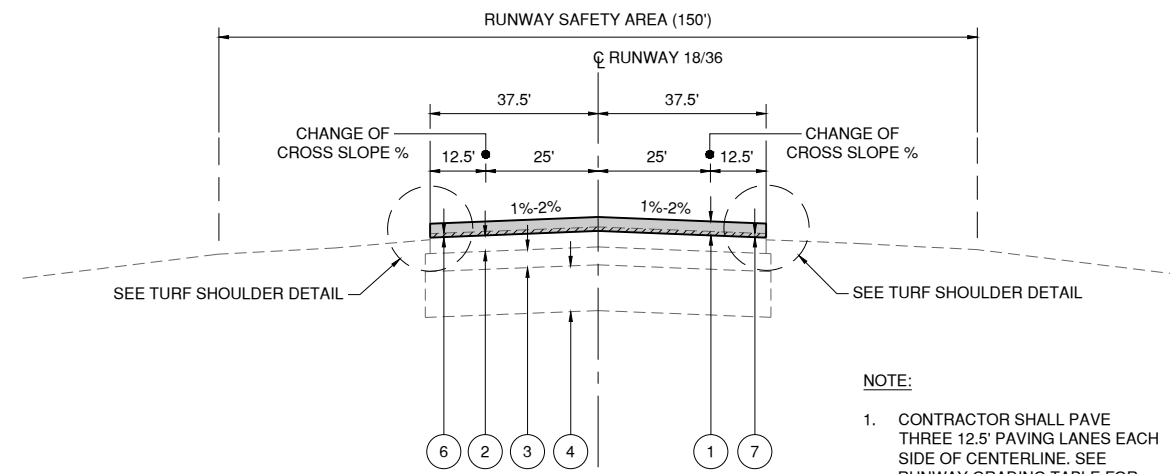
**TAXIWAY B (STA. 200+00 TO 209+82.50) TYPICAL SECTION C-C**  
NOT TO SCALE



**TAXIWAY G (STA. 301+66 TO STA. 311+15) TYPICAL SECTION D-D**  
NOT TO SCALE



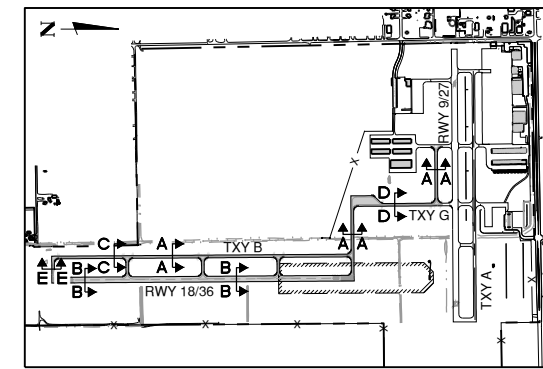
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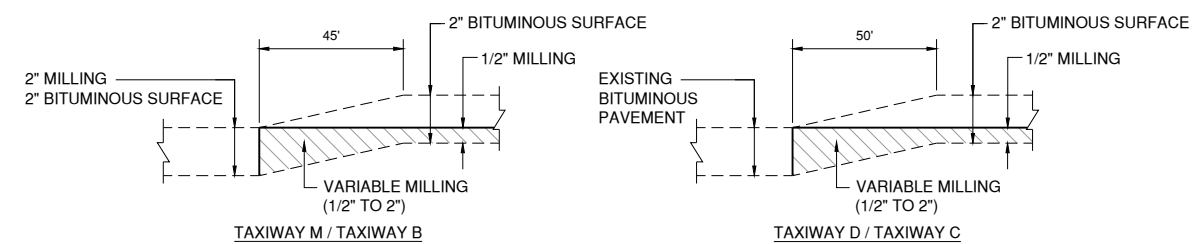
**RUNWAY 18/36 TYPICAL SECTION B-B**  
NOT TO SCALE

**NOTE:**  
1. CONTRACTOR SHALL PAVE THREE 12.5' PAVING LANES EACH SIDE OF CENTERLINE. SEE RUNWAY GRADING TABLE FOR CROSS SLOPES AND CHANGE OF CROSS SLOPE FROM CENTERLINE - 25' LEFT/RIGHT AND 25' - 37.5' LEFT/RIGHT.

- LEGEND**
- 1 NEW 2" AND VARIABLE BITUMINOUS SURFACE COURSE OVERLAY (401)
  - 2 EXISTING 2" BITUMINOUS SURFACE COURSE (401)
  - 3 EXISTING 2" BITUMINOUS SURFACE COURSE (403)
  - 4 EXISTING 11" CRUSHED AGGREGATE BASE COURSE (209)
  - 5 EXISTING 6" AGGREGATE BASE - PGE (208)
  - 6 EXISTING 5/8" POROUS FRICTION COURSE
  - 7 1" (AND VARIES) HMA PAVEMENT MILLING (101)
  - 8 NEW TACK COAT (603)
  - 9 2" (AVG.) HMA PAVEMENT MILLING (AR401650) AND 2" BITUMINOUS SURFACE COURSE (401)
  - 10 1/2" HMA PAVEMENT MILLING (AR401650)



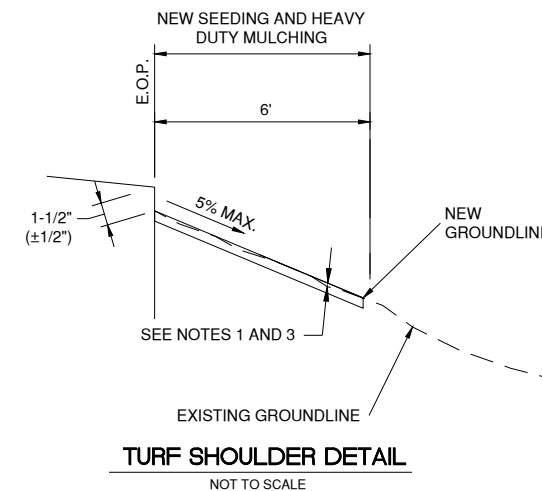
**KEY MAP**  
NOT TO SCALE



**BUTT JOINT CONSTRUCTION (ITEM AR401655)**  
NOT TO SCALE

**TURF SHOULDER NOTES:**

1. TILL AND PULVERIZE EXISTING TURF SHOULDER IN PLACE (COST INCIDENTAL TO SEEDING PAY ITEM).
2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD BY THE RESIDENT ENGINEER AS REQUIRED.
3. TURF SHALL BE SMOOTHLY GRADED TO THE SATISFACTION OF THE RESIDENT ENGINEER BEFORE SEEDING. ADDITIONAL TOPSOIL WILL BE REQUIRED FOR TURF SHOULDER RESTORATION AND SHALL BE INCIDENTAL TO SEEDING AND PAY ITEM.
4. NO FILL IN FLOODPLAIN.



**TURF SHOULDER DETAIL**  
NOT TO SCALE



CONSULTANTS

MATCHLINE STA. 113+25

FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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VILLAGE OF LANSING



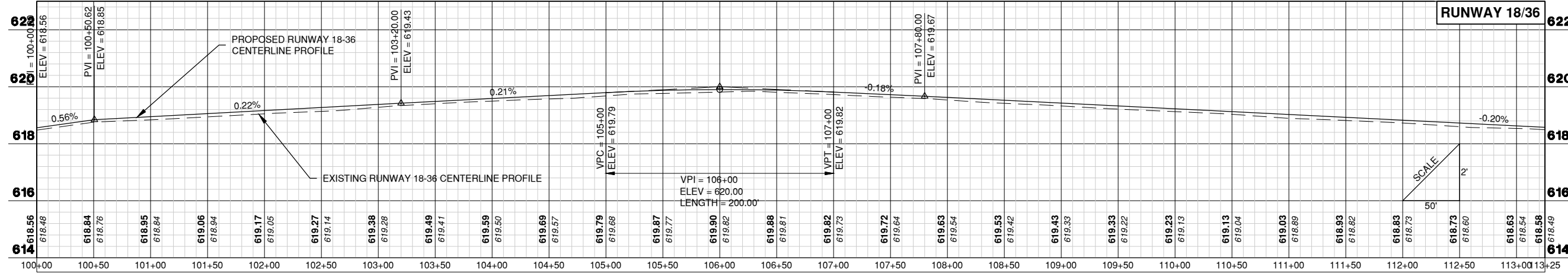
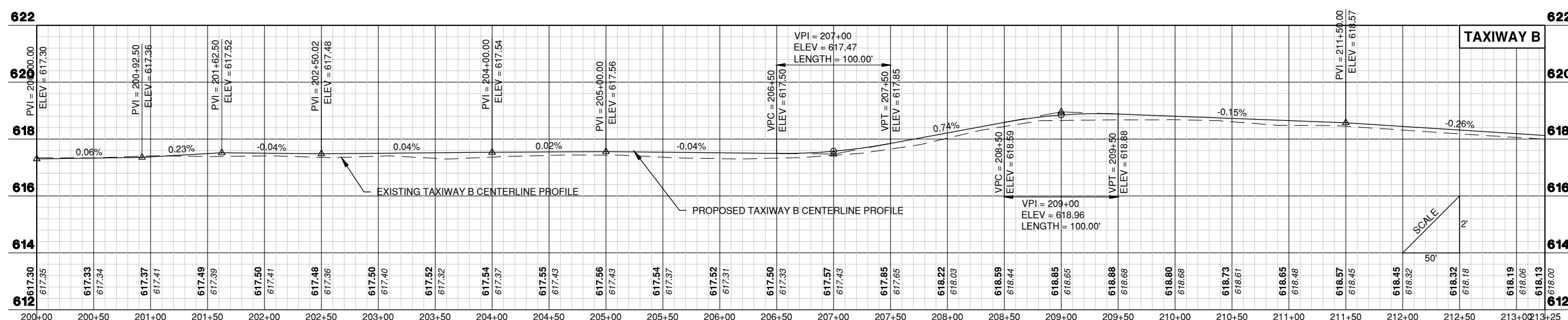
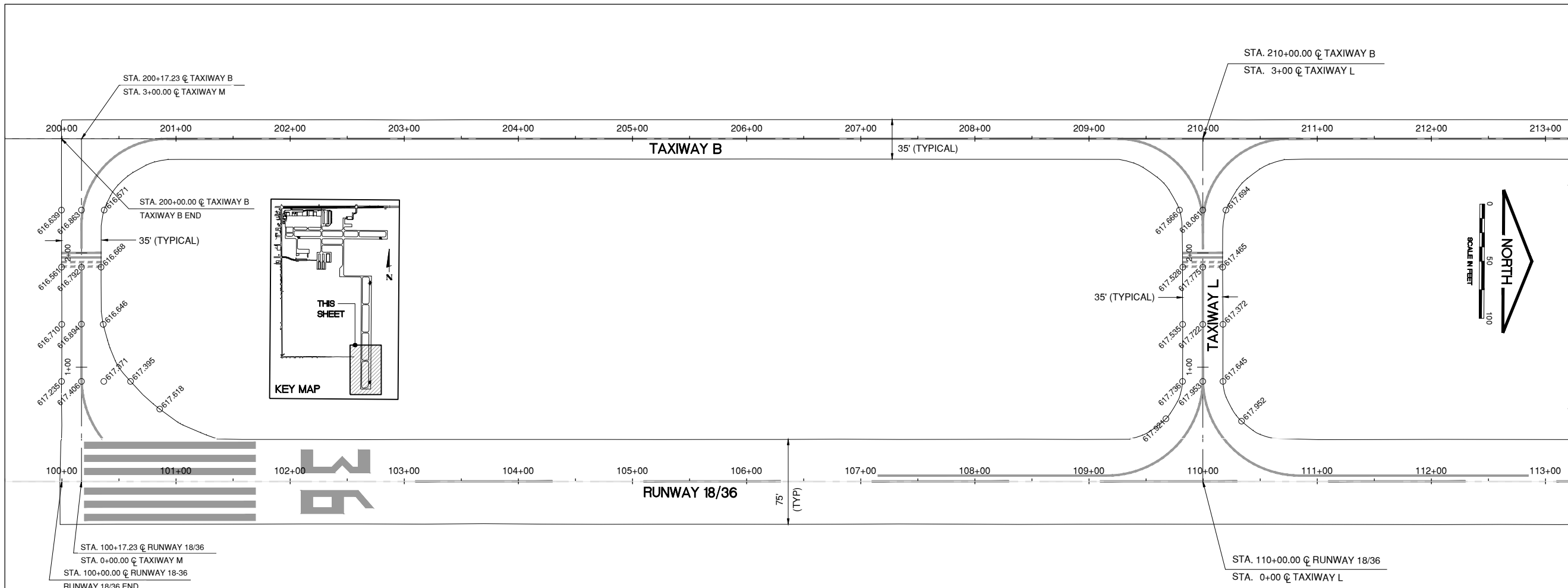
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SHEET TITLE  
**PLAN AND PROFILE  
RUNWAY 18-36 - 1**

CS200  
SHEET 15 OF 28



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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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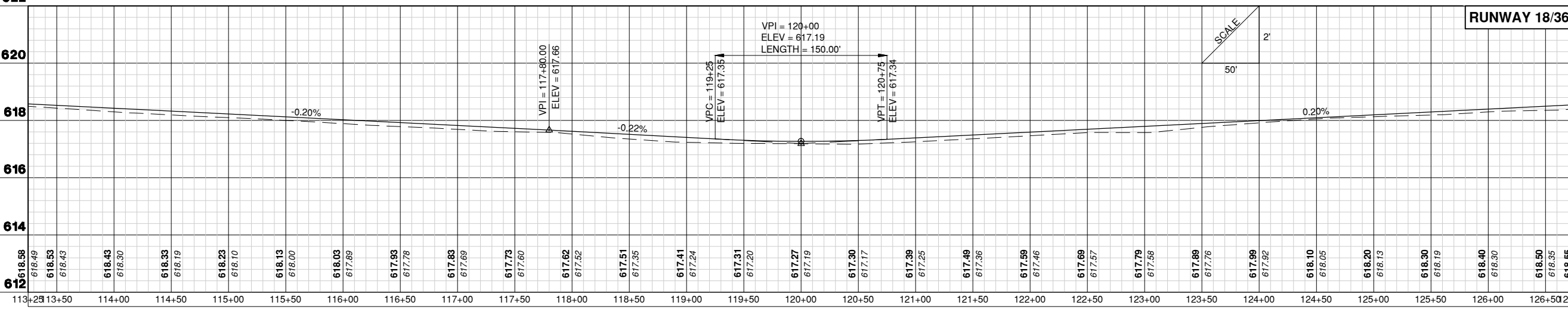
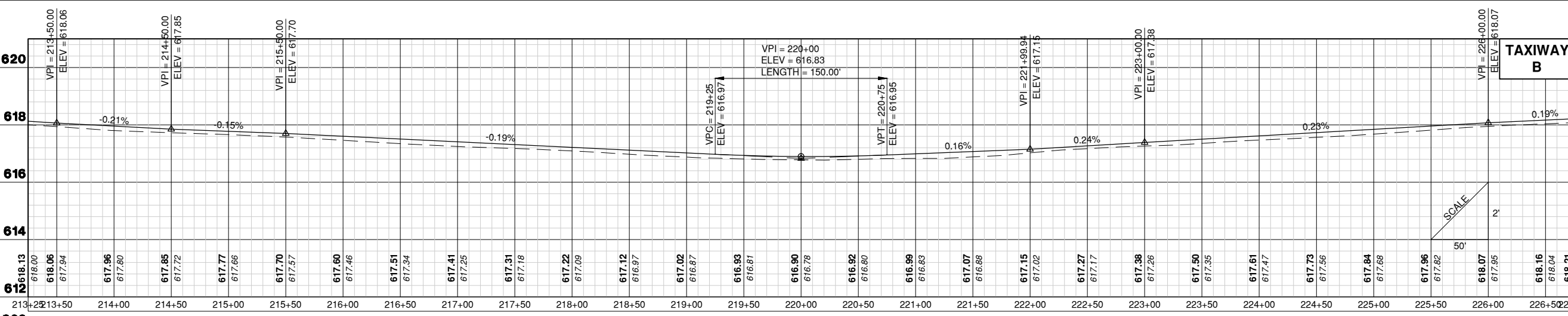
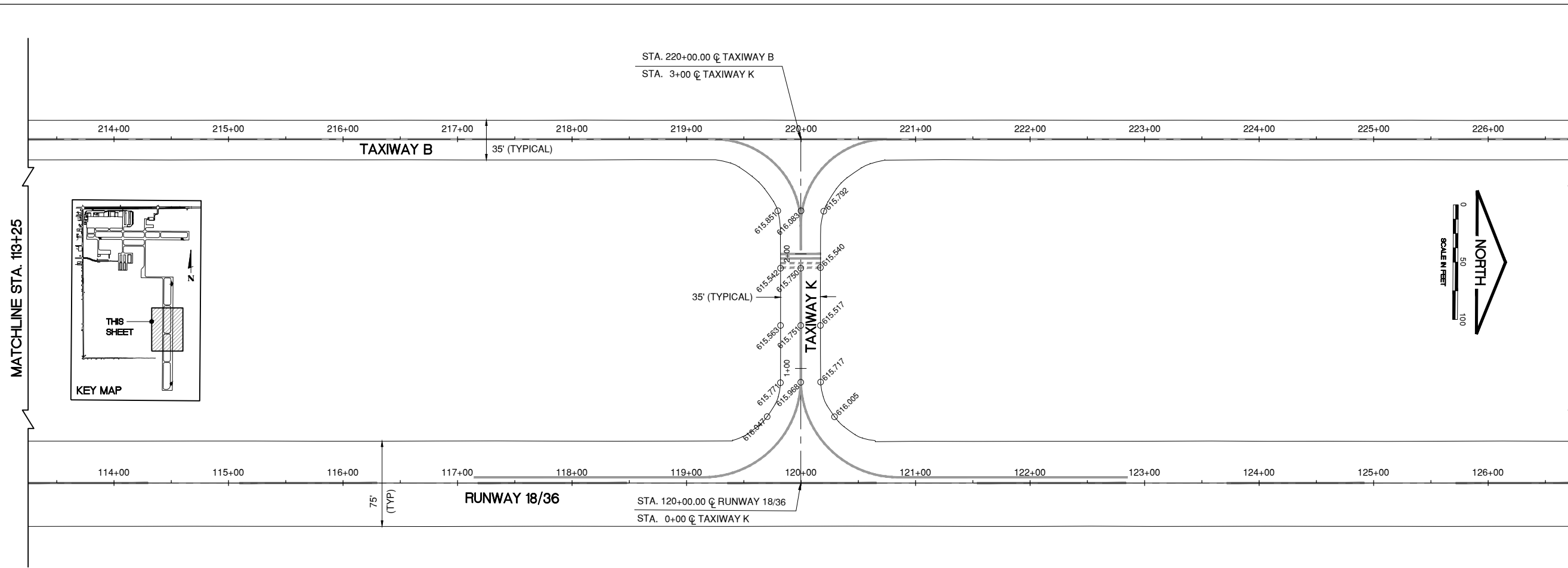
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SHEET TITLE  
**PLAN AND PROFILE  
RUNWAY 18-36 - 2**

CS201  
SHEET 16 OF 28



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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



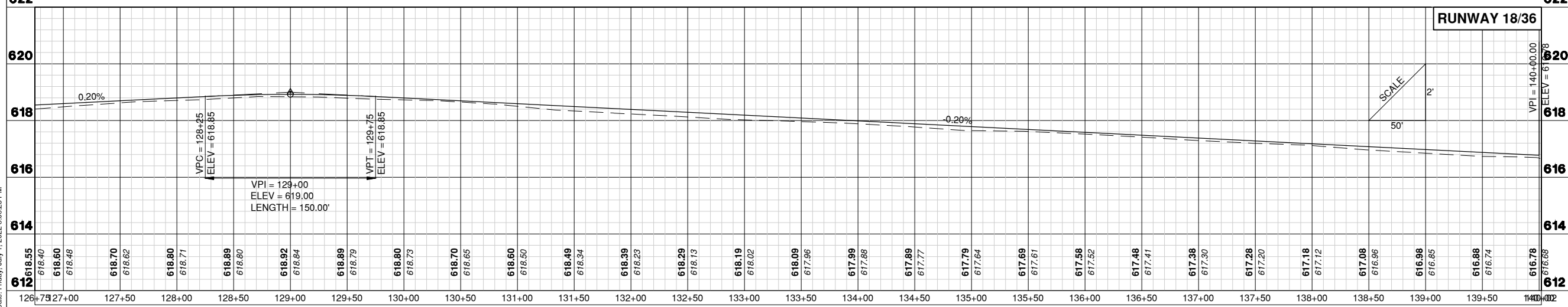
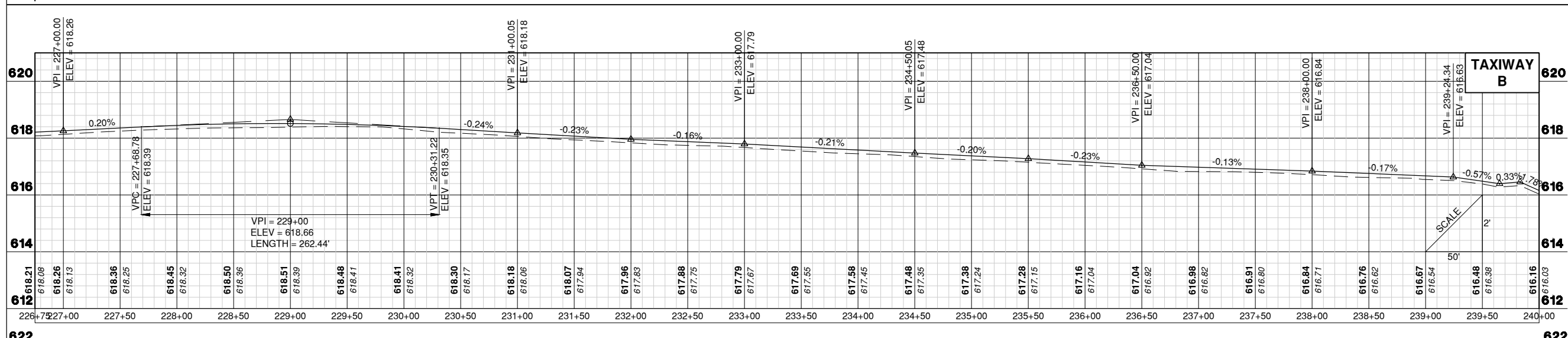
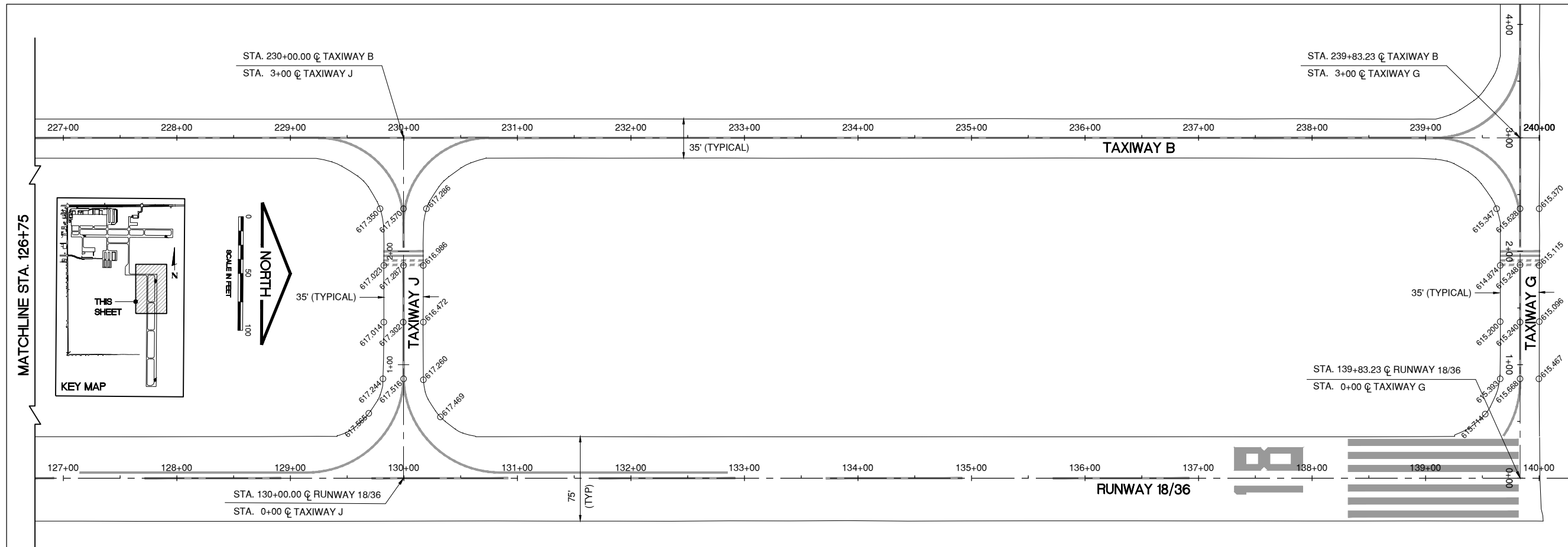
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RUNWAY 18-36 - 3**



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CONSULTANTS

FINAL  
JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G

OWNER



VILLAGE OF LANSING



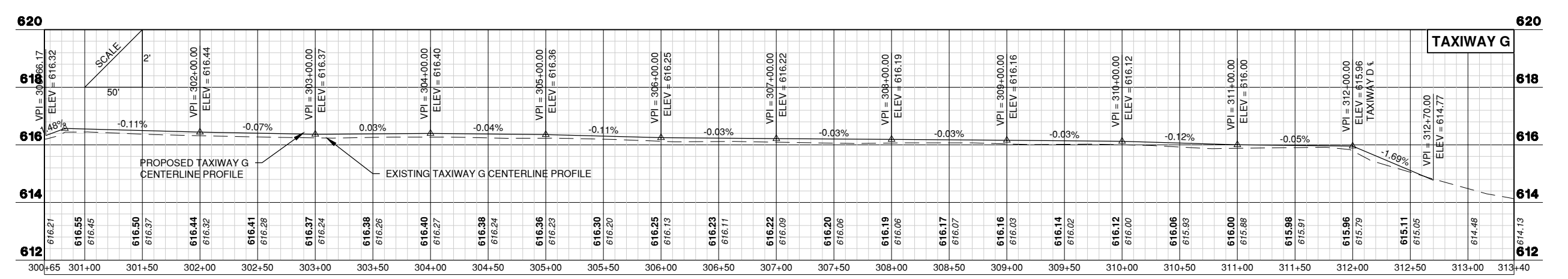
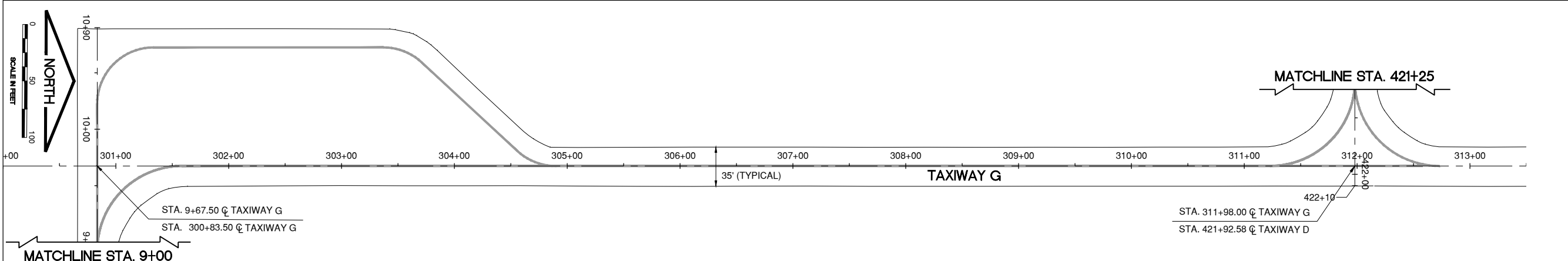
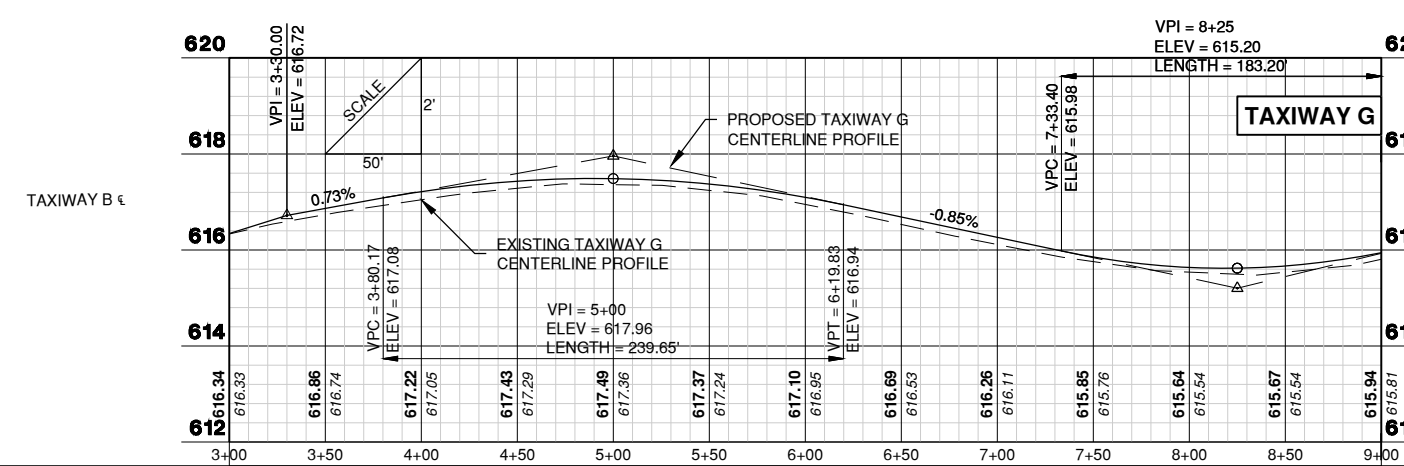
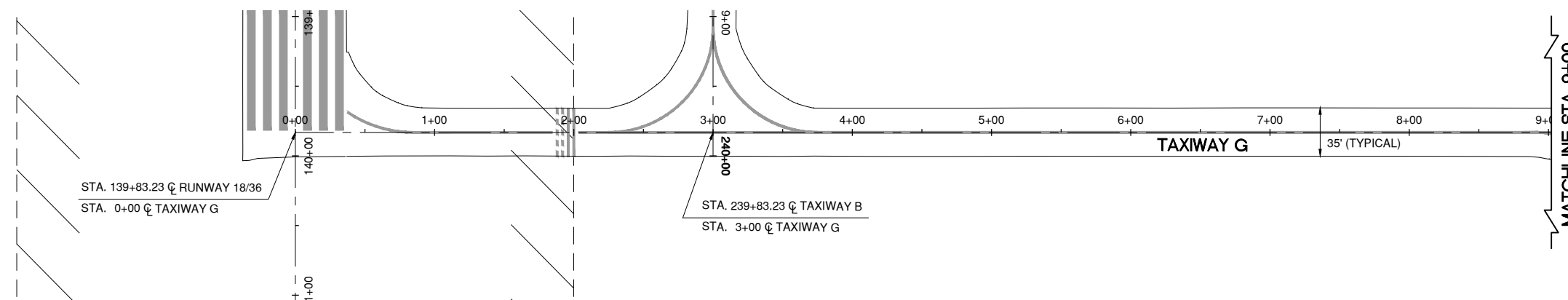
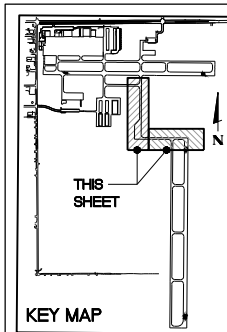
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SHEET TITLE  
**PLAN AND PROFILE  
TAXIWAY G**

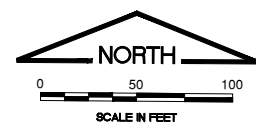
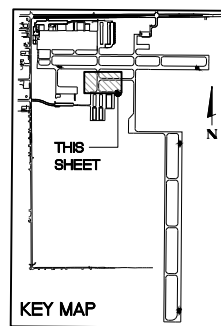
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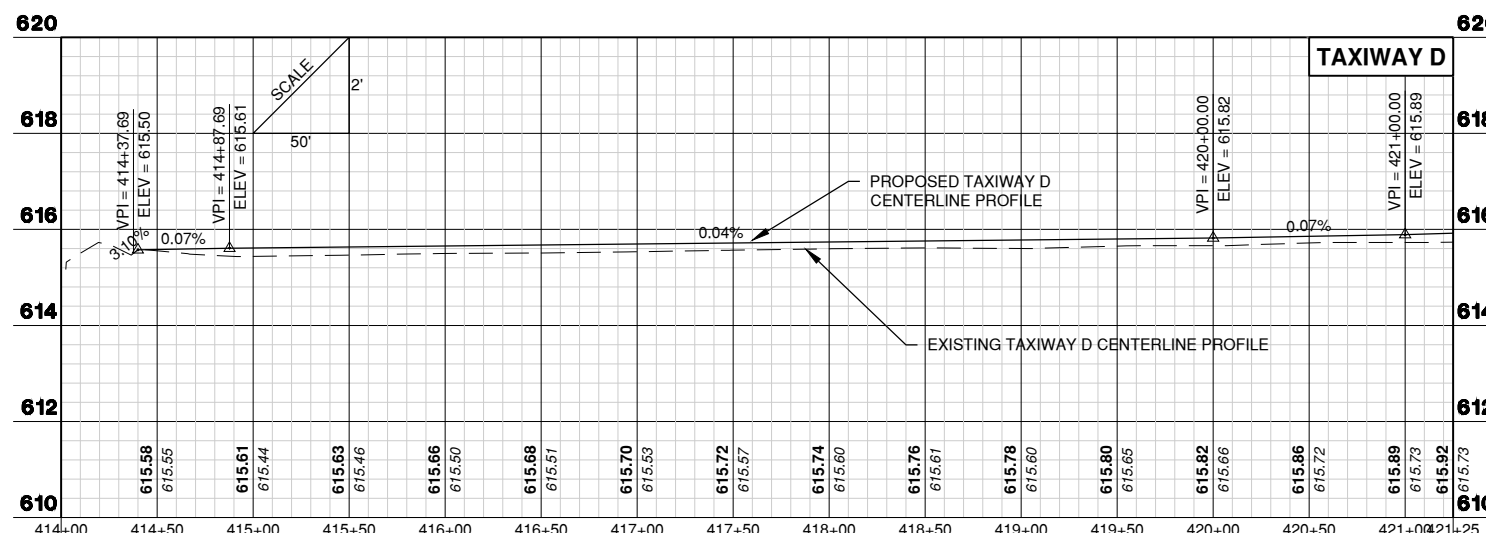
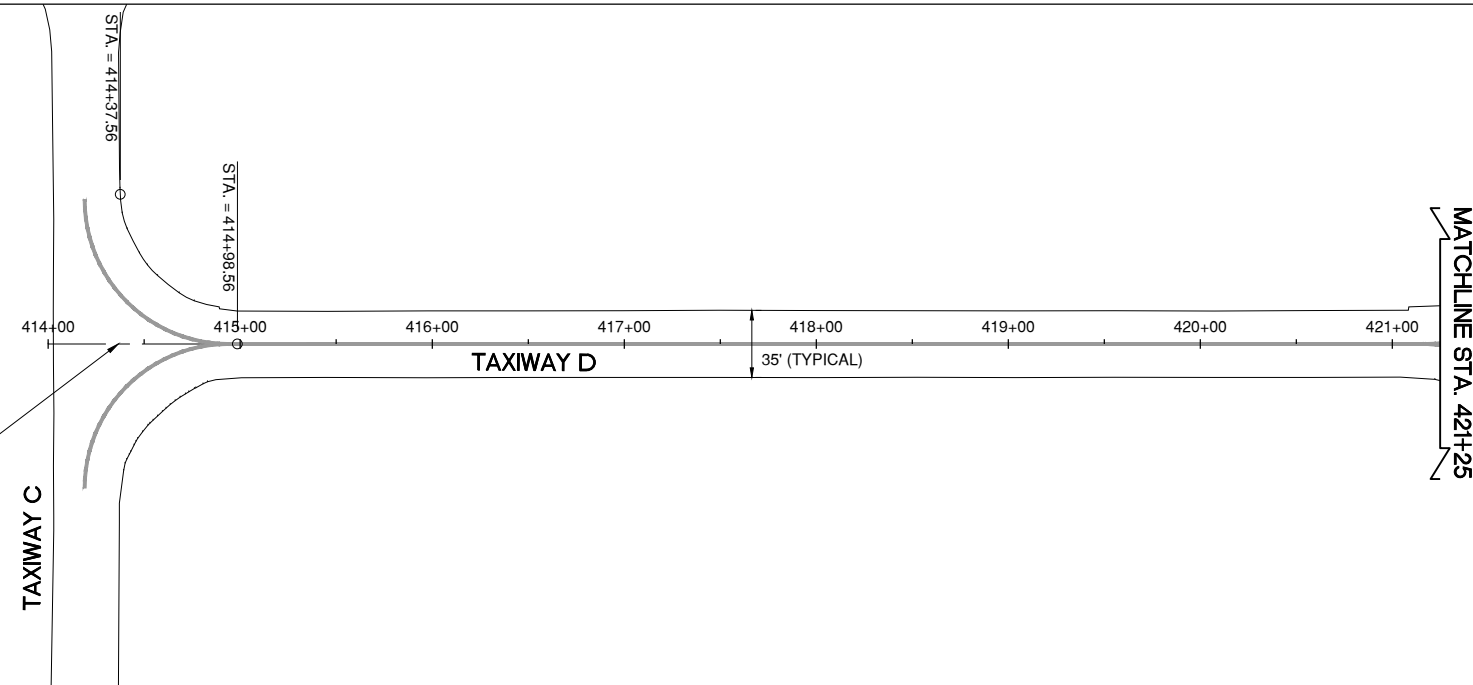
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STA. 414+36.50 C TAXIWAY G  
STA. 12+50.00 C TAXIWAY C, 17.50' LT.



FINAL  
JUNE 10, 2022

REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

VILLAGE OF LANSING



Lansing  
Municipal Airport

LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-XXX

IL PROJ. NO. IGQ-4941

CMT PROJECT NO: 200922-79-00

CAD DWG FILE: CS204.DWG

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SHEET TITLE

PLAN AND PROFILE  
TAXIWAY D

CS204

SHEET 19 OF 28



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TAXIWAY B AND TAXIWAY G



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MARK DATE DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-XXX  
IL PROJ. NO. IGQ-4941  
CMT PROJECT NO: 200922-79-00  
CAD DWG FILE: CS205.DWG  
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SHEET TITLE

RUNWAY AND  
TAXIWAY GRADE  
TABLES - 1

TAXIWAY B GRADING TABLE. Table with columns: STATION, 17.5' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 17.5' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), STATION. Rows range from 200+00.00 to 239+50.00.

RUNWAY 18/36 GRADING TABLE. Table with columns: STATION, 37.5' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 25' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 12.5' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 12.5' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 25' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 37.5' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), STATION. Rows range from 100+00.00 to 140+00.00.

Table with columns: STATION, 37.5' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 25' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 12.5' LEFT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 12.5' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 25' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), 37.5' RIGHT OF CENTERLINE (PROP ELEV., EXST ELEV., FILL DEPTH), STATION. Rows range from 100+00.00 to 140+00.00.

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REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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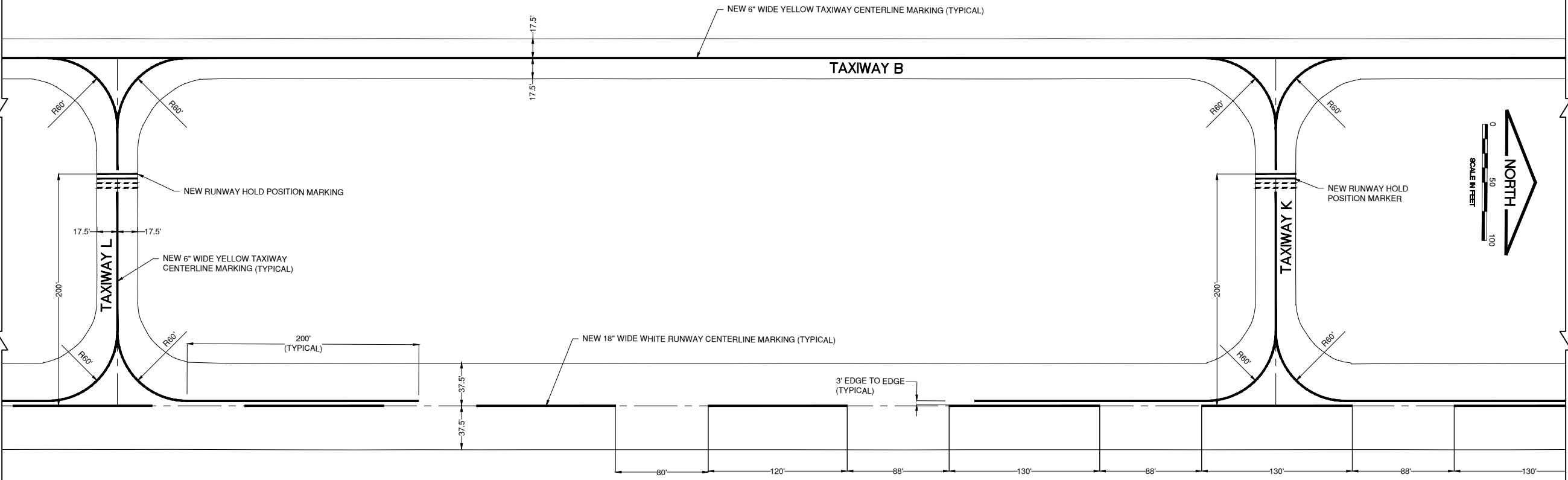
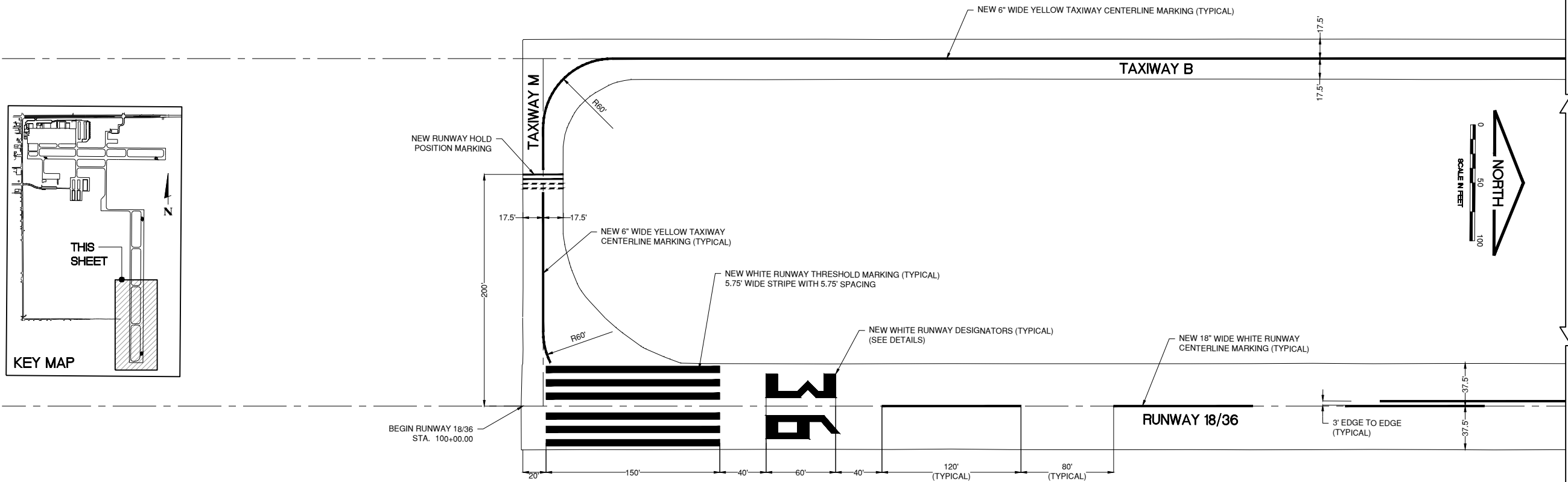
VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK	DATE	DESCRIPTION
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IL PROJ. NO. IGQ-4941		
CMT PROJECT NO: 200922-79-00		
CAD DWG FILE: CM100.DWG		
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SHEET TITLE  
**PAVEMENT MARKING  
PLAN - 1**





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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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VILLAGE OF LANSING



Lansing  
Municipal Airport

LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK DATE DESCRIPTION

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IL PROJ. NO. IGQ-4941

CMT PROJECT NO: 200922-79-00

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SHEET TITLE

PAVEMENT MARKING  
PLAN - 2

CM101

SHEET 23 OF 28

MATCHLINE STA. 122+50

MATCHLINE STA. 136+00

MATCHLINE STA. 136+00

MATCHLINE STA. 4+00

NEW 6" WIDE YELLOW TAXIWAY CENTERLINE MARKING (TYPICAL)

TAXIWAY B

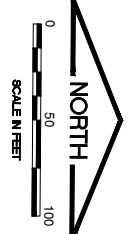
TAXIWAY J

RUNWAY 18/36

NEW RUNWAY HOLD POSITION MARKING

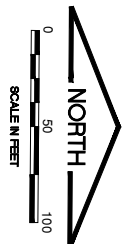
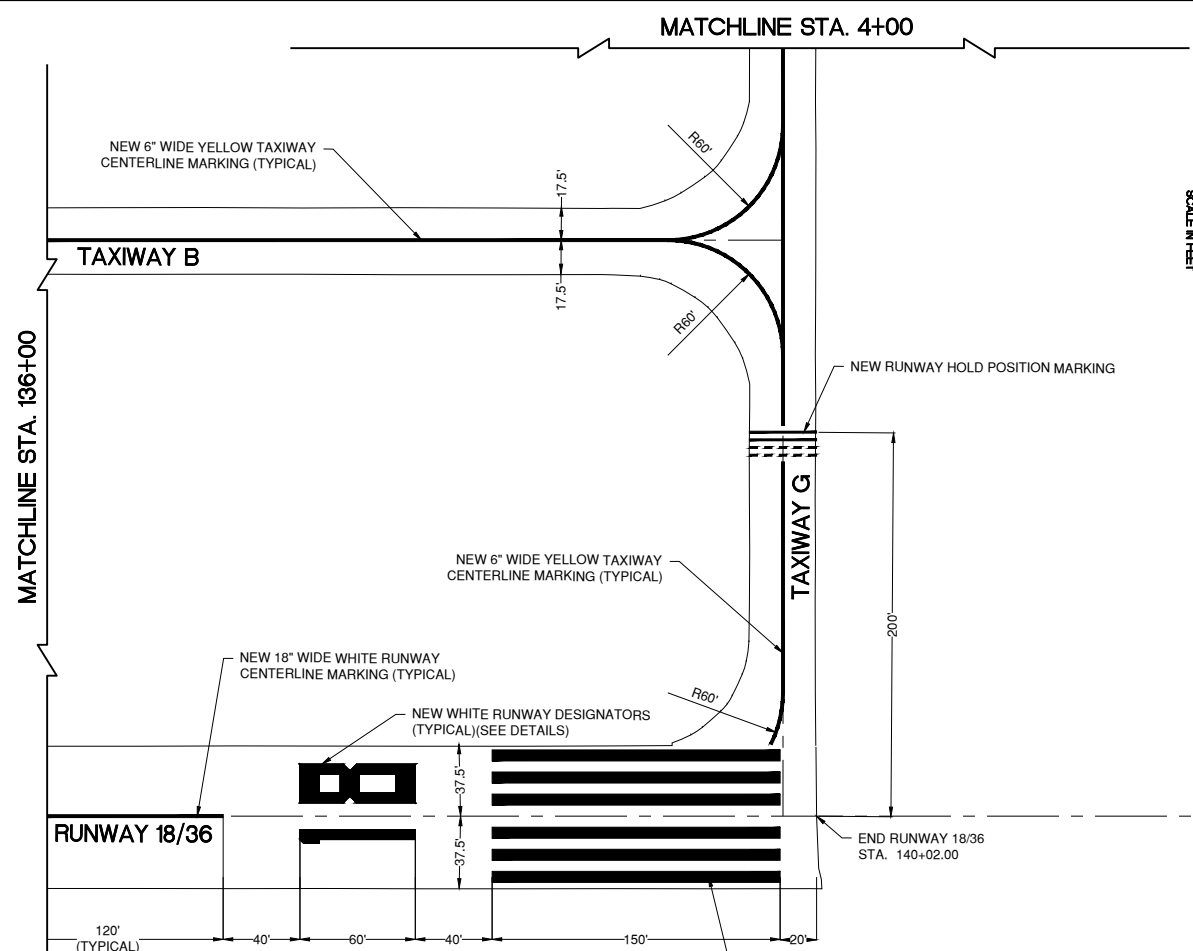
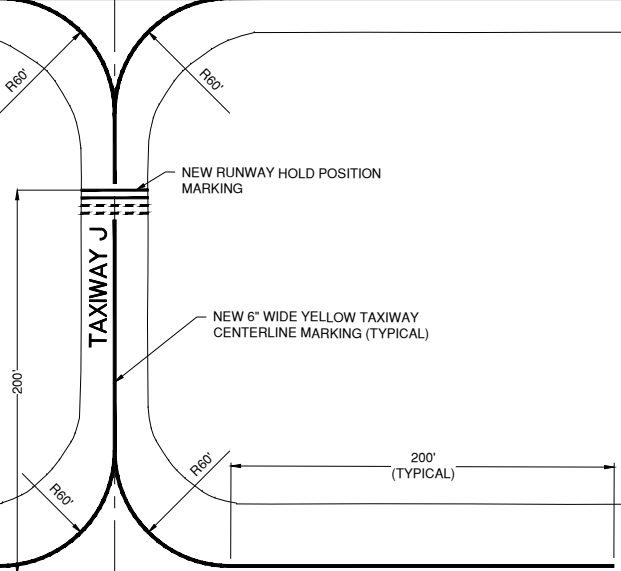
NEW 6" WIDE YELLOW TAXIWAY CENTERLINE MARKING (TYPICAL)

NEW 18" WIDE WHITE RUNWAY CENTERLINE MARKING (TYPICAL)



130' 88' 120' 80' 120' (TYPICAL) 80' 120' 80' (TYPICAL)

17.5' 37.5' 17.5'



RUNWAY 18/36

NEW WHITE RUNWAY DESIGNATORS (TYPICAL)(SEE DETAILS)

NEW RUNWAY HOLD POSITION MARKING

NEW 6" WIDE YELLOW TAXIWAY CENTERLINE MARKING (TYPICAL)

NEW 18" WIDE WHITE RUNWAY CENTERLINE MARKING (TYPICAL)

TAXIWAY G

120' (TYPICAL) 40' 60' 40' 150' 20'

37.5'

37.5'

17.5'

R60'

R60'

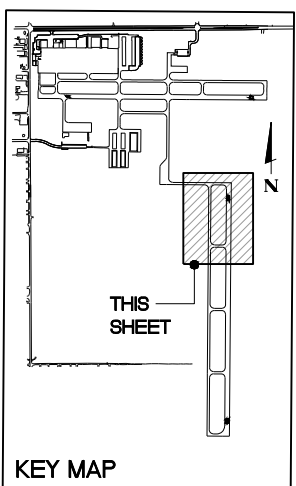
R60'

R60'

R60'

END RUNWAY 18/36 STA. 140+02.00

NEW WHITE RUNWAY THRESHOLD MARKING (TYPICAL) 5.75' WIDE STRIPE WITH 5.75' SPACING



Path: K:\lansing\20092279-00\_RehabRwy18-36\Draw\Sheets\CM101.dwg Date: Friday, July 1, 2022 3:06:50 PM



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REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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VILLAGE OF LANSING



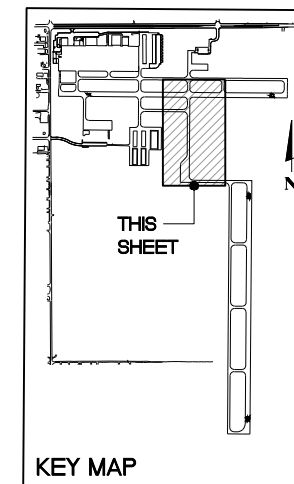
LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

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IL PROJ. NO. IGQ-4941
CMT PROJECT NO: 200922-79-00
CAD DWG FILE: CM102.DWG
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**PAVEMENT MARKING  
PLAN - 3**

CM102  
SHEET 24 OF 28



MATCHLINE STA. 4+00

NEW 6" WIDE YELLOW TAXIWAY  
CENTERLINE MARKING (TYPICAL)

TAXIWAY G

MATCHLINE STA. 303+50

MATCHLINE STA. 420+00

MATCHLINE STA. 303+50

NEW 6" WIDE YELLOW TAXIWAY  
CENTERLINE MARKING (TYPICAL)

TAXIWAY G

TAXIWAY D

RUNWAY 9/27

TAXIWAY A







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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-XXX

IL PROJ. NO. IGQ-4941

CMT PROJECT NO: 200922-79-00

CAD DWG FILE: CM103.DWG

DESIGNED BY: ARM

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: ARM

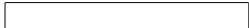


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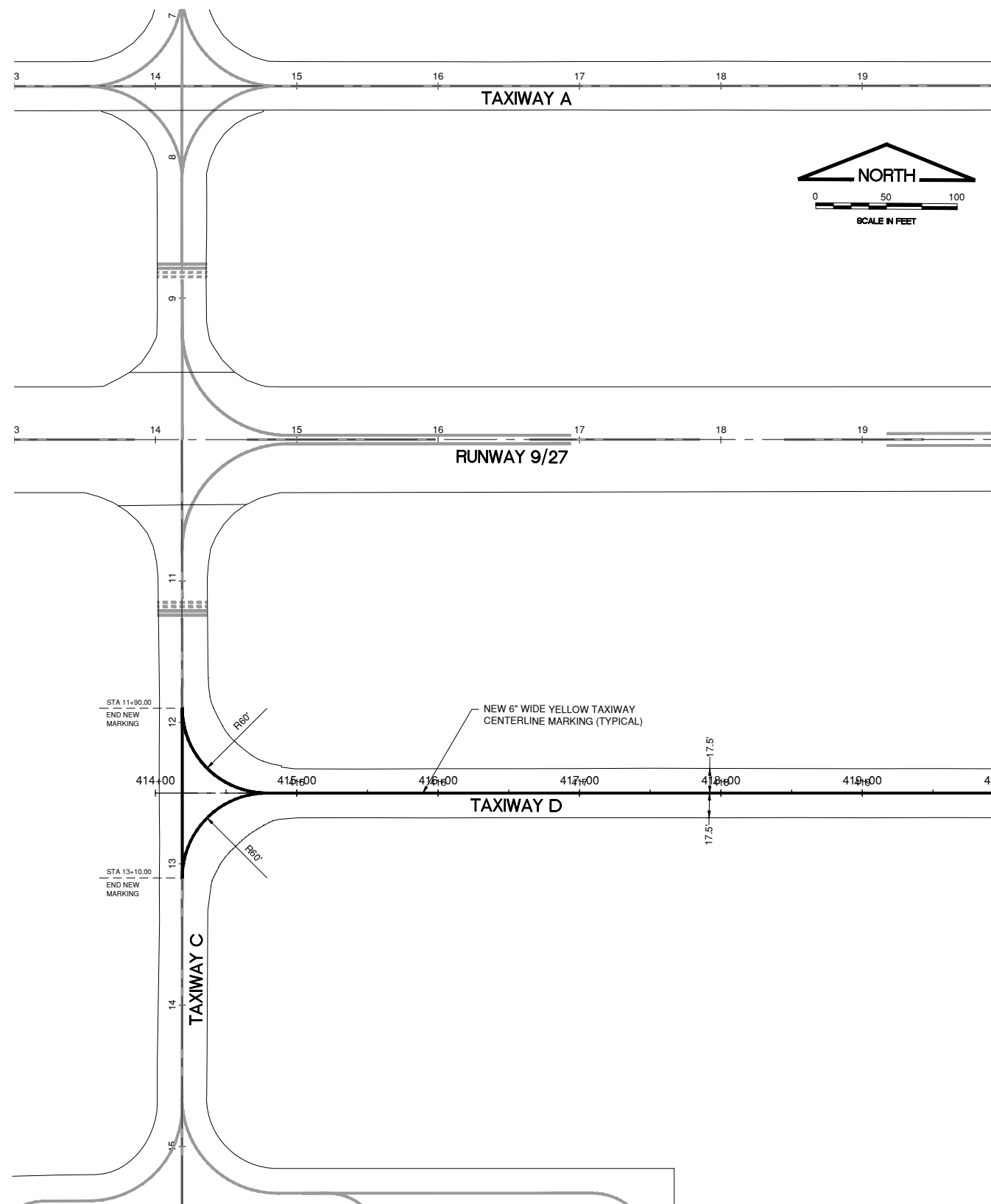
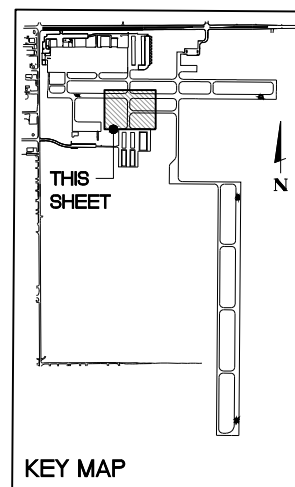
SHEET TITLE

PAVEMENT MARKING  
PLAN - 4

CM103  
SHEET 25 OF 28

**LEGEND**

	EXISTING PAVEMENT
	EXISTING PAVEMENT MARKING
	NEW PAVEMENT MARKING





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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



OWNER

VILLAGE OF LANSING



LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

MARK DATE DESCRIPTION

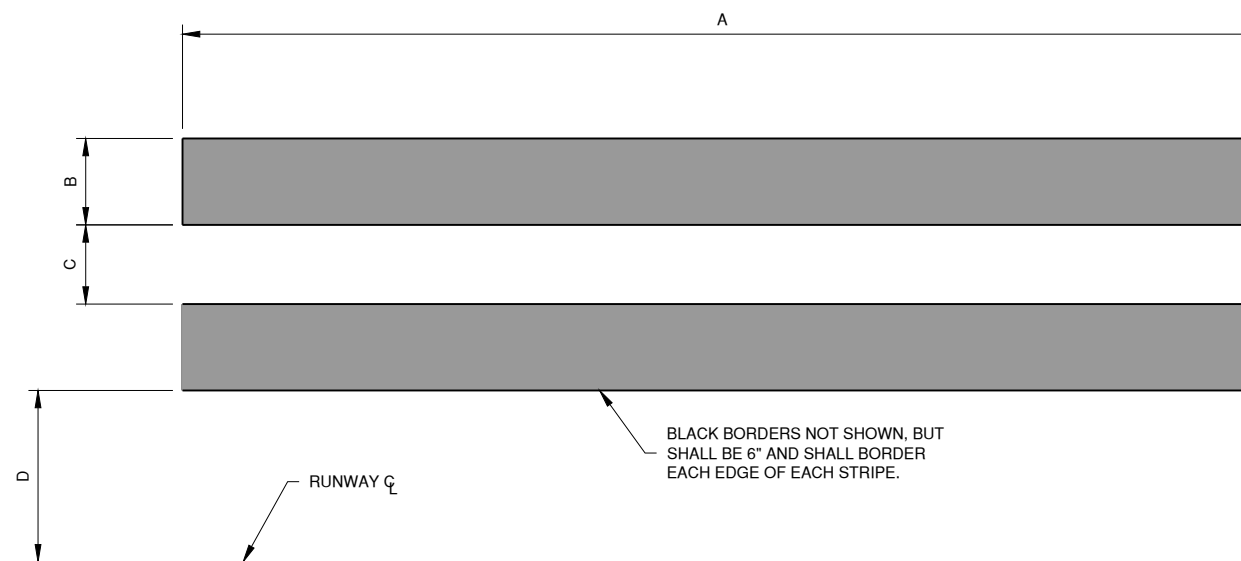
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CMT PROJECT NO.	200922-79-00
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SHEET TITLE  
**PAVEMENT MARKING  
DETAILS**

GC115  
SHEET 26 OF 28

**PAVEMENT MARKING NOTES**

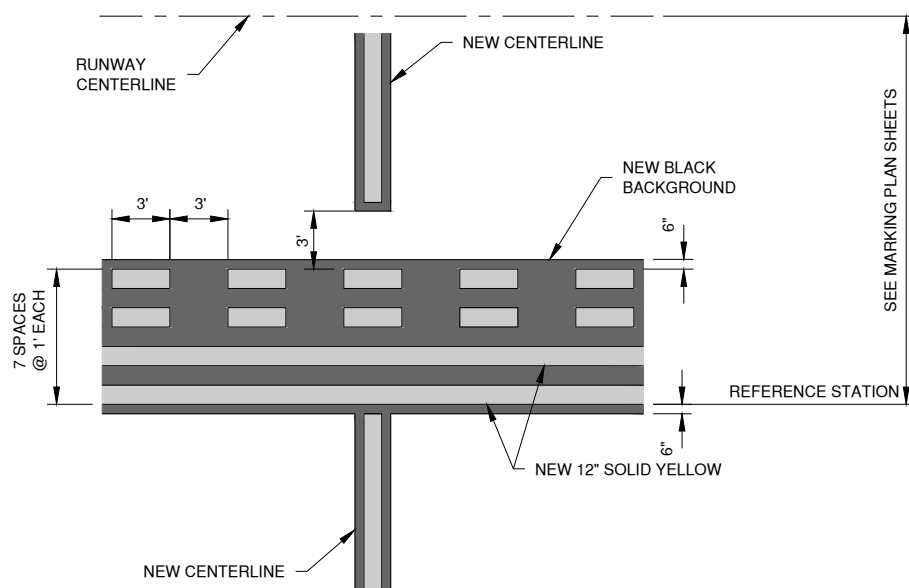
1. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
2. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE TERMINATED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE.
3. ALL NEW AIRFIELD PAVEMENT YELLOW AND WHITE MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER. REFLECTIVE BEADS SHALL BE INCIDENTAL TO PAVEMENT MARKING.
4. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
5. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1 (LATEST EDITION).
6. WHERE EXISTING MARKINGS ARE TO BE REPAINTED OVER, THE CONTRACTOR SHALL SCRAPE THE EXISTING MARKINGS TO REMOVE LOOSE PAINT PRIOR TO MARKING APPLICATION.
7. RUNWAY 18/36 SHALL BE MARKED WITH TWO COATS OF PAINT. THE FIRST COAT OF PAINT SHALL BE PLACED AT 50% OF THE SPECIFIED APPLICATION RATE PRIOR TO RUNWAY GROOVING AND REOPENING TO TRAFFIC. THE SECOND COAT SHALL BE APPLIED AFTER THE RUNWAY IS GROOVED. THE SECOND COAT AND REFLECTIVE BEADS SHALL BE APPLIED AT 100% OF THE SPECIFIED APPLICATION RATE.



MARKING (PER RUNWAY END)	DIMENSION *			
	A	B	C	D
THRESHOLD MARKING (6 STRIPES)	150'	5.75'	5.75'	5.75'

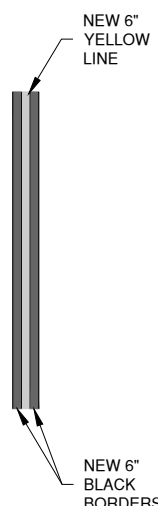
**RUNWAY THRESHOLD MARKING**

N.T.S.



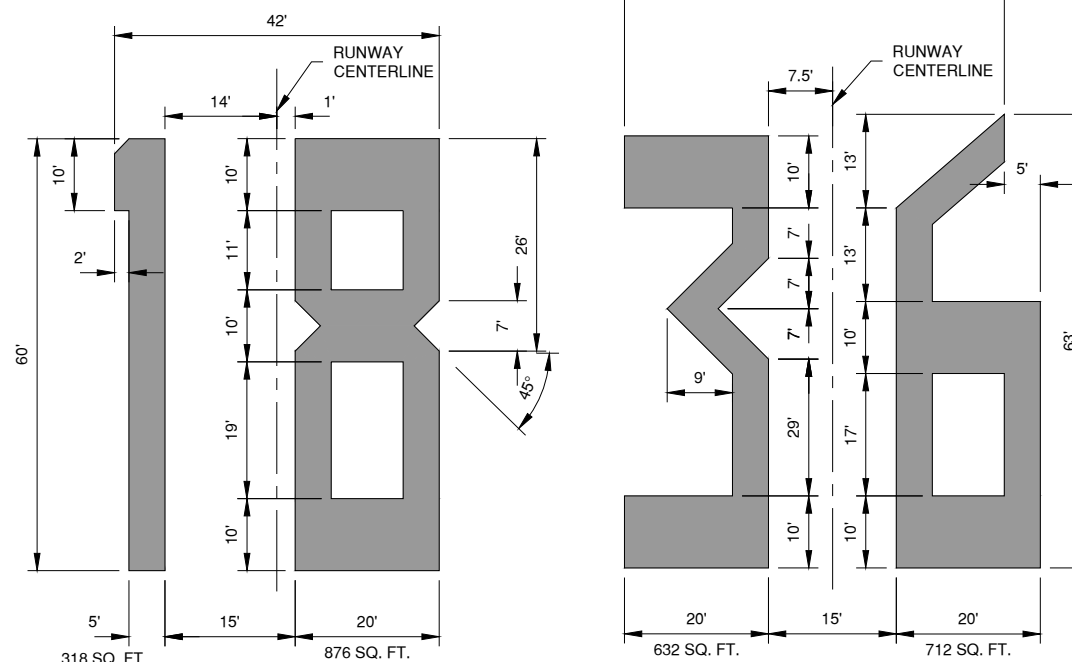
**RUNWAY HOLD POSITION MARKING**

N.T.S.



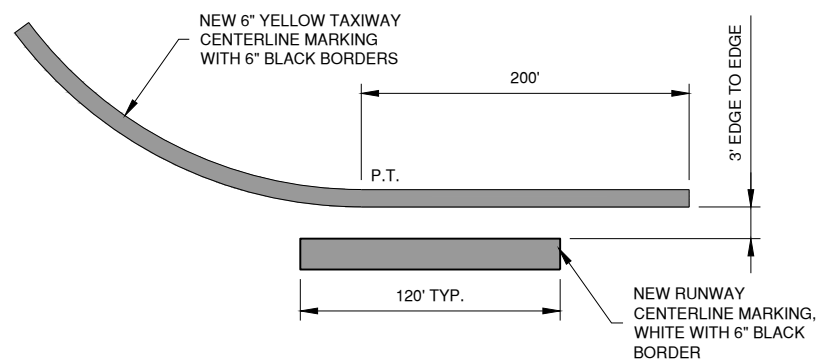
**TAXIWAY CENTERLINE MARKING**

N.T.S.



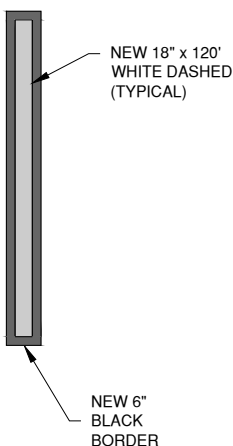
**RUNWAY 18/36 LANDING DESIGNATORS**

N.T.S.



**NEW TAXIWAY EXIT MARKING DETAIL**

N.T.S.

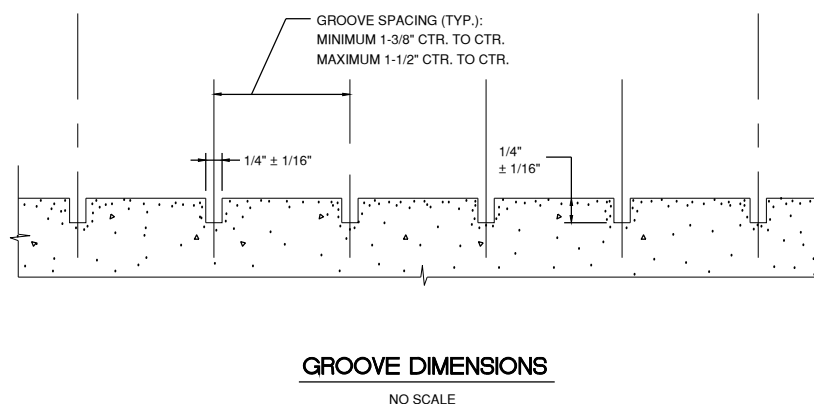
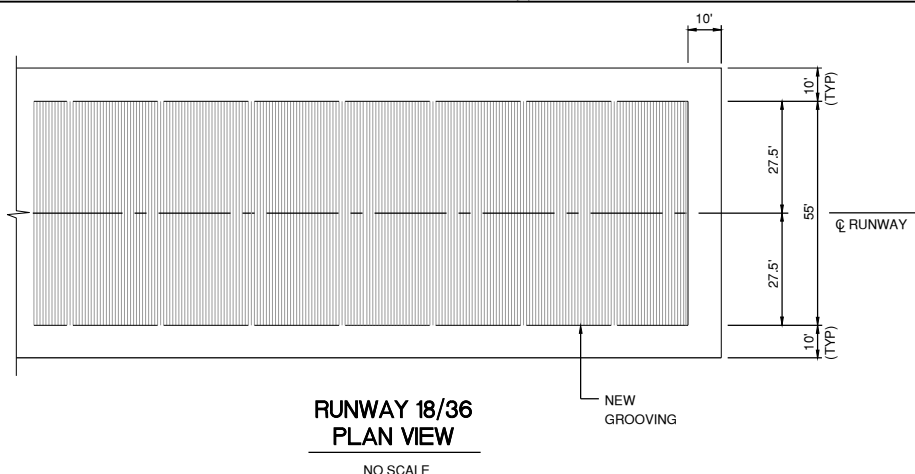
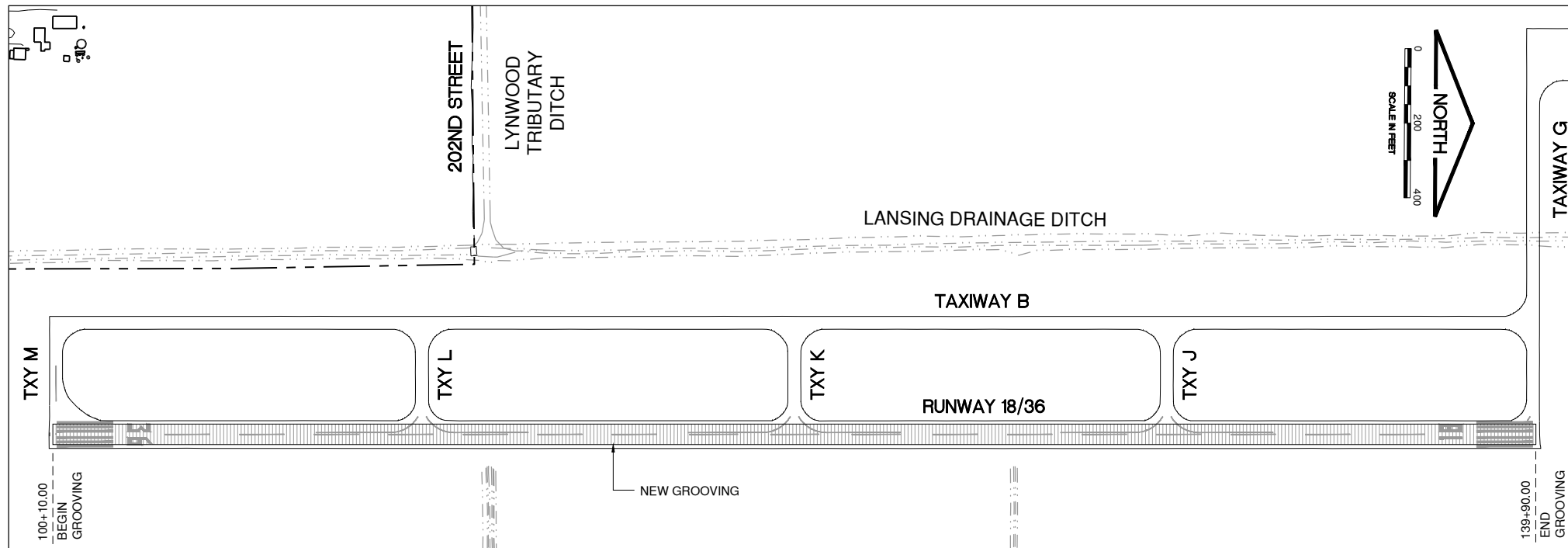
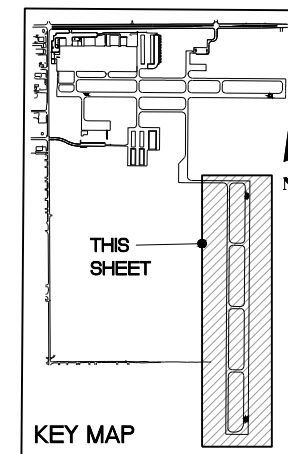


**RUNWAY CENTERLINE MARKING**

N.T.S.



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**GROOVING NOTES**

1. AFTER COMPLETION OF HMA PAVING, RUNWAY 18/36 SHALL BE ALLOWED TO CURE A MINIMUM OF 30 DAYS PRIOR TO THE COMMENCEMENT OF GROOVING OPERATIONS.
2. SEE PAVEMENT MARKING PLANS AND NOTES.
3. GROOVING SHALL EXTEND OUTWARD 27.5' FROM CENTERLINE OF RUNWAY 18/36.
4. SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
5. EXTREME CARE SHALL BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES AND SUBSURFACE WIRING. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" TO SUCH FACILITIES.
6. CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.

**GROOVING PLAN**

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JUNE 10, 2022  
REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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Lansing Municipal Airport  
LANSING, ILLINOIS

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AIP PROJ. NO. 3-17-SBGP-XXX

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CMT PROJECT NO: 200922-79-00

CAD DWG FILE: GROOVING PLAN.DWG

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GROOVING PLAN AND MISCELLANEOUS DETAILS

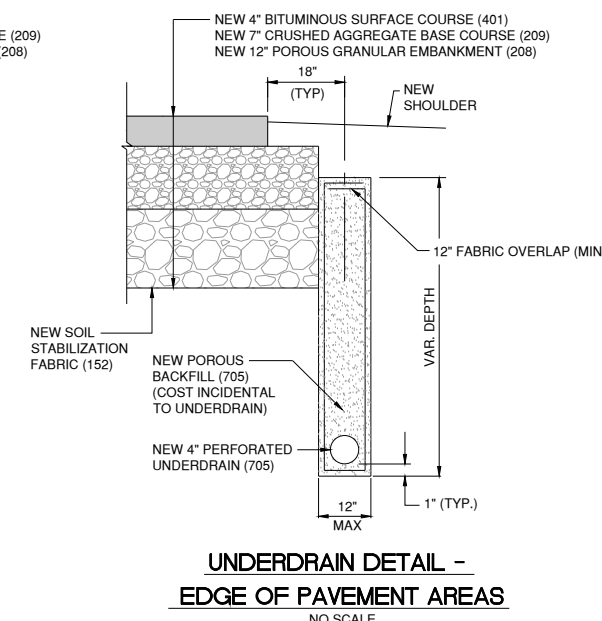
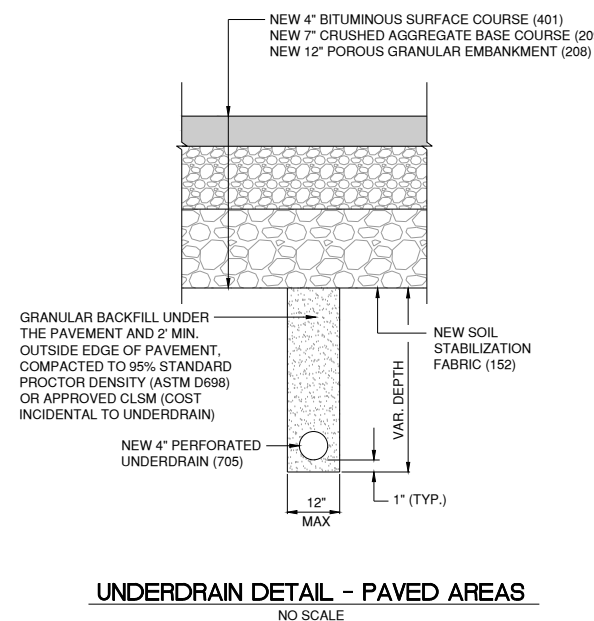
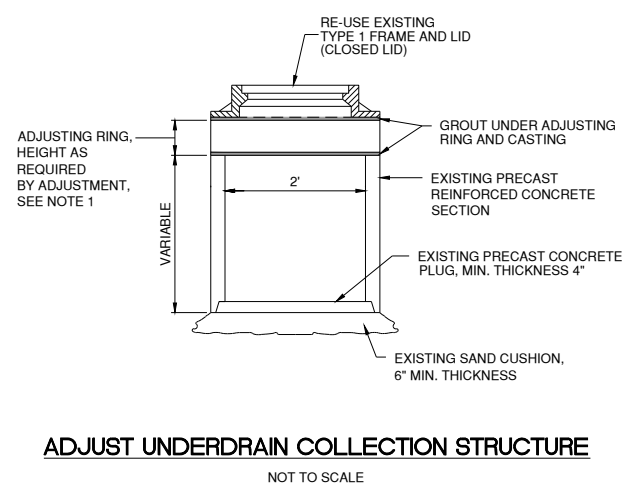
GV100

SHEET 27 OF 28

**MISCELLANEOUS DETAILS**

**STRUCTURE ADJUSTMENT NOTES:**

1. CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
2. THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
3. THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
4. ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
5. CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
6. TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
7. AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
8. EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.





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REHABILITATE RUNWAY 18-36,  
TAXIWAY B AND TAXIWAY G



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LANSING MUNICIPAL AIRPORT  
LANSING, ILLINOIS

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IL PROJ. NO. IGQ-4941

CMT PROJECT NO: 200922-79-00

CAD DWG FILE: GC500.DWG

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**TAXIWAY A  
IMPROVEMENTS - ADDITIVE  
ALTERNATE NO. 1**

GC500  
SHEET 28 OF 28

