

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

F.A.P. 344: (ILLINOIS ROUTE 83)

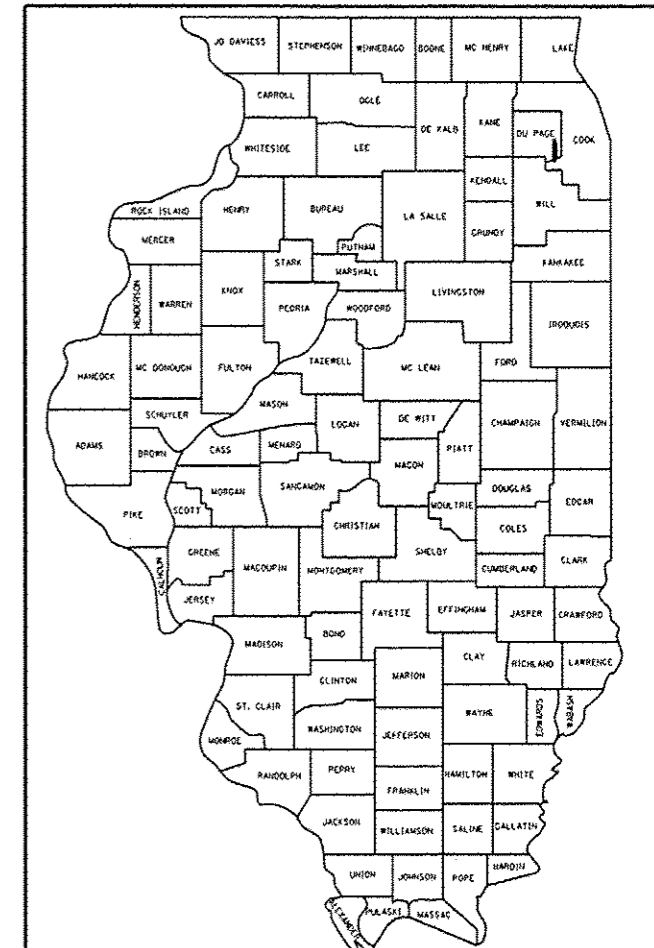
SECTION: 544-RS-4

THE COOK COUNTY LINE TO KNOLLWOOD ROAD RESURFACING (3P)

DUPAGE COUNTY C-91-322-11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60N52		

D-91-322-11

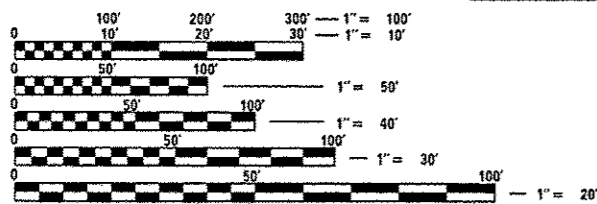


LOCATION OF SECTION INDICATED THUS: - [shaded box] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED WITHIN THE VILLAGE OF WILLOWBROOK, THE CITY OF DARIEN, AND THE VILLAGE OF BURR RIDGE

TRAFFIC DATA
ILLINOIS ROUTE 83
2009 ADT = 29,000-55,000
SPEED LIMIT = 45 MPH



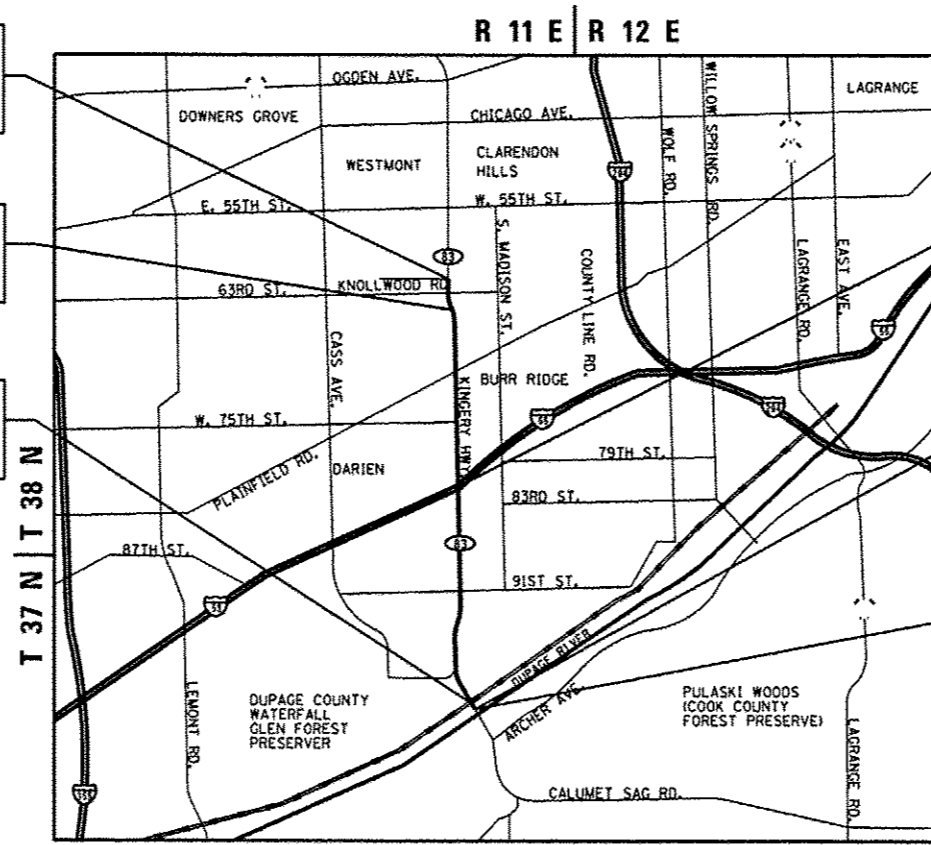
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENDS
STA. 379 + 00

ROADWAY OMISSION
STA. 361 + 57 TO
STA. 367 + 22.57

BRIDGE OMISSION
STA. 109 + 04 TO
STA. 117 + 67



BRIDGE OMISSION
STA. 238 + 79 TO STA. 243 + 42
STA. 239 + 32 TO STA. 243 + 87

ROADWAY OMISSION
STA. 104 + 92.8 TO
STA. 109 + 04

PROJECT BEGINS
STA. 104 + 92.8

SCALE: NTS
DOWNS GROVE TOWNSHIP
GROSS LENGTH OF PROJECT = 27,442 FT = 5.19 MILES
NET LENGTH OF PROJECT = 25,550 FT = 4.84 MILES

PROJECT ENGINEER: KARI SMITH (847) 705-4437
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60N52

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 9, 2014

John Fortman, P.E.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

July 18, 2014

Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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LIST OF STATE STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
202001-01	EARTH MEDIAN DITCH CHECK
280001-07	TEMPORARY EROSION CONTROL SYSTEM
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAINS
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-10	STEEL PLATE BEAM GUARDRAIL
631011-09	TRAFFIC BARRIER TERMINAL, TYPE 2
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701101-04	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24' (600 mm) FROM PAVEMENT EDGE
701411-08	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701421-06	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY FOR SPEEDS > OR = 45 MPH TO 55 MPH
701426-06	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS > OR = 45 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES

GENERAL NOTES

ALL CONSTRUCTION SHALL BE DONE ACCORDING TO THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JAN. 1, 2012. THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2013. THE DETAILS IN THESE PLANS, AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, THE VILLAGE OF WILLOWBROOK, THE CITY OF DARIEN, AND THE VILLAGE OF BURR RIDGE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, THE TRAFFIC FIELD TECHNICIAN AT (847)741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

COMMITMENTS

THE CONTRACTOR SHALL CONTACT TIM HALIK, VILLAGE ADMINISTRATOR OF THE VILLAGE OF WILLOWBROOK AT (630) 920-2261 TWO WEEKS BEFORE THE START OF CONSTRUCTION.

THE CONTRACTOR SHALL CONTACT DAN COMBAC, DIRECTOR COMMUNITY DEVELOPMENT/MUNICIPAL SERVICES FOR THE CITY OF DARIEN AT (630) 353-8106 THREE WEEKS BEFORE THE START OF CONSTRUCTION.

FILE NAME :	USER NAME : mcoronado	DESIGNED -	REVISED -	STATE OF ILLINOIS	INDEX OF SHEETS, LIST OF IDOT HIGHWAY STANDARDS, GENERAL NOTES AND COMMENTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
\\p-work\p\dot\mcoronado\003166\012211-shc-plan.dgn	DRAWN -	REVISED -	344			544-RS-4	DUPAGE	53	2	
PLOT SCALE = 100.0000' / in	CHECKED -	REVISED -	CONTRACT NO. 60N52							
Default	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
					SHEET 1 OF 1 SHEETS STA. TO STA.					

GENERAL NOTES

THE LOCATIONS AND ELEVATION OF THE VARIOUS UNDERGROUND UTILITIES AS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL USE SPECIAL CARE WHEN CONDUCTING CONSTRUCTION OPERATIONS NEAR THEM AND IS RESPONSIBLE FOR ANY DAMAGE CAUSED.

THE CONTRACTOR SHALL NOTIFY THE AGENCIES AND UTILITIES AT LEAST 10 DAYS PRIOR TO ANY CONSTRUCTION IN THE AREA AND SHALL COMPLY WITH ALL RESTRICTIONS FOR EQUIPMENT MOVEMENTS AND CLEARANCES IN REGARDS TO THEIR FACILITIES.

THE CONTRACTOR SHALL COORDINATE WITH ALL ABOVE AND UNDERGROUND UTILITY COMPANIES IF UTILITIES NEED TO BE RELOCATED.

THE REMOVAL OF GAURDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER LINEAL FOOT FOR "GUARDRAIL REMOVAL."

TOP SOIL SHALL BE FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH WEEDS AND OTHER LITTER AND FREE OF ROOT STUMPS, STONES AND OTHER MATERIAL HARMFUL TO PLANT GROWTH, TOP SOIL MUST MEET REQUIREMENTS OF ARTICLE 1081.05.

THE COST OF CONNECTING EXISTING PIPE CULVERTS TO THE PROPOSED PIPE CULVERTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED PIPE CULVERT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, OR CATCH BASINS. THEY SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. THEY SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT ROADSIDE DRAINAGE SYSTEM IS BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE INCLUDED IN THE COST OF GRADING AND SHAPING DITCHES.

PROPOSED END SECTIONS SHALL BE SLOPED TO MATCH THE PROPOSED CULVERTS PIPE SLOPES. STORM SEWER PIPE LENGTHS IN SCHEDULES DO NOT INCLUDE THE LENGTH OF END SECTION WHERE APPLICABLE. DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. DRAINAGE STRUCTURES MAY REQUIRE REVISIONS TO MEET EXISTING FIELD CONDITIONS. ANY ADJUSTMENT SHALL BE AS DIRECTED BY THE ENGINEER.

ALL SUBSURFACE DRAINS AND/OR FIELD TILES ENCOUNTERED IN THE ROADWAY EXCAVATION SHALL BE REMOVED AND CONSIDERED TO BE INCLUDED IN THE COST OF GRADING AND SHAPING DITCHES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING TO THE PROPOSED DITCHES AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE OWNER. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE INCLUDED IN THE COST OF GRADING AND SHAPING DITCHES.

TRANSVERSE UNDERDRAINS SHALL MATCH THE CROSS SLOPE OF THE ROADWAY SURFACE.

REMOVAL OF EXISTING PIPE DRAINS WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

REMOVAL OF PIPE CULVERT END SECTIONS SHALL BE PAID PER FOOT UNDER PIPE CULVERT REMOVAL.

THE PROPOSED DRAINAGE STRUCTURES AND PIPES SHALL BE PROTECTED IMMEDIATELY AFTER THEY ARE CONSTRUCTED UNTIL THE SURFACES THAT DRAIN TO THEM ARE FULLY PAVED OR VEGETATED.

FILE NAME :	USER NAME : mecranada	DESIGNED -	REVISED -	STATE OF ILLINOIS	INDEX OF SHEETS, LIST OF IDOT HIGHWAY STANDARDS, GENERAL NOTES AND COMMENTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pwork\puidot\mecranada\0816166\0130211\shd-plan.dgn	DRAWN -	REVISED -	344			544-RS-4	DUPAGE	53	3		
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -			CONTRACT NO. 60N52					
Default	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SHEET 1 OF 1 SHEETS		STA.	TO STA.			

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005				
20800150	TRENCH BACKFILL	CU YD	18	18				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	692	692				
21400100	GRADING AND SHAPING DITCHES	FOOT	227	227				
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	13	13				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	13	13				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	13	13				
25100125	MULCH, METHOD 3	ACRE	0.25	0.25				
25100630	EROSION CONTROL BLANKET	SQ YD	692	692				
25200110	SODDING, SALT TOLERANT	SQ YD	588	588				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	13	13				
28000305	TEMPORARY DITCH CHECKS	FOOT	38	38				
28000500	INLET AND PIPE PROTECTION	EACH	4	4				
28100105	STONE RIPRAP, CLASS A3	SQ YD	52	52				
28200200	FILTER FABRIC	SQ YD	52	52				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	453	453				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	11455	11455				
40600895	CONSTRUCTING TEST STRIP	EACH	4	4				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1503	1503				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	997	997				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	612	612				
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	24207	24207				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6858	6858				
42001300	PROTECTIVE COAT	SQ YD	1176	1176				
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	50165	50165				
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	191778	191778				
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	2374	2374				
44004250	PAVED SHOULDER REMOVAL	SQ YD	22	22				
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	1400	1400				

FILE NAME :	USER NAME : mtrando	DESIGNED -	REVISED -
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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				F.A.P. RTE. 344	SECTION 544-RS-4	COUNTY	TOTAL SHEETS 53	SHEET NO. 4
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60N52					
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

* SPECIALTY ITEM

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005			
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	95	95			
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	899	899			
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	340	340			
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	1040	1040			
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	95854	95854			
48101620	AGGREGATE SHOULDERS, TYPE B 10"	SQ YD	39	39			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1418	1418			
50104400	CONCRETE HEADWALL REMOVAL	EACH	1	1			
50105220	PIPE CULVERT REMOVAL	FOOT	82	82			
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	2	2			
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	64	64			
542A1063	PIPE CULVERTS, CLASS A, TYPE 2 18"	FOOT	24	24			
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	1	1			
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	7	7			
60250200	CATCH BASINS TO BE ADJUSTED	EACH	3	3			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005			
60255500	MANHOLES TO BE ADJUSTED	EACH	5	5			
60260100	INLETS TO BE ADJUSTED	EACH	25	25			
60262700	INLETS TO BE RECONSTRUCTED	EACH	10	10			
60404950	FRAMES AND GRATES, TYPE 24	EACH	29	29			
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	4	4			
63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	86.5	86.5			
63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1			
63200310	GUARDRAIL REMOVAL	FOOT	96	96			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6			
67100100	MOBILIZATION	L SUM	1	1			
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1			
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1			

FILE NAME :	USER NAME :	DESIGNED -	REVISED -
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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RATE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	5
CONTRACT NO. 60N52				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

* SPECIALTY ITEM

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005			
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	235917	235917			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	3266	3266			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	89004	89004			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	14828	14828			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	13059	13059			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	4438	4438			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1542	1542			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	56576	56576			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	3266	3266			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	89004	89004			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	14828	14828			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	13059	13059			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	4438	4438			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1542	1542			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2159	2159			
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	2	2			
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	39	39			
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2159	2159			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	5518	5518			
K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	L SUM	1	1			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	560	560			
X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	173444	173444			
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	8455	8455			
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1752	1752			
X5538000	STORM SEWERS TO BE CLEANED 18"	FOOT	152	152			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	12	12			
XZ043900	PREFORMED JOINT FILLER REMOVAL	FOOT	22188	22188			

FILE NAME : c:\pr... \work\mtr\mtr\mtr\031650-013221-11-plan.dwg	USER NAME : mtr\mtr	DESIGNED -	REVISED -
PLOT SCALE : 100,0000' / 1" =		DRAWN -	REVISED -
PLOT DATE : 6/15/2014		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.P. RTE. 344	SECTION 544-RS-4	COUNTY DUPAGE	TOTAL SHEETS 53	SHEET NO. 6
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 60N52				

* SPECIALTY ITEM

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER	FOOT	3527	3527				
	REMOVAL AND REPLACEMENT							
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	105	105				
Z0030850	TEMPORARY INFORMATION SIGNING	50 FT	257	257				
Z0034105	MATERIAL TRANSFER DEVICE	TON	24207	24207				
X0327771	SAWCUT AND SEAL NEW JOINTS	FOOT	86	86				
X0327584	GRADING AND SHAPING SHOULDERS, SPECIAL	FOOT	99	99				
X0327585	CAST-IN-PLACE REINFORCED CONCRETE END	EACH	1	1				
	SECTIONS 2-18", SPECIAL							
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	140	140				
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
Ø 7007660	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005				

12

FILE NAME =	USER NAME = mtr/mtr	DESIGNED -	REVISED -
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PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -
PLOT DATE = 6/15/2014		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	

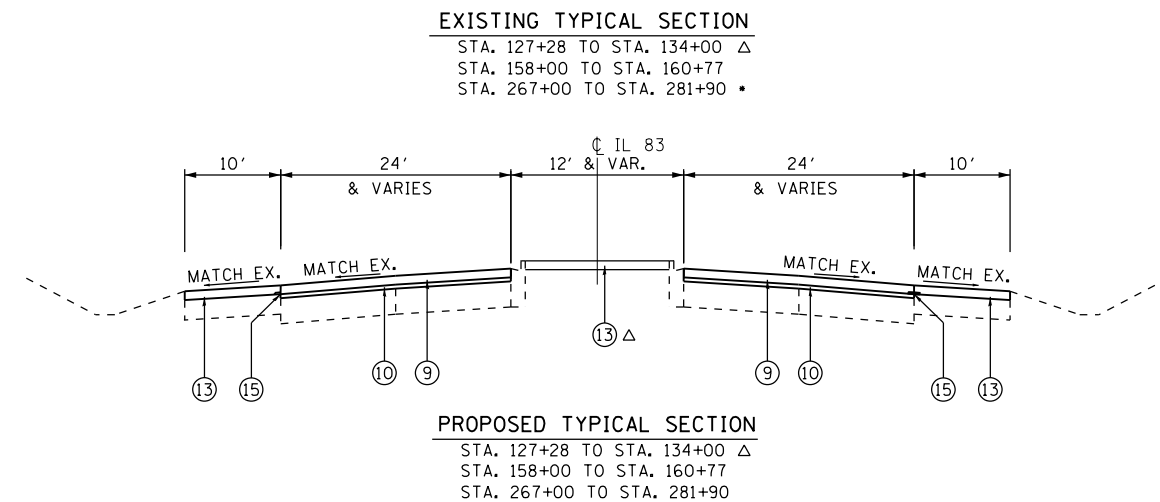
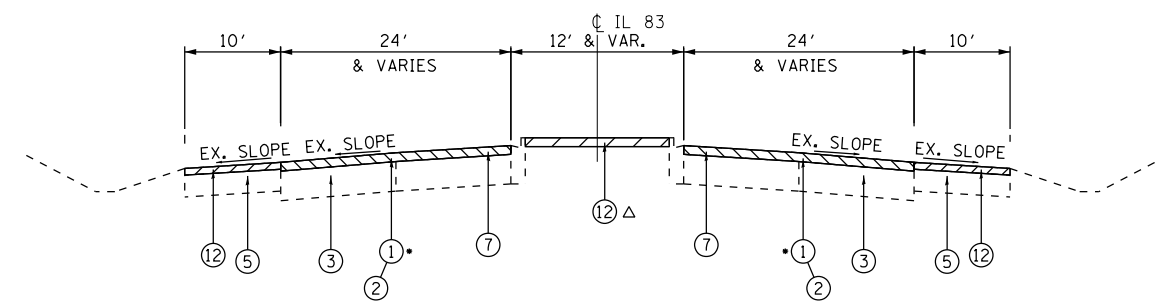
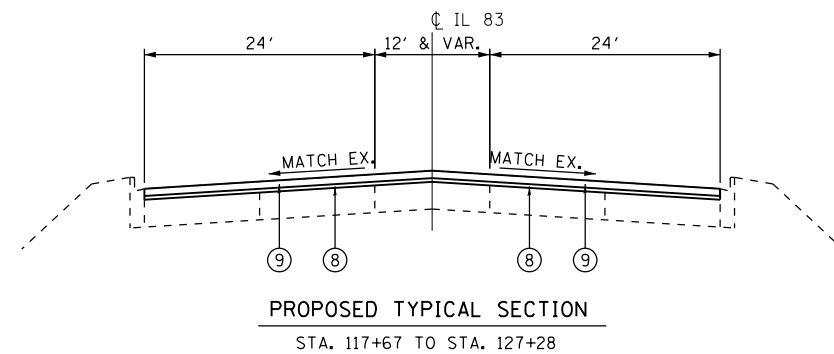
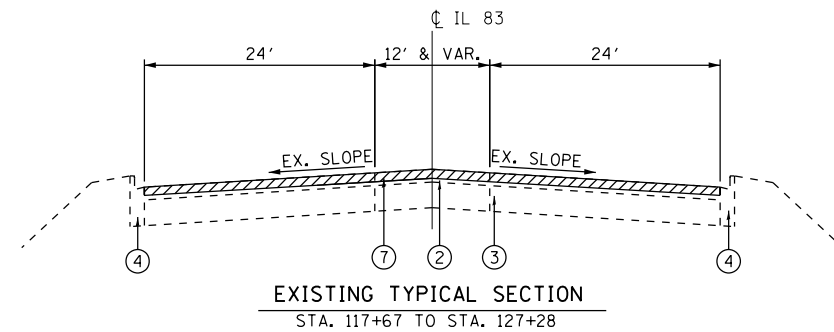
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4		53	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60N52	

Rev.

* SPECIALTY ITEM

LEGEND

- ① EXISTING +/-3" HMA (BEFORE MILLING)
- ② EXISTING +/-4" HMA (BEFORE MILLING)
- ③ EXISTING +/-10" P.C.C. BASE
- ④ EXISTING B-6.24 CURB AND GUTTER
- ⑤ EXISTING BITUMINOUS SHOULDER +/-8"
- ⑥ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑦ HMA SURFACE REMOVAL - 3" (SEE NOTE 1)
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE NOTE 2)
- ⑪ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ HMA SURFACE REMOVAL - 2"
- ⑬ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑭ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -3"
- ⑮ STRIP REFLECTIVE CRACK CONTROL TREATMENT



NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
2. CLASS D PATCH TYPE IV, 8 INCH TO BE USED ON 8 INCH HMA SHOULDER. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
3. REFER TO DETAIL HMA TAPER AT EDGE OF P.C.C. PAVEMENT FOR DEPTH OF MILLING AT GUTTER FLAG.

OMISSIONS

STA. 109+04 TO STA. 117+67

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS	Ndes	
PAVEMENT RESURFACING			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (IL-12.5 mm)	3.5% @ 80 GYR.		PFP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.		PFP
HMA SHOULDER RESURFACING			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm)	4% @ 70 GYR.		OCP
HMA MEDIAN RESURFACING			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm)	4% @ 70 GYR.		OCP
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.		QC/OA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.		QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP); PAY FOR PERFORMANCE (PFP)			

MIXTURE NOTES:

- 1.) THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- 2.) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- 3.) QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

LEGEND

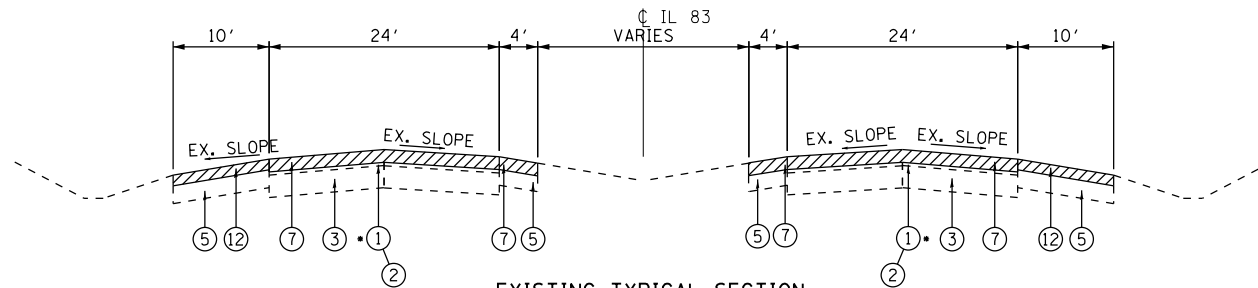
- ① EXISTING +/-3" HMA (BEFORE MILLING)
- ② EXISTING +/-4" HMA (BEFORE MILLING)
- ③ EXISTING +/-10" P.C.C. BASE
- ④ EXISTING B-6.24 CURB AND GUTTER
- ⑤ EXISTING BITUMINOUS SHOULDER +/-8"
- ⑥ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑦ HMA SURFACE REMOVAL - 3" (SEE NOTE 1)
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE NOTE 2)
- ⑪ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ HMA SURFACE REMOVAL - 2"
- ⑬ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑭ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -3"
- ⑮ STRIP REFLECTIVE CRACK CONTROL TREATMENT

NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
2. CLASS D PATCH TYPE IV, 8 INCH TO BE USED ON 8 INCH HMA SHOULDER. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
3. REFER TO DETAIL HMA TAPER AT EDGE OF P.C.C. PAVEMENT FOR DEPTH OF MILLING AT GUTTER FLAG.

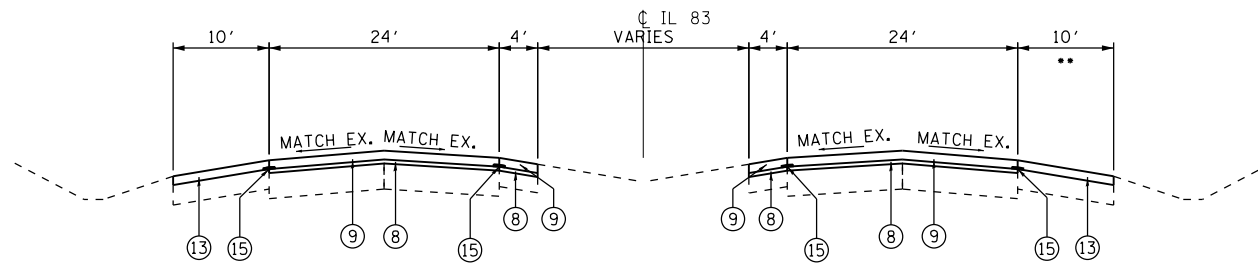
OMISSIONS

- STA. 238+79 TO STA. 243+42 (SB)
- STA. 239+32 TO STA. 243+87 (NB)



EXISTING TYPICAL SECTION

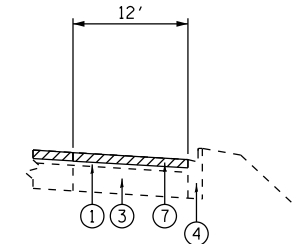
STA. 134+00 TO STA. 158+00 •
 STA. 160+77 TO STA. 199+00 •
 STA. 281+90 TO STA. 349+48 (SB)
 STA. 281+90 TO STA. 349+61 (NB) ••



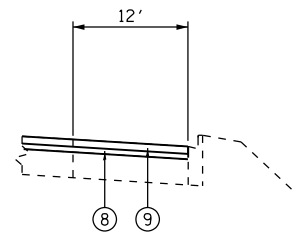
PROPOSED TYPICAL SECTION

STA. 134+00 TO STA. 158+00
 STA. 160+77 TO STA. 199+00
 STA. 281+90 TO STA. 349+48 (SB)
 STA. 281+90 TO STA. 349+61 (NB) ••

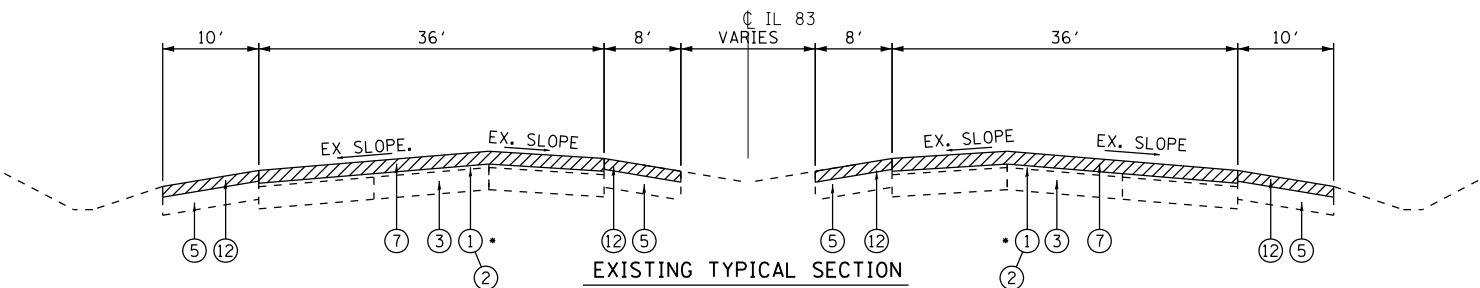
•• SUBSTITUTE 10' SHOULDER FOR 12' LANE



STA. 291+74 TO STA. 311+73 (NB)

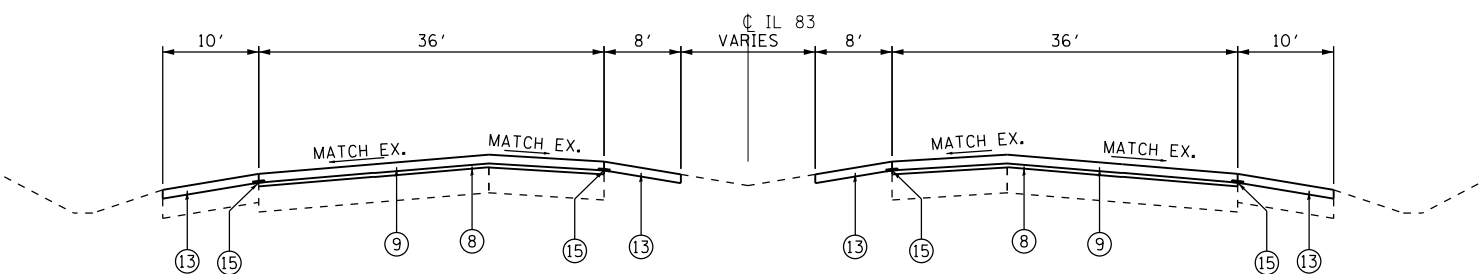


STA. 291+74 TO STA. 311+73 (NB)



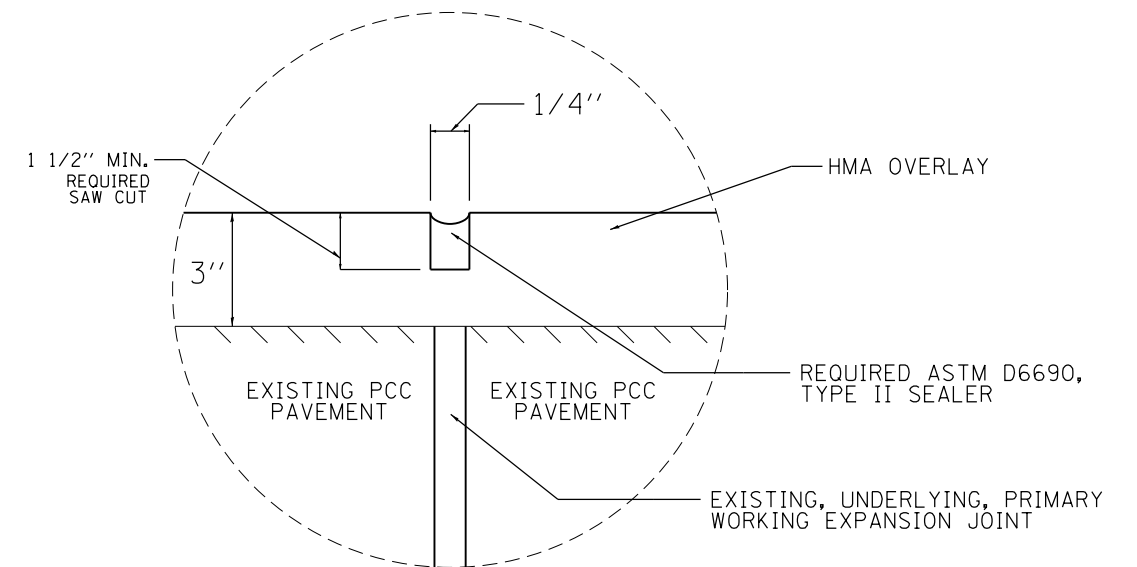
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STA. 199+00 TO STA. 238+79 (SB)
 STA. 243+42 TO STA. 267+00 (SB) •
 STA. 199+00 TO STA. 239+32 (NB)
 STA. 243+87 TO STA. 267+00 (NB) •



PROPOSED TYPICAL SECTION

STA. 199+00 TO STA. 238+79 (SB)
 STA. 243+42 TO STA. 267+00 (SB)
 STA. 199+00 TO STA. 239+32 (NB)
 STA. 243+87 TO STA. 267+00 (NB)



SAW CUT AND SEAL NEW JOINTS DETAIL "B"

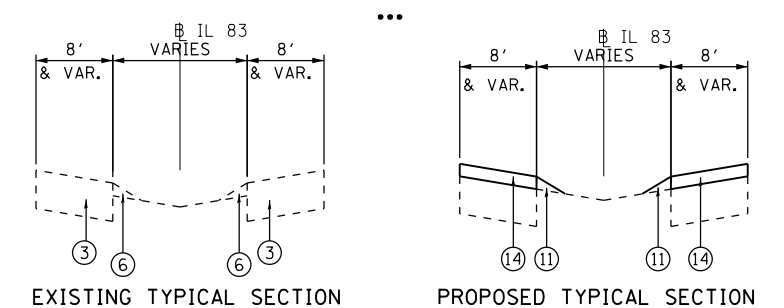
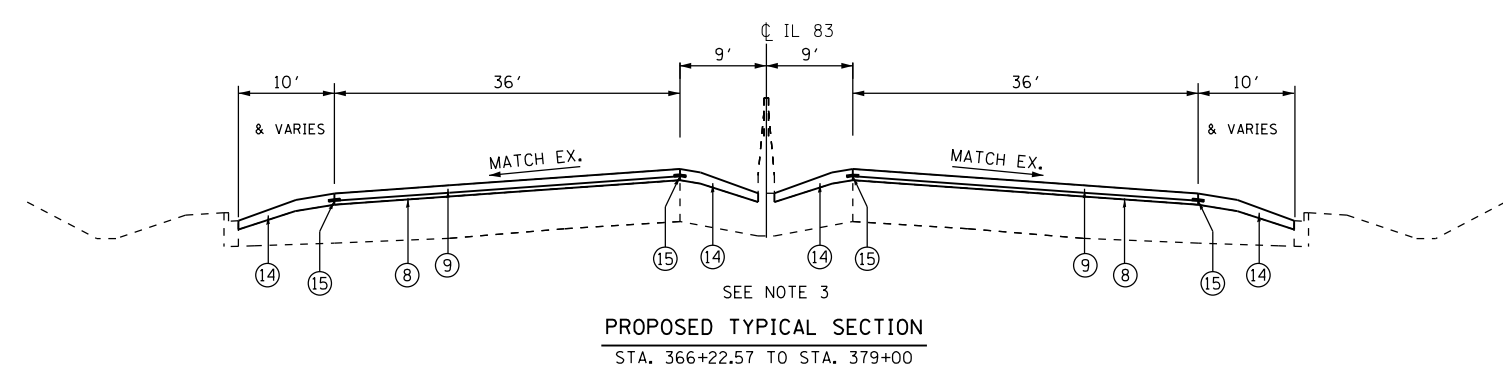
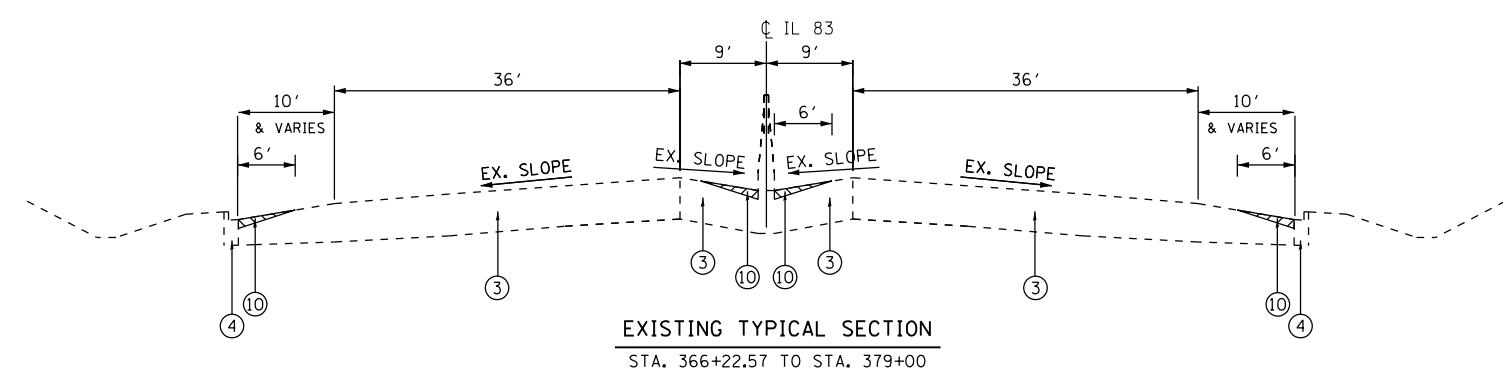
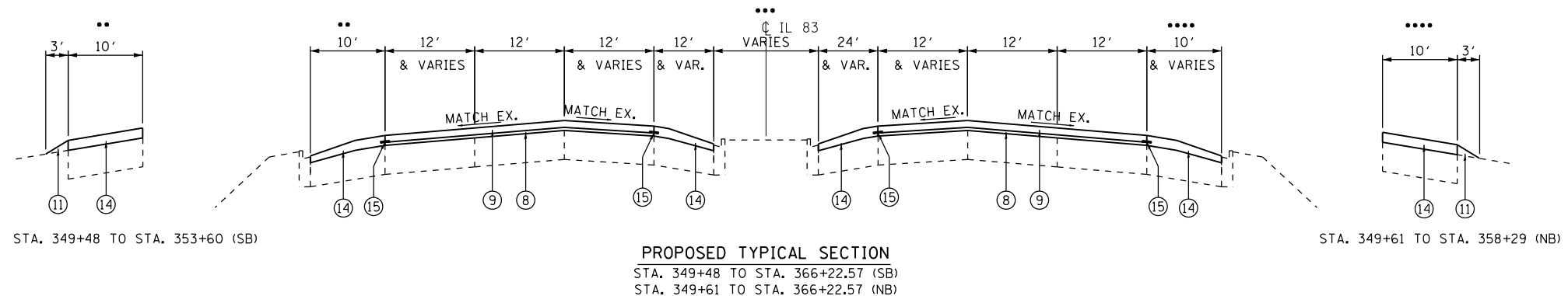
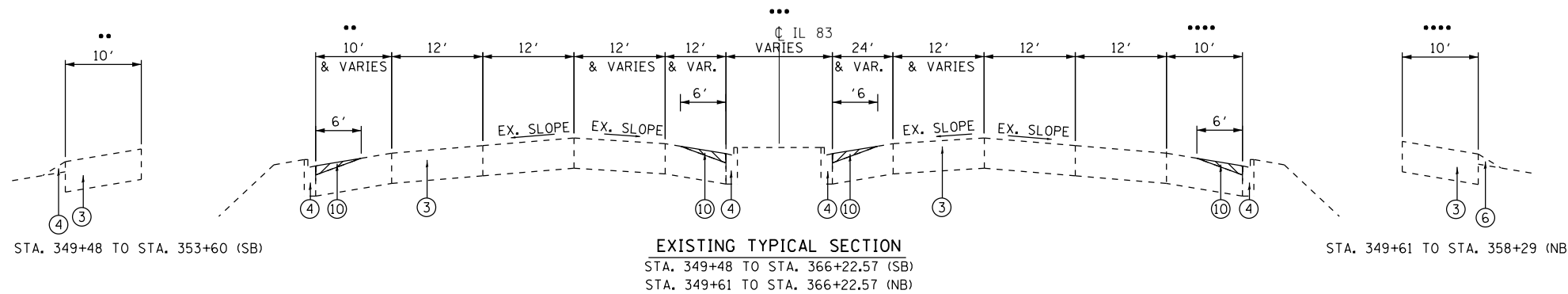
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Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			344	544-RS-4	DUPAGE	53	9	
	PLOT DATE = 6/13/2014	CHECKED -	REVISED -			CONTRACT NO. 60N52					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

LEGEND

- ① EXISTING +/-3" HMA (BEFORE MILLING)
- ② EXISTING +/-4" HMA (BEFORE MILLING)
- ③ EXISTING +/-10" P.C.C. BASE
- ④ EXISTING B-6.24 CURB AND GUTTER
- ⑤ EXISTING BITUMINOUS SHOULDER +/-8"
- ⑥ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑦ HMA SURFACE REMOVAL - 3" (SEE NOTE 1)
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE NOTE 2)
- ⑪ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ HMA SURFACE REMOVAL - 2"
- ⑬ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑭ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -3"
- ⑮ STRIP REFLECTIVE CRACK CONTROL TREATMENT

NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
2. CLASS D PATCH TYPE IV, 8 INCH TO BE USED ON 8 INCH HMA SHOULDER. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
3. REFER TO DETAIL HMA TAPER AT EDGE OF P.C.C. PAVEMENT FOR DEPTH OF MILLING AT GUTTER FLAG.
4. NO ADDITIONAL COMPENSATION WILL BE GIVEN TO THE CONTRACTOR FOR ANY EXTRA WORK REQUIRED TO REMOVE THE HMA SHOULDER ADJACENT TO THE EXISTING BARRIER WALL. THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HMA SURFACE REMOVAL - 3".

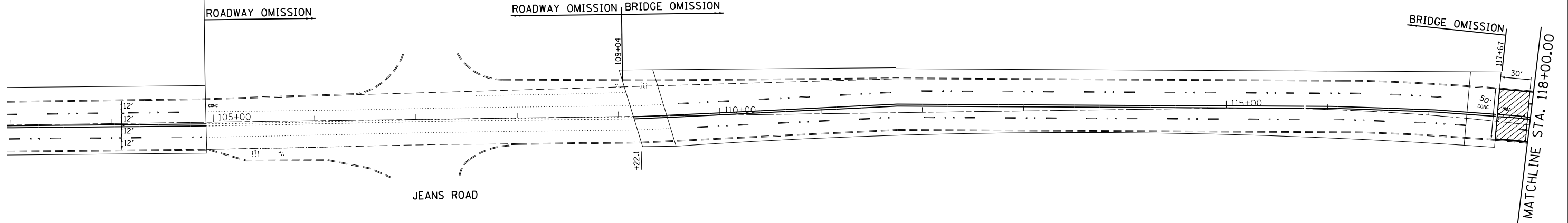


STA. 349+48 TO STA. 355+00(SB)
 STA. 349+61 TO STA. 355+00(NB)

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			344	544-RS-4	DUPAGE	53	10	
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		DATE -	REVISED -			SHEET 3 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

IL RTE 83
(KINGERY HIGHWAY)

PROJECT BEGINS
STA. 104 + 92.8



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

- SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)
- TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

RESURFACING LEGEND (MAINLINE)

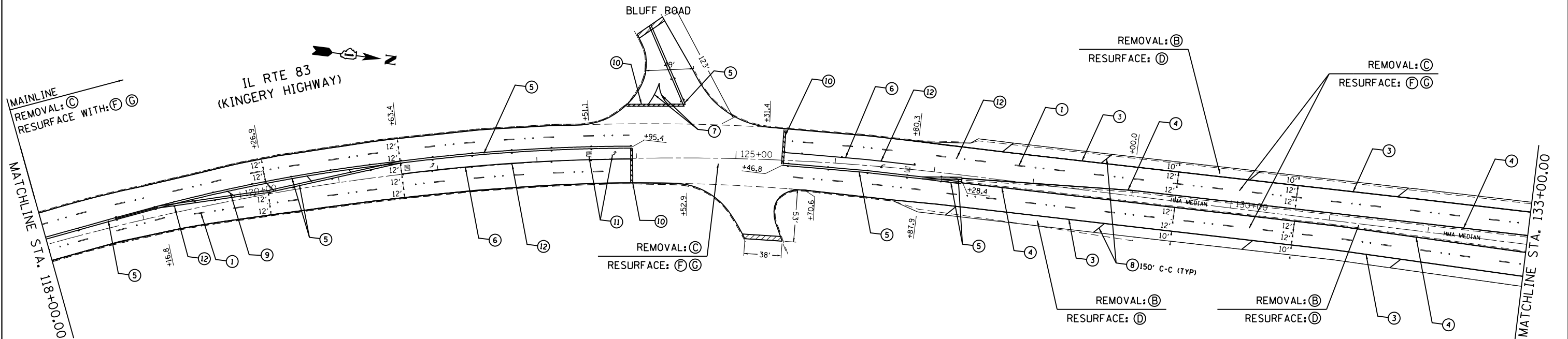
- Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
- Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
- Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"

NOTES:

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
3. REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
4. REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

RESURFACING LEGEND (SHOULDER)

- OUTSIDE SHOULDERS:
STA. 117+67 - STA. 349+61.2
- Ⓑ HMA SURFACE REMOVAL - 2"
 - RESURFACE WITH:
 - Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- INSIDE SHOULDERS:
STA. 134+00 - STA. 349+61
- Ⓒ HMA SURFACE REMOVAL - 3"
 - RESURFACE WITH:
 - Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
 - Ⓖ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
- OUTSIDE AND INSIDE SHOULDERS:
STA. 349+61.2 - STA. 379+00.0
- OVERLAY WITH:
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"



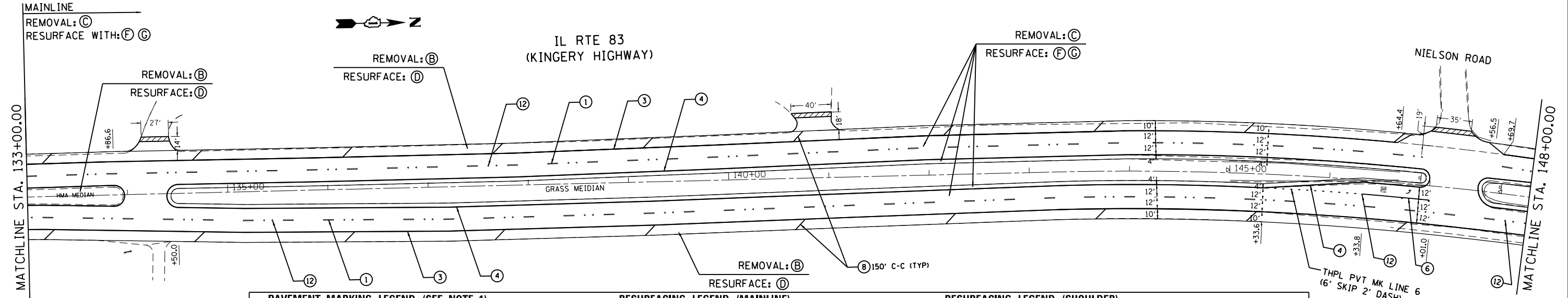
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

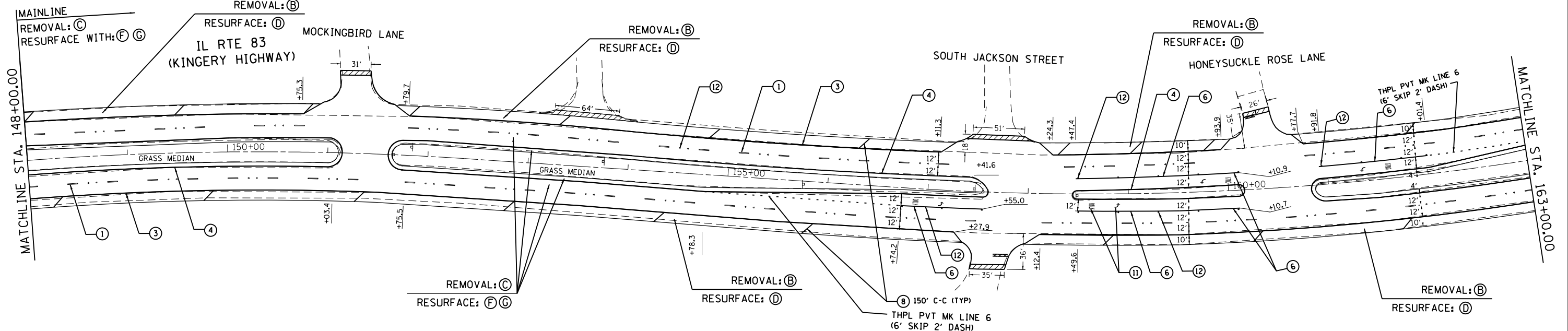
EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)

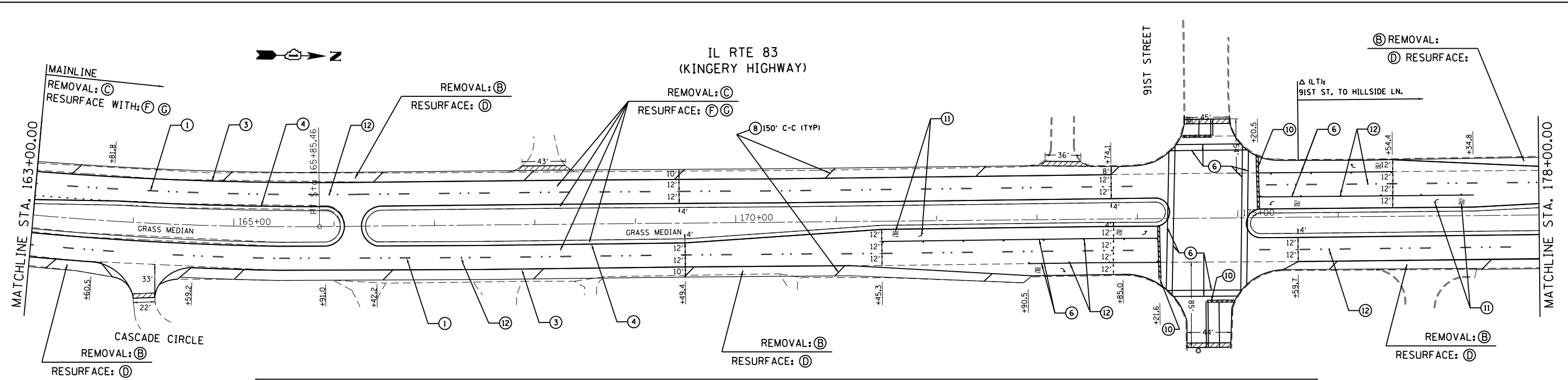
SCALE: 1" = 50' SHEET 1 OF 10 SHEETS STA. 103+00.00 TO STA. 133+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60N52	



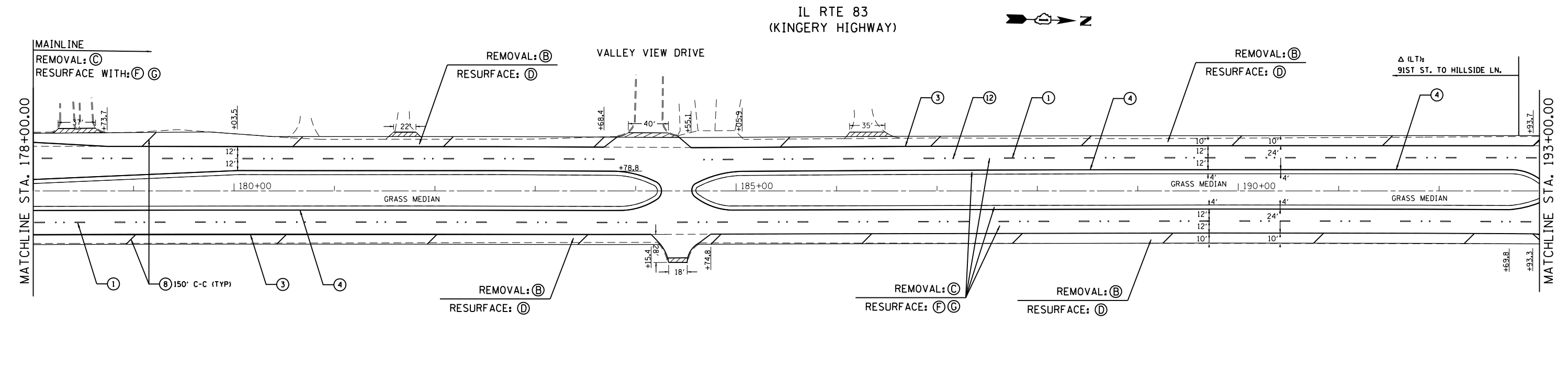
PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH) ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP) ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)	(A) PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3) (B) HOT-MIX ASPHALT SURFACE REMOVAL, 2" (C) HOT-MIX ASPHALT SURFACE REMOVAL, 3" (D) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2" (E) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3" (F) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1" (G) POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2 (B) HMA SURFACE REMOVAL - 2" RESURFACE WITH: (D) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2" INSIDE SHOULDERS: STA. 134+00 - STA. 349+61 (C) HMA SURFACE REMOVAL - 3" RESURFACE WITH: (F) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1" (G) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2" OUTSIDE AND INSIDE SHOULDERS: STA. 349+61.2 - STA. 379+00.0 OVERLAY WITH: (E) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
[Hatched Box] SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32) [Triangle] TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	NOTES: 1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION. 2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION. 3. REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31). 4. REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.	





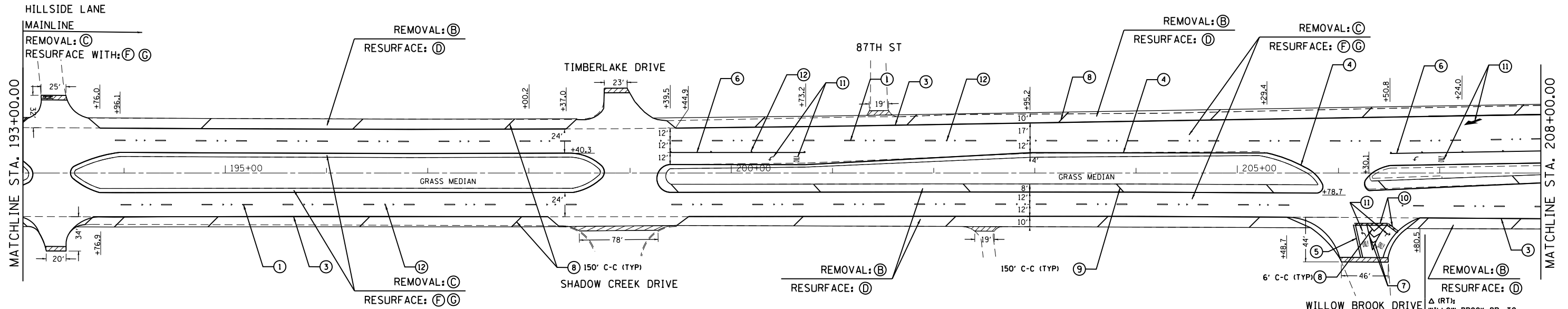
PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2
② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)	Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"	Ⓑ HMA SURFACE REMOVAL - 2"
③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE	Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	RESURFACE WITH: Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE	Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"	••INSIDE SHOULDERS: STA. 134+00 - STA. 349+61
⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"	Ⓒ HMA SURFACE REMOVAL - 3"
⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	RESURFACE WITH: Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE	Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"	Ⓖ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE		
⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE		
⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR		
⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)		
⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)		
▨ SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)		
△ TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE		

- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 - REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
 - REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
 - REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			344	544-RS-4	DUPAGE	53	13	
	PLOT DATE = 6/13/2014	CHECKED -	REVISED -			CONTRACT NO. 60N52					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

IL RTE 83
(KINGERY HIGHWAY)



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

- SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)
- TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

RESURFACING LEGEND (MAINLINE)

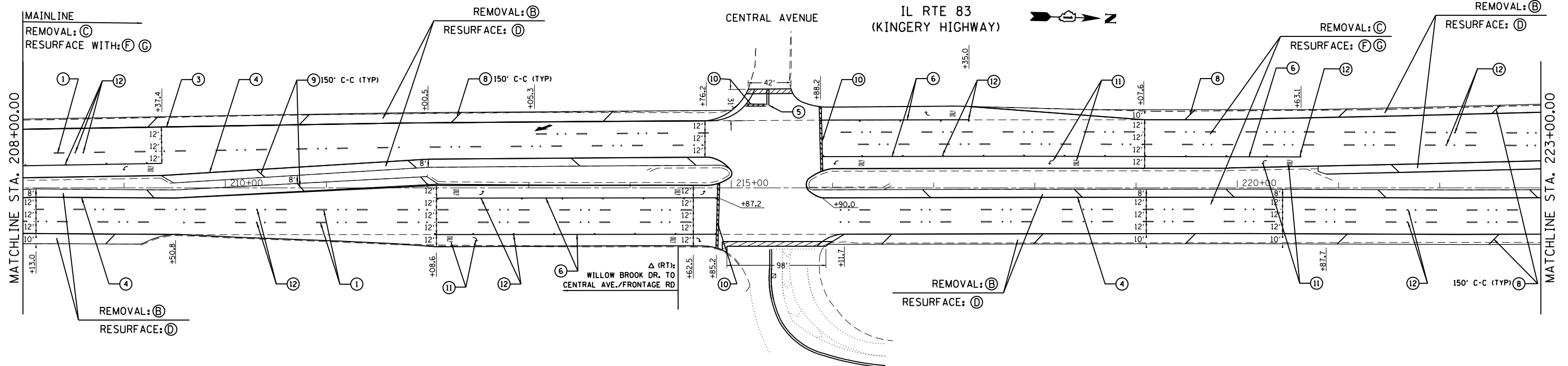
- Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
- Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
- Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"

NOTES:

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
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4. REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

RESURFACING LEGEND (SHOULDER)

- OUTSIDE SHOULDERS:
STA. 117+67 - STA. 349+61.2
- Ⓑ HMA SURFACE REMOVAL - 2"
 - Ⓓ RESURFACE WITH:
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- INSIDE SHOULDERS:
STA. 134+00 - STA. 349+61
- Ⓒ HMA SURFACE REMOVAL - 3"
 - Ⓓ RESURFACE WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
 - Ⓕ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
- OUTSIDE AND INSIDE SHOULDERS:
STA. 349+61.2 - STA. 379+00.0
- Ⓔ OVERLAY WITH:
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"



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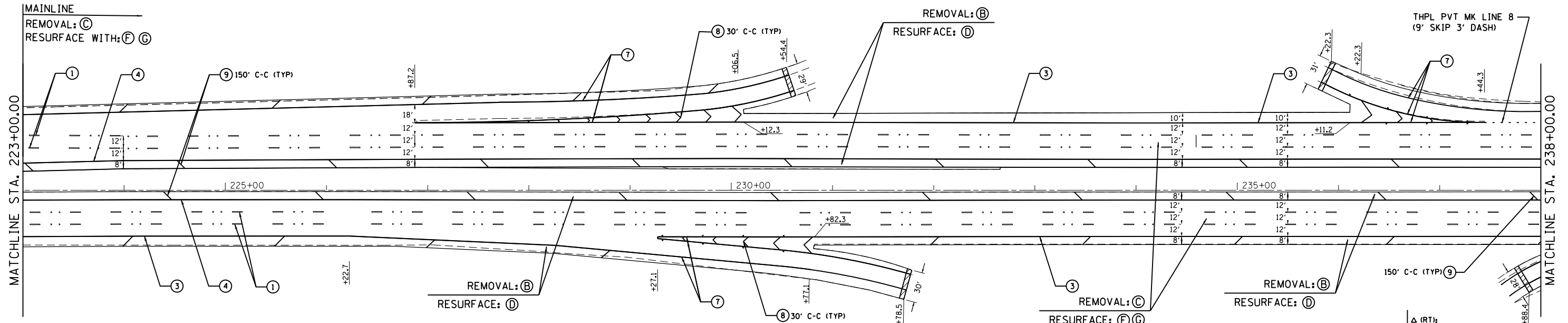
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)

SCALE: 1" = 50' SHEET 4 OF 10 SHEETS STA. 193+00.00 TO STA. 223+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	14
			CONTRACT NO. 60N52	
ILLINOIS FED. AID PROJECT				

IL RTE 83
(KINGERY HIGHWAY)



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

- SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)
- TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

RESURFACING LEGEND (MAINLINE)

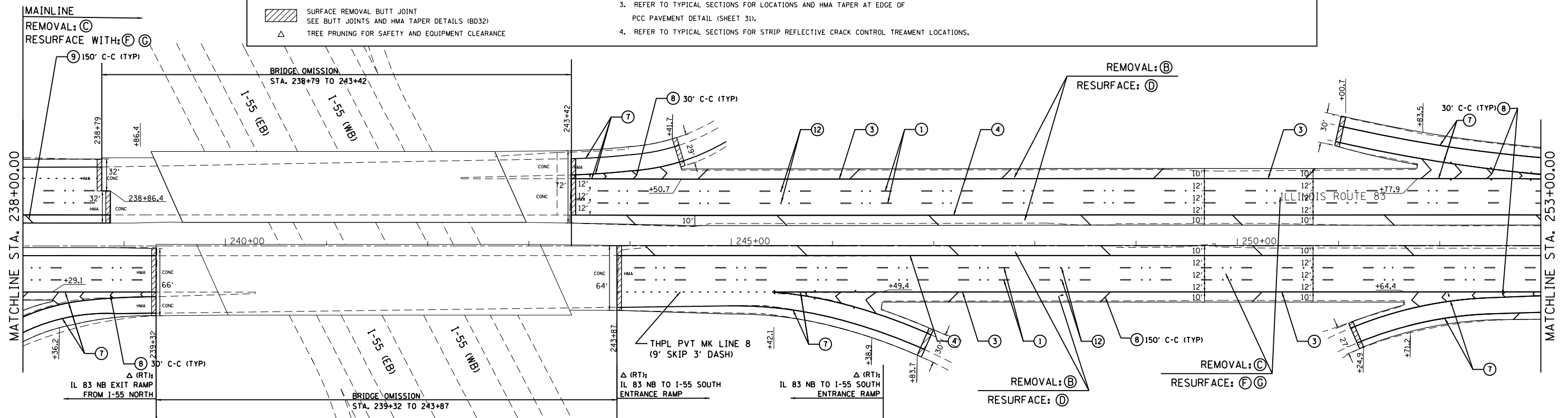
- Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
- Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
- Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"

NOTES:

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
3. REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
4. REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

RESURFACING LEGEND (SHOULDER)

- OUTSIDE SHOULDERS:
STA. 117+67 - STA. 349+61.2
- Ⓑ HMA SURFACE REMOVAL - 2"
 - RESURFACE WITH:
 - Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- INSIDE SHOULDERS:
STA. 134+00 - STA. 349+61
- Ⓒ HMA SURFACE REMOVAL - 3"
 - RESURFACE WITH:
 - Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
 - Ⓖ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
- OUTSIDE AND INSIDE SHOULDERS:
STA. 349+61.2 - STA. 379+00.0
- OVERLAY WITH:
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"



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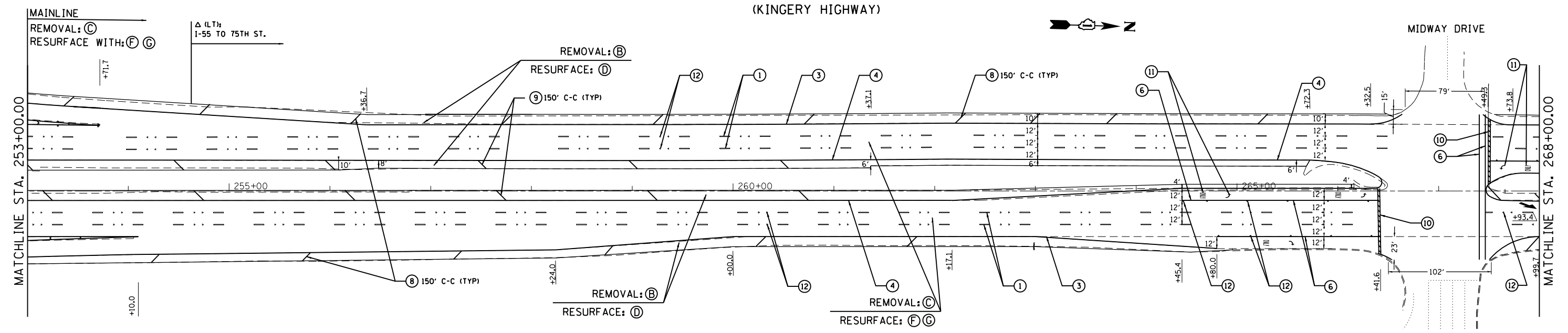
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)

SCALE: 1" = 50' SHEET 5 OF 10 SHEETS STA. 223+00.00 TO STA. 253+00.00

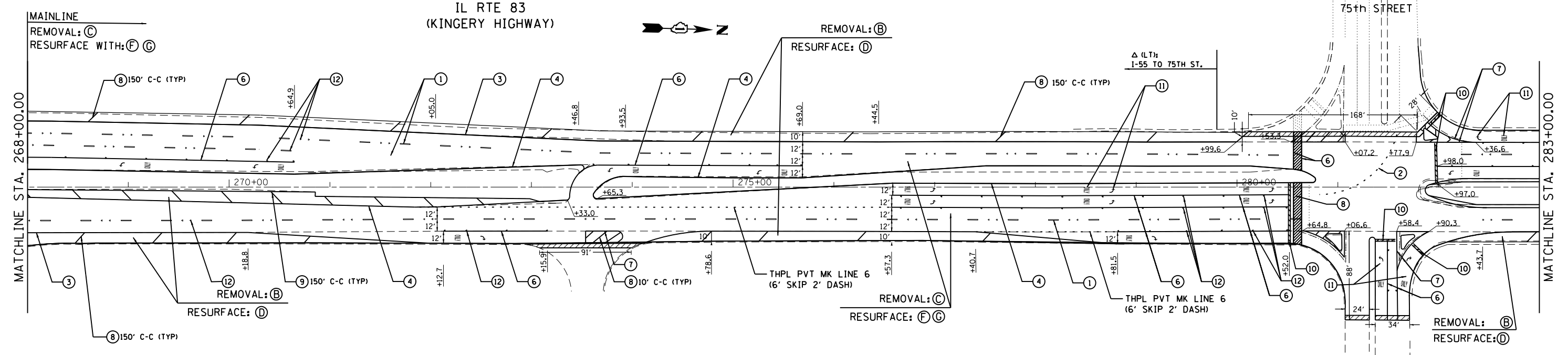
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	15
CONTRACT NO. 60N52			ILLINOIS FED. AID PROJECT	

IL RTE 83
(KINGERY HIGHWAY)

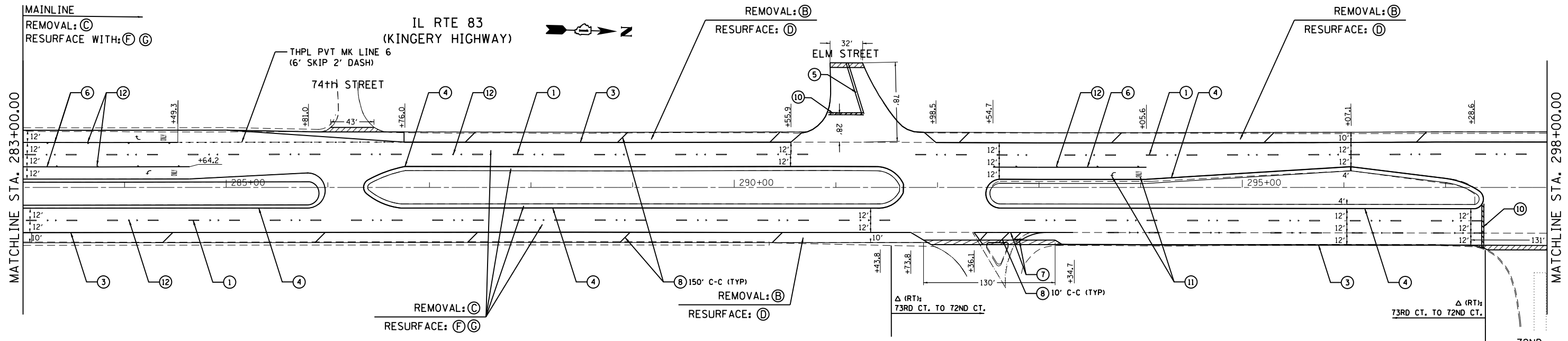


PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2 Ⓑ HMA SURFACE REMOVAL - 2" RESURFACE WITH: Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)	Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"	••INSIDE SHOULDERS: STA. 134+00 - STA. 349+61 Ⓒ HMA SURFACE REMOVAL - 3" RESURFACE WITH: Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE	Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE	Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE	Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
▨ SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
△ TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE		Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"

- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 - REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
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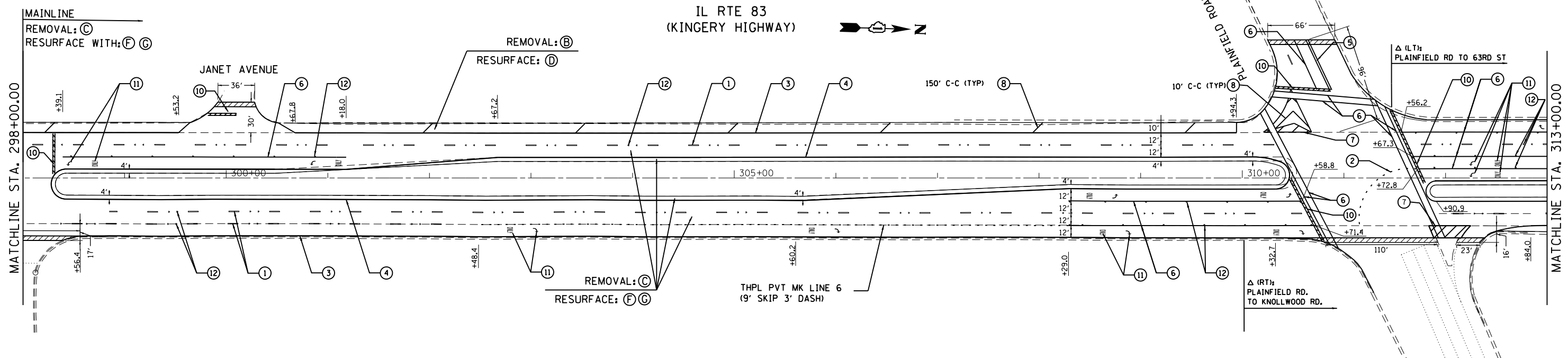


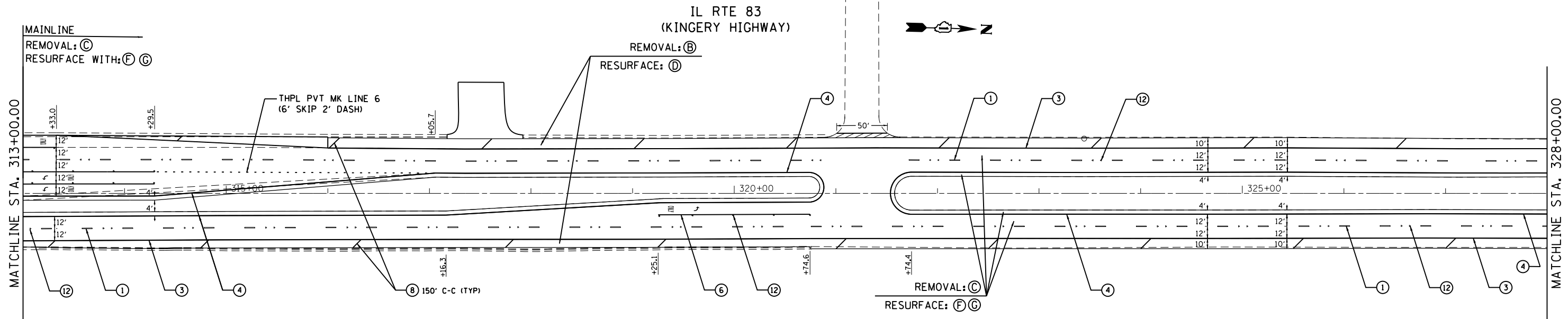
FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60N52							
DATE = 6/13/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							



PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2
② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)	Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"	Ⓑ HMA SURFACE REMOVAL - 2"
③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE	Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	RESURFACE WITH: Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE	Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"	••INSIDE SHOULDERS: STA. 134+00 - STA. 349+61
⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"	Ⓒ HMA SURFACE REMOVAL - 3"
⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	RESURFACE WITH: Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE	Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"	Ⓖ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE		
⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE		
⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR		
⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)		
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▨ SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)		
△ TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE		

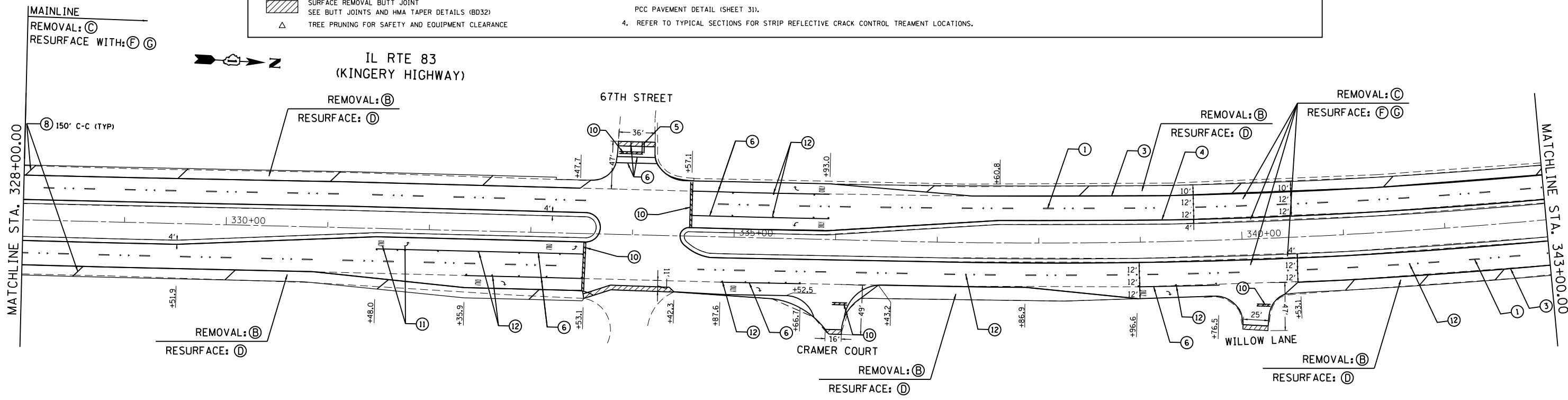
- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
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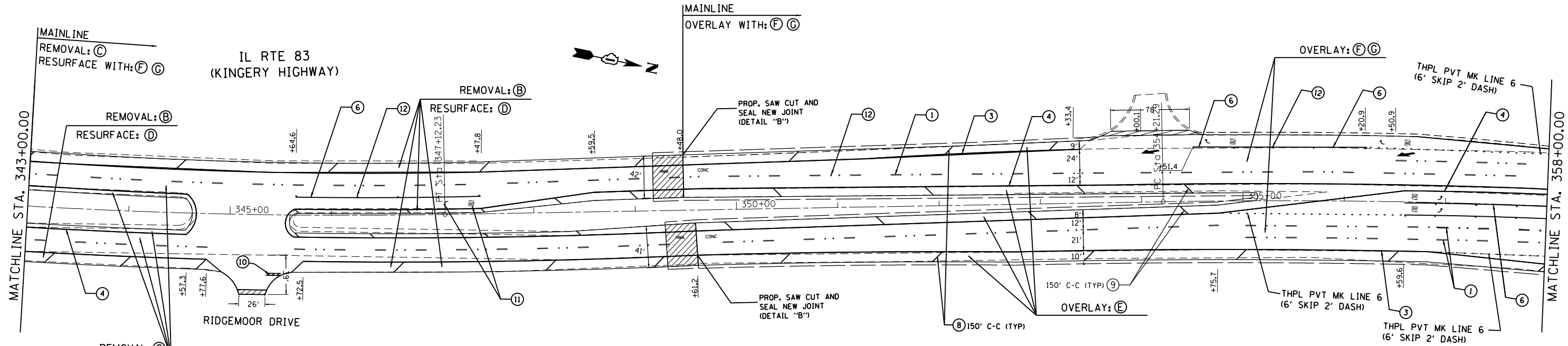




PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2 Ⓑ HMA SURFACE REMOVAL - 2" RESURFACE WITH: Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)	Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"	••INSIDE SHOULDERS: STA. 134+00 - STA. 349+61 Ⓒ HMA SURFACE REMOVAL - 3" RESURFACE WITH: Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE	Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
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⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	Ⓖ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
▨ SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)		OUTSIDE AND INSIDE SHOULDERS: STA. 349+61.2 - STA. 379+00.0 OVERLAY WITH: Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
△ TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE		

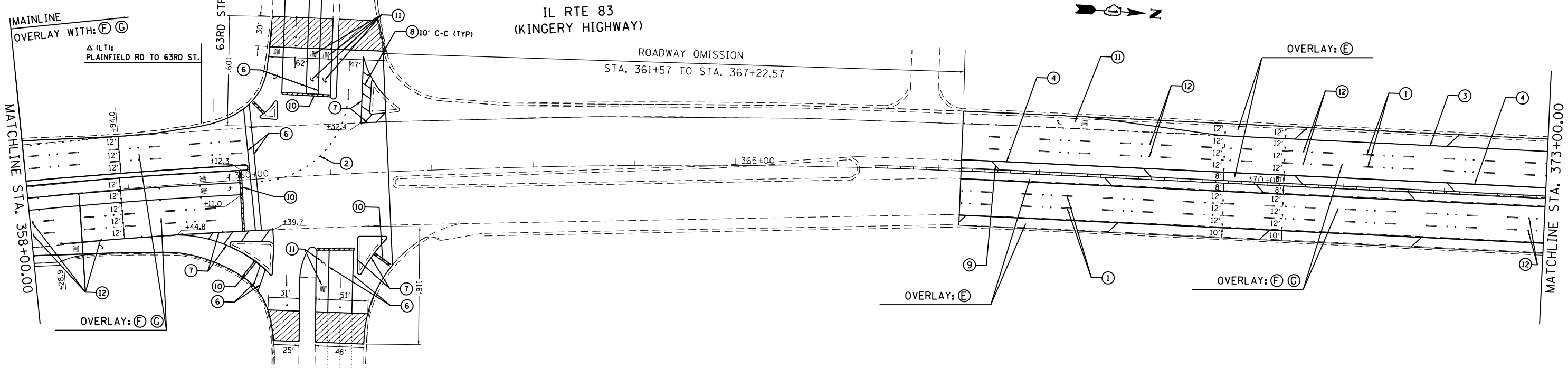
- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 - REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
 - REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
 - REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

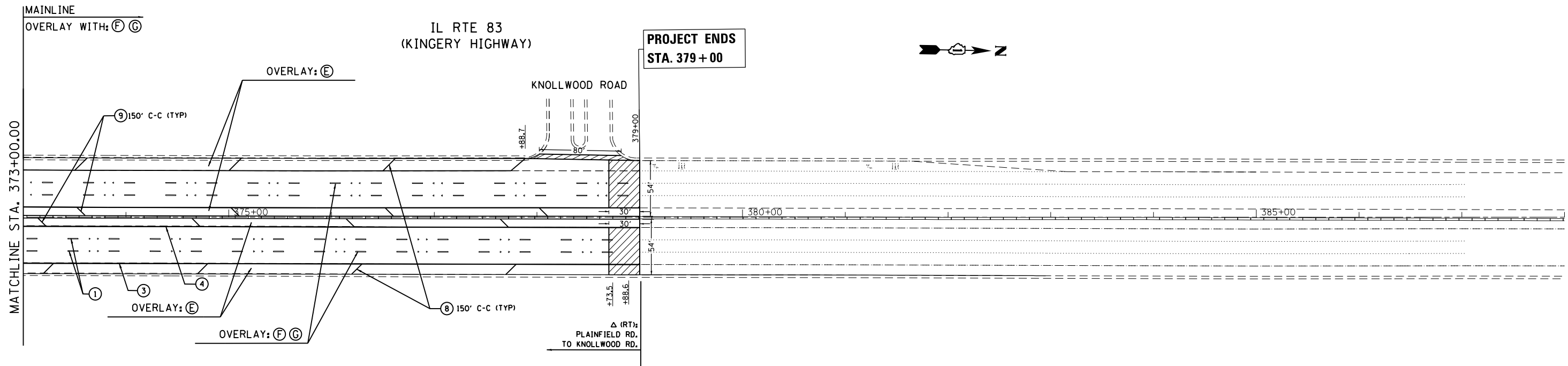




PAVEMENT MARKING LEGEND (SEE NOTE 1)	RESURFACING LEGEND (MAINLINE)	RESURFACING LEGEND (SHOULDER)
(1) THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	(A) PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)	OUTSIDE SHOULDERS: STA. 117+67 - STA. 349+61.2 (B) HMA SURFACE REMOVAL - 2" RESURFACE WITH: (D) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
(2) THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)	(B) HOT-MIX ASPHALT SURFACE REMOVAL, 2"	••INSIDE SHOULDERS: STA. 134+00 - STA. 349+61 (C) HMA SURFACE REMOVAL - 3" RESURFACE WITH: (F) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1" (G) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"
(3) THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE	(C) HOT-MIX ASPHALT SURFACE REMOVAL, 3"	OUTSIDE AND INSIDE SHOULDERS: STA. 349+61.2 - STA. 379+00.0 OVERLAY WITH: (E) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
(4) THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE	(D) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"	
(5) THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	(E) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"	
(6) THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	(F) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"	
(7) THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE	(G) POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"	
(8) THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE		
(9) THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE		
(10) THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR		
(11) THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)		
(12) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)		
▨ SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)		
△ TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE		

- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 - REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
 - REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
 - REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.





PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

SURFACE REMOVAL BUTT JOINT
 SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)
 TREE PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

RESURFACING LEGEND (MAINLINE)

- Ⓐ PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
- Ⓑ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- Ⓒ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"
- Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- Ⓖ POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"

RESURFACING LEGEND (SHOULDER)

OUTSIDE SHOULDERS:
 STA. 117+67 - STA. 349+61.2
 Ⓑ HMA SURFACE REMOVAL - 2"
 RESURFACE WITH:
 Ⓓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"

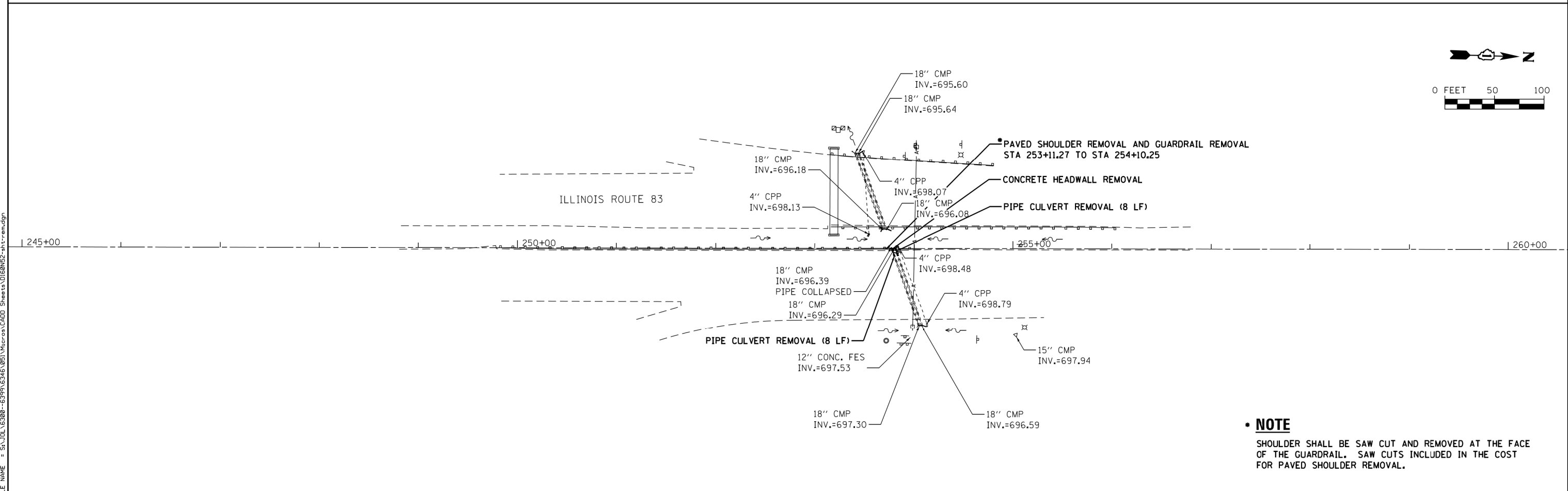
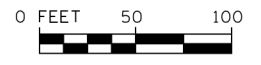
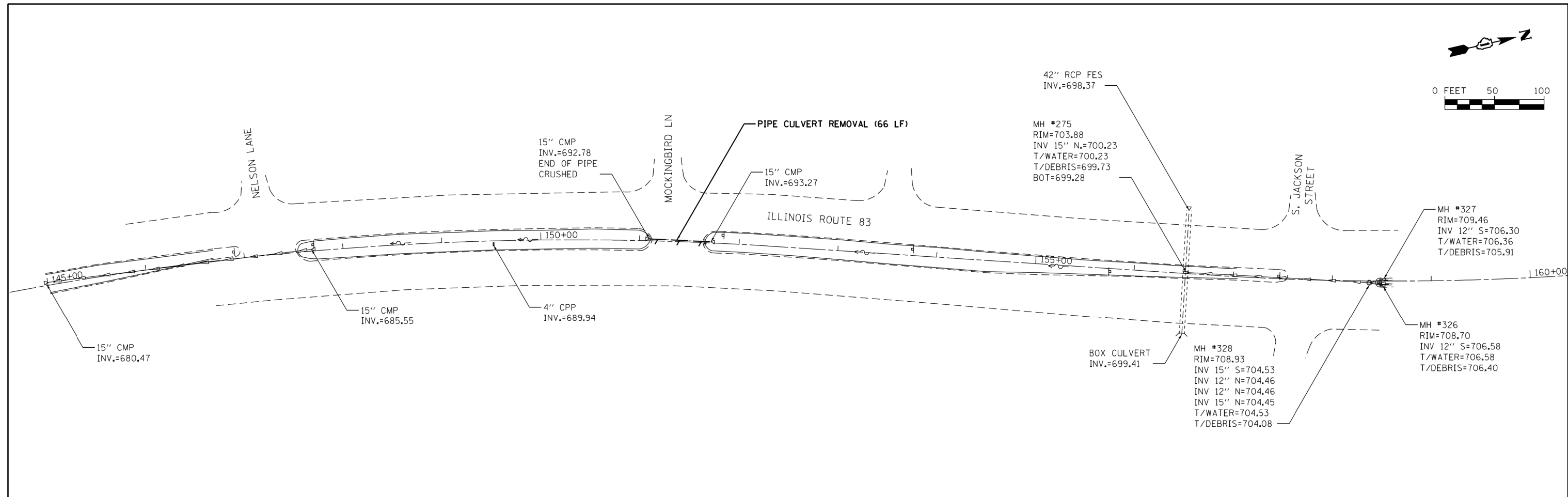
INSIDE SHOULDERS:
 STA. 134+00 - STA. 349+61
 Ⓒ HMA SURFACE REMOVAL - 3"
 RESURFACE WITH:
 Ⓕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
 Ⓖ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"

OUTSIDE AND INSIDE SHOULDERS:
 STA. 349+61.2 - STA. 379+00.0
 OVERLAY WITH:
 Ⓔ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 3"

NOTES:

- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
- REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
- REFER TO TYPICAL SECTIONS FOR LOCATIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL (SHEET 31).
- REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL. ROUTE 83 (KINGERY HIGHWAY) (COOK CO. LINE TO KNOLLWOOD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	et:\pw_work\p\dot\marenade\d0316166\0132211-sht-plan.dgn	DRAWN -	REVISED -			344	544-RS-4		53	20	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N52					
	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



NOTE
 SHOULDER SHALL BE SAW CUT AND REMOVED AT THE FACE OF THE GUARDRAIL. SAW CUTS INCLUDED IN THE COST FOR PAVED SHOULDER REMOVAL.

FILE NAME = S:\JUL16300-6399\6346\051\Micro\CA00D Sheets\0160652-ph-tr-rem.dgn



USER NAME = dennisw	DESIGNED - MAG	REVISED -
MODEL NAME = Default	DRAWN - DJW	REVISED -
PLOT SCALE = 100.0000' / 1in.	CHECKED - BMA	REVISED -
PLOT DATE = 1/4/2013	DATE - 01-03-13	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING ROADWAY PLAN AND REMOVALS

SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.

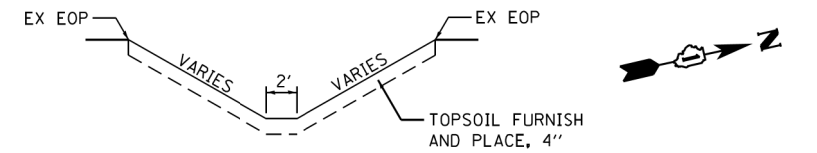
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	21
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				

CONTROL POINTS

CP #3 N: 1,839,760.608 E: 1,090,315.058 ELEV.=696.903 SET 5/8" REBAR W/CAP

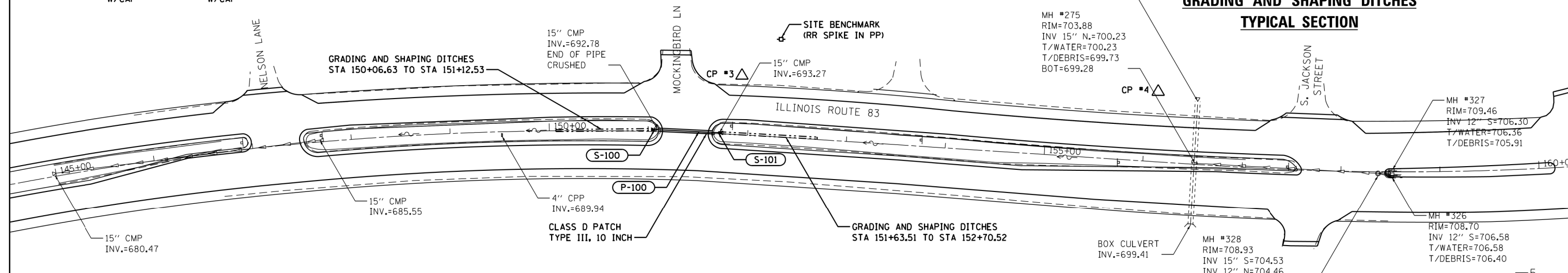
CP #4 N: 1,840,166.560 E: 1,090,424.900 ELEV.=702.364 SET 5/8" REBAR W/CAP

END SECTION SCHEDULE					
END SECTION NUMBER	STATION	OFFSET (FT)	SIZE (IN)	TYPE OF STRUCTURE	INVERT ELEVATION
S-100	151+06.58	-2.00	15	PRECAST REINFORCED CONCRETE FLARED END SECTION	691.98
S-101	151+70.54	-1.07	15	PRECAST REINFORCED CONCRETE FLARED END SECTION	693.25



**GRADING AND SHAPING DITCHES
TYPICAL SECTION**

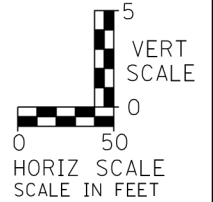
DATE	
BY	
PLAN	SURVEYED
	PLOTTED
	ALIGNED
	CHECKED
	FILED
	NO.
	NO.



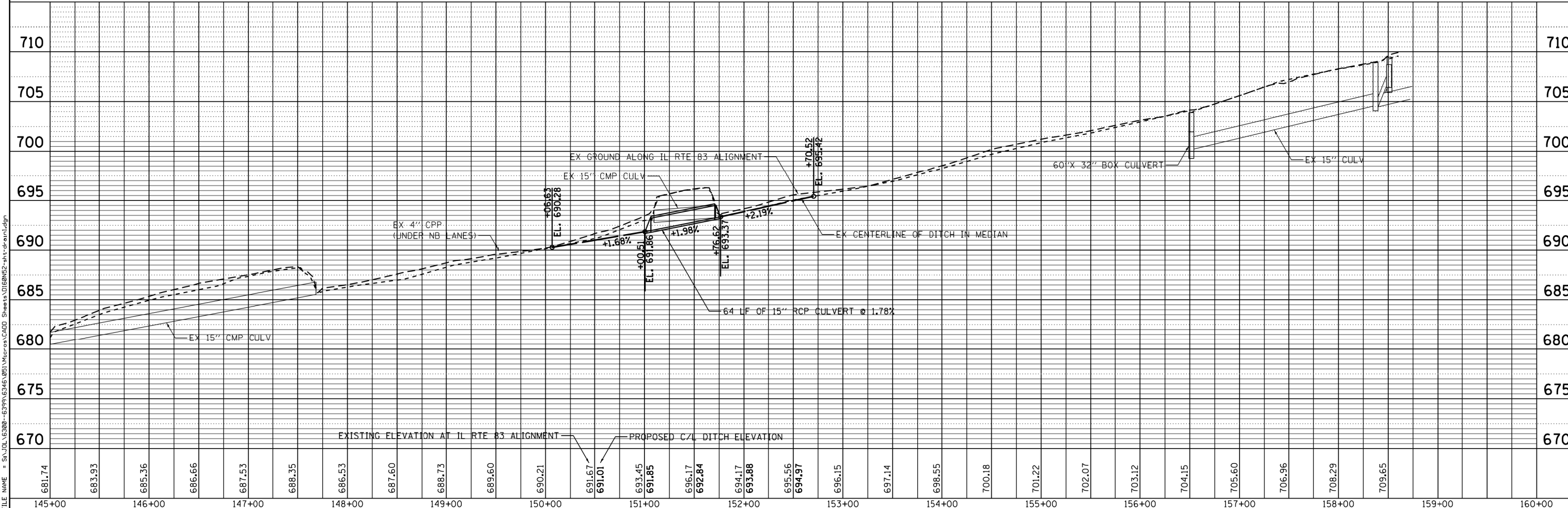
SITE BENCHMARK

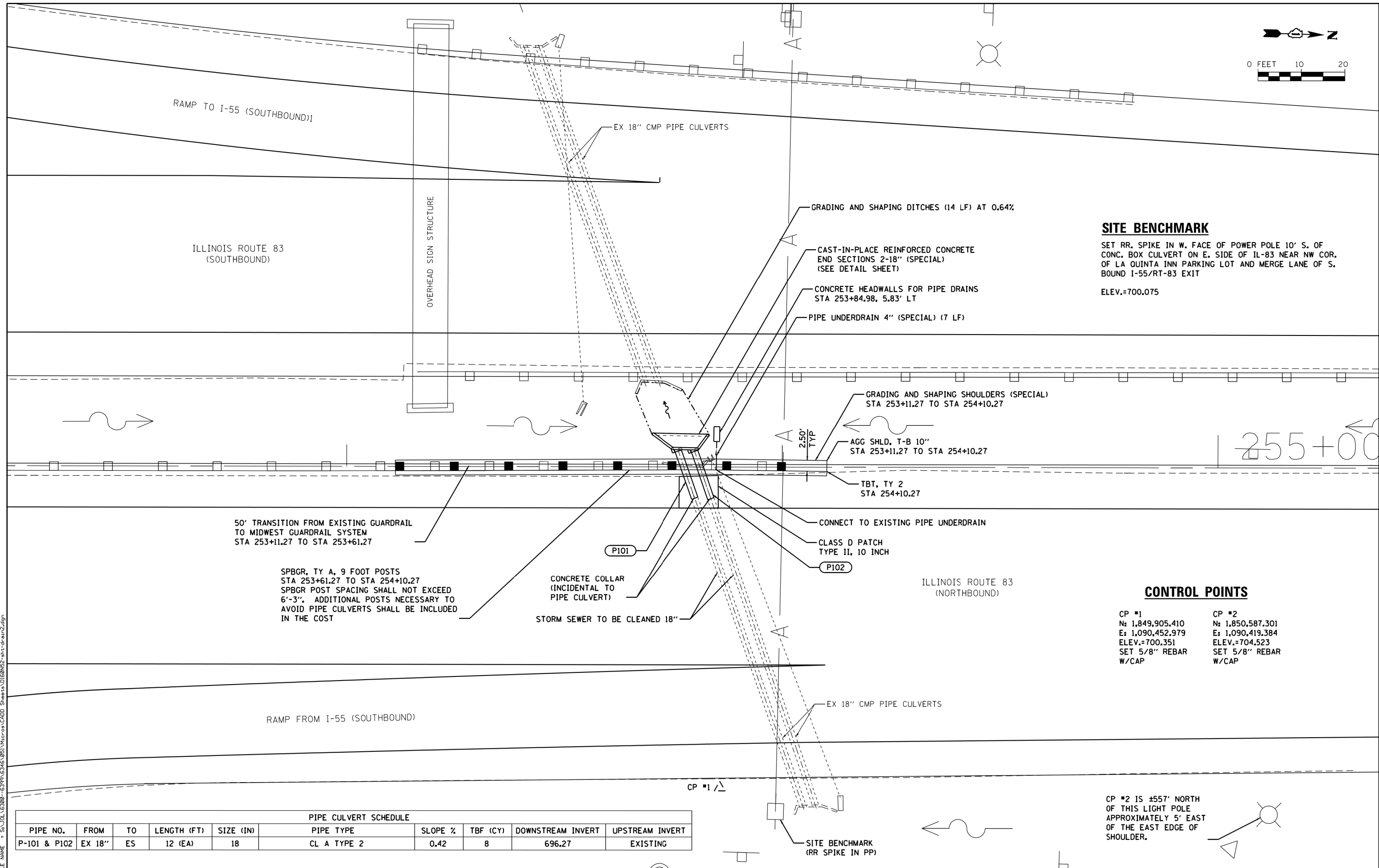
SET RR. SPIKE IN E. FACE POWER POLE 1' ABOVE GRADE ON W. SIDE OF IL-83 BETWEEN MCDONALD'S ENTRANCE AND ENTRANCE TO WATERFORD GLEN CONDOMINIUMS
ELEV.=695.52

PIPE CULVERT SCHEDULE										
PIPE NO.	FROM	TO	LENGTH (FT)	SIZE (IN)	PIPE TYPE	SLOPE %	TBF (CY)	DOWNSTREAM INVERT	UPSTREAM INVERT	
P-100	S-100	S-101	64	15	CL A TYPE 1	1.98	10	691.98	693.25	



DATE	
BY	
PROFILE	SURVEYED
	PLOTTED
	GRADES CHECKED
	STRUCTURE NOTATIONS CHECKED
	NO.
	NO.





SITE BENCHMARK

SET RR. SPIKE IN W. FACE OF POWER POLE 10' S. OF CONC. BOX CULVERT ON E. SIDE OF IL-83 NEAR NW COR. OF LA QUINTA INN PARKING LOT AND MERGE LANE OF S. BOUND I-55/RT-83 EXIT
ELEV.=700.075

CONTROL POINTS

CP #1 N: 1,849,905.410 E: 1,090,452.979 ELEV.=700.351 SET 5/8" REBAR W/CAP
CP #2 N: 1,850,587.301 E: 1,090,419.384 ELEV.=704.523 SET 5/8" REBAR W/CAP

50' TRANSITION FROM EXISTING GUARDRAIL TO MIDWEST GUARDRAIL SYSTEM STA 253+11.27 TO STA 253+61.27

SPBGR, TY A, 9 FOOT POSTS STA 253+61.27 TO STA 254+10.27 SPBGR POST SPACING SHALL NOT EXCEED 6'-3". ADDITIONAL POSTS NECESSARY TO AVOID PIPE CULVERTS SHALL BE INCLUDED IN THE COST

CONCRETE COLLAR (INCIDENTAL TO PIPE CULVERT)
STORM SEWER TO BE CLEANED 18"

PIPE CULVERT SCHEDULE									
PIPE NO.	FROM	TO	LENGTH (FT)	SIZE (IN)	PIPE TYPE	SLOPE %	TBF (CY)	DOWNSTREAM INVERT	UPSTREAM INVERT
P-101 & P102	EX 18"	ES	12 (EA)	18	CL A TYPE 2	0.42	8	696.27	EXISTING

FILE NAME = S:\JUL16300-6399\6346\051\Micro\CA00D Sheets\0160652-shh-drain2.dgn



USER NAME = dennissw	DESIGNED - MAG	REVISED -
MODEL NAME = Default	DRAWN - DJW	REVISED -
PLOT SCALE = 20.0000' / 1"	CHECKED - BMA	REVISED -
PLOT DATE = 1/4/2013	DATE - 01-03-13	REVISED -

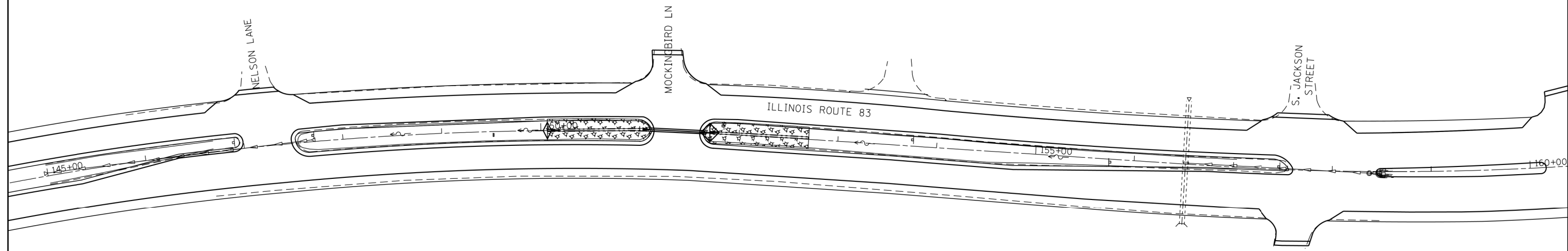
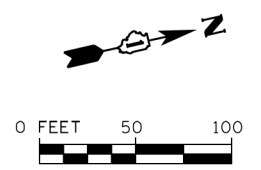
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE PLAN

SCALE: 1" = 10' SHEET 2 OF 2 SHEETS STA. TO STA.

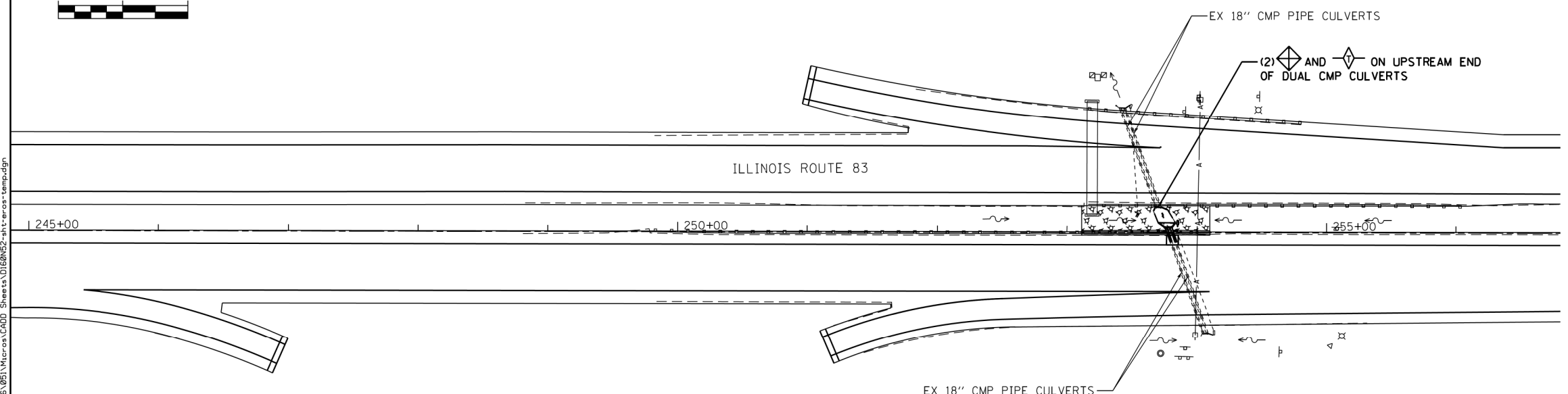
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	23
CONTRACT NO. 60N52				

ILLINOIS FED. AID PROJECT



EROSION CONTROL LEGEND

- INLET AND PIPE PROTECTION
 - TEMPORARY DITCH CHECK
 - TEMPORARY EROSION CONTROL SEEDING WITH MULCH METHOD 3
- DITCH FLOW
 - DRAINAGE SUMMIT



EROSION AND SEDIMENT CONTROL

1. PROPOSED CULVERTS RECEIVING RUNOFF SHALL BE PROTECTED WITH INLET AND PIPE PROTECTION IMMEDIATELY AFTER CONSTRUCTION.
2. EROSION CONTROL MEASURES SHALL BE INSPECTED WITHIN 24 HOURS OF ANY STORM EXCEEDING 1/2 INCH OF PRECIPITATION.
3. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
4. THE EROSION CONTROL MEASURES SHOWN ARE A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN CONTRACTOR AND THE DEPARTMENT.
5. MULCH, METHOD 3 SHALL BE PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AFTER TEMPORARY SEEDING HAS BEEN COMPLETED ON ALL AREAS EXCEPT DITCH BOTTOMS.
6. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
7. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
8. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.

EROSION AND SEDIMENT CONTROL MAINTENANCE

1. EROSION AND SEDIMENT CONTROL: AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.

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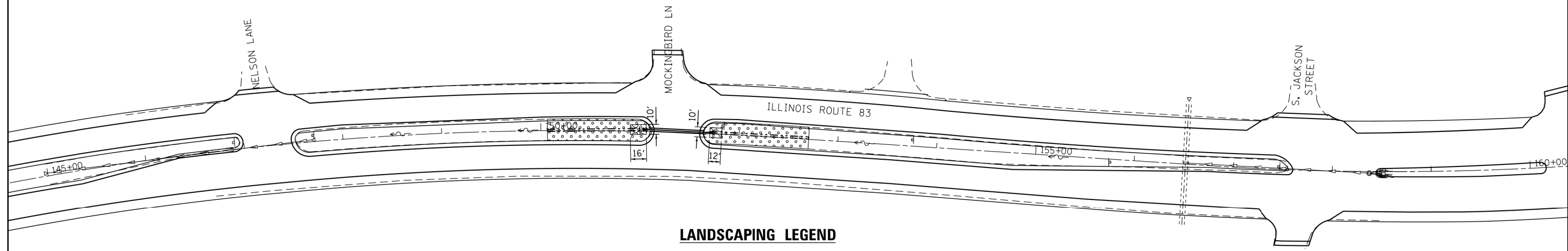
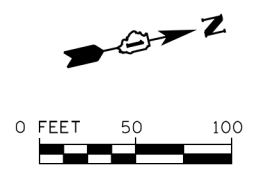


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PLOT DATE = 1/4/2013	DATE - 01-03-13	REVISED -	



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

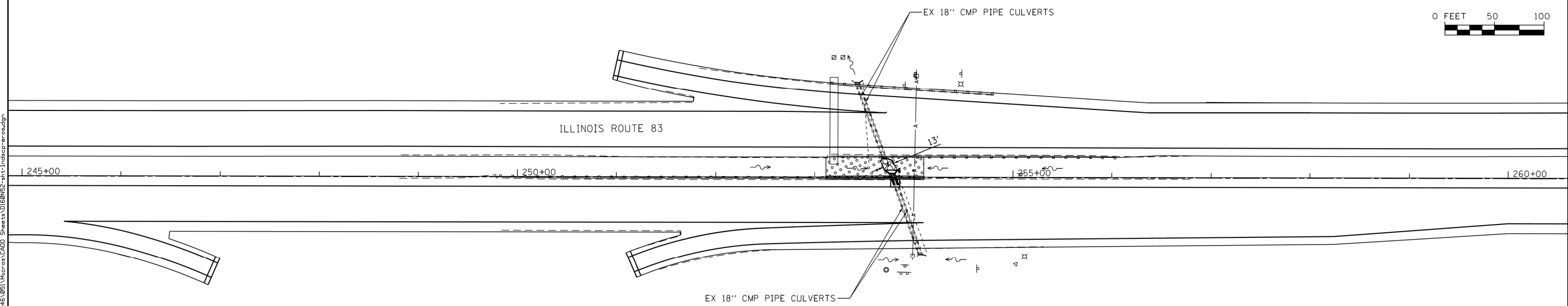
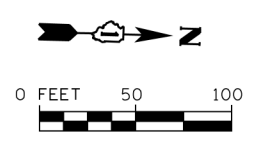
TEMPORARY EROSION AND SEDIMENT CONTROL			
SCALE: 1" = 50'	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	24
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				



LANDSCAPING LEGEND

	SEEDING CLASS 2A TOPSOIL FURNISH AND PLACE, 4" AND EROSION CONTROL BLANKET
	STONE RIPRAP, CLASS A3 OR A4



FILE NAME = SA\JUL16380-6395\6346\051\Micro\CA000 Sheets\016062-ht-landscape.dwg

SA STRAND ASSOCIATES
 1170 SOUTH HOUBOLT ROAD
 JULIET, ILLINOIS 60431
 (815) 744-4200

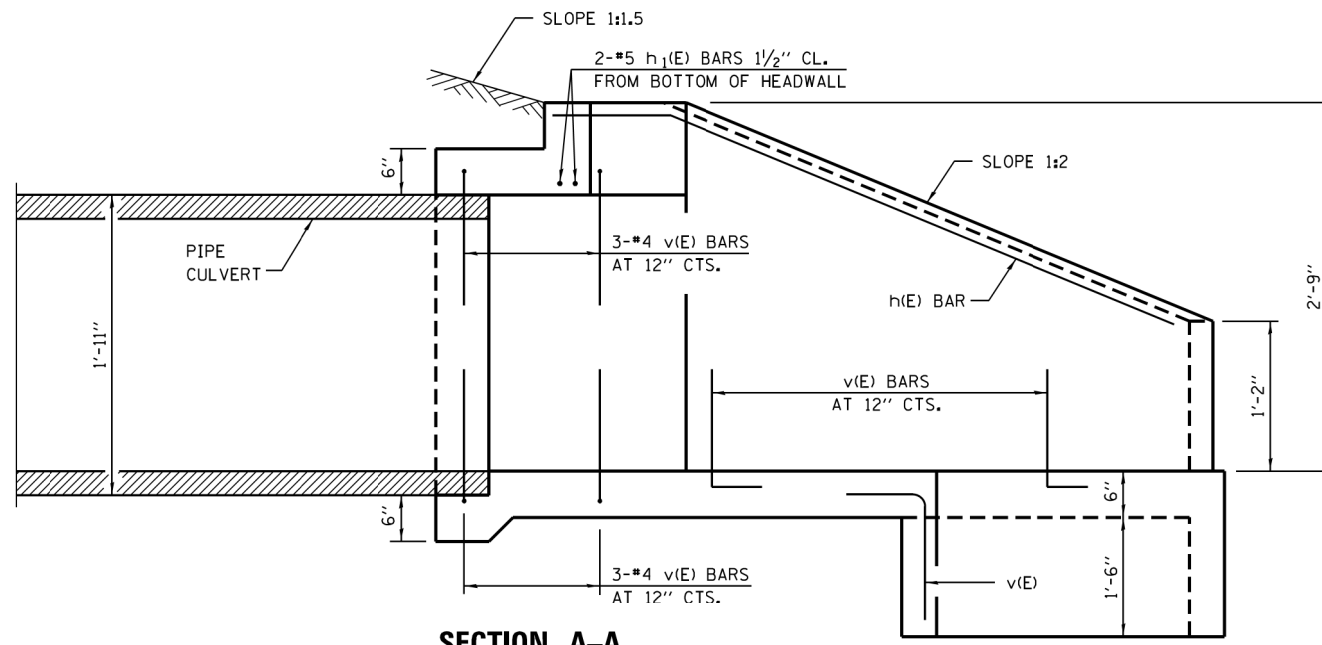
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

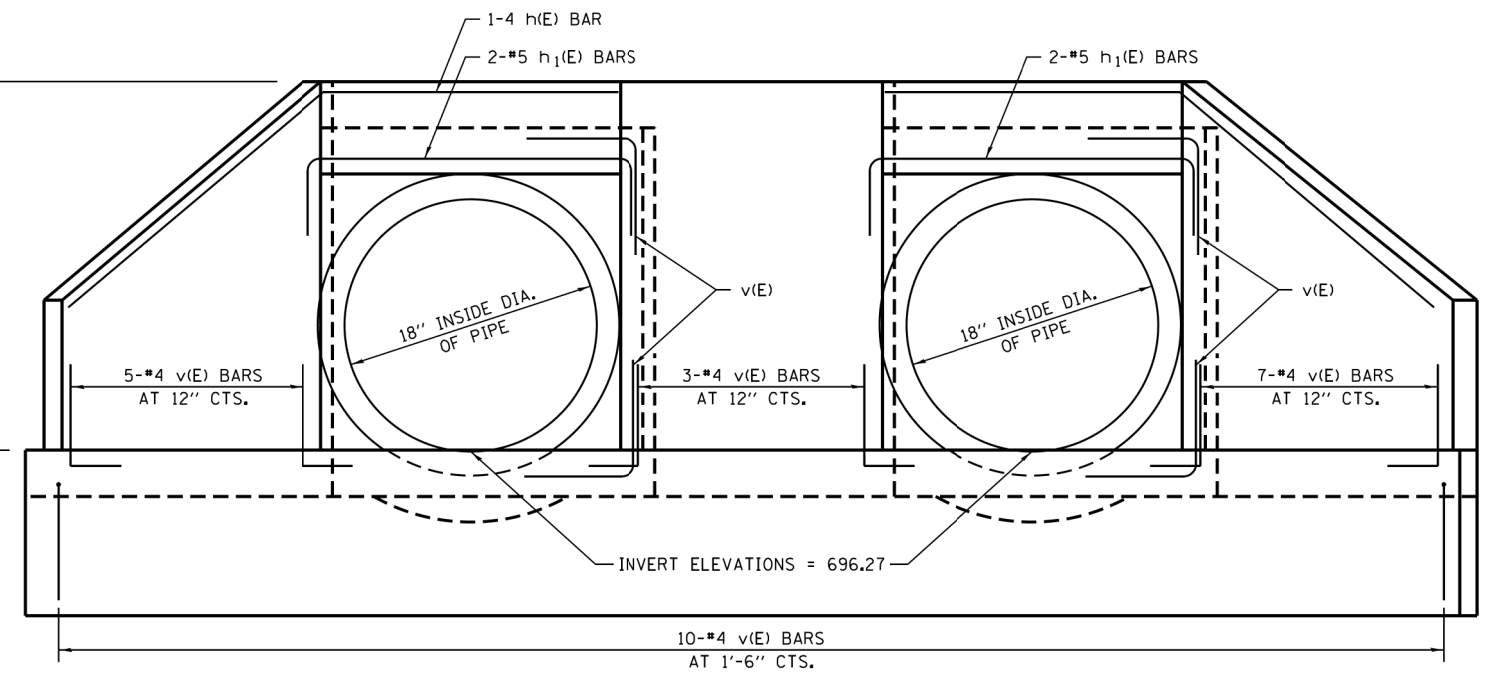
LANDSCAPING AND PERMANENT EROSION AND SEDIMENT CONTROL

SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.

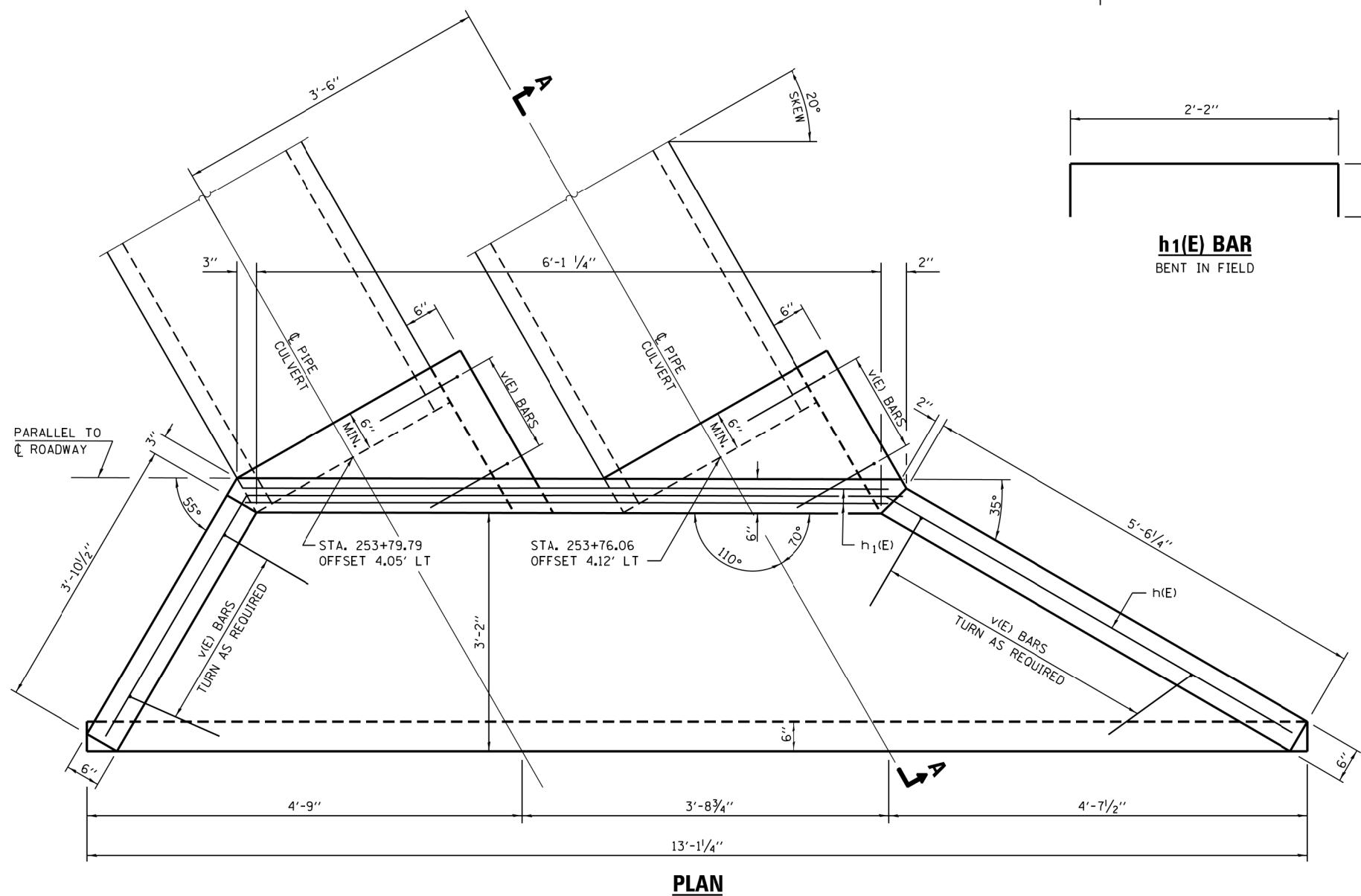
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344	544-RS-4	DUPAGE	53	25
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				



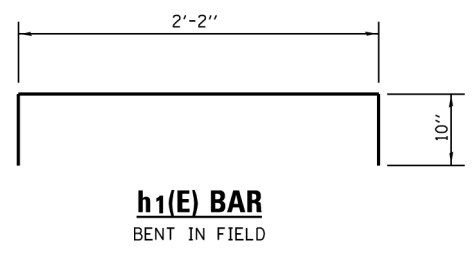
SECTION A-A



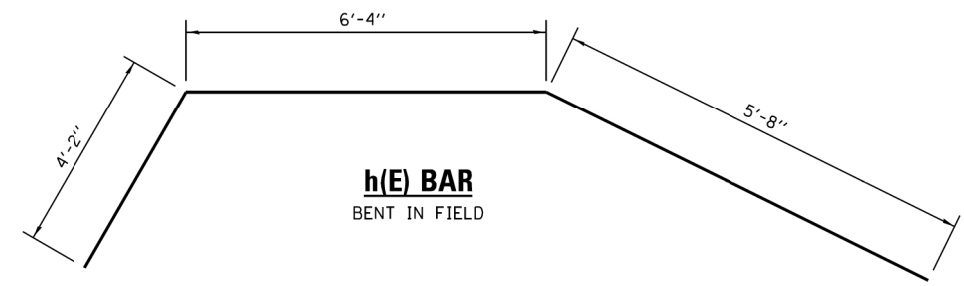
END VIEW



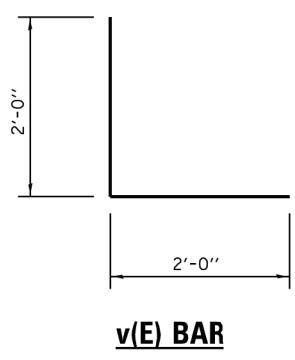
PLAN



**h1(E) BAR
BENT IN FIELD**



**h(E) BAR
BENT IN FIELD**



v(E) BAR

BILL OF MATERIAL *

BAR	NO.	SIZE	LENGTH	SHAPE	
h(E)	1	#4	16'-2"		
h1(E)	4	#5	3'-10"		
v(E)	37	#4	4'-0"		
REINFORCEMENT BARS				POUND	130
CONCRETE STRUCTURES				CU. YD.	1.7

* FOR INFORMATION ONLY. COST OF REINFORCEMENT BARS, CONCRETE STRUCTURE AND ANY OTHER REQUIRED MATERIAL AND LABOR WILL BE INCLUDED IN THE PAYMENT IN THE CONTRACT UNIT PRICE FOR CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 2-18" (SPECIAL).

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

FILE NAME = SA\JUL163200-6339\6346\051\Micro\CA00D_Sheet\0160N52-sh1-CULVERT.dgn



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 MODEL NAME = Default
 PLOT SCALE = 2.0000' / in.
 PLOT DATE = 1/4/2013

DESIGNED - RRD
 DRAWN - B/JF
 CHECKED - AJS
 DATE - 01-03-13

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS

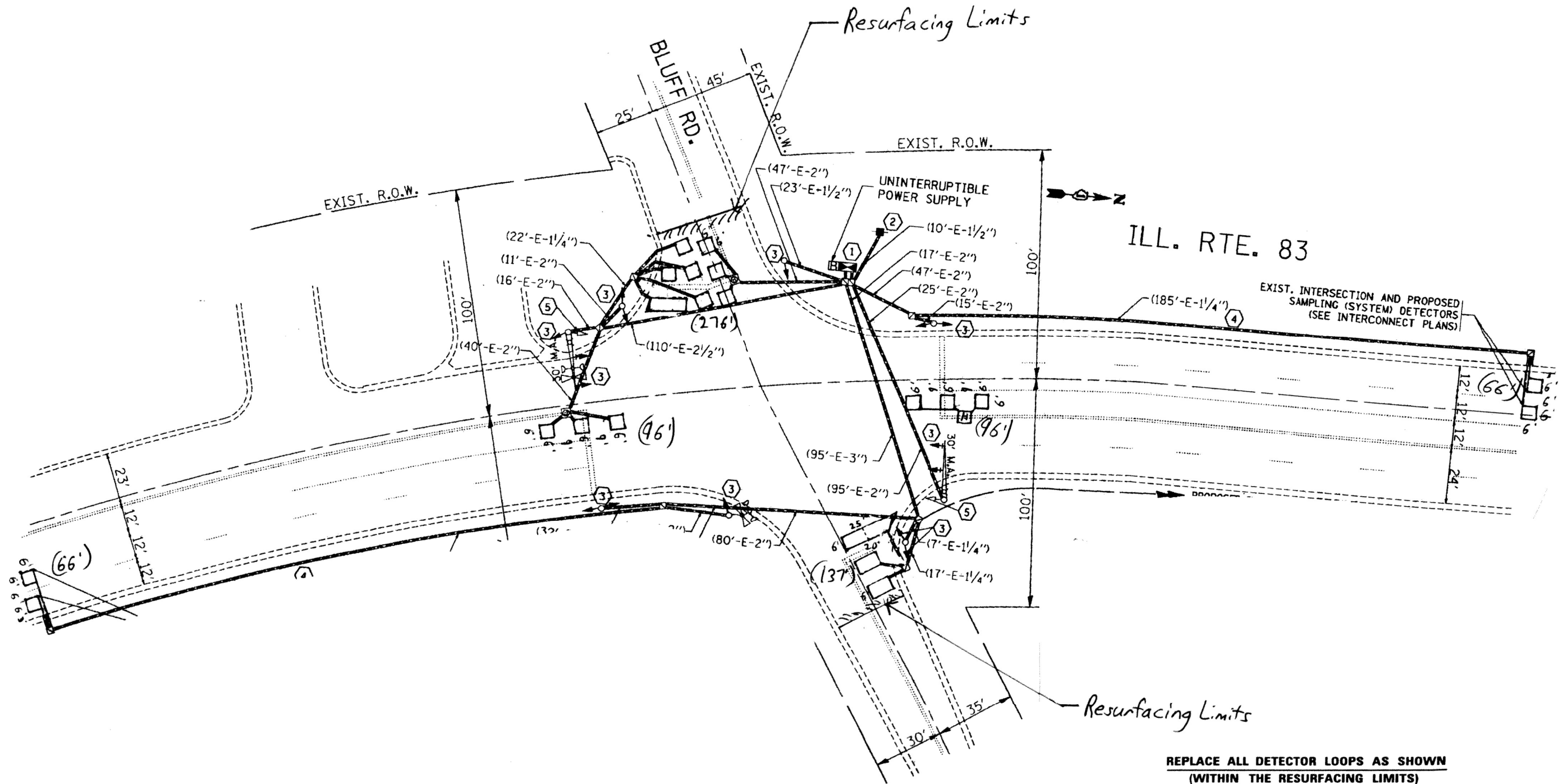
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	26
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	737	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/4/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL 83 AT BLUFF ROAD**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	27
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET OF SHEETS STA. TO STA.

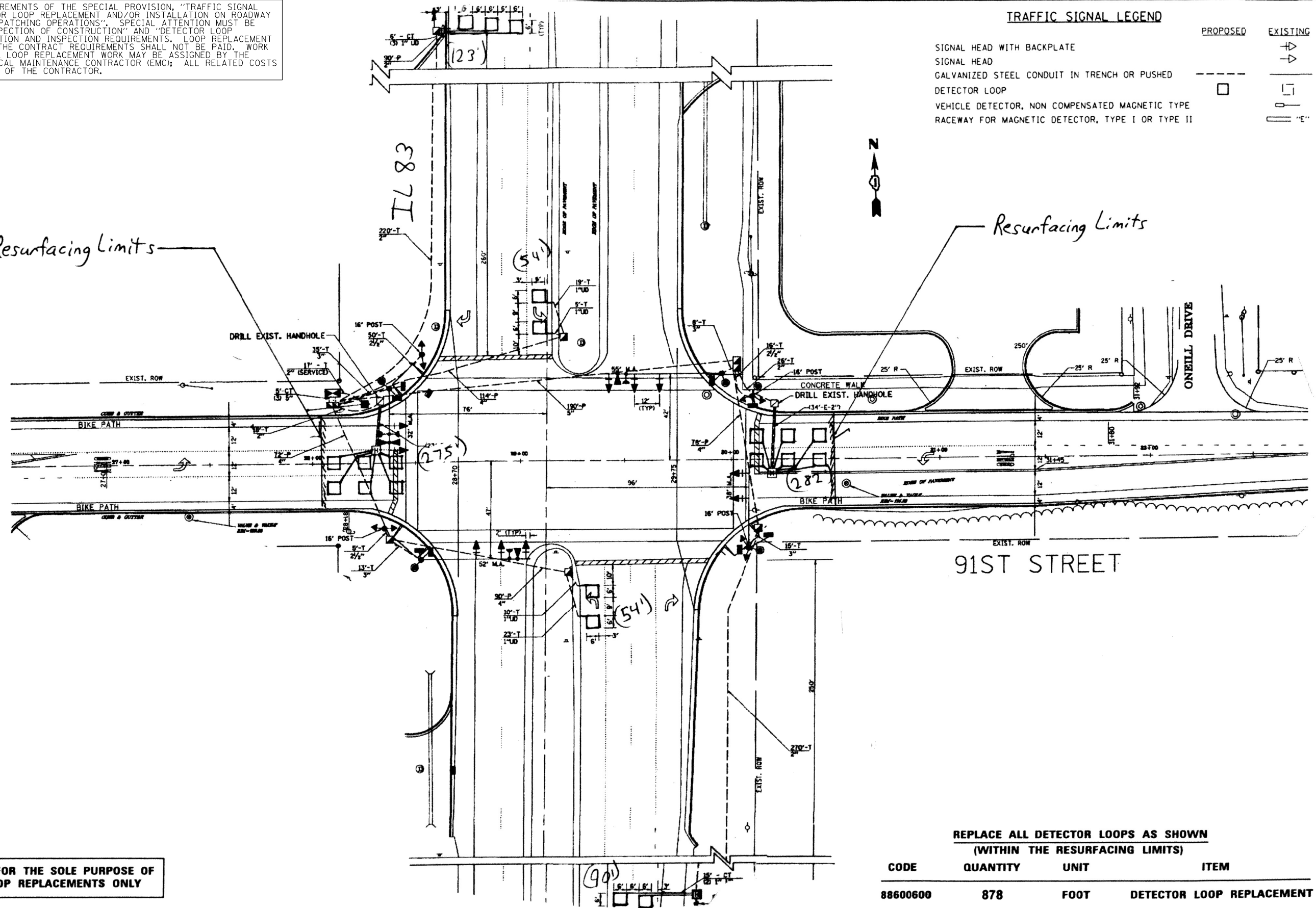
WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

Resurfacing Limits

Resurfacing Limits



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

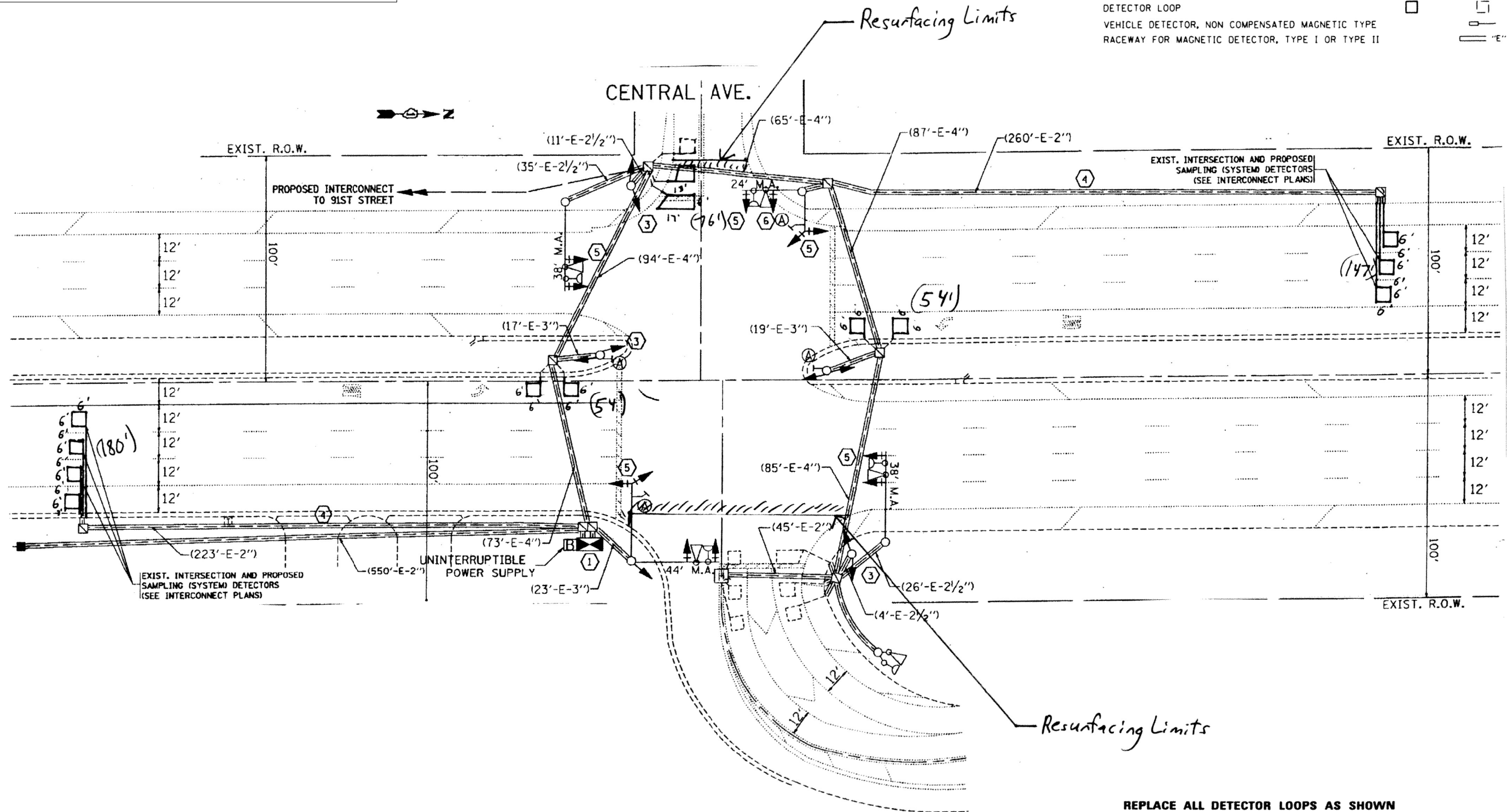
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	878	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	⊕	⊕
SIGNAL HEAD	⊕	⊕
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	□	□
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	— "E"	— "E"



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

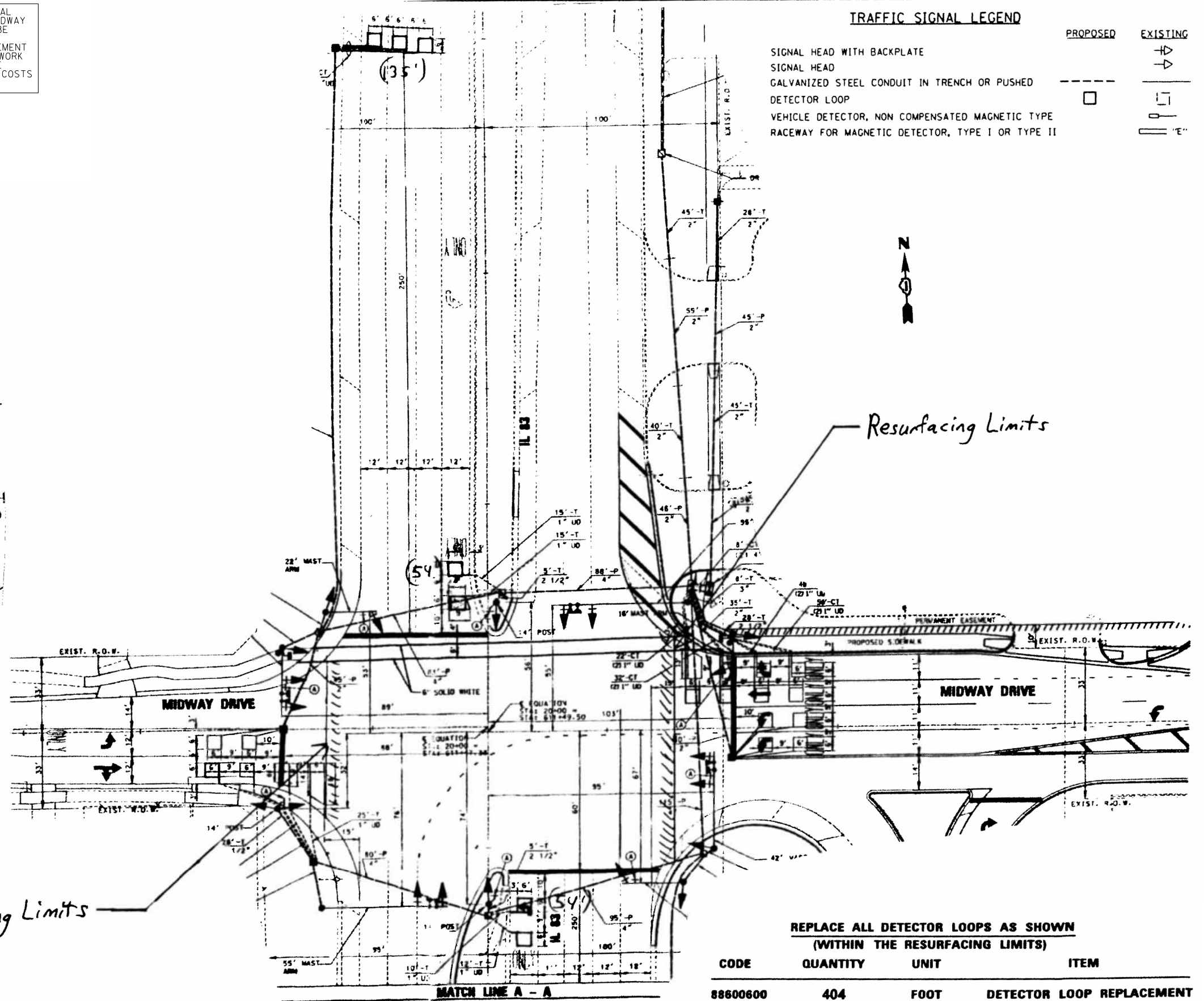
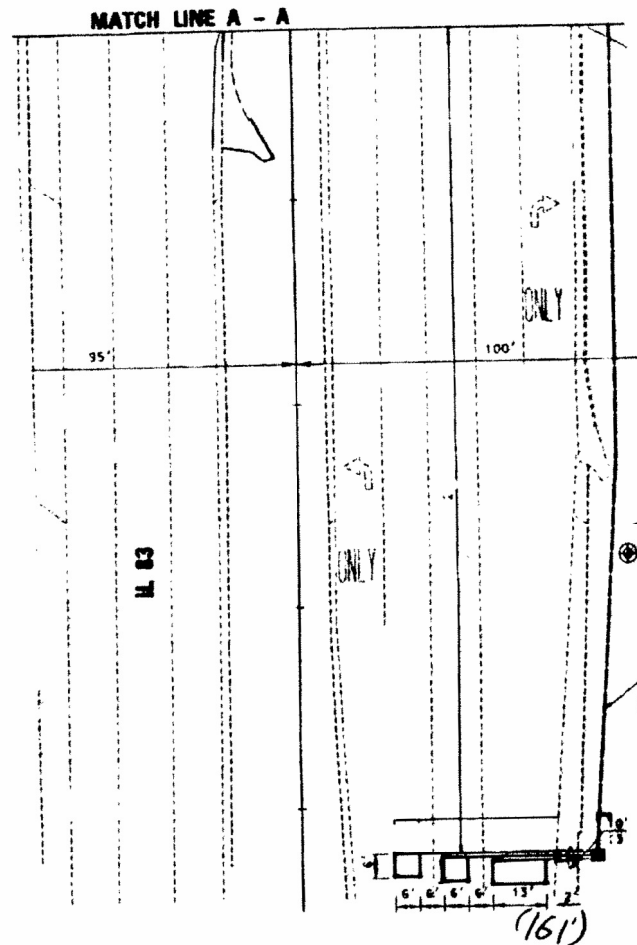
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	511	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

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SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

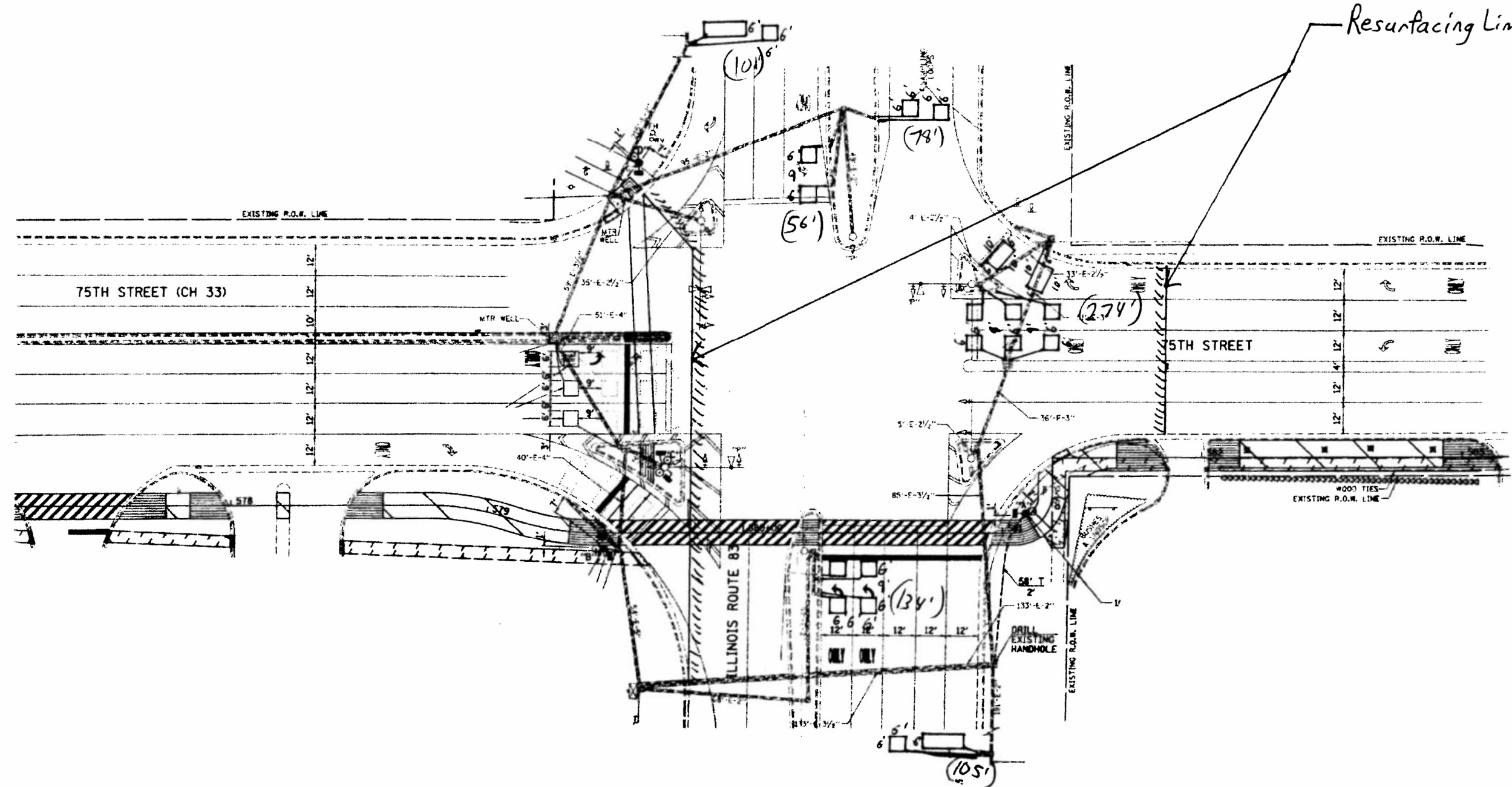
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	404	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

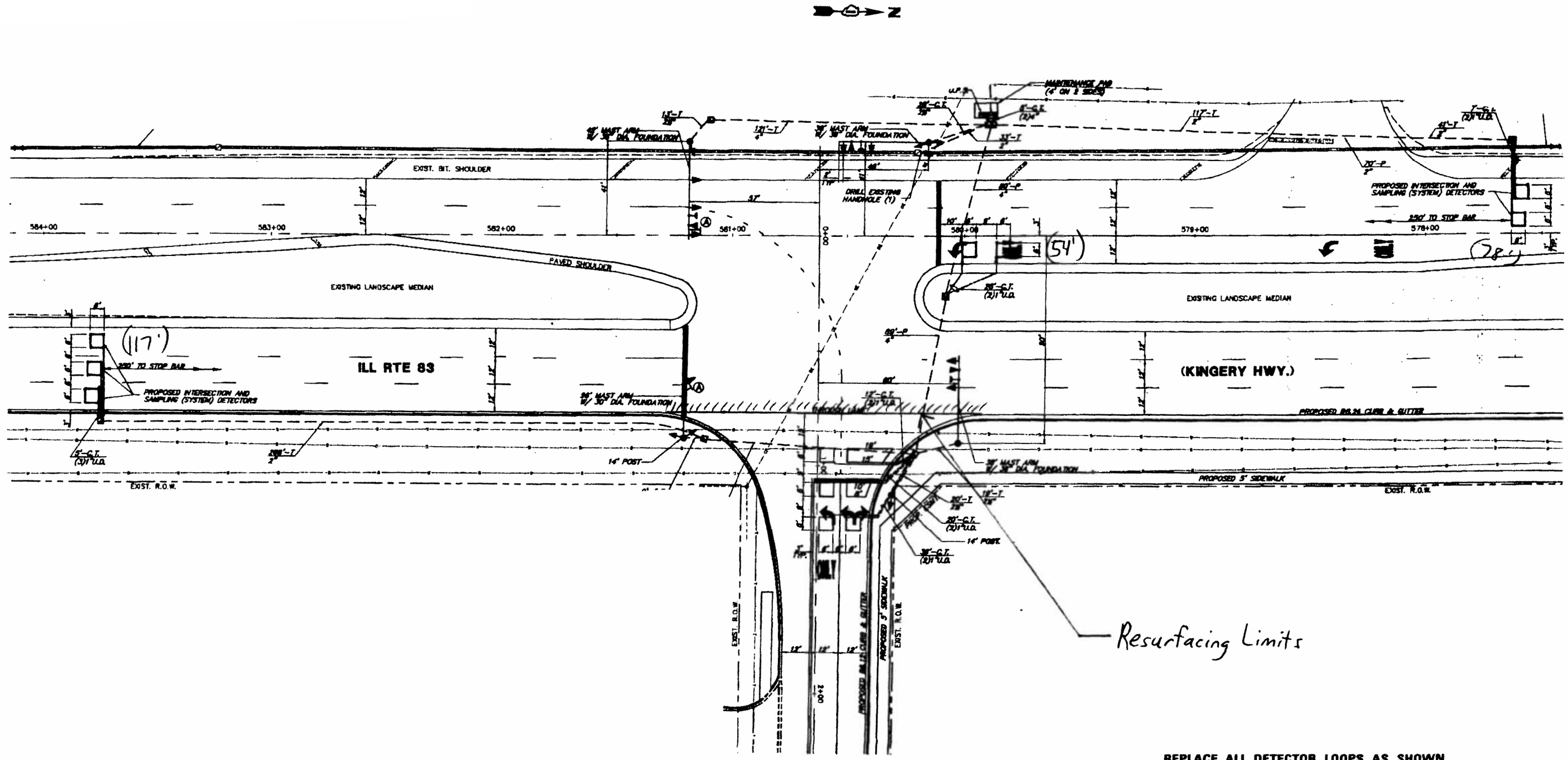
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	748	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	◀	▶
SIGNAL HEAD	◀	▶
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

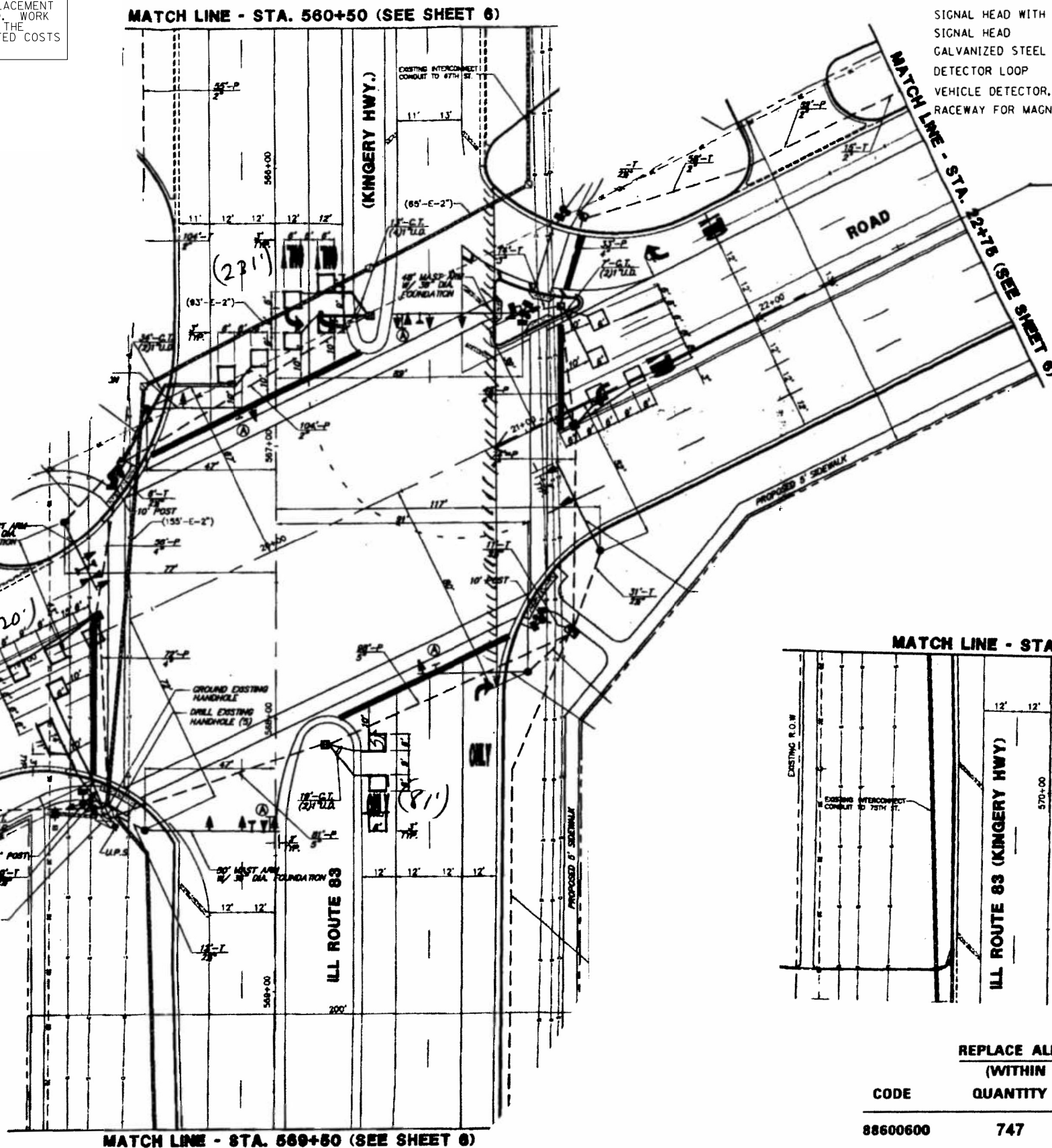
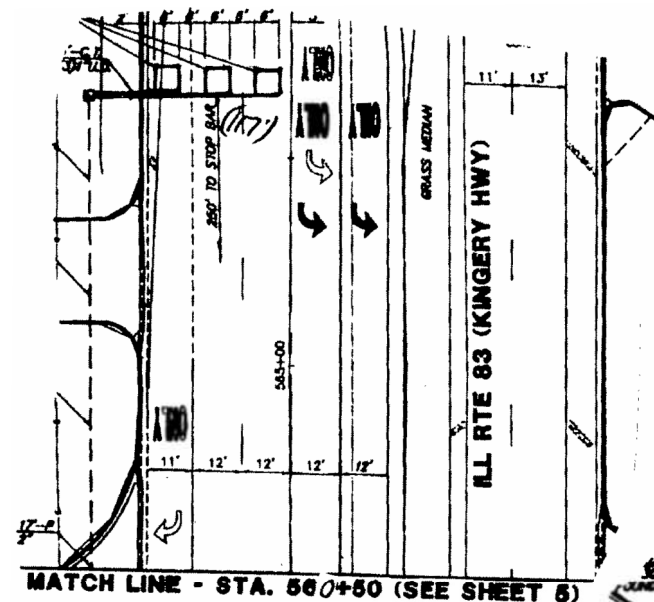
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	249	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

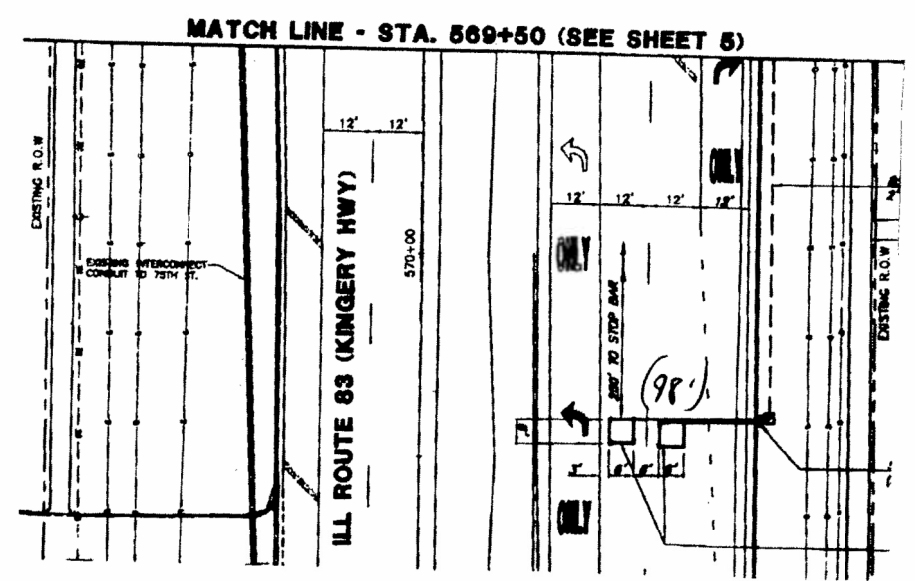
	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



Resurfacing Limits



Resurfacing Limits



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

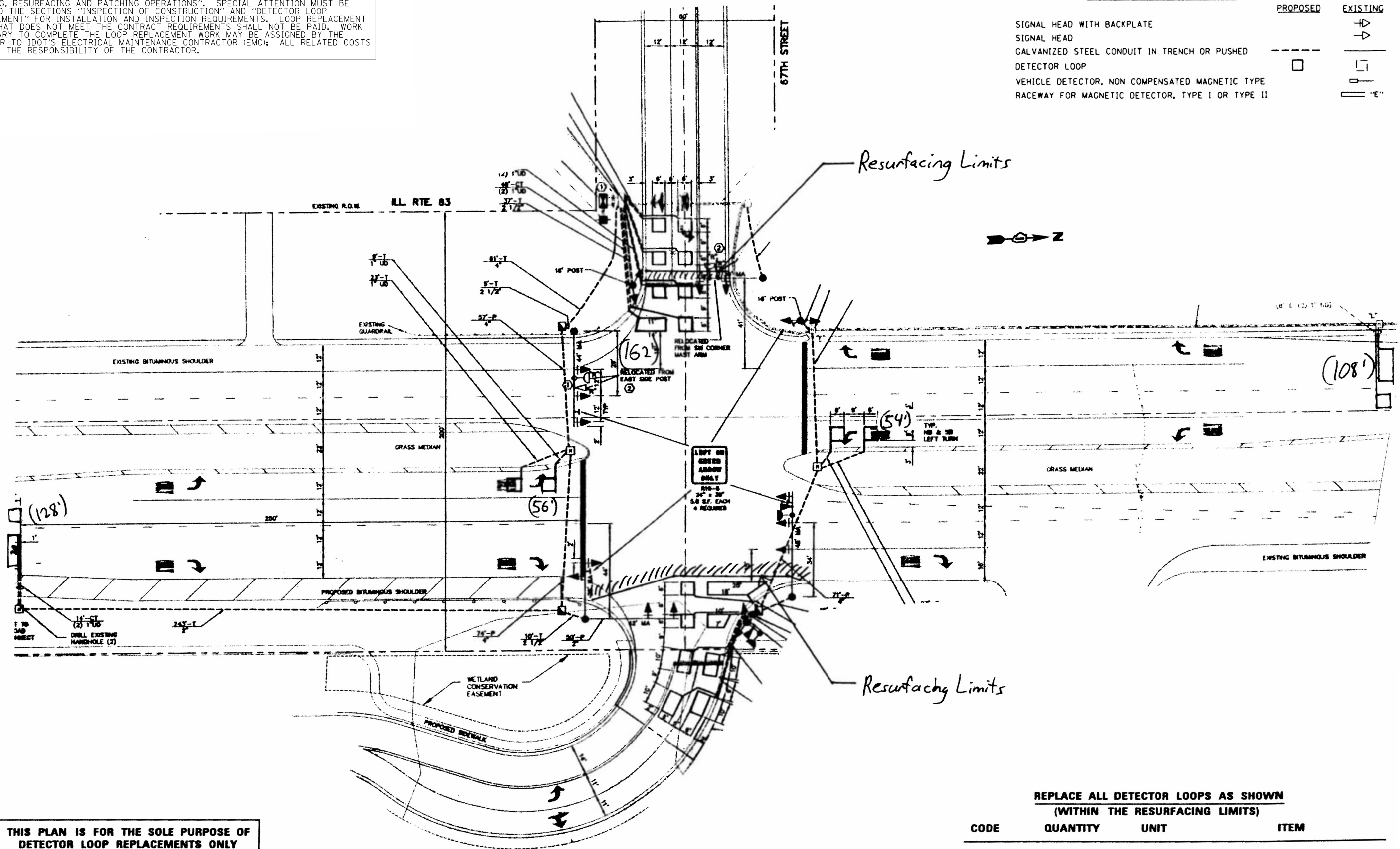
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	747	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

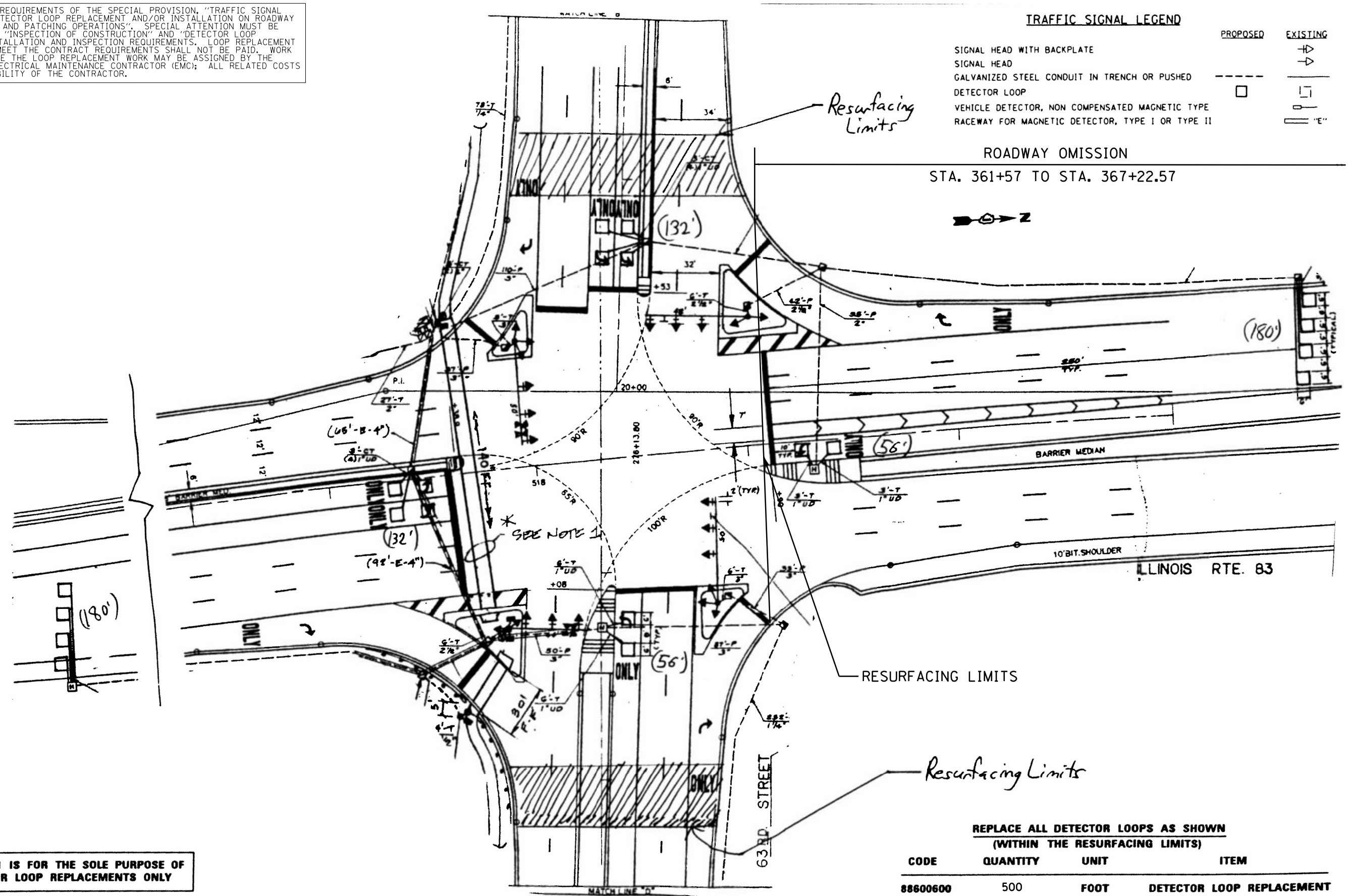
CODE	QUANTITY	UNIT	ITEM
88600600	508	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

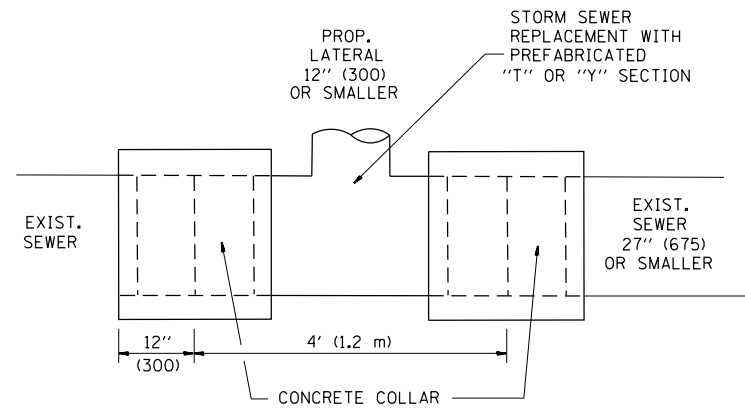
ROADWAY OMISSION
STA. 361+57 TO STA. 367+22.57



**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

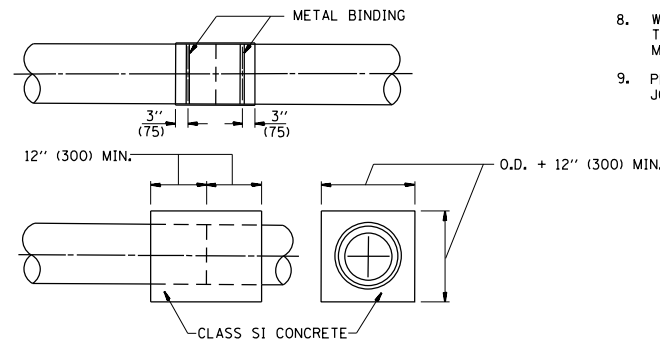
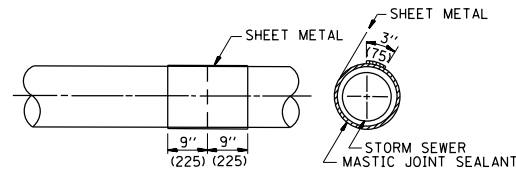
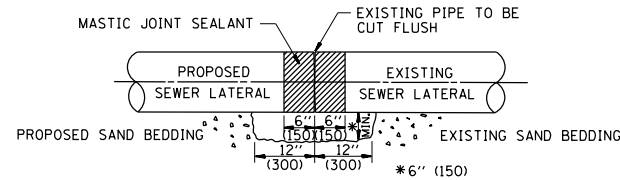
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	500	FOOT	DETECTOR LOOP REPLACEMENT



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



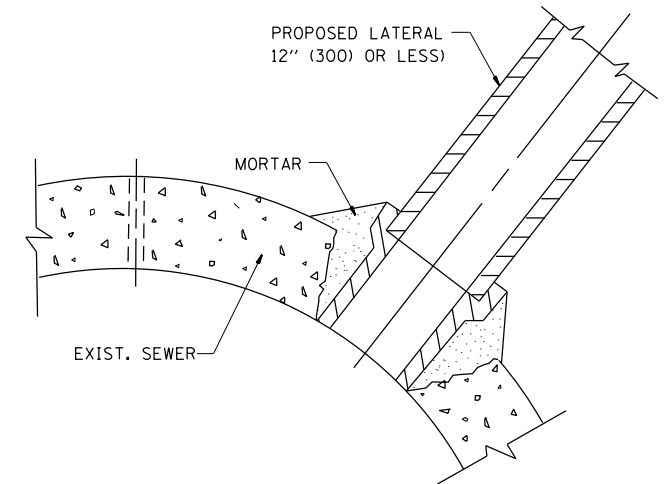
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

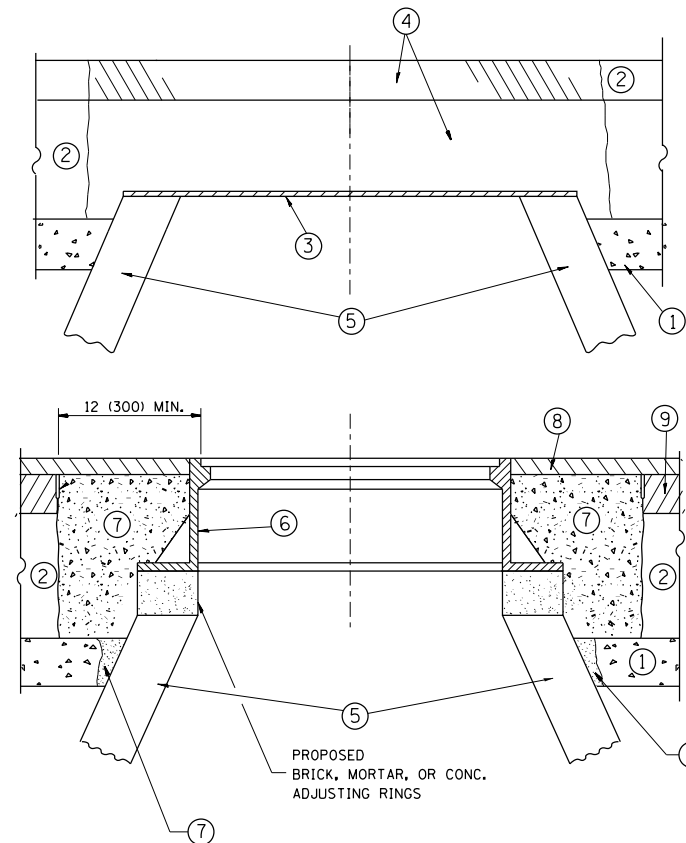
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	PLOT DATE = 6/13/2014	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	36
BD500-01 (BD-7)		CONTRACT NO. 60N52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

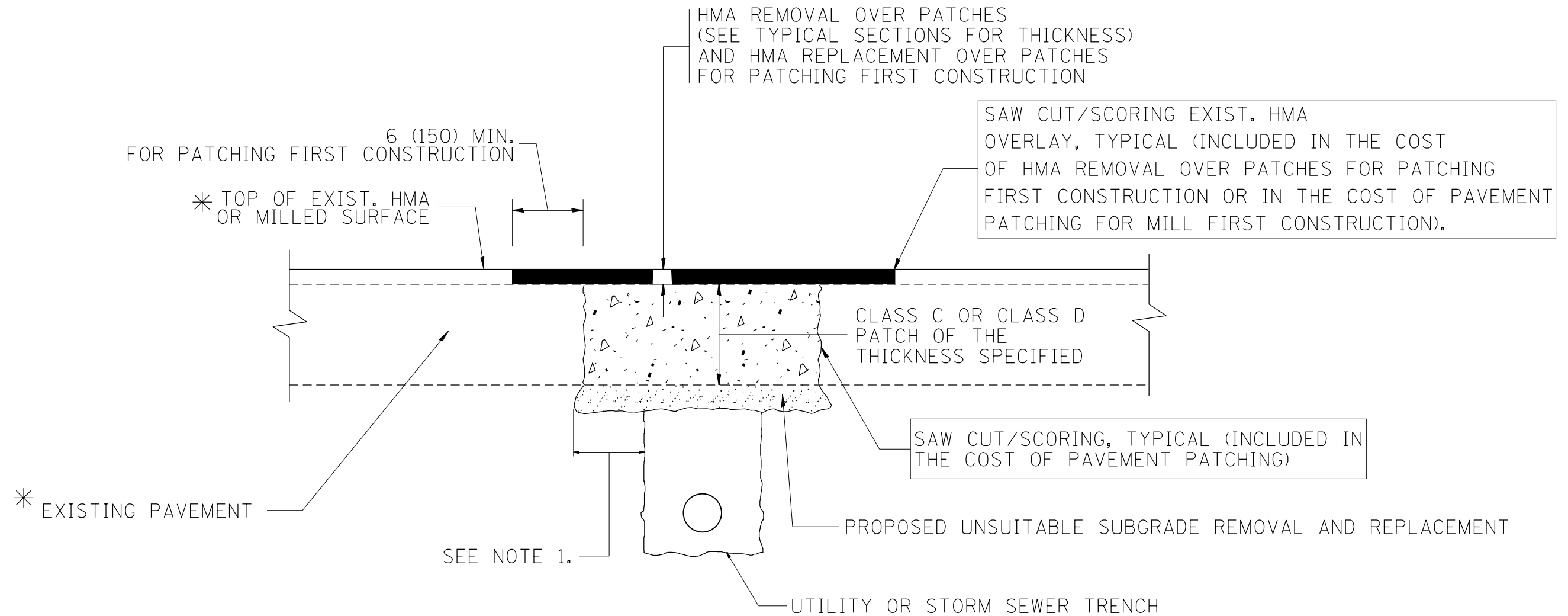
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = marenade	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT DATE = 6/13/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	37
BD600-03 (BD-8)		CONTRACT NO. 60N52		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
et:\pw\work\p\dot\marenade\d0316166\01sStd.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 6/13/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	38
BD400-04 (BD-22)			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

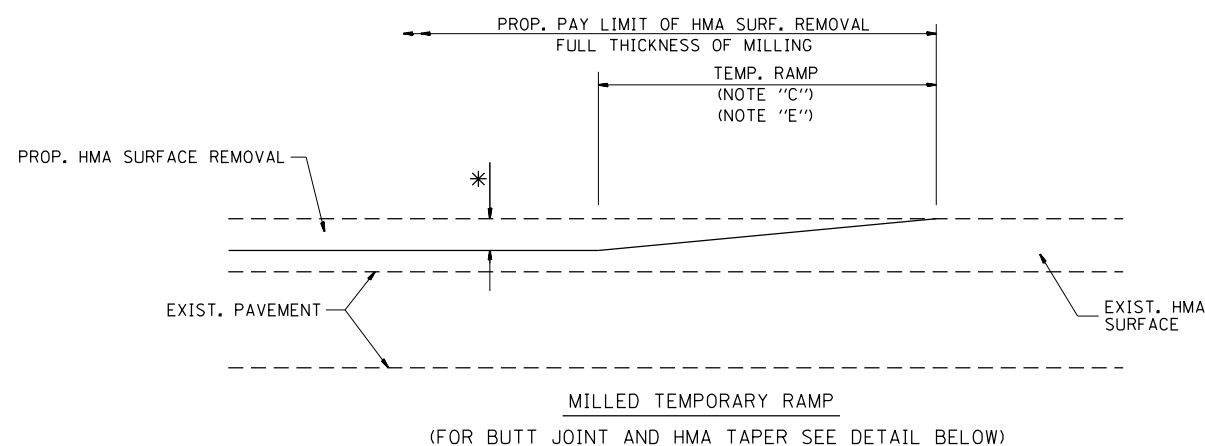
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

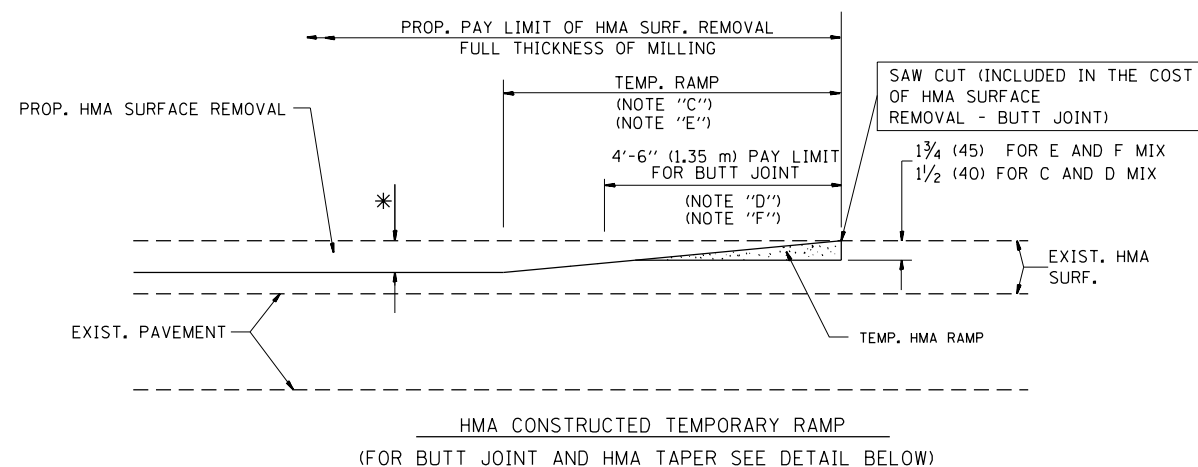
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\marenade\d0316166\0195Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					344	544-RS-4	DUPAGE	53	39
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09		BD600-06 (BD-24)			CONTRACT NO. 60N52				
PLOT DATE = 6/13/2014	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

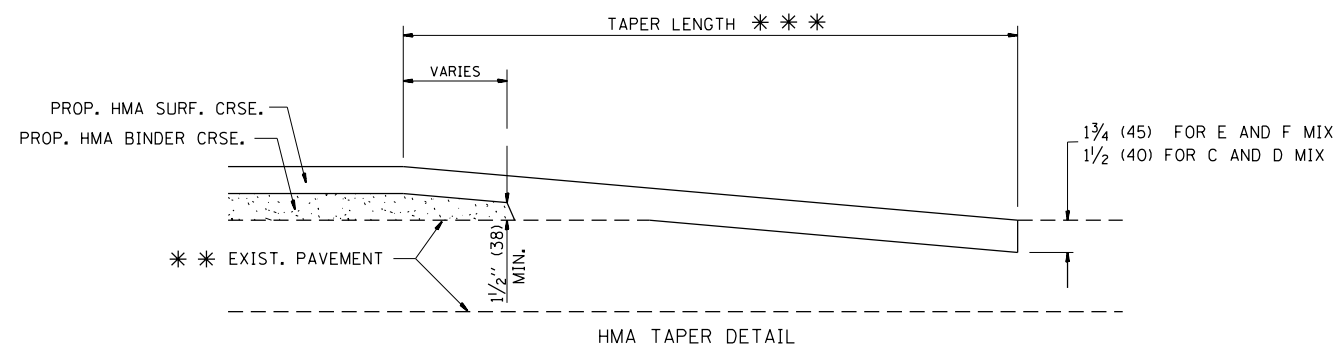
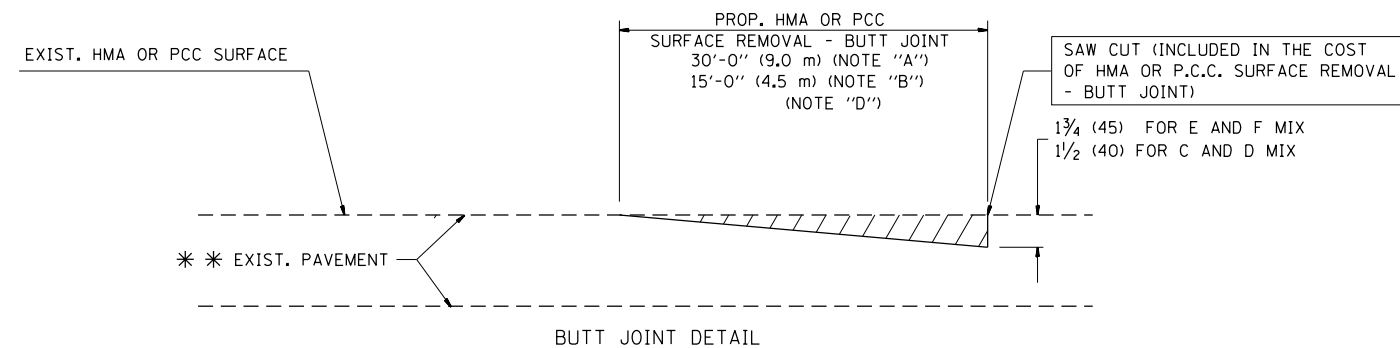


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

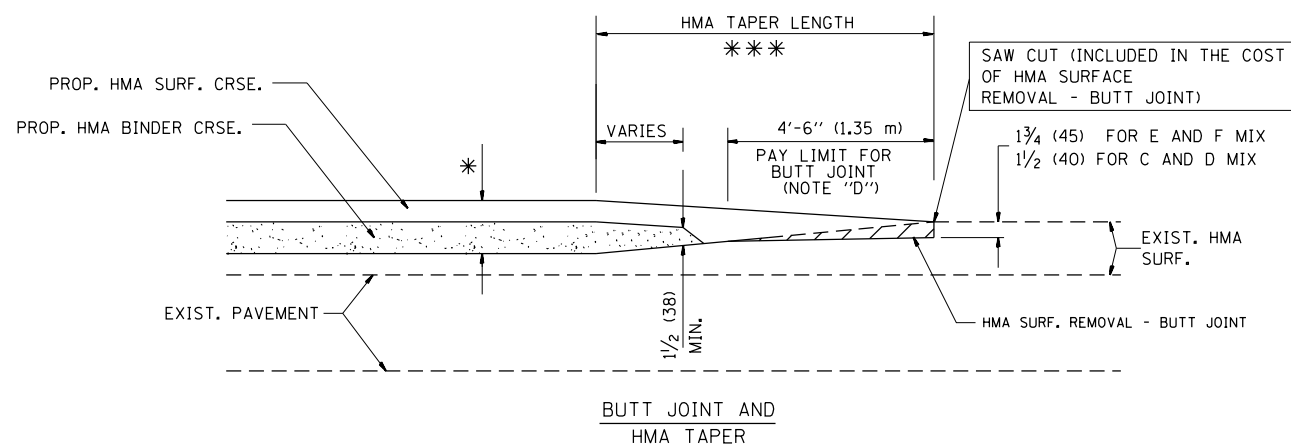
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

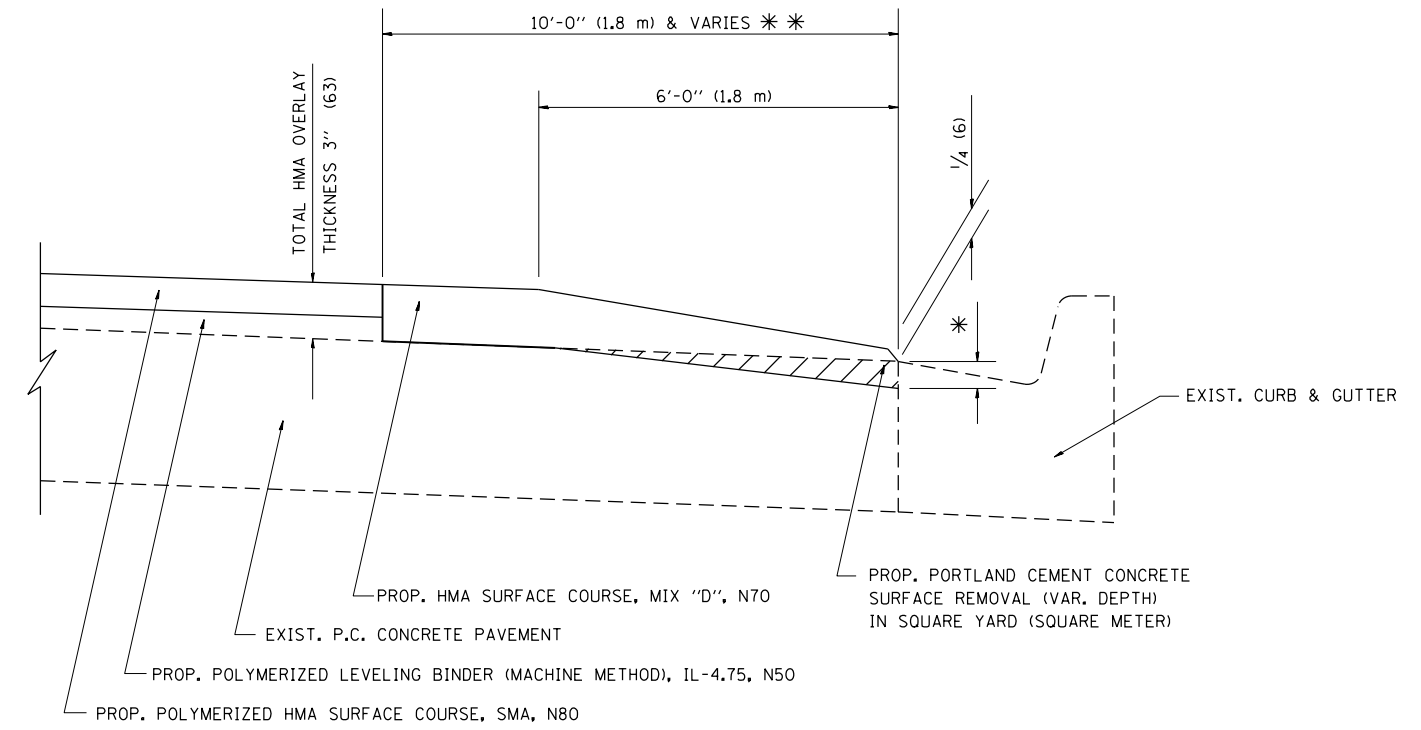
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	PLOT DATE = 6/13/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	40
BD400-05 BD32		CONTRACT NO. 60N52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE		
MIX	THICKNESS	* MILLING AT GUTTER FLAG
* *	D	3" 2 3/4"

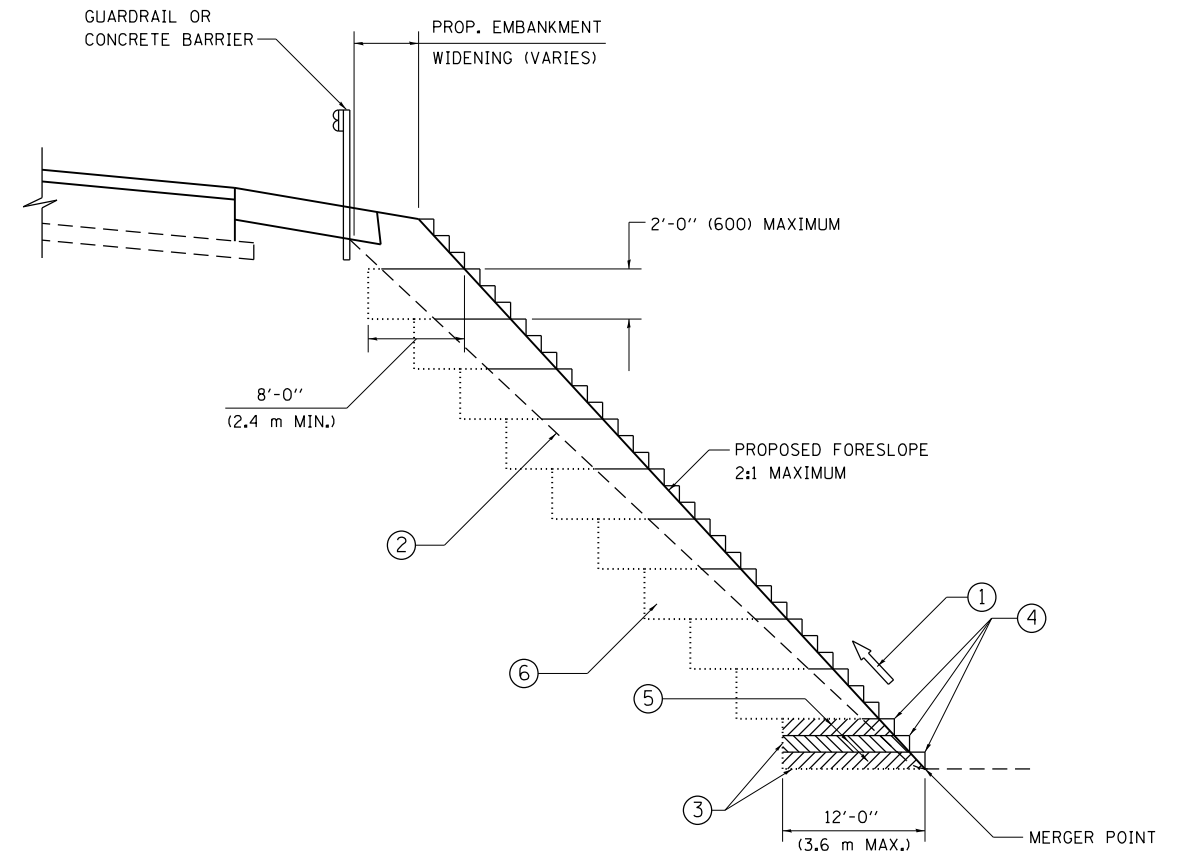
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4		53	41
CONTRACT NO. 60N52				
ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -
et:\pw\work\p\idot\marenade\d0316166\0195Std.dgn		DRAWN - CADD	REVISED -
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	PLOT DATE = 6/13/2014	DATE - 06-16-04	REVISED -

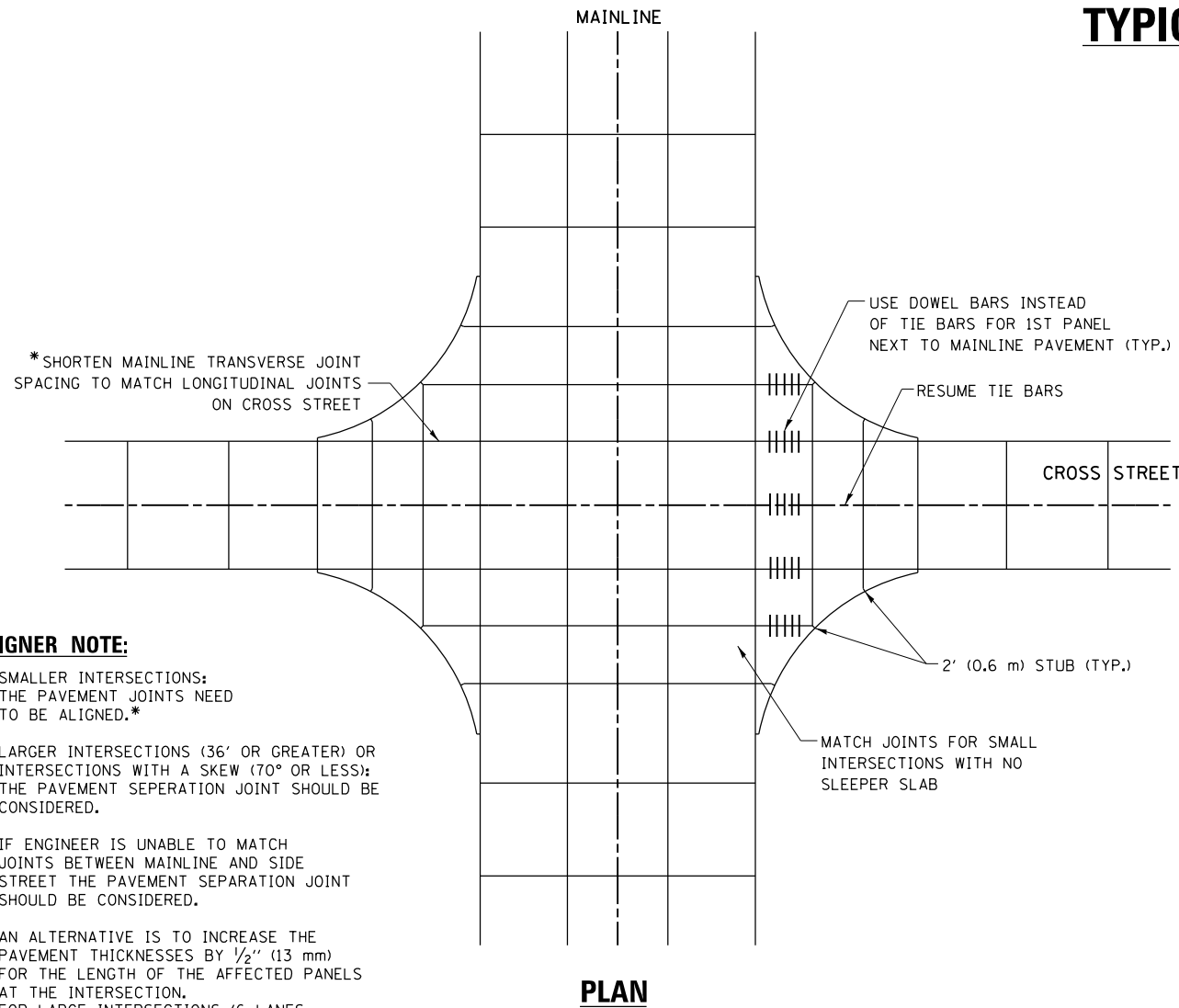
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BENCHING DETAIL FOR EMBANKMENT WIDENING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	42
BD-51			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TYPICAL APPLICATION

THE USE OF CROSS STREET PAVEMENT SEPARATION JOINTS FOR SKEWED OR LARGE INTERSECTIONS WHERE JOINTS MAY NOT MATCH



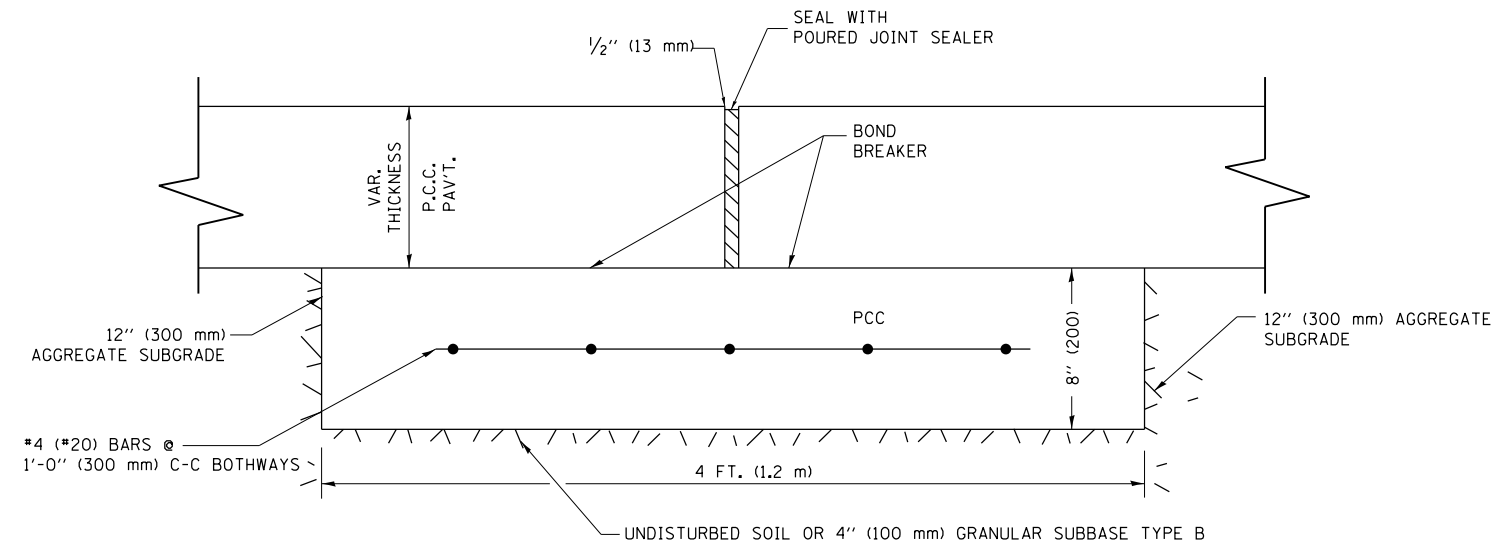
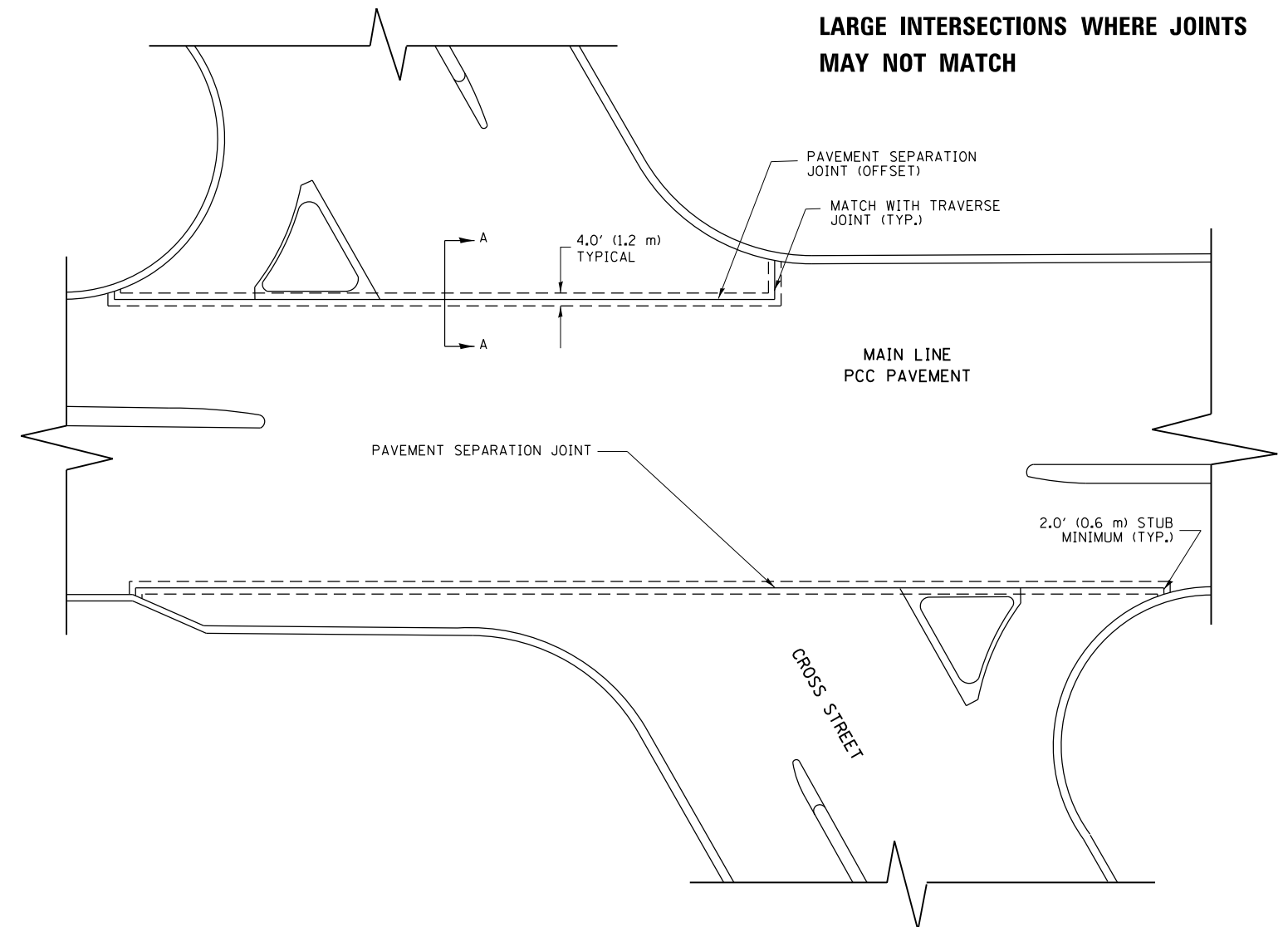
PLAN

DESIGNER NOTE:

1. SMALLER INTERSECTIONS: THE PAVEMENT JOINTS NEED TO BE ALIGNED.*
2. LARGER INTERSECTIONS (36' OR GREATER) OR INTERSECTIONS WITH A SKEW (70° OR LESS): THE PAVEMENT SEPERATION JOINT SHOULD BE CONSIDERED.
3. IF ENGINEER IS UNABLE TO MATCH JOINTS BETWEEN MAINLINE AND SIDE STREET THE PAVEMENT SEPARATION JOINT SHOULD BE CONSIDERED.
4. AN ALTERNATIVE IS TO INCREASE THE PAVEMENT THICKNESSES BY 1/2" (13 mm) FOR THE LENGTH OF THE AFFECTED PANELS AT THE INTERSECTION. FOR LARGE INTERSECTIONS (6 LANES OR MORE) WHERE JOINTS CAN BE MATCHED, USE #8 (25) DOWEL BARS INSTEAD OF #8 (25) TIE BARS AT EDGE OF MAINLINE PAVEMENT WHEN NO PAVEMENT SEPARATION JOINTS USED.

NOTE:

1. JOINT FILLER SHALL CONSIST OF A SHEET OF 1/2" (13 mm) BITUMINOUS PREFORMED FIBER JOINT FILLER CONFORMING TO ARTICLE 1051.03 OF THE STANDARD SPECIFICATIONS.
2. THE JOINT SHALL BE SEALED WITH A HOT POUR JOINT SEALER CONFORMING TO ARTICLE 1050.02 OF THE STANDARD SPECIFICATIONS.
3. A SINGLE LAYER OF FELT ROOFING PAPER SHALL SERVE AS A BOND BREAKER.
4. JOINT SHALL CONTINUE THROUGH COMBINATION CURB & GUTTER OR PCC SHOULDER.
5. PAVEMENT SEPARATION JOINT IS TO BE PAID FOR AS "SLEEPER SLAB" AND IS TO BE MEASURED IN PLACE BY THE LINEAL FOOT.
6. BOND BREAKER AND 1/2" (13 mm) JOINT AND FILLER SHALL BE INCIDENTAL TO THE PAY ITEM "SLEEPER SLAB".



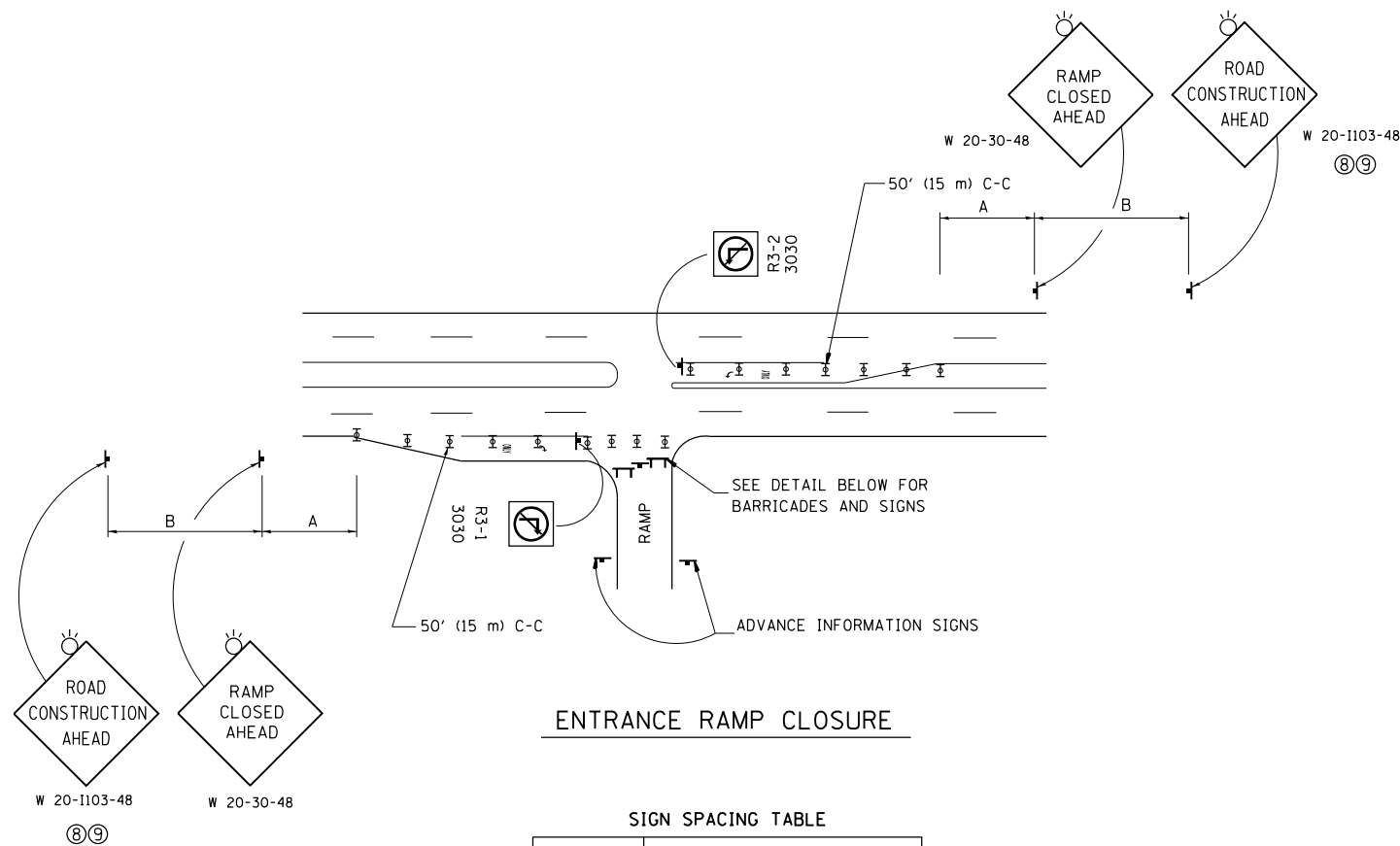
PROPOSED SECTION A-A

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		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF PAVEMENT SEPARATION JOINT FOR JOINTED PCC PAVEMENTS AT INTERSECTIONS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE. 344	SECTION 544-RS-4	COUNTY DUPAGE	TOTAL SHEETS 53	SHEET NO. 43
BD52		CONTRACT NO. 60N52		
ILLINOIS FED. AID PROJECT				

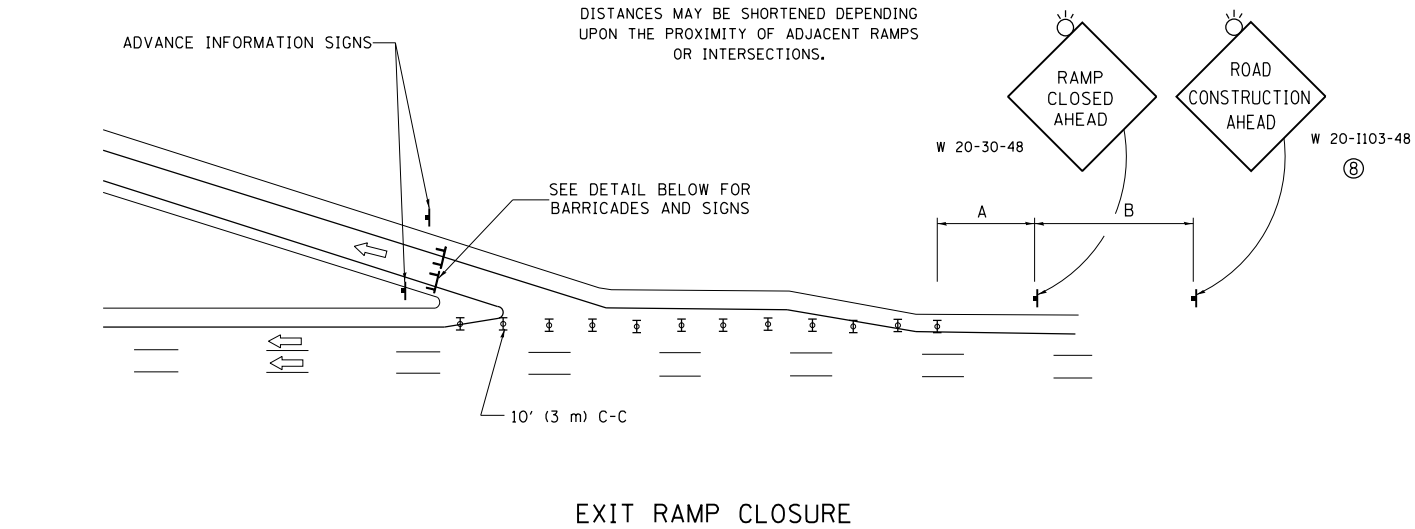


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

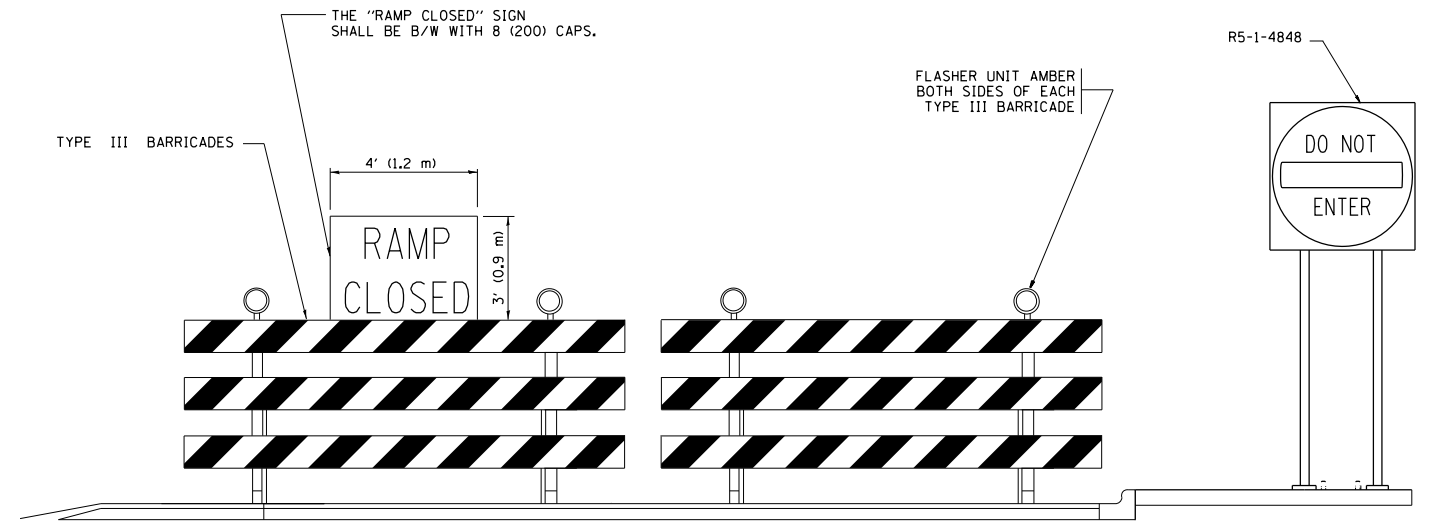
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

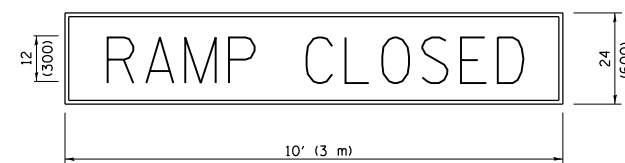
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

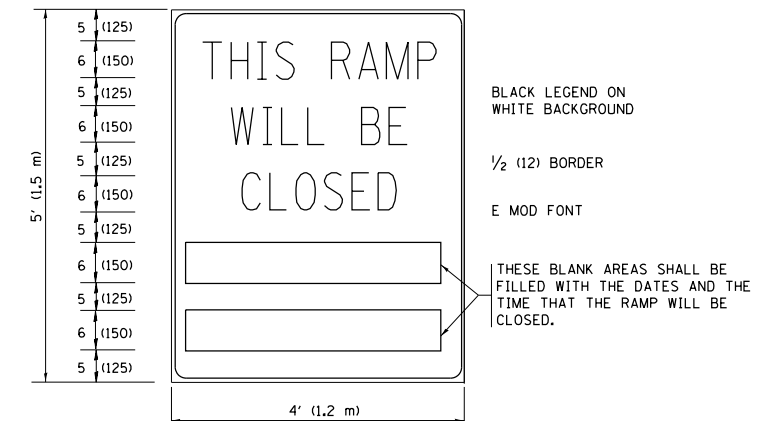
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

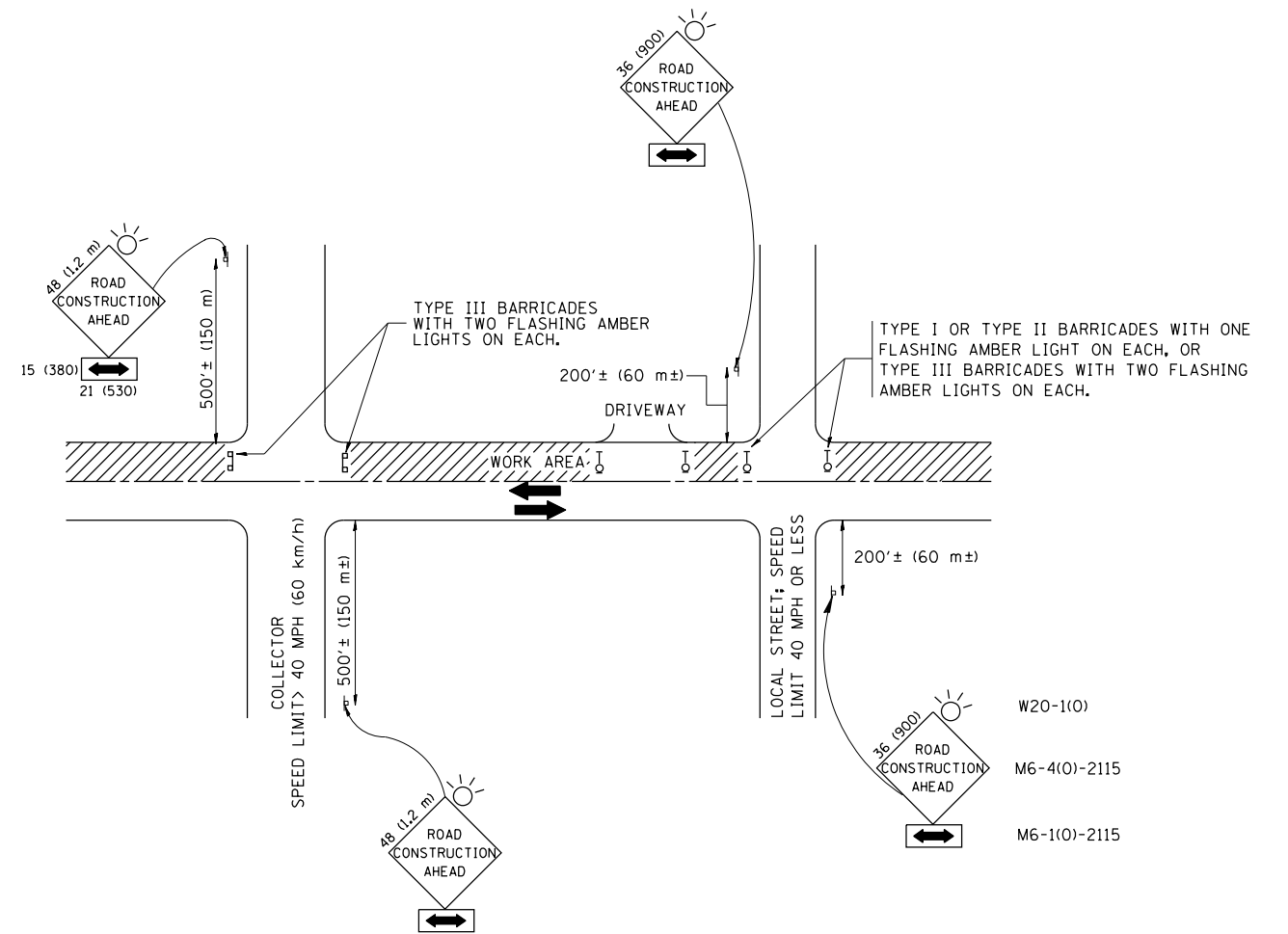
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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - SPB 12-09
	PLOT DATE = 6/13/2014	DATE - 02-83	REVISED - MD 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	44
TC-08		CONTRACT NO. 60N52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

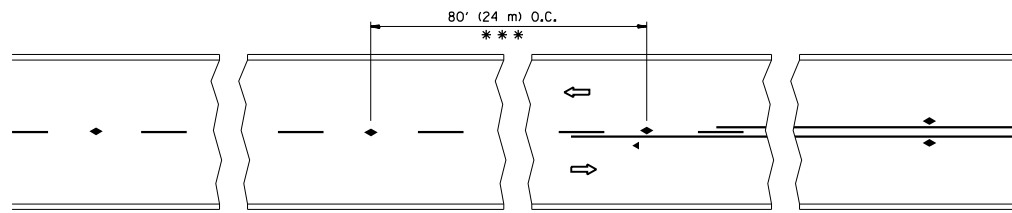
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 6/13/2014	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

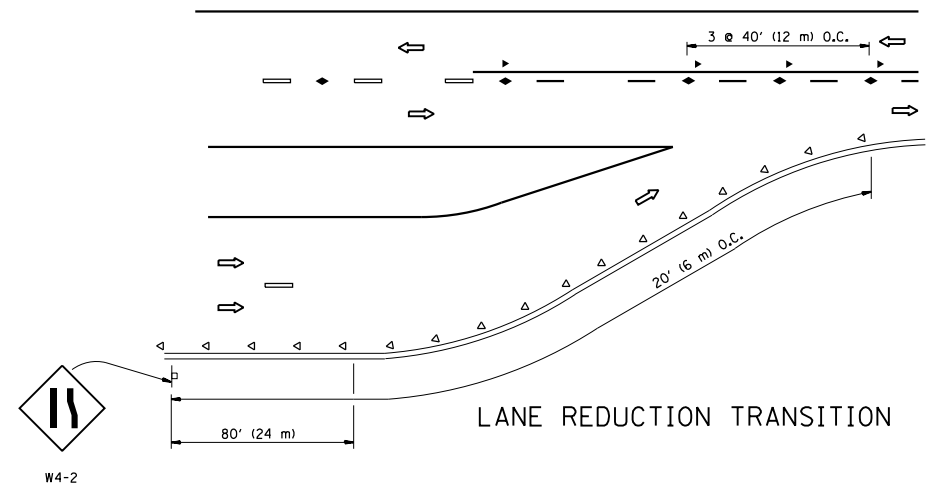
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

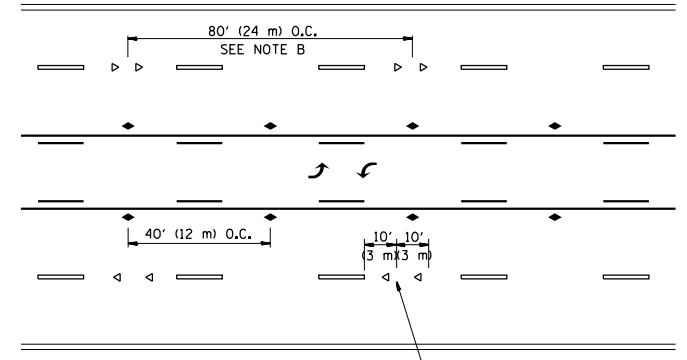


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

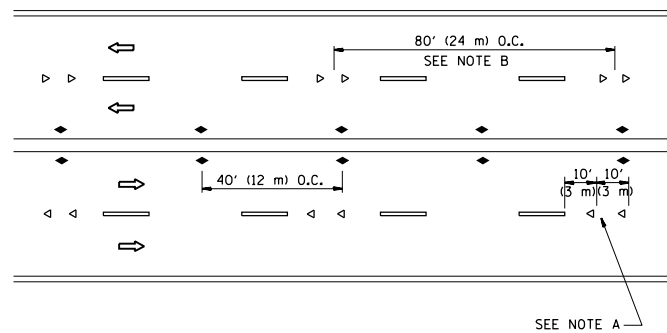
TWO-LANE/TWO-WAY



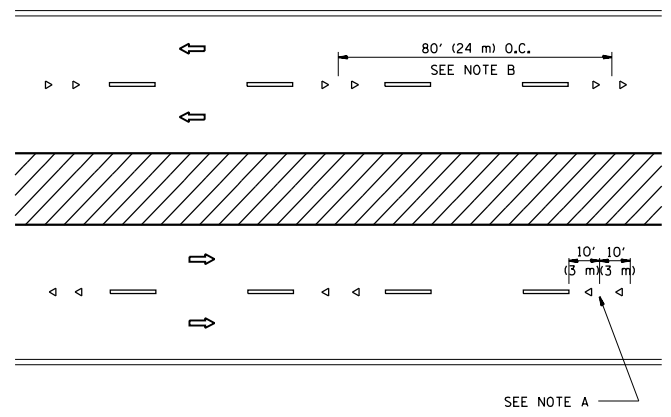
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

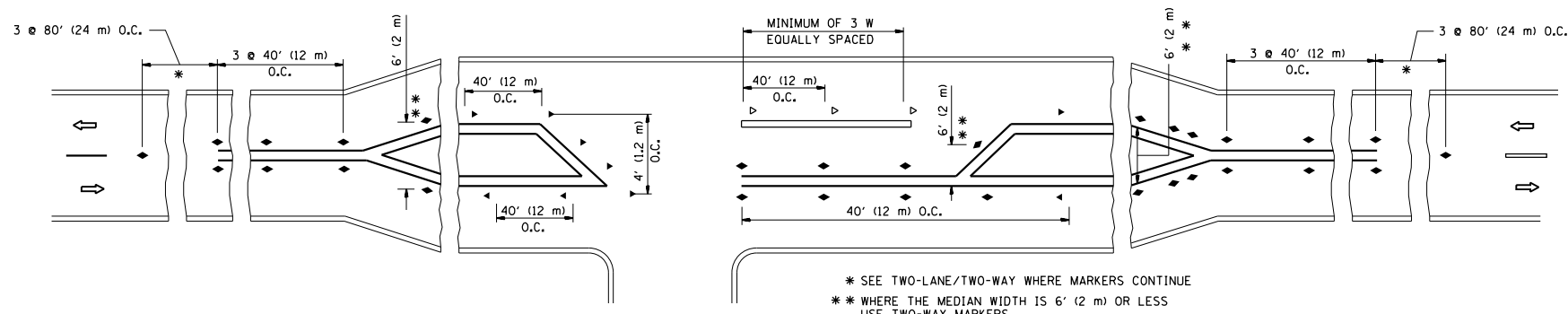
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

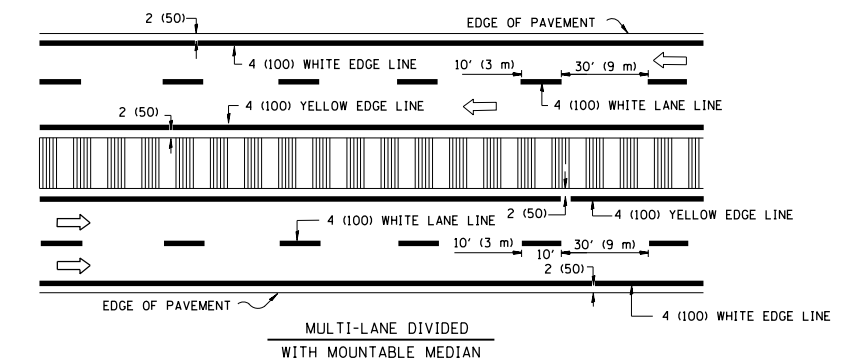
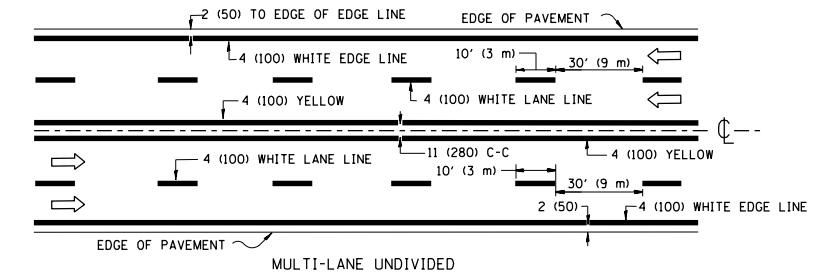
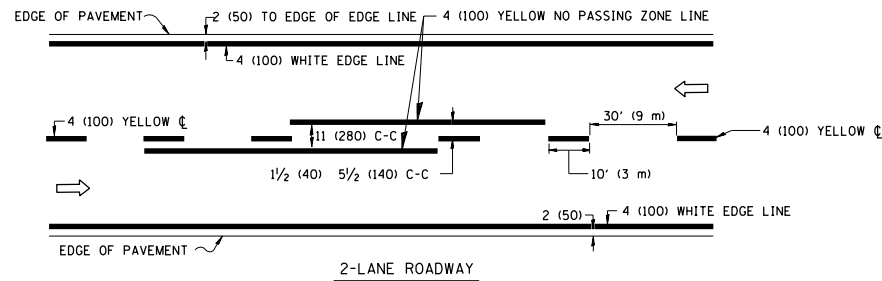


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

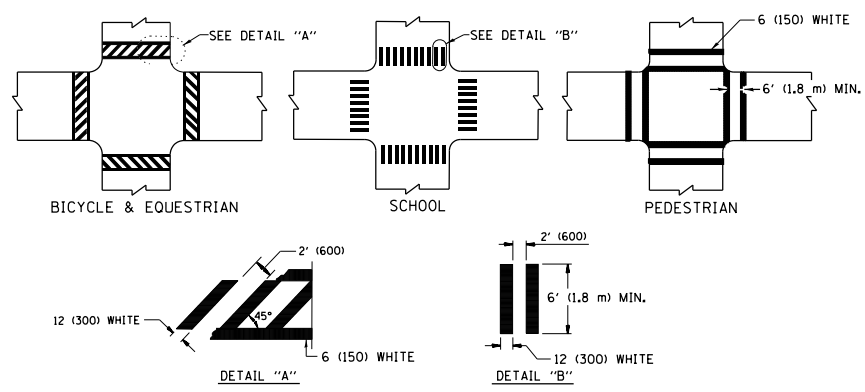
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\midot\marenade\d0316166\0195Std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99					344	544-RS-4	DUPAGE	53	46
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 60N52				
	PLOT DATE = 6/13/2014	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

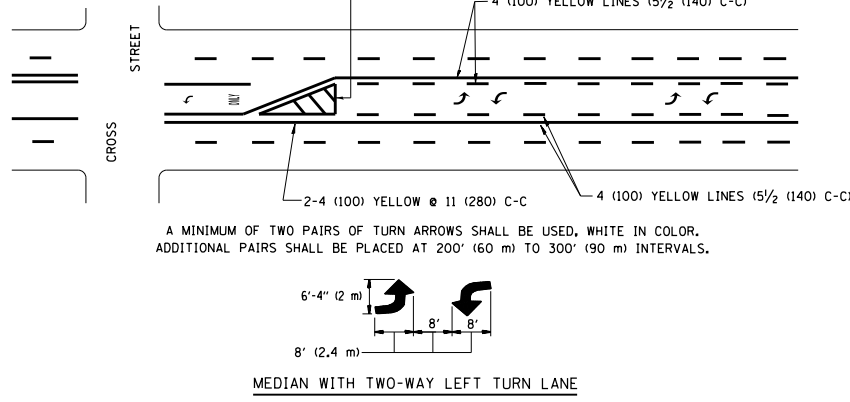
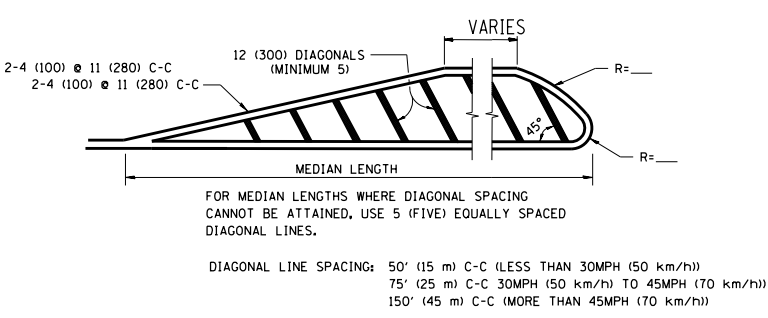
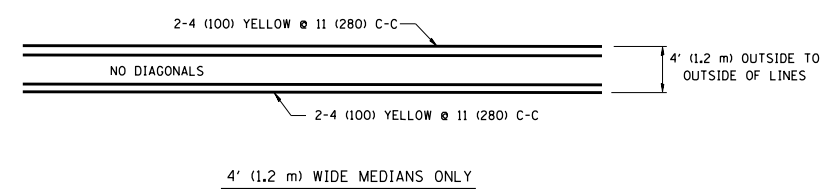


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

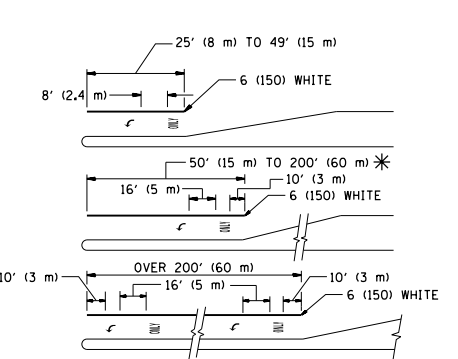


TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING

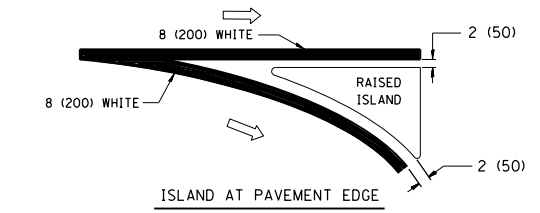
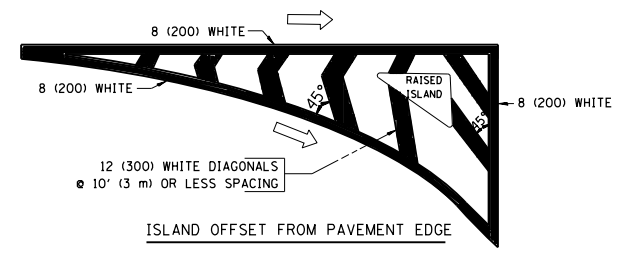
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

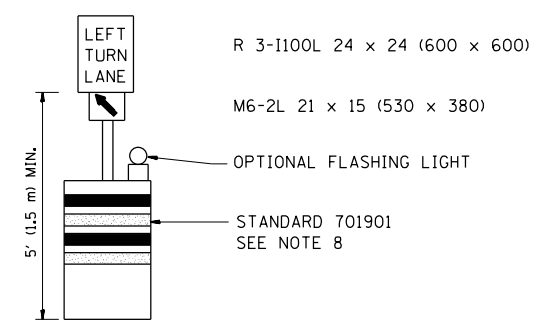
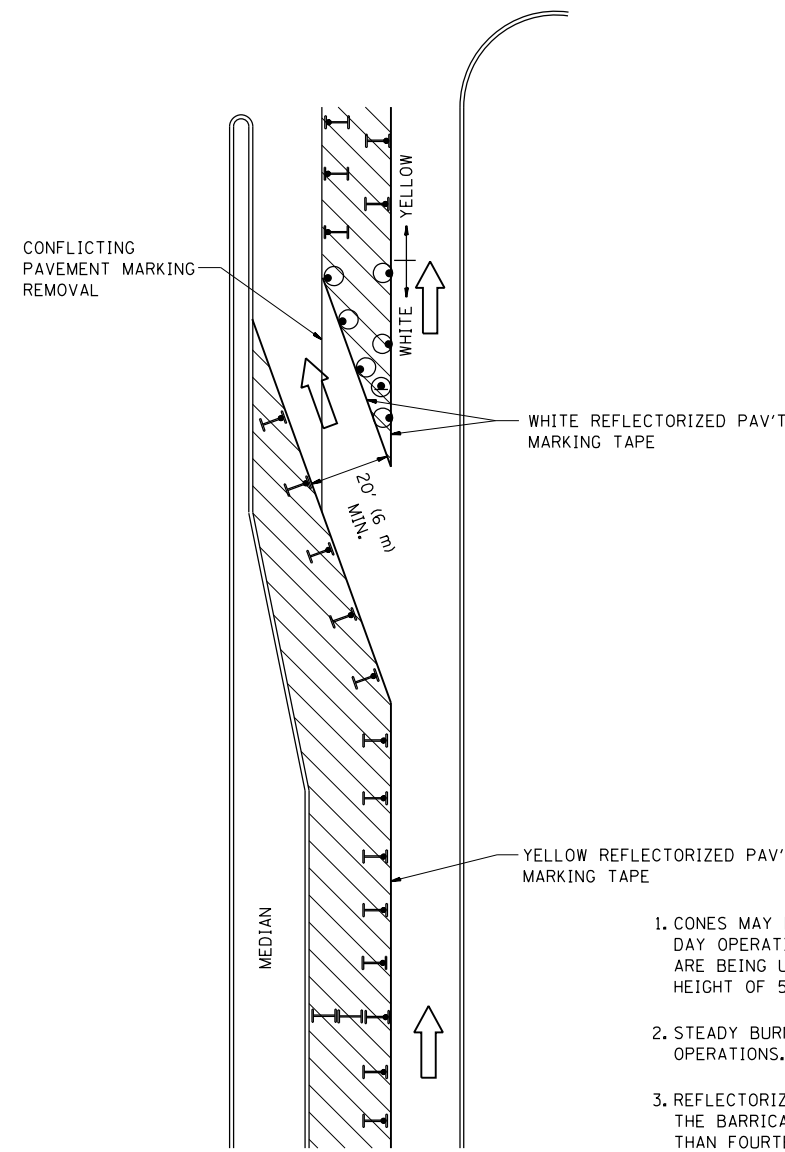
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 6/13/2014	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	47
TC-13		CONTRACT NO. 60N52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = marenade	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
et:\pw\work\pwidot\marenade\d0316166\019\Std.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 6/13/2014	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	48
TC-14			CONTRACT NO. 60N52	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\dot\marenade\d0316166\01s\Std.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 6/13/2014	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

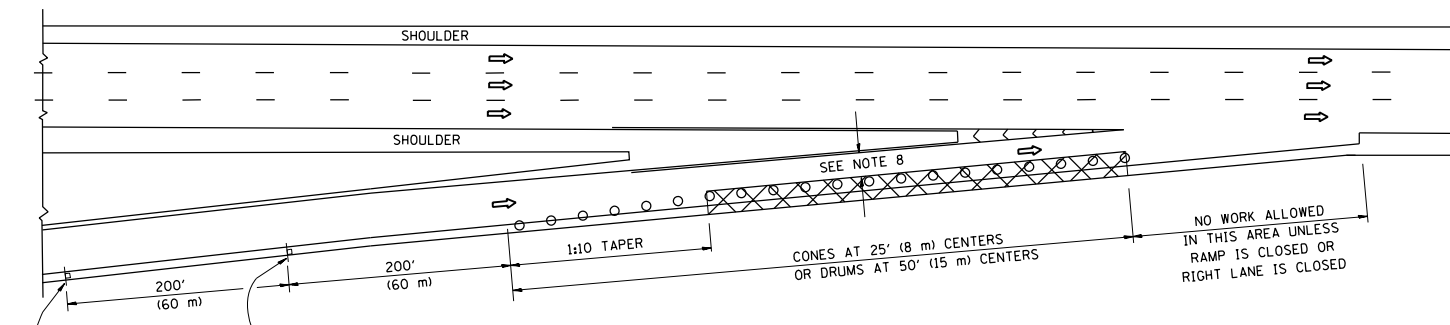
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

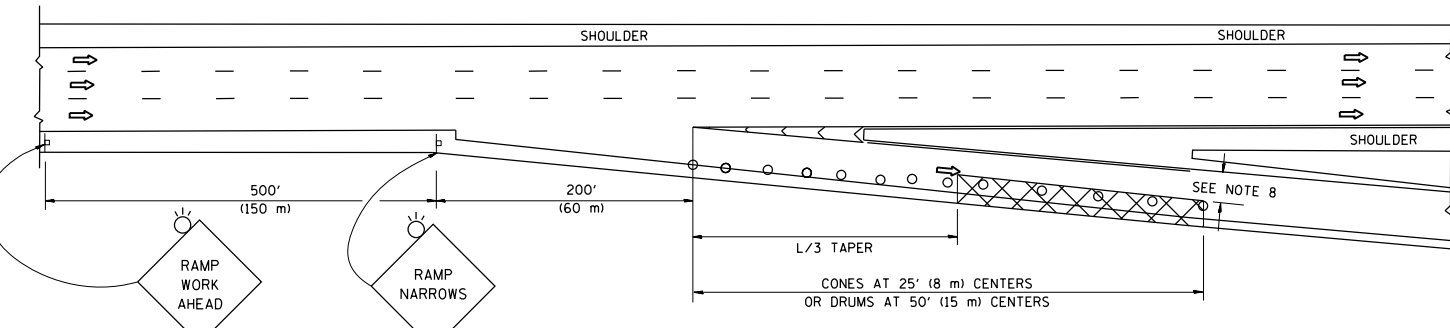
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

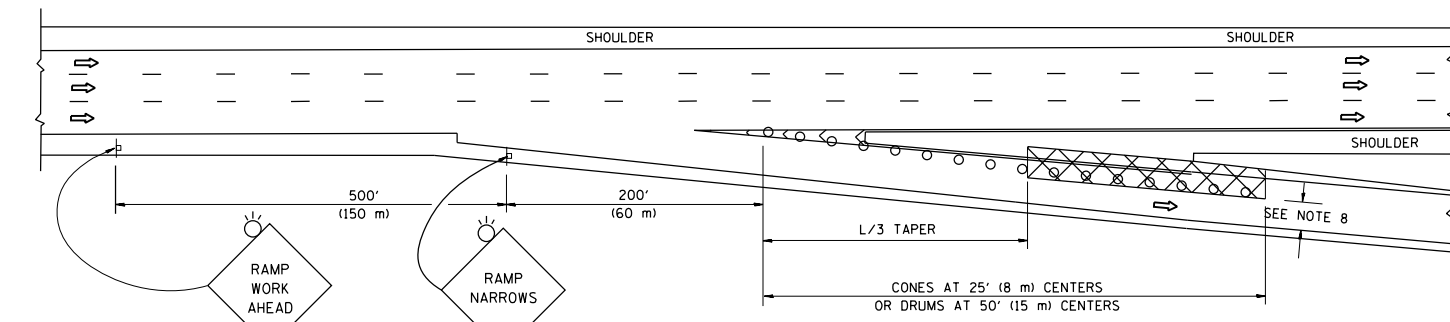
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

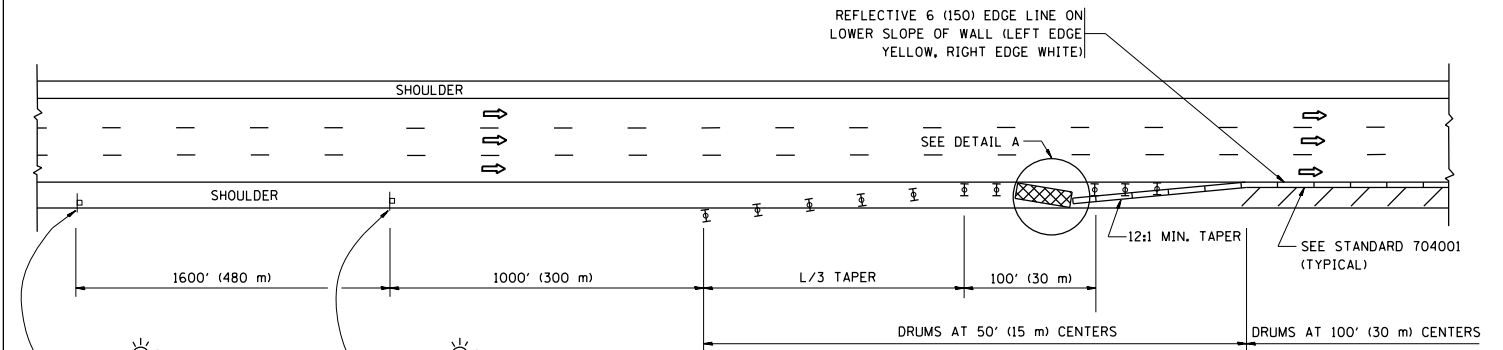
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

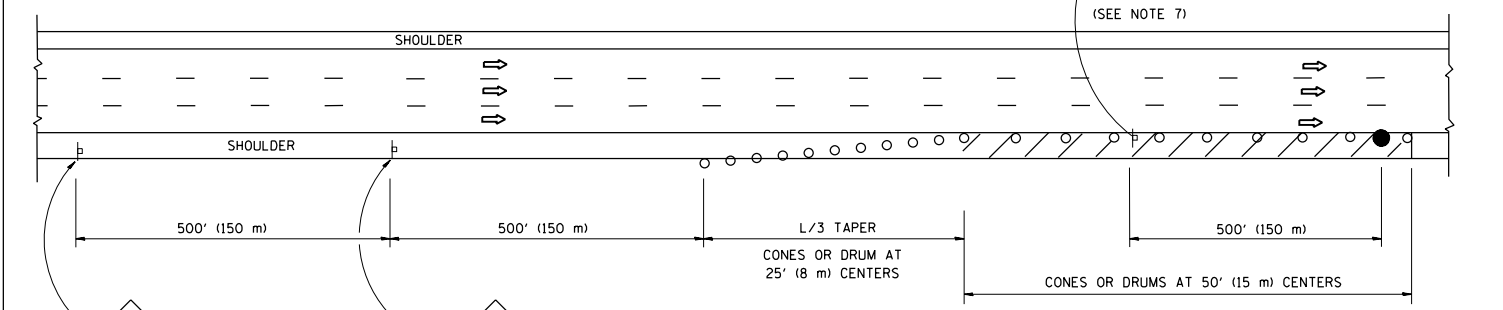
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

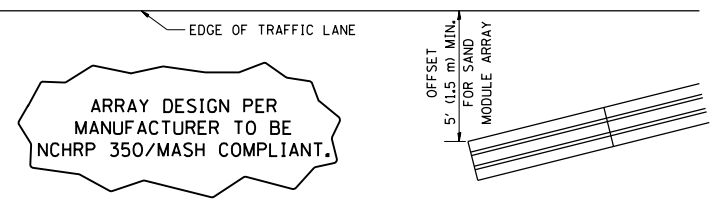


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

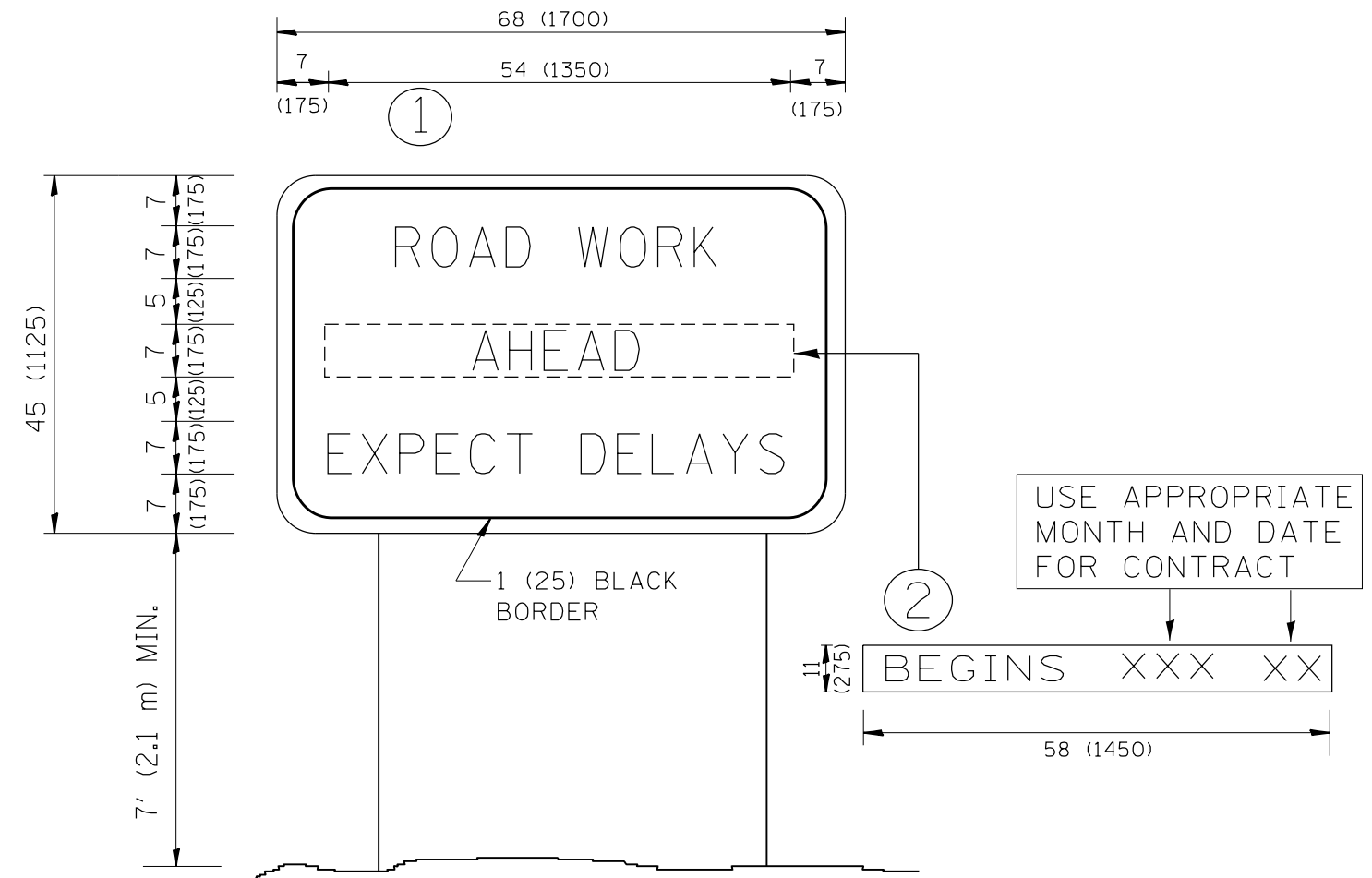


DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCRACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\dot\marenade\0316166\019Std.dgn	DRAWN - D.W.S.	REVISED - S.P.B. 01-07	344			544-RS-4	DUPAGE	53	50	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 12-09	TC-17			CONTRACT NO. 60N52				
PLOT DATE = 6/13/2014	DATE - 11-96	REVISED - M.D. 06-13	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pw\work\p1dot\marenade\d0316166\01Std.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 6/13/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

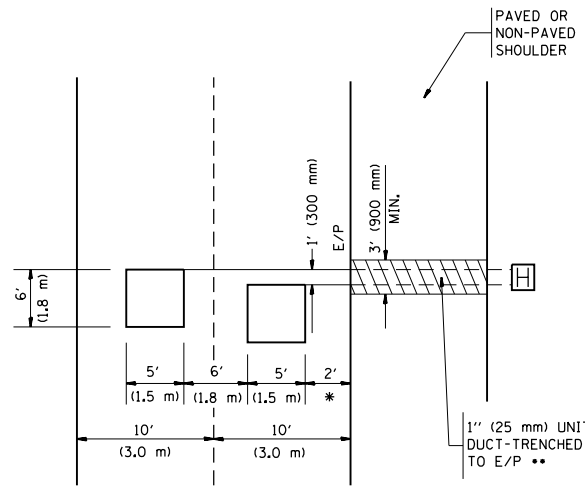
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	51
TC-22			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



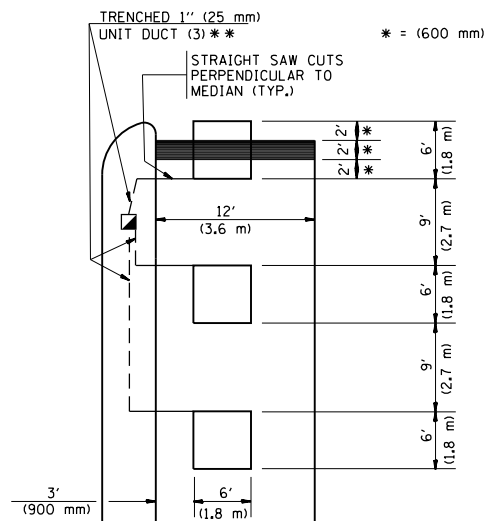
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

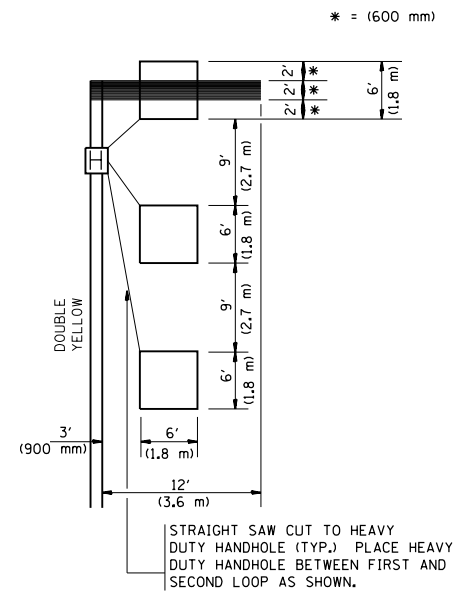


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

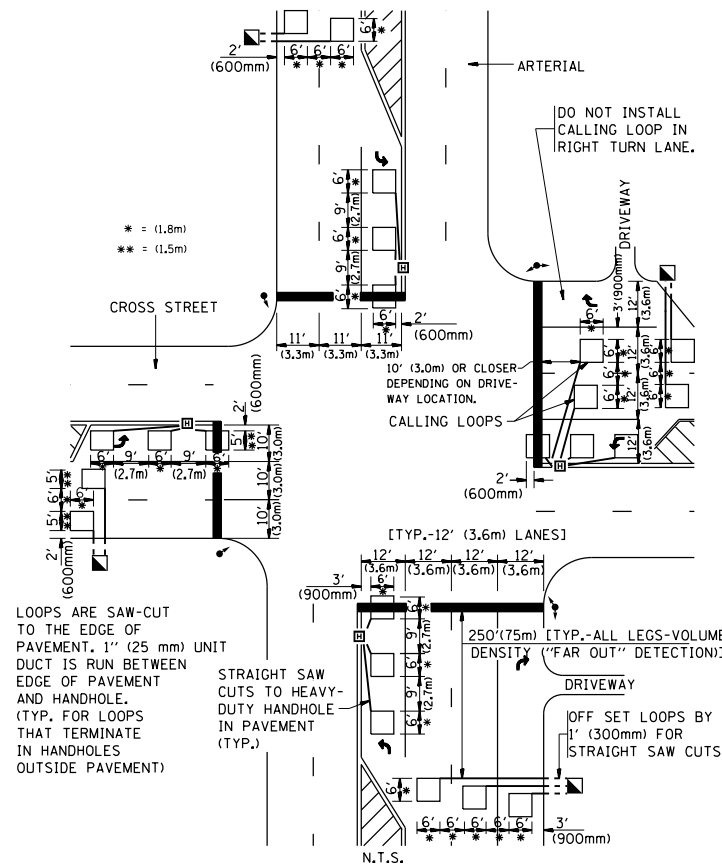
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



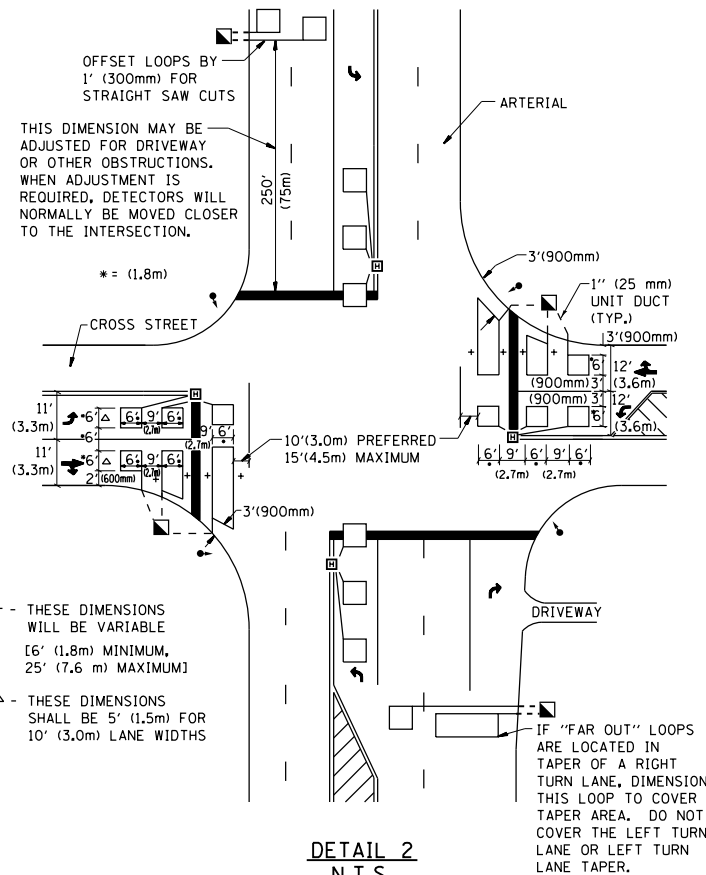
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

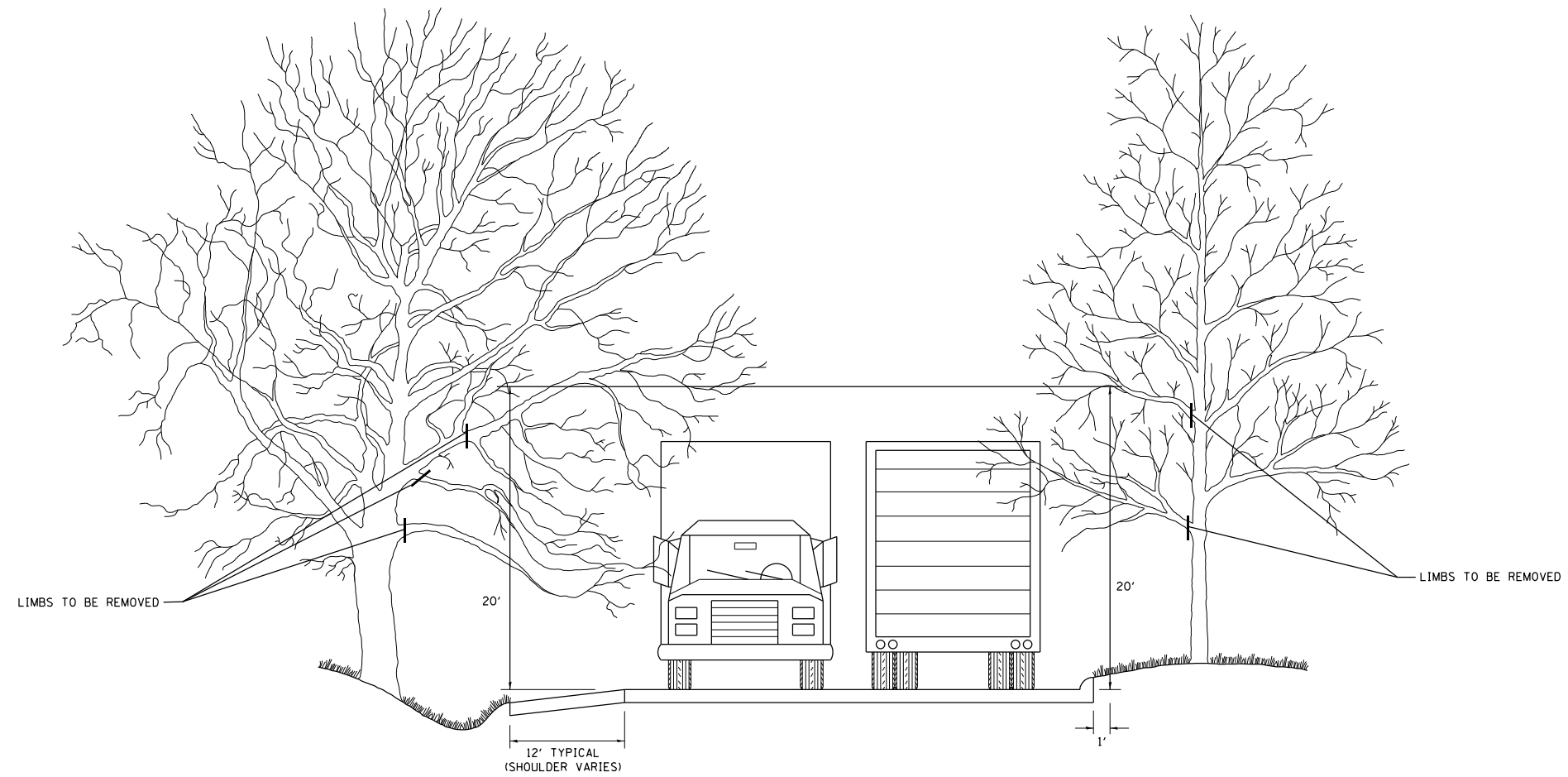
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p1dot\marenade\d0316166\Dist1Std.dgn		DRAWN -	REVISED -			344	544-RS-4	DUPAGE	53	52
		CHECKED - R.K.F.	REVISED -			TS-07		CONTRACT NO. 60N52		
		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - R. BORO 10-31-06
c:\pwork\pwork\marenade\d0316166\0195Std.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PRUNING FOR SAFETY AND
EQUIPMENT CLEARANCE**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	544-RS-4	DUPAGE	53	53
BM-20			CONTRACT NO. 60N52	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				