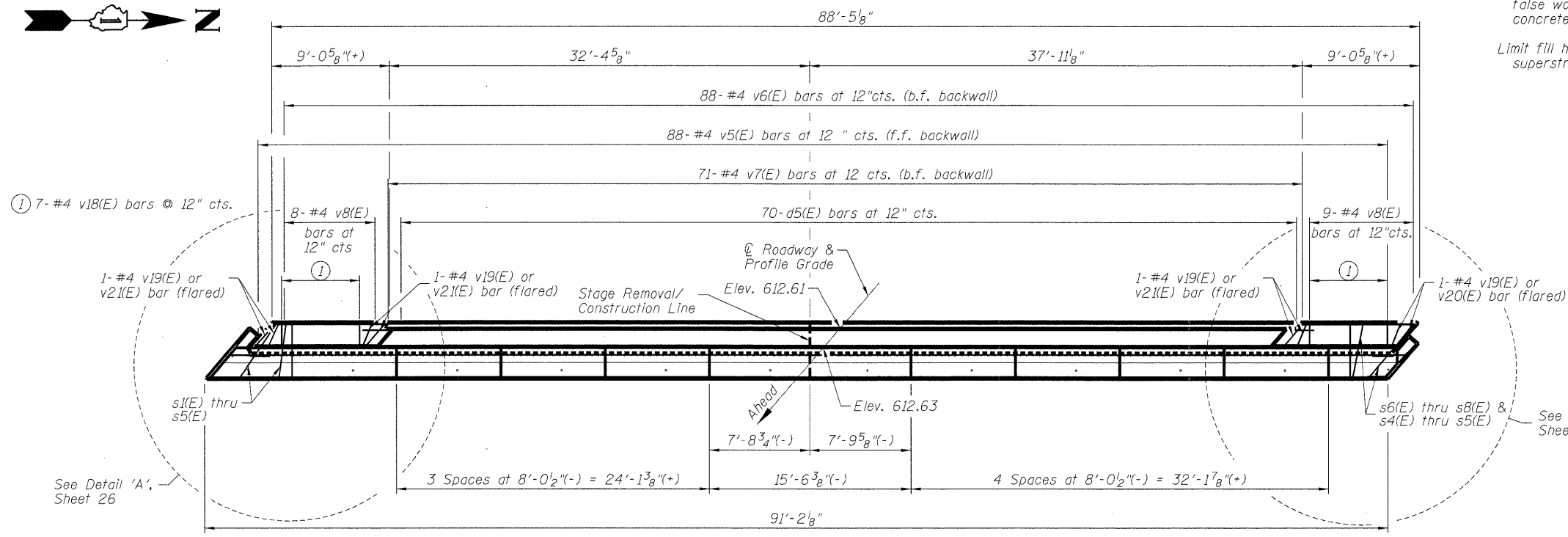


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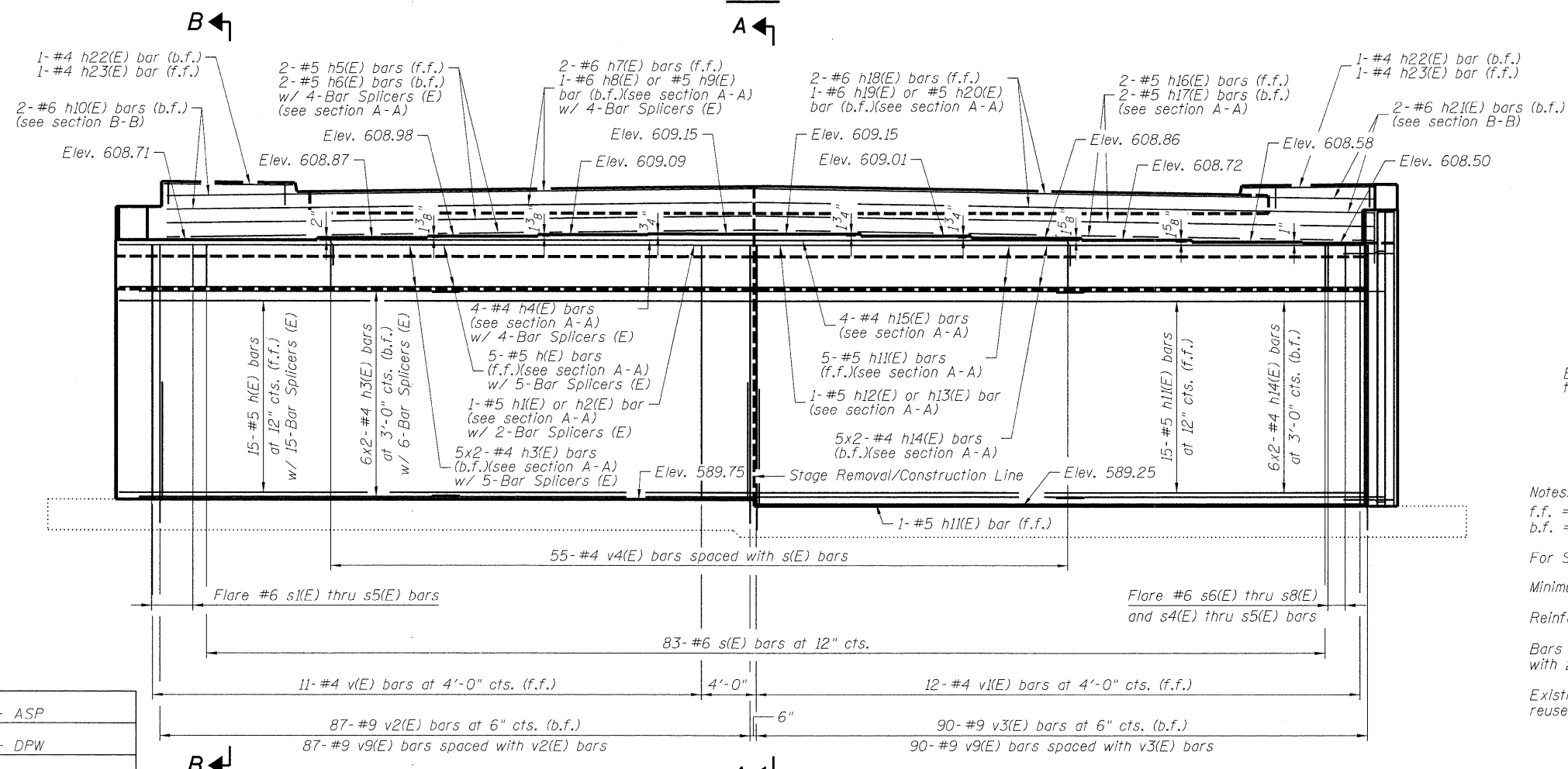
Notes:

Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.

Limit fill height to Elev. 608.25 without superstructure in place.

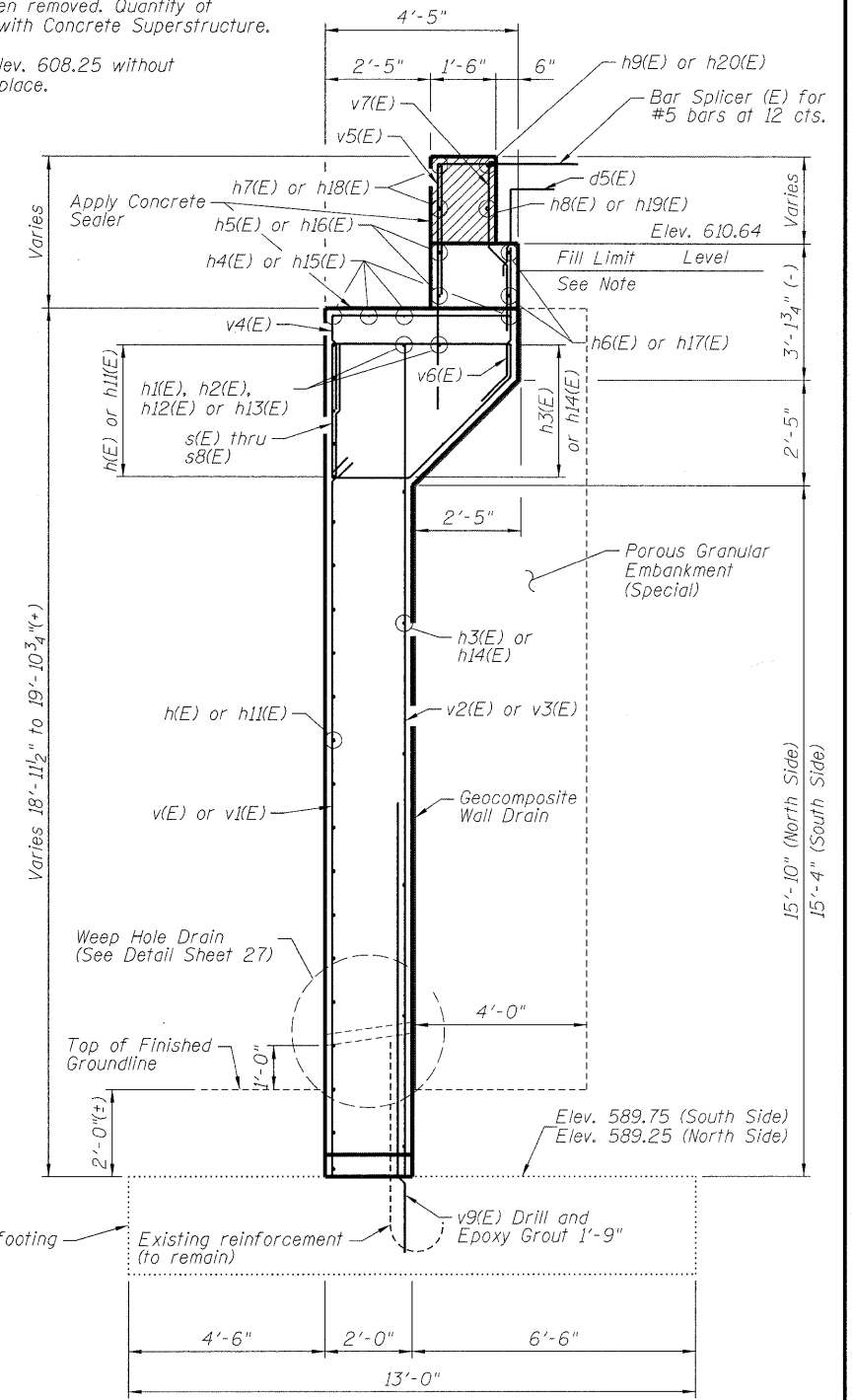


PLAN



ELEVATION

(Looking Back)



SECTION A-A

Notes:

f.f. = Front Face
b.f. = Back Face

For Section B-B, See sheet 26.

Minimum lap length for #4 bars -2'-0".

Reinforcement bars designated (E) shall be epoxy coated.

Bars indicated thus 6x2 #4 etc. indicated 6 lines of bars with 2 lengths per line.

Existing reinforcement that is to be reused shall be cleaned and straightened.

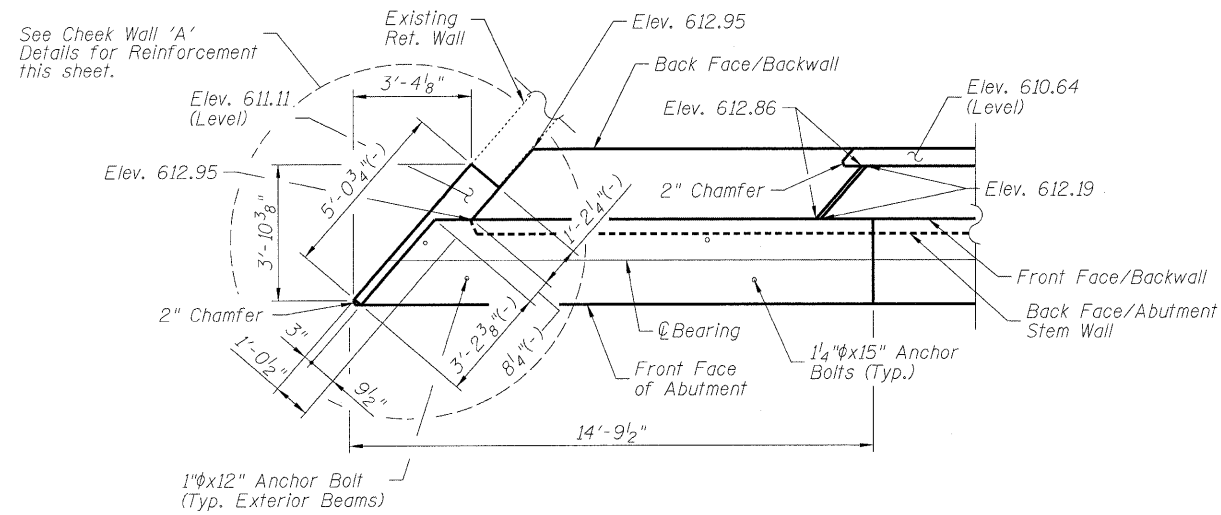
DESIGNED - ASP
CHECKED - DPW
DRAWN - BEM
CHECKED - ASP

WEST ABUTMENT I
STRUCTURE NUMBER 016-0610

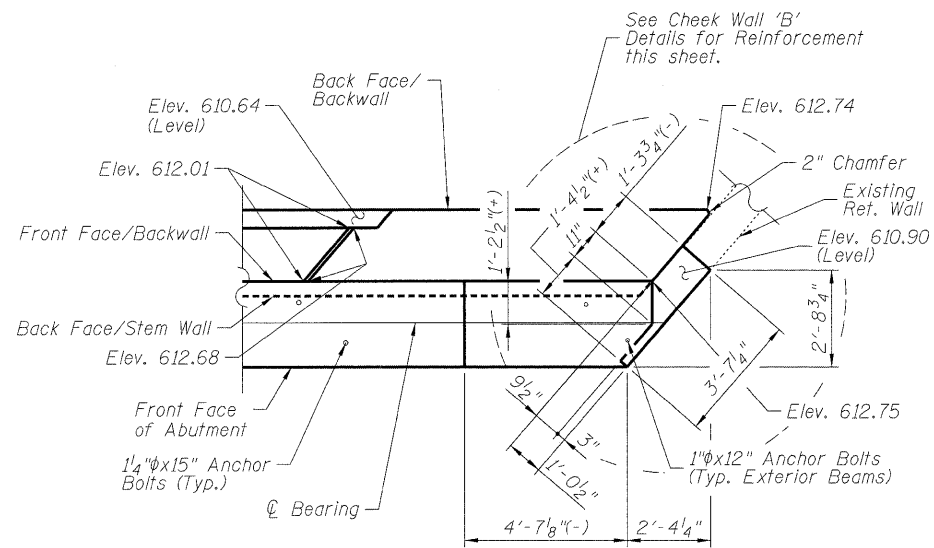
SHEET NO. 25 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 101
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

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Arlington Heights IL 60005
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Detail 'A' Plan

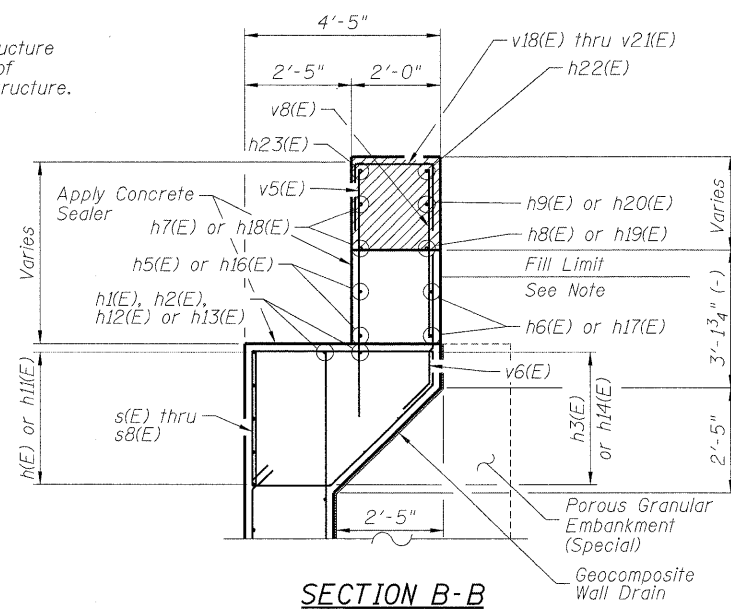


Detail 'B' Plan

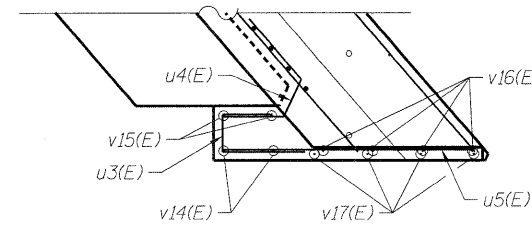
Notes:

Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.

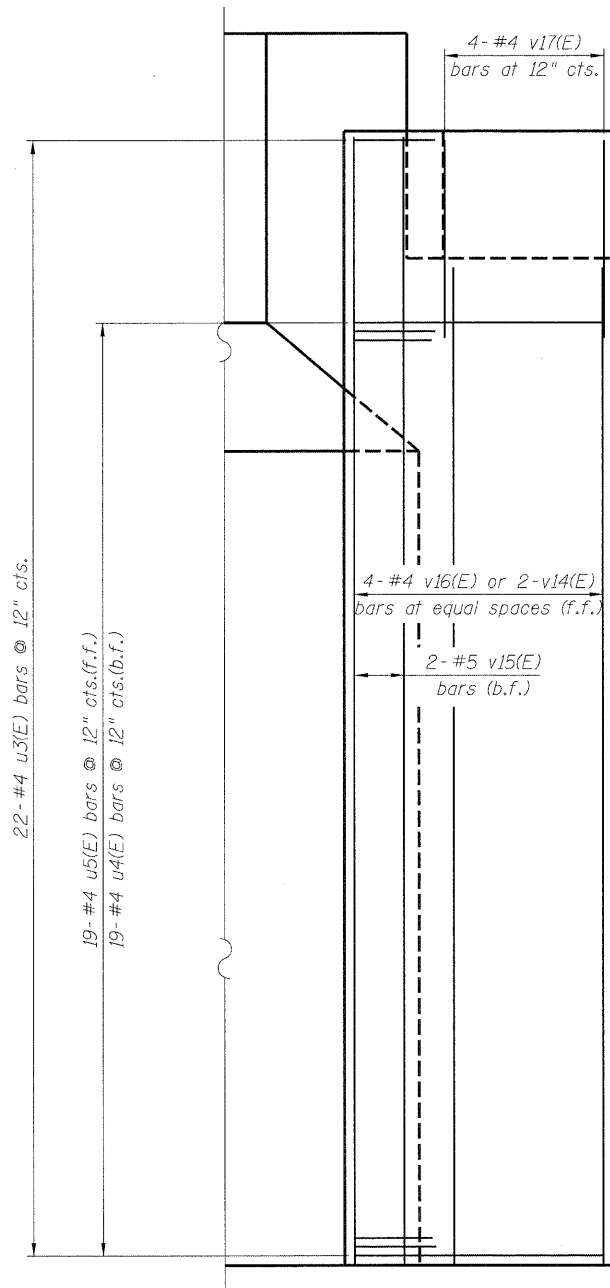
Limit fill height to Elev. 608.25 without superstructure in place.



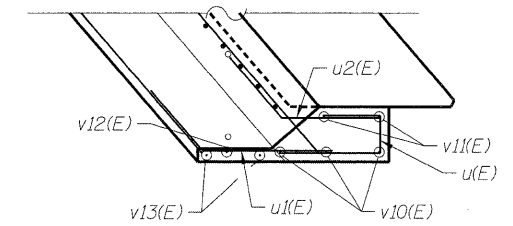
SECTION B-B



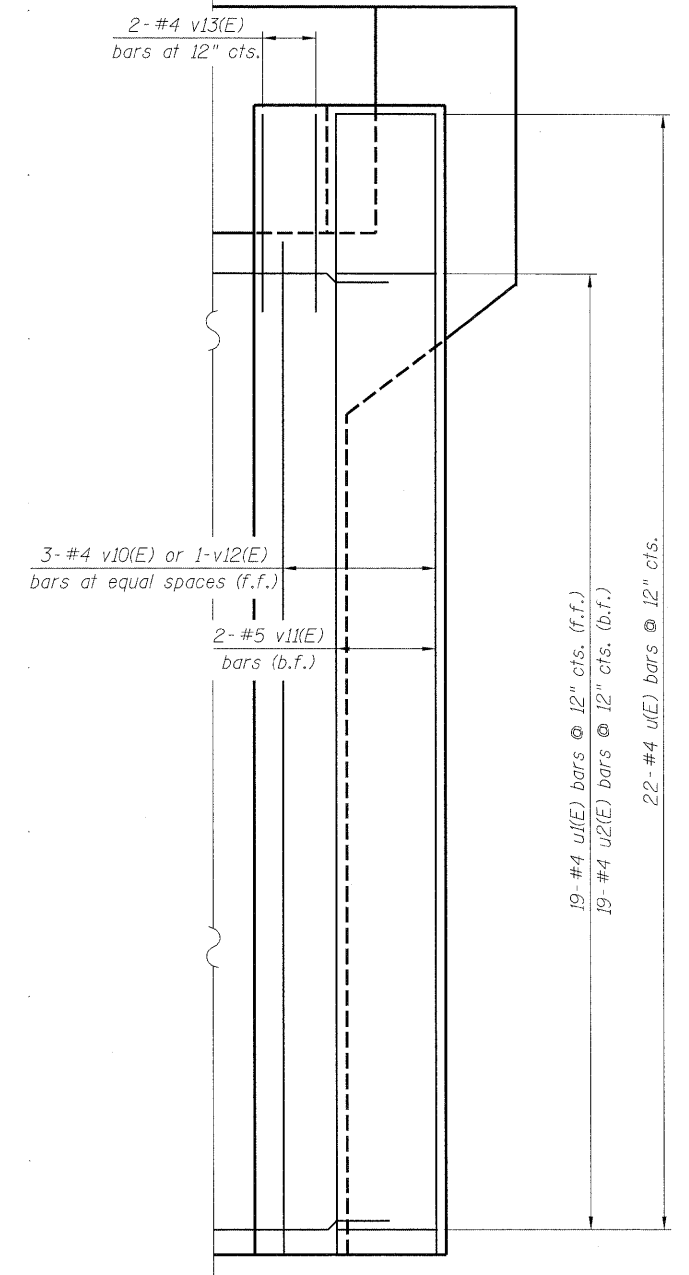
PLAN



ELEVATION 'A'



PLAN



ELEVATION 'B'

WEST ABUTMENT II
STRUCTURE NUMBER 016-0610

DESIGNED - ASP
CHECKED - DPW
DRAWN - BEM
CHECKED - ASP

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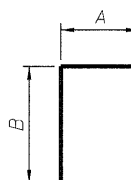
SHEET NO. 26 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 102
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

BILL OF MATERIAL

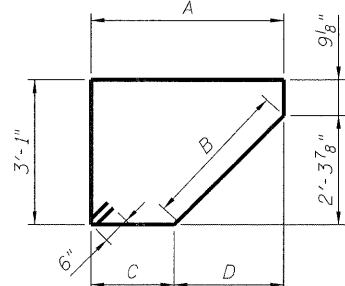
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Bar	No.	Size	Length	Shape
d5(E)	70	5	3'-0"	L
h(E)	20	5	46'-1"	—
h1(E)	1	5	44'-8"	—
h2(E)	1	5	44'-0"	—
h3(E)	22	4	23'-5"	—
h4(E)	4	4	31'-6"	—
h5(E)	2	5	42'-8"	—
h6(E)	2	5	41'-3"	—
h7(E)	2	6	42'-8"	—
h8(E)	1	6	34'-4"	—
h9(E)	1	5	34'-4"	—
h10(E)	2	6	8'-7"	—
h11(E)	21	5	44'-4"	—
h12(E)	1	5	45'-9"	—
h13(E)	1	5	46'-6"	—
h14(E)	22	4	24'-3"	—
h15(E)	4	4	23'-7"	—
h16(E)	2	5	45'-0"	—
h17(E)	2	5	46'-6"	—
h18(E)	2	6	45'-0"	—
h19(E)	1	6	38'-9"	—
h20(E)	1	5	38'-9"	—
h21(E)	2	6	8'-7"	—
h22(E)	2	4	8'-7"	—
h23(E)	2	4	9'-4"	—
s(E)	83	6	14'-0"	U
s1(E)	1	6	14'-1"	U
s2(E)	1	6	14'-4"	U
s3(E)	1	6	14'-11"	U
s4(E)	2	6	15'-7"	U
s5(E)	2	6	16'-5"	U
s6(E)	1	6	14'-2"	U
s7(E)	1	6	14'-6"	U
s8(E)	1	6	15'-1"	U
u(E)	22	4	3'-8"	L
u1(E)	19	4	4'-0"	L
u2(E)	19	4	3'-4"	L
u3(E)	22	4	3'-2"	L
u4(E)	19	4	3'-6"	L
u5(E)	19	4	4'-8"	L

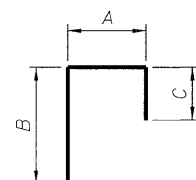
Bar	No.	Size	Length	Shape
v(E)	11	4	18'-7"	—
v1(E)	12	4	19'-1"	—
v2(E)	87	9	18'-7"	—
v3(E)	90	9	19'-1"	—
v4(E)	55	4	6'-3"	L
v5(E)	88	4	5'-8"	—
v6(E)	88	4	3'-9"	—
v7(E)	71	4	4'-2"	—
v8(E)	17	4	3'-10"	—
v9(E)	177	9	10'-4"	—
v10(E)	3	4	21'-5"	—
v11(E)	2	5	21'-5"	—
v12(E)	1	4	19'-1"	—
v13(E)	2	4	3'-9"	—
v14(E)	2	4	21'-2"	—
v15(E)	2	5	21'-2"	—
v16(E)	4	4	18'-9"	—
v17(E)	4	4	3'-9"	—
v18(E)	14	4	4'-8"	—
v19(E)	4	4	4'-9"	—
v20(E)	1	4	5'-1"	—
v21(E)	3	4	5'-2"	—
Porous Granular Embankment, Special			Cu. Yd.	214
Concrete Removal			Cu. Yd.	146.4
Structure Excavation			Cu. Yd.	669
Concrete Structures			Cu. Yd.	176.6
Concrete Superstructures			Cu. Yd.	10.0
Reinforcement Bars, Epoxy Coated			Pound	25,525
Bar Splicers			Each	45
Concrete Sealer			Sq. Ft.	540
Geocomposite Wall Drain			Sq. Yd.	188



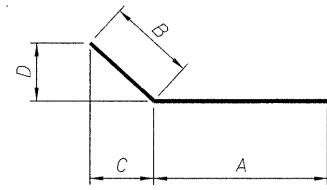
BARS d5(E) AND v4(E)



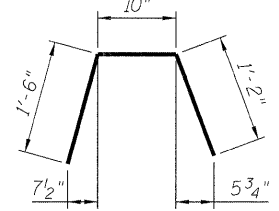
BARS s(E) THRU s8(E)



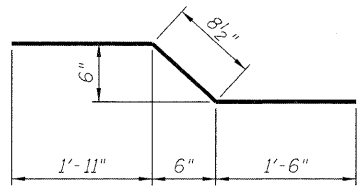
BAR u(E) and u3(E)



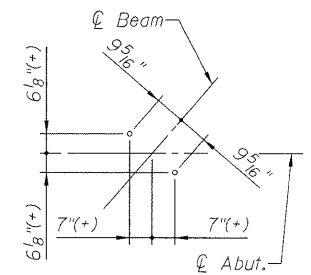
BARS u1(E) THRU u2(E) AND v6(E)



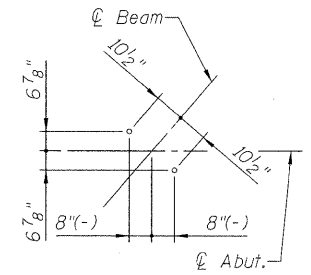
BAR u4(E)



BAR v7(E)



Exterior Beams

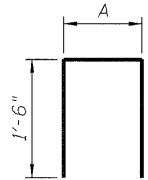


Interior Beams

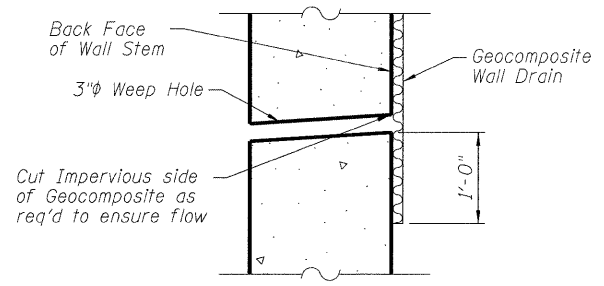
ANCHOR BOLT DETAIL

BAR DIMENSIONS

Bar	A	B	C	D
d5(E)	1'-0"	2'-0"		
s(E)	4'-1"	3'-3 3/8"	1'-9 1/8"	2'-3 7/8"
s1(E)	4'-1 3/8"	3'-3 1/2"	1'-9 3/8"	2'-4"
s2(E)	4'-3 3/8"	3'-4 1/2"	1'-10 1/8"	2'-5 1/8"
s3(E)	4'-6 3/4"	3'-5 3/4"	1'-11 5/8"	2'-7 1/8"
s4(E)	4'-11 1/4"	3'-7 7/8"	2'-1 5/8"	2'-9 5/8"
s5(E)	5'-4 3/4"	3'-10 1/8"	2'-4"	3'-0 3/4"
s6(E)	4'-1 7/8"	3'-3 3/4"	1'-9 1/2"	2'-4 3/8"
s7(E)	4'-4 3/8"	3'-4 3/4"	1'-10 5/8"	2'-5 3/4"
s8(E)	4'-8 1/8"	3'-6 1/4"	2'-0 1/4"	2'-7 7/8"
u	8 1/2"	1'-10 1/2"	1'-1"	
u1	2'-6"	1'-6"	11 3/4"	1'-1 5/8"
u2	1'-10"	1'-6"	11 3/4"	1'-1 5/8"
u3	8 1/2"	1'-6 1/2"	11"	
v4	2'-2"	4'-1"		
v6	2'-11"	10"	7 1/8"	7 1/8"
v18	1'-8"			
v19	1'-9"			
v20	2'-1"			
v21	2'-2"			



BARS v18(E) THRU v21(E)



WEEP HOLE DRAIN DETAIL

(Typical spacing = 8'-0")

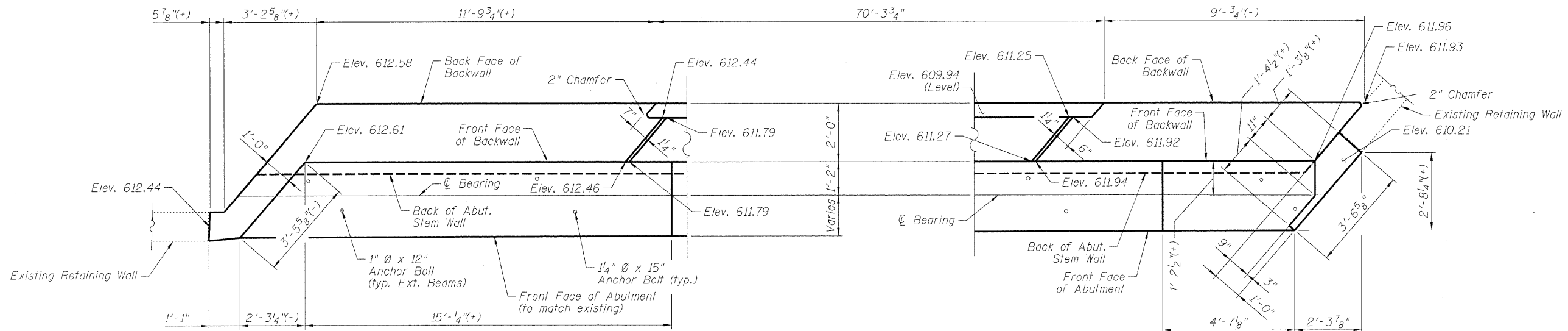
DESIGNED - ASP
CHECKED - DPW
DRAWN - BEM
CHECKED - ASP

**WEST ABUTMENT III
STRUCTURE NUMBER 016-0610**

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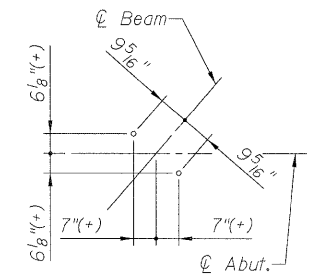
SHEET NO. 27 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	103
			CONTRACT NO. 60C18		
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

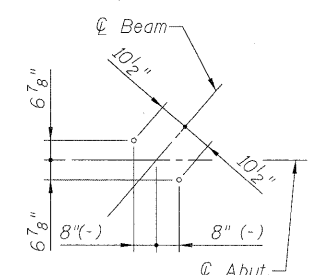


DETAIL 'A' - PLAN
EAST ABUTMENT - NORTH END

DETAIL 'B' - PLAN
EAST ABUTMENT - SOUTH END

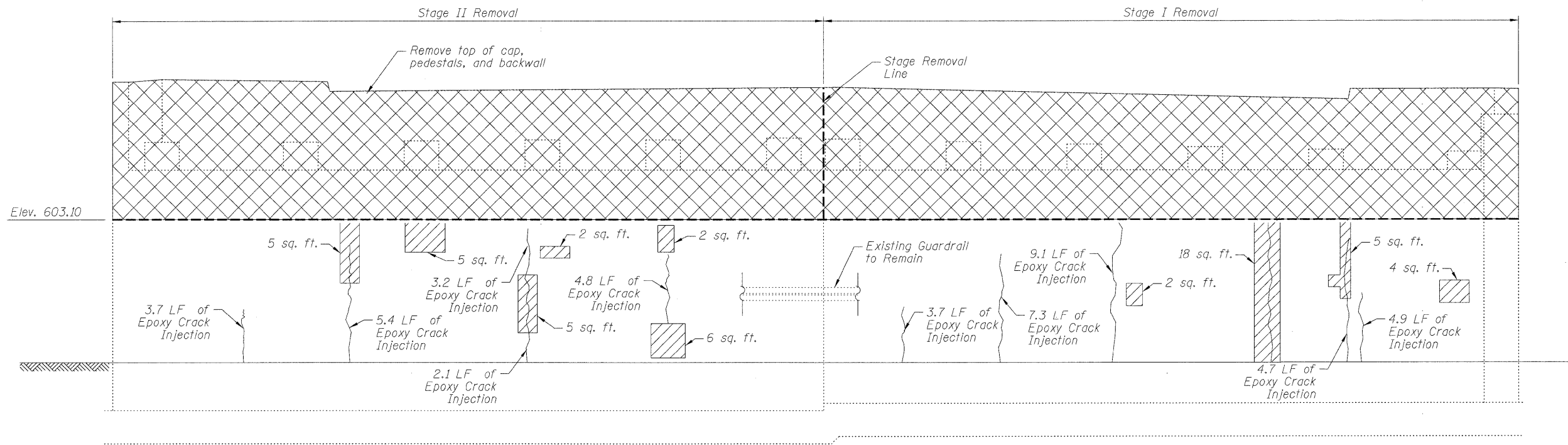


Exterior Beams

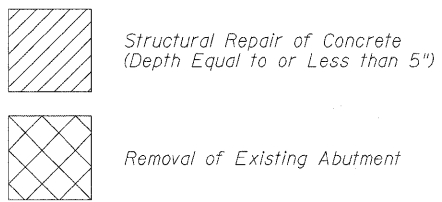


Interior Beams

ANCHOR BOLT DETAIL



ELEVATION VIEW (EXISTING)
(Looking Ahead)



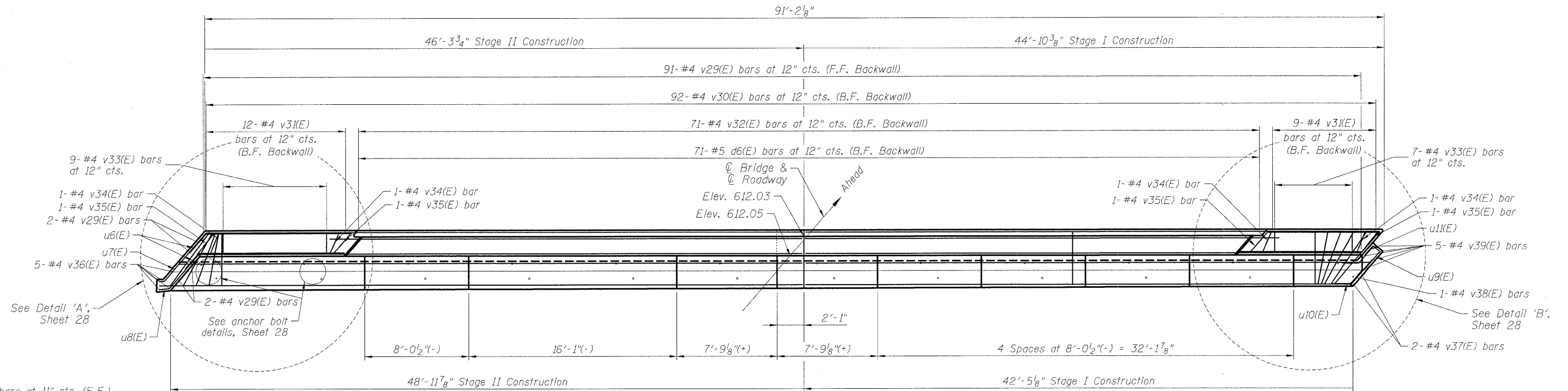
DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

EAST ABUTMENT I
STRUCTURE NUMBER 016-0610

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Arlington Heights IL 60005
(847)-640-0840

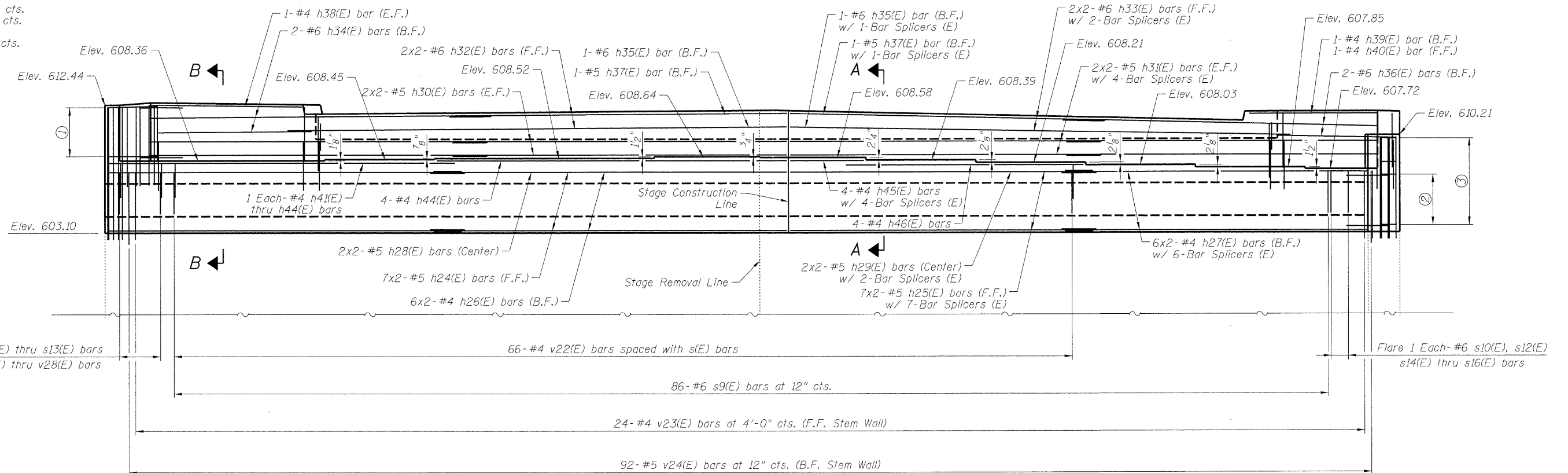
SHEET NO. 28 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 104
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



PLAN VIEW (PROPOSED)

- ① 5-#4 u6(E) bars at 11" cts. (E.F.)
5-#4 u7(E) bars at 11" cts. (B.F.)
5-#4 u8(E) bars at 11" cts. (F.F.)
- ② 5-#4 u10(E) bars at 11" cts.
5-#4 u11(E) bars at 11" cts.
- ③ 8-#4 u9(E) bars at 11" cts.



PARTIAL ELEVATION VIEW (PROPOSED)

(Looking Ahead)

Notes:

F.F. = Front Face
B.F. = Back Face
E.F. = Each Face
See Sections A-A and B-B, Sheet 30
for horizontal bar placement.

Bars indicated thus 7x2 - #5 etc.
indicates 7 lines of bars with
2 lengths per line.

DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

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SHEET NO. 29 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 105
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

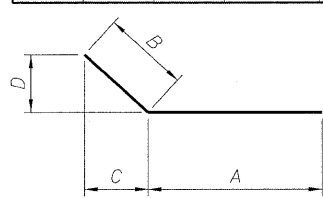
EAST ABUTMENT II
STRUCTURE NUMBER 016-0610

STATE OF ILLINOIS
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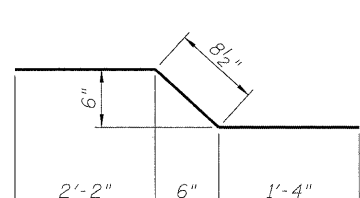
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d6(E)	71	5	3'-0"	L
h24(E)	14	5	26'-2"	—
h25(E)	14	5	22'-5"	—
h26(E)	12	4	25'-11"	—
h27(E)	12	4	22'-2"	—
h28(E)	4	5	25'-4"	—
h29(E)	4	5	23'-5"	—
h30(E)	8	5	25'-1"	—
h31(E)	8	5	23'-6"	—
h32(E)	4	6	24'-11"	—
h33(E)	4	6	23'-0"	—
h34(E)	2	6	11'-3"	—
h35(E)	2	6	36'-8"	—
h36(E)	2	6	8'-7"	—
h37(E)	2	5	36'-8"	—
h38(E)	2	4	11'-3"	—
h39(E)	1	4	8'-7"	—
h40(E)	1	4	9'-4"	—
h41(E)	1	4	21'-10"	—
h42(E)	1	4	23'-11"	—
h43(E)	1	4	24'-8"	—
h44(E)	5	4	25'-5"	—
h45(E)	4	4	5'-4"	—
h46(E)	4	4	17'-5"	—
s9(E)	86	6	14'-0"	✓
s10(E)	2	6	14'-2"	✓
s11(E)	1	6	14'-6"	✓
s12(E)	2	6	15'-1"	✓
s13(E)	1	6	15'-10"	✓
s14(E)	1	6	14'-5"	✓
s15(E)	1	6	15'-8"	✓
s16(E)	1	6	16'-6"	✓
u6(E)	10	4	3'-0"	—
u7(E)	5	4	5'-4"	—
u8(E)	5	4	4'-4"	—
u9(E)	8	4	3'-8"	—
u10(E)	5	4	4'-0"	—
u11(E)	5	4	3'-4"	—

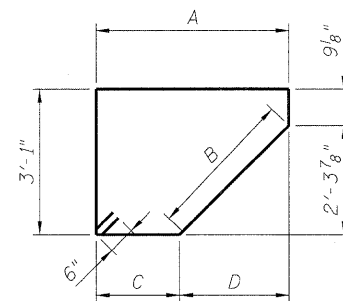
Bar	No.	Size	Length	Shape
v22(E)	66	4	6'-5"	L
v23(E)	24	4	4'-11"	—
v24(E)	92	5	5'-3"	—
v25(E)	1	4	6'-6"	L
v26(E)	1	4	6'-8"	L
v27(E)	1	4	7'-0"	L
v28(E)	1	4	7'-5"	L
v29(E)	95	4	5'-10"	—
v30(E)	92	4	3'-10"	—
v31(E)	21	4	4'-0"	—
v32(E)	71	4	4'-3"	—
v33(E)	16	4	4'-8"	—
v34(E)	4	4	4'-10"	—
v35(E)	4	4	5'-2"	—
v36(E)	5	4	9'-9"	—
v37(E)	2	4	3'-10"	—
v38(E)	1	4	4'-11"	—
v39(E)	5	4	7'-5"	—
Porous Granular Embankment, Special		Cu. Yd.	49	
Concrete Removal		Cu. Yd.	64.7	
Structure Excavation		Cu. Yd.	118	
Concrete Structures		Cu. Yd.	83.1	
Concrete Superstructures		Cu. Yd.	10.9	
Reinforcement Bars, Epoxy Coated		Pound	6,678	
Bar Splicers		Each	27	
Concrete Sealer		Sq. Ft.	536	
Epoxy Crack Injection		Foot	49	
Geocomposite Wall Drain		Sq. Yd.	65	
Structural Repair of Concrete (Depth Equal to or less than 5in.)		Sq. Ft.	54	



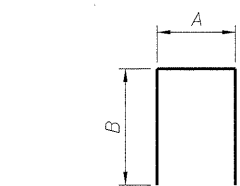
BARS u6(E) THRU u8(E)
u10(E), u11(E), AND v30(E)



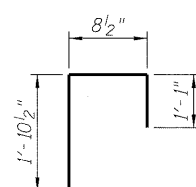
BAR v32(E)



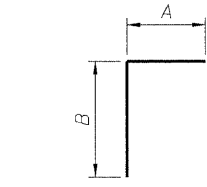
BARS s9(E) THRU s16(E)



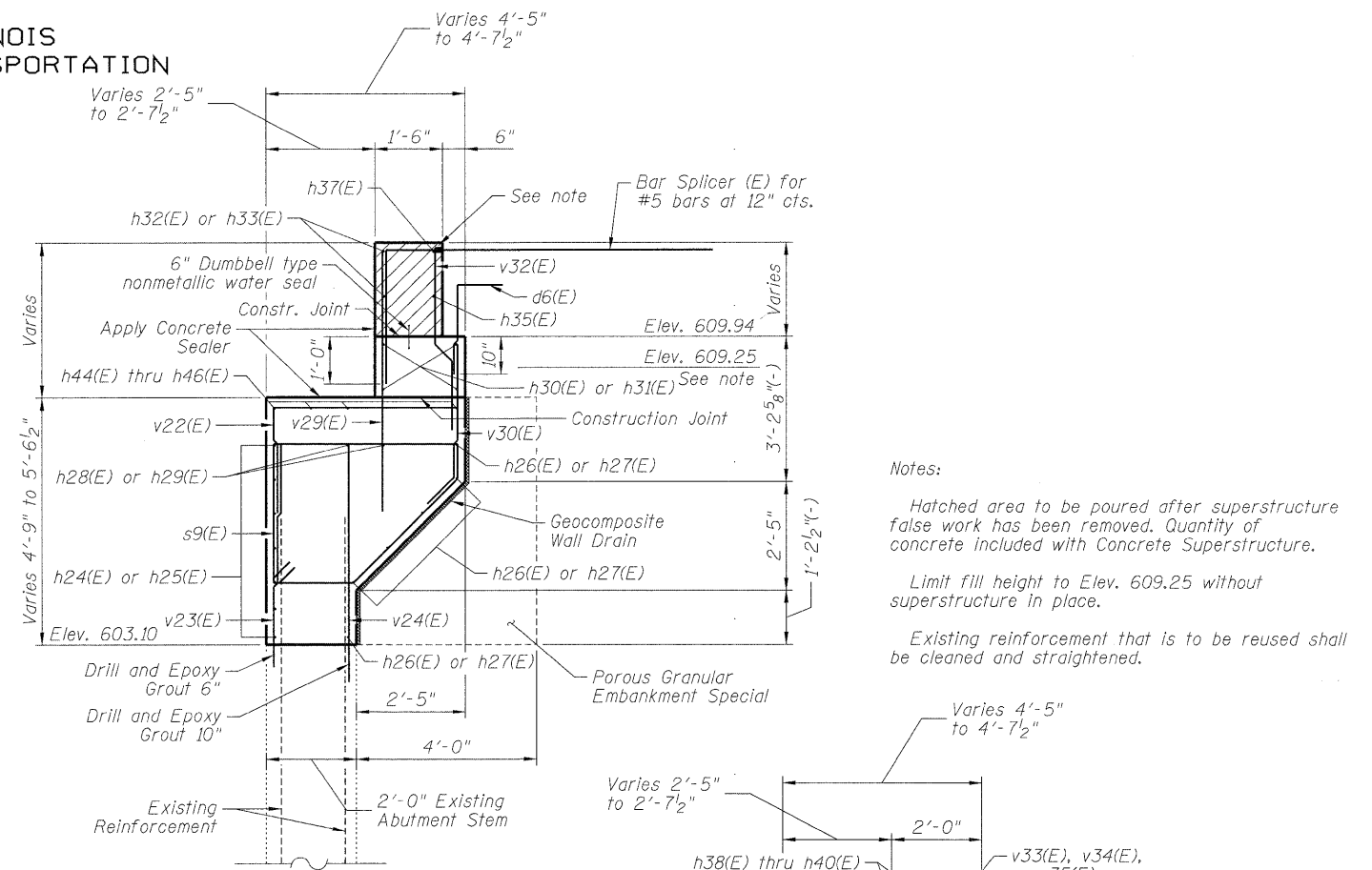
BARS v33(E) THRU v35(E)



BAR u9(E)



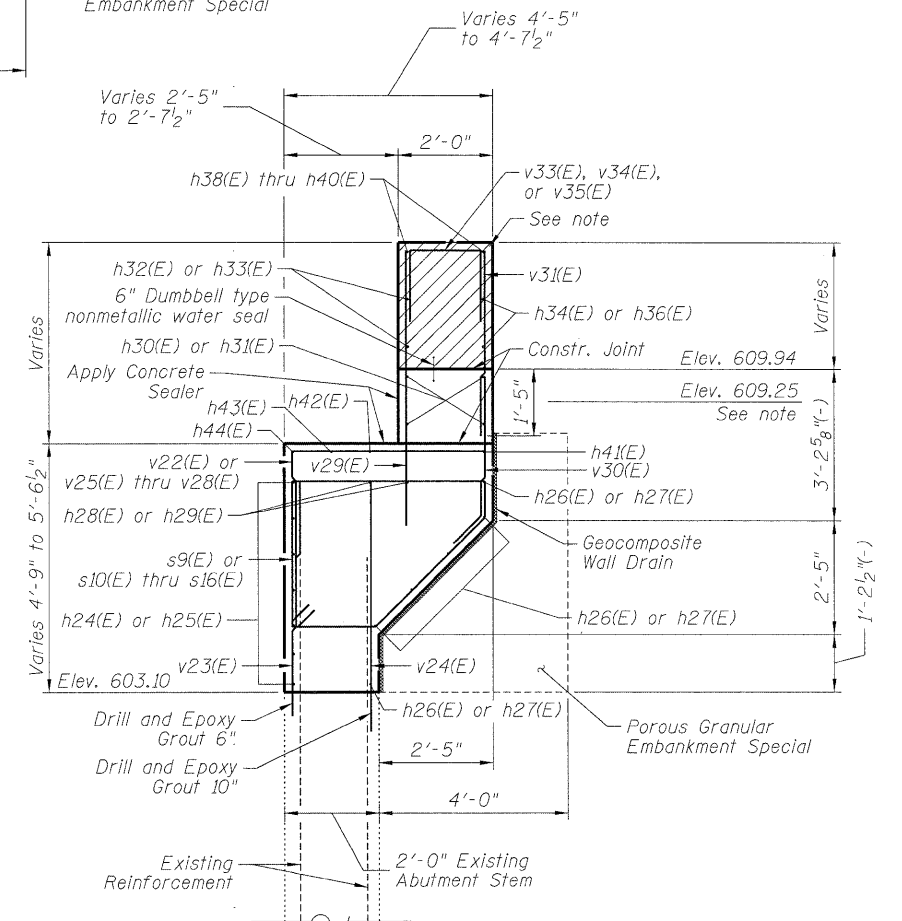
BARS d6(E) AND v22(E)
v25(E) THRU v28(E)



SECTION A-A

BAR DIMENSIONS

Bar	A	B	C	D
u6(E)	1'-6"	1'-6"	11 3/4"	1'-1 5/8"
u7(E)	4'-11 1/8"	4 7/8"	3 1/4"	3 3/4"
u8(E)	3'-6 1/8"	9 1/8"	7 1/8"	6 1/8"
u10(E)	1'-6"	2'-6"	1'-7 1/2"	1'-10 1/2"
u11(E)	1'-6"	1'-10"	1'-2 5/8"	1'-4 7/8"
v30(E)	3'-0"	10"	7 1/8"	7 1/8"
s9(E)	4'-1"	3'-3 3/8"	1'-9 1/8"	2'-3 1/8"
s10(E)	4'-1 1/8"	3'-3 3/4"	1'-9 1/2"	2'-4 3/8"
s11(E)	4'-4 1/4"	3'-4 5/8"	1'-10 1/2"	2'-5 5/8"
s12(E)	4'-8"	3'-6 1/4"	2'-1 1/8"	2'-7 3/4"
s13(E)	5'-3 1/4"	3'-8 3/8"	2'-2 1/4"	2'-10 1/2"
s14(E)	4'-3 7/8"	3'-4 1/2"	1'-10 1/2"	2'-5 3/8"
s15(E)	4'-11 5/8"	3'-7 1/8"	2'-1 3/4"	2'-9 1/8"
s16(E)	5'-4 1/8"	3'-10 1/4"	2'-4"	3'-1 1/8"
v33(E)	1'-8"	1'-6"		
v34(E)	1'-10"	1'-6"		
v35(E)	2'-2"	1'-6"		
d6(E)	1'-0"	2'-0"		
v22(E)	2'-4"	4'-1"		
v25(E)	2'-4"	4'-2"		
v26(E)	2'-4"	4'-4"		
v27(E)	2'-4"	4'-8"		
v28(E)	2'-4"	5'-1"		



SECTION B-B

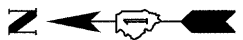
EAST ABUTMENT III
STRUCTURE NUMBER 016-0610

DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

DLZ 85 W. Algonquin Rd. Ste. 220
Arlington Heights IL 60005
DLZ Illinois, Inc. (847)-640-0840

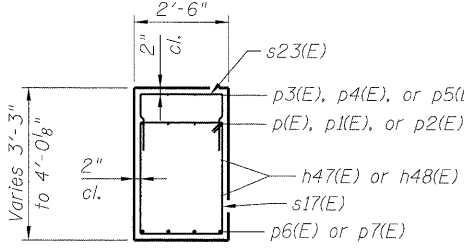
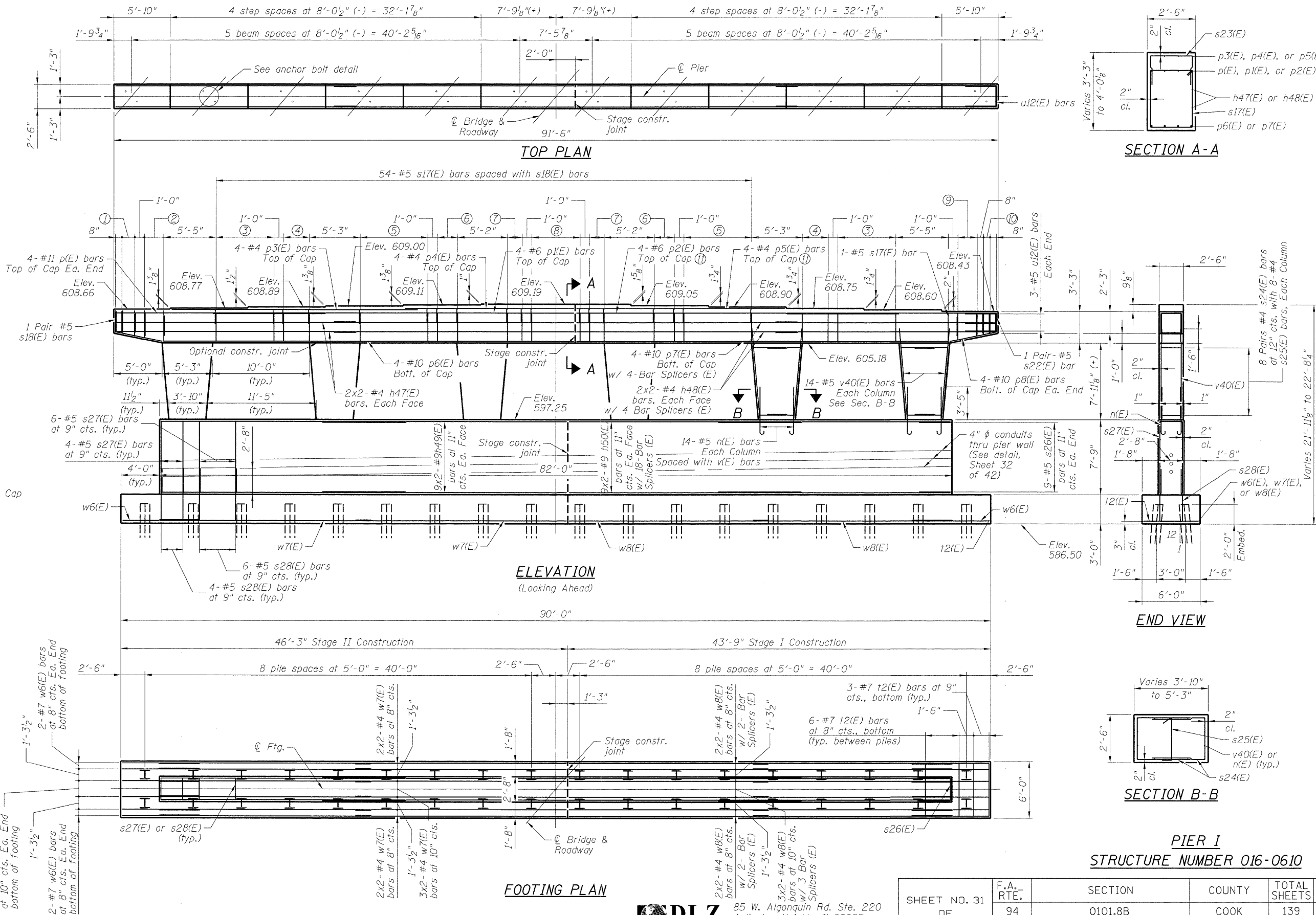
SHEET NO. 30 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 106
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

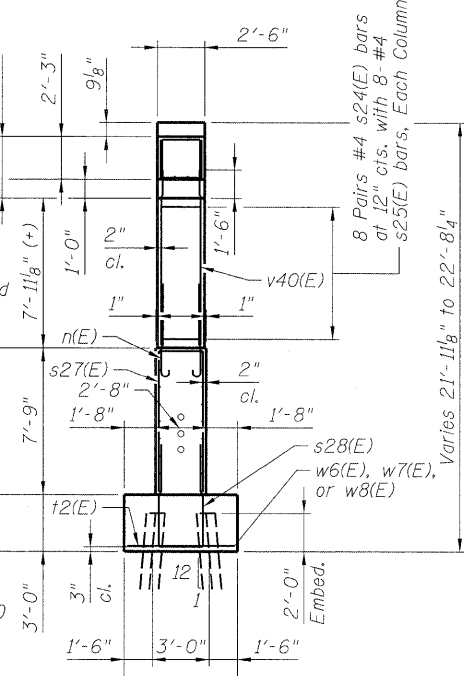


Notes:
Space reinforcement in cap to miss anchor bolts.
Pour steps monolithically with cap.
For details of piles, see sheet 39 of 42.
Concrete Sealer shall be applied to all exposed surfaces of the pier.

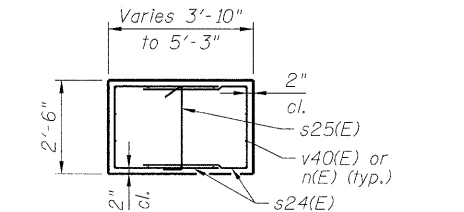
- ① 3 Pairs #5 s19(E) bars at 8" cts.
- ② 3 Pairs #5 s20(E) bars at 12" cts.
- ③ 7-#5 s17(E) bars at 12" cts.
- ④ 5-#5 s17(E) bars at 8" cts.
- ⑤ 8-#5 s17(E) bars at 12" cts.
- ⑥ 6-#5 s17(E) bars at 5" cts.
- ⑦ 4-#5 s17(E) bars at 6" cts.
- ⑧ 6-#5 s17(E) bars at 12" cts.
- ⑨ 3 Pairs #5 s21(E) bars at 12" cts.
- ⑩ 2 Pairs #5 s18(E) bars at 8" cts.
- ⑪ w/ 4-Bar Splicers (E)



SECTION A-A

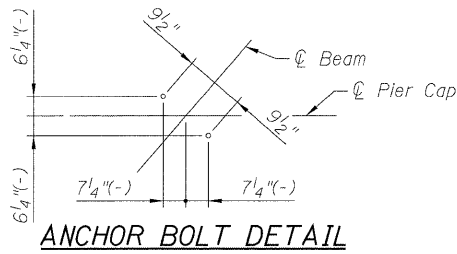


END VIEW



SECTION B-B

PIER I
STRUCTURE NUMBER 016-0610



ANCHOR BOLT DETAIL

Bars indicated thus 9x2-#9 etc., indicates 9 lines of bars with 2 lengths per line.

PILE DATA

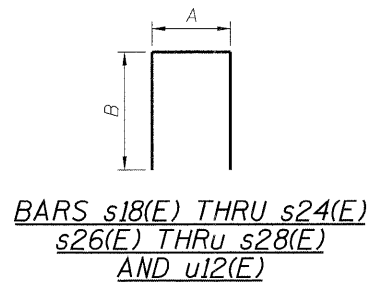
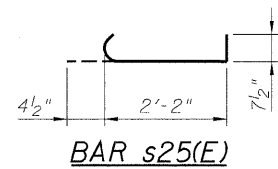
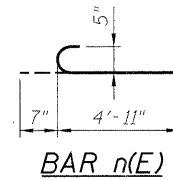
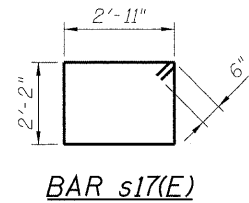
Type: HP 12 x 53
Nominal Required Bearing: 360 kips
Factored Resistance Available: 160 kips
Est. Length: 37.5 feet
No. Production Piles: 35
No. Test Piles: 1
Req. Tip Elev. = 551.20

DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

SHEET NO. 31 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	107
CONTRACT NO. 60C18					
ILLINOIS FED. AID PROJECT					

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Arlington Heights IL 60005
DLZ Illinois, Inc. (847)-640-0840

STATE OF ILLINOIS
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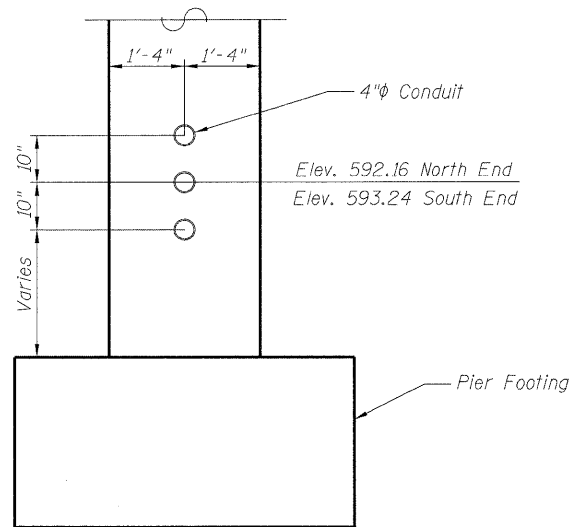
BARS s18(E) THRU s24(E)
s26(E) THRU s28(E)
AND u12(E)

A & B DIMENSIONS

Bar	A	B
s18(E)	2'-2"	2'-1"
s19(E)	2'-2"	2'-3"
s20(E)	2'-2"	2'-7"
s21(E)	2'-2"	2'-4"
s22(E)	2'-2"	1'-11"
s23(E)	2'-2"	1'-6"
s24(E)	2'-2"	3'-5"
s26(E)	2'-4"	3'-3"
s27(E)	2'-4"	7'-4"
s28(E)	2'-4"	5'-6"
u12(E)	2'-2"	3'-3"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h47(E)	8	4	24'-11"	—
h48(E)	8	4	22'-11"	—
h49(E)	36	9	25'-4"	—
h50(E)	36	9	24'-1"	—
n(E)	84	5	5'-6"	U
p(E)	8	11	21'-8"	—
p1(E)	4	6	37'-5"	—
p2(E)	4	6	33'-5"	—
p3(E)	4	4	25'-6"	—
p4(E)	4	4	17'-6"	—
p5(E)	4	4	21'-6"	—
p6(E)	4	10	42'-6"	—
p7(E)	4	10	38'-6"	—
p8(E)	8	10	5'-0"	—
s17(E)	67	5	11'-2"	U
s18(E)	6	5	6'-4"	U
s19(E)	6	5	6'-8"	U
s20(E)	6	5	7'-4"	U
s21(E)	6	5	6'-10"	U
s22(E)	2	5	6'-0"	U
s23(E)	54	5	5'-2"	U
s24(E)	96	4	9'-0"	U
s25(E)	48	4	3'-2"	U
s26(E)	18	5	8'-10"	U
s27(E)	98	5	17'-0"	U
s28(E)	98	5	13'-4"	U
t2(E)	108	7	5'-7"	—
u12(E)	6	5	8'-8"	U
v40(E)	84	5	9'-4"	—
w6(E)	14	7	8'-0"	—
w7(E)	14	4	21'-11"	—
w8(E)	14	4	20'-8"	—
Structure Excavation		Cu. Yd.	153	
Concrete Structures		Cu. Yd.	173.8	
Reinforcement Bars, Epoxy Coated		Pound	17,802	
Bar Splicers		Each	41	
Furnishing Steel Piles, HP 12 x 53		Foot	1,313	
Driving Piles		Foot	1,313	
Test Pile, Steel HP 12 x 53		Each	1	
Concrete Sealer		Sq. Ft.	2,647	



CONDUIT DETAIL

Note: Cost of conduit included in Concrete Structures.

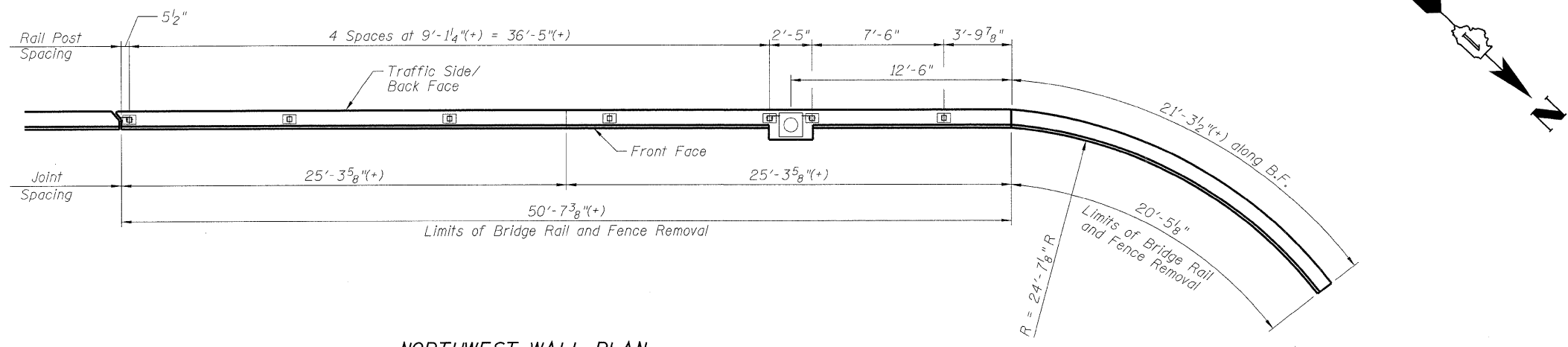
DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

PIER II
STRUCTURE NUMBER 016-0610

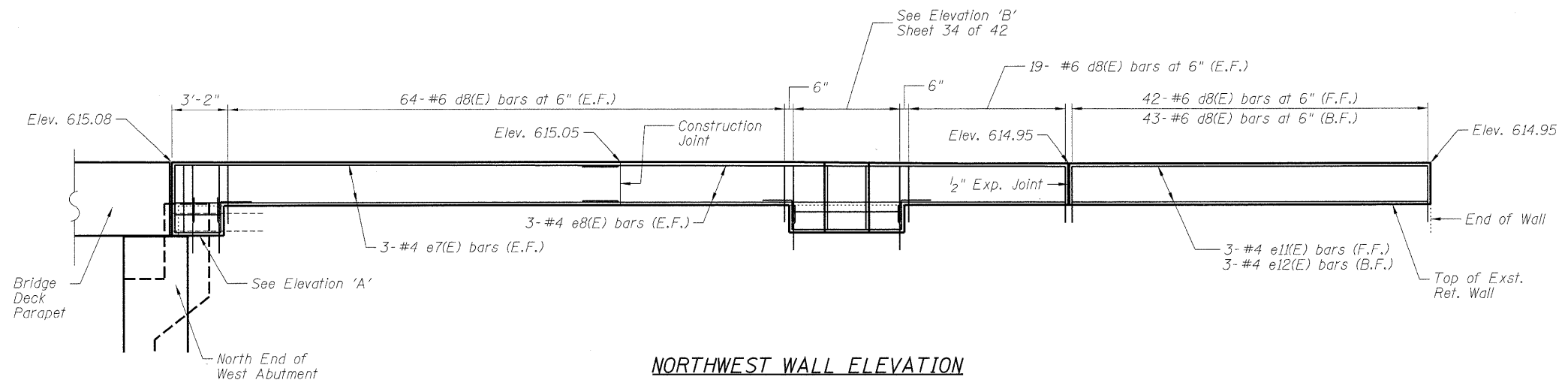
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Arlington Heights IL 60005
(847)-640-0840

SHEET NO. 32 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	108
CONTRACT NO. 60C18					
ILLINOIS FED. AID PROJECT					

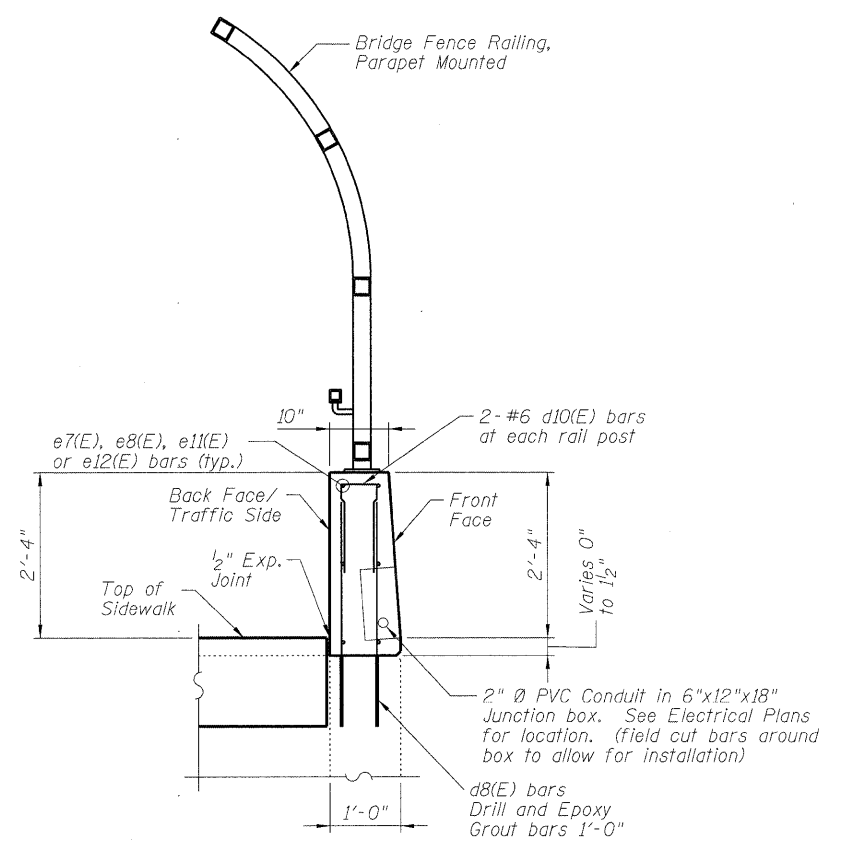
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



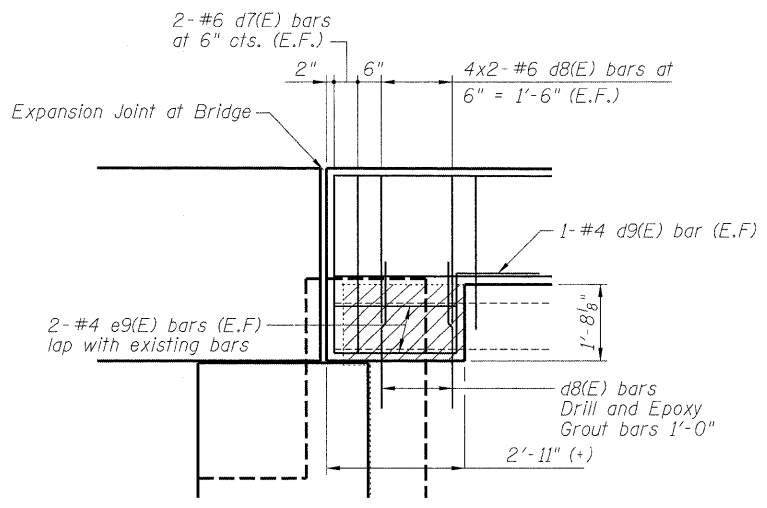
NORTHWEST WALL PLAN



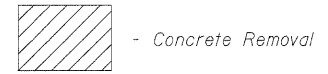
NORTHWEST WALL ELEVATION



SECTION THRU TOP OF RETAINING WALL



ELEVATION 'A'



Legend
F.F. denotes front face
B.F. denotes back face
E.F. denotes each face

DESIGNED - ASP
CHECKED - DPW
DRAWN - JMH
CHECKED - ASP

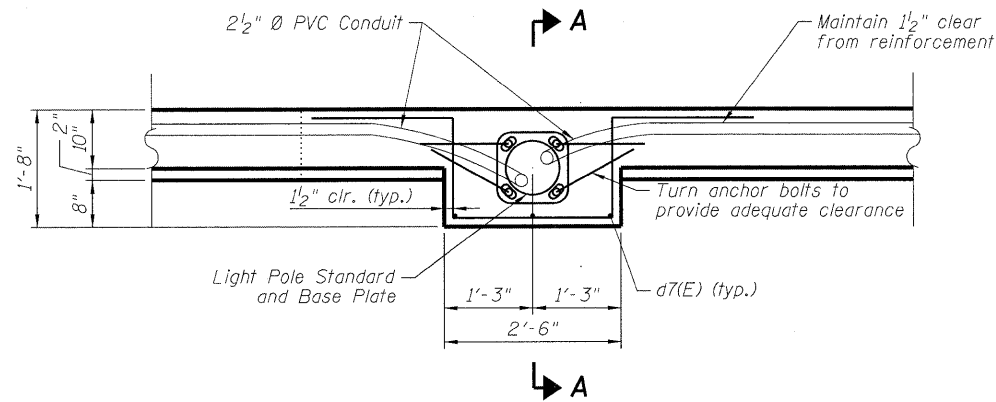
Notes:
The Cost of Existing Bridge Rail Removal and Fence Removal is included in the cost of Removal of Existing Superstructure, See Special Provision.
Bars indicated 4 x 2 - #6 etc. indicates 4 lines of bars with 2 lengths per line.
Existing reinforcement that is to be reused in the proposed wall shall be cleaned and straightened.

**NORTHWEST PARAPET WALL I
STRUCTURE NUMBER 016-0610**

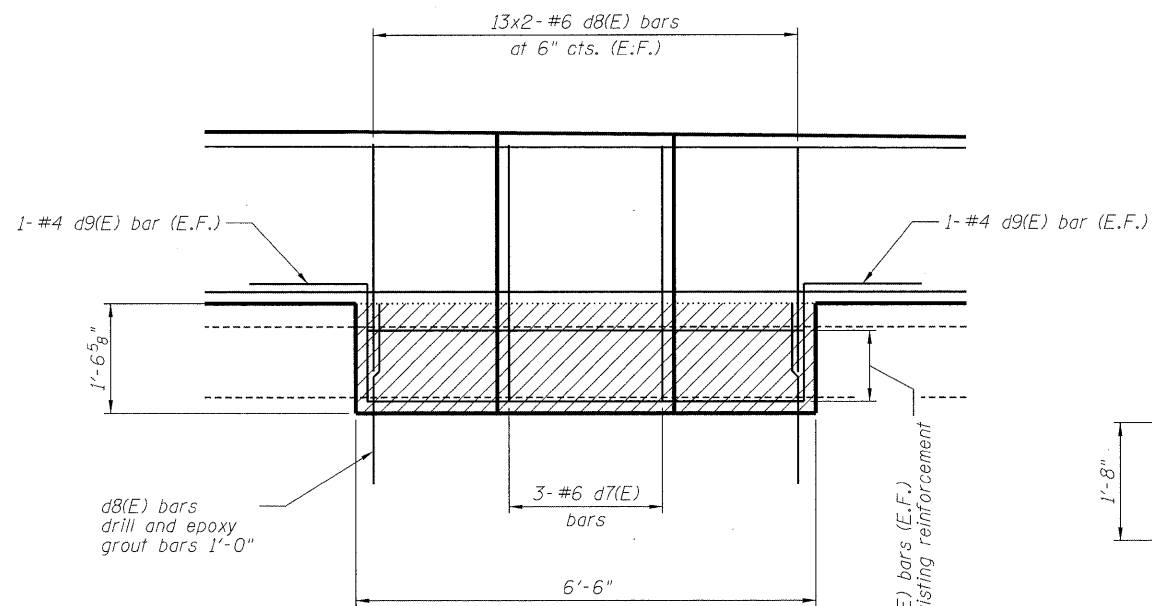
SHEET NO. 33 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 109
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

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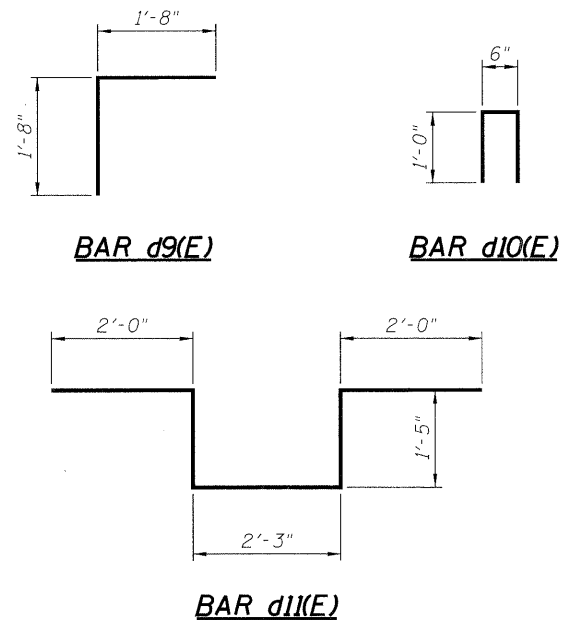
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



LIGHT POLE BLISTER PLAN

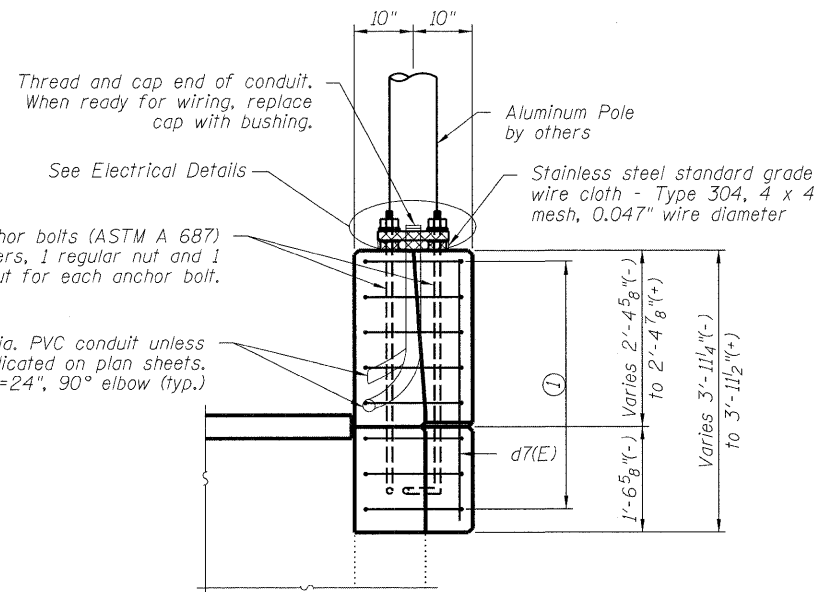


ELEVATION 'B'



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d7(E)	7	6	3'-7"	—
d8(E)	319	6	3'-2"	—
d9(E)	6	4	3'-4"	L
d10(E)	22	4	2'-6"	—
d11(E)	8	6	9'-1"	—
e7(E)	6	4	24'-11"	—
e8(E)	6	4	27'-4"	—
e9(E)	4	4	2'-6"	—
e10(E)	4	4	6'-2"	—
e11(E)	3	4	20'-4"	—
e12(E)	3	4	20'-10"	—
Concrete Removal		Cu. Yd.	0.6	
Concrete Structures		Cu. Yd.	6.7	
Bridge Fence Railing		Foot	47	
Reinforcement Bars, Epoxy Coated		Pound	2,029	
Protective Coat		Sq. Yd.	26	

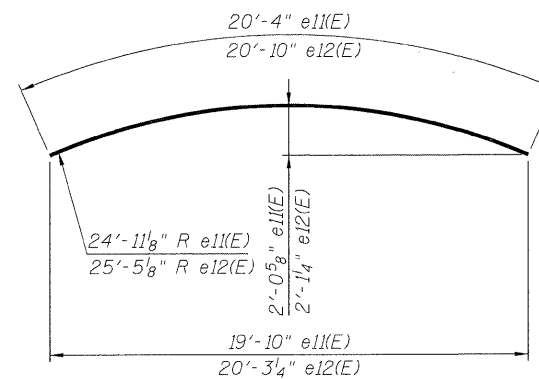


① 8-#6 d11(E) bars

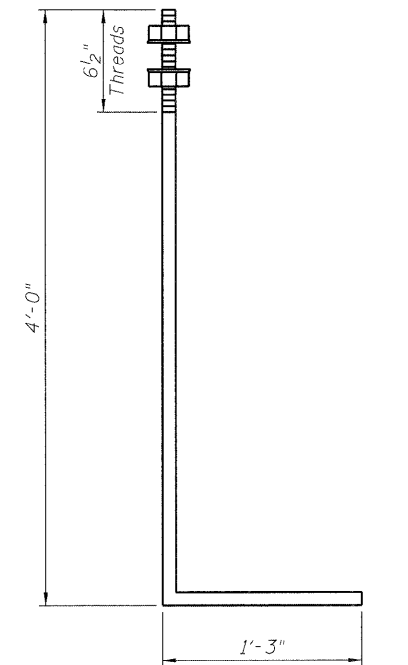
SECTION A-A

Notes:

1. Level light pole plates, using the flange nuts, prior to pouring the parapet wall. The top of the plate shall be at the same elevation as the finished concrete parapet.
2. The cost of anchor bolts, conduit, base plate and foundation is included in the cost of Concrete Structures.

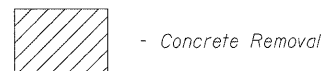


BARS e11(E) AND e12(E)



1/4" DIA. ANCHOR BOLT

DESIGNED - ASP
CHECKED - DPW
DRAWN - JMH
CHECKED - ASP

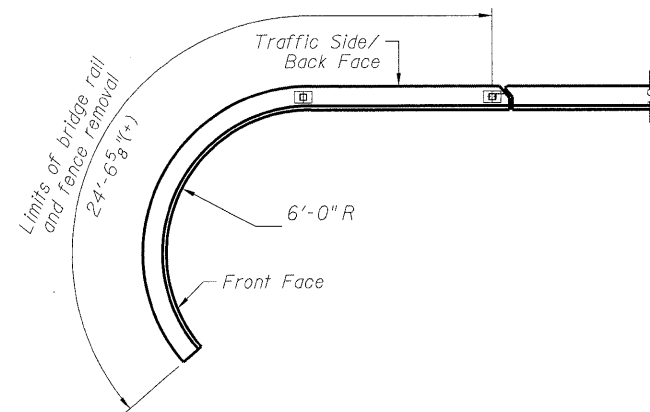


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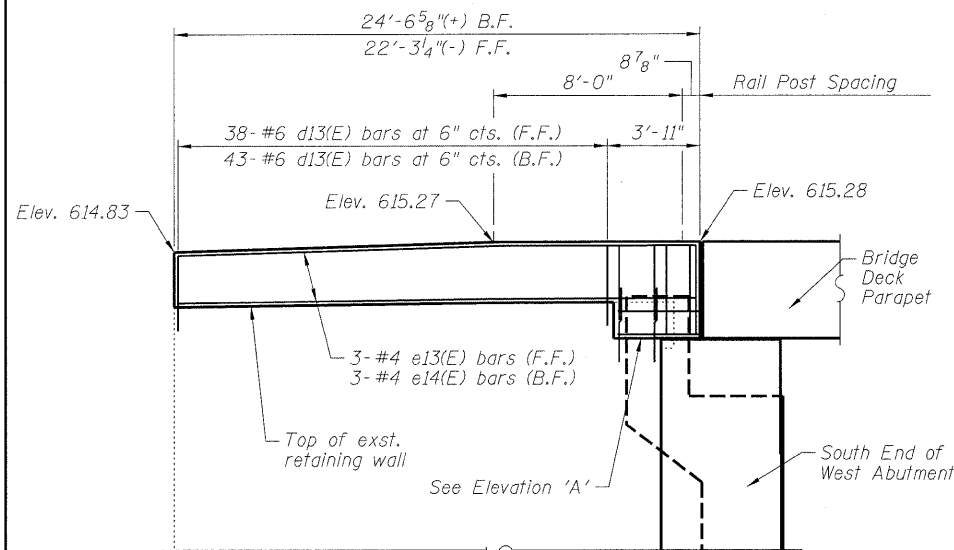
SHEET NO. 34 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	110
			CONTRACT NO. 60C18		
ILLINOIS FED. AID PROJECT					

**NORTHWEST PARAPET WALL II
STRUCTURE NUMBER 016-0610**

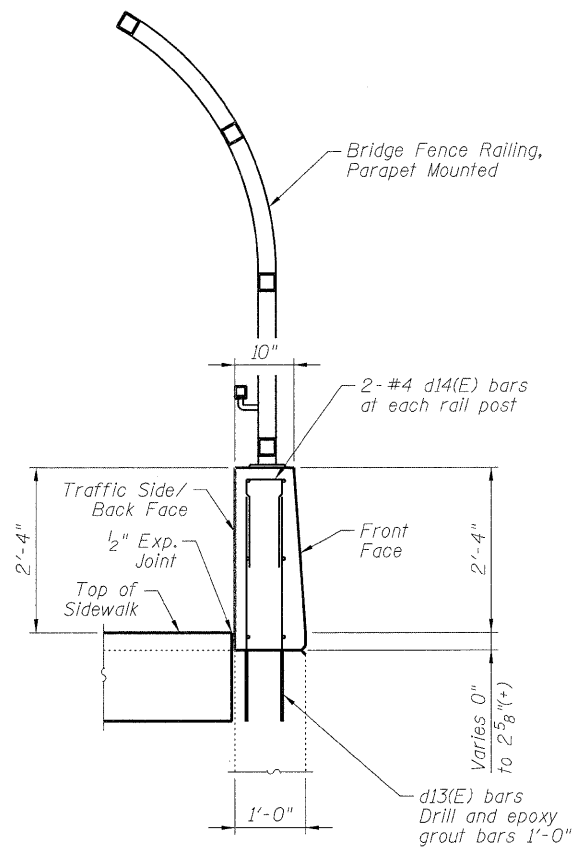
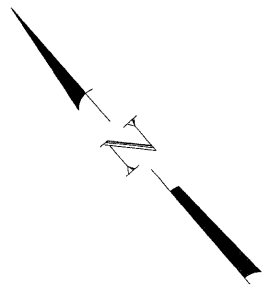
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SOUTHWEST WALL PLAN



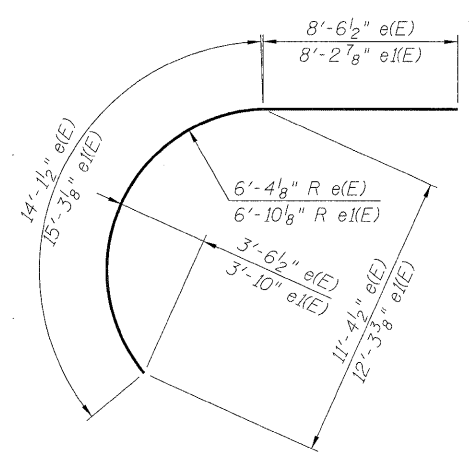
SOUTHWEST WALL ELEVATION (DEVELOPED)



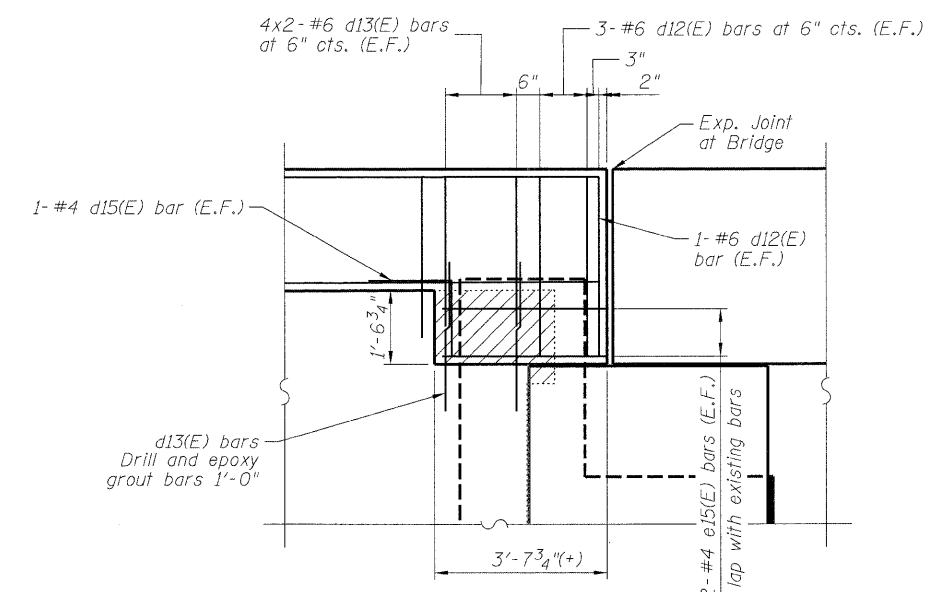
SECTION THRU TOP OF RETAINING WALL

BILL OF MATERIAL

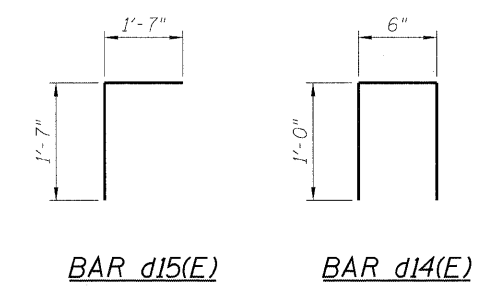
Bar	No.	Size	Length	Shape
d12(E)	8	6	3'-9"	—
d13(E)	97	6	3'-2"	—
d14(E)	4	4	2'-6"	⊓
d15(E)	2	4	3'-2"	⊓
e13(E)	3	4	22'-8"	—
e14(E)	3	4	23'-6"	—
e15(E)	4	4	3'-4"	—
Concrete Removal		Cu. Yd.	0.2	
Concrete Structures		Cu. Yd.	2.4	
Bridge Fence Railing		Foot	9	
Reinforcement Bars, Epoxy Coated		Pound	619	
Protective Coat		Sq. Yd.	9	



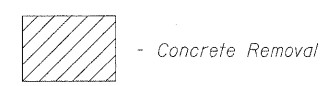
BARS e13(E) AND e14(E)



ELEVATION 'A'



BAR d15(E) BAR d14(E)



Legend
F.F. denotes front face
B.F. denotes back face
E.F. denotes each face

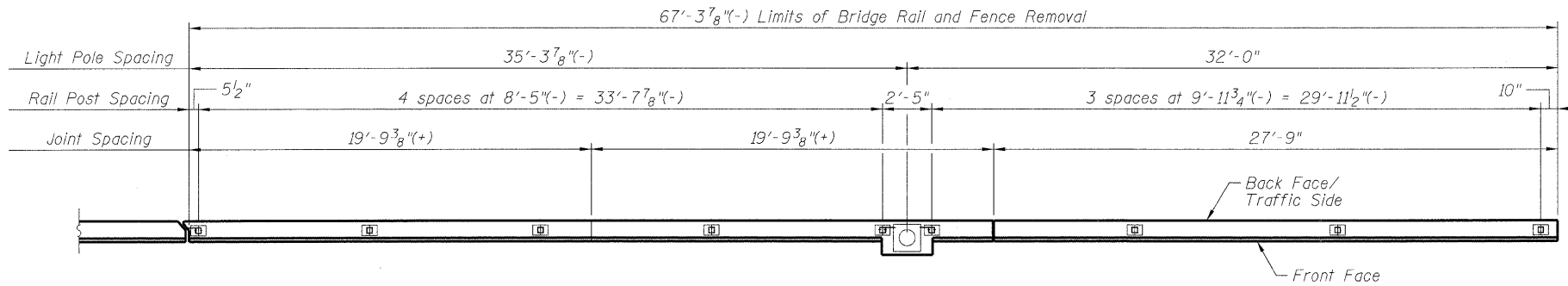
Notes:
The cost of Existing Bridge Rail Removal and Fence Removal is included in the cost of Removal of Existing Superstructure. See Special Provision.
Bars indicated thus 4x2 - #6 etc. indicates 4 lines of bars with 2 lengths per line.
Reinforcement bars that are to be reused in the proposed wall shall be cleaned and straightened.

DESIGNED - ASP
CHECKED - DPW
DRAWN - JMH
CHECKED - ASP

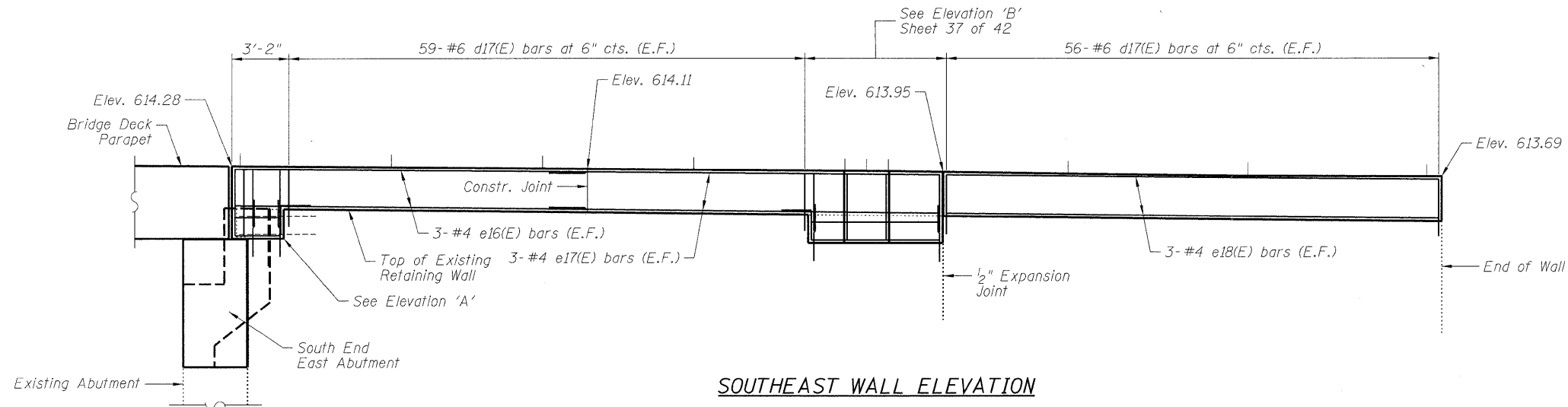
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SHEET NO. 35 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 111
	CONTRACT NO. 60C18				ILLINOIS FED. AID PROJECT

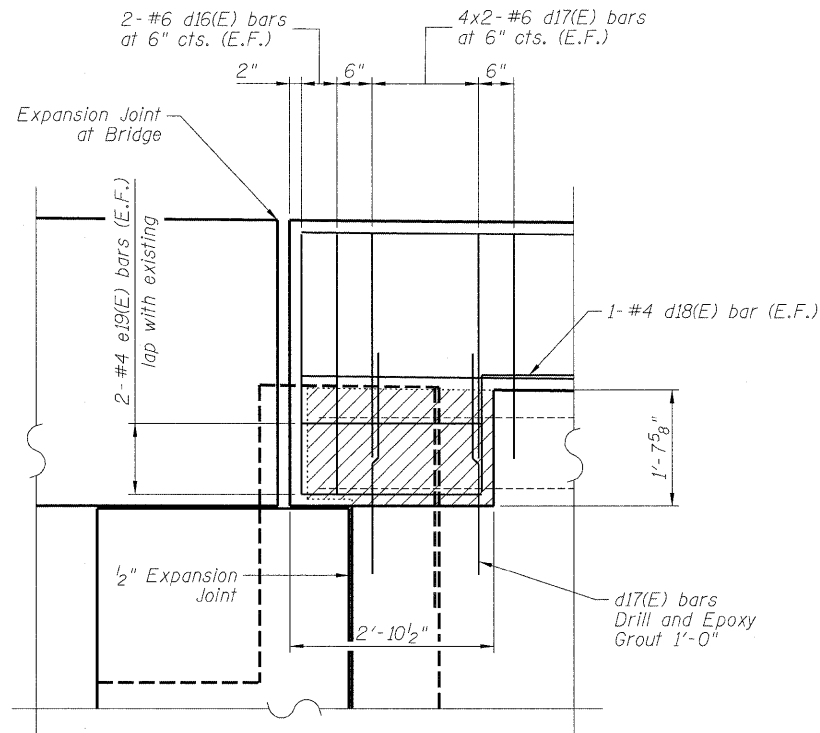
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



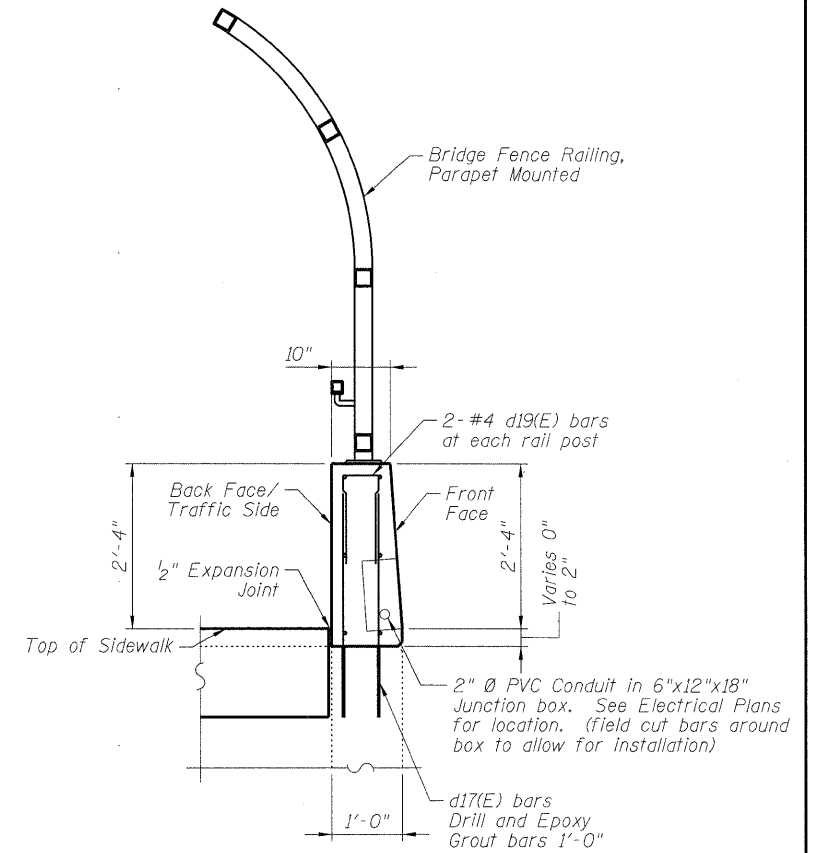
SOUTHEAST WALL PLAN



SOUTHEAST WALL ELEVATION



ELEVATION 'A'



SECTION THRU TOP OF RETAINING WALL

Notes:
Bars indicated thus 4x2 - #6 etc. indicates 4 lines of bars with 2 lengths per line.

The cost of existing Bridge Rail Removal & Fence Removal is included in the cost of Removal of Existing Superstructure. See Special Provision.

Existing reinforcement that is to be reused in the proposed wall shall be cleaned and straightened.

- Concrete Removal

Legend
F.F. denotes front face
B.F. denotes back face
E.F. denotes each face

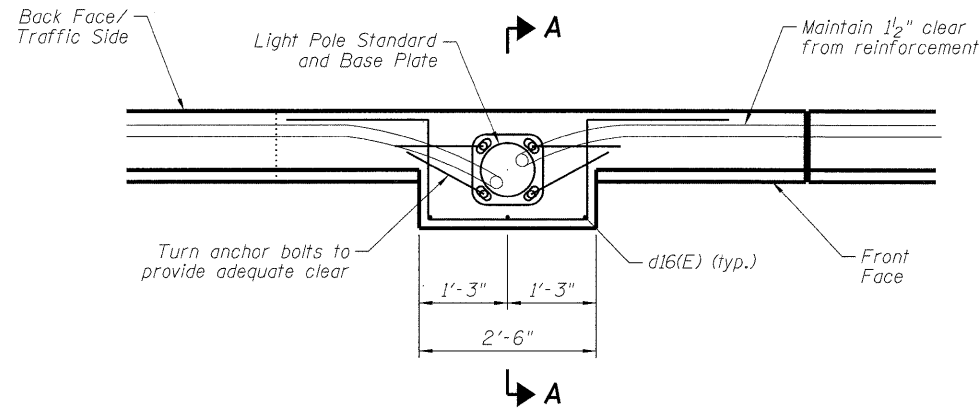
DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP

SOUTHEAST PARAPET WALL I
STRUCTURE NUMBER 016-0610

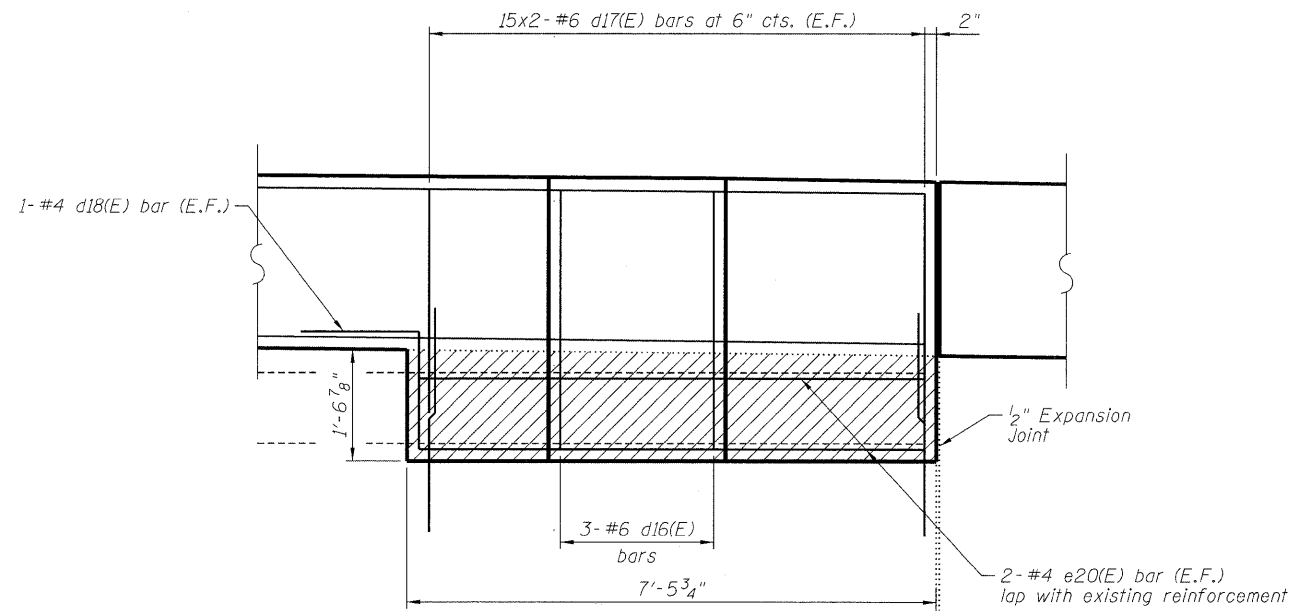
SHEET NO. 36 OF 42 SHEETS	F.A. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 112
	CONTRACT NO. 60C18				
ILLINOIS FED. AID PROJECT					

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DEPARTMENT OF TRANSPORTATION



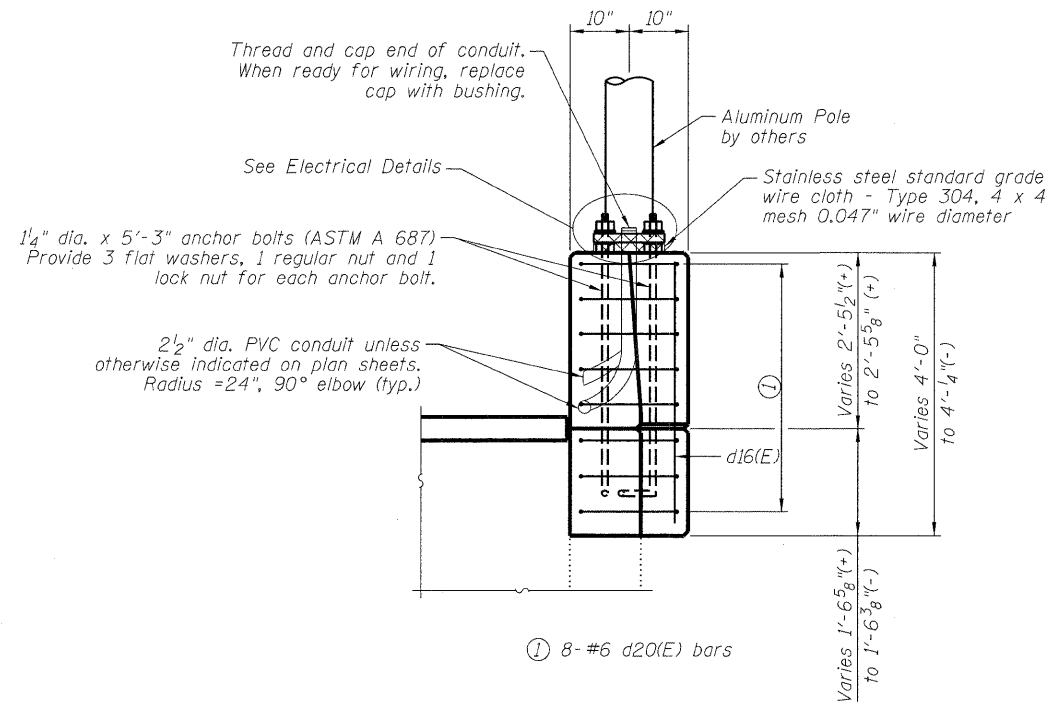
LIGHT POLE BLISTER PLAN



ELEVATION 'B'

- Concrete Removal

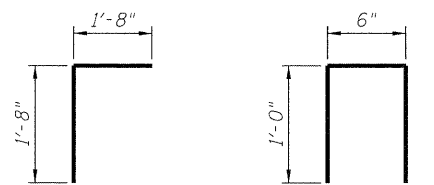
DESIGNED - ASP
CHECKED - DPW
DRAWN - LNB
CHECKED - ASP



SECTION A-A

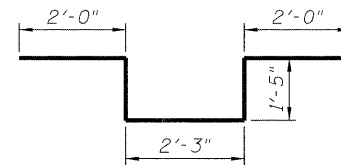
Notes:

1. Level light pole plates, using the flange nuts, prior to pouring the parapet wall. The top of the plate shall be at the same elevation as the finished concrete parapet.
2. The cost of anchor bolts, conduit, base plate and foundation is included in the cost of Concrete Superstructure.



BAR d18(E)

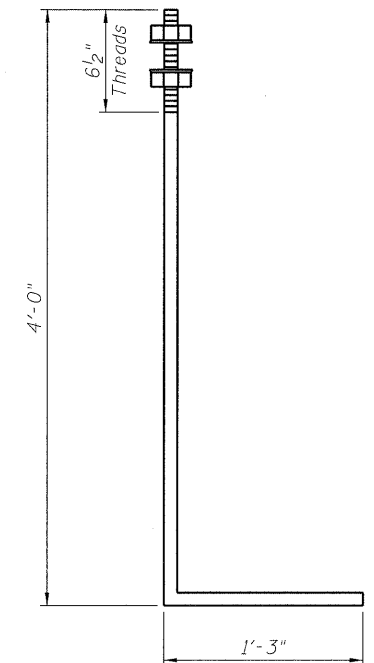
BAR d19(E)



BAR d20(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d16(E)	7	6	3'-7"	—
d17(E)	306	6	3'-2"	—
d18(E)	4	4	3'-4"	—
d19(E)	18	4	2'-6"	—
d20(E)	8	6	9'-1"	—
e16(E)	6	4	19'-6"	—
e17(E)	6	4	21'-10"	—
e18(E)	6	4	27'-4"	—
e19(E)	4	4	2'-6"	—
e20(E)	4	4	7'-1"	—
Concrete Removal		Cu. Yd.	0.6	
Concrete Structures		Cu. Yd.	6.3	
Bridge Fence Railing		Foot	67	
Reinforcement Bars, Epoxy Coated		Pound	1,942	
Protective Coat		Sq. Yd.	24	



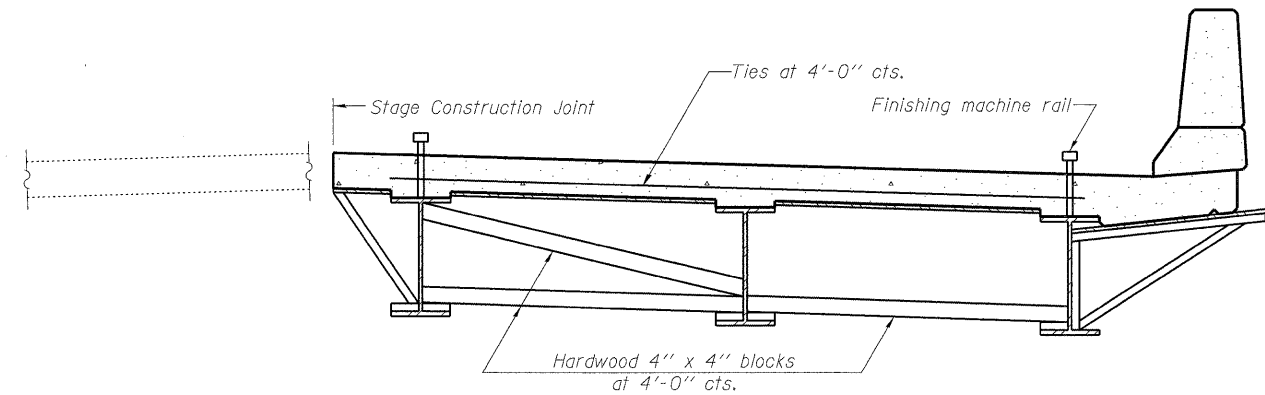
1/4" DIA. ANCHOR BOLT

**SOUTHEAST PARAPET WALL II
STRUCTURE NUMBER 016-0610**

SHEET NO. 37 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	113
			CONTRACT NO. 60C18		
ILLINOIS FED. AID PROJECT					

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Arlington Heights IL 60005
DLZ Illinois, Inc. (847)-640-0840

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



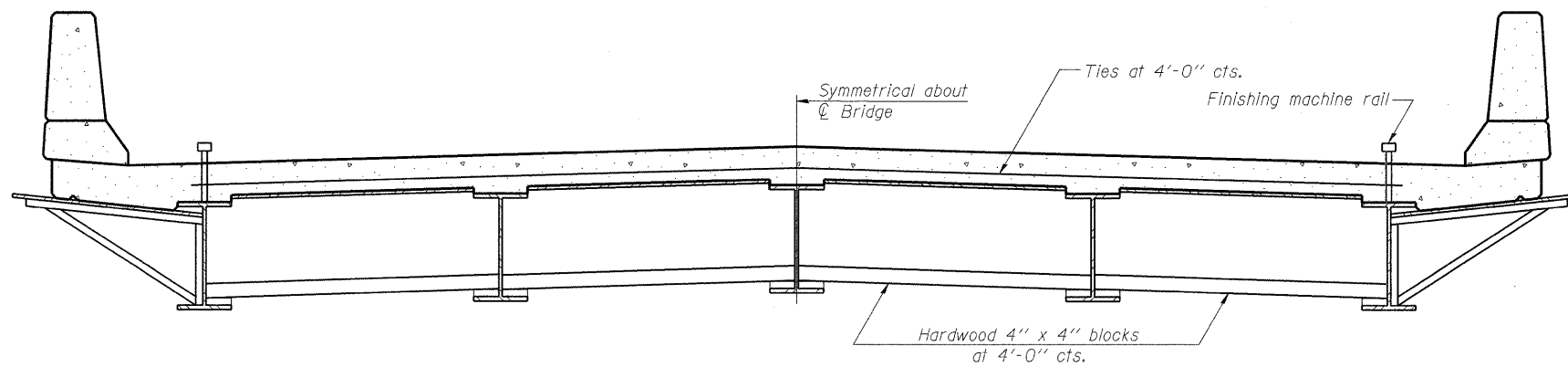
FORM BRACES FOR
STAGE CONSTRUCTION

When cantilever forming brackets are used, the work shall be done according to Article 503.06(b) of the Standard Specifications, except as modified below and in the details shown on this sheet.

The finishing machine rails shall be placed on the top flange of the exterior beams.

The beams or girders, supporting cantilever forming brackets, shall be tied together at 4 foot intervals.

For Standard construction, or Stage Construction the Hardwood bracing materials shall be placed as shown between webs of beams in each bay.



FORM BRACES FOR
STANDARD CONSTRUCTION

DESIGNED - ASP
CHECKED - WSP
DRAWN - BEM
CHECKED - ASP

SB-1

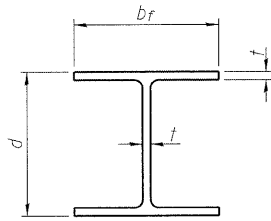
11-1-09

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Arlington Heights IL 60005
DLZ Illinois, Inc. (847)-640-0840

CANTILEVER FORMING BRACKETS
STRUCTURE NUMBER 016-0610

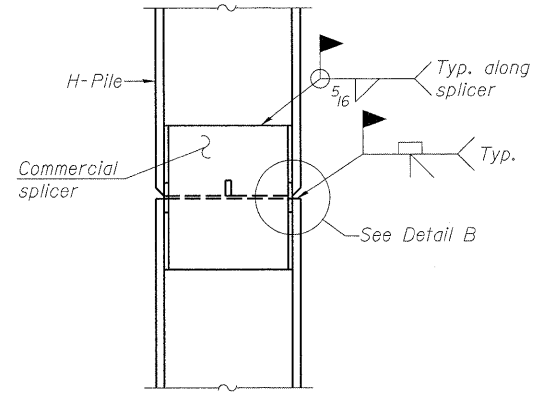
SHEET NO. 38 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	114
CONTRACT NO. 60C18					
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

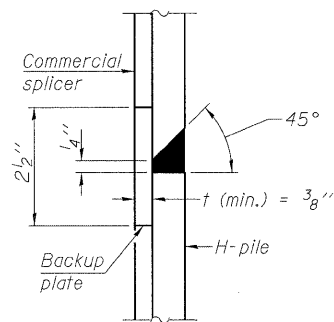


STEEL PILE TABLE

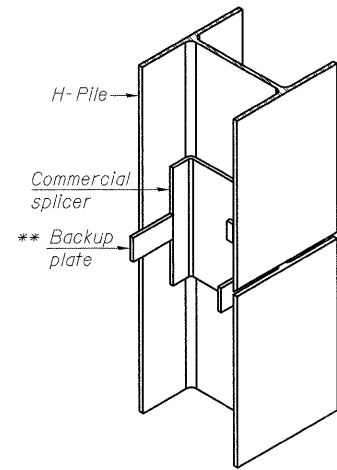
Designation	Depth d	Flange width b _f	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	11/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	11/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION

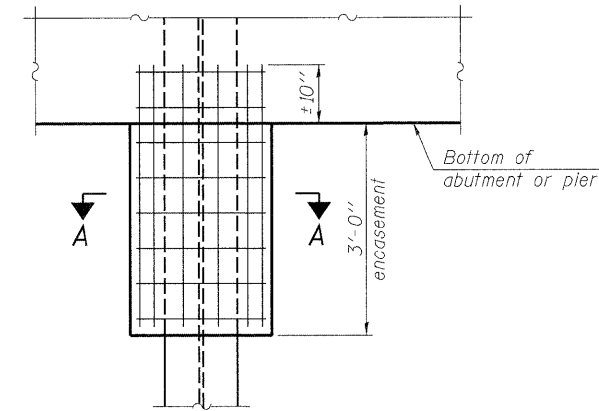


DETAIL "B"



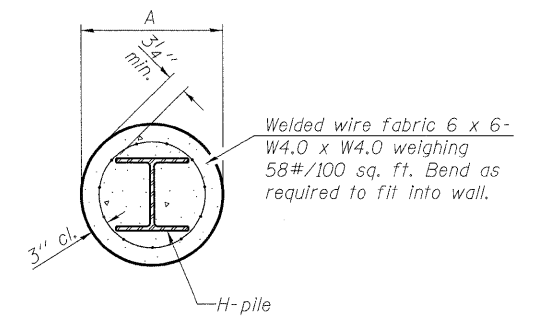
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE



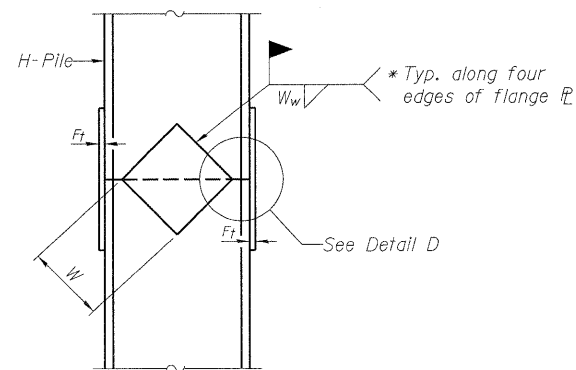
ELEVATION

PILE ENCASEMENT

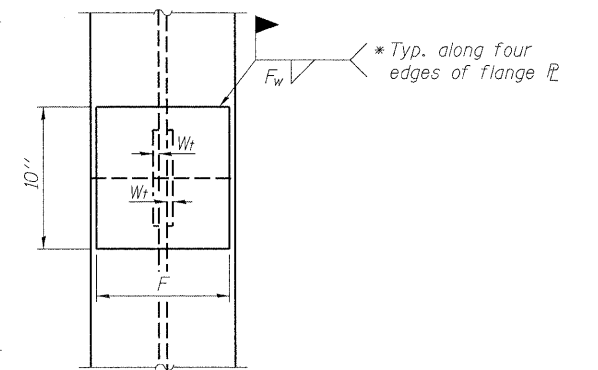


Note:
Forms for encasement may be omitted when soil conditions permit.

SECTION A-A



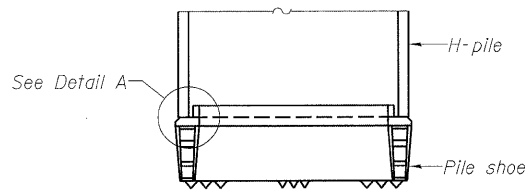
ELEVATION



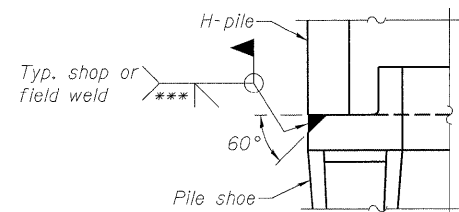
END VIEW

Designation	F	F _t	F _w	W	W _t	W _w
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5 1/2"	1 1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5 1/2"	1 1/2"
x89	12 1/2"	3/4"	11/16"	7 3/4"	5 1/2"	1 1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5 1/2"	1 1/2"
HP 12x84	10"	7/8"	11/16"	6 1/2"	5 1/2"	1 1/2"
x74	10"	7/8"	11/16"	6 1/2"	5 1/2"	1 1/2"
x63	10"	5/8"	1/2"	6 1/2"	1 1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1 1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1 1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1 1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1 1/2"	3/8"

WELDED PLATE FIELD SPLICE

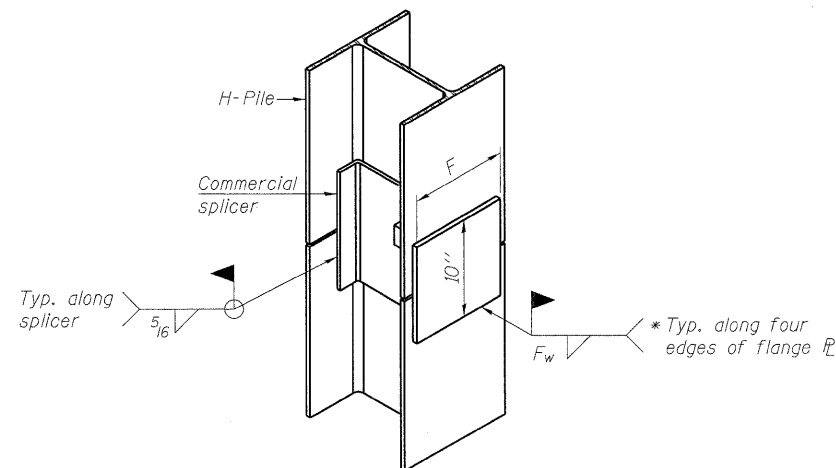


ELEVATION



DETAIL A

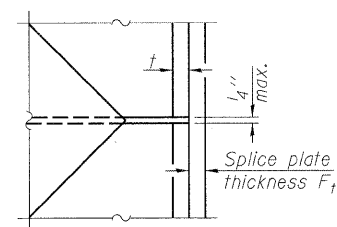
H-PILE SHOE ATTACHMENT



ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

- * Interrupt welds 1/4" from end of web and/or each flange.
- ** Remove portions of backup plates that extend outside the flanges.
- *** Weld size per pile shoe manufacturer (5/16" min.).



DETAIL D

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.

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HP PILE DETAILS
STRUCTURE NUMBER 016-0610

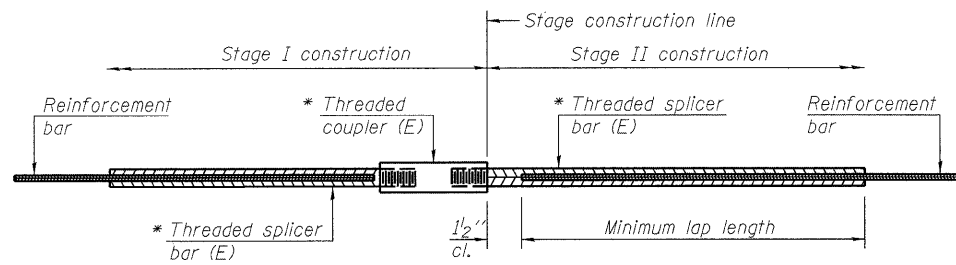
SHEET NO. 39 OF 42 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	115
CONTRACT NO. 60C18					
ILLINOIS FED. AID PROJECT					

DESIGNED - ASP
CHECKED - WSP
DRAWN - BEM
CHECKED - ASP

F-HP

11-1-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

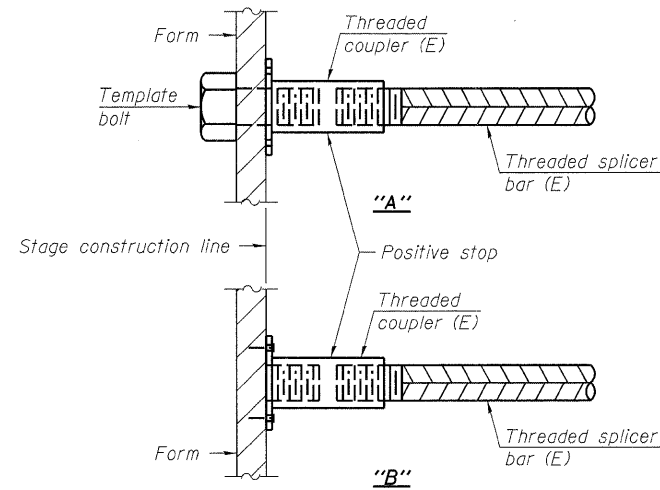
Bar size to be spliced	Minimum Lap Lengths			
	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

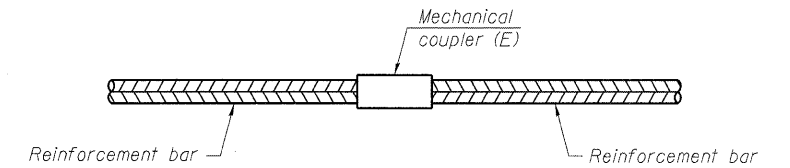
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Top of Deck	#5	255	3
Bottom of Deck	#5	179	3
Deck Edge Beam	#6	14	3
West Abutment	#4	11	3
West Abutment	#4	4	4
West Abutment	#5	20	3
West Abutment	#5	7	4
West Abutment	#6	3	3
East Abutment	#4	6	3
East Abutment	#4	4	4
East Abutment	#5	12	3
East Abutment	#5	2	4
East Abutment	#6	3	3
Pier	#4	11	3
Pier	#4	4	4
Pier	#6	4	4
Pier	#9	18	3
Pier	#10	4	3
West Approach Slab	#4	25	4
West Approach Slab	#5	46	3
West Approach Ftg.	#5	42	3
East Approach Slab	#4	25	4
East Approach Slab	#5	46	3
East Approach Ftg.	#5	42	3



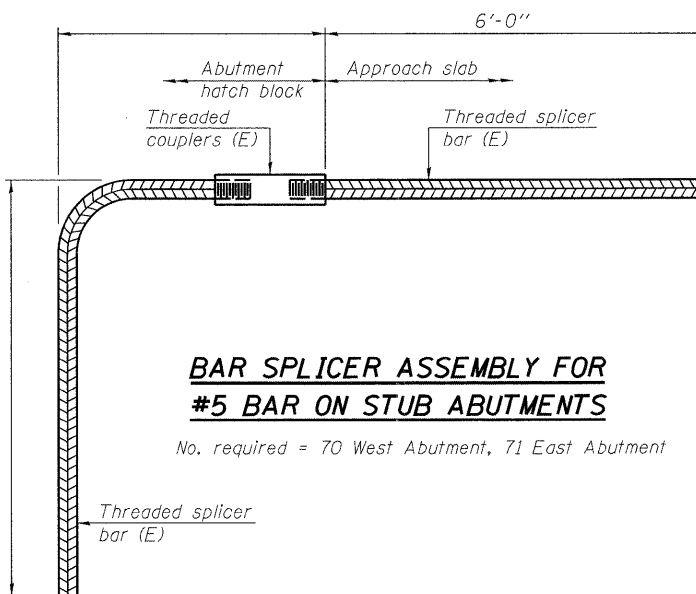
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required = 70 West Abutment, 71 East Abutment

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NUMBER 016-0610**

DESIGNED - ASP
CHECKED - WSP
DRAWN - BEM
CHECKED - ASP

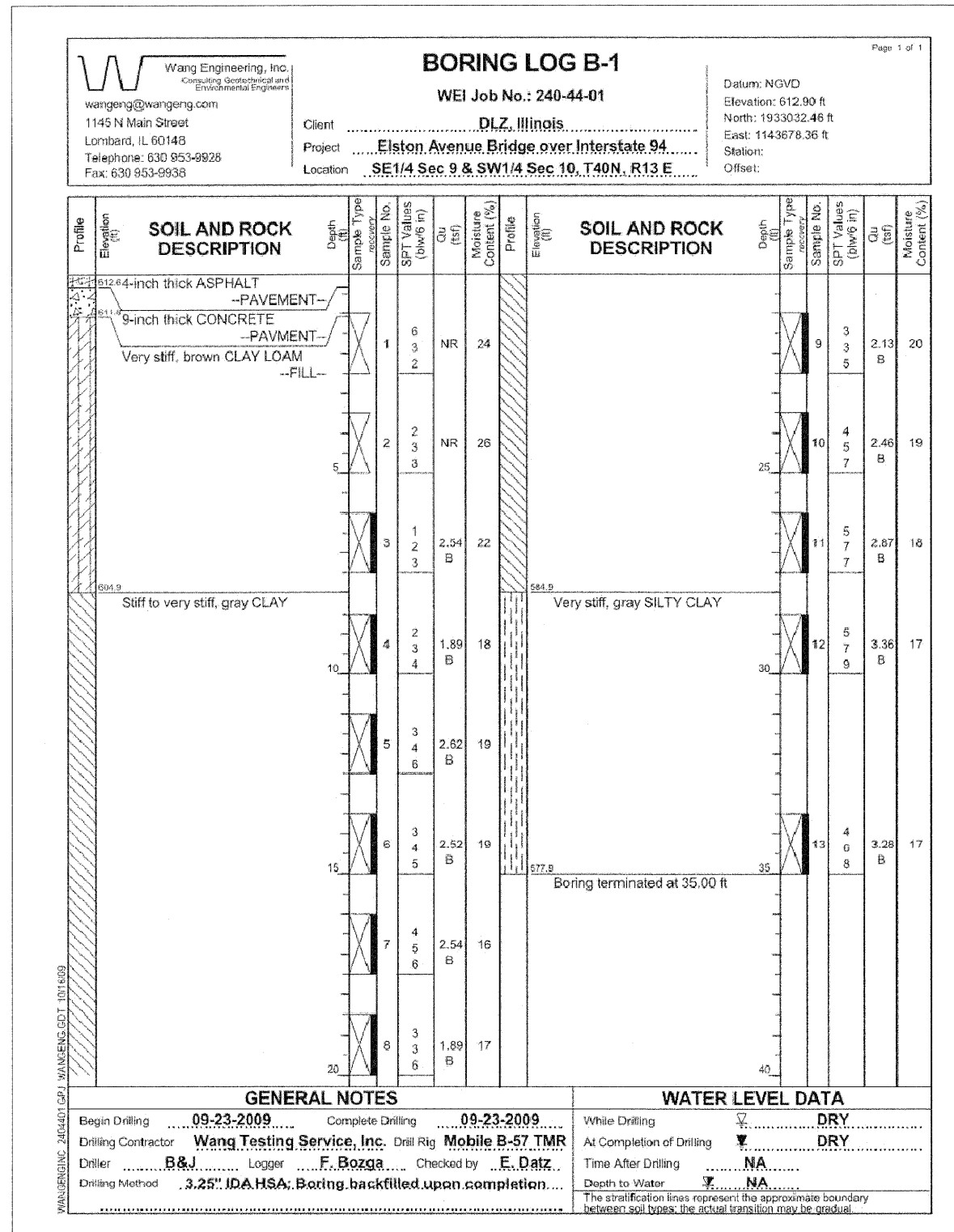
BSD-1

11-1-09

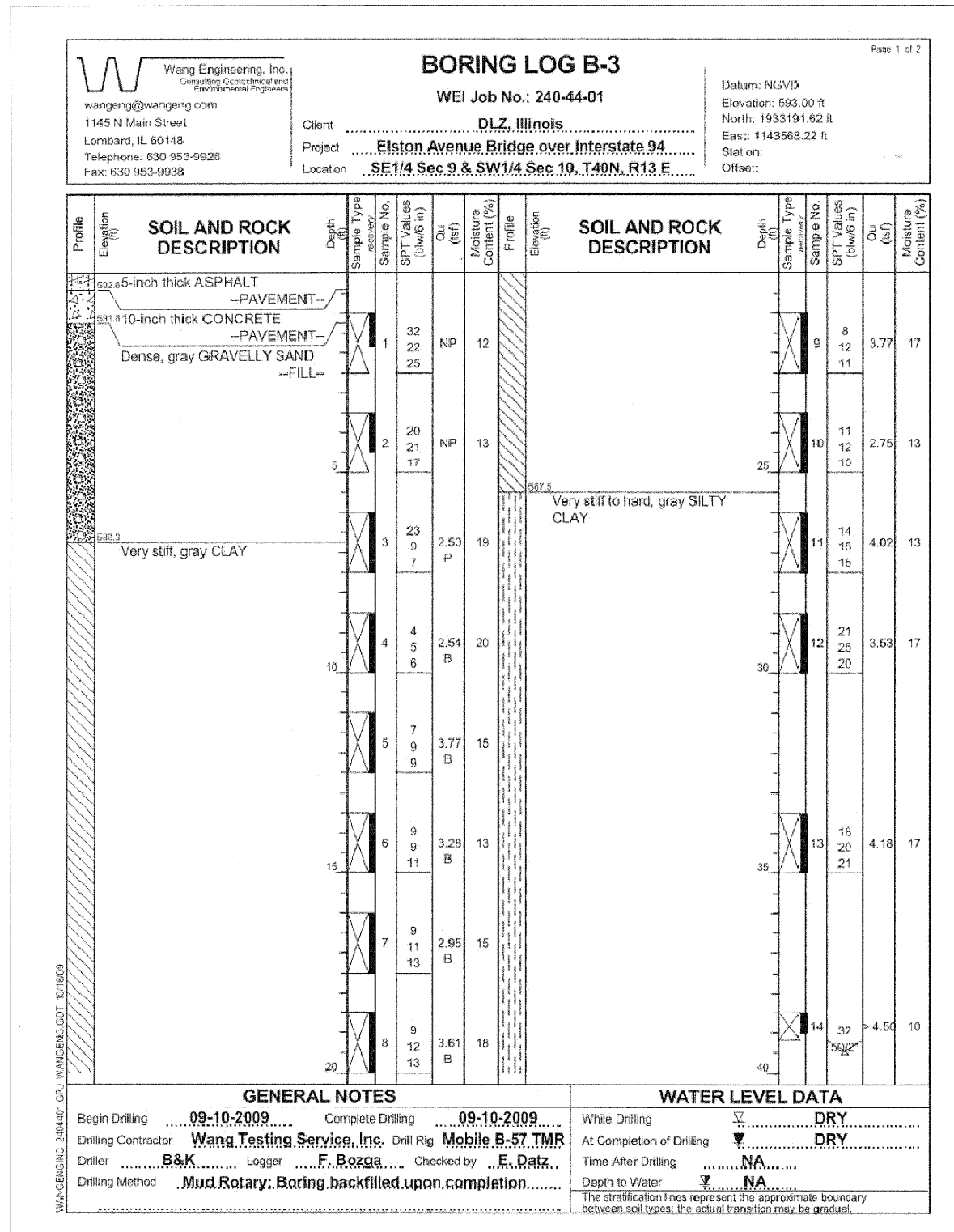
DLZ 85 W. Algonquin Rd. Ste. 220
Arlington Heights IL 60005
DLZ Illinois, Inc. (847)-640-0840

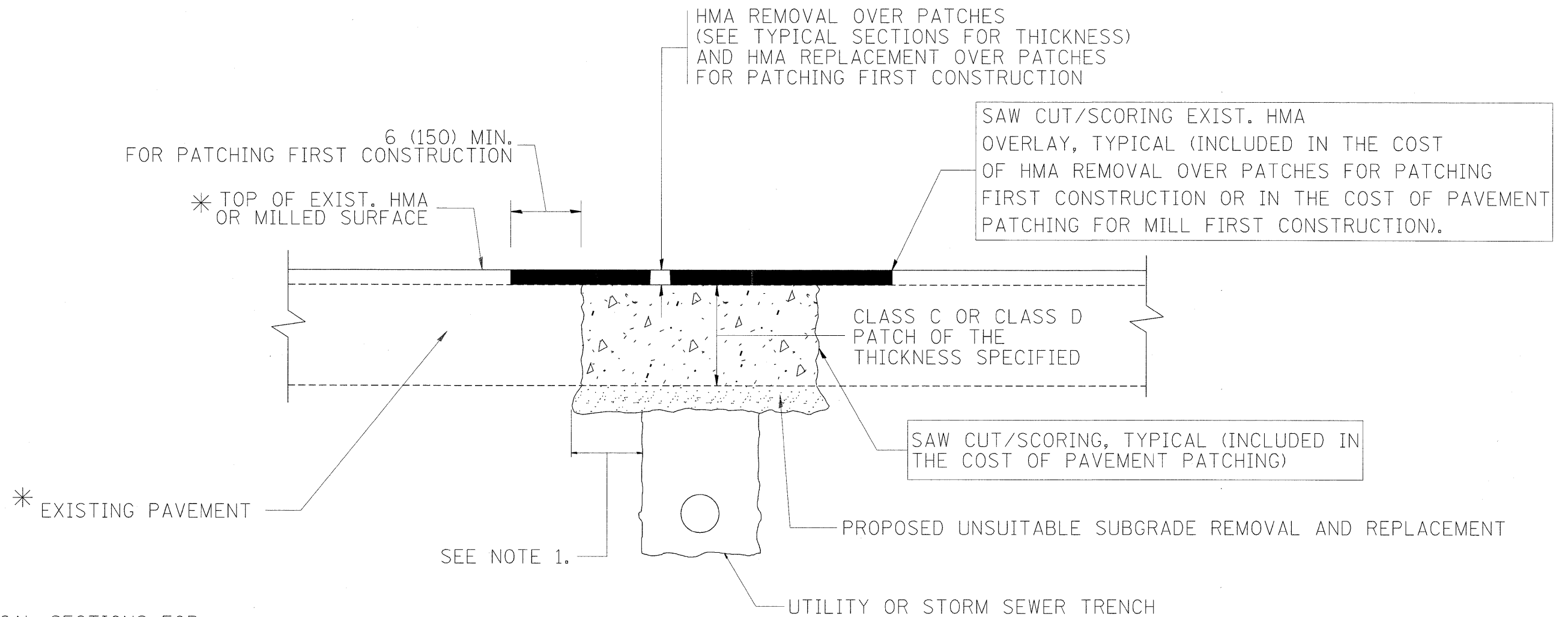
SHEET NO. 40 OF 42 SHEETS	F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	0101.8B	COOK	139	116
			CONTRACT NO. 60C18		
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



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DEPARTMENT OF TRANSPORTATION





* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

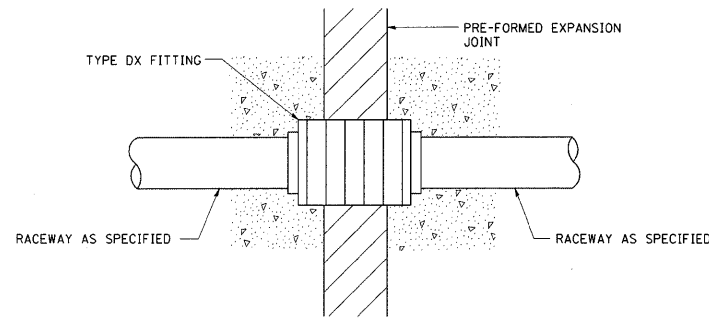
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

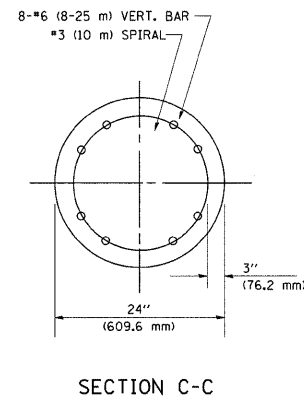
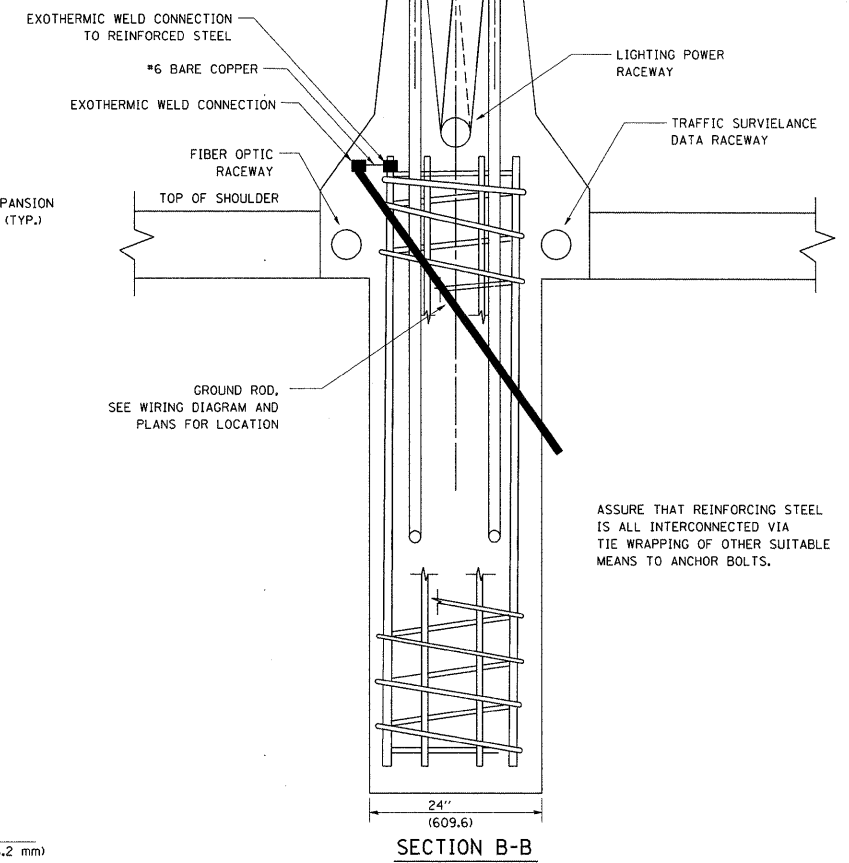
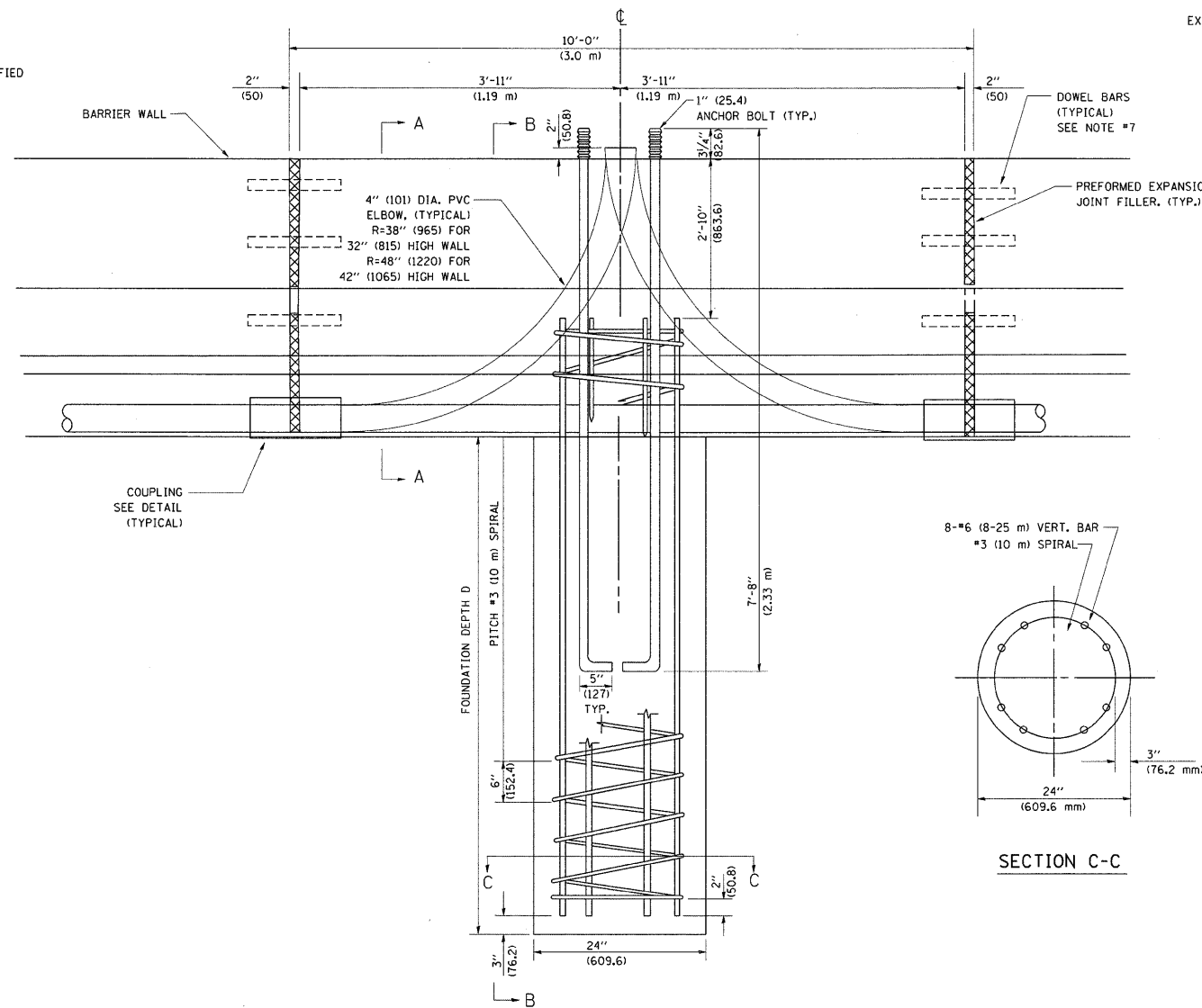
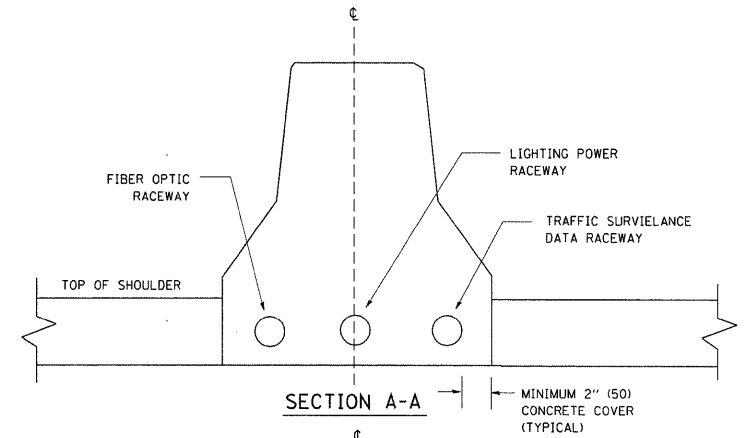
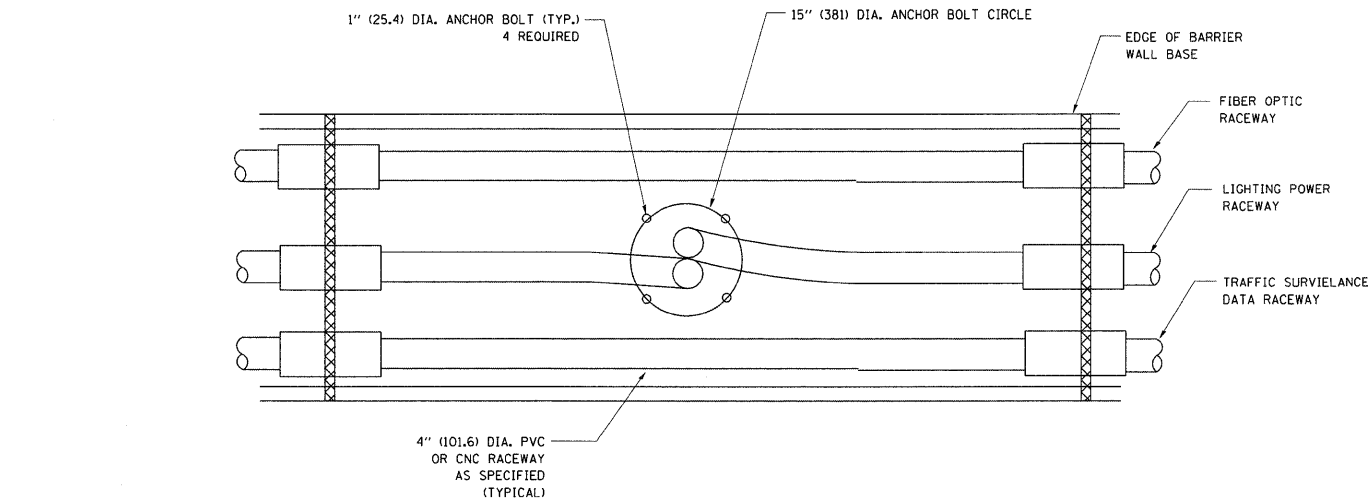
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60C18	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)							
			REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

NOTES:

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORMS.
3. THE CONTRACTOR AT HIS OPTION MAY SUBSTITUTE #4 (15 m) TIES AT 12" (304.8 m) CTRS. FOR THE #3 (3 m) SPIRAL. TACKWELDED TYPE BOLT MAY BE SUBSTITUTED FOR THE HOOK TYPE BOLT.
4. COLD BENDING OF THE HOOK BOLT SHALL NOT BE ALLOWED.
5. EXCAVATION FOR THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER 24 INCHES (609.6 m) IN DIAMETER.
6. THE ENGINEER SHALL DETERMINE THE TYPE OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE USING THE DOMINANT CHARACTERISTIC OF THE SOIL ENCOUNTERED.
7. BARRIER WALL EXPANSION AND CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH STANDARD DETAIL 637001-02 AND 637006 AS APPLICABLE.



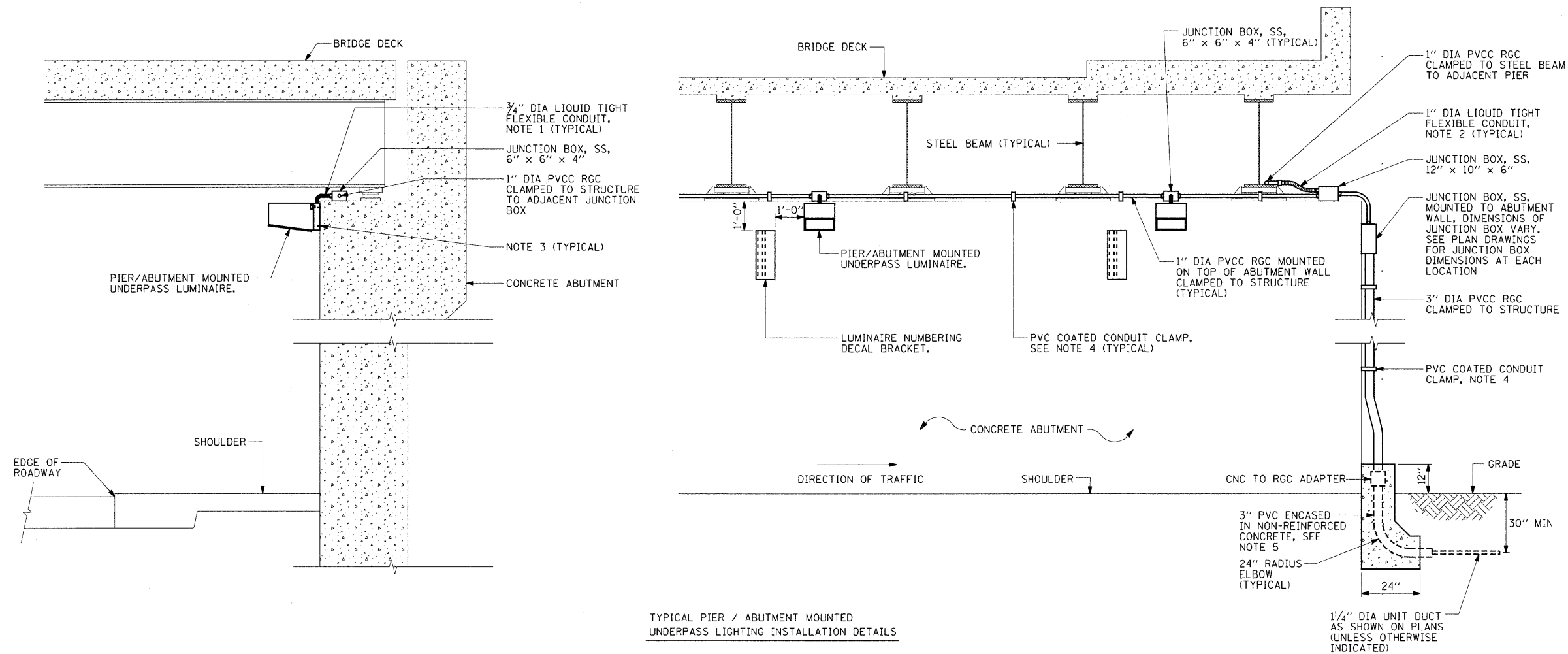
CONDUIT DEFLECTION FITTING DETAIL



FOUNDATION DEPTH

TYPE OF SOIL	FOUND FEPTH D	REINFORCEMENT IN FOUNDATION	
		VERTICAL BARS	SPIRAL
ROCK OR SOLIDIFIED SLAG	5'-0" (1.52 m)	NONE	NONE
DENSE SAND	7'-9" (2.36 m)	8-#6 x 9'-0" (8-20 m x 2.74 m)	#3 x 90' (3 m x 27.43 m)
MEDIUM SAND	8'-3" (2.51 m)	8-#6 x 9'-5" (8-20 m x 2.87 m)	#3 x 94' (3 m x 28.65 m)
LOOSE SAND	9'-0" (2.74 m)	8-#6 x 10'-2" (8-20 m x 3.09 m)	#3 x 100' (3 m x 30.48 m)
STIFF CLAY	7'-0" (2.13 m)	8-#6 x 10'-8" (8-20 m x 2.48 m)	#3 x 80' (3 m x 24.38 m)
MEDIUM CLAY	9'-6" (2.89 m)	8-#6 x 10'-8" (8-20 m x 3.25 m)	#3 x 104' (3 m x 31.69 m)
SOFT CLAY	13'-0" (3.96 m)	8-#6 x 14'-2" (8-20 m x 4.32 m)	#3 x 144' (3 m x 43.89 m)

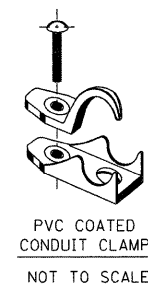
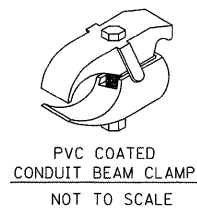
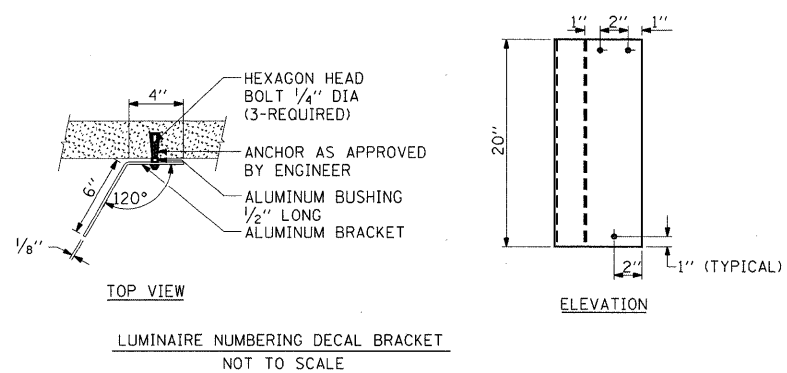
DESIGN: 80 MPH AASHTO



TYPICAL PIER / ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS

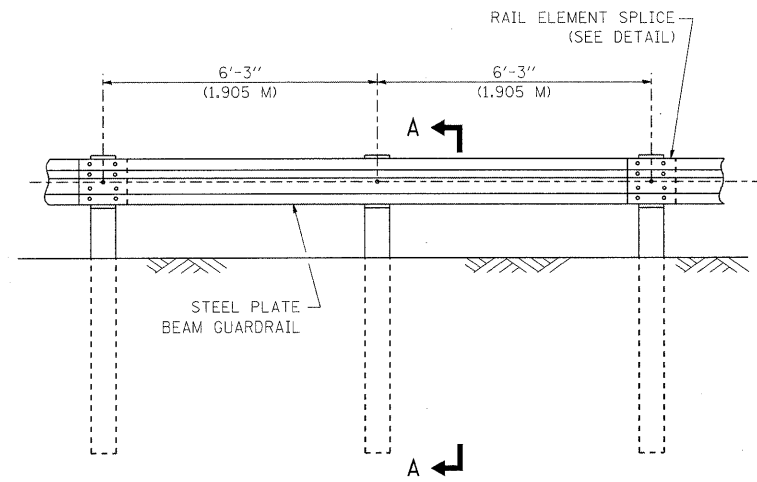
NOTES:

- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE 3/4" DIA. RIGID STEEL CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

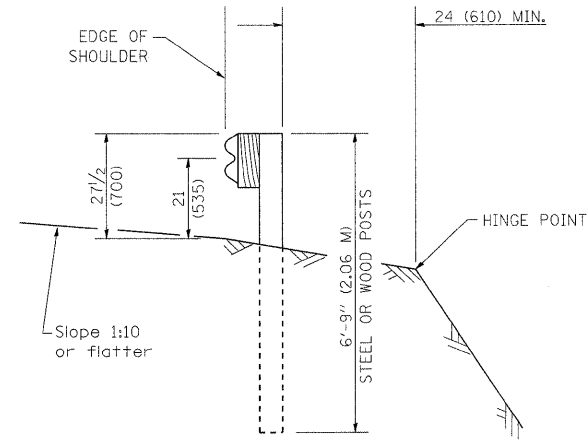


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	PLOT SCALE = 50,000 ' / IN.	CHECKED - DATE -	REVISED - REVISED -			SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	94	0101.8B	COOK	139	123
	PLOT DATE = 1/4/2008										BE-902		CONTRACT NO. 60C18		

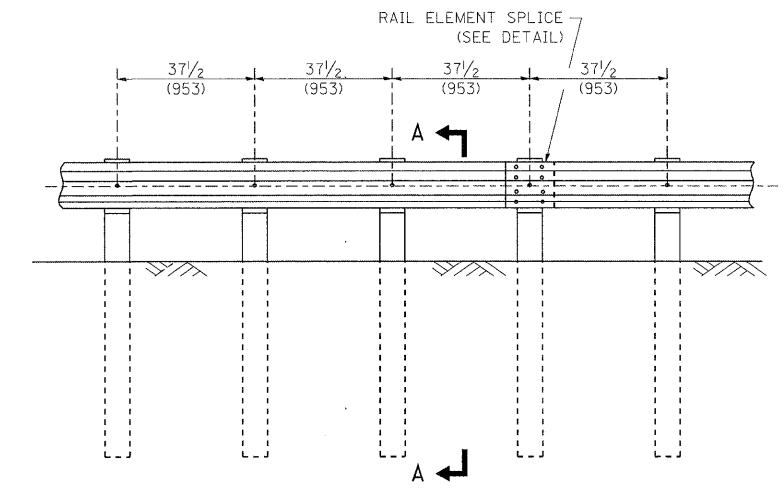
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



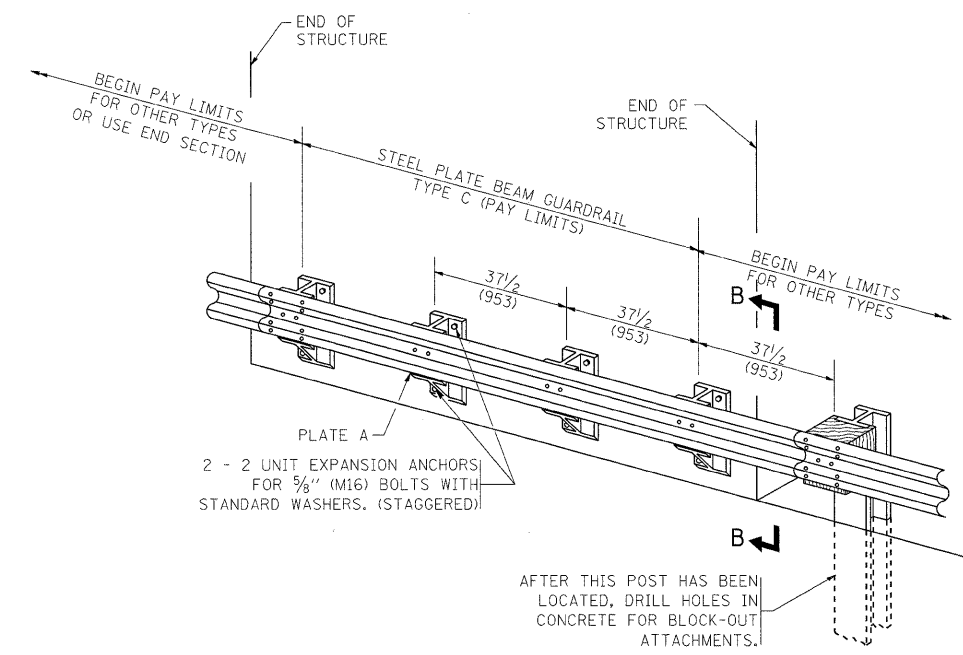
TYPE A
6'-3" (1.905 M) TYPICAL POST SPACING



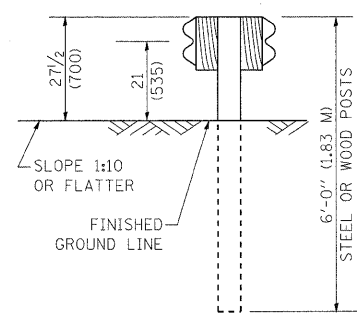
SECTION A-A



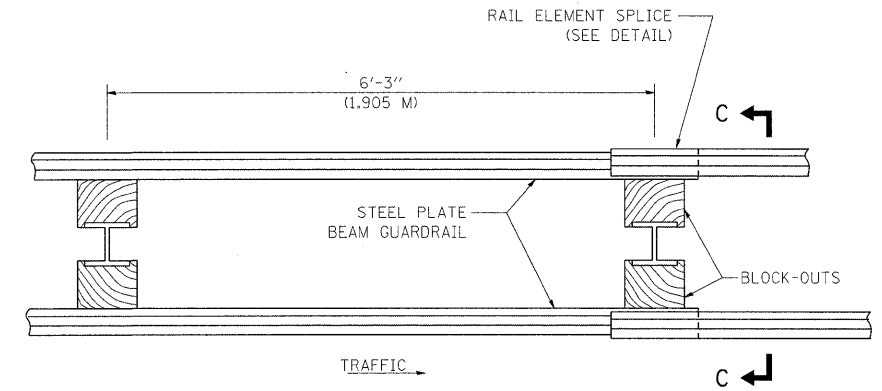
TYPE A
37 1/2 (953) CLOSED POST SPACING



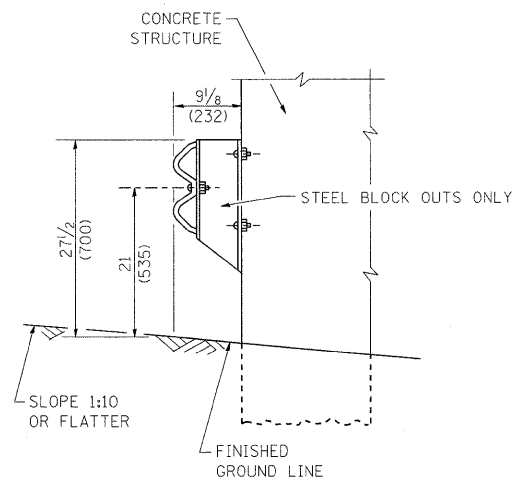
TYPE C
37 1/2 (953) BLOCK-OUT SPACING



SECTION C-C



TYPE D
DOUBLE STEEL PLATE BEAM GUARDRAIL
6'-3" (1.905 M) TYPICAL POST SPACING

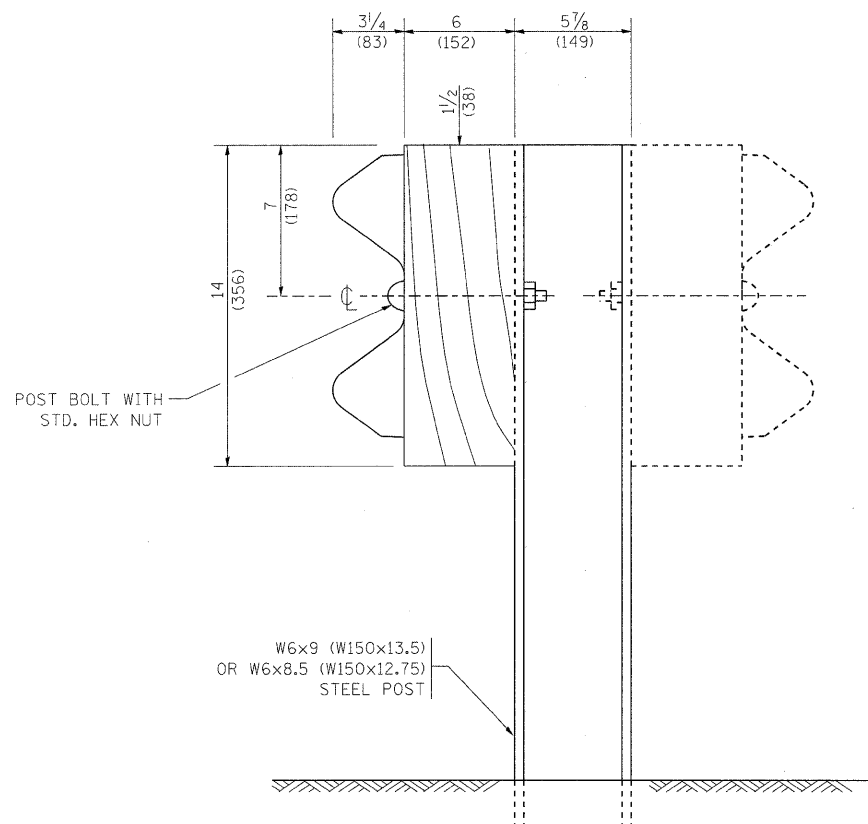


SECTION B-B

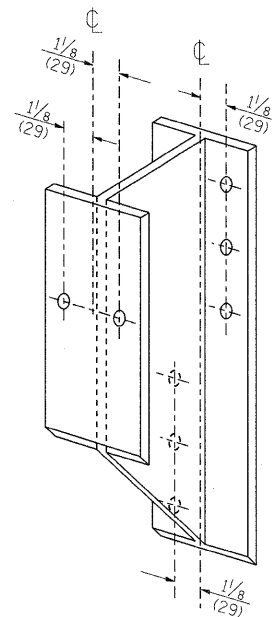
GENERAL NOTES

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.
THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

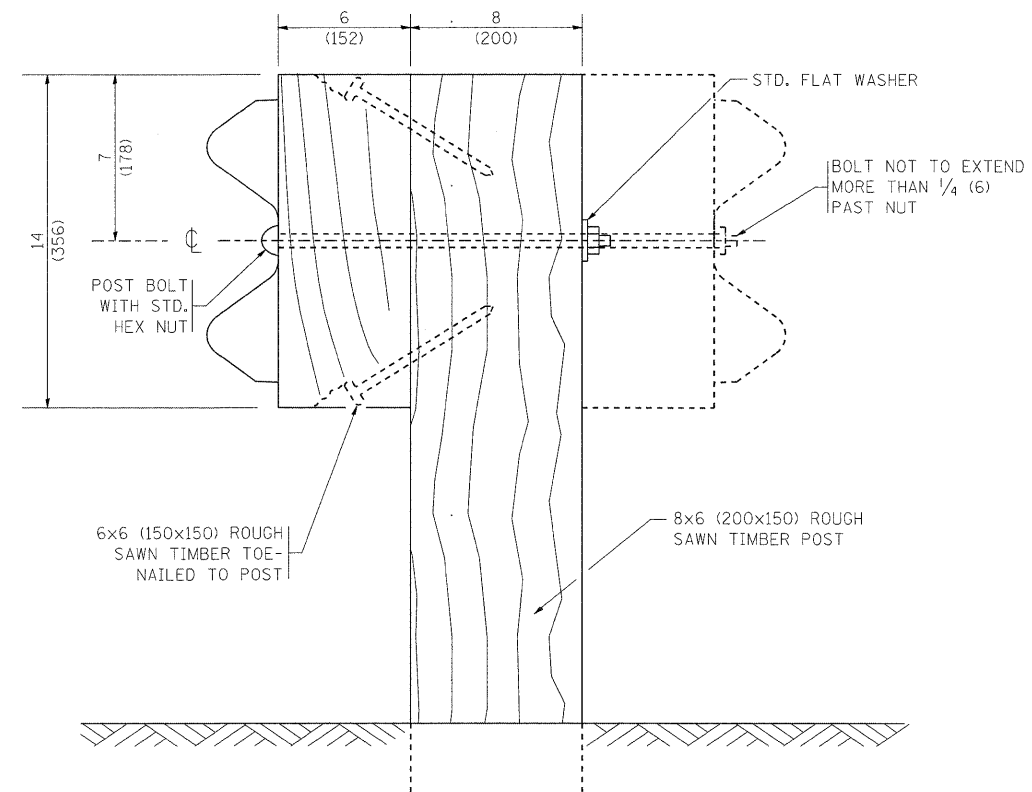
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PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.	BM-21			
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
CONTRACT NO. 60C18												



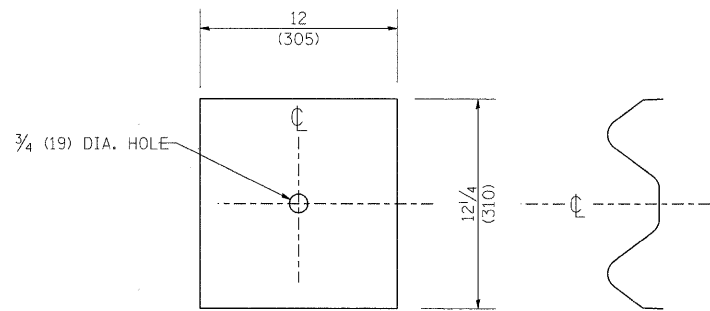
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



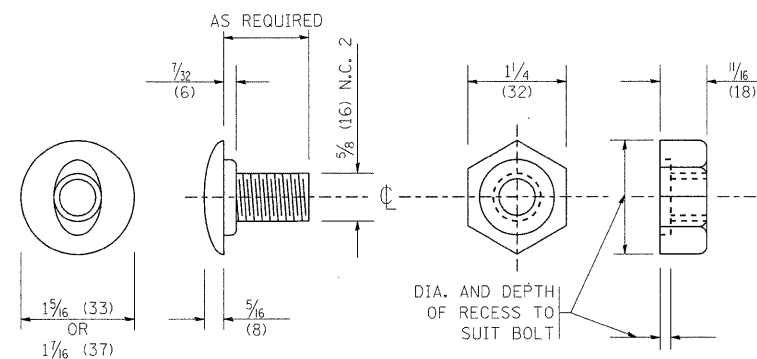
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



POST OR SPLICE BOLT & NUT

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DRAWN -

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PLOT DATE = 1/4/2008

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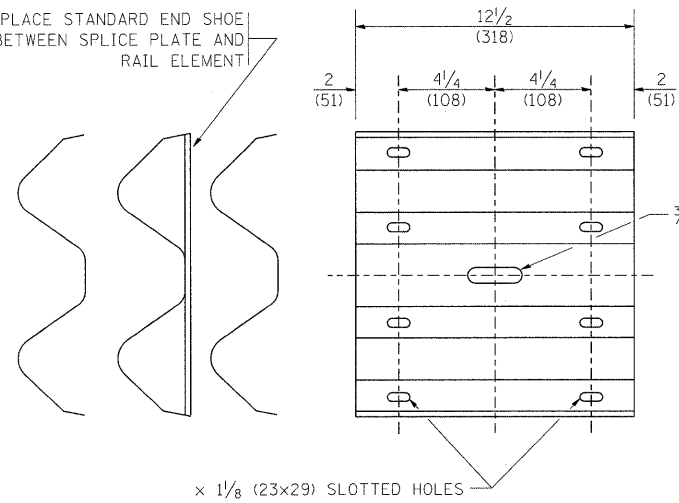
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

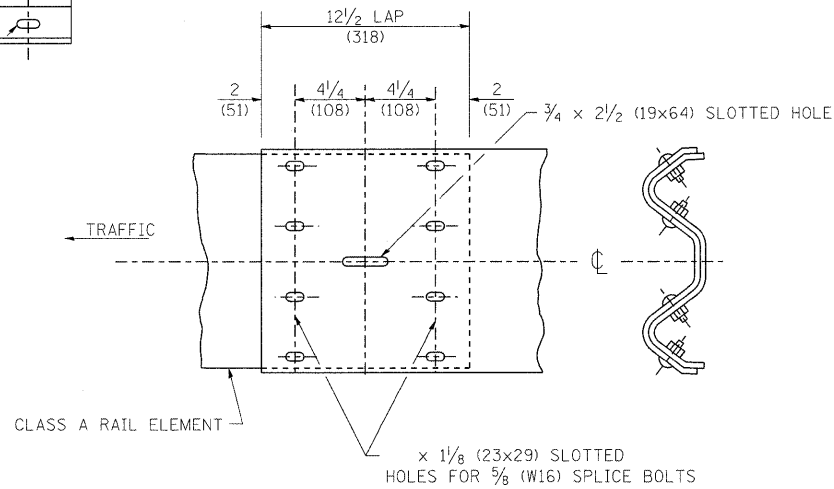
SCALE: NONE SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0101.8B	COOK	139	125
BM-21		CONTRACT NO. 60C18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

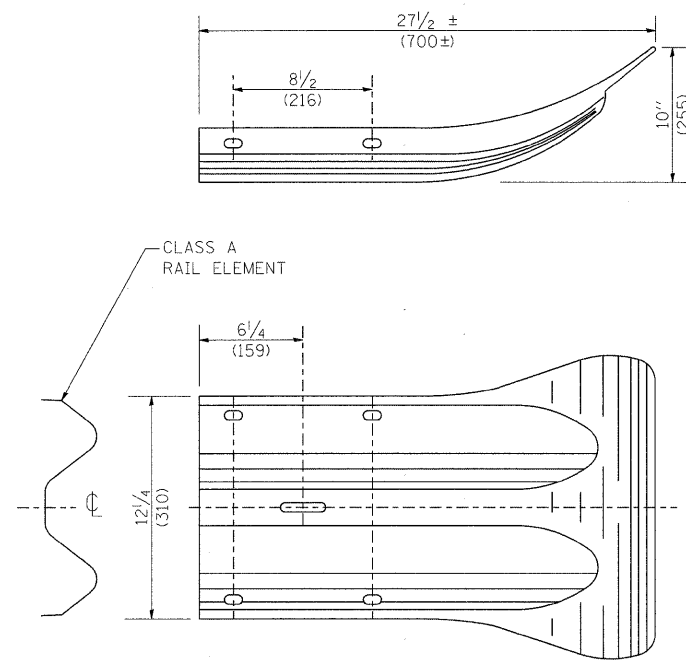
PLACE STANDARD END SHOE BETWEEN SPLICE PLATE AND RAIL ELEMENT



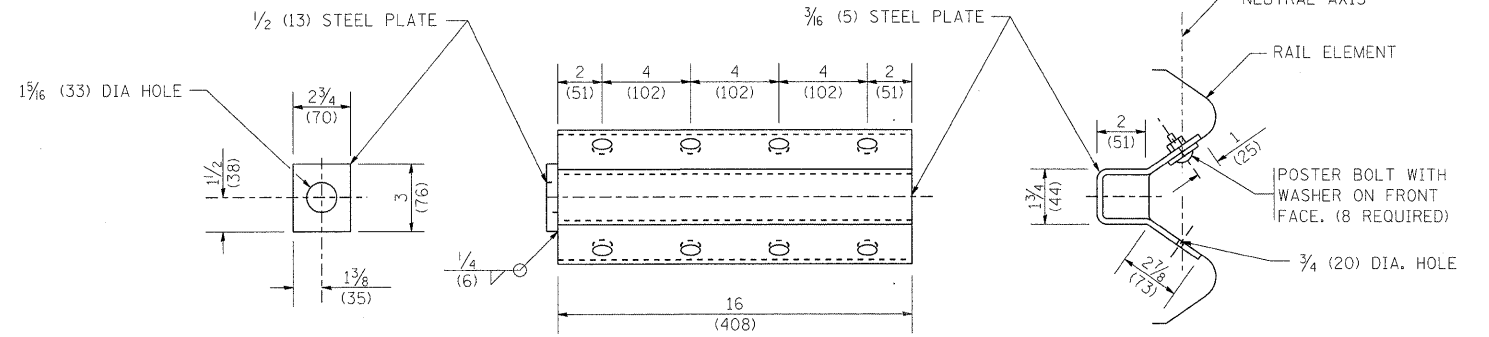
SPLICE PLATE



RAIL ELEMENT SPLICE



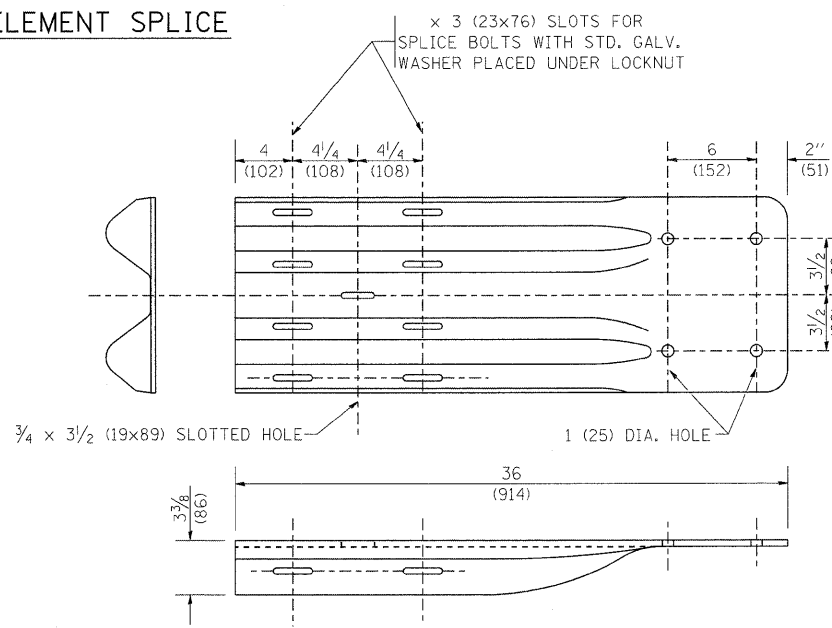
END SECTION



NOTE:

ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

ANCHORE PLATE T DETAILS



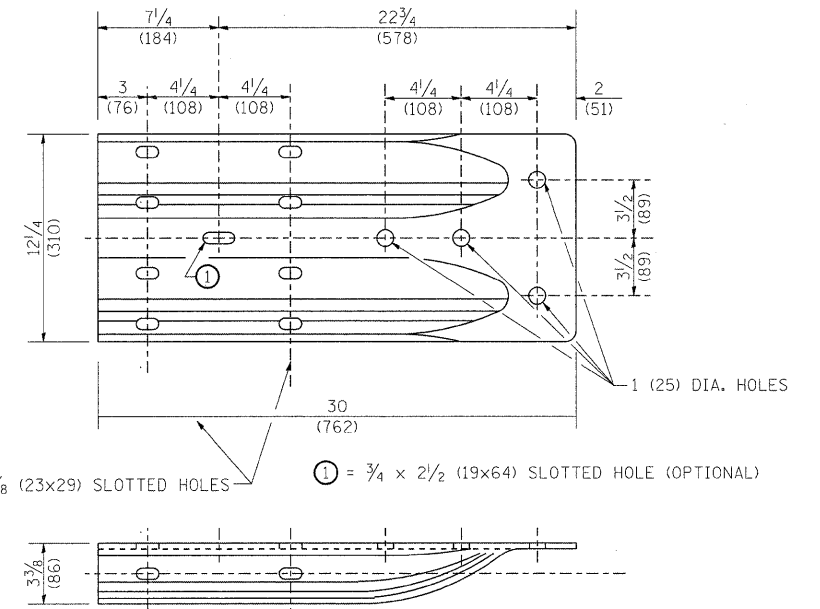
END SHOE

NOTE:

WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.



ALTERNATE END SHOE

FILE NAME =	USER NAME = goglianobt
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PLOT SCALE = 50,0000 "/ IN.	
PLOT DATE = 1/4/2008	

DESIGNED -	REVISD - 10-31-06
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DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

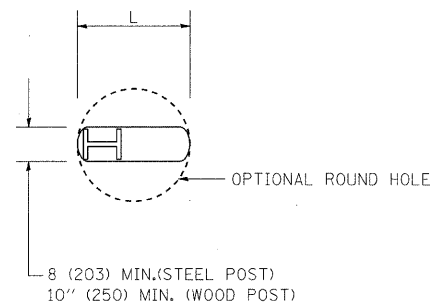
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CHECKED -	REVISD -
DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

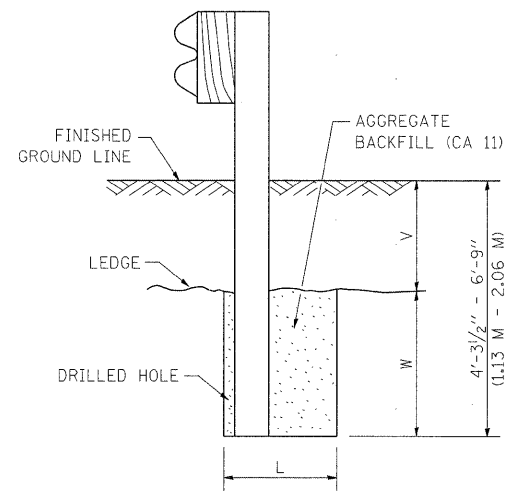
**REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 0101.6B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 126
BM-21		CONTRACT NO. 60C18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN

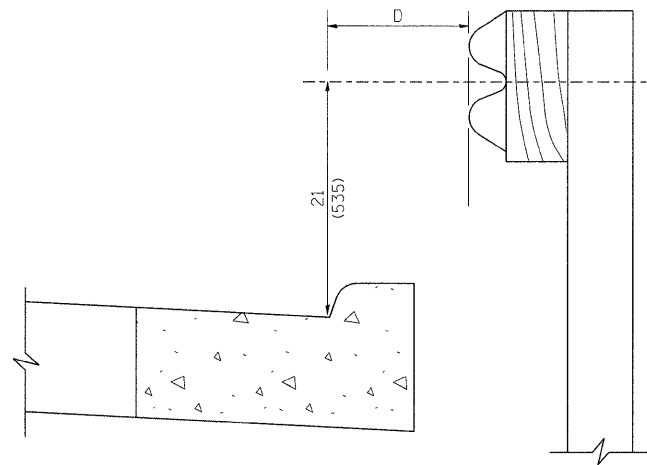


NOTE:

LEDGE LINE IS TOP OF ROCK LEDGE OR HARD SLAG FILL.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED



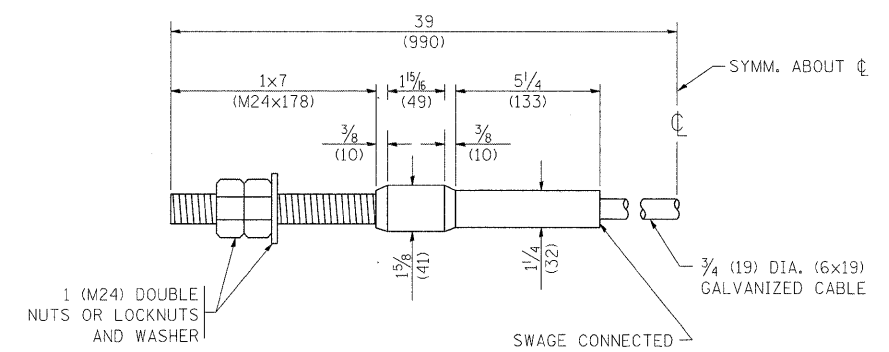
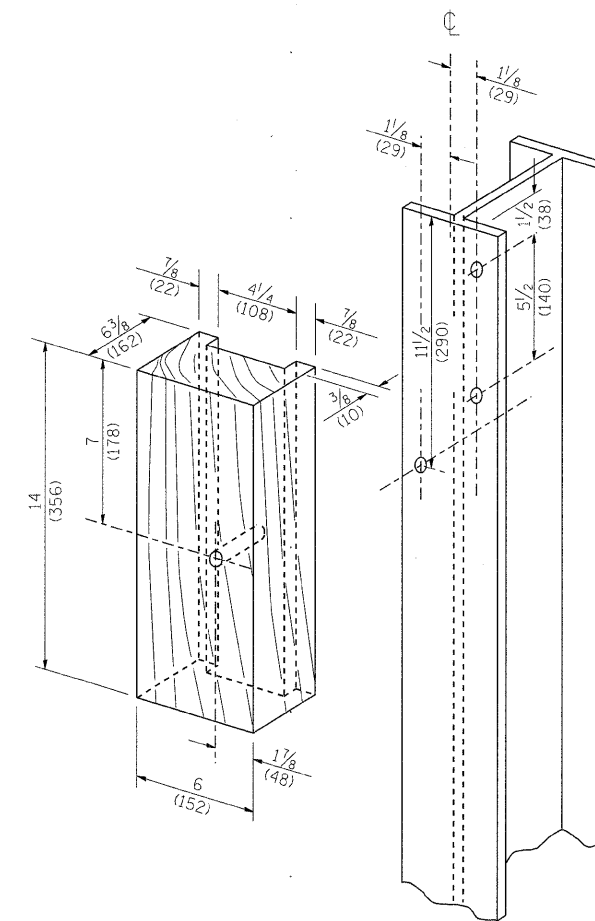
NOTE:

IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0" (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

GUARDRAIL PLACED BEHIND CURB

(D = 0 DESIRABLE TO 12 (300) MAXIMUM)

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



CABLE ASSEMBLY

(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)
TIGHTEN TO TAUT TENSION

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DESIGNED - REVISED - 10-31-06

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PLOT SCALE = 50.0000' / IN.

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PLOT DATE = 1/4/2008

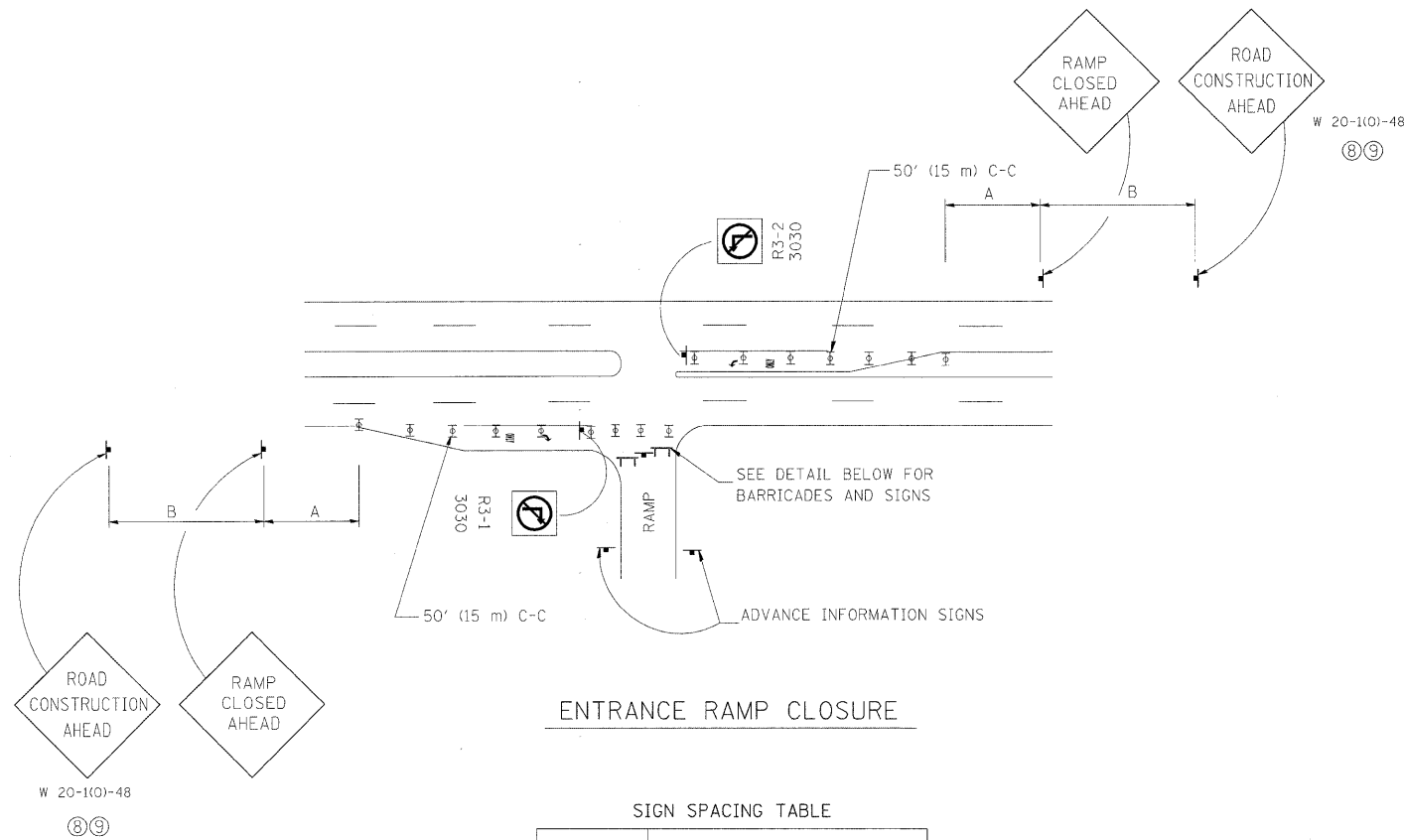
DATE - REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0101.8B	COOK	139	127
BM-21			CONTRACT NO. 60C18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

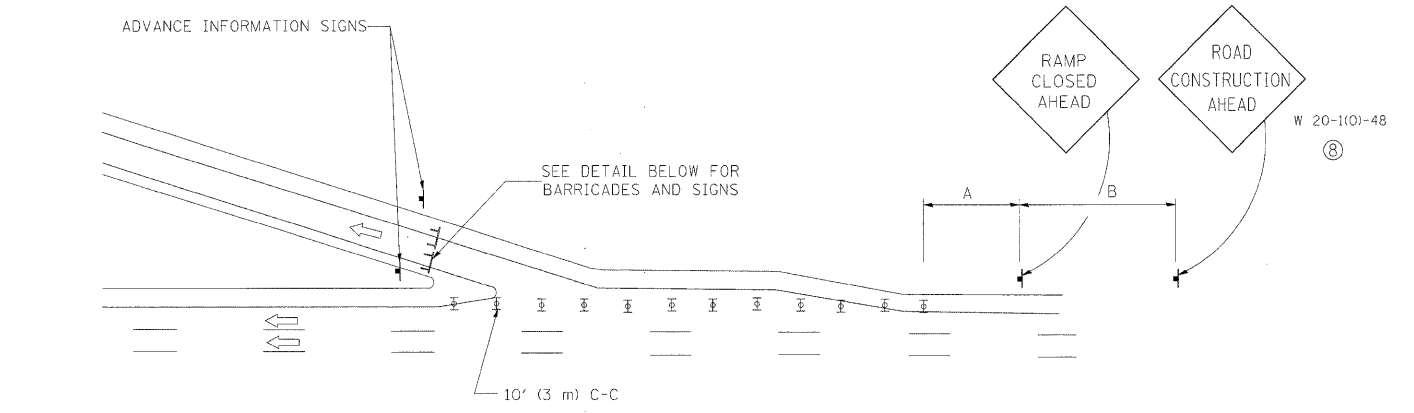


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

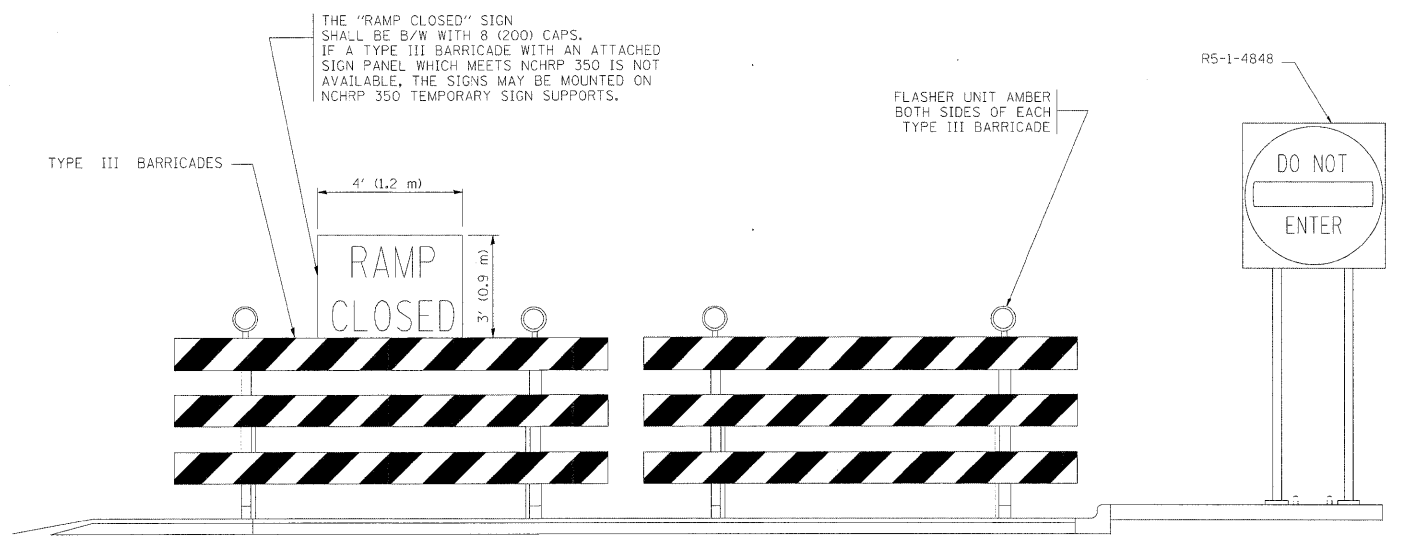
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL >45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

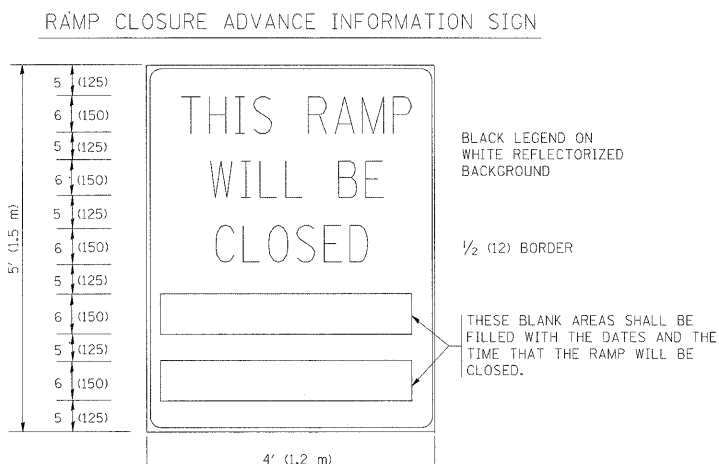
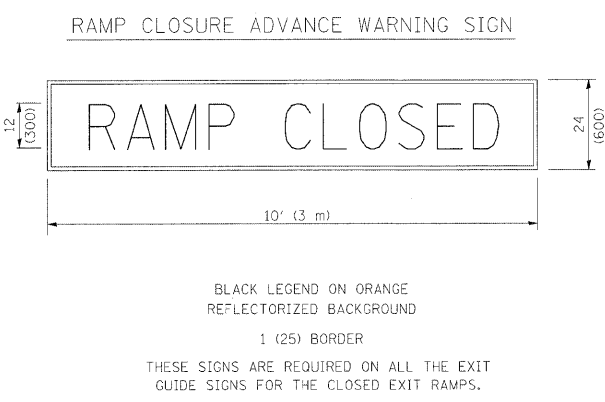


EXIT RAMP CLOSURE

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊥ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS

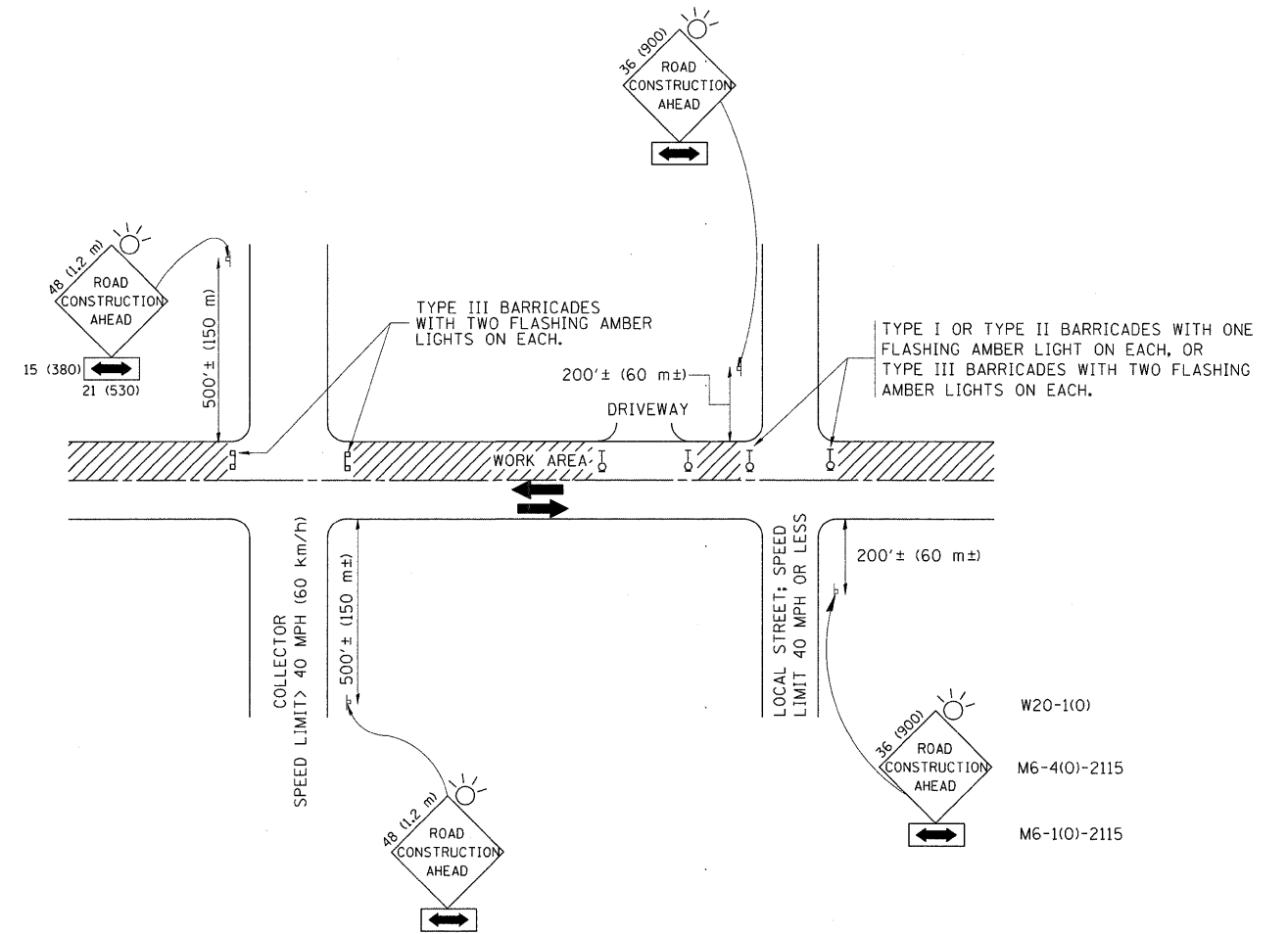


THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

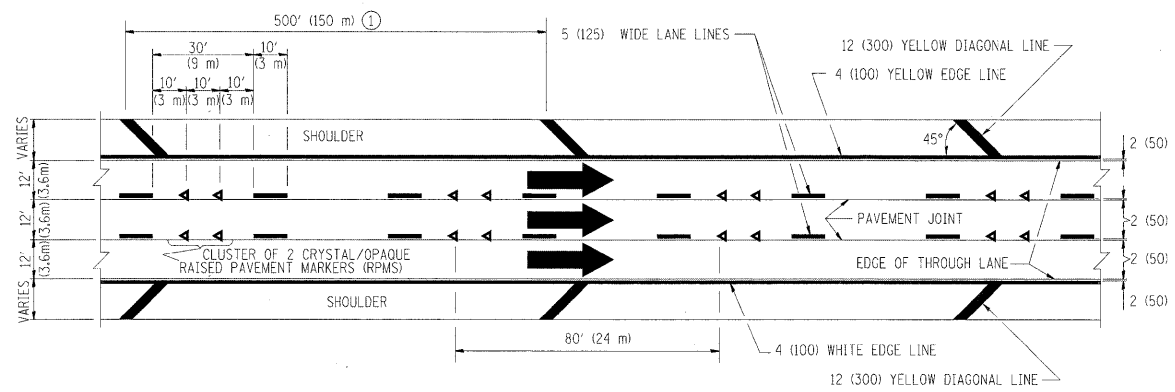
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

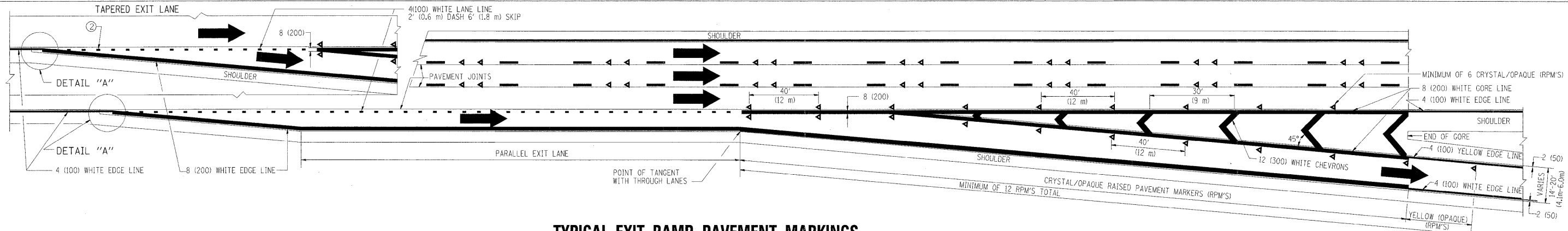
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 130
TC-10			CONTRACT NO. 60C18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

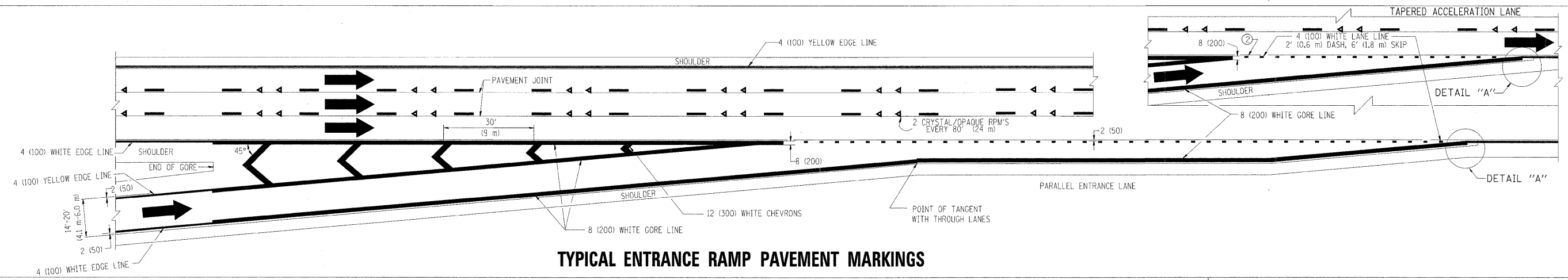


TYPICAL EDGE LINES & LANE LINES

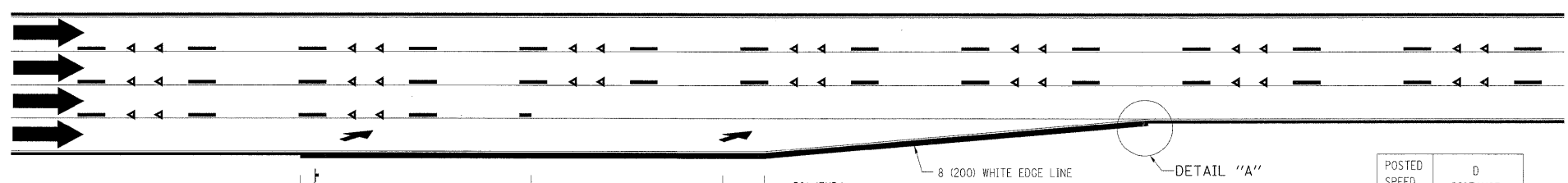
- PAVEMENT MARKING MATERIALS**
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
 3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

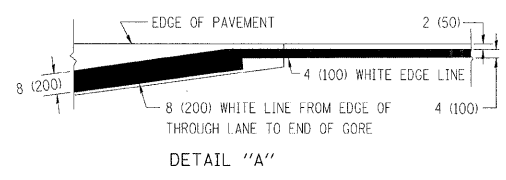


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

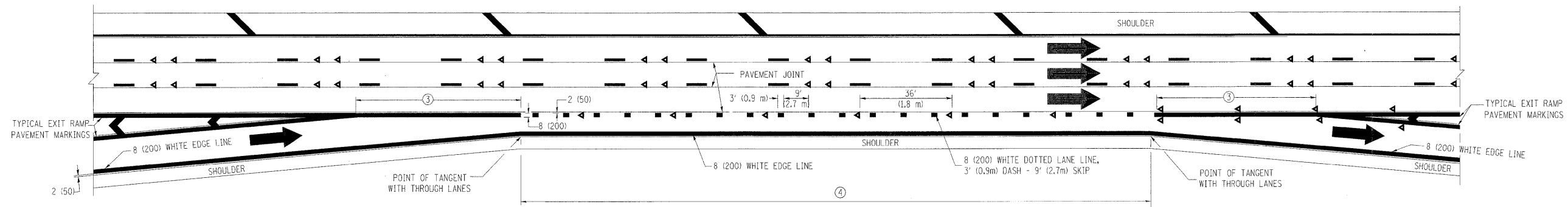


LANE REDUCTION PAVEMENT MARKINGS

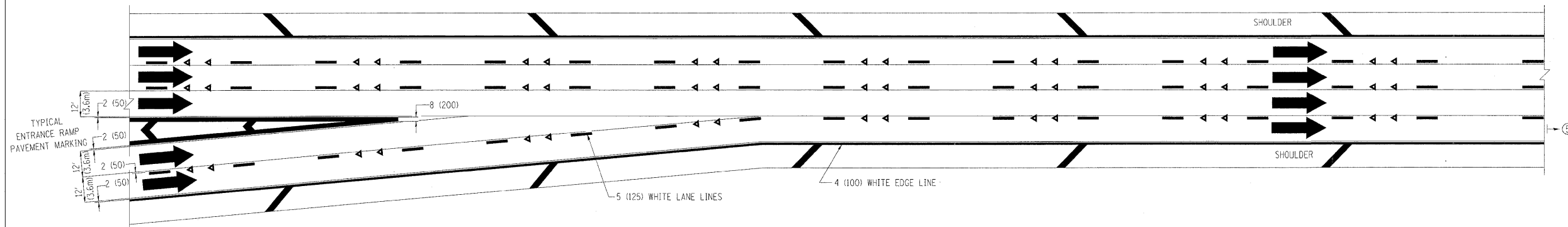
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



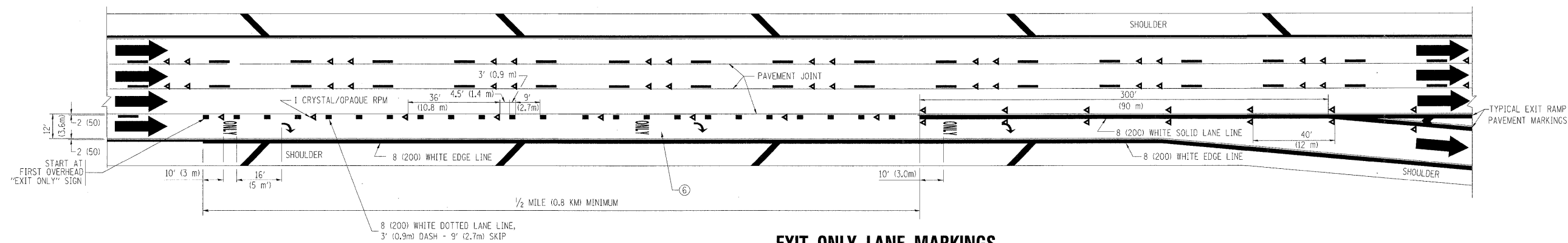
- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



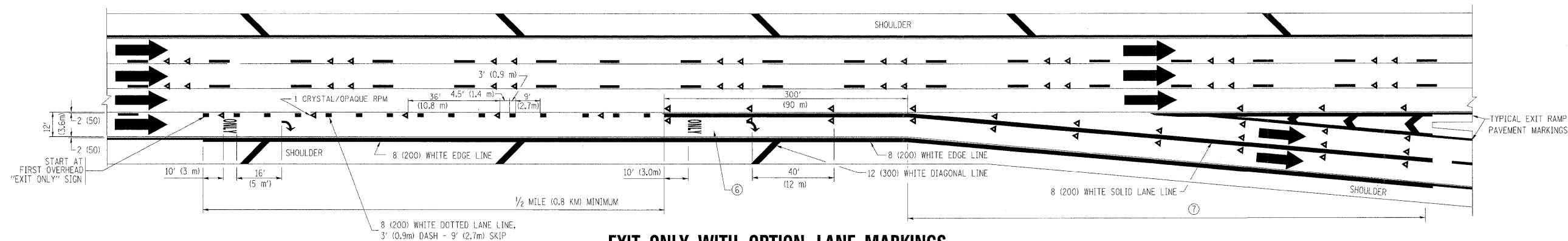
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS

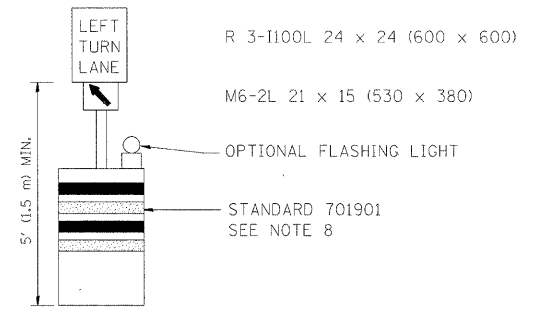
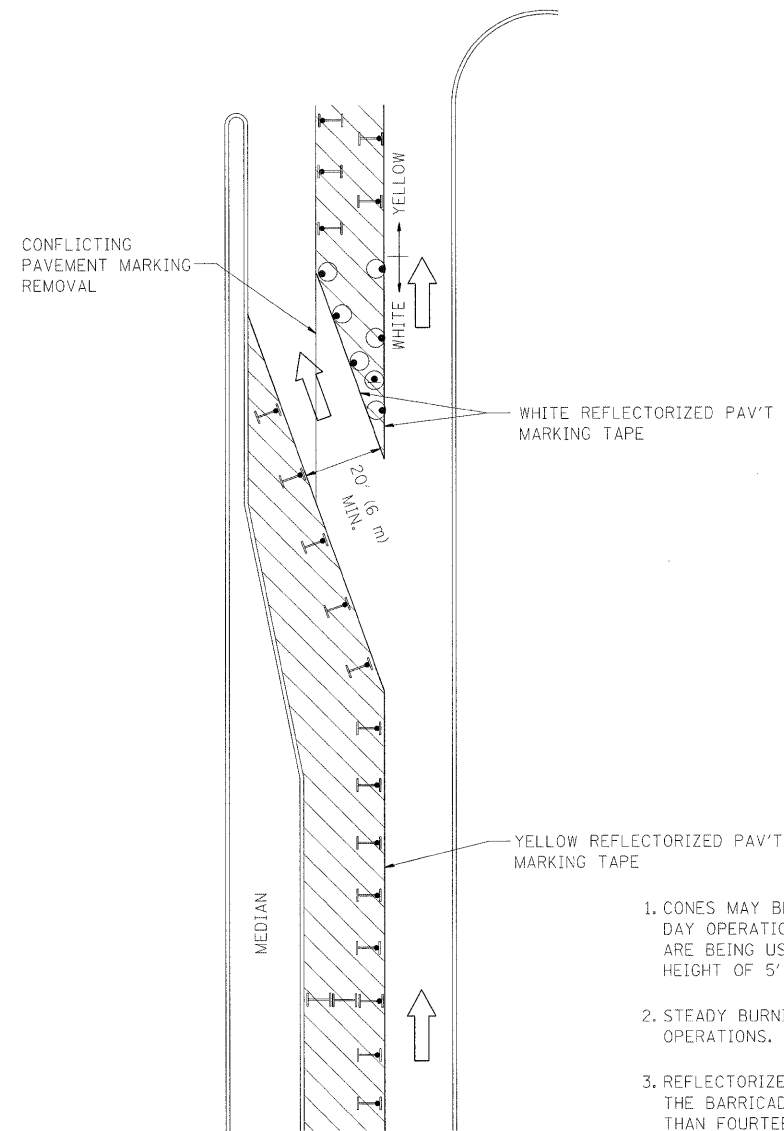


EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = drvakasgn	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
c:\pw\work\p\DOT\DRIVAK05GN\d0108315\14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 49.9999" / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 9/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0101.8B	COOK	139	133
TC-14			CONTRACT NO. 60C18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

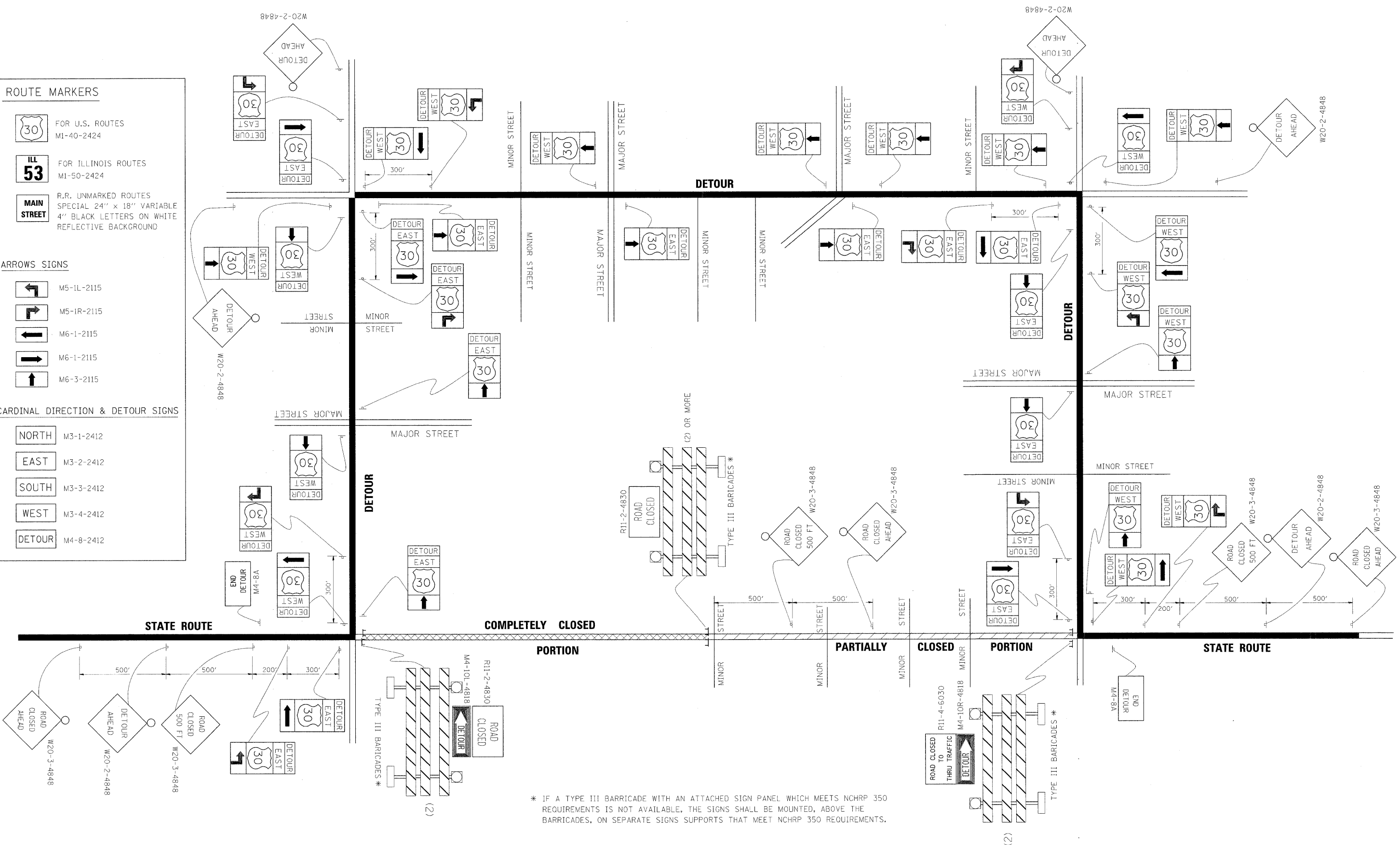
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



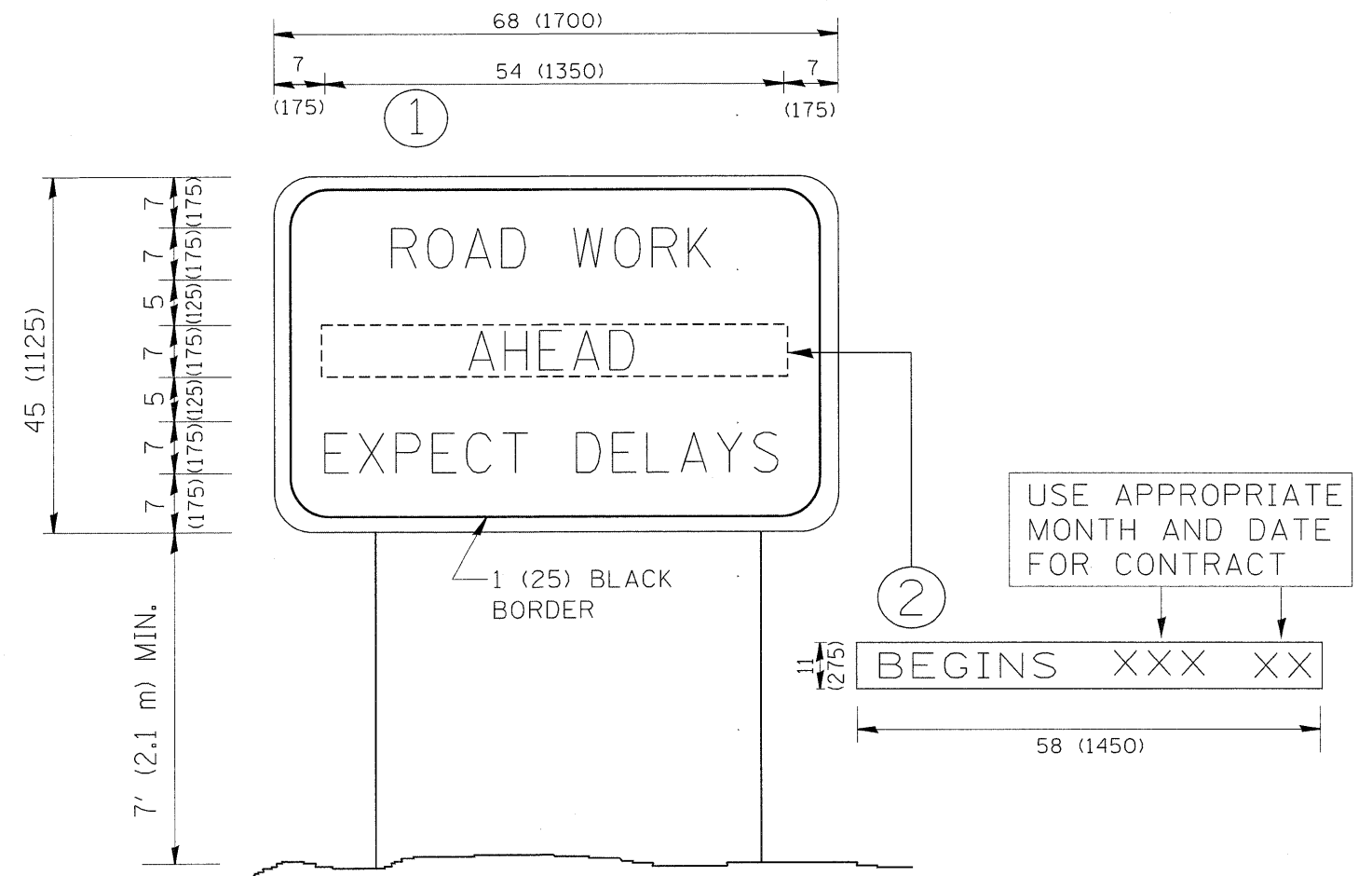
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = drivekosgn	DESIGNED -	REVISED - 10-18-02
ct:\p\work\p\DOT\DRIVAKOSGN\d0189315\21.dgn		DRAWN -	REVISED - R. BORO 09-14-09
	PLOT SCALE - 49.9999" / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	0101.8B	COOK	139	135
TC-21			CONTRACT NO. 60C18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

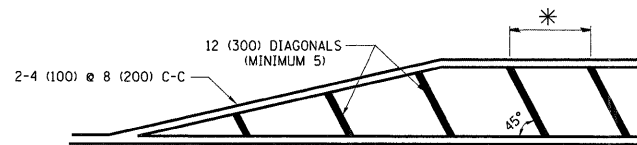
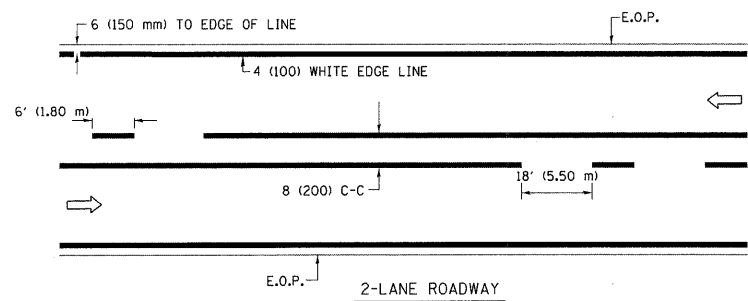


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

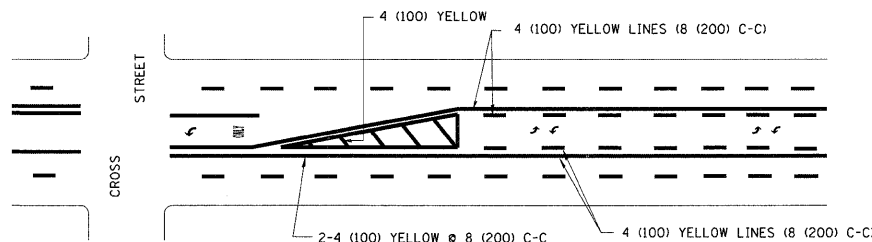
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		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22		CONTRACT NO. 60C18	
		PLOT SCALE = 50.000' / IN.	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		PLOT DATE = 1/4/2008	REVISED - C. JUCIUS 01-31-07									



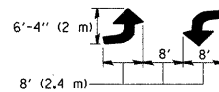
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

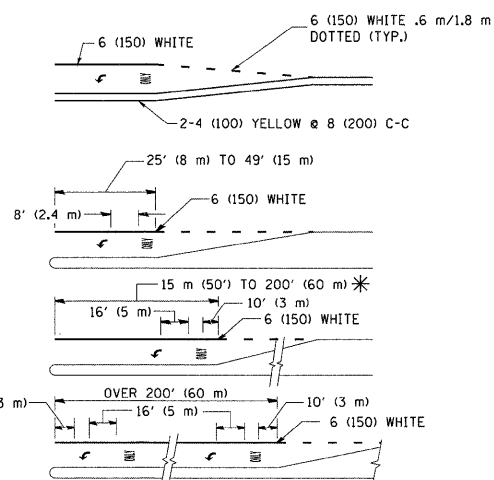


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

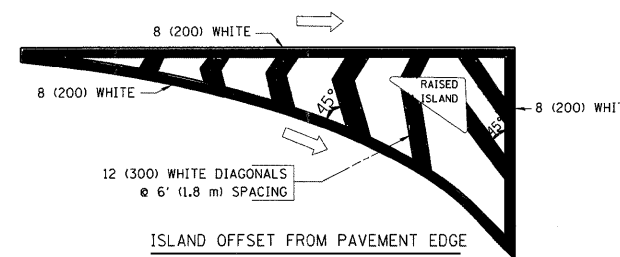


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

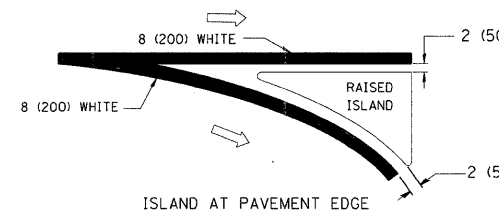
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

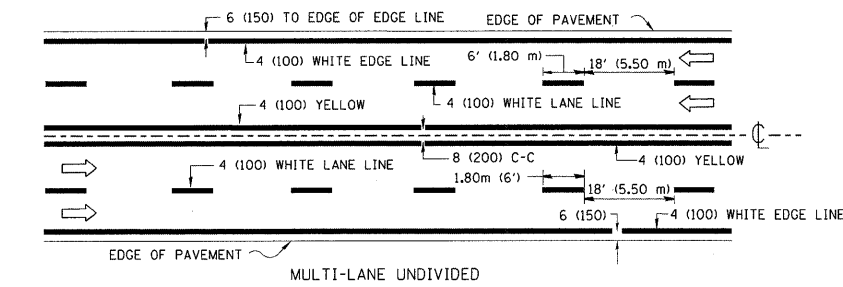


ISLAND OFFSET FROM PAVEMENT EDGE

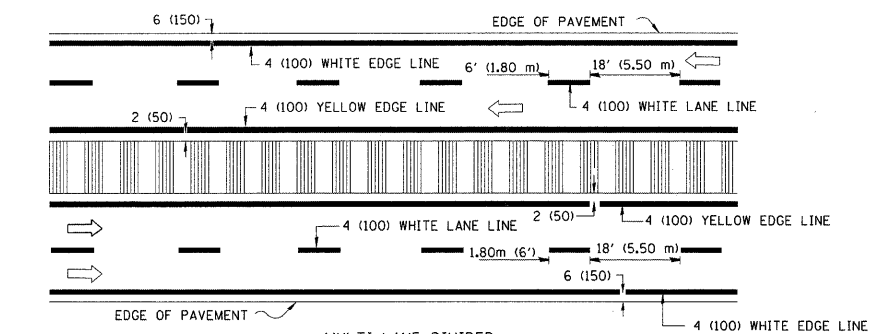


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



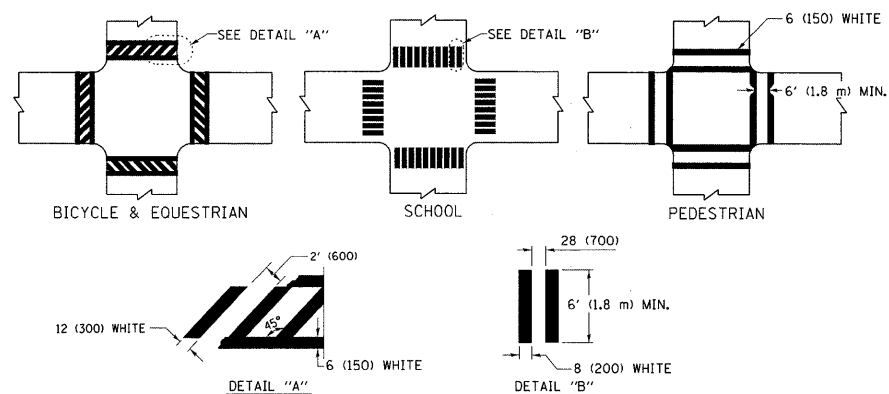
MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 8 (200) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2'-4" (700) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GOPE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' 6" (4.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

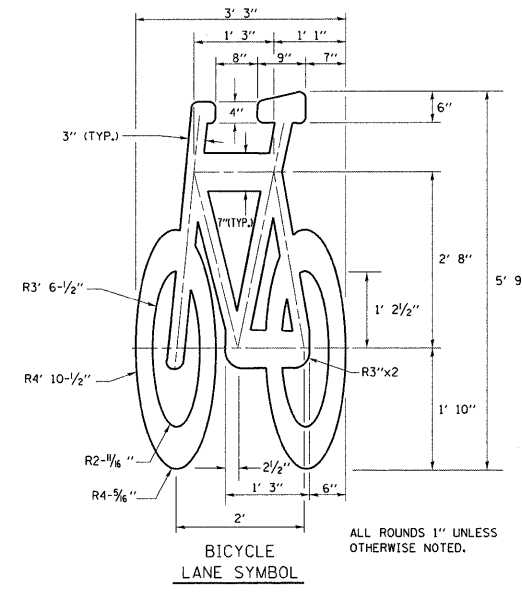
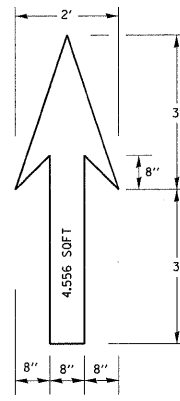
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	PLOT SCALE = 5/8" = 1' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS**

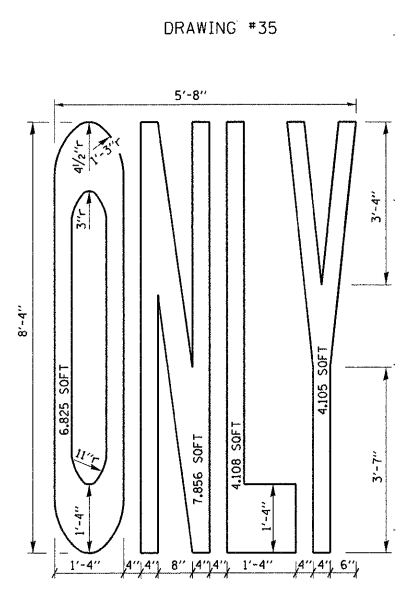
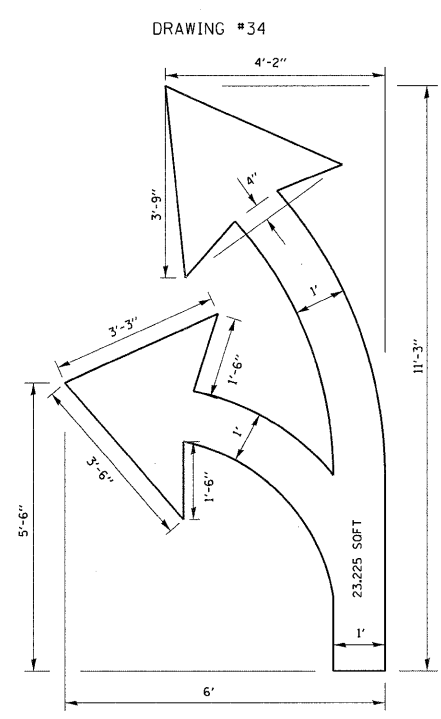
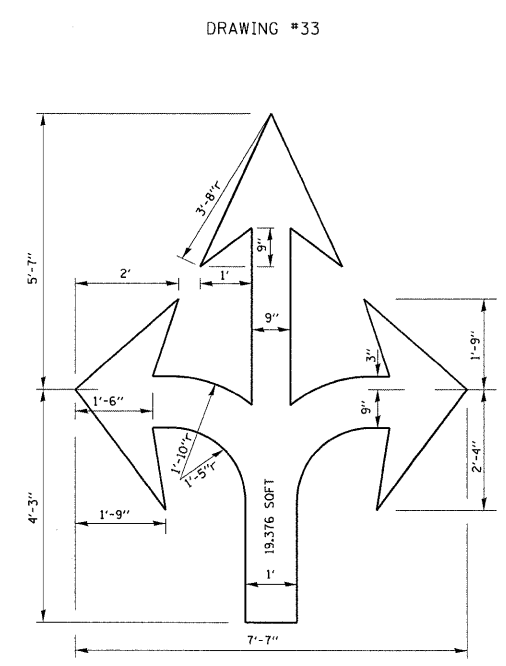
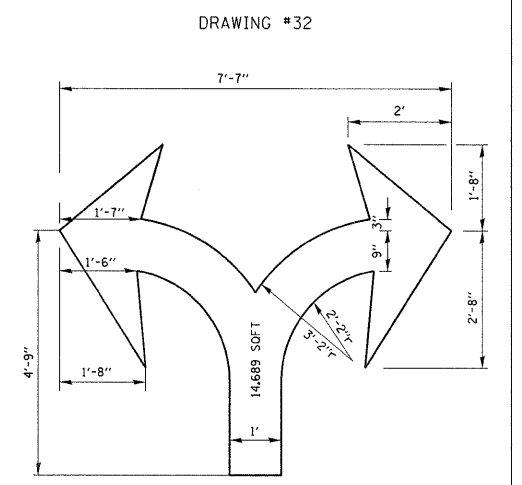
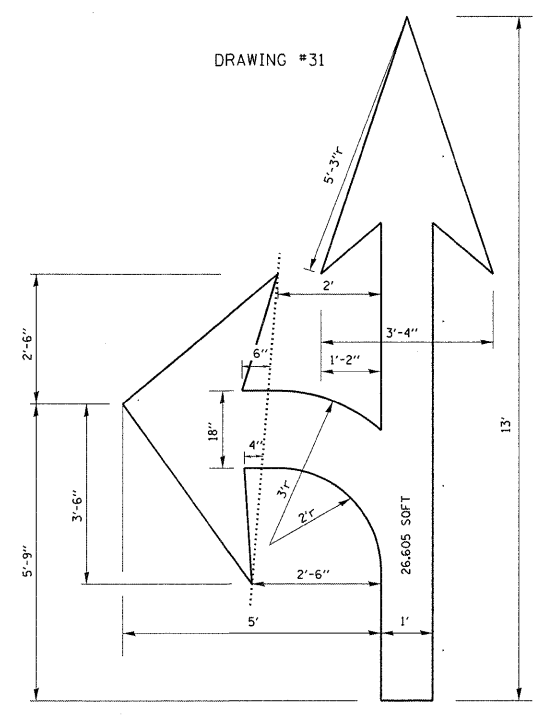
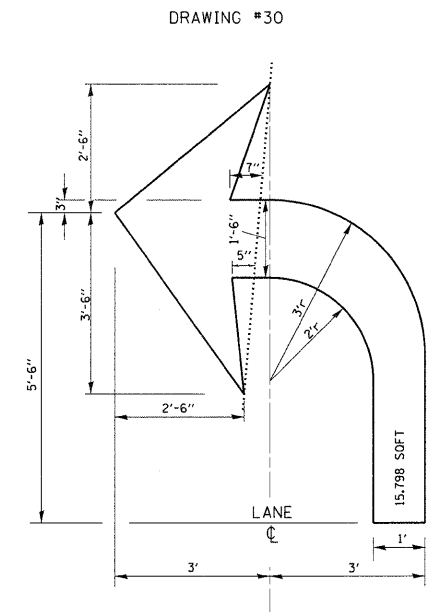
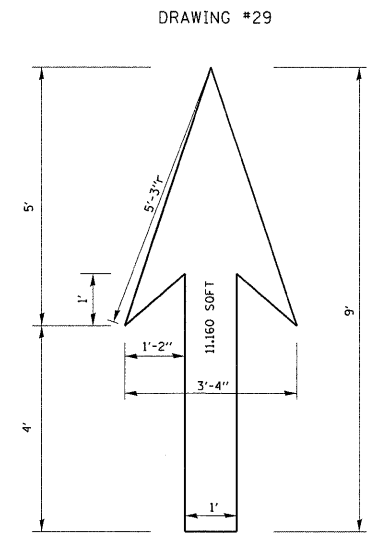
SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 137
TC-24			CONTRACT NO. 60C18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

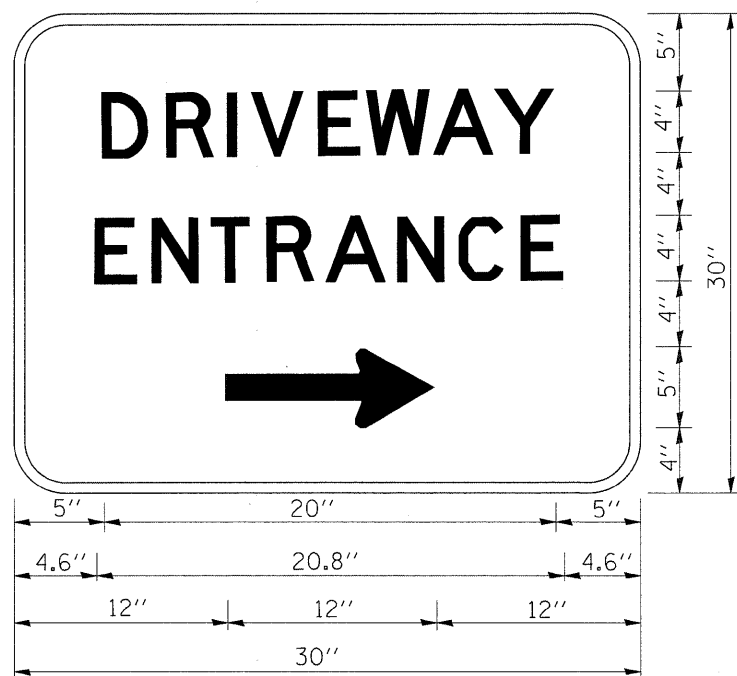
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		DRAWN - -	REVISED - -
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	PLOT DATE = 1/4/2008	DATE - -	REVISED - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 138
TC-24		CONTRACT NO. 60C18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\to26.dgn	USER NAME = gegljanobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING				F.A.I. RTE. 94	SECTION 0101.8B	COUNTY COOK	TOTAL SHEETS 139	SHEET NO. 139	
		DRAWN -	REVISED -						TC-26		CONTRACT NO. 60C18			
		CHECKED -	REVISED -		SCALE: NONE				SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
		DATE -	REVISED -											