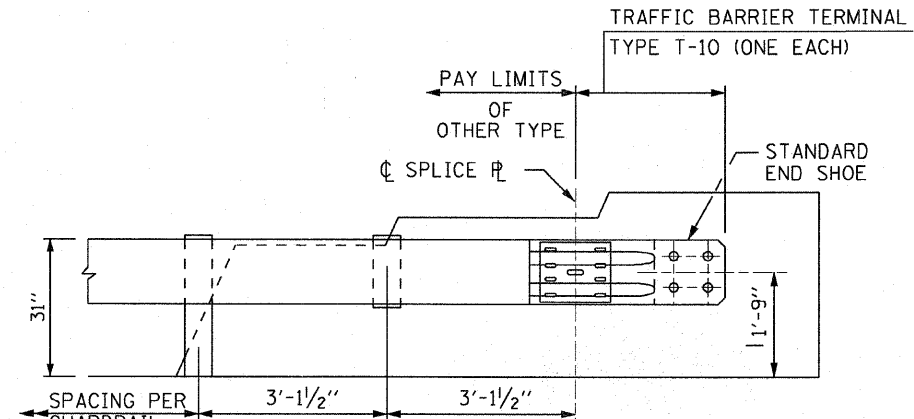
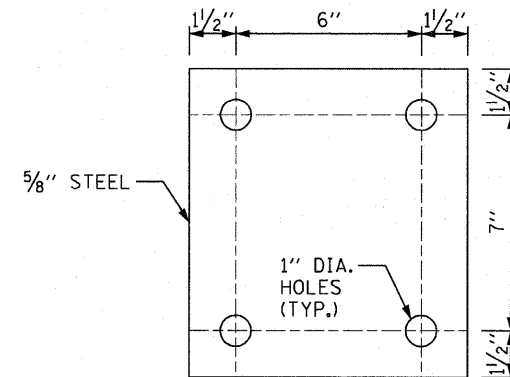


ELEVATION

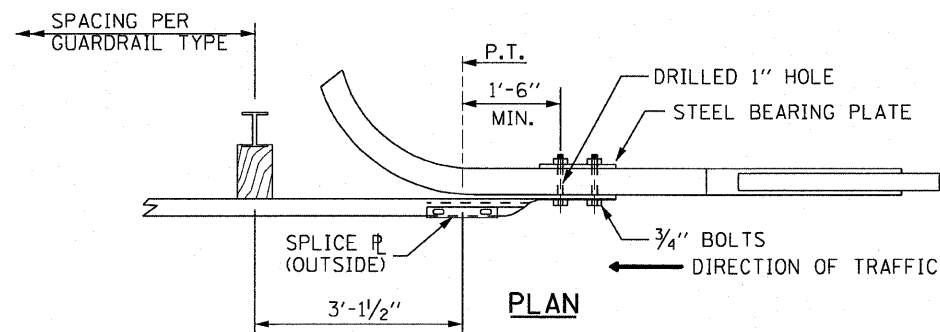


ELEVATION

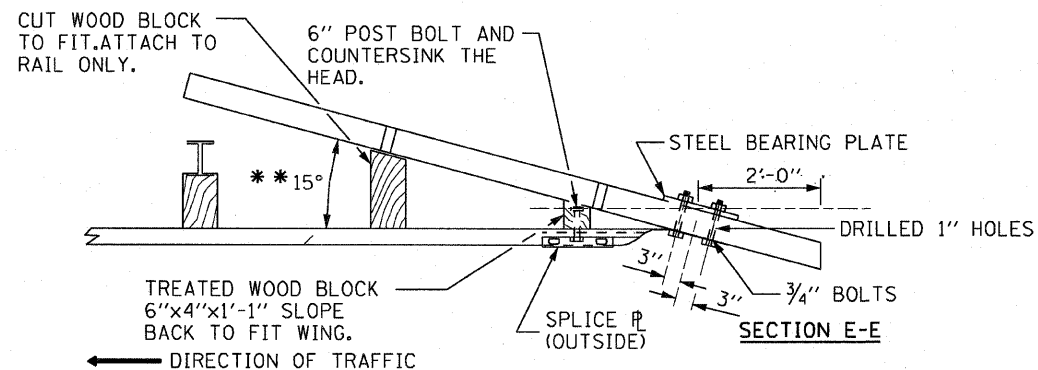


PARAPET STEEL BEARING  
PLATE DETAIL

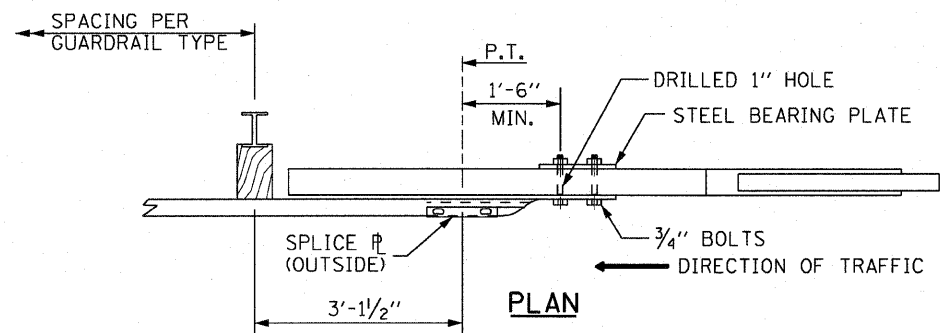
(4 EACH INDIVIDUAL 5"x5"x5/8" STEEL  
PLATES WITH CENTERED HOLES MAYBE  
SUBSTITUTED FOR THE PLATE SHOWN)



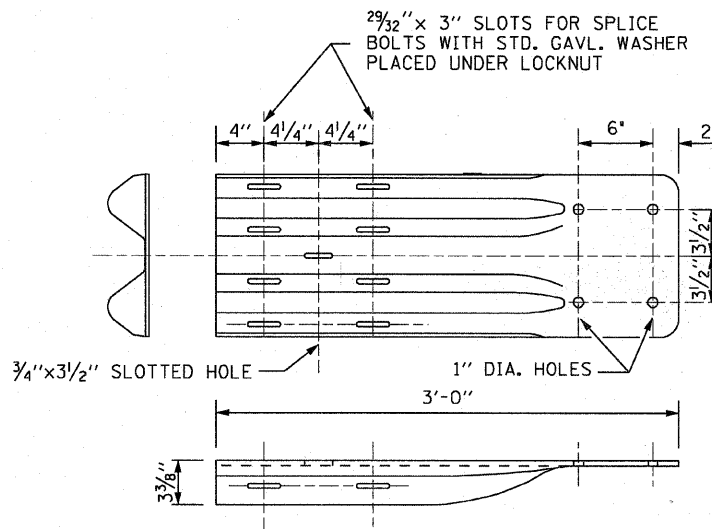
PLAN  
CURVED WING



PLAN  
FLARED WING



PLAN  
TANGENT WING



END SHOE

GENERAL NOTES:

- \* TAPER GUARDRAIL TO 2'-7/8" AT THIRD POST.
- \*\* OR TO BE DETERMINED IN THE FIELD.

NOTES:


1. SEE STANDARD C1 FOR DETAILS OF GUARDRAIL NOT SHOWN.
2. THE TYPE T10 TERMINAL IS TYPICALLY UTILIZED TO CONNECT GALVANIZED STEEL PLATE BEAM GUARDRAIL TO THE DEPARTING END OF AN EXISTING BRIDGE CONCRETE WING WALL.
3. UNDER NO CIRCUMSTANCES SHALL AN EXISTING TERMINAL, THAT WAS DESIGNED USING A PREVIOUS STANDARD, BE ATTACHED TO OR MODIFIED IN ANYWAY FROM ITS ORIGINAL DESIGN. IF ANY MODIFICATION IS REQUIRED AND A PROPER BARRIER WARRANT HAS BEEN COMPLETED, THE ENTIRE BARRIER INSTALLATION SHALL BE COMPLETELY REMOVED AND REPLACED WITH A NEW SYSTEM THAT CONFORMS TO THE CURRENT STANDARD.
4. TRAFFIC BARRIER TERMINAL SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S DETAILS AND SPECIFICATIONS.
5. WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.
6. THE TERMINAL SYSTEM HAS BEEN PERFORMANCE-TESTED FOR CRASHWORTHINESS UNDER PROCEDURES DEFINED IN THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350. NO MODIFICATION TO THIS STANDARD DRAWING SHALL BE PERMITTED.



DATE	REVISIONS
3-1-2010	REVISED NOTES, ADDED END SHOE AND PARAPET BEARING PLATE DETAIL.

TRAFFIC BARRIER  
TERMINAL, TYPE T10

STANDARD C11-01

APPROVED.....  
  
 CHIEF ENGINEER DATE 7-1-2009