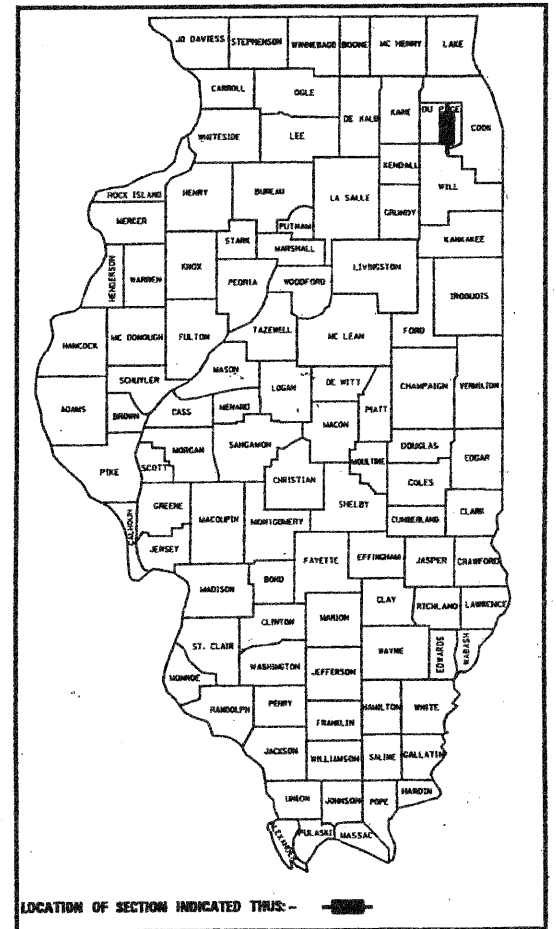


CONTRACT NO. 63348

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 3573 (WOODRIDGE DRIVE)
HOBSON ROAD TO CENTER DRIVE
SECTION: 09-00064-00-RS
PROJECT NO: ARA-9003 (398)
RESURFACING
JOB NO: C-91-787-09
VILLAGE OF WOODRIDGE
DuPAGE COUNTY



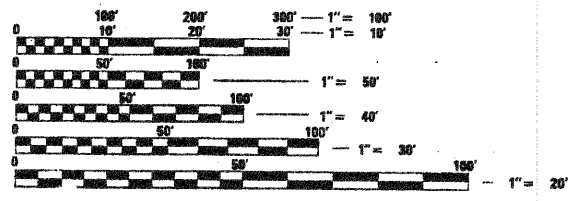
SHEET #	DESCRIPTION
1	TITLE
2	GENERAL NOTES AND STANDARDS
3	SUMMARY OF QUANTITIES
4	LOCATION MAP AND TERMINI DETAIL
5	EXISTING TYPICAL CROSS SECTIONS
6	PROPOSED TYPICAL CROSS SECTIONS
7	SCHEDULE OF QUANTITIES
8	SCHEDULE OF QUANTITIES
9	RESURFACING SCHEDULE
10	PAVEMENT MARKING PLAN
11	TC 10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (DISTRICT 1)
12	TC 13 TYPICAL PAVEMENT MARKINGS (DISTRICT 1)
13	BD 24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (DISTRICT 1)
14	BD 32 BUTT JOINT AND HMA TAPER DETAILS (DISTRICT 1)
15	TS 07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (DISTRICT 1)

HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGE OF WOODRIDGE

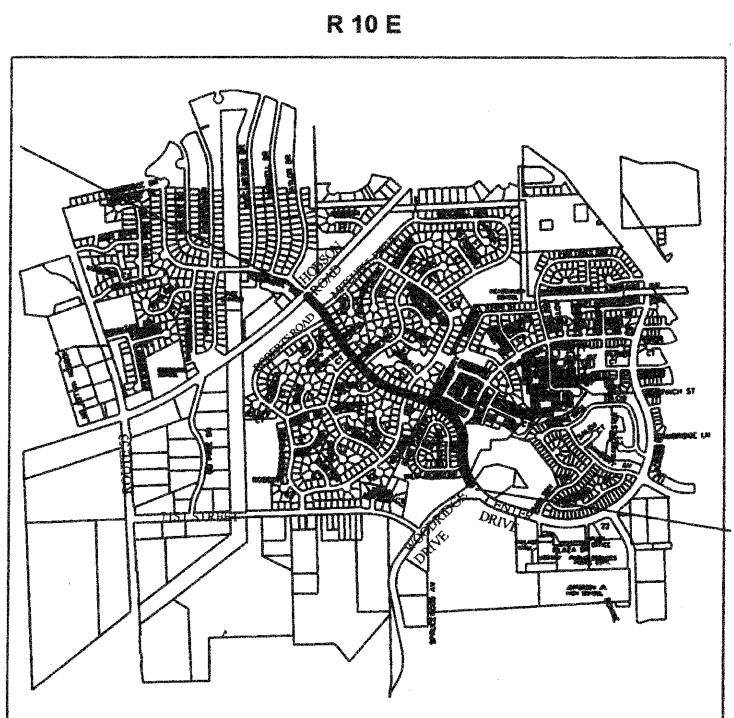
TRAFFIC INFORMATION

CLASSIFICATION	URBAN COLLECTOR
DESIGN SPEED =	25 MPH
POSTED SPEED =	25 MPH
ADT (2009) =	7,750



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

BEGIN IMPROVEMENTS
STA. 1+00



LOCATION MAP IS NOT TO SCALE

LISLE TOWNSHIP

GROSS LENGTH = 3,512 FEET (.665 mi)
NET LENGTH = 3,512 FEET (.665 mi)

END IMPROVEMENTS
STA. 36+12



Thomas Adomski
License Expires: 11/30/2011
Lic. 062-045065

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS
Approved <u>July 2</u> 20 <u>10</u> VILLAGE OF WOODRIDGE, DIRECTOR OF PUBLIC WORKS
Passed <u>July 12</u> 20 <u>10</u> DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
Releasing for Bid Based on Limited Review <u>July 15</u> 20 <u>10</u> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PLANS PREPARED BY:
THE VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS

PLANS REVISED PER IDOT REVIEW 7/2/10
PLANS REVISED PER IDOT REVIEW 10/12/09
PLANS REVISED PER IDOT REVIEW 8/4/09
PLANS PREPARED 6/6/09

J.U.L.I.E
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

CONTRACT NO. 63348

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406, SCHAMBERG, IL

GENERAL NOTES

CONTRACT NO. 63348

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS:

ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS ", ADOPTED JANUARY 1, 2010; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS, THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PROJECT COORDINATION:

THE CONTRACTOR SHALL NOTIFY THE VILLAGE ENGINEER AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. ATTENTION IS CALLED TO SECTION 701 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION. THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.

SAFETY:

ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR FLORESCENT ORANGE, FLUORESCENT YELLOW/GREEN, OR A COMBINATION OF FLORESCENT ORANGE AND FLUORESCENT GREEN/YELLOW SAFETY VESTS MEETING THE REQUIREMENTS OF THE STANDARDS INSTITUTE SPECIFICATION ANSI/ISEA 107-1999 FOR THE CONSPICUITY CLASS 2 GARMENTS AT ALL TIMES WHILE ON THE CONSTRUCTION SITE.

TRAFFIC CONTROL AND MAINTENANCE:

THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT ONLY ONE TEMPORARY LANE CLOSURE ON ONE STREET IN ONE DIRECTION IS IMPLEMENTED AT A TIME. A LANE CLOSURE WILL ONLY BE PERMITTED DURING CONSTRUCTION OPERATIONS AND IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARDS. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN ALL SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES, INCLUDING FLAGGERS, REQUIRED TO MAINTAIN TRAFFIC FLOW.

ACCESS TO ABUTTING PROPERTY SHALL BE PROVIDED AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT. TIME REQUIRED FOR CONSTRUCTION AT DRIVEWAYS SHALL BE LIMITED TO THE MINIMUM TIME REQUIRED FOR SAID CONSTRUCTION.

SAW CUTTING FOR REMOVAL:

THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUT AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL CONCRETE CURB OR COMBINATION CONCRETE CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS AS SPECIFIED, OR AS DIRECTED BY THE ENGINEER. THE COST SHALL BE CONSIDERED INCLUDED IN THE COST FOR REMOVAL OF THE SPECIFIED ITEM IN THE CONTRACT.

BASE COURSE CLEANING:

PRIOR TO APPLYING THE HOT-MIX ASPHALT PRIME COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE GRINDINGS, LEAVES, OF ALL DUST, DIRT, WEEDS AND OTHER FOREIGN MATERIALS. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE. COST TO BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT SURFACE WORK.

CLEAN-UP AND DISPOSAL:

THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER. DEBRIS AND ANY SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF-SITE.

CONSTRUCTION LIMITS:

THE CONTRACTOR SHALL CONFINE OPERATIONS WITHIN THE CONSTRUCTION LIMITS OF THE PROJECT. ANY DAMAGE INSIDE AND OUTSIDE OF SAID CONSTRUCTION LIMITS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

PROPERTY ACCESS:

DURING CONSTRUCTION, ALL SIGNS, BARRICADES, AND TEMPORARY PAVEMENT MARKINGS WILL CONFORM TO THE ILLINOIS MUTCD. VEHICULAR ACCESS TO LOCAL PROPERTIES WILL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION, EXCEPT WHEN CONSTRUCTION OPERATIONS OCCUR DIRECTLY ON OR IN FRONT OF DRIVEWAYS. IN THIS CASE, FLAG PERSONS WILL BE USED TO DIRECT AND CONTROL TRAFFIC.

CURB AND GUTTER REPLACEMENT:

THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10" UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.

HIGHWAY STANDARDS

STANDARD #	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-06	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
701006-03	OFF-ROAD OPERATIONS 2L, 2W, 15' TO 24' FROM PAVEMENT EDGE
701011-02	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

VILLAGE OF WOODRIDGE PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

**WOODRIDGE DRIVE
REHABILITATION PROJECT**

GENERAL NOTES AND STANDARDS

SUMMARY OF QUANTITIES

CONTRACT NO. 63348

CODE NO	ITEM	UNIT	TOTAL QUANTITY
			CONSTRUCTION CODE 1000
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	30
20042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	30
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	560
25000110	SEEDING, CLASS 1A	ACRE	0.2
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	18
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	18
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	18
35501287	HOT-MIX ASPHALT BASE COURSE, 2 1/4"	SQ YD	200
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3421
40600300	AGGREGATE (PRIME COAT)	TON	51
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	958
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	547
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	22
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1676
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5462
42400800	DETECTABLE WARNINGS	SQ FT	48
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	17106
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	200
44000600	SIDEWALK REMOVAL	SQ FT	5462
20004542	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2208
44201705	CLASS D PATCHES, TYPE II, 5 INCH	SQ YD	25

CODE NO	ITEM	UNIT	TOTAL QUANTITY
			CONSTRUCTION CODE 1000
44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	50
44201711	CLASS D PATCHES, TYPE IV, 5 INCH	SQ YD	200
60255500	MANHOLES TO BE ADJUSTED	EACH	2
60262700	INLETS TO BE RECONSTRUCTED	EACH	2
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	500
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	104
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1300
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	48
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	200
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	140
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3930
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1556
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	932
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	220
88600100	DETECTOR LOOP, TYPE I	FOOT	562

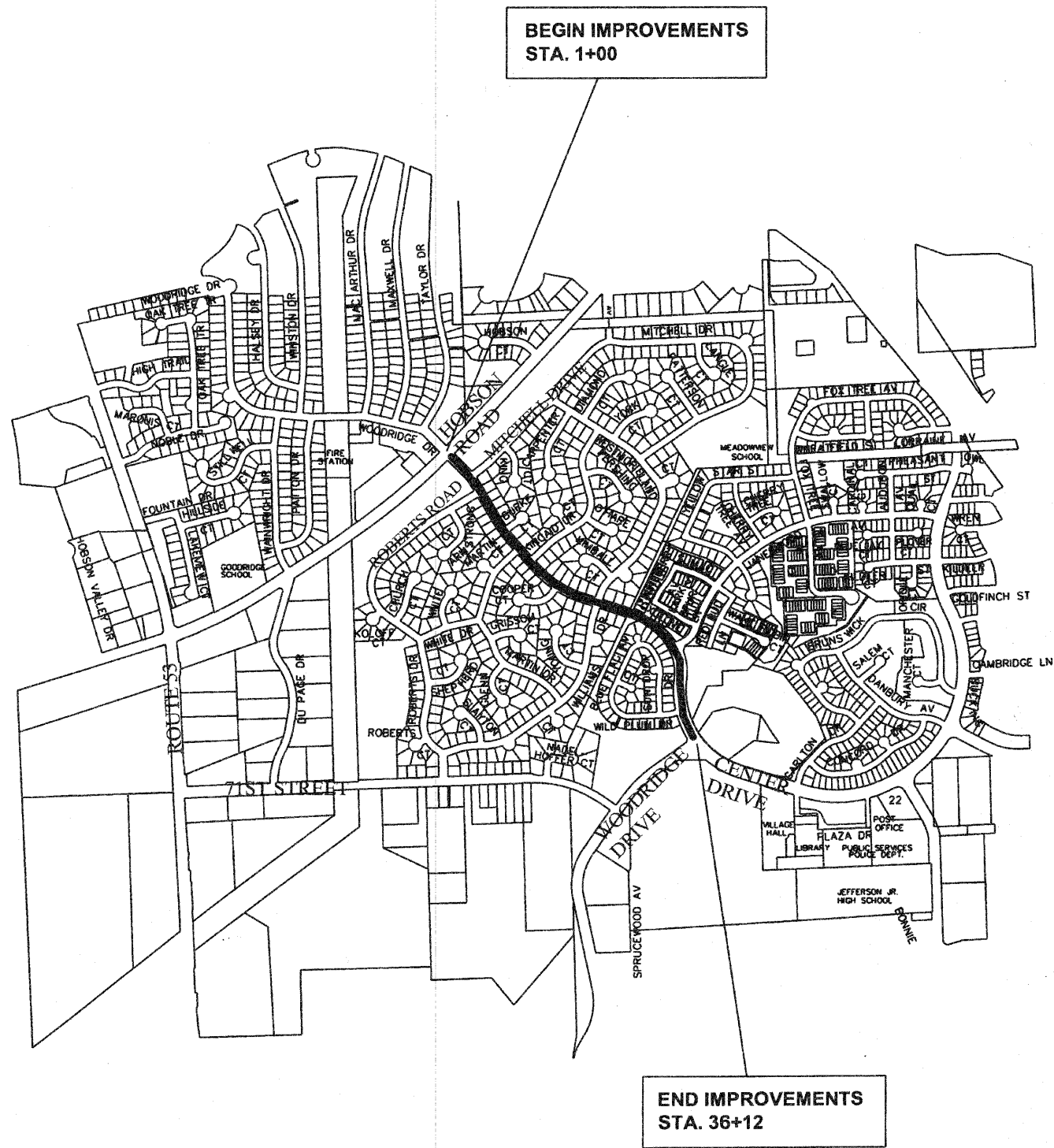
* DENOTES SPECIALTY ITEM(S)

VILLAGE OF WOODRIDGE PUBLIC WORKS 1 Plaza Drive, Woodridge IL 60517 (630) 719-4753
WOODRIDGE DRIVE REHABILITATION PROJECT
SUMMARY OF QUANTITIES
DRAWN BY: AF CHECKED BY: SM

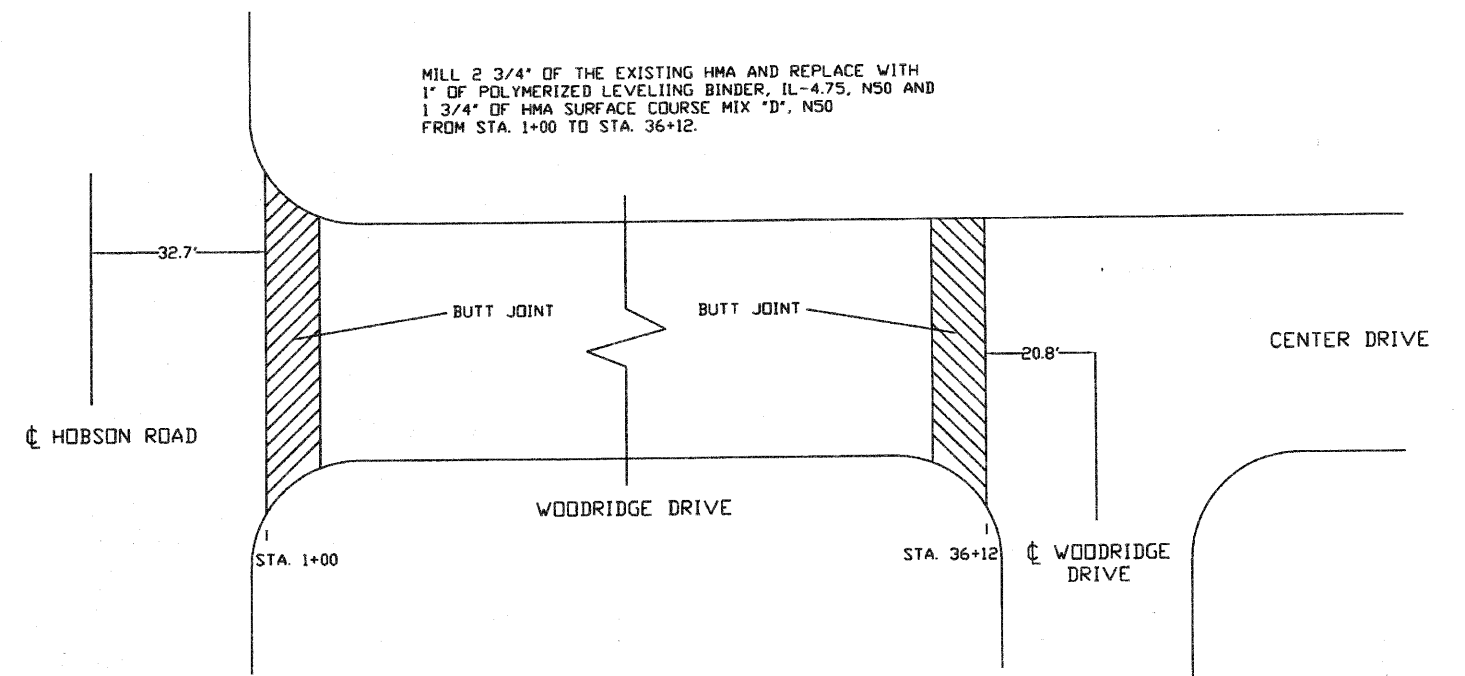
SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
09-00064-00-RS	DuPAGE	15	4

CONTRACT NO. 63348

LOCATION MAP



TERMINI DETAIL



MAPS ARE NOT TO SCALE

VILLAGE OF WOODRIDGE PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

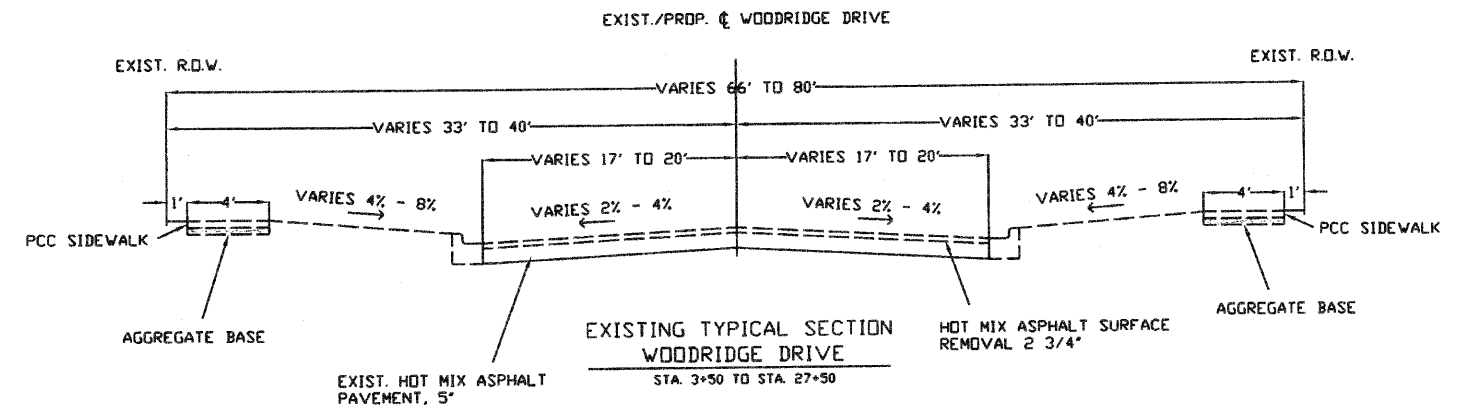
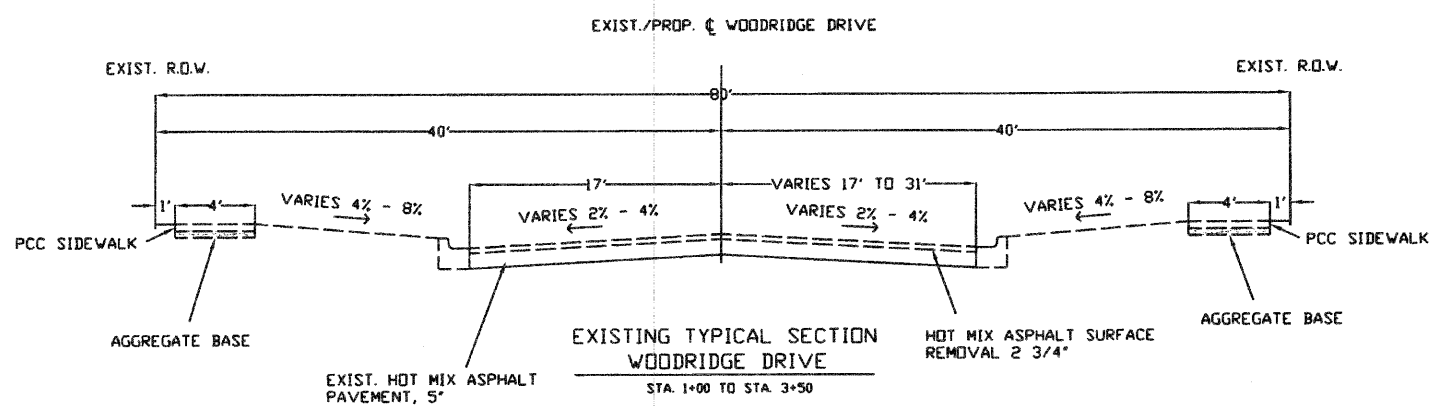
WOODRIDGE DRIVE
REHABILITATION PROJECT

LOCATION MAP AND TERMINI DETAIL

DRAWN BY: AF

CHECKED BY: SM

CONTRACT NO. 63348



CONTRACTOR SHALL MILL BEFORE PATCHING

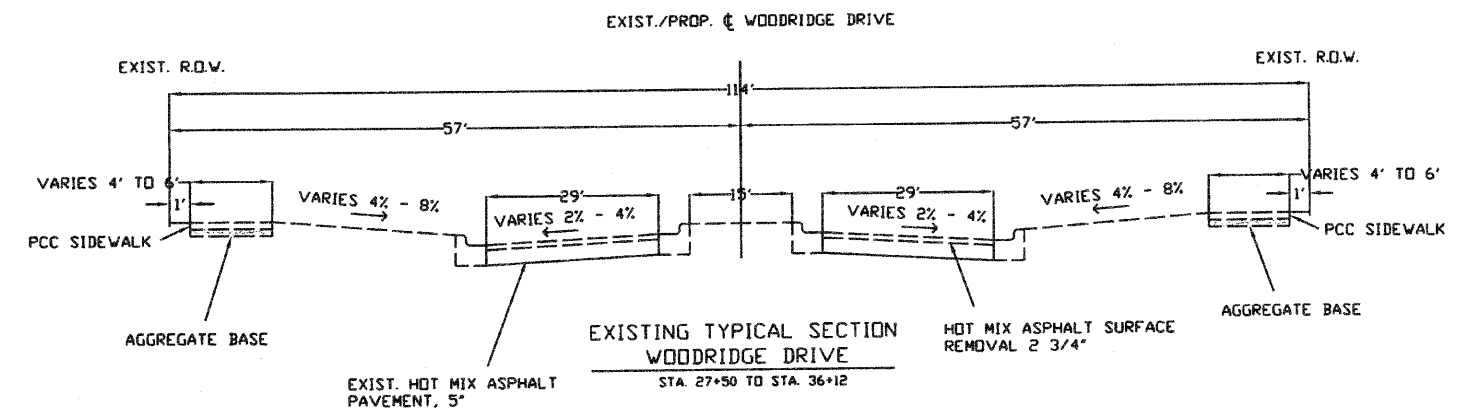
HOT-MIX ASPHALT MIXTURE REQUIREMENTS:

MIXTURE TYPE	AIR VOIDS @Ndes
STREETS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 1 3/4"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 1"	4% @ 50 Gyr.
PATCHING CLASS D PATCHES (HMA BINDER IL-19mm) 5"	4% @ 70 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm) 2 1/4"	4% @ 50 Gyr.
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm) 2"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



CROSS SECTIONS ARE NOT TO SCALE

VILLAGE OF WOODRIDGE PUBLIC WORKS
 1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

**WOODRIDGE DRIVE
 REHABILITATION PROJECT**

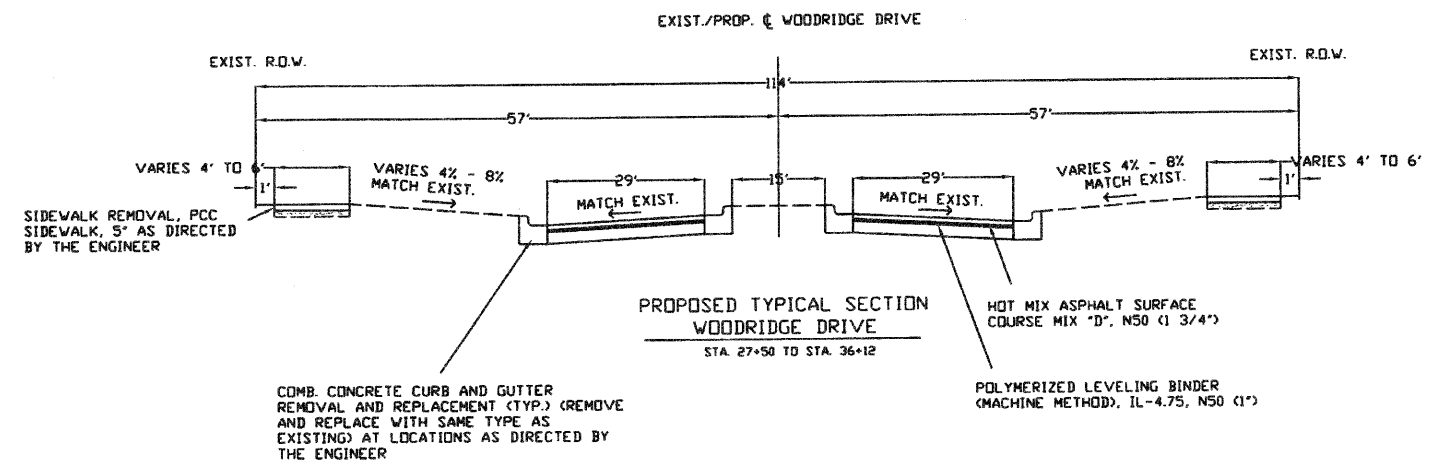
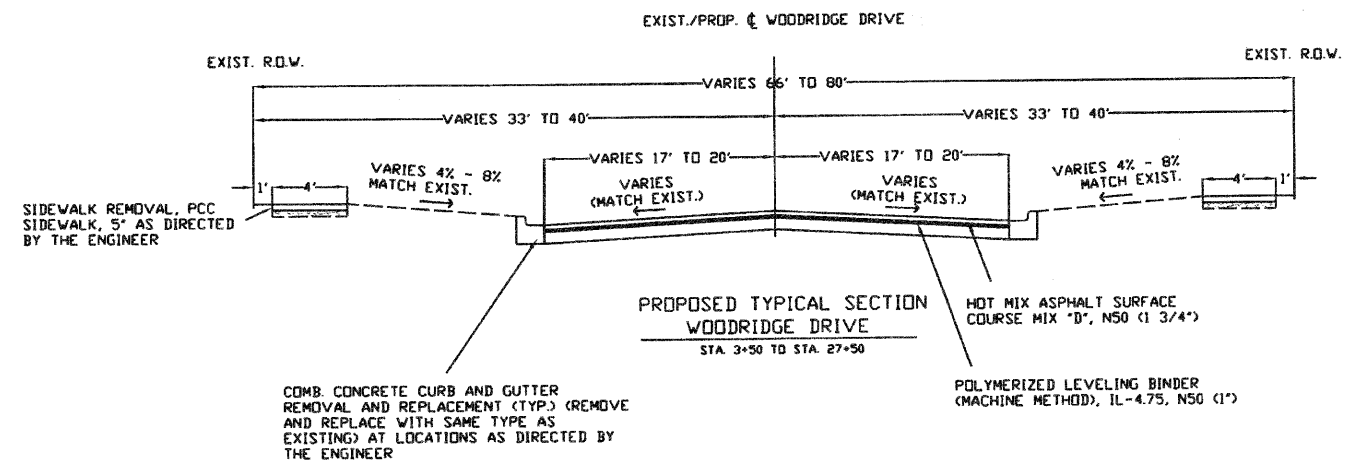
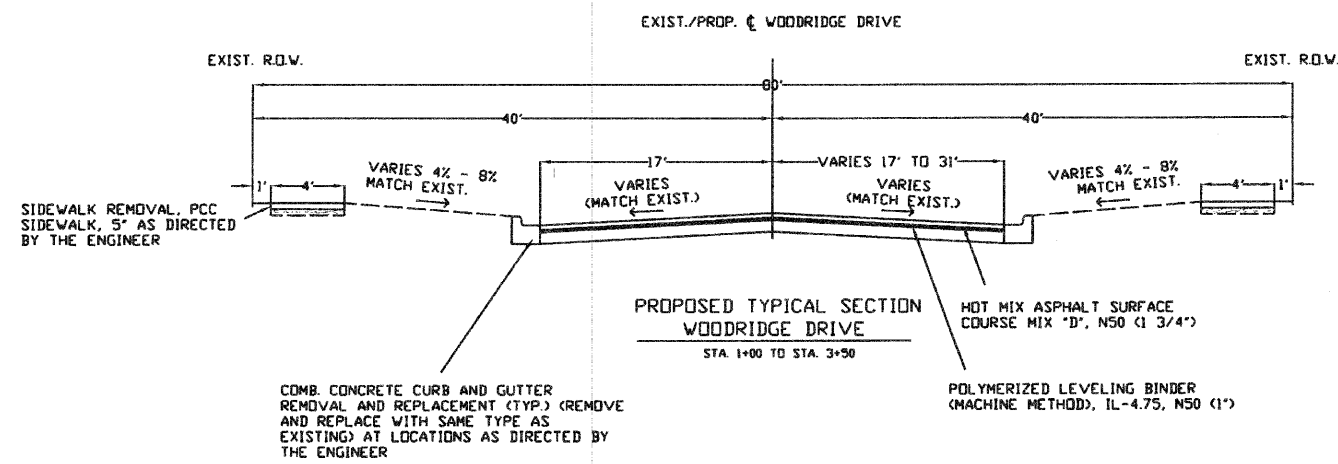
EXISTING TYPICAL CROSS SECTIONS

DRAWN BY: AF

CHECKED BY: SM

SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
09-00064-00-RS	DuPAGE	15	6

CONTRACT NO. 63348



CROSS SECTIONS ARE NOT TO SCALE

VILLAGE OF WOODRIDGE PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

WOODRIDGE DRIVE
REHABILITATION PROJECT

PROPOSED TYPICAL CROSS SECTIONS

DRAWN BY: AF

CHECKED BY: SM

CONTRACT NO. 63348

SIDEWALK REMOVAL AND REPLACEMENT SCHEDULE

STA	SIDE	SIDEWALK REMOVAL WIDTH (FT)	SIDEWALK REMOVAL LENGTH (FT)	44000600 SIDEWALK REMOVAL (SQ. FT)	PCC SIDEWALK WIDTH (FT)	PCC SIDEWALK LENGTH (FT)	42400200 PCC SIDEWALK, 5" (SQ. FT)	DETECTABLE WARNINGS WIDTH (FT)	DETECTABLE WARNINGS LENGTH (FT)	42400800 DETECTABLE WARNINGS (SQ. FT)
1+00	LT	4	45	180	4	45	180	2	4	8
1+10	RT	5	16	80	5	16	80			
1+47	RT	4	5	20	4	5	20			
3+10	LT	4	6	24	4	6	24			
3+40	LT	4	15	60	4	15	60			
4+00	RT	4	20	80	4	20	80			
4+00	LT	4	9	36	4	9	36			
4+75	LT	4	38	152	4	38	152			
5+00	RT	4	10	40	4	10	40			
5+50	RT	4	4	16	4	4	16			
6+00	RT	4	5	20	4	5	20			
6+00	LT	4	13	52	4	13	52			
6+50	LT	4	5	20	4	5	20			
6+90	LT	4	5	20	4	5	20			
7+10	RT	4	16	64	4	16	64			
7+55	RT	5	4	20	5	4	20			
7+60	LT	4	20	80	4	20	80			
8+10	LT	4	10	40	4	10	40			
8+60	RT	4	20	80	4	20	80			
9+00	LT	4	12	48	4	12	48			
9+50	RT	4	5	20	4	5	20			
9+50	LT	4	5	20	4	5	20			
9+70	RT	4	5	20	4	5	20			
10+00	RT	4	12	48	4	12	48			
10+20	LT	4	10	40	4	10	40			
10+40	RT	4	15	60	4	15	60			
10+50	LT	4	15	60	4	15	60			
10+60	RT	4	10	40	4	10	40			
11+00	RT	4	15	60	4	15	60	2	4	8
11+10	LT	4	10	40	4	10	40			
11+50	RT	4	8	32	4	8	32			
12+00	RT	4	10	40	4	10	40			
12+50	RT	4	10	40	4	10	40			
12+50	LT	4	10	40	4	10	40			
13+00	RT	4	5	20	4	5	20			
13+00	LT	4	4	16	4	4	16			
13+18	RT	4	15	60	4	15	60			
13+70	LT	4	5	20	4	5	20	2	4	8
15+00	RT	4	9	36	4	9	36			
15+10	LT	4	16	64	4	16	64			
15+50	LT	4	9	36	4	9	36			
15+60	RT	4	13	52	4	13	52			
16+10	LT	4	20	80	4	20	80			
16+15	RT	4	10	40	4	10	40			
16+60	LT	4	9	36	4	9	36			

STA	SIDE	SIDEWALK REMOVAL WIDTH (FT)	SIDEWALK REMOVAL LENGTH (FT)	44000600 SIDEWALK REMOVAL (SQ. FT)	PCC SIDEWALK WIDTH (FT)	PCC SIDEWALK LENGTH (FT)	42400200 PCC SIDEWALK, 5" (SQ. FT)	DETECTABLE WARNINGS WIDTH (FT)	DETECTABLE WARNINGS LENGTH (FT)	42400800 DETECTABLE WARNINGS (SQ. FT)
16+80	LT	4	5	20	4	5	20			
17+20	RT	4	41	164	4	41	164			
17+40	LT	4	15	60	4	15	60			
17+80	LT	4	40	160	4	40	160			
18+50	RT	4	10	40	4	10	40			
18+50	LT	4	6	24	4	6	24			
18+80	RT	4	35	140	4	35	140			
19+40	LT	4	25	100	4	25	100			
19+50	RT	4	5	20	4	5	20			
20+05	RT	4	9	36	4	9	36			
20+10	LT	4	5	20	4	5	20			
20+40	RT	4	10	40	4	10	40			
20+90	RT	4	5	20	4	5	20			
21+05	LT	4	10	40	4	10	40			
21+50	RT	4	15	60	4	15	60			
21+60	LT	4	15	60	4	15	60			
22+00	RT	4	5	20	4	5	20	2	4	8
22+40	LT	4	17	68	4	17	68			
23+00	LT	4	11	44	4	11	44			
23+50	RT	6	49	294	6	49	294			
24+55	RT	6	5	30	6	5	30			
24+70	LT	4	14	56	4	14	56			
25+50	RT	6	50	300	6	50	300			
26+70	LT	4	32	128	4	32	128			
27+15	LT	4	5	20	4	5	20			
27+50	LT	4	10	40	4	10	40			
28+10	RT	5	6	30	5	6	30			
28+50	LT	5	6	30	5	6	30			
29+00	LT	5	6	30	5	6	30			
29+50	RT	6	15	90	6	15	90			
29+55	LT	5	6	30	5	6	30			
31+00	RT	6	15	90	6	15	90			
32+00	RT	5	6	30	5	6	30			
32+50	RT	6	10	60	6	10	60			
33+00	RT	5	6	30	5	6	30			
33+00	LT	5	6	30	5	6	30			
33+10	LT	6	20	120	6	20	120			
33+40	LT	9	19	171	9	19	171			
33+60	RT	5	6	30	5	6	30			
34+00	RT	14	7	98	14	7	98			
34+10	RT	7	9	63	7	9	63			
35+40	RT	8	10	80	8	10	80			
35+65	LT	6	14	84	6	14	84			
35+90	RT	30	7	210	30	7	210	2	8	16
				5462			5462			48
				SF			SF			SF

DRIVEWAY SCHEDULE						
LOCATION	SIDE	LENGTH (FT)	WIDTH (FT)	44000200 DRIVEWAY PAVEMENT REMOVAL (SQ. YD.)	35501287 HOT-MIX ASPHALT BASE COURSE, 2 1/4" (SQ. YD.)	40603310 HMA SURFACE COURSE, MIX "C", N50 (TON)
10+49	RT	8	20	18	18	2.0
12+39	RT	8	20	18	18	2.0
14+70	LT	2	20	4	4	0.5
14+90	LT	2	22	5	5	0.5
15+32	RT	6	20	13	13	1.5
18+61	LT	4	20	9	9	1.0
18+70	RT	2	15	3	3	0.4
19+77	RT	3	13	4	4	0.5
20+50	LT	2	21	5	5	0.5
20+58	RT	4	19	8	8	0.9
20+70	LT	2	22	5	5	0.5
21+50	RT	6	20	13	13	1.5
21+60	LT	2	22	5	5	0.5
21+85	LT	2	21	5	5	0.5
VARIOUS LOCATIONS BETWEEN STA. 1+75 TO 26+50				84	84	9.4
				200	200	22
				SQ YD	SQ YD	TON

VILLAGE OF WOODRIDGE PUBLIC WORKS
 1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

WOODRIDGE DRIVE REHABILITATION PROJECT

SCHEDULE OF QUANTITIES

DRAWN BY: AF CHECKED BY: SM

CONTRACT NO. 63348

CURB AND GUTTER REMOVAL AND REPLACEMENT SCHEDULE					
STA.	SIDE	44001700 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REP. LENGTH (FT)	STA.	SIDE	44001700 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REP. LENGTH (FT)
1+50	RT	32	22+15	RT	30
2+00	RT	18	22+50	RT	20
3+00	RT	14	22+70	LT	7
3+20	RT	5	23+00	RT	13
3+50	RT	54	23+50	RT	7
4+00	LT	21	25+00	LT	15
4+45	LT	17	25+30	RT	22
4+50	RT	5	25+30	LT	15
4+80	RT	8	25+50	LT	12
5+40	LT	30	25+70	RT	18
5+60	RT	8	25+70	LT	5
6+00	LT	20	26+15	LT	16
6+30	LT	8	26+40	RT	5
7+40	LT	8	26+60	RT	5
7+50	LT	23	27+00	RT	67
8+15	RT	22	27+50	LT	33
8+40	LT	10	27+70	RT	5
8+55	LT	12	27+90	RT	5
9+00	RT	10	28+10	LT	16
10+00	LT	32	28+15	RT	5
10+50	RT	27	28+50	RT	15
11+50	LT	22	28+80	LT	5
12+00	RT	5	29+00	RT	10
12+00	LT	10	29+20	RT	15
12+40	RT	27	29+50	RT	5
13+00	RT	7	30+00	RT	14
13+15	LT	5	30+10	RT	5
13+50	RT	9	30+30	LT	12
13+90	RT	14	30+50	LT	15
14+35	RT	10	30+65	RT	5
14+60	RT	25	31+00	RT	15
14+90	LT	35	31+00	LT	40
15+30	RT	5	31+35	RT	15
15+35	RT	22	31+60	LT	5
15+50	LT	10	31+80	LT	5
16+65	LT	10	31+90	RT	25
17+00	RT	10	32+00	LT	10
17+40	RT	5	32+15	LT	5
17+50	RT	30	32+30	RT	12
17+60	LT	15	32+40	LT	5
18+00	LT	5	32+50	LT	5
18+40	LT	15	32+80	RT	5
18+70	RT	15	33+00	LT	22
18+80	LT	25	33+45	LT	9
19+00	RT	28	33+60	LT	5
19+10	LT	21	33+80	RT	15
19+35	LT	7	34+15	LT	12
20+50	RT	20	34+20	RT	15
20+70	LT	38	34+40	LT	12
21+00	RT	8	35+60	RT	5
21+20	RT	20	35+60	LT	10
21+45	LT	20	35+80	RT	15
21+50	RT	45	VARIOUS LOCATIONS STA. 26+95 TO 27+95		140
21+60	LT	22	VARIOUS LOCATIONS STA. 28+20 TO 30+00		100
22+10	LT	30	VARIOUS LOCATIONS STA. 30+25 TO 33+25		100
			VARIOUS LOCATIONS STA. 33+50 TO 36+12		200
					2208
					FEET

PATCHING SCHEDULE									
STA.	SIDE	PATCH WIDTH (FT)	PATCH LENGTH (FT)	PATCH AREA (SQ. FT.)	20201200 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU. YD.)	20700420 POROUS GRANULAR EMBANKMENT SUBGRADE (CU. YD.)	44201705 CLASS D PATCHES TYPE II, 5" (SQ. YD.)	44201709 CLASS D PATCHES TYPE III, 5" (SQ. YD.)	44201711 CLASS D PATCHES TYPE IV, 5" (SQ. YD.)
7+00	RT	16	34	544	15	15			60
10+15	RT	6	18	108			12		
17+45	RT	12	35	420					47
18+10	LT	6	19	114			13		
26+15	LT	5	70	350					39
27+48	RT	12	19	228				25	
27+55	LT	10	22.5	225				25	
29+20	LT	7	70	490	15	15			54
					30	30	25	50	200
					CY	CY	SQ YD	SQ YD	SQ YD

DRAINAGE AND UTILITY STRUCTURE SCHEDULE			
STA.	SIDE	60255500 MANHOLES TO BE ADJUSTED	60262700 INLETS TO BE RECONSTRUCTED
11+10	RT		1
12+55	LT	1	
19+60	LT	1	
20+10	RT		
33+75	LT		1
		2	2
		EACH	EACH

VILLAGE OF WOODRIDGE PUBLIC WORKS
 1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

**WOODRIDGE DRIVE
 REHABILITATION PROJECT**

SCHEDULE OF QUANTITIES

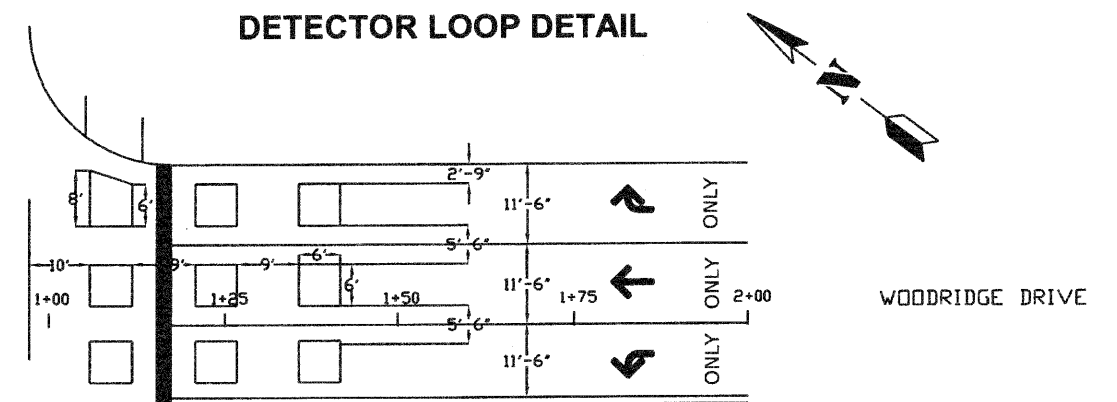
DRAWN BY: AF CHECKED BY: SM

CONTRACT NO. 63348

PAVEMENT QUANTITIES SCHEDULE												
STA.	SIDE	LOCATION	PAVEMENT WIDTH (FT)	PAVEMENT LENGTH (FT)	PAVEMENT AREA (SQ. FT.)	PAVEMENT AREA (SQ. YD.)	40600100 BITUMINOUS MATERIALS PRIME COAT (GALLON)	40600300 AGGREGATE PRIME COAT (TON)	40600826 POLYMERIZED LEVEL BINDER (TON)	40603335 HMA SURFACE COURSE, MIX "D", N50 (TON)	44000160 HMA SURFACE REMOVAL, 2 3/4" (SQ. YD.)	
1+00 TO 2+35		MAINLINE	48	135	6480	720.0	144	2.2	40.3	70.6	720.0	
2+30 TO 4+00		MAINLINE	41.2	170	7004	778.2	156	2.3	43.6	76.3	778.2	
4+00 TO 9+00		MAINLINE	34.1	500	17050	1894.4	379	5.7	106.1	185.7	1894.4	
9+00 TO 24+00		MAINLINE	34	1500	51000	5666.7	1133	17.0	317.3	555.3	5666.7	
24+00 TO 26+00		MAINLINE	35.8	200	7160	795.6	159	2.4	44.6	78.0	795.6	
26+00 TO 36+12		MAINLINE	58	1012	58696	6521.8	1304	19.6	365.2	639.1	6521.8	
3+50	LT	MITCHELL DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		675	75.0	15	0.2	4.2	7.4	75.0	
5+00	RT	ROBERTS DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		675	75.0	15	0.2	4.2	7.4	75.0	
9+25	LT	BURKE COURT	BEGIN REMOVAL 15' FROM MAINLINE		615	68.3	14	0.2	3.8	6.7	68.3	
11+25	RT	MARTIN DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		525	58.3	12	0.2	3.3	5.7	58.3	
12+50	LT	KINCAID DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		585	65.0	13	0.2	3.6	6.4	65.0	
16+00	RT	COOPER COURT	BEGIN REMOVAL 15' FROM MAINLINE		540	60.0	12	0.2	3.4	5.9	60.0	
19+75	RT	WILLIAMS DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		705	78.3	16	0.2	4.4	7.7	78.3	
19+75	LT	WESTMORELAND DRIVE	BEGIN REMOVAL 15' FROM MAINLINE		720	80.0	16	0.2	4.5	7.8	80.0	
27+50	LT	JANESWOOD DRIVE	BEGIN REMOVAL 40' FROM MAINLINE		1520	168.9	34	0.5	9.5	16.6	168.9	
							3421	51	958	1676	17106	
							GAL	TON	TON	TON	SY	

RESURFACING LIMITS (BUTT JOINT SCHEDULE)

STA.	SIDE	LOCATION	BUTT JOINT	PAVEMENT BUTT JOINT LENGTH (FT)	PAVEMENT BUTT JOINT WIDTH (FT)	40600982 HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT (SQ. YD.)
1+00		HOBSON ROAD		88	10	98
3+50	LT	MITCHELL DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	38	10	42
5+00	RT	ROBERTS DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	38	10	42
9+25	LT	BURKE COURT	BEGIN REMOVAL 15' FROM MAINLINE	28	10	31
11+25	RT	MARTIN DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	28	10	31
12+50	LT	KINCAID DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	28	10	31
16+00	RT	COOPER COURT	BEGIN REMOVAL 15' FROM MAINLINE	28	10	31
19+75	RT	WILLIAMS DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	38	10	42
19+75	LT	WESTMORELAND DRIVE	BEGIN REMOVAL 15' FROM MAINLINE	38	10	42
27+50	LT	JANESWOOD DRIVE	BEGIN REMOVAL 40' FROM MAINLINE	50	10	56
36+12		CENTER DRIVE		90	10	100
						547
						SY



DETECTOR LOOP SCHEDULE

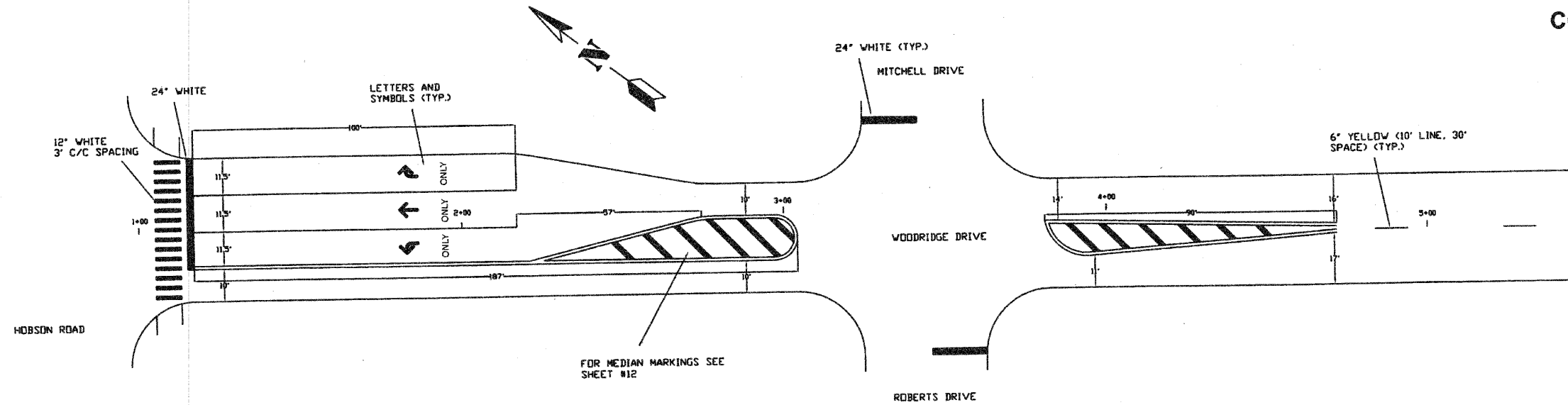
STA.	LOCATION/DIRECTION	88600100 DETECTOR LOOP TYPE 1 (FOOT)
1+10	LEFT TURN LANE AT HOBSON	178
1+10	THROUGH LANE AT HOBSON	222
1+10	RIGHT TURN LANE AT HOBSON	162
		562
		FEET

VILLAGE OF WOODRIDGE PUBLIC WORKS
 1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

**WOODRIDGE DRIVE
 REHABILITATION PROJECT**

RESURFACING SCHEDULE

CONTRACT NO. 63348



1" = 20'

PAVEMENT MARKING SCHEDULE								
STA.	SIDE	LOCATION	78000100 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (SQ. FT)	78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (FT)	78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (FT)	78000600 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (FT)	78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24" (FT)	COMMENTS
1+00 - 3+50		MAINLINE	105	1000	600	290	70	SEE INTERSECTION DETAIL ABOVE
3+50	LT	MITCHELL DRIVE					17	STOP BAR
3+50	RT	ROBERTS DRIVE					17	STOP BAR
3+50 - 28+00		MAINLINE			370			SKIP DASH
9+00	LT	BURKE COURT					12	STOP BAR
11+50	RT	MARTIN DRIVE					12	STOP BAR
11+65		MAINLINE			70			CROSSWALK
12+60	LT	KINCAID DRIVE			56		12	CROSSWALK AND STOP BAR
16+00	RT	COOPER COURT				54	12	SCHOOL CROSSWALK AND STOP BAR
19+50		MAINLINE				78	17	SCHOOL CROSSWALK AND STOP BAR
19+75	RT	WILLIAMS DRIVE				80	17	SCHOOL CROSSWALK AND STOP BAR
19+75	LT	WESTMORELAND DRIVE				80	17	SCHOOL CROSSWALK AND STOP BAR
20+00		MAINLINE				78	17	SCHOOL CROSSWALK AND STOP BAR
22+75		MAINLINE			70	72		BIKE PATH CROSSING
22+50 - 27+50		MAINLINE		1000		20		DOUBLE YELLOW AND MEDIAN
26+50 - 36+12		MAINLINE		380				WHITE SKIPDASH, BOTH DIRECTIONS OF TRAVEL
28+00	LT	JANESWOOD DRIVE			100			JANESWOOD DOUBLE YELLOW
28+00 - 29+50		MAINLINE		350				YELLOW AROUND ISLAND
29+00	LT	JANESWOOD DRIVE	35		290			WOODRIDGE DRIVE/JANESWOOD RIGHT TURN LANE
30+00 - 33+00		MAINLINE		700				YELLOW AROUND ISLAND
33+50 - 36+00		MAINLINE		500				YELLOW AROUND ISLAND
36+05		MAINLINE				180		SCHOOL CROSSWALK
			140	3930	1556	932	220	
			SF	FT	FT	FT	FT	

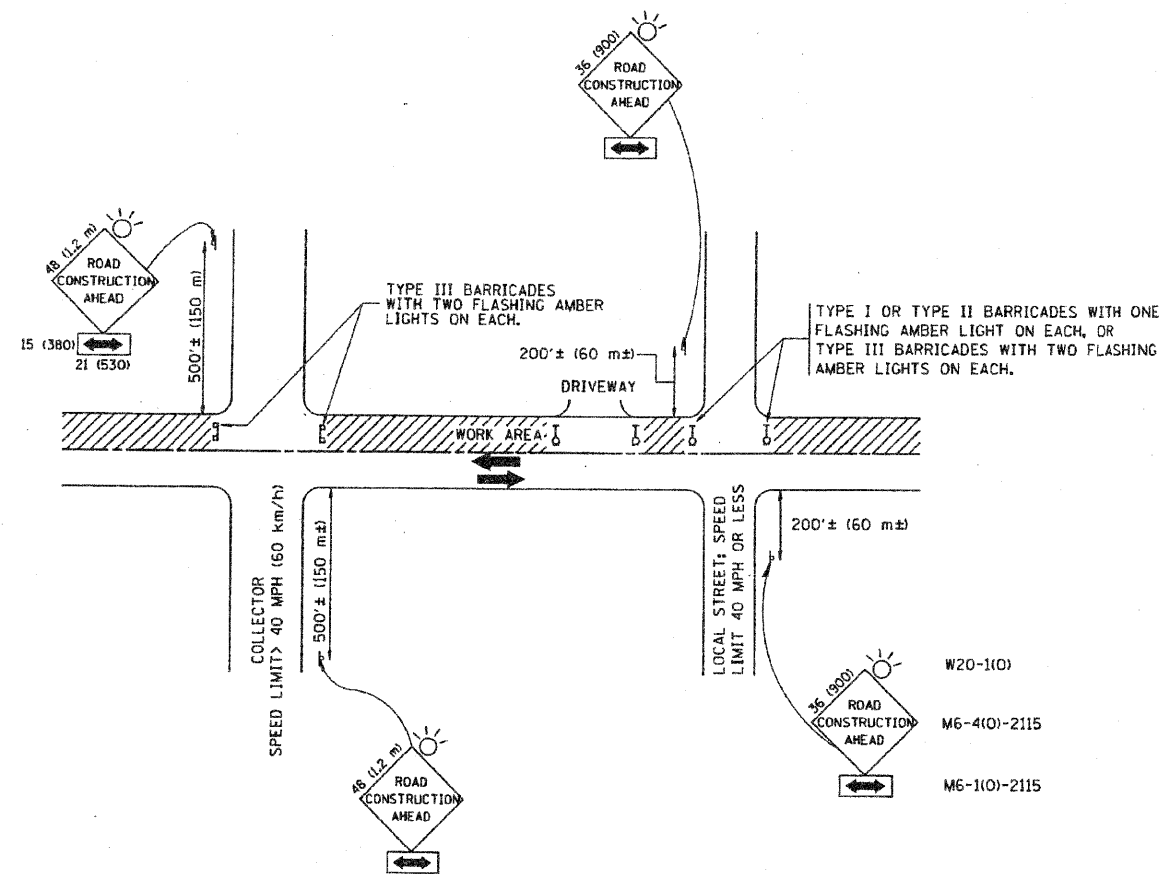
VILLAGE OF WOODRIDGE PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

WOODRIDGE DRIVE
REHABILITATION PROJECT

PAVEMENT MARKING PLAN

DRAWN BY: AF

CHECKED BY: SM



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

All dimensions are in millimeters (inches) unless otherwise shown.

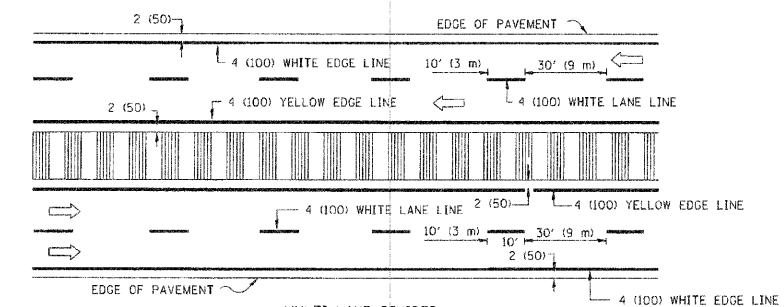
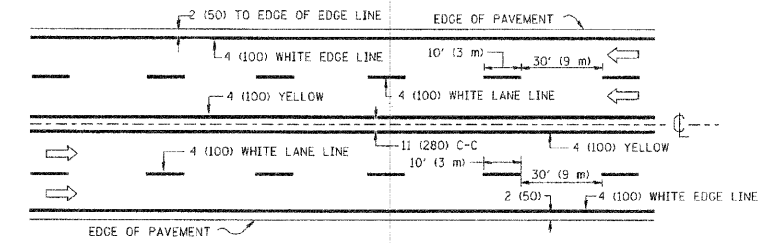
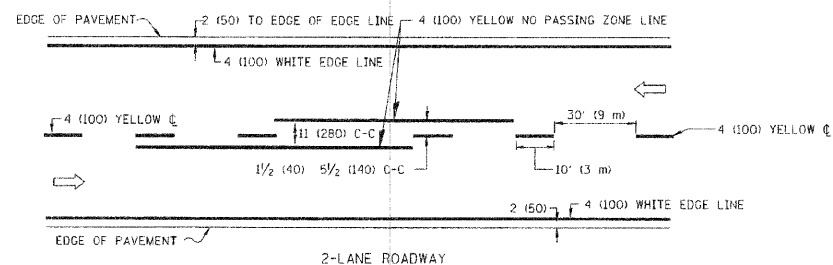
FILE NAME =	USER NAME = gegljanobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\divstsd\22x34\to18.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

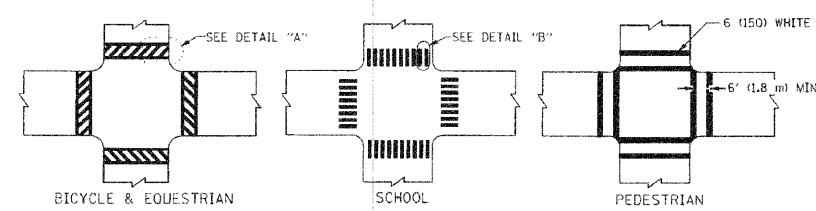
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

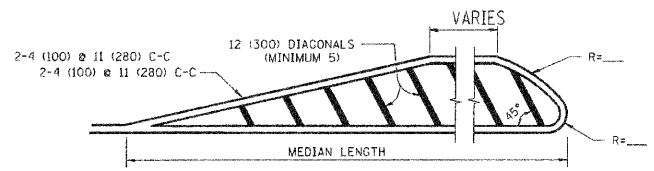
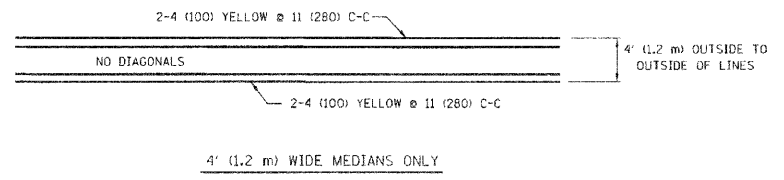
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3573	09-00064-00-15	DuPage	15	11
TC-10			CONTRACT NO. 63347	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



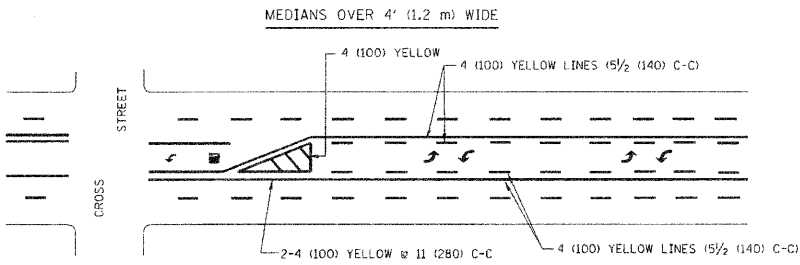
TYPICAL LANE AND EDGE LINE MARKING



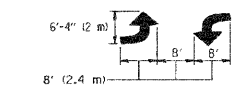
TYPICAL CROSSWALK MARKING



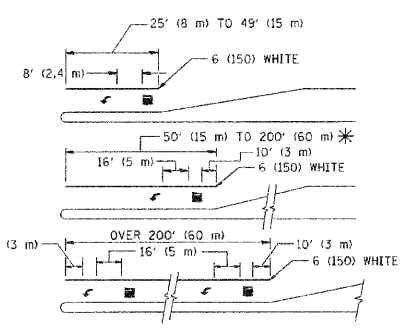
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



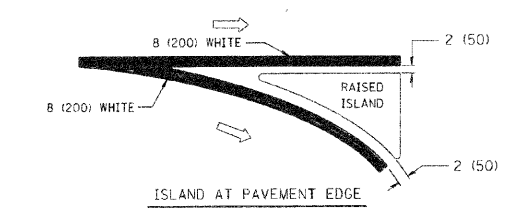
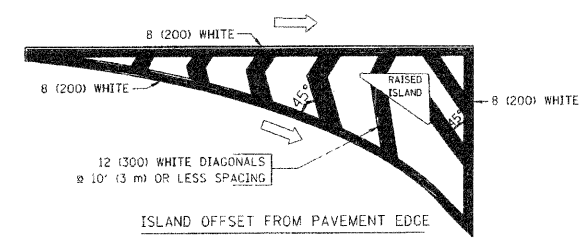
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINES; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C OVER 45MPH (70 km/h)
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' 6" (4.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

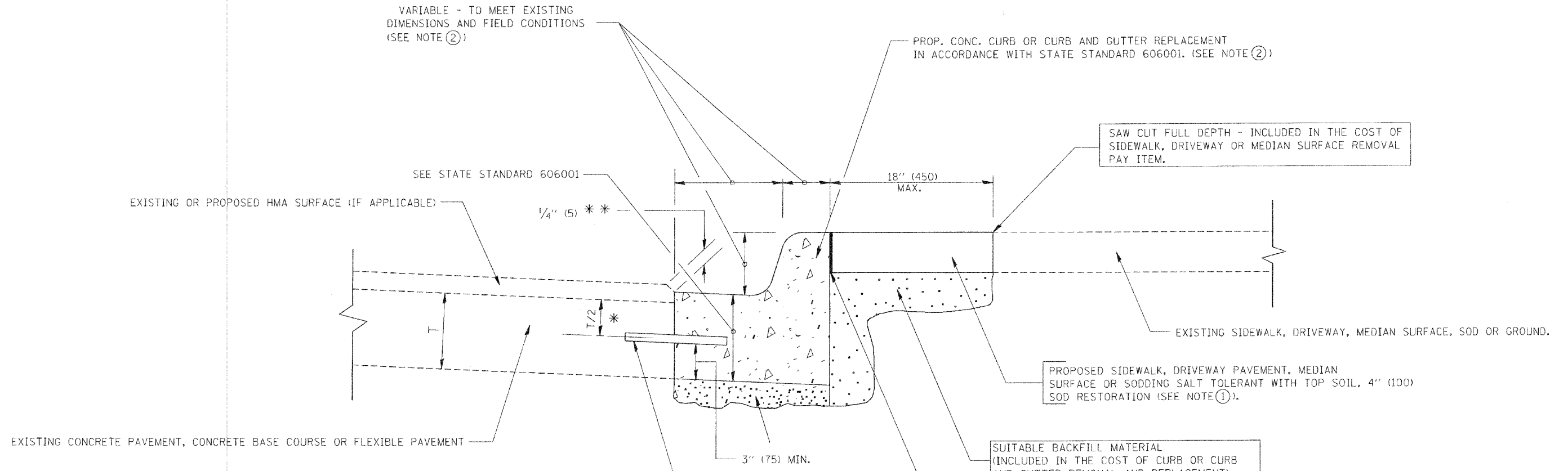
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = driveksgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
c:\pwork\pudot\driveksgn\d0188015\td3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50:200 "/ IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		3573	09-0064-00-RS	DuPage	15	12
SHEET NO. 1 OF 1 SHEETS		TC-13		CONTRACT NO. 63348		
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

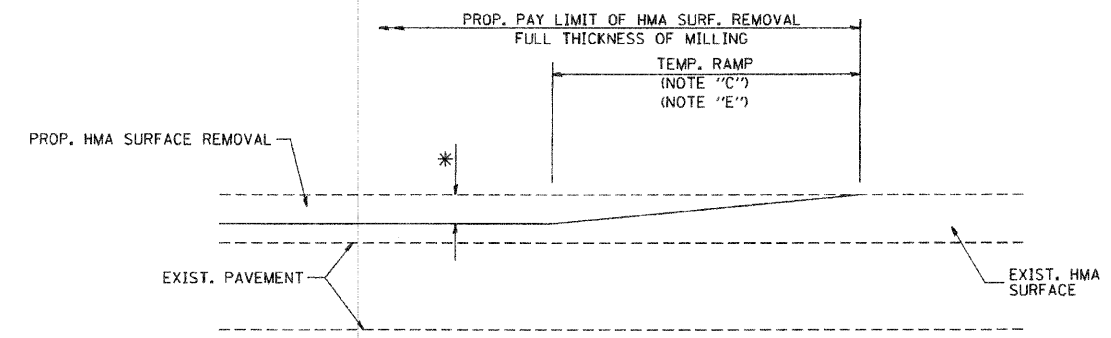
BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

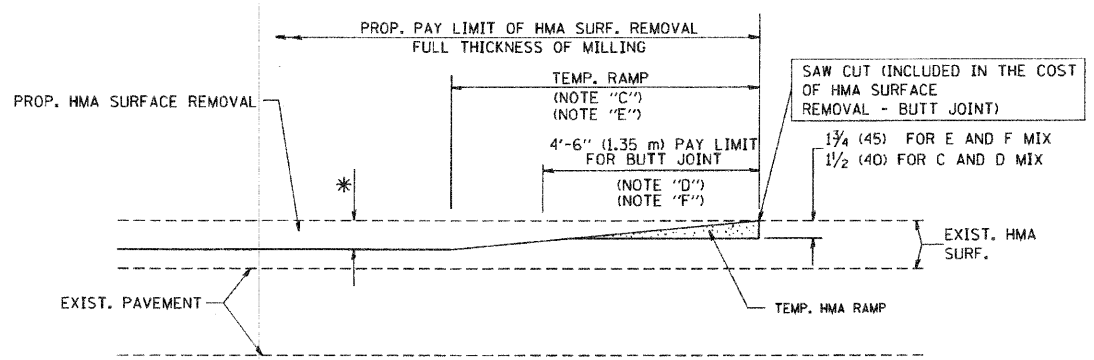
FILE NAME =	USER NAME = drivekosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr\p\work\p\dot\drivekosgn\d0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			357B	09-00064-00-RS	DoPage	15	13	
PLOT SCALE = 50,000 1/1 IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 63348			
PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
51B	09-000(4+0)B	DuPage	15	14
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



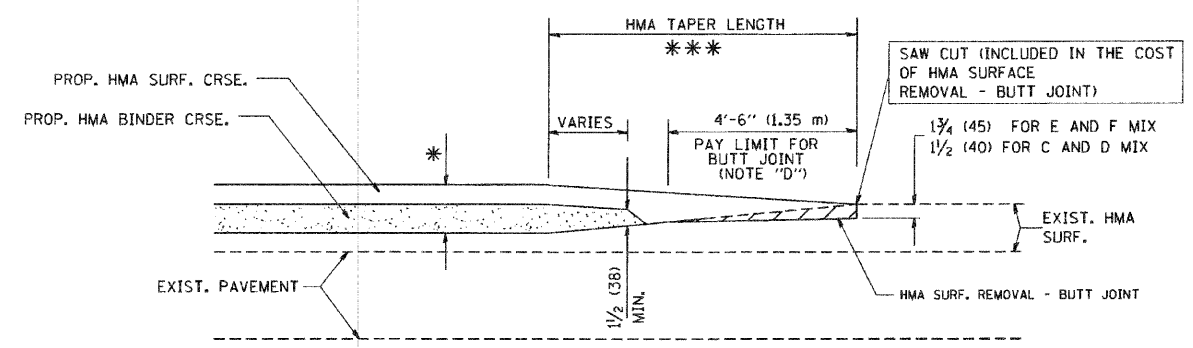
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



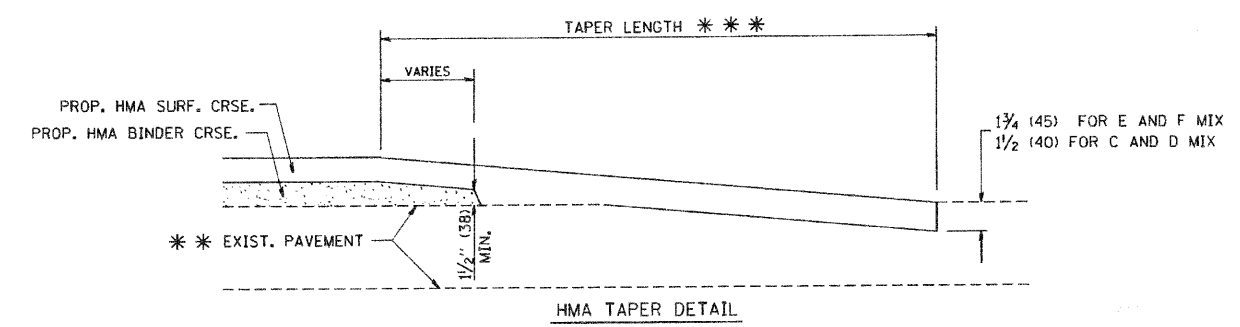
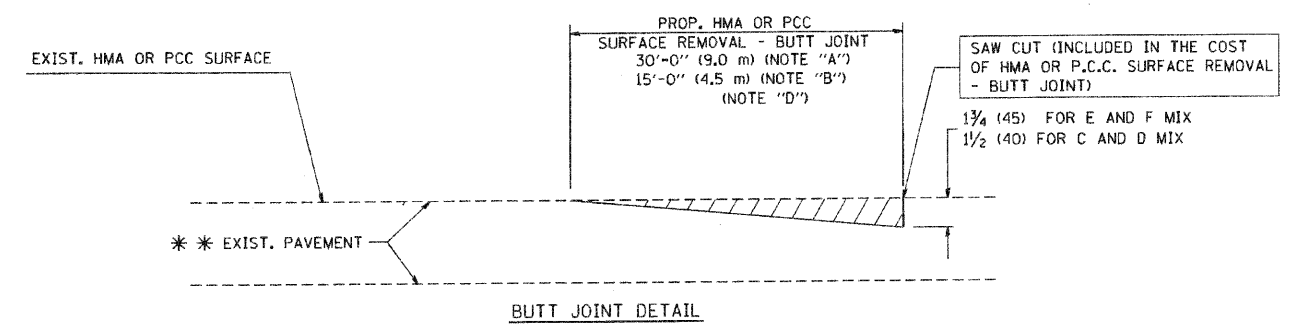
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
HORIZ. NONE

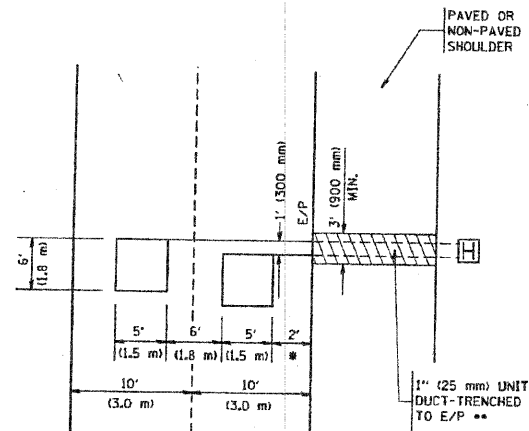
DRAWN BY
CHECKED BY

BD400-05 (VI-BD32)

PLOT DATE = 3/9/2007
PLOT SCALE = 1/8" = 1'-0"
PLOT SHEET = 14
USER NAME = bbornd01

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

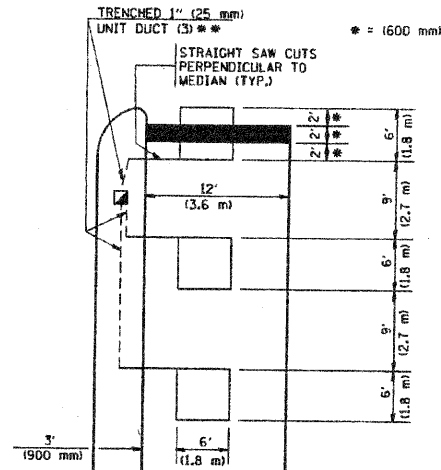


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

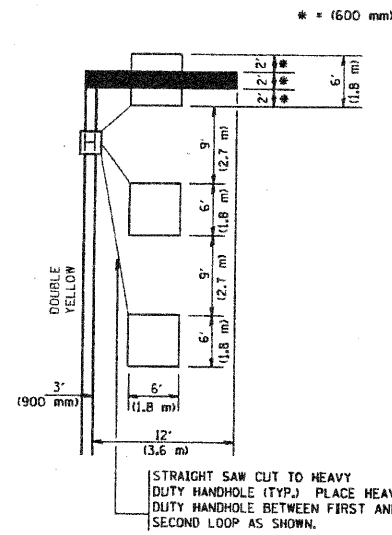
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

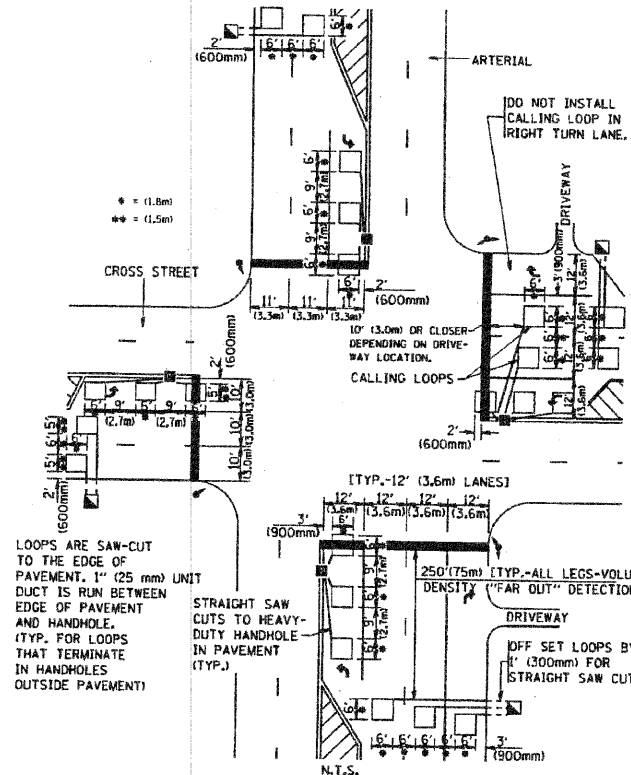
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



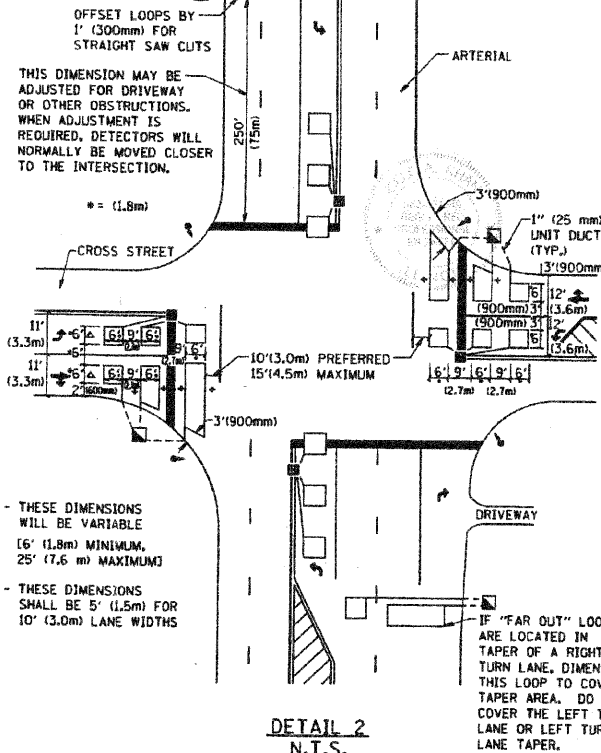
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = M:\distato\22\34\ts07.dgn	USER NAME = geglencbt	DESIGNED -	REVISED -
PLOT SCALE = 56.8888" / IN.	CHECKED - R.J.F.	DRAWN -	REVISED -
PLOT DATE = 1/4/2009	DATE -	REVISIONS -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
357	19-0004-CORS	DuPage	15 / 15
TS-07			CONTRACT NO. 02348
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			