

EXISTING STRUCTURE: S.N. 060-0196(SB) & 0197(NB), originally constructed in 1973 as FAI Route 55 Sec. 60-3VB at Station 318+48.55, using 36" steel I-beams with 8" concrete deck and 1 1/2" bituminous wearing surface, 3 spans, 179'-6" back-back abutments, 42'-0" out-out width, stub abutments on concrete piles, multi-column piers on concrete piles. Wearing surface was replaced with a concrete overlay (year unknown).

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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GENERAL NOTES

Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 7/8 in. ϕ , holes 5/16 in. ϕ , unless otherwise noted.
No field welding is permitted except as specified in the contract documents.
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
Reinforcement bars designated (E) shall be epoxy coated.
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
Where shown on the plans, reinforcement bars shall be epoxy grouted into existing concrete according to Section 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
Protective Shield shall be provided for the full width of the bridge deck over the railroad for 10 feet each side of the track centerline.
Protective Coat shall be applied to the surfaces of the deck and parapets, including the wings.
Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

Staged construction shall be used to maintain one lane of traffic in each direction.

No Salvage.

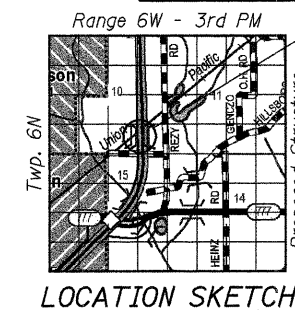
ELEVATION

PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Stone Riprap, Class A1	Ton	--	26	26
Concrete Removal	Cu Yd	36.2	--	36.2
Bridge Rail Removal	Foot	702	--	702
Slope Wall Removal	Sq Yd	--	32	32
Protective Shield	Sq Yd	238	--	238
Concrete Superstructure	Cu Yd	52.3	--	52.3
Bridge Deck Grooving	Sq Yd	1422	--	1422
Protective Coat	Sq Yd	1882	--	1882
Furnishing and Erecting Structural Steel	Pound	--	3840	3840
Jack and Remove Existing Bearings	Each	--	24	24
Cleaning and Painting Steel Bridge No. 3	L Sum	1	--	1
Cleaning and Painting Steel Bridge No. 4	L Sum	1	--	1
Cont. & Disp. of Lead Paint Clean. Resid. No.3	L Sum	1	--	1
Cont. & Disp. of Lead Paint Clean. Resid. No.4	L Sum	1	--	1
Reinforcement Bars, Epoxy Coated	Pound	8120	--	8120
Bar Splicers	Each	--	48	48
Preformed Joint Strip Seal	Foot	212	--	212
Elastomeric Bearing Assembly, Type I	Each	--	24	24
Anchor Bolts, 1"	Each	--	48	48
Concrete Sealer	Sq Ft	--	614	614
Pipe Drains 12"	Foot	12	--	12
Structural Repair of Concrete (Depth = < 5")	Sq Ft	--	36	36
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq Yd	1456	--	1456
Bridge Deck Hydro-Scarification 2 1/2"	Sq Yd	1456	--	1456

GENERAL PLAN & ELEVATION
I-55 OVER UNION PACIFIC RAILROAD
F.A.I. RTE. 55 SECTION 60-3RS-1
MADISON COUNTY
STATION 318+48.55
STRUCTURE NO. 060-0196(SB) & 0197(NB)

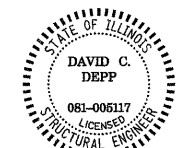


SHEET 1 OF 14	F.A.I. RTE. 55	SECTION 60-3RS-1, 60-3HB,3HB-111	COUNTY MADISON	TOTAL SHEETS 123	SHEET NO. 104
STA. 318+48.55			CONTRACT NO. 76601		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

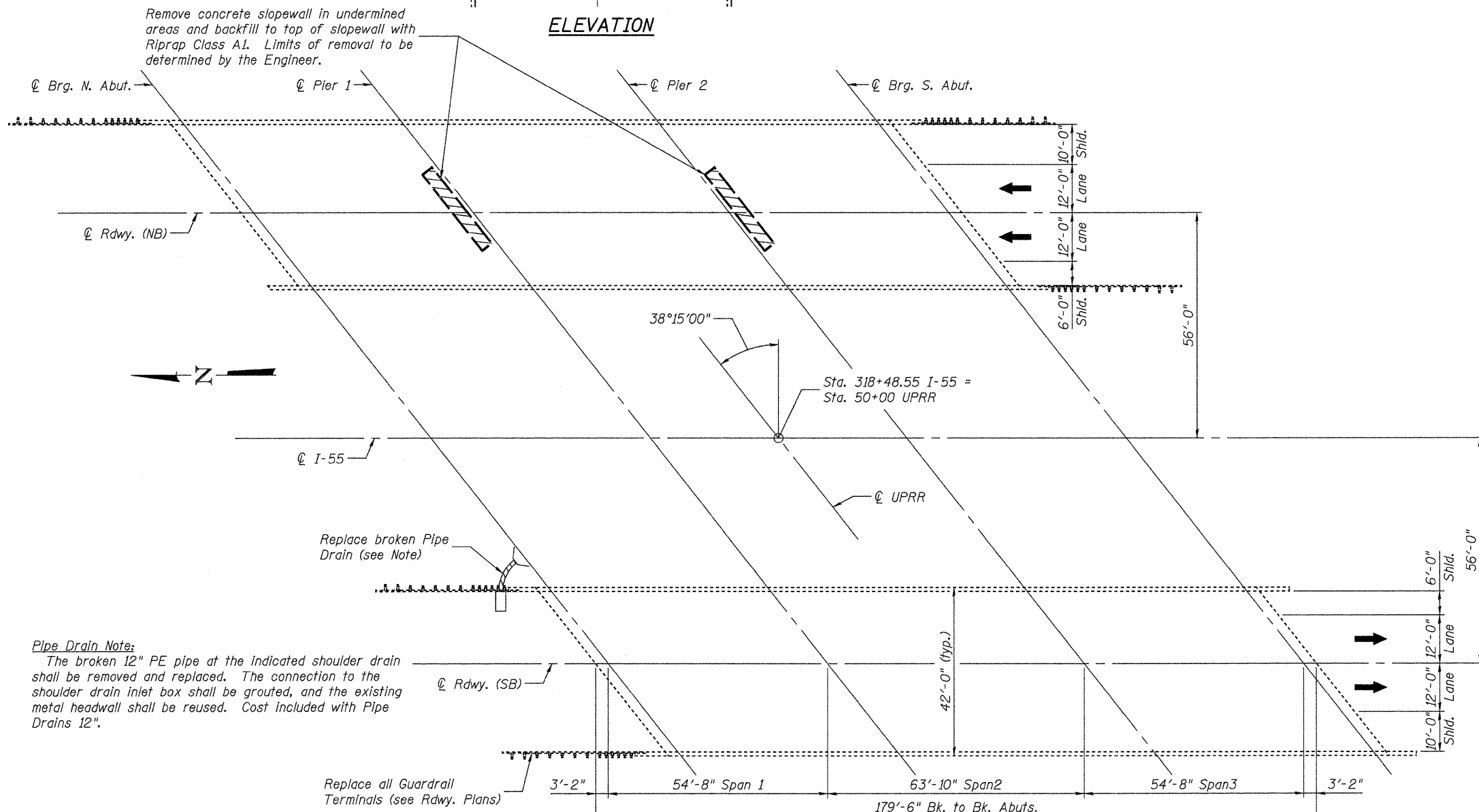
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 SAVE DATE: 6/22/2010

JD Johnson, Depp & Quisenberry
CONSULTING ENGINEERS
Springfield, Illinois

DESIGNED: CMV DRAWN: SJS
CHECKED: DCD CHECKED: CMV/DCD



Signed: *David Depp*
Date: *6-22-2010*
Lic. Expires: 11-30-2010



Remove concrete slopewall in undermined areas and backfill to top of slopewall with Riprap Class A1. Limits of removal to be determined by the Engineer.

Replace broken Pipe Drain (see Note)

Pipe Drain Note:
The broken 12" PE pipe at the indicated shoulder drain shall be removed and replaced. The connection to the shoulder drain inlet box shall be grouted, and the existing metal headwall shall be reused. Cost included with Pipe Drains 12".

Replace all Guardrail Terminals (see Rdwy. Plans)