

# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

## CONSTRUCTION PLANS FOR

### DUPAGE AIRPORT

#### NE QUADRANT DRAINAGE IMPROVEMENTS

A.I.P. PROJECT: 3-17-0017-B24  
ILLINOIS PROJECT: DPA-3891

JULY 23, 2010



**PRIMARY BENCHMARK**

W107001 (DUPAGE COUNTY)  
A DISK MONUMENT ESTABLISHED 58.40' SOUTH OF THE CENTERLINE OF IL ROUTE 38 (ROOSEVELT ROAD) AND 405' WEST OF THE CENTERLINE OF KRESS ROAD. ELEVATION (NAVD29) = 746.27  
NGS ELEVATION (NAVD88) = 746.01

**SECONDARY BENCHMARK**

WC105 (NGS)  
3.5" ALUMINUM DISK WITH LID LOCATED 55.4' EAST OF THE CENTERLINE OF POWIS ROAD, 47.8' NORTH OF THE CENTERLINE OF POWIS COURT AND 16.2' NORTHWEST OF A STORM MANHOLE. ELEVATION (NAVD88) = 757.10

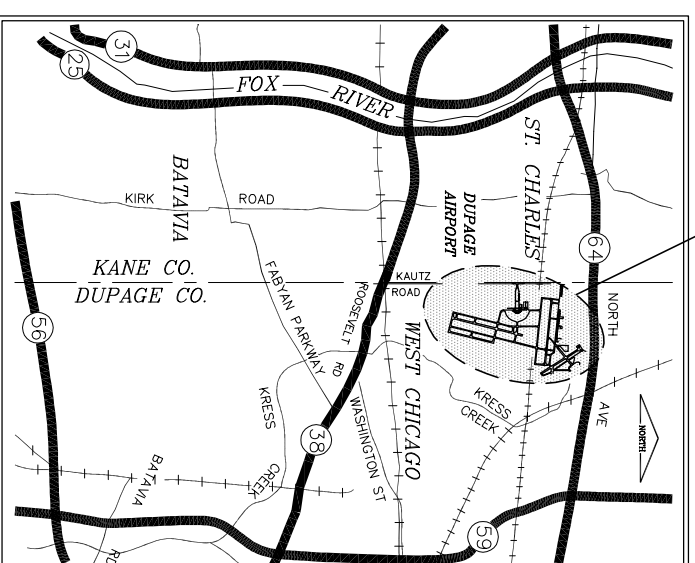
WC165 (NGS)  
3.5" ALUMINUM DISK WITH LID LOCATED 29' EAST OF THE CENTERLINE OF HOWARD DRIVE, NORTHEAST OF THE NORTHEAST CORNER OF CHAINLINK FENCE AND 12.9' WEST OF THE WEST EDGE OF PAVEMENT OF HOWARD DRIVE. ELEVATION (NAVD88) = 751.73

**DUPAGE AIRPORT**

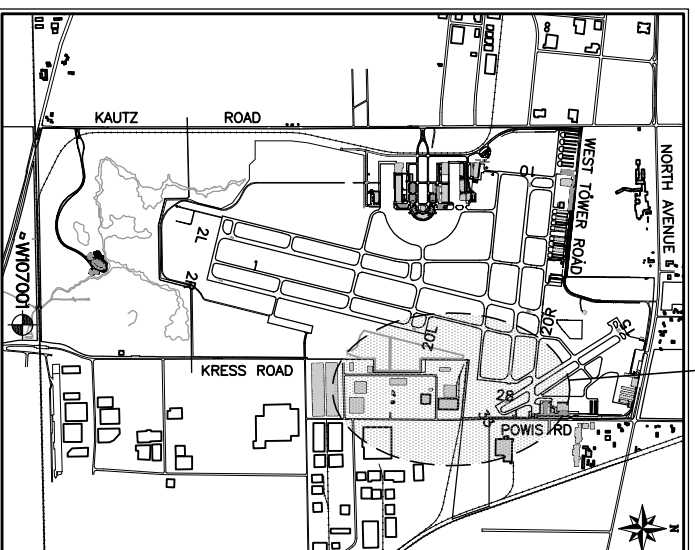
TOWNSHIP: 40 NORTH      WAYNE TOWNSHIP  
RANGE: 9 EAST      (SECTIONS: 31)  
DUPAGE COUNTY

**CONSTRUCT EAST SIDE  
DRAINAGE SYSTEM**  
DESIGN AIRCRAFT APPROACH CATEGORY B  
DESIGN AIRCRAFT GROUP II

**CALL JULIE  
BEFORE EXCAVATING**  
1-800-892-0123



LOCATION MAP



SITE PLAN

PROJECT LOCATION

PROJECT LOCATION

| SHEET NUMBER | INDEX TO SHEETS  | SHEET TITLE |
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**GMT** 08257-03-00

GRAVERDOR, LAPPIN & TILLY, INC.  
CONSULTING ENGINEERS  
600 N. COAKSONS DRIVE  
SUITE 07  
AURORA, IL 60504  
PHONE (630) 820-0222  
FAX (630) 820-0250

SUBMITTED BY *Antonio R. Marin* ANTONIO R. MARIN, P.E.  
DATE July 23, 2010

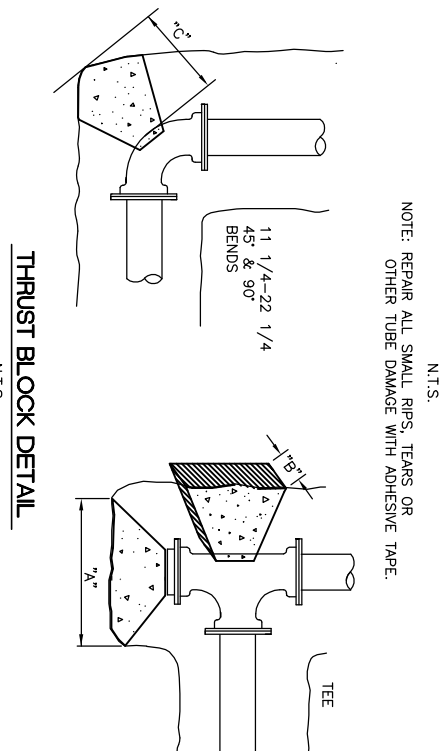
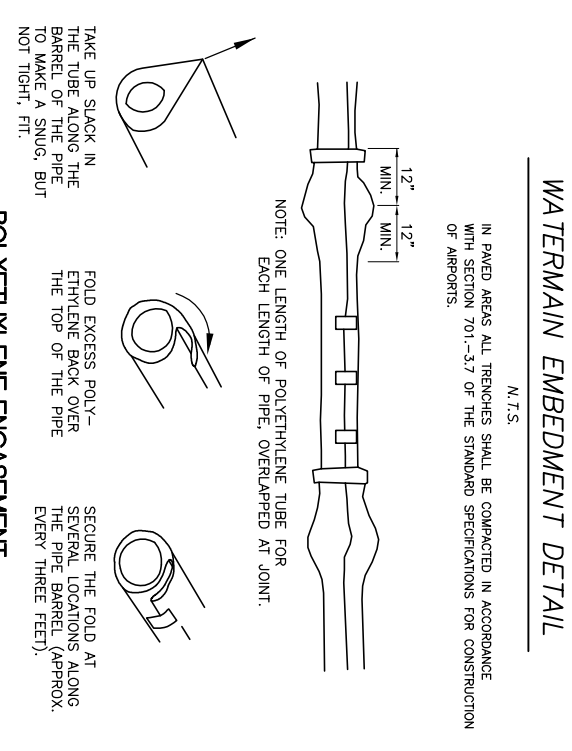
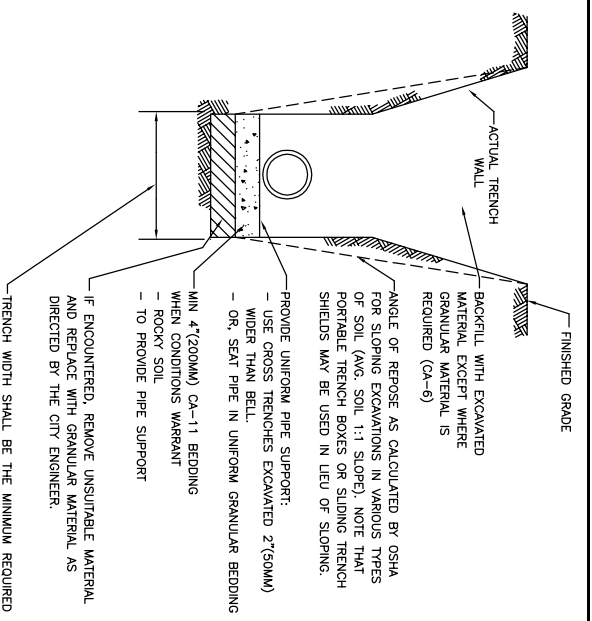
**DDMI**  
Dupage Airport

2700 INTERNATIONAL DRIVE  
WEST CHICAGO, IL 60591

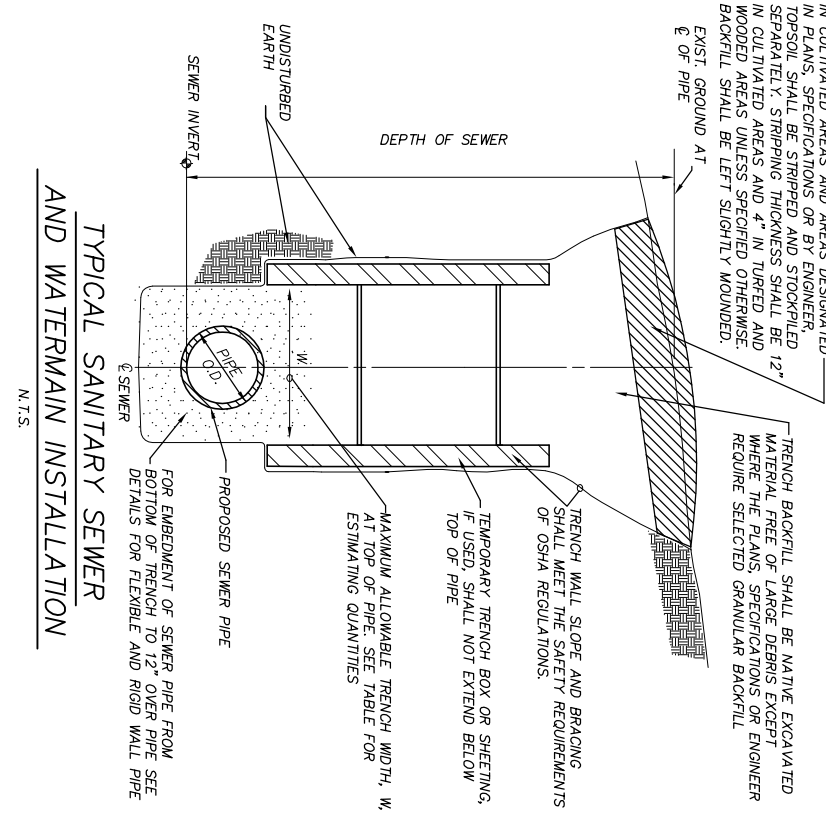
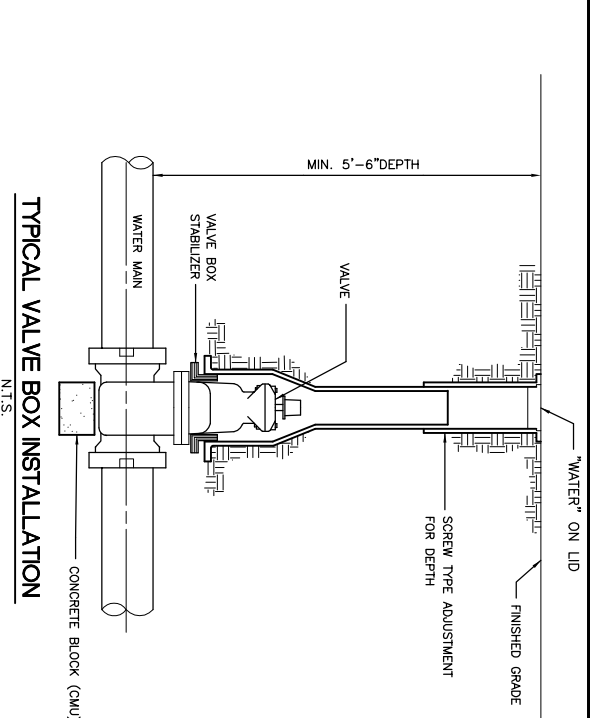
APPROVED BY *David Bird*  
DAVID BIRD - EXECUTIVE DIRECTOR  
DATE July 20, 2010

**SUMMARY OF QUANTITIES**

| ITEM NO. | DESCRIPTION                      | UNIT | ESTIMATED QUANTITY | RECORD QUANTITY |
|----------|----------------------------------|------|--------------------|-----------------|
| AR108158 | 1/1C #8 5KV UG CABLE IN UD       | LF   | 800                |                 |
| AR125962 | RELOCATE BASE MOUNTED LIGHT      | EACH | 3                  |                 |
| AR150510 | ENGINEER'S FIELD OFFICE          | LS   | 1                  |                 |
| AR150520 | MOBILIZATION                     | LS   | 1                  |                 |
| AR152531 | EXPLORATION TRENCH               | LF   | 1,000              |                 |
| AR152410 | UNCLASSIFIED EXCAVATION          | CY   | 525                |                 |
| AR156510 | SILT FENCE                       | LF   | 1,850              |                 |
| AR156520 | INLET PROTECTION                 | EACH | 20                 |                 |
| AR156521 | HEADWALL PROTECTION              | EACH | 1                  |                 |
| AR156530 | TEMPORARY SEEDING                | ACRE | 3                  |                 |
| AR156531 | EROSION CONTROL BLANKET          | SY   | 21,500             |                 |
| AR156540 | RIPRAP                           | SY   | 25                 |                 |
| AR162960 | RELOCATE CLASS E FENCE           | LF   | 20                 |                 |
| AR163000 | TEMPORARY CONSTRUCTION FENCE     | LF   | 40                 |                 |
| AR208615 | POROUS GRANULAR EMBANKMENT       | CY   | 470                |                 |
| AR401915 | REM & REP BIT PAVEMENT - TYPE A  | SY   | 550                |                 |
| AR401916 | REM & REP BIT PAVEMENT - TYPE B  | SY   | 435                |                 |
| AR501910 | REMOVE & REPLACE PCC PAVEMENT    | SY   | 1,000              |                 |
| AR620520 | PAVEMENT MARKING-WATERBORNE      | SF   | 825                |                 |
| AR620525 | PAVEMENT MARKING-BLACK BORDER    | SF   | 700                |                 |
| AR701008 | 8" PVC STORM SEWER               | LF   | 40                 |                 |
| AR701360 | 60" RCP CLASS II                 | LF   | 5,480              |                 |
| AR701412 | 12" RCP CLASS III                | LF   | 15                 |                 |
| AR701418 | 18" RCP CLASS III                | LF   | 33                 |                 |
| AR701424 | 24" RCP CLASS III                | LF   | 25                 |                 |
| AR701436 | 36" RCP CLASS III                | LF   | 16                 |                 |
| AR701830 | TRENCH BACKFILL                  | CY   | 2,250              |                 |
| AR701900 | REMOVE PIPE                      | LF   | 1,948              |                 |
| AR705526 | 6" PERFORATED UNDERDRAIN W/ SOCK | LF   | 195                |                 |
| AR705900 | REMOVE UNDERDRAIN                | LF   | 400                |                 |
| AR705905 | REMOVE COLLECTION STRUCTURE      | EACH | 1                  |                 |
| AR751540 | MANHOLE 4'                       | EACH | 2                  |                 |
| AR751567 | MANHOLE 7'                       | EACH | 8                  |                 |
| AR751568 | MANHOLE 8'                       | EACH | 5                  |                 |
| AR751570 | MANHOLE - SPECIAL                | EACH | 1                  |                 |
| AR751903 | REMOVE MANHOLE                   | EACH | 8                  |                 |
| AR752660 | CONCRETE HEADWALL 60"            | EACH | 1                  |                 |
| AR752900 | REMOVE END SECTION               | EACH | 1                  |                 |
| AR754410 | COMB CONCRETE CURB & GUTTER      | LF   | 60                 |                 |
| AR754804 | REMOVE COMB CURB & GUTTER        | LF   | 60                 |                 |
| AR760830 | WATER VALVE                      | EACH | 2                  |                 |
| AR760820 | REPLACE WATERMAIN                | LF   | 120                |                 |
| AR800125 | FILL PIPE WITH CLSM              | LF   | 110                |                 |
| AR800127 | RELOCATE GATE                    | EACH | 1                  |                 |
| AR800146 | LIFT STATION REMOVAL             | LS   | 1                  |                 |
| AR800153 | CONCRETE WASHOUT                 | LS   | 1                  |                 |
| AR800165 | DRAINAGE STRUCTURE A             | EACH | 1                  |                 |
| AR800166 | DRAINAGE STRUCTURE B             | EACH | 1                  |                 |
| AR800170 | ADJUST SANITARY FOREMAN          | EACH | 1                  |                 |
| AR901510 | SEEDING                          | ACRE | 4                  |                 |
| AR910965 | RELOCATE RDWV LGT POLE W/ XTR    | EACH | 2                  |                 |



| SIZE | 90° BEND |        |       | 45° BEND |        |     | 22-1/2° BEND |       |     | 11-1/4° BEND |       |    | TEE OR PLUG |        |     |
|------|----------|--------|-------|----------|--------|-----|--------------|-------|-----|--------------|-------|----|-------------|--------|-----|
|      | A        | B      | C     | A        | B      | C   | A            | B     | C   | A            | B     | C  | A           | B      | C   |
| 6"   | 2'-3"    | 1'-2"  | 8"    | 1'-3"    | 1'-2"  | 8"  | 0'-8"        | 1'-2" | 8"  | 0'-6"        | 1'-2" | 7" | 1'-8"       | 1'-2"  | 8"  |
| 8"   | 3'-7"    | 1'-4"  | 9"    | 2'-3"    | 1'-4"  | 9"  | 1'-4"        | 1'-4" | 9"  | 0'-7"        | 1'-4" | 8" | 3'-2"       | 1'-4"  | 9"  |
| 10"  | 5'-0"    | 1'-6"  | 10"   | 3'-2"    | 1'-6"  | 10" | 1'-5"        | 1'-6" | 10" | 0'-8"        | 1'-6" | 8" | 3'-6"       | 1'-6"  | 10" |
| 12"  | 5'-10"   | 1'-10" | 1'-0" | 3'-2"    | 1'-10" | 11" | 1'-10"       | 1'-8" | 11" | 0'-8"        | 1'-8" | 8" | 4'-2"       | 1'-10" | 10" |



**TABLE OF QUANTITIES FOR ESTIMATING PURPOSES**  
 BASED ON STANDARD DRAWING NO. 2 OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

| INSIDE DIAMETER OF CONDUIT IN INCHES "D" | MAXIMUM TRENCH WIDTH IN FEET | BEDDING C.Y./FOOT WHERE ELIGIBLE | HAUNCHING C.Y./FOOT WHERE ELIGIBLE | INITIAL BACKFILL C.Y./FOOT WHERE ELIGIBLE | FINAL BACKFILL C.Y./FOOT PER FOOT | PERMANENT PAVEMENT REMOVAL AND REPLACEMENT S.Y./FOOT |
|--|------------------------------|----------------------------------|------------------------------------|---|-----------------------------------|--|
| 6  | 3.58                         | 0.04                             | 0.03                               | 0.17                                      | 0.13                              | 0.62   |
| 8  | 3.78                         | 0.05                             | 0.05                               | 0.19                                      | 0.14                              | 0.64   |
| 10                                       | 3.97                         | 0.05                             | 0.06                               | 0.20                                      | 0.15                              | 0.66   |
| 12                                       | 4.17                         | 0.05                             | 0.07                               | 0.22                                      | 0.15                              | 0.69   |
| 14                                       | 4.36                         | 0.05                             | 0.08                               | 0.24                                      | 0.16                              | 0.71   |
| 15                                       | 4.46                         | 0.06                             | 0.09                               | 0.25                                      | 0.17                              | 0.72   |
| 16                                       | 4.56                         | 0.06                             | 0.10                               | 0.26                                      | 0.17                              | 0.73   |
| 18                                       | 4.75                         | 0.06                             | 0.11                               | 0.29                                      | 0.18                              | 0.75   |
| 20                                       | 4.94                         | 0.06                             | 0.12                               | 0.31                                      | 0.18                              | 0.77   |
| 21                                       | 5.04                         | 0.06                             | 0.13                               | 0.32                                      | 0.19                              | 0.78   |
| 24                                       | 5.33                         | 0.07                             | 0.15                               | 0.35                                      | 0.20                              | 0.81   |
| 27                                       | 5.63                         | 0.07                             | 0.17                               | 0.38                                      | 0.21                              | 0.85   |
| 30                                       | 5.92                         | 0.07                             | 0.18                               | 0.39                                      | 0.21                              | 0.86   |
| 33                                       | 6.21                         | 0.08                             | 0.22                               | 0.41                                      | 0.22                              | 0.88   |
| 36                                       | 6.50                         | 0.08                             | 0.24                               | 0.48                                      | 0.23                              | 0.94   |

I.L. CONTRACT: **DU079**  
 I.L. LETTING ITEM: **1A**  
 I.L. PROJECT: **DPA-3891**  
 A.L.P. PROJECT: **3-17-0017-824**

SURVEY BOOK # ###  
 REVISIONS  
 NUMBER BY DATE

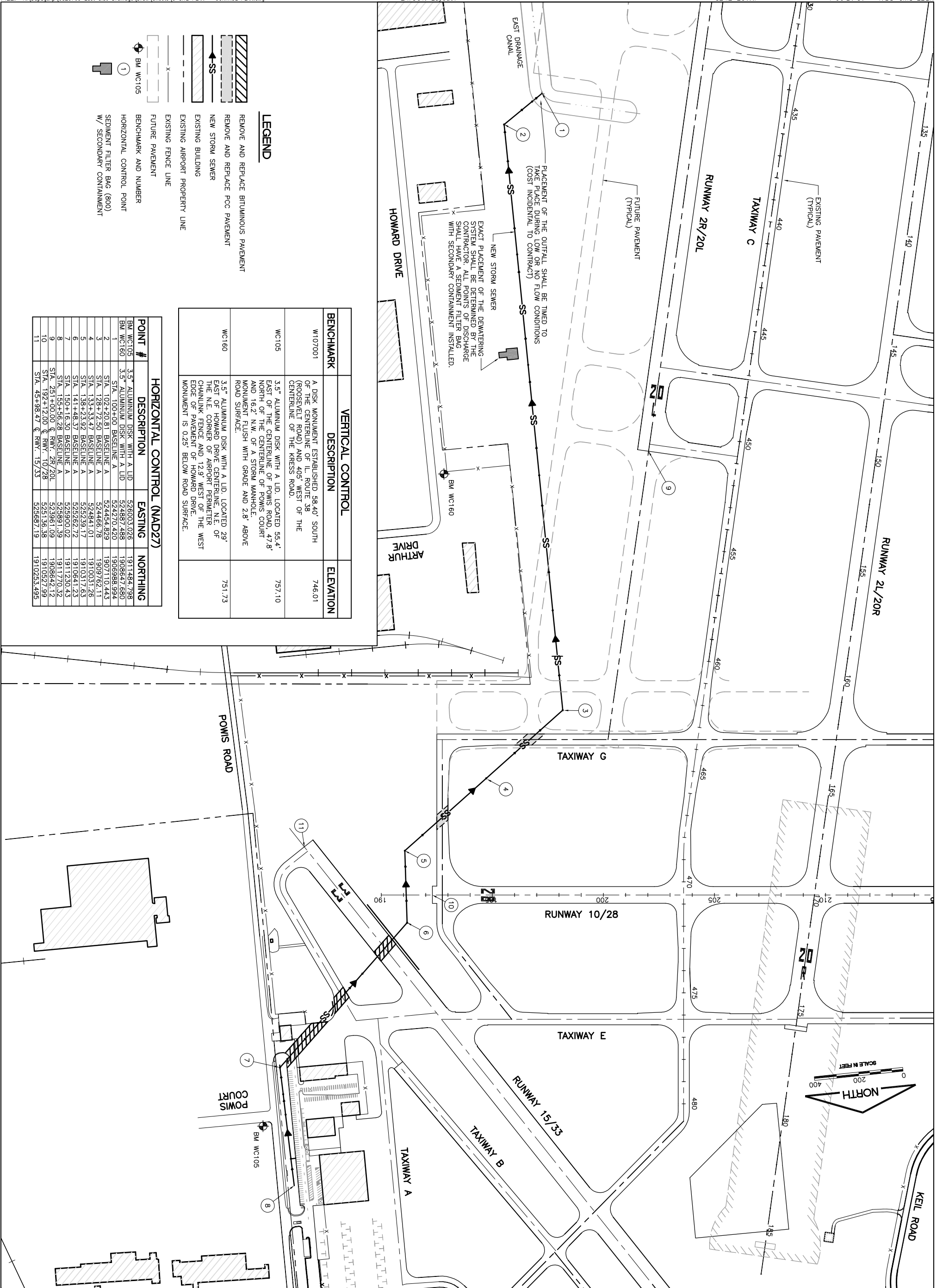
0 1 2  
 THIS BAR IS EQUAL TO 2' AT FULL SCALE (344X22).

**DUPAGE AIRPORT WEST CHICAGO, ILLINOIS**  
**NE QUADRANT DRAINAGE IMPROVEMENTS**  
**SUMMARY OF QUANTITIES AND WATERMAIN DETAILS**

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**DDM**  
 DuPage Airport

DESIGN BY: ARM  
 DRAWN BY: JRO  
 CHECKED BY: ARM  
 APPROVED BY: DLP  
 DATE: 07/23/2010  
 JOB No.: 08257-03-00

SHEET 2 OF 25 SHEETS



**LEGEND**

- REMOVE AND REPLACE BITUMINOUS PAVEMENT
- REMOVE AND REPLACE PCC PAVEMENT
- NEW STORM SEWER
- EXISTING AIRPORT PROPERTY LINE
- EXISTING AIRPORT PROPERTY LINE
- FUTURE PAVEMENT
- BENCHMARK AND NUMBER
- HORIZONTAL CONTROL POINT
- SEDIMENT FILTER BAG (SFB) W/ SECONDARY CONTAINMENT

| BENCHMARK | VERTICAL CONTROL DESCRIPTION   | ELEVATION |
|-----------|--|-----------|
| W107001   | A DISK MONUMENT ESTABLISHED 58.40' SOUTH OF THE CENTERLINE OF I. ROUTE 38 (ROOSEVELT ROAD) AND 405' WEST OF THE CENTERLINE OF THE KRESS ROAD.  | 746.01    |
| WC105     | 3.5" ALUMINUM DISK WITH A LID. LOCATED 55.4' EAST OF THE CENTERLINE OF POWIS ROAD, 47.8' NORTH OF THE CENTERLINE OF POWIS COURT AND 16.2' N.W. OF A STORM MANHOLE. MONUMENT FLUSH WITH GRADE AND 2.8' ABOVE ROAD SURFACE.                  | 757.10    |
| WC160     | 3.5" ALUMINUM DISK WITH A LID. LOCATED 29' EAST OF HOWARD DRIVE CENTERLINE, N.E. OF THE N.E. CORNER OF AIRPORT PERMETER CHAINLINK FENCE AND 12.9' WEST OF THE WEST EDGE OF PAVEMENT OF HOWARD DRIVE. MONUMENT IS 0.25' BELOW ROAD SURFACE. | 751.73    |

| POINT #  | DESCRIPTION                   | EASTING    | NORTHING    |
|----------|-------------------------------|------------|-------------|
| BM WC105 | 3.5" ALUMINUM DISK WITH A LID | 526003.026 | 1911484.798 |
| BM WC160 | 3.5" ALUMINUM DISK WITH A LID | 524887.488 | 1908647.680 |
| 1        | STA. 100+00 BASELINE A        | 524270.420 | 1906988.994 |
| 2        | STA. 102+20.81 BASELINE A     | 524454.829 | 1907110.443 |
| 3        | STA. 128+72.50 BASELINE A     | 524466.78  | 1909762.11  |
| 4        | STA. 133+33.47 BASELINE A     | 524841.01  | 1910031.26  |
| 5        | STA. 138+23.92 BASELINE A     | 525239.17  | 1910317.63  |
| 6        | STA. 141+48.37 BASELINE A     | 525262.72  | 1910641.23  |
| 7        | STA. 150+16.30 BASELINE A     | 525900.02  | 1911230.43  |
| 8        | STA. 155+56.28 BASELINE A     | 525891.39  | 1911770.32  |
| 9        | STA. 251+00.00 R.W. 2R/20L    | 523961.09  | 1908642.12  |
| 10       | STA. 192+12.00 R.W. 10/28     | 525156.38  | 1910527.99  |
| 11       | STA. 45+98.47 R.W. 15/33      | 525687.19  | 1910253.495 |

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**DPA**  
 DuPage Airport

**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**SITE PLAN/  
 PROJECT CONTROL PLAN**

IL CONTRACT: **DU079**  
 IL LETTING ITEM: **1A**  
 IL PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-824**

SURVEY BOOK # ###  
 REVISIONS

| NUMBER | BY | DATE |
|--------|----|------|
|        |    |      |

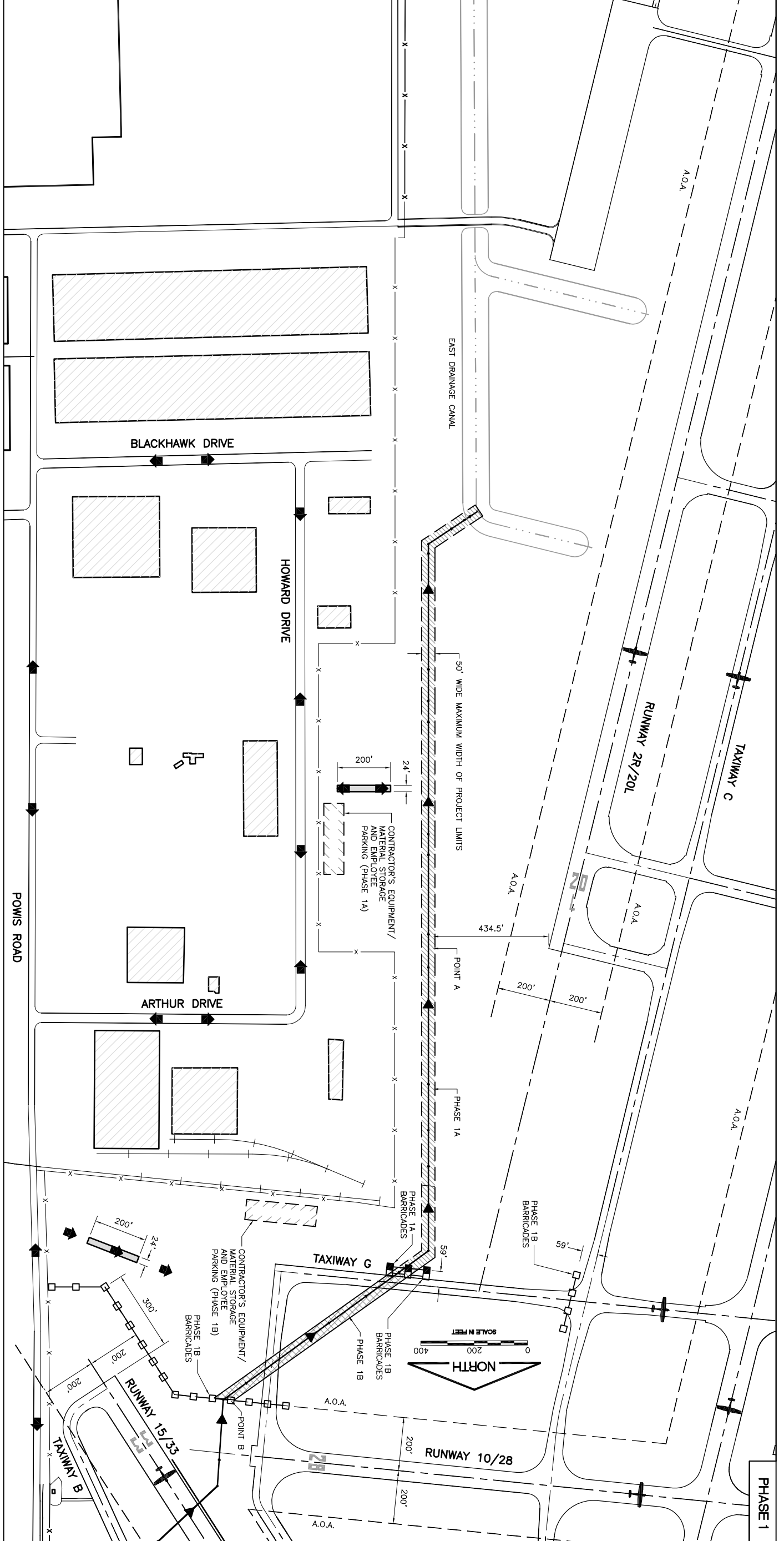
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DESIGN BY: ARJ  
 DRAWN BY: JRO  
 CHECKED BY: ARJ  
 APPROVED BY: DLP

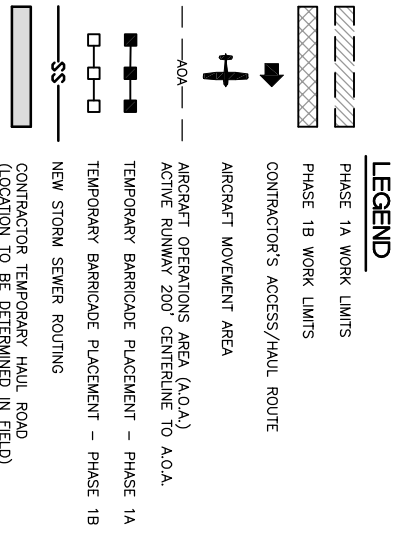
DATE: 07/23/2010

JOB No: 08257-03-00

SHEET 3 OF 25 SHEETS



| PHASE | WORK AREA  | ALLOWABLE WORK PERIODS    | AIRPORT OPERATIONAL RESTRICTIONS  |
|-------|--|---------------------------|---|
| 1A    | AREA OUTSIDE RUNWAY 2R/20L A.O.A. AND OUTSIDE TAXIWAY G T.S.A. | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>ALL TAXIWAYS OPEN   |
| 1B    | AREA OUTSIDE RUNWAY 10/28 A.O.A. AND WITHIN TAXIWAY G T.S.A.   | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>TAXIWAY G PARTIAL CLOSURE<br>FROM TAXIWAY C TO RUNWAY 10/28 |
| 2A    | AREA WITHIN RUNWAY 10/28 AND 15/33 A.O.A. AND TAXIWAY B T.S.A. | UNRESTRICTED FOR DURATION | RUNWAY 10/28 AND 15/33 CLOSED<br>TAXIWAY B AND G PARTIAL CLOSURE                |
| 2B    | AREA OUTSIDE TAXIWAY B T.S.A. AND ALONG POWIS ROAD             | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>ALL TAXIWAYS OPEN   |



**PHASE 1A - WORK OUTSIDE RUNWAY 2R/20L A.O.A. AND TAXIWAY G T.S.A. SUGGESTED SEQUENCE OF CONSTRUCTION**

- PLACE LATHE AND RIBBON AT AIR OPERATIONS AREA (A.O.A.) FOR ALL RUNWAYS.
- PLACE TEMPORARY BARRICADES AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- PHASE 1A SHALL BE COMPLETED WITHIN 46 CALENDAR DAYS.
- CONSTRUCT STABILIZED CONSTRUCTION ENTRANCE/CONTRACTOR'S EQUIPMENT/MATERIAL STORAGE/PARKING AREA.
- VERIFY UTILITIES ALONG POWIS ROAD.
- ADJUST SANITARY FORCE MAIN AT STORM SEWER CROSSING.
- INSTALL NEW STORM SEWER AND DRAINAGE STRUCTURES.

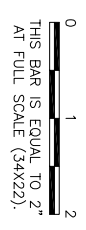
**PHASE 1B - WORK OUTSIDE RUNWAY 10/28 A.O.A. AND WITHIN TAXIWAY G T.S.A. SUGGESTED SEQUENCE OF CONSTRUCTION**

- CLOSE TAXIWAY G (PARTIALLY) AND PLACE TEMPORARY BARRICADES AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- PLACE LATHE AND RIBBON AT AIR OPERATIONS AREA (A.O.A.) FOR RUNWAYS WITHIN PROJECT LIMITS.
- PHASE 1B SHALL BE COMPLETED WITHIN 27 CALENDAR DAYS.
- INSTALL TEMPORARY AIRFIELD CABLING.
- REMOVE EXISTING P.C.C. PAVEMENT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- INSTALL NEW STORM SEWER, UNDERDRAIN AND DRAINAGE STRUCTURES.
- PLACE NEW P.C.C. PAVEMENT.
- STABILIZE PHASE 1 PROJECT LIMITS IN ACCORDANCE WITH THE PROJECT STORMWATER POLLUTION PREVENTION PLAN.

PHASE 1

ILL. CONTRACT: DU079  
 ILL. LETTING ITEM: 1A  
 ILL. PROJECT: DPA-3891  
 A.I.P. PROJECT: 3-17-0017-824

| NUMBER | BY | DATE |
|--------|----|------|
|        |    |      |



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

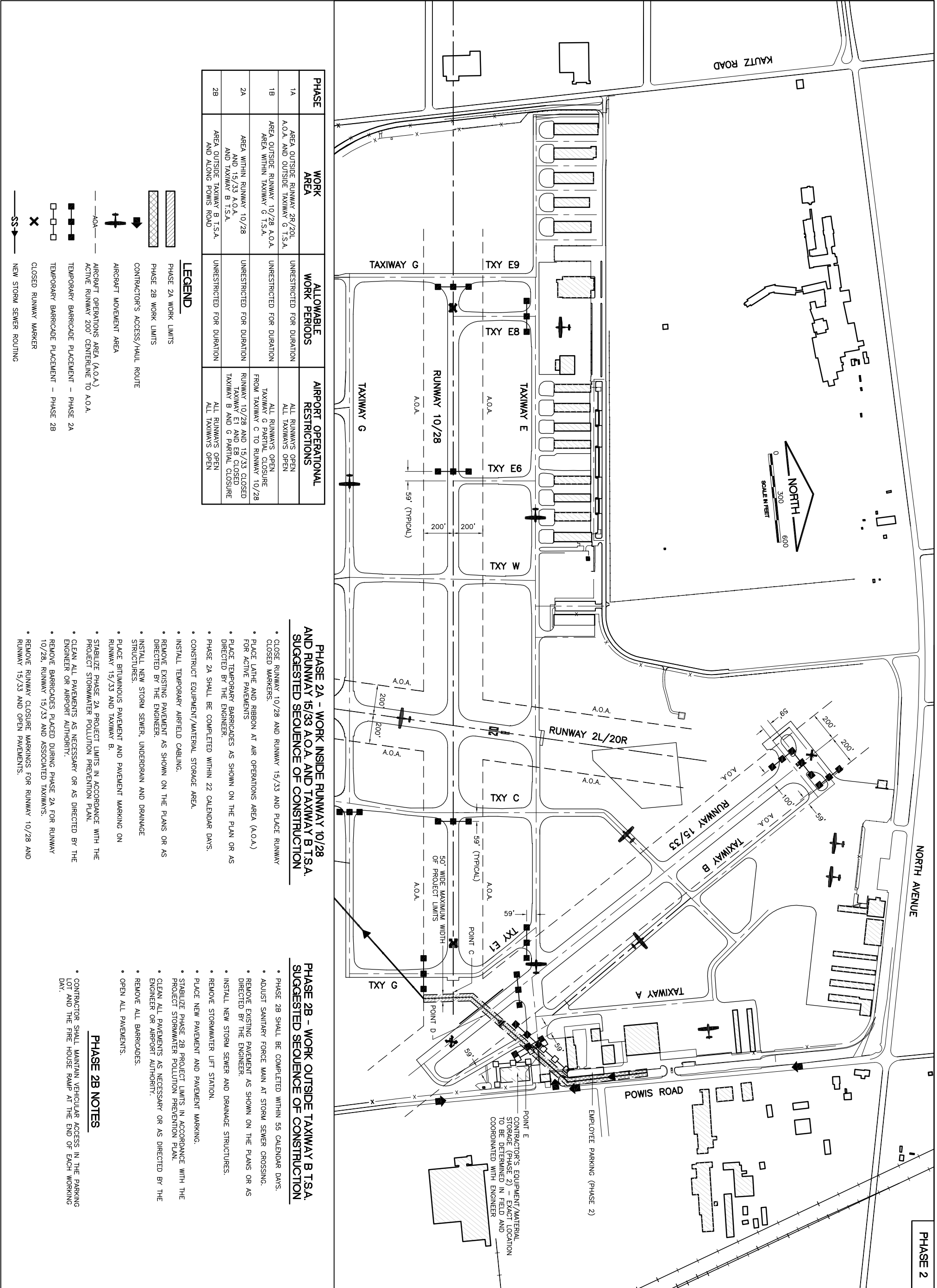
**SEQUENCE OF CONSTRUCTION  
 PER AC 150/5370 - 2E (LATEST EDITION) - SHEET 1**

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**DPA**  
 DuPage Airport

|              |                |
|--------------|----------------|
| DESIGN BY:   | ARM            |
| DRAWN BY:    | JRO            |
| CHECKED BY:  | ARM            |
| APPROVED BY: | DLP            |
| DATE:        | 07/23/2010     |
| JOB No:      | 08257-03-00    |
| SHEET        | 4 OF 25 SHEETS |



| PHASE | WORK AREA  | ALLOWABLE WORK PERIODS    | AIRPORT OPERATIONAL RESTRICTIONS   |
|-------|--|---------------------------|--|
| 1A    | AREA OUTSIDE RUNWAY 2R/20L A.O.A. AND OUTSIDE TAXIWAY G T.S.A. | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>ALL TAXIWAYS OPEN  |
| 1B    | AREA OUTSIDE RUNWAY 10/28 A.O.A. AREA WITHIN TAXIWAY G T.S.A.  | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>TAXIWAY G PARTIAL CLOSURE FROM TAXIWAY C TO RUNWAY 10/28           |
| 2A    | AREA WITHIN RUNWAY 10/28 AND 15/33 A.O.A. AND TAXIWAY B T.S.A. | UNRESTRICTED FOR DURATION | RUNWAY 10/28 AND 15/33 CLOSED TAXIWAY E1 AND E8 CLOSED TAXIWAY B AND G PARTIAL CLOSURE |
| 2B    | AREA OUTSIDE TAXIWAY B T.S.A. AND ALONG POWIS ROAD             | UNRESTRICTED FOR DURATION | ALL RUNWAYS OPEN<br>ALL TAXIWAYS OPEN  |

- LEGEND**
- ▨ PHASE 2A WORK LIMITS
  - ▨ PHASE 2B WORK LIMITS
  - ➡ CONTRACTOR'S ACCESS/HAUL ROUTE
  - ✈ AIRCRAFT MOVEMENT AREA
  - A.O.A. AIRCRAFT OPERATIONS AREA (A.O.A.)
  - ACTIVE RUNWAY 200' CENTERLINE TO A.O.A.
  - TEMPORARY BARRICADE PLACEMENT - PHASE 2A
  - TEMPORARY BARRICADE PLACEMENT - PHASE 2B
  - ✕ CLOSED RUNWAY MARKER
  - NEW STORM SEWER ROUTING

**PHASE 2A - WORK INSIDE RUNWAY 10/28 AND RUNWAY 15/33 A.O.A. AND TAXIWAY B T.S.A. SUGGESTED SEQUENCE OF CONSTRUCTION**

**PHASE 2B - WORK OUTSIDE TAXIWAY B T.S.A. SUGGESTED SEQUENCE OF CONSTRUCTION**

- CLOSE RUNWAY 10/28 AND RUNWAY 15/33 AND PLACE RUNWAY CLOSED MARKERS.
- PLACE LATHE AND RIBBON AT AIR OPERATIONS AREA (A.O.A.) FOR ACTIVE PAVEMENTS
- PLACE TEMPORARY BARRICADES AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- PHASE 2A SHALL BE COMPLETED WITHIN 22 CALENDAR DAYS.
- CONSTRUCT EQUIPMENT/MATERIAL STORAGE AREA.
- INSTALL TEMPORARY AIRFIELD CABLING.
- REMOVE EXISTING PAVEMENT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- INSTALL NEW STORM SEWER, UNDERDRAIN AND DRAINAGE STRUCTURES.
- PLACE BITUMINOUS PAVEMENT AND PAVEMENT MARKING ON RUNWAY 15/33 AND TAXIWAY B.
- STABILIZE PHASE 2A PROJECT LIMITS IN ACCORDANCE WITH THE PROJECT STORMWATER POLLUTION PREVENTION PLAN.
- CLEAN ALL PAVEMENTS AS NECESSARY OR AS DIRECTED BY THE ENGINEER OR AIRPORT AUTHORITY.
- REMOVE BARRICADES PLACED DURING PHASE 2A FOR RUNWAY 10/28, RUNWAY 15/33 AND ASSOCIATED TAXIWAYS.
- REMOVE RUNWAY CLOSURE MARKINGS FOR RUNWAY 10/28 AND RUNWAY 15/33 AND OPEN PAVEMENTS.

- PHASE 2B SHALL BE COMPLETED WITHIN 55 CALENDAR DAYS.
- ADJUST SANITARY FORCE MAIN AT STORM SEWER CROSSING.
- REMOVE EXISTING PAVEMENT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- INSTALL NEW STORM SEWER AND DRAINAGE STRUCTURES.
- REMOVE STORMWATER LIFT STATION.
- PLACE NEW PAVEMENT AND PAVEMENT MARKING.
- STABILIZE PHASE 2B PROJECT LIMITS IN ACCORDANCE WITH THE PROJECT STORMWATER POLLUTION PREVENTION PLAN.
- CLEAN ALL PAVEMENTS AS NECESSARY OR AS DIRECTED BY THE ENGINEER OR AIRPORT AUTHORITY.
- REMOVE ALL BARRICADES.
- OPEN ALL PAVEMENTS.

**PHASE 2B NOTES**

- CONTRACTOR SHALL MAINTAIN VEHICULAR ACCESS IN THE PARKING LOT AND THE FIRE HOUSE RAMP AT THE END OF EACH WORKING DAY.

|   |   |   |   |  |
|---|---|---|---|--|
| <p>ILL. CONTRACT: DU079<br/>                 ILL. LETTING ITEM: 1A<br/>                 ILL. PROJECT: DPA-3891<br/>                 A.I.P. PROJECT: 3-17-0017-824</p> | <p>SURVEY BOOK # ###<br/>                 REVISIONS<br/>                 NUMBER BY DATE</p> | <p>0 1 2<br/>                 THIS BAR IS EQUAL TO 2"<br/>                 AT FULL SCALE (34X22).</p> | <p><b>DUPAGE AIRPORT<br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</b></p> <p><b>SEQUENCE OF CONSTRUCTION<br/>                 PER AC 150/5370-2E (LATEST EDITION) - SHEET 2</b></p> | <p>© Copyright CMT, Inc.<br/> <b>CMT</b><br/>                 CRAWFORD, MURPHY &amp; TILLY, INC.<br/>                 CONSULTING ENGINEERS<br/>                 License No. 184-000613</p> <p><b>DPA</b><br/>                 DuPage Airport</p> |
| <p>DESIGN BY: ARM<br/>                 DRAWN BY: JRO</p>  | <p>CHECKED BY: ARM<br/>                 APPROVED BY: DLP</p>                                | <p>DATE: 07/23/2010<br/>                 JOB No: 08257-03-00</p>                                      | <p>DESIGN BY: ARM<br/>                 DRAWN BY: JRO</p>  | <p>SHEET 5 OF 25 SHEETS</p>  |



**GENERAL NOTES:**

- ALL CONSTRUCTION, SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION) **PAYMENT FOR MAINTENANCE OF TRAFFIC AND REQUIREMENTS OF THE SEQUENCE OF CONSTRUCTION SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.**
- THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS, HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT WHO WILL ISSUE APPROPRIATE NOTAMS. THE MOVING AND MAINTENANCE OF ALL TRAFFIC CONTROL SIGNS, BARRICADES, LAHÉ/RIBBON AT A.O.A. LIMITS ETC. SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AND SHALL BE INCIDENTAL TO THE CONTRACT.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE HEIGHTED TO PREVENT BLOWING, HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 701901-01, TYPE II. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
- THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE AIRPORT DIRECTOR.
- EXISTING AREAS BEYOND PROJECT LIMITS **INCLUDING THE HAIL ROADS** DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
- EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE AIRPORT DIRECTOR, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS AND HAIL ROADS. THE COST OF SWEEPING SHALL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR'S WORK WITHIN AIRCRAFT OPERATIONS AREA (A.O.A.) SHALL BE EXPEDITED TO MINIMIZE ANY TEMPORARY CLOSURE TIME.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS. AT NO ADDITIONAL COST TO THE OWNER, PAVEMENT SHALL BE CONTINUALLY SWEEP TO PROVIDE A DEBRIS FREE SURFACE DURING ALL HAIL ROAD OPERATIONS. THIS COST OF CROSSING GUARDS, PAVEMENT REPAIR OR ANY NECESSARY ITEMS ASSOCIATED WITH THE SEQUENCE OR STAGING SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAIL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- IF REQUESTED BY THE AIRPORT, FAA, OR ENGINEER, REMOVAL ITEMS/MATERIALS SHALL BE DELIVERED TO A DESIGNATED LOCATION AT THE AIRPORT MAINTENANCE BUILDING. IF NOT REQUESTED, MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY (INCIDENTAL), AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LAHÉ AND RIBBON ALONG THE AIR OPERATIONS AREA (A.O.A.) LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS.

**DESIGN AIRCRAFT APPROACH CATEGORY: B  
 DESIGN AIRPORT GROUP: II**

|  |   |
|--|---|
| MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'<br>POINT "A" - @ PROJECT LIMITS<br>CLOSEST POINT OF CONSTRUCTION TO RUNWAY 28/20L<br>STA. 117+38.40, 434.5' LT.<br>BASELINE A<br>GEOGRAPHIC NAD 83:<br>LATITUDE: 41°54'24.82"<br>LONGITUDE: 88°14'37.00"<br>POTENTIAL OBJECT ELEVATION: 777.4 =<br>(GROUND ELEVATION 752.4 + EXCAVATION 25')                       | MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'<br>POINT "B" - @ RUNWAY 10/28 A.O.A.<br>CLOSEST POINT OF CONSTRUCTION TO RUNWAY 10/28<br>STA. 138+29.00, 171.5' LT.<br>BASELINE A<br>GEOGRAPHIC NAD 83:<br>LATITUDE: 41°54'41.52"<br>LONGITUDE: 88°14'26.82"<br>POTENTIAL OBJECT ELEVATION: 775.6 =<br>(GROUND ELEVATION 750.6 + EXCAVATION 25')  |
| MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'<br>POINT "C" - @ RUNWAY 28 RSA<br>CLOSEST POINT OF CONSTRUCTION TO RUNWAY 10/28 (10/28 AND 15/33 CLOSED)<br>STA. 140+28.70, 17.5' LT.<br>BASELINE A<br>GEOGRAPHIC NAD 83:<br>LATITUDE: 41°54'43.48"<br>LONGITUDE: 88°14'26.49"<br>POTENTIAL OBJECT ELEVATION: 780.2 =<br>(GROUND ELEVATION 755.2 + EXCAVATION 25') | MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'<br>POINT "D" - @ RUNWAY 15/33 CENTERLINE<br>CLOSEST POINT OF CONSTRUCTION TO RUNWAY 15/33 (10/28 AND 15/33 CLOSED)<br>STA. 143+07.00,<br>BASELINE A<br>GEOGRAPHIC NAD 83:<br>LATITUDE: 41°54'45.78"<br>LONGITUDE: 88°14'24.72"<br>POTENTIAL OBJECT ELEVATION: 781.8 =<br>(GROUND ELEVATION 756.8 + EXCAVATION 25')  |
| MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'<br>POINT "E" - @ APRON EDGE<br>CLOSEST POINT OF CONSTRUCTION TO RUNWAY 15/33<br>STA. 147+06.09, 17.5' RT.<br>BASELINE A<br>GEOGRAPHIC NAD 83:<br>LATITUDE: 41°54'48.67"<br>LONGITUDE: 88°14'20.93"<br>POTENTIAL OBJECT ELEVATION: 781.1 =<br>(GROUND ELEVATION 756.1 + EXCAVATION 25')                             | CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAIL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:<br><ul style="list-style-type: none"> <li>REHABILITATION OF RUNWAY 2L/20R - PHASE 1</li> <li>AIRFIELD SIGNAGE UPDATE - PHASE 1</li> </ul> |

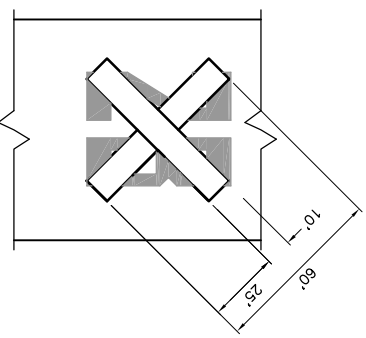
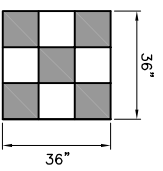
- THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING PAVING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING.
- VEHICLES AND EQUIPMENT OPERATING ON THE AIRPORT SHALL DISPLAY ORANGE AND WHITE FLAGS (SEE DETAIL THIS SHEET) AND HAVE BEACON LIGHTS.
- ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. THE PAVEMENT SHALL BE CONTINUALLY SWEEP TO PROVIDE A DEBRIS-FREE SURFACE DURING HAIL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED AT ALL TIMES UNLESS THE GATE IS MANNED WITH A GUARD (COST INCIDENTAL). THE CONTRACTOR SHALL PROVIDE AN "AUTHORIZED PERSONNEL ONLY" SIGN AT THE ACCESS GATE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COSTS RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BECOME NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, HE SHALL DO SO AT NO EXTRA COST TO THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE/ASPHALT TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A FULL DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION, THE CONTRACTOR SHALL PLACE "RUNWAY CLOSED" MARKERS OVER THE NUMERALS AS DETAILED IN THE PLANS. OTHERWISE, "RUNWAY CLOSED" MARKERS SHALL BE PLACED IN THE TIE AT THE ENDS OF THE RUNWAY AS DETAILED IN THE PLANS.
- RUNWAYS SHALL BE CLOSED DURING ANY TIME THE CONTRACTOR IS WORKING WITHIN THE RUNWAY A.O.A. AND/OR OPEN EXCAVATIONS NOT MEETING THE RUNWAY GRADING CRITERIA EXIST WITHIN 200' OF THE AFFECTED RUNWAY CENTERLINE.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPlicing NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

**LIMITATIONS ON CONSTRUCTION WITHIN  
 AIRPORT OPERATIONS AREA (AOA)**

RUNWAYS:  
 THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION PHASE CHANGES. WORK SHALL BE EXPEDITED IN RUNWAY 10/28 AND RUNWAY 15/33 A.O.A. AND NEAR OPEN TAXIWAYS. THE CONTRACTOR SHALL BE GIVEN 18 CALENDAR DAYS TO COMPLETE WORK WITHIN RUNWAY 10/28 AND RUNWAY 15/33 A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

**CONSTRUCTION EQUIPMENT  
 AND TRUCK SIGNAL FLAG**

ORANGE/WHITE



1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.

2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.

3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.

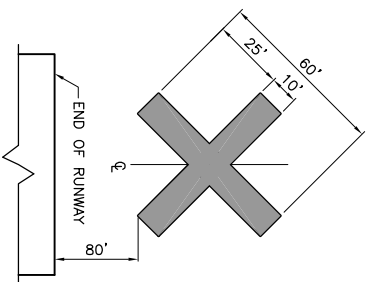
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.

5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS. AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL, THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DETAILED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

**CLOSED RUNWAY MARKER DETAIL**

ON PAVEMENT - NO SCALE



1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.

2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.

3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.

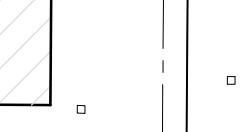
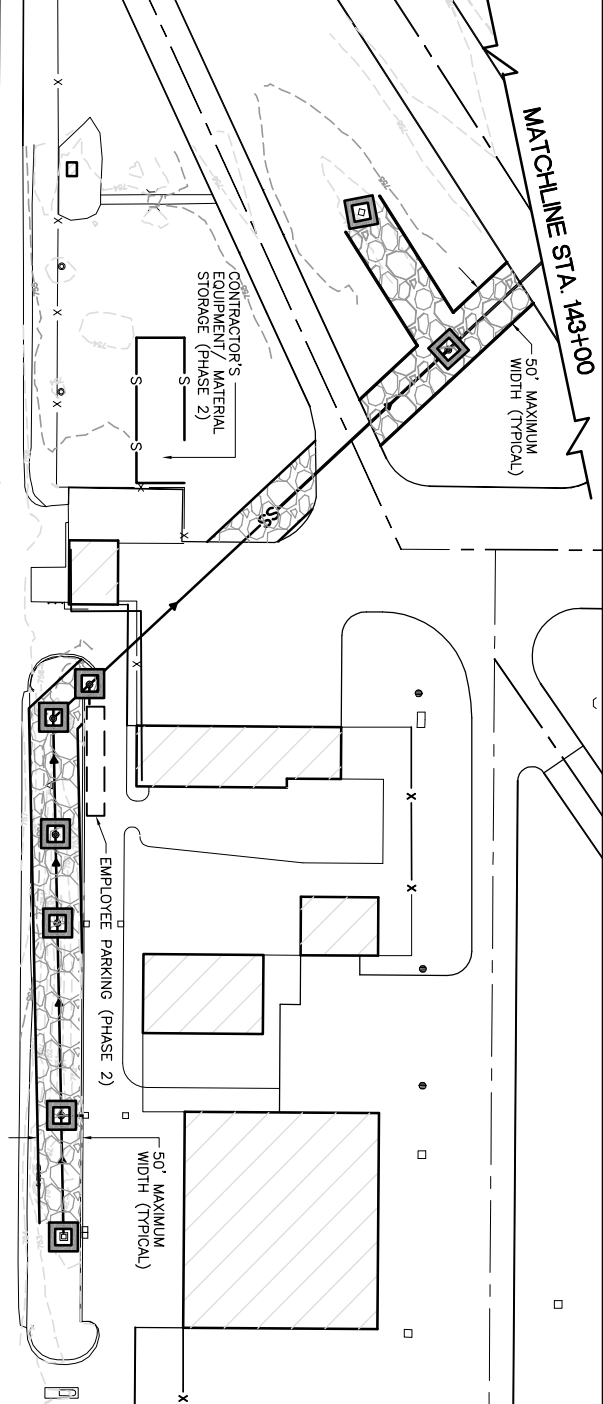
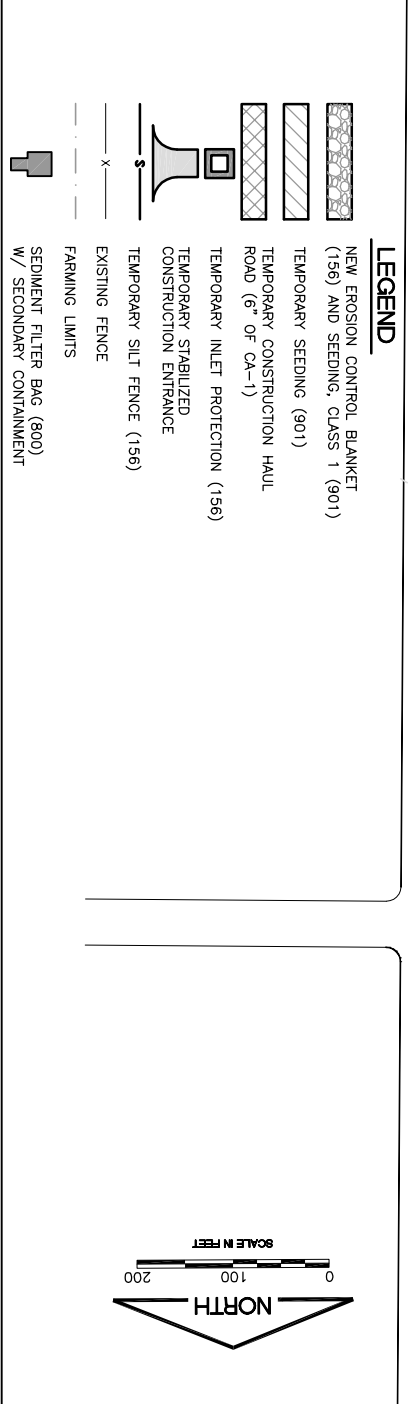
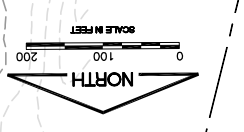
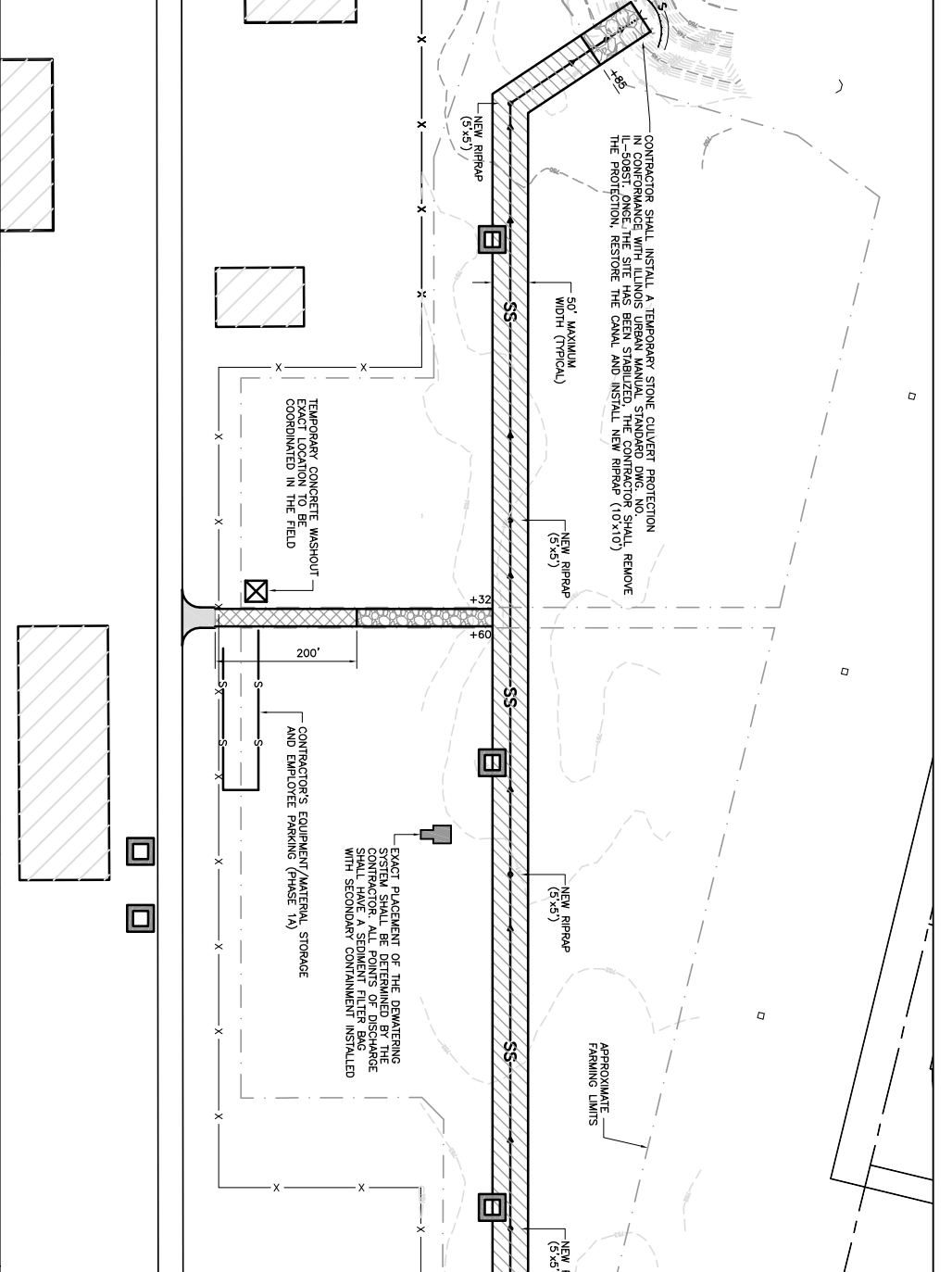
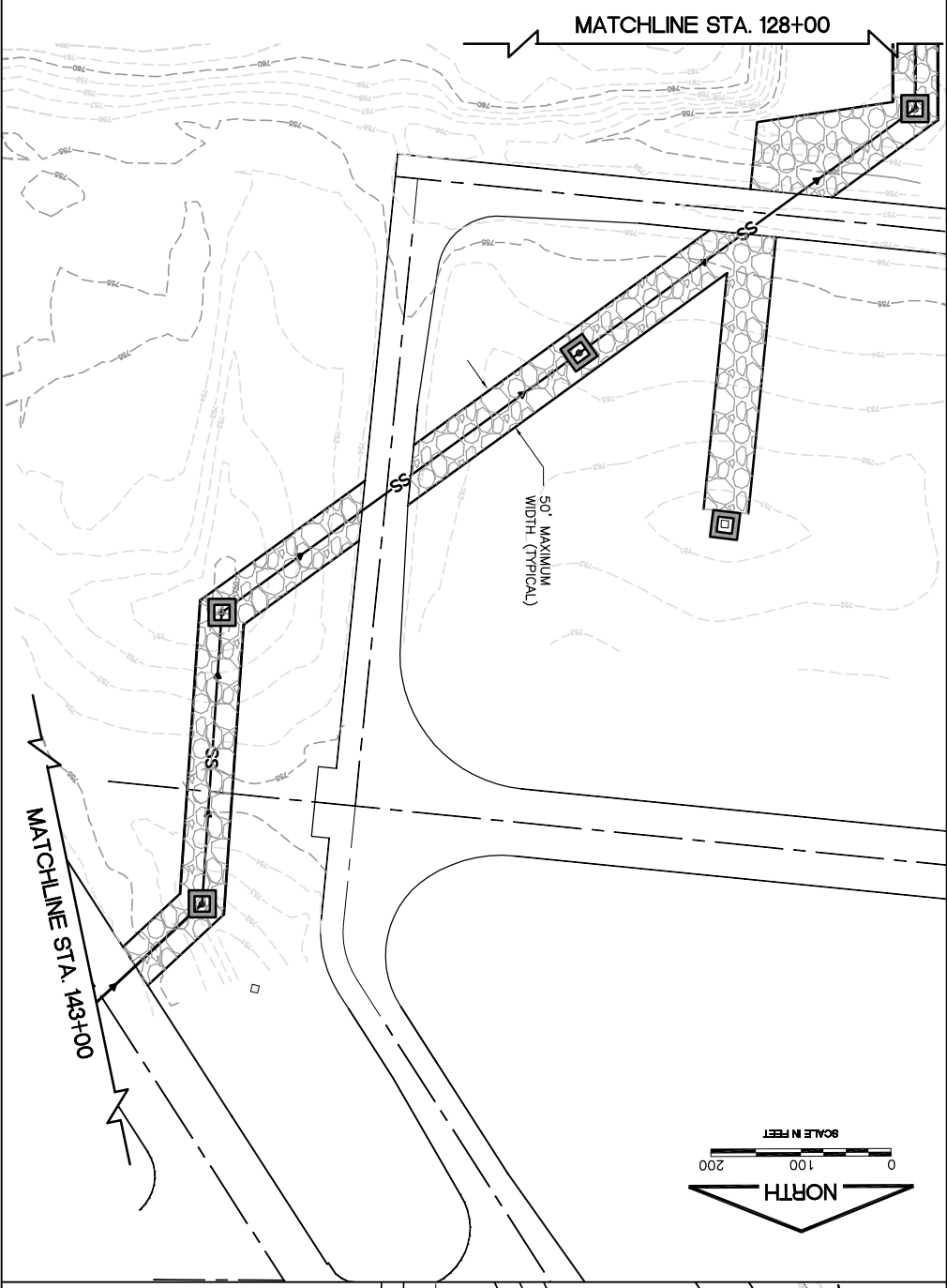
4. MARKERS SHALL BE PLACED 80- FEET OFF THE RUNWAY-END OR AS NOTED.

5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**CLOSED RUNWAY MARKER DETAIL**

OFF PAVEMENT - NO SCALE

|  |   |   |
|--|---|---|
| I.L. CONTRACT: <b>DU079</b><br>I.L. LETTING ITEM: <b>1A</b><br>I.L. PROJECT: <b>DPA-3891</b><br>A.I.P. PROJECT: <b>3-17-0017-824</b> | SURVEY BOOK # ###<br>REVISIONS<br>NUMBER BY DATE  | 0<br>1<br>2<br>THIS BAR IS EQUAL TO 2"<br>AT FULL SCALE (34X22'). |
| <b>DUPAGE AIRPORT<br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</b>             |   |   |
| <b>SEQUENCE OF CONSTRUCTION -<br/>                 GENERAL NOTES AND DETAILS</b>   |   |   |
| DESIGN BY: ARV<br>DRAWN BY: JRO<br>CHECKED BY: ARV<br>APPROVED BY: DLP<br>DATE: 07/23/2010<br>JOB No: 08257-03-00                    | © Copyright CMT, Inc.<br><b>CMT</b><br>CRAWFORD, MURPHY & TILLY, INC.<br>CONSULTING ENGINEERS<br>License No. 184-000613<br> | SHEET 6 OF 25 SHEETS  |

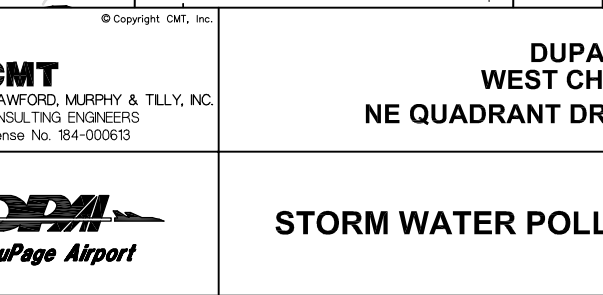


**NOTES FOR EROSION CONTROL**

1. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER PERMIT FOR CONSTRUCTION SITE ACTIVITIES.
2. ALL PROJECT AREAS INCLUDING STOCKPILES, ABANDONED HALL ROADS AND STAGING AREAS AS SHOWN ON THE PLANS, SHALL HAVE 4 INCHES OF TOPSOIL PLACED AND BE SEEDED AND BLANKETED IN ACCORDANCE WITH THE SPECIFICATIONS. AREAS DISTURBED OUTSIDE THE PROJECT LIMITS WILL BE SEEDED AND BLANKETED BY THE CONTRACTOR AT HIS COST AND RESTORED TO ORIGINAL CONDITIONS.
3. FOR DETAILS, SEE STORMWATER POLLUTION PREVENTION NOTES AND DETAILS SHEET.
4. FOR EXACT LOCATIONS OF INLETS, SEE PLAN AND PROFILE PLAN SHEETS.
5. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH 1/2" RAIN EVENT.
6. THE CONTRACTOR SHALL HAVE A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN ON THE SITE AT ALL TIMES.
7. DURING DEMATERING OPERATION, WATER SHALL BE PLUMBED INTO SEDIMENTING/SEDIMENT BASINS/SILT TRAPS IS INDETERMINATE TO THE CONTRACTOR.
8. ANY FIELD/DRAIN TILES THAT ARE DAMAGED IN ACCORDANCE TO THE PLANS/SPECIFICATIONS AND SHALL BE PROTECTED FROM SEDIMENT-LADEN WATER.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AROUND STOCKPILES, STORAGE AREAS AND ANY OTHER AREAS WITHIN THE CITY OF WEST CHICAGO OR ENGINEER.

LEGEND

- NEW EROSION CONTROL BLANKET (156) AND SEEDING, CLASS 1 (901)
- TEMPORARY SEEDING (901)
- TEMPORARY CONSTRUCTION HAUL ROAD (6" OF CA-1)
- TEMPORARY INLET PROTECTION (156)
- TEMPORARY STABILIZED CONSTRUCTION ENTRANCE
- TEMPORARY SILT FENCE (156)
- EXISTING FENCE
- FARMING LIMITS
- SEDIMENT FILTER BAG (900) W/ SECONDARY CONTAINMENT



SURVEY BOOK # ###

| NUMBER | BY | DATE |
|--------|----|------|
|        |    |      |

CONTRACT: DU079  
 I.L. LETTING ITEM: 1A  
 I.L. PROJECT: DPA-3891  
 A.I.P. PROJECT: 3-17-0017-824

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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**DPA**  
 DuPage Airport

DESIGN BY: ARVI  
 DRAWN BY: JRO  
 CHECKED BY: ARVI  
 APPROVED BY: DLP

DATE: 07/23/2010  
 JOB No: 08257-03-00

**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**STORM WATER POLLUTION PREVENTION PLAN**

# STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

## SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN: THIS PROJECT CONSISTS OF CONSTRUCTING A 60" REINFORCED CONCRETE STORM SEWER TRUNK LINE AT THE DUPAGE AIRPORT. THE PROJECT INCLUDES EXCAVATION, EMBANKMENT, DRAINAGE, VARIOUS PAVEMENT ITEMS, FENCING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. PAVEMENT REMOVAL AND REPLACEMENT.
3. FENCING AND ELECTRICAL IMPROVEMENTS.
4. RESTORATION GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND EROSION CONTROL BLANKET.

## AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 5 ACRES OF WHICH 4.6 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES, AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
  2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.
- DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:**
- THE CONSTRUCTION SITE DRAINS INTO THE KRESS CREEK THROUGH A STORM SEWER SYSTEM.
- CONTROLS—EROSION CONTROLS AND SEDIMENT CONTROL**

## 1. DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES. DEAD, DISEASED OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

## 2. DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED. AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN DAYS. STOCKPILES SHALL NOT BE LOCATED IN SPECIAL MANAGEMENT AREAS. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- E. ANY WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION SHALL BE FILTERED.

THE DOWNSIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

## 3. DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

**COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF TOPSOILED, RESEEDED AND/OR SOODED.**

## MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

## CONTRACTORS

1. THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G. (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
2. CERTIFICATION STATEMENT: ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE NATIONAL WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G. OF THIS PERMIT; THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT AND ANY OTHER POTENTIAL HAZARDOUS THAT MAY EXIST ON-SITE.
4. NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

## CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY ON SEPTEMBER 29, 2008.

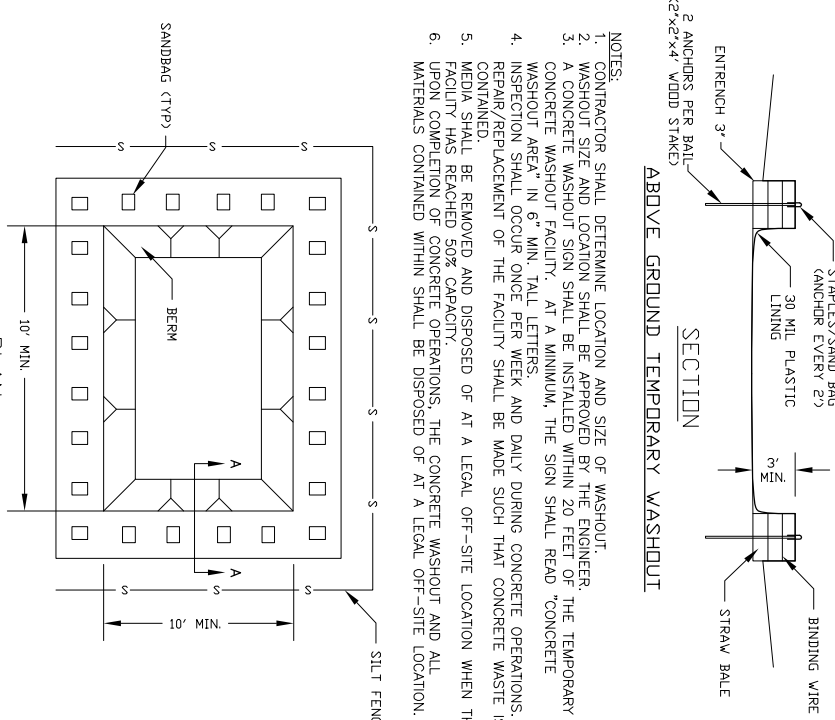
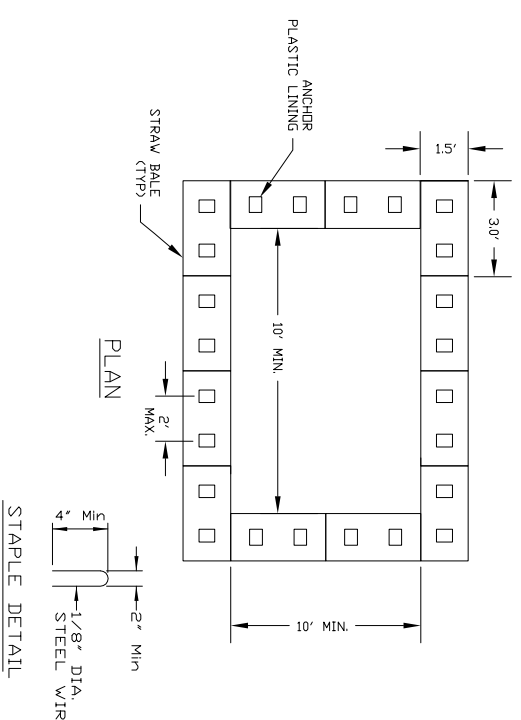
## PROJECT INFORMATION:

ROUTE: \_\_\_\_\_ DUPAGE MARKED: \_\_\_\_\_ CONSTRUCT EAST SIDE DRAINAGE IMPROVEMENTS - PHASE 1  
 SECTION: 29 AND 32 PROJECT NUMBER: DPA-3891  
 COUNTY: DUPAGE CONTRACT NUMBER: N/A

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_  
 PRINTED NAME: \_\_\_\_\_ TITLE: \_\_\_\_\_  
 NAME OF FIRM: \_\_\_\_\_  
 STREET ADDRESS: \_\_\_\_\_  
 CITY, STATE, ZIP: \_\_\_\_\_  
 PHONE NUMBER: \_\_\_\_\_

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT AND SHALL BECOME PART OF THE STORMWATER POLLUTION PREVENTION PLAN. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.



- NOTES:
1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
  2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
  3. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY WASHOUT AREA. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT AREA" IN 6" MIN. TALL LETTERS.
  4. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
  5. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
  6. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.

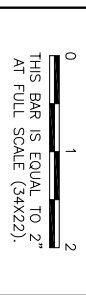
- NOTES:
1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
  2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
  3. SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING AND THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
  4. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY WASHOUT AREA. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT AREA" IN 6" MIN. TALL LETTERS.
  5. THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
  6. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
  7. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
  8. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.

## CONCRETE WASHOUT

NOT TO SCALE

IL CONTRACT: **DU079**  
 IL LETTING ITEM: **1A**  
 IL PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-824**

| SURVEY BOOK # ### | REVISIONS | DATE |
|-------------------|-----------|------|
| NUMBER            | BY        | DATE |



## DUPAGE AIRPORT WEST CHICAGO, ILLINOIS NE QUADRANT DRAINAGE IMPROVEMENTS

## STORM WATER POLLUTION PREVENTION NOTES AND DETAILS

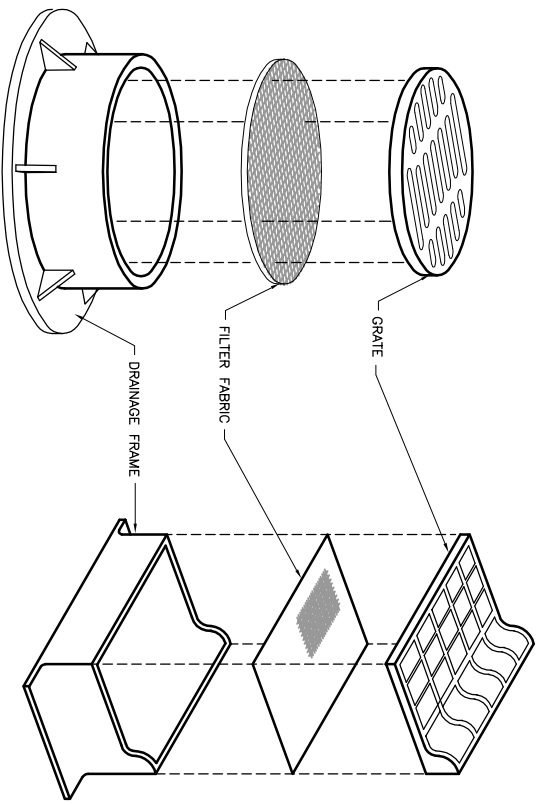
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**DPA**  
 DuPage Airport

|              |                |
|--------------|----------------|
| DESIGN BY:   | ARV            |
| DRAWN BY:    | JRO            |
| CHECKED BY:  | ARV            |
| APPROVED BY: | DLP            |
| DATE:        | 07/23/2010     |
| JOB No:      | 08257-03-00    |
| SHEET        | 8 OF 25 SHEETS |

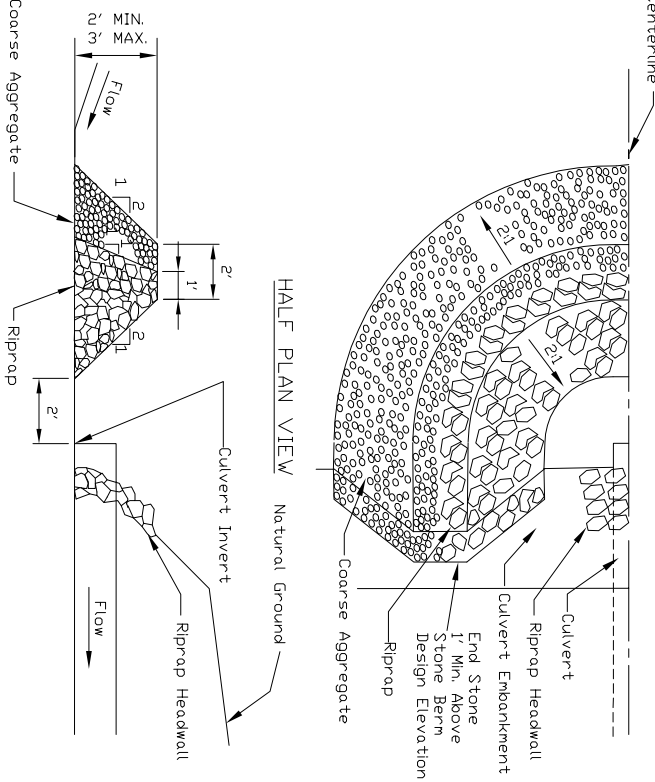




- NOTES:**
1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
  2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
  3. FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
  4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
  5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
  6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO ARI156000 EROSION CONTROL.

**DRAINAGE STRUCTURE FILTER WRAP**

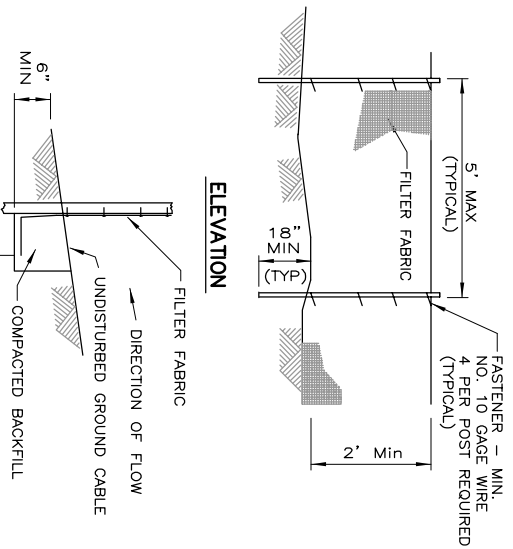
NOT TO SCALE



**CENTERLINE CROSS SECTION  
 CULVERT INLET PROTECTION - STONE**

FROM NRCS STANDARD DRAWING IL-508ST

- Notes:**
1. Sediment shall be removed when the sediment has accumulated to one-half the height of the stone berm.
  2. Coarse aggregate shall meet one of the following IDOT coarse aggregate gradations, CA-1, CA-2, CA-3 or RR-4. Any permanent riprap, such as CA-1, CA-2, CA-3 or RR-4.
  3. Riprap shall meet IDOT gradation RR-3 or RR-4. Any permanent riprap, such as CA-1, CA-2, CA-3 or RR-4.
  4. Coarse aggregate and riprap shall be placed according to construction specification 23 RUCKFILL using placement Method I and Class III compaction.
  5. The maximum drainage area to the culvert being protected is 3 acres.
  6. The stone berm into the culvert embankment a minimum of 1 foot above the design elevation of the stone berm.

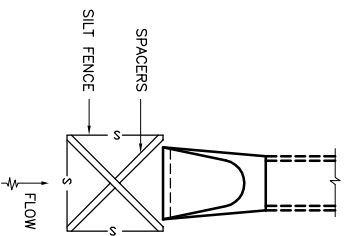


**FABRIC ANCHOR DETAIL**

NOT TO SCALE  
 FROM NRCS STANDARD DRAWING NO. IL-620

**NOTES FOR ALL  
 INLET PROTECTION**

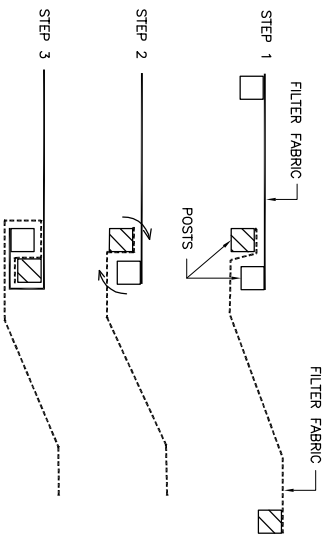
1. SILT FENCE AND SPACERS SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT WILL BE INCLUDED IN THE UNIT PRICE FOR INLET PROTECTION.
2. INSPECTION SHALL BE FREQUENT AND REPAIR AS NEEDED.
3. AFTER FINAL APPROVAL OF THE ENGINEER, SILT FENCE MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS.
4. SILT FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM 1080.03, OF THE IDOT STANDARD SPECIFICATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND REQUIRING SPECIAL PROVISIONS.



**INLET PROTECTION (END SECTION)**

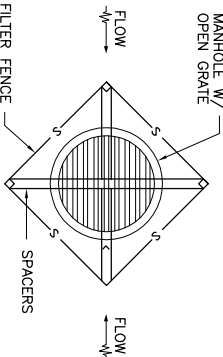
NOT TO SCALE  
 IDOT STANDARD 280001-03

- NOTES:**
1. TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
  2. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM ARI156000 EROSION CONTROL IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND REQUIRING SPECIAL PROVISIONS.
  3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
  4. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.
  5. FENCE POSTS SHALL BE EITHER STANDARD STEEL POSTS OR WOOD POSTS WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.
  6. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
  7. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
  8. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.



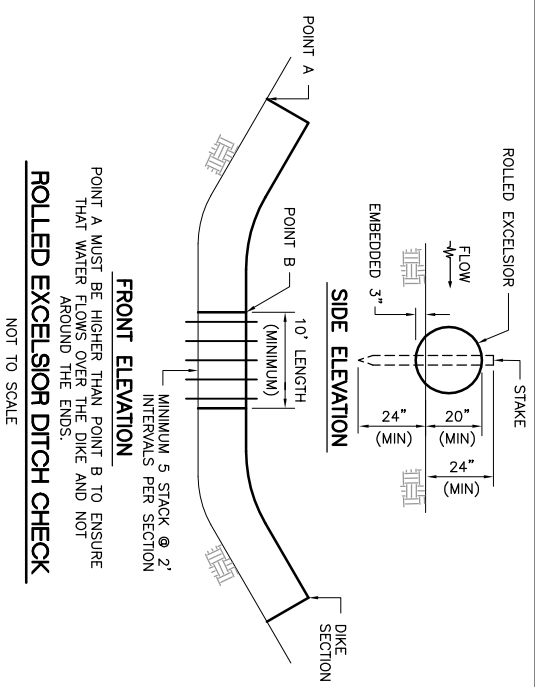
**ATTACHING TWO SILT FENCES**

NOT TO SCALE  
 FROM NRCS STANDARD DRAWING NO. IL-620



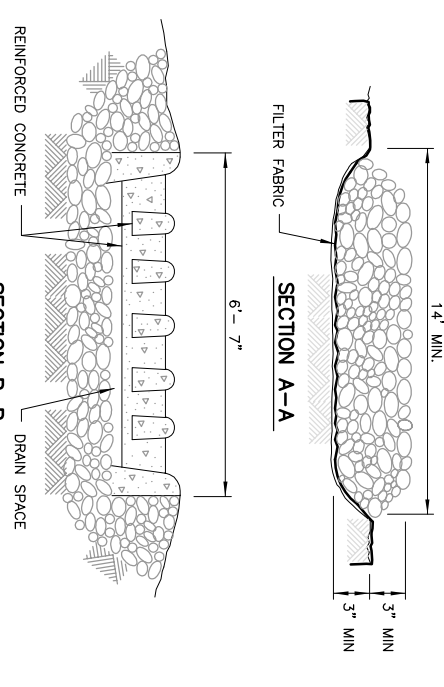
**INLET PROTECTION (INLET/MANHOLES)**

NOT TO SCALE  
 IDOT STANDARD 280001-03



**ROLLED EXCELSIOR DITCH CHECK**

NOT TO SCALE



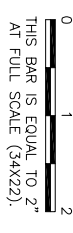
**STABILIZED CONSTRUCTION ENTRANCE**

FROM NRCS STANDARD DRAWING NO. IL-630

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2007.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURER'S SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC, DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE PAID FOR AS ITEM.

ILL. CONTRACT: **DU079**  
 ILL. LETTING ITEM: **1A**  
 ILL. PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-824**

| SURVEY BOOK # | REVISIONS |
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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**STORM WATER POLLUTION PREVENTION DETAILS**

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| DESIGN BY:   | ARM            |
| DRAWN BY:    | JRO            |
| CHECKED BY:  | ARM            |
| APPROVED BY: | DLP            |
| DATE:        | 07/23/2010     |
| JOB No:      | 08257-03-00    |
| SHEET        | 9 OF 25 SHEETS |

### LEGEND

- NEW MANHOLE/END SECTION
- ⊙ EXISTING INLET/MANHOLE/END SECTION
- ⊕ EXISTING FIRE HYDRANT
- ⊕ EXISTING WATER VALVE
- ⊕ EXISTING SIGN
- ⊕ EXISTING ROADWAY LIGHT POLE
- ⊕ EXISTING BASE MOUNTED RUNWAY/TAXIWAY LIGHT
- ⊕ EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING CONDUIT
- ⊕ EXISTING AIRFIELD GUIDANCE SIGN
- 745--- EXISTING CONTOUR
- SS NEW STORM SEWER
- SS EXISTING STORM SEWER
- SM EXISTING SANITARY SEWER
- SM EXISTING SANITARY FORCEMAIN
- FM EXISTING WATERMAIN
- E EXISTING ELECTRICAL CABLE
- T EXISTING TELEPHONE LINE
- G EXISTING GAS LINE
- X EXISTING FENCE LINE
- EXISTING PROPERTY LINE
- EXISTING FAA ELECTRICAL CABLE
- EXISTING RUNWAY 28 END IDENTIFICATION LIGHT CIRCUIT
- EXISTING TAXIWAY E ELECTRICAL CIRCUIT
- EXISTING TAXIWAY B ELECTRICAL CIRCUIT
- ▨ EXISTING 8" PCC PAVEMENT TO BE REMOVED AND REPLACED - TYPE A
- ▨ EXISTING BITUMINOUS PAVEMENT TO BE REMOVED AND REPLACED - TYPE A
- ▨ EXISTING BITUMINOUS PAVEMENT TO BE REMOVED AND REPLACED - TYPE B
- NEW EXPLORATORY TRENCH/VACUUM EXCAVATION

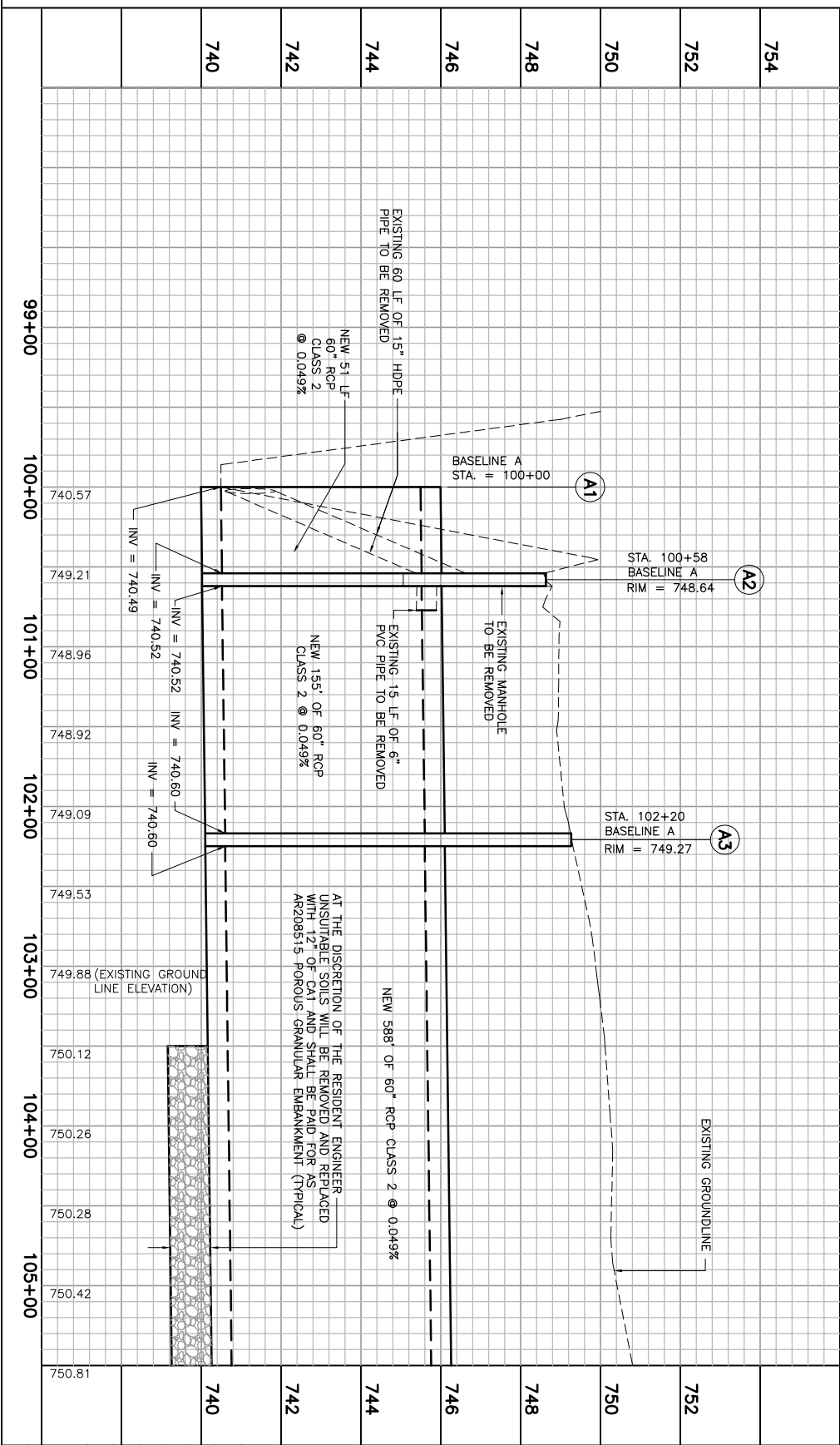
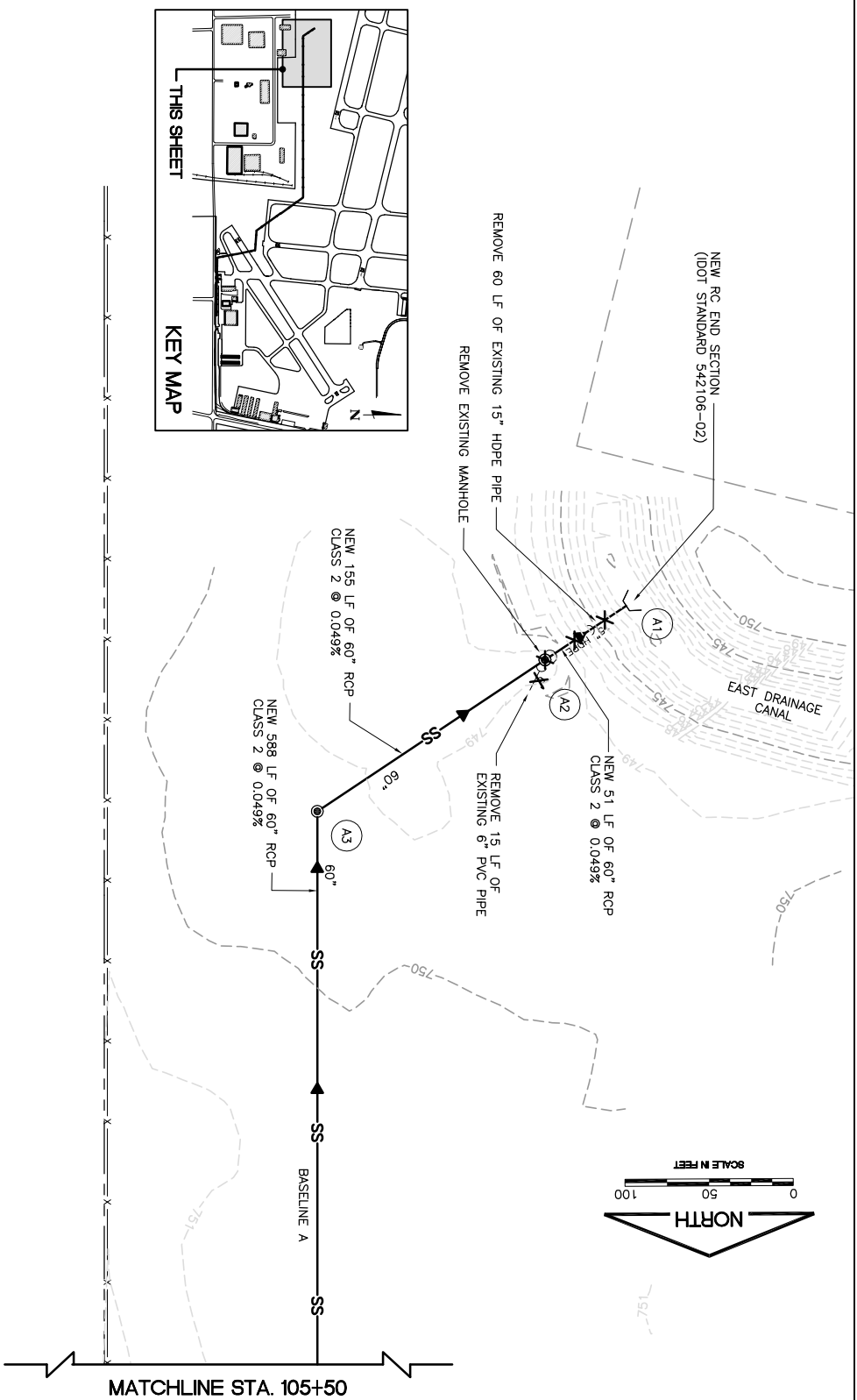
THE LOCATION, SIZE AND TYPE OF MATERIALS OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE REGARDING THE LOCATION OF THEIR FACILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED BY THE CONTRACTOR'S ACTIVITY. IT SHALL BE RESTORED TO SERVICE AS SOON AS POSSIBLE. SCHEDULED WORK SHALL BE REQUIRED, THE CONTRACTOR SHALL BE NOTIFIED TWO (2) WORKING DAYS IN ADVANCE OF THE SCHEDULED INTERRUPTION.

### NOTES

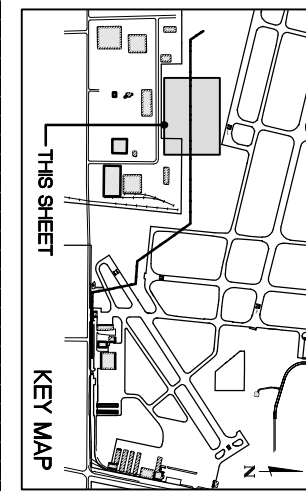
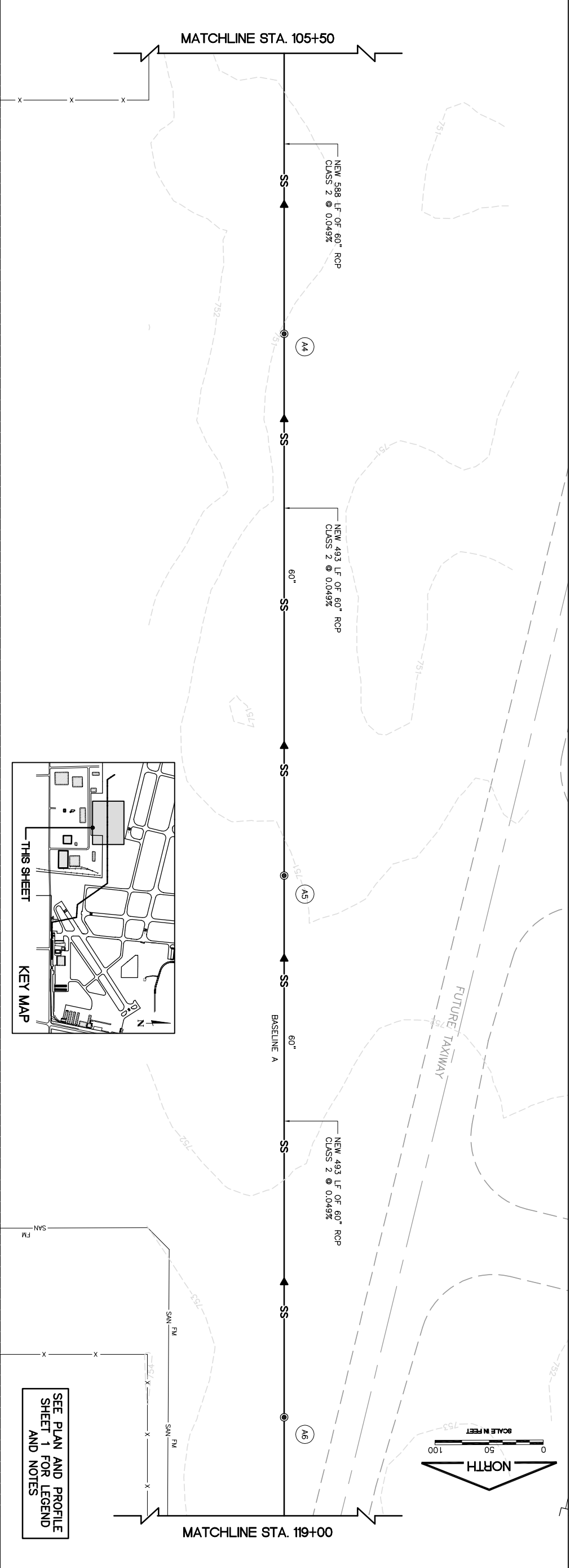
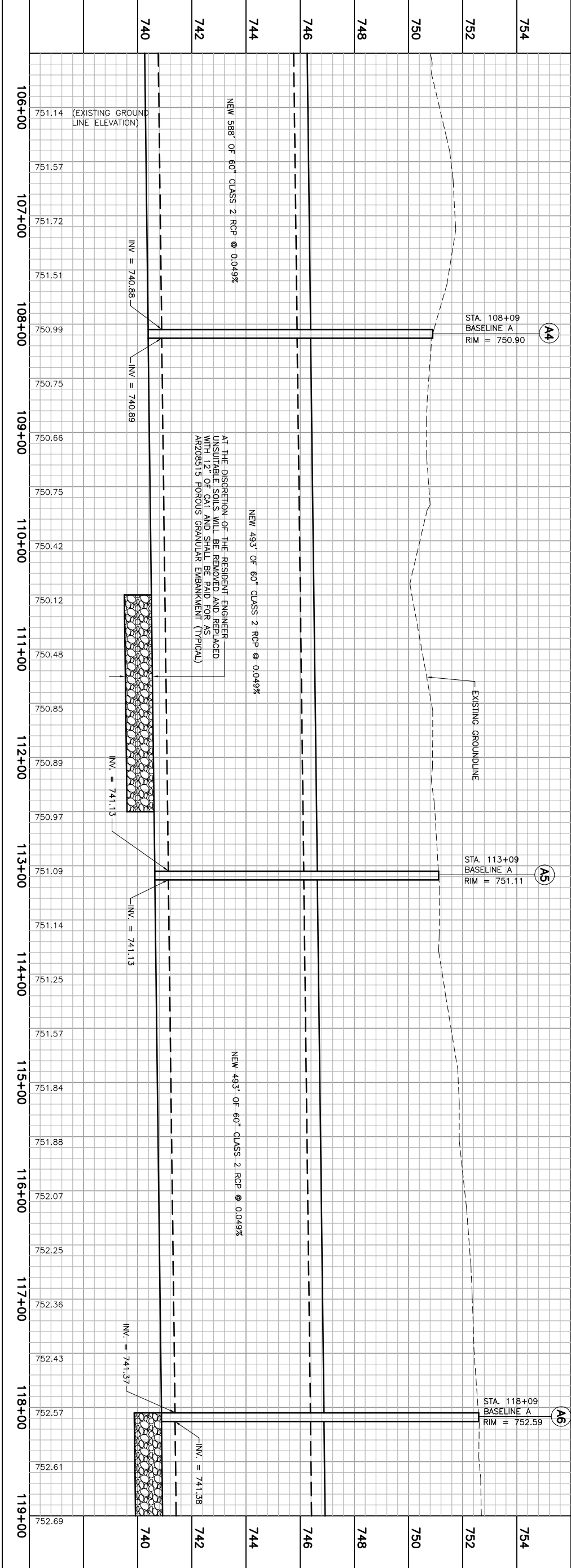
1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM. ANY DAMAGE TO PAVEMENT BEYOND THE LIMITS SHOWN ON THE PLANS SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THESE AREAS SHALL BE SAW CUT TO A UNIFORM WIDTH.
2. TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. THE EXISTING PIPE TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PIPE SHALL BE CONSIDERED INCIDENTAL TO A45701900.
4. CONTRACTOR SHALL FIELD VERIFY EXISTING UNDERDRAIN INVERTS BEFORE INSTALLING NEW UNDERDRAIN CONNECTIONS.
5. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
6. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
7. REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES, CORING AND CONCRETE COLLARS TO FACILITATE CONNECTIONS OF NEW STORM SEWER PIPE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.
8. IN AREAS WHERE MANHOLES, UNDERDRAIN AND/OR STORM SEWER PIPE BEING REMOVED IS BELOW LIMITS OF PROPOSED OR FUTURE PAVEMENTS, TRENCH SHALL BE BACKFILLED WITH COMPACTED CRUSHED AGGREGATE BACKFILL (209). COST OF BACKFILLING SHALL BE INCIDENTAL TO THE COST OF THE PIPE REMOVAL.
9. MAXIMUM WIDTH OF TRENCH BACKFILL MEASURED FOR PAYMENT SHALL BE IN ACCORDANCE WITH THE TRENCH DETAIL TABLE ON SHEET 19.
10. ALL PAVEMENT MARKINGS SHALL BE RESTORED TO MATCH THE EXISTING MARKINGS PRIOR TO PAVEMENT REMOVAL. MARKING SHALL BE PAID FOR ON A SQUARE FOOT BASIS AS DETAILED IN THE SPECIFICATION.

### LIGHTING NOTES

1. CONNECT PROPOSED CABLE AT HANDHOLE, GUIDANCE SIGN, TAXIWAY LIGHT, RUNWAY LIGHT OR SPLICE CAN. THE COST OF SPLICING SHALL BE INCIDENTAL TO AIRFIELD LIGHTING CABLE INSTALLATION.
2. THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY/TAXIWAY LIGHTING CIRCUITS ARE OPERATIONAL AT THE END OF EACH WORKING DAY.
3. THE ROUTING OF PROPOSED AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD COORDINATED WITH THE RESIDENT ENGINEER.
4. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHALL BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
5. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
6. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
7. EACH CABLE SHALL HAVE A MINIMUM OF 6' SLACK IN EACH ELECTRICAL HANDHOLE/MANHOLE AND SHALL BE TAGGED AND PAID FOR PER LINEAL FOOT. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR SPLICE CAN.

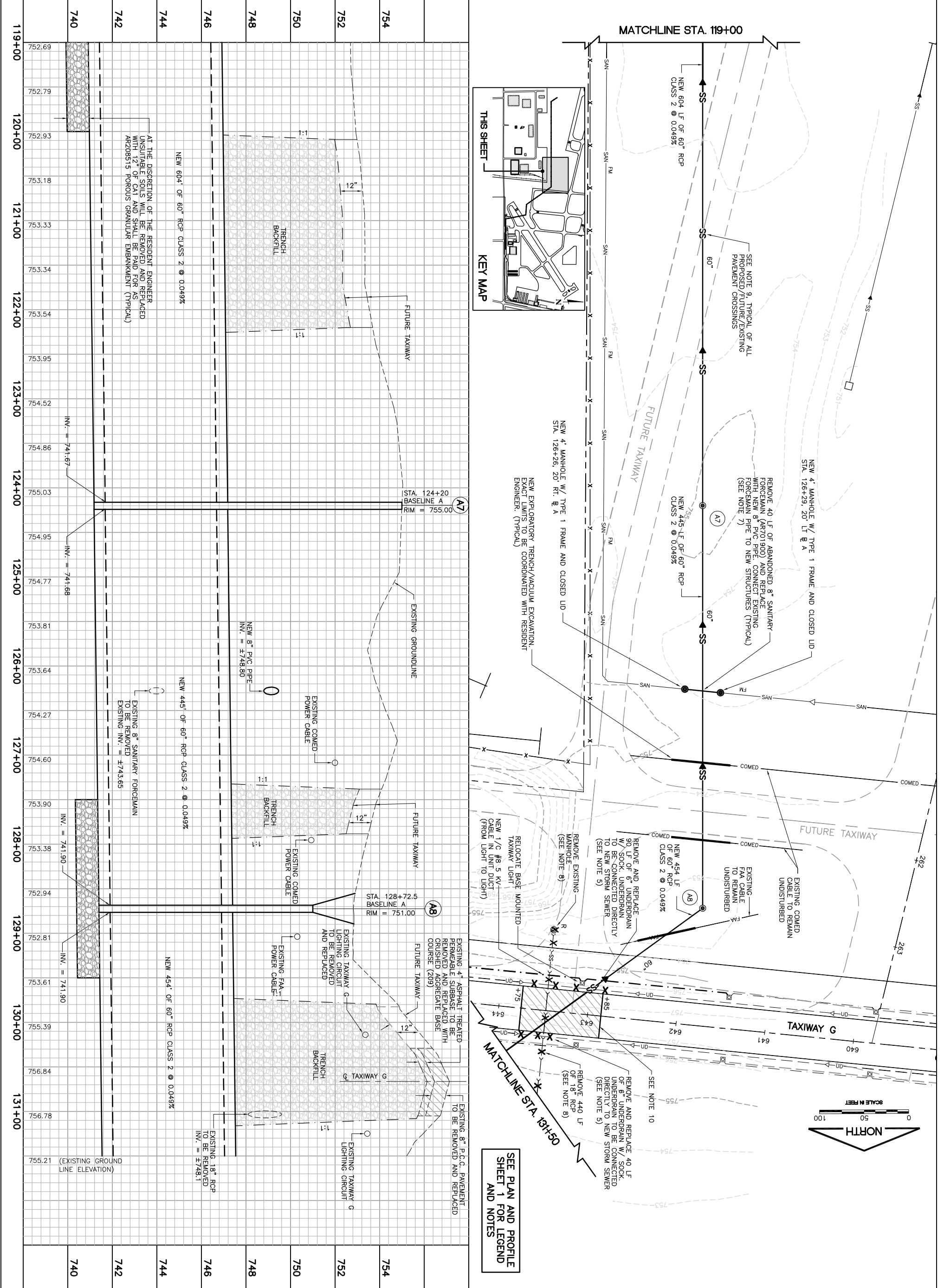


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|--|--|---|---|
| I.L. CONTRACT: <b>DU079</b><br>I.L. LETTING ITEM: <b>1A</b><br>I.L. PROJECT: <b>DPA-3891</b><br>A.I.P. PROJECT: <b>3-17-0017-824</b> | SURVEY BOOK # ###<br>REVISIONS<br>NUMBER BY DATE | THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).<br>0 1 2 | <p><b>DUPAGE AIRPORT</b><br/> <b>WEST CHICAGO, ILLINOIS</b><br/> <b>NE QUADRANT DRAINAGE IMPROVEMENTS</b></p> <p><b>PLAN AND PROFILE - SHEET 1</b></p> <p>Copyright CMT, Inc.<br/> <b>CMT</b><br/>                 CRAWFORD, MURPHY &amp; TILLY, INC.<br/>                 CONSULTING ENGINEERS<br/>                 License No. 184-000613</p> <p><b>DPA</b><br/>                 DuPage Airport</p> |
| DESIGN BY: AMM<br>DRAWN BY: JBO<br>CHECKED BY: AMM<br>APPROVED BY: DLP<br>DATE: 07/23/2010<br>JOB No: 08257-03-00                    | SHEET 10 OF 25 SHEETS                            |   |   |



SEE PLAN AND PROFILE SHEET 1 FOR LEGEND AND NOTES

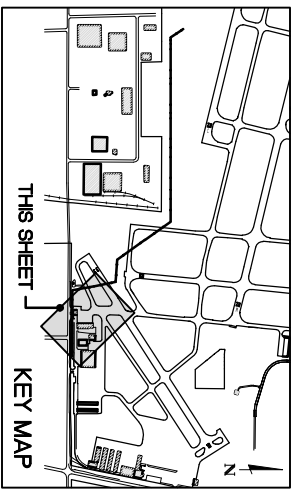
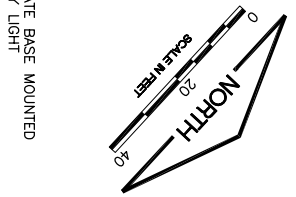
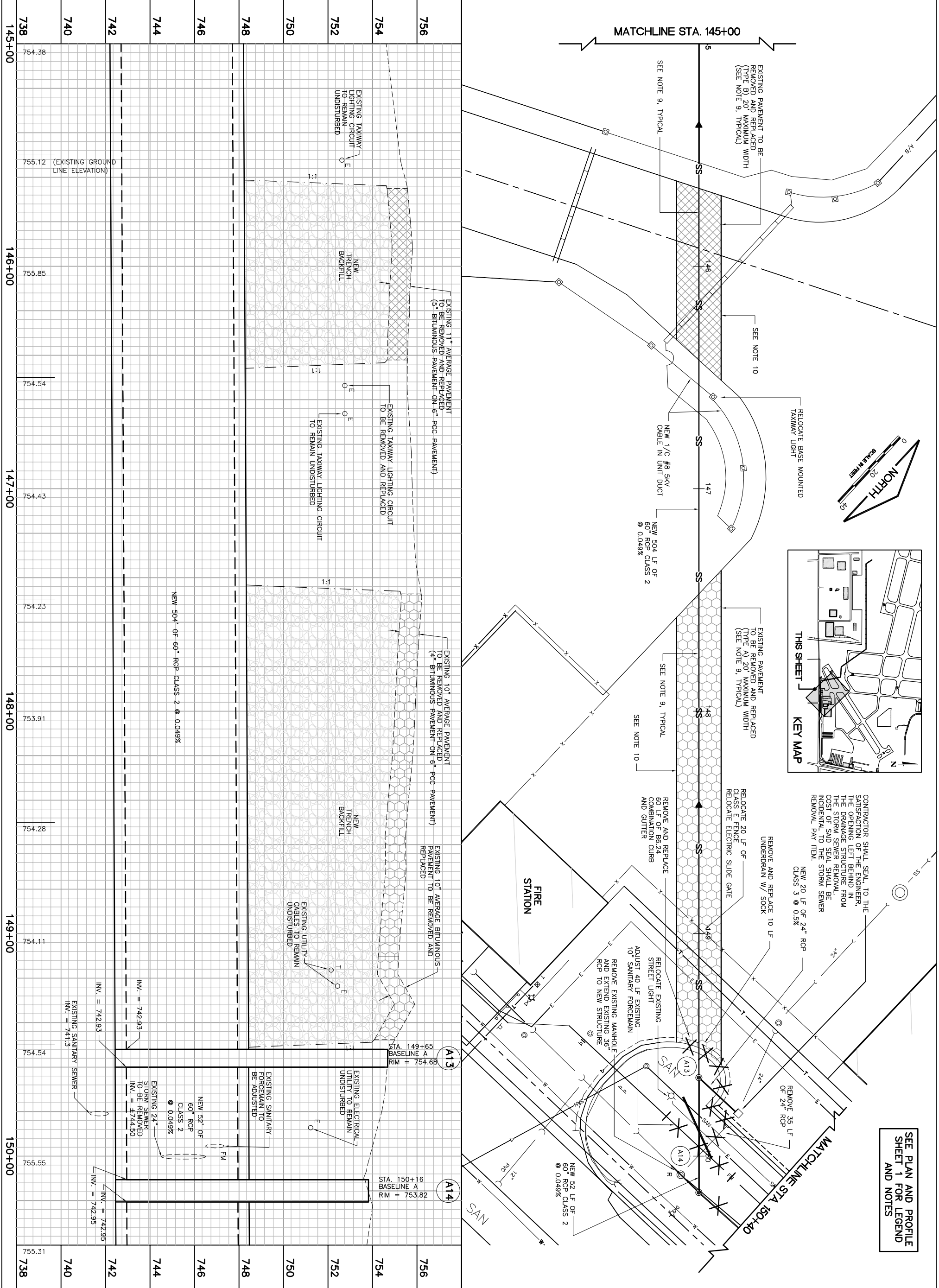
| SHEET 11 OF 25 SHEETS | JOB No: 08257-03-00<br>DATE: 07/23/2010<br>APPROVED BY: DLP<br>CHECKED BY: ARM<br>DESIGN BY: ARM<br>DRAWN BY: JRO | © Copyright CMT, Inc.<br>CRAWFORD, MURPHY & TILLY, INC.<br>CONSULTING ENGINEERS<br>License No. 184-000613 | DUPAGE AIRPORT<br>WEST CHICAGO, ILLINOIS<br>NE QUADRANT DRAINAGE IMPROVEMENTS | SURVEY BOOK # ###<br>REVISIONS<br><table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | NUMBER | BY | DATE |  |  |  |  |  |  | I.L. CONTRACT: DU079<br>I.L. LETTING ITEM: 1A<br>I.L. PROJECT: DPA-3891<br>A.I.P. PROJECT: 3-17-0017-B24 |
|-----------------------|---|---|---|---|--------|----|------|--|--|--|--|--|--|--|
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|                       |   | <b>PLAN AND PROFILE - SHEET 2</b>   |   | THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).<br>  |        |    |      |  |  |  |  |  |  |  |



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| SHEET 12 OF 25 SHEETS  | JOB No: 08257-03-00<br>DATE: 07/23/2010<br>APPROVED BY: DLP<br>CHECKED BY: ARM<br>DRAWN BY: JFO | DESIGN BY: ARM<br>DESIGN BY: JFO | AT THE DISCRETION OF THE RESIDENT ENGINEER INSURABLE SOILS WILL BE REMOVED AND REPLACED WITH 12" OF C&I AND SHALL BE PAID FOR AS AR20851'S POROUS GRANULAR EMBANKMENT (TYPICAL) | NEW 604 LF OF 60" RCP CLASS 2 @ 0.049%<br>NEW 445' OF 60" RCP CLASS 2 @ 0.049%<br>NEW 454' OF 60" RCP CLASS 2 @ 0.049%<br>EXISTING 18" RCP TO BE REMOVED INV. = ±748.1 | 740<br>742<br>744<br>746<br>748<br>750<br>752<br>754<br>754<br>752<br>750<br>748<br>746<br>744<br>742<br>740 |
| <p><b>DUPAGE AIRPORT<br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</b></p> <p><b>PLAN AND PROFILE - SHEET 3</b></p>   |   |                                  |   |  |  |
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| <p>CONTRACT: <b>DU079</b><br/>                 I.L. LETTING ITEM: <b>1A</b><br/>                 I.L. PROJECT: <b>DP-A-3891</b><br/>                 A.I.P. PROJECT: <b>3-17-0017-B24</b></p>  |   |                                  |   |  |  |
| <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p>  |   |                                  |   |  |  |





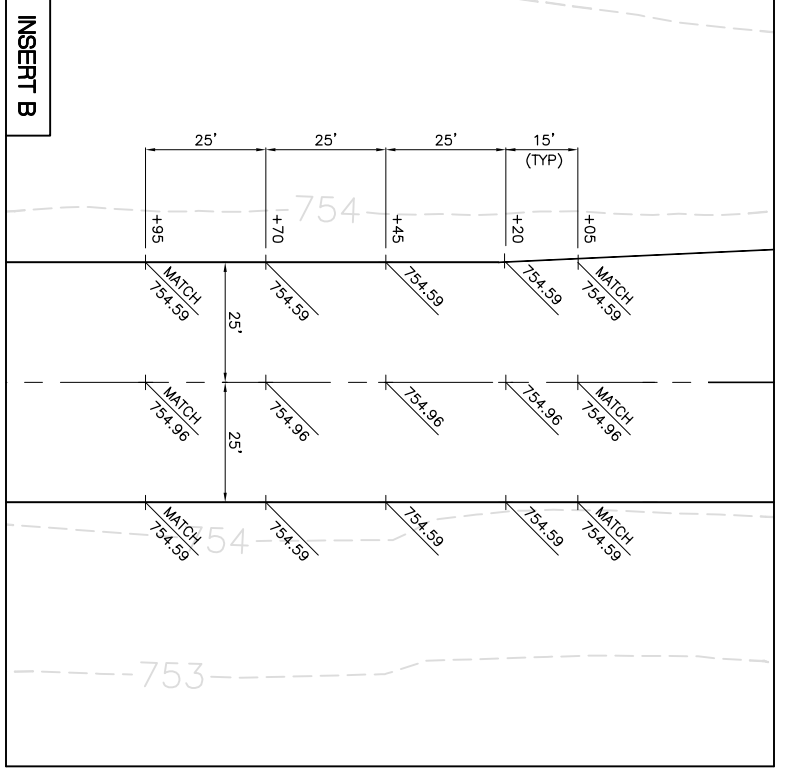
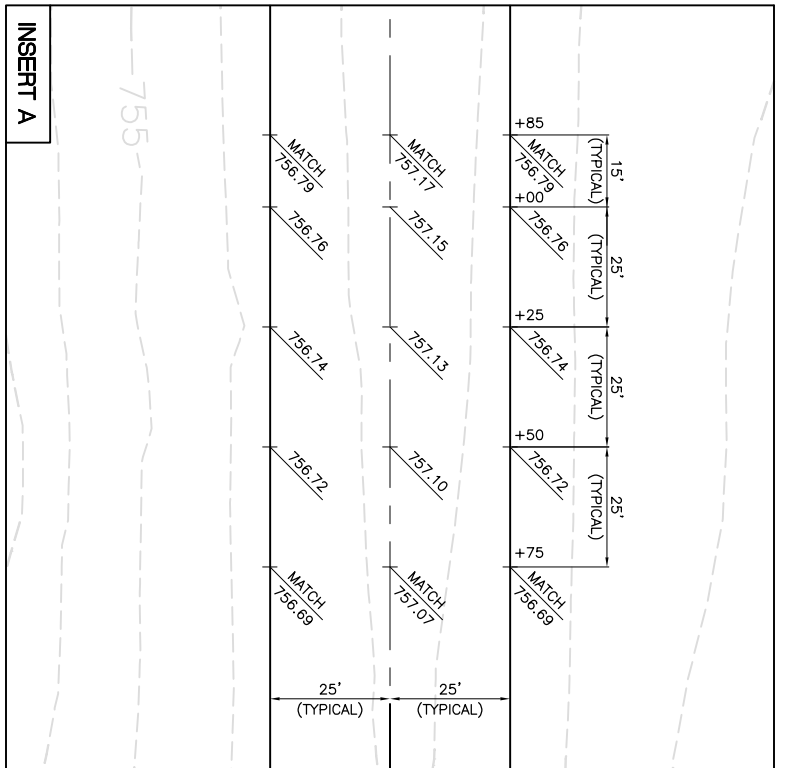


CONTRACTOR SHALL SEAL TO THE SATISFACTION OF THE ENGINEER, THE OPENING LEFT BEHIND IN THE DRAINAGE STRUCTURE FROM THE STORM SEWER REMOVAL. COST OF SAID SEAL SHALL BE INCIDENTAL TO THE STORM SEWER REMOVAL PAY ITEM.

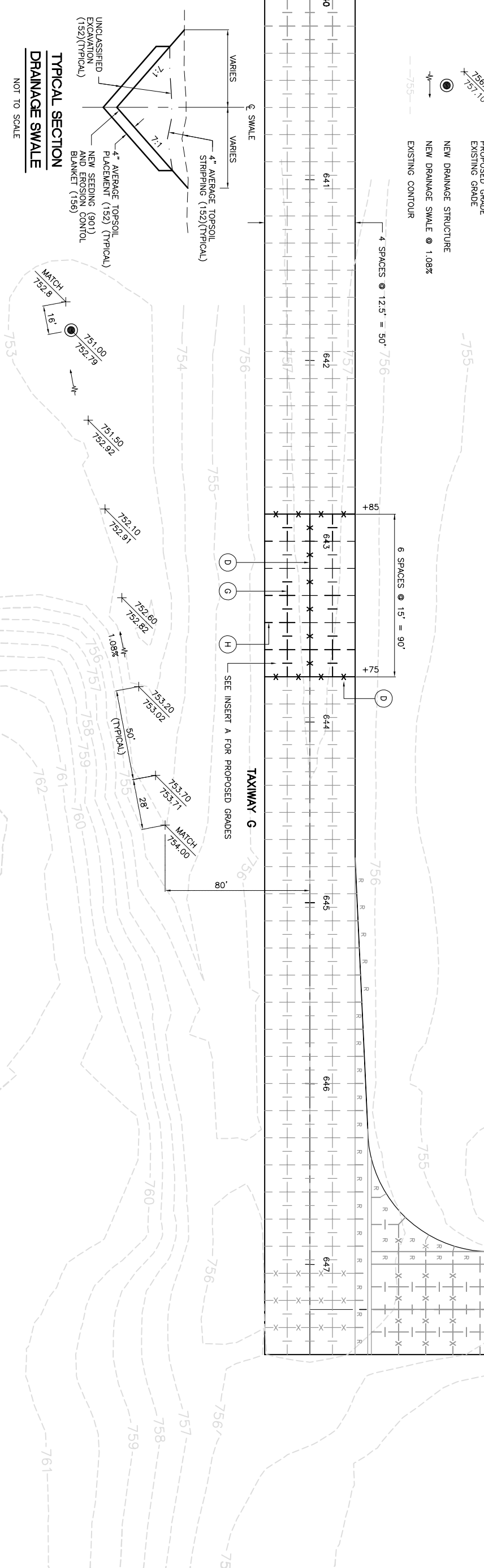
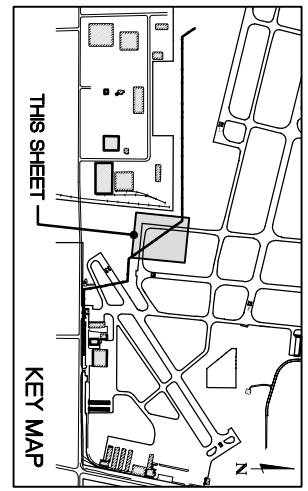
SEE PLAN AND PROFILE SHEET 1 FOR LEGEND AND NOTES

| SHEET 14 OF 25 SHEETS   | JOB No: 08257-03-00<br>DATE: 07/23/2010<br>APPROVED BY: DLP<br>CHECKED BY: ARM<br>DESIGN BY: JRO | STA. 149+65<br>BASELINE A<br>RIM = 754.68 | STA. 150+16<br>BASELINE A<br>RIM = 753.82 | STA. 149+65<br>BASELINE A<br>RIM = 754.68 | STA. 150+16<br>BASELINE A<br>RIM = 753.82 |        |    |      |  |  |  |  |  |  |
|---|--|---|---|---|---|--------|----|------|--|--|--|--|--|--|
| <p><b>DUPAGE AIRPORT</b><br/> <b>WEST CHICAGO, ILLINOIS</b><br/> <b>NE QUADRANT DRAINAGE IMPROVEMENTS</b></p> <p><b>PLAN AND PROFILE - SHEET 5</b></p>  |  |   |   |   |   |        |    |      |  |  |  |  |  |  |
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| I.L. CONTRACT: <b>DU079</b><br>I.L. LETTING ITEM: <b>1A</b><br>I.L. PROJECT: <b>DPA-3891</b><br>A.I.P. PROJECT: <b>3-17-0017-824</b>  |  |   |   |   |   |        |    |      |  |  |  |  |  |  |
| SURVEY BOOK # ###<br>REVISIONS<br><table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> |  |   |   |   |   | NUMBER | BY | DATE |  |  |  |  |  |  |
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| THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).<br>0 1 2   |  |   |   |   |   |        |    |      |  |  |  |  |  |  |





- LEGEND**
- (B) TYPE B - EXPANSION JOINT
  - (D) TYPE D - DOWELED CONSTRUCTION JOINT
  - (E) TYPE E - HINGED CONSTRUCTION JOINT
  - (F) TYPE F - DOWELED CONTRACTION JOINT
  - (G) TYPE G - HINGED CONTRACTION JOINT
  - (H) TYPE H - DUMMY CONTRACTION JOINT
  - R REINFORCED PANEL (SEE JOINT DETAILS)
- THICKENED EDGE  
 PROPOSED GRADE  
 EXISTING GRADE  
 NEW DRAINAGE STRUCTURE  
 NEW DRAINAGE SWALE @ 1.08%  
 EXISTING CONTOUR



IL. CONTRACT: **DU079**  
 IL. LETTING ITEM: **1A**  
 IL. PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-B24**

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SURVEY BOOK # ###  
 SEE INSERT B FOR PROPOSED GRADES

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**PAVEMENT JOINTING / GRADING PLAN  
 TAXIWAY G**

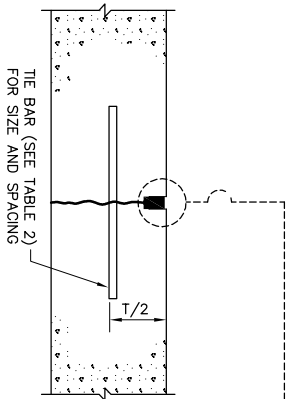
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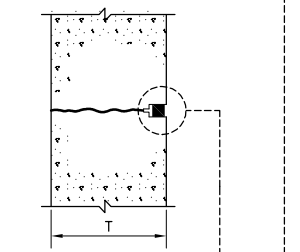
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| DRAWN BY:    | JRO             |
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| DATE:        | 07/23/2010      |
| JOB No:      | 08257-03-00     |
| SHEET        | 16 OF 25 SHEETS |





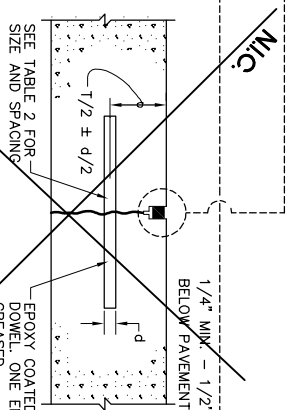
**TYPE G HINGED (TIED)**

SYMBOL



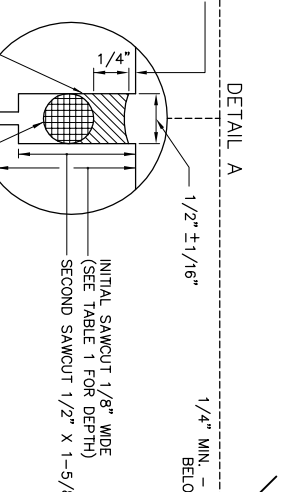
**TYPE H DUMMY**

SYMBOL



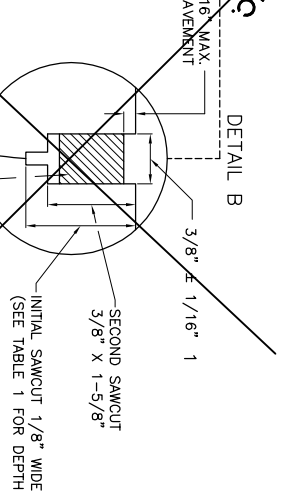
**TYPE F DOWELED**

SYMBOL



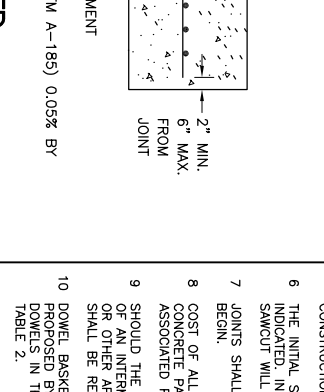
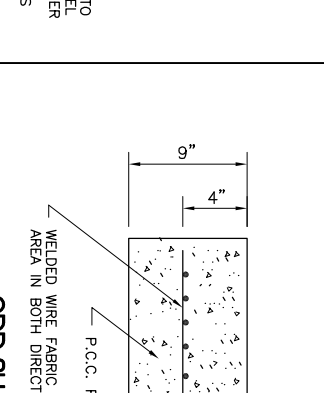
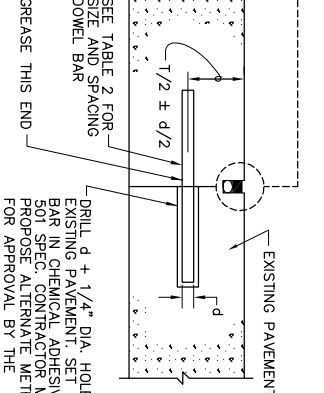
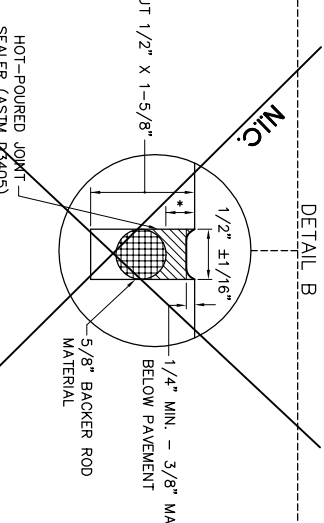
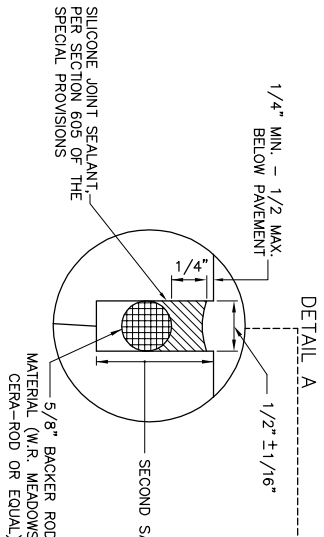
**TYPE D DOWELED**

SYMBOL



**ODD SHAPED PANEL REINFORCEMENT**

SYMBOL "R"



**CONSTRUCTION JOINTS**

NOTE: CONTRACTOR SHALL HAVE THE OPTION OF USING DETAIL A OR DETAIL B BUT NOT A COMBINATION OF BOTH

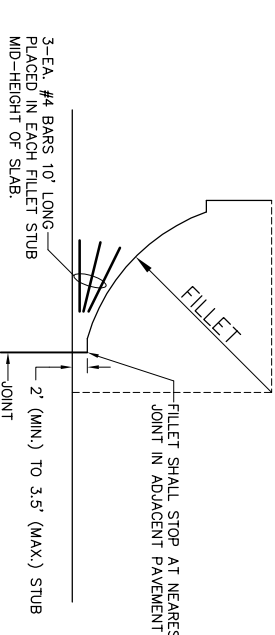
\* DEPTH AS RECOMMENDED BY SEALANT MANUFACTURER

**TYPE D DOWELED**

SYMBOL

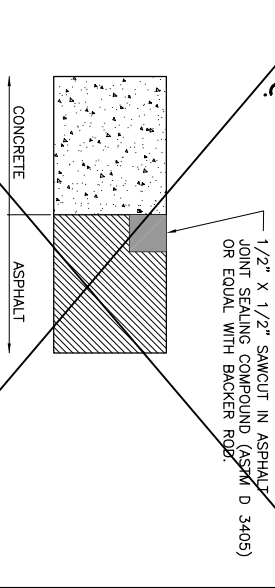
**ODD SHAPED PANEL REINFORCEMENT**

SYMBOL "R"



**CONCRETE FILLET STUB DETAIL**

N.T.S.



**JOINT SEALING AT CONCRETE ASPHALT INTERFACE**

N.T.S.

NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION

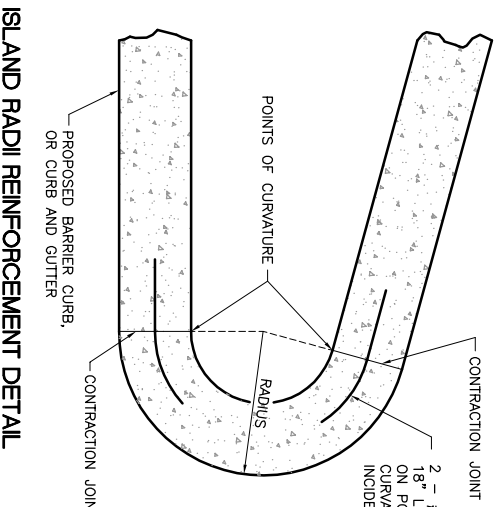
TABLE 1

| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T INCHES |
|-------------------------------|---|
| 6                             | 1.50"   |
| 7                             | 1.75"   |
| 8                             | 2.00"   |
| 9                             | 2.25"   |
| 10                            | 2.50"   |

TABLE 2

| PAVEMENT THICKNESS T - INCHES | DOWEL BAR DETAILS | THE BAR DETAILS |         |          |        |         |
|-------------------------------|-------------------|-----------------|---------|----------|--------|---------|
| T - INCHES                    | DIA. (D)          | LENGTH          | SPACING | BAR SIZE | LENGTH | SPACING |
| 6                             | 3/4"              | 18"             | 12"     | #5       | 30"    | 30"     |
| 7                             | 3/4"              | 18"             | 12"     | #5       | 30"    | 30"     |
| 8                             | 1"                | 19"             | 12"     | #5       | 30"    | 30"     |
| 9                             | 1"                | 19"             | 12"     | #5       | 30"    | 30"     |
| 10                            | 1"                | 19"             | 12"     | #5       | 30"    | 30"     |

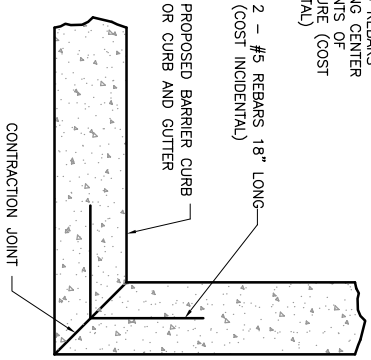
**DIMENSION TABLES**



**ISLAND RADI REINFORCEMENT DETAIL**

N.T.S.

INSTALL THE BARS IN LIEU OF DOWEL BARS WHEN RADI ARE 7.5' AND LESS.



**CORNER REINFORCEMENT DETAIL**

N.T.S.

REINFORCEMENTS SHALL BE INSTALLED AT ALL CORNERS.

**JOINT NOTES**

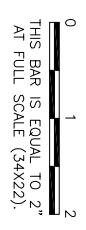
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL, HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAVING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS CUT AS SOON AS POSSIBLE AFTER CONSTRUCTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY OR MECHANICALLY INSTALLED PER ARTICLE 420.10 JOINTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, PAVEMENT LANE. THE DOWEL BAR ASSEMBLY OR MECHANICAL METHOD SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL THE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTENED DURING CONSTRUCTION. THE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL, MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONTRACTOR MAY ELECT TO BEVEL ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
- FROM TAXIWAY CENTERLINE, ALL LONGITUDINAL AND TRANSVERSE JOINTS ARE TO BE SEALED PER THE DETAILS.

**BARRIER CURB, DEPRESSED CURB AND GUTTER**

- CONTRACTION JOINTS SHALL BE PLACED AT TEN (10) FOOT MINIMUM ON CENTERS AND SHALL BE SAW CUT TO MINIMUM DEPTH OF TWO (2) INCHES FROM FRONT TO BACK AS SOON AS THE CONCRETE IS SUFFICIENTLY CURED TO ALLOW CUTTING. AS A MAXIMUM, THE CURB SHALL BE SAWS WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP-1 OR EQUIVALENT.
- EXPANSION JOINTS SHALL BE CONSTRUCTED AT A 50' MAXIMUM SPACING.
- ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FURTIVE DYE, AS PER TYPE II MEMBRANE CURING.
- CURB AND GUTTER SHALL HAVE A LIGHT BROOM FINISH.
- EXPANSION JOINTS SHALL ALSO BE PLACED AT ALL POINTS OF CURVATURE. AT THE END OF THE DAYS POUR, OR AS DIRECTED BY THE ENGINEER.
- TWO (2) NO. 5 REBARS (TEN) 10' LONG SHALL BE PLACED ON EITHER SIDE OF ALL PROPOSED UTILITY TRENCHES.
- THREE (3) NO. 5 REBARS SPACED 5' APART SHALL BE PLACED THE LENGTH OF ALL DEPRESSED CURB.

ILL. CONTRACT: **DU079**  
 ILL. LETTING ITEM: **1A**  
 ILL. PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-824**

| SURVEY BOOK # | REVISIONS |
|---------------|-----------|
| NUMBER        | BY DATE   |
|               |           |
|               |           |



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**PAVEMENT JOINTING DETAILS**

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 DRAWN BY: JRO  
 CHECKED BY: ARV  
 APPROVED BY: DLP

DATE: 07/23/2010  
 JOB No: 08257-03-00

| STORM SEWER SCHEDULE |                             |   |        |  |
|----------------------|-----------------------------|---|--------|--|
| NUMBER               | LOCATION                    | DESCRIPTION   | RIM    | INVERT   |
| A1                   | STA. 100+00<br>BASELINE A   | NEW REINFORCED CONCRETE END SECTION (IDOT STD. S42106-02) | N/A    | NEW 60" (S) = 740.49                             |
| A2                   | STA. 100+58<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 748.64 | NEW 60" (N) = 740.52<br>NEW 60" (SW) = 740.52    |
| A3                   | STA. 102+20<br>BASELINE A   | NEW TYPE A-8 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 749.27 | NEW 60" (N) = 740.60<br>NEW 60" (S) = 740.60     |
| A4                   | STA. 108+09<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 750.90 | PROP. 60" (N) = 740.89<br>PROP. 60" (S) = 740.88 |
| A5                   | STA. 113+09<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 751.11 | PROP. 60" (N) = 741.13<br>PROP. 60" (S) = 741.13 |
| A6                   | STA. 118+09<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 752.59 | NEW 60" (N) = 741.38<br>NEW 60" (S) = 741.37     |
| A7                   | STA. 124+20<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 755.00 | NEW 60" (N) = 741.68<br>NEW 60" (S) = 741.67     |
| A8                   | STA. 128+72.5<br>BASELINE A | NEW TYPE A-8 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 751.00 | NEW 60" (NE) = 741.90<br>NEW 60" (S) = 741.90    |
| A9                   | STA. 133+33.5<br>BASELINE A | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 753.59 | NEW 60" (NE) = 742.13<br>NEW 60" (SW) = 742.12   |
| A10                  | STA. 138+24<br>BASELINE A   | NEW TYPE A-8 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID      | 749.49 | NEW 60" (N) = 742.37<br>NEW 60" (SW) = 742.36    |

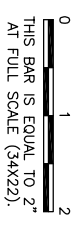
**NOTES:**  
 1. CONTRACTOR SHALL VERIFY ALL EXISTING STRUCTURES/PIPES AND UTILITIES PRIOR TO ORDERING MATERIALS.  
 2. ALL STRUCTURE STATIONS AND OFFSETS ARE TO CENTER OF DRAINAGE STRUCTURE UNLESS OTHERWISE NOTED ON DRAINAGE DETAILS. CONTRACTOR SHALL ADJUST STRUCTURES TO MAINTAIN POSITIVE DRAINAGE.

| STORM SEWER SCHEDULE |                             |  |        |   |
|----------------------|-----------------------------|--|--------|---|
| NUMBER               | LOCATION                    | DESCRIPTION  | RIM    | INVERT  |
| A11                  | STA. 141+48.4<br>BASELINE A | NEW TYPE A-9 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID                             | 751.75 | NEW 60" (NE) = 742.53<br>EXISTING 24" (NW) = 745.80<br>NEW 60" (S) = 742.52                                 |
| A12                  | STA. 144+53.6<br>BASELINE A | NEW TYPE A-8 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID                             | 753.92 | NEW 60" (NE) = 742.68<br>EXISTING 12" (NW) = 749.10<br>NEW 60" (SW) = 742.67                                |
| A13                  | STA. 149+65<br>BASELINE A   | NEW 8' x 5'-5" INLET W/ TYPE 1<br>FRAME AND CLOSED LID<br>(DRAINAGE STRUCTURE A) | 754.68 | NEW 60" (NE) = 742.93<br>NEW 24" (N) = 744.30<br>NEW 60" (SW) = 742.93                                      |
| A14                  | STA. 150+16<br>BASELINE A   | NEW 12' x 8' INLET W/ TYPE 1<br>FRAME AND OPEN LID<br>(DRAINAGE STRUCTURE B)     | 753.82 | NEW 60" (E) = 742.95<br>NEW 60" (SW) = 742.95<br>EXISTING 36" (S) = 744.30                                  |
| A15                  | STA. 151+37<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID                             | 751.97 | NEW 60" (N) = 743.01<br>NEW 12" (W) = 743.50<br>NEW 60" (S) = 743.01  |
| A16                  | STA. 152+30<br>BASELINE A   | NEW TYPE A-7 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID                             | 751.49 | NEW 60" (N) = 743.06<br>NEW 60" (S) = 743.06<br>NEW 12" (W) = 743.40  |
| A17                  | STA. 154+30<br>BASELINE A   | NEW TYPE A-8 MANHOLE W/<br>TYPE 1 FRAME AND OPEN LID                             | 749.90 | NEW 60" (N) = 743.16<br>NEW 60" (S) = 743.16<br>NEW 18" (W) = 743.20  |
| A18                  | STA. 155+56<br>BASELINE A   | EXISTING 8' x 7' INLET   | 751.52 | EXISTING 60" (N) = 743.22<br>NEW 60" (S) = 743.22<br>EXISTING 72" (E) = 743.22<br>EXISTING 12" (W) = 743.22 |

IL. CONTRACT: **DU079**  
 IL. LETTING ITEM: **1A**  
 IL. PROJECT: **DPa-3891**  
 A.I.P. PROJECT: 3-17-0017-824

SURVEY BOOK # ###

| REVISIONS |    |      |
|-----------|----|------|
| NUMBER    | BY | DATE |
|           |    |      |



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 NE QUADRANT DRAINAGE IMPROVEMENTS**

**DRAINAGE SCHEDULE**

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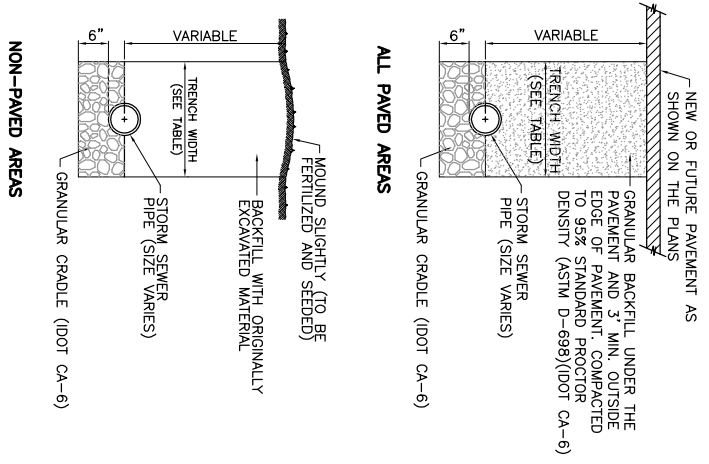


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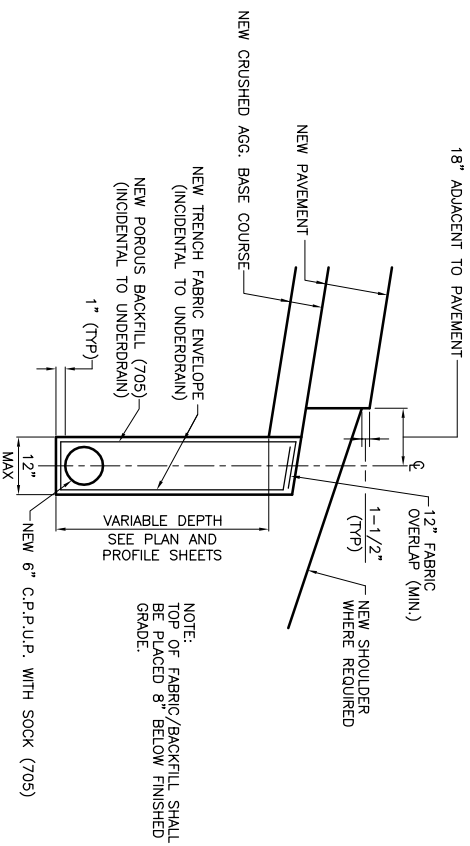


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 DRAWN BY: JRO  
 CHECKED BY: ARW  
 APPROVED BY: DLP  
 DATE: 07/23/2010  
 JOB No: 08257-03-00

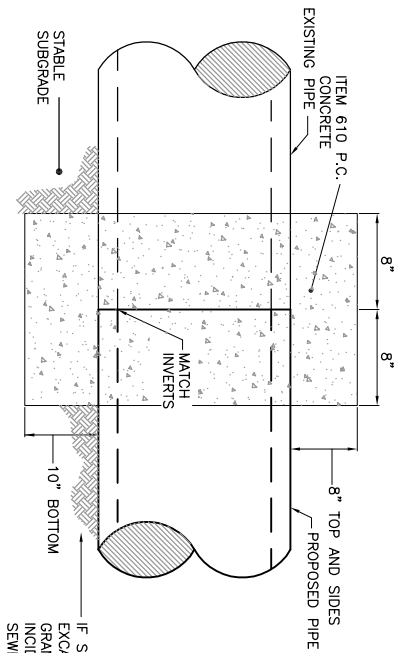
| INSIDE DIAMETER OF STORM SEWER (INCHES) | MAXIMUM TRENCH WIDTH |
|---|----------------------|
| 6                                       | 3'-7"                |
| 8                                       | 3'-9"                |
| 12                                      | 4'-2"                |
| 15                                      | 4'-6"                |
| 18                                      | 4'-9"                |
| 21                                      | 5'-0"                |
| 24                                      | 5'-4"                |
| 27                                      | 5'-7"                |
| 30                                      | 5'-11"               |
| 36                                      | 6'-6"                |
| 42                                      | 7'-1"                |
| 48                                      | 7'-8"                |
| 54                                      | 8'-3"                |
| 60                                      | 9'-0"                |
| 66                                      | 9'-7"                |



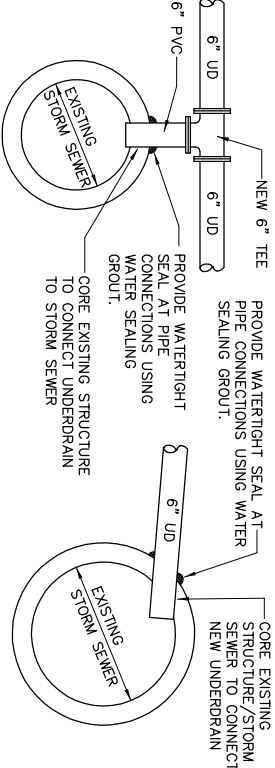
**TRENCH DETAILS - STORM SEWER**  
 N.T.S.



**UNDERDRAIN DETAIL**  
**EDGE OF PAVEMENT AREAS**  
 NOT TO SCALE



**CONCRETE COLLAR - STORM SEWER**  
 NOT TO SCALE



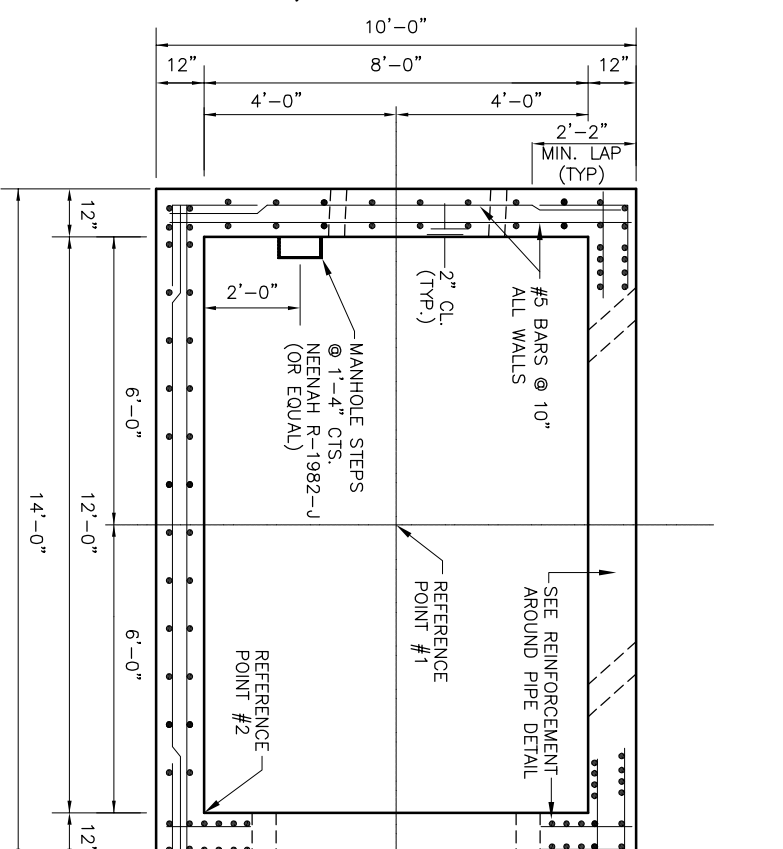
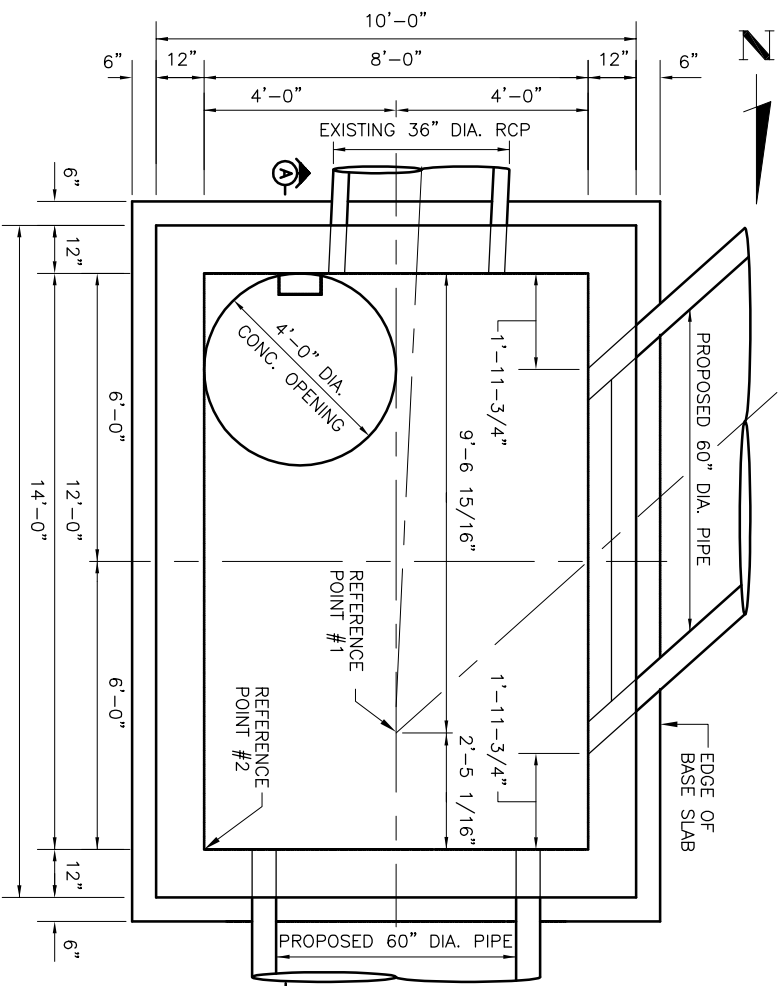
**UNDERDRAIN CONNECTION DETAILS**  
 NOT TO SCALE

- NOTES**
- UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO NEW STRUCTURES / EXISTING STORM SEWERS SHALL BE CONSIDERED INCIDENTAL TO THE NEW UNDERDRAIN.
  - UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO NEW/EXISTING UNDERDRAIN SHALL BE CONSIDERED INCIDENTAL TO THE NEW UNDERDRAIN.

|  |   |   |  |   |
|--|---|---|--|---|
| <p>IL. CONTRACT: <b>DU079</b><br/>                 IL. LETTING ITEM: <b>1A</b><br/>                 IL. PROJECT: <b>DPA-3891</b><br/>                 A.I.P. PROJECT: <b>3-17-0017-824</b></p>                     | <p>SURVEY BOOK # ###<br/>                 REVISIONS<br/>                 NUMBER BY DATE</p> | <p>0 1 2<br/>                 THIS BAR IS EQUAL TO 2"<br/>                 AT FULL SCALE (34X22).</p> | <p><b>DUPAGE AIRPORT</b><br/> <b>WEST CHICAGO, ILLINOIS</b><br/> <b>NE QUADRANT DRAINAGE IMPROVEMENTS</b></p> <p><b>DRAINAGE DETAILS - SHEET 1</b></p> | <p>© Copyright CMT, Inc.<br/> <b>CMT</b><br/>                 CRAWFORD, MURPHY &amp; TILLY, INC.<br/>                 CONSULTING ENGINEERS<br/>                 License No. 184-000613</p> <p><b>DPMA</b><br/>                 DuPage Airport</p> |
| <p>DESIGN BY: ARM<br/>                 DRAWN BY: JRO<br/>                 CHECKED BY: ARM<br/>                 APPROVED BY: DLP<br/>                 DATE: 07/23/2010<br/>                 JOB No: 08257-03-00</p> |   |   |  | <p>SHEET 19 OF 25 SHEETS</p>  |





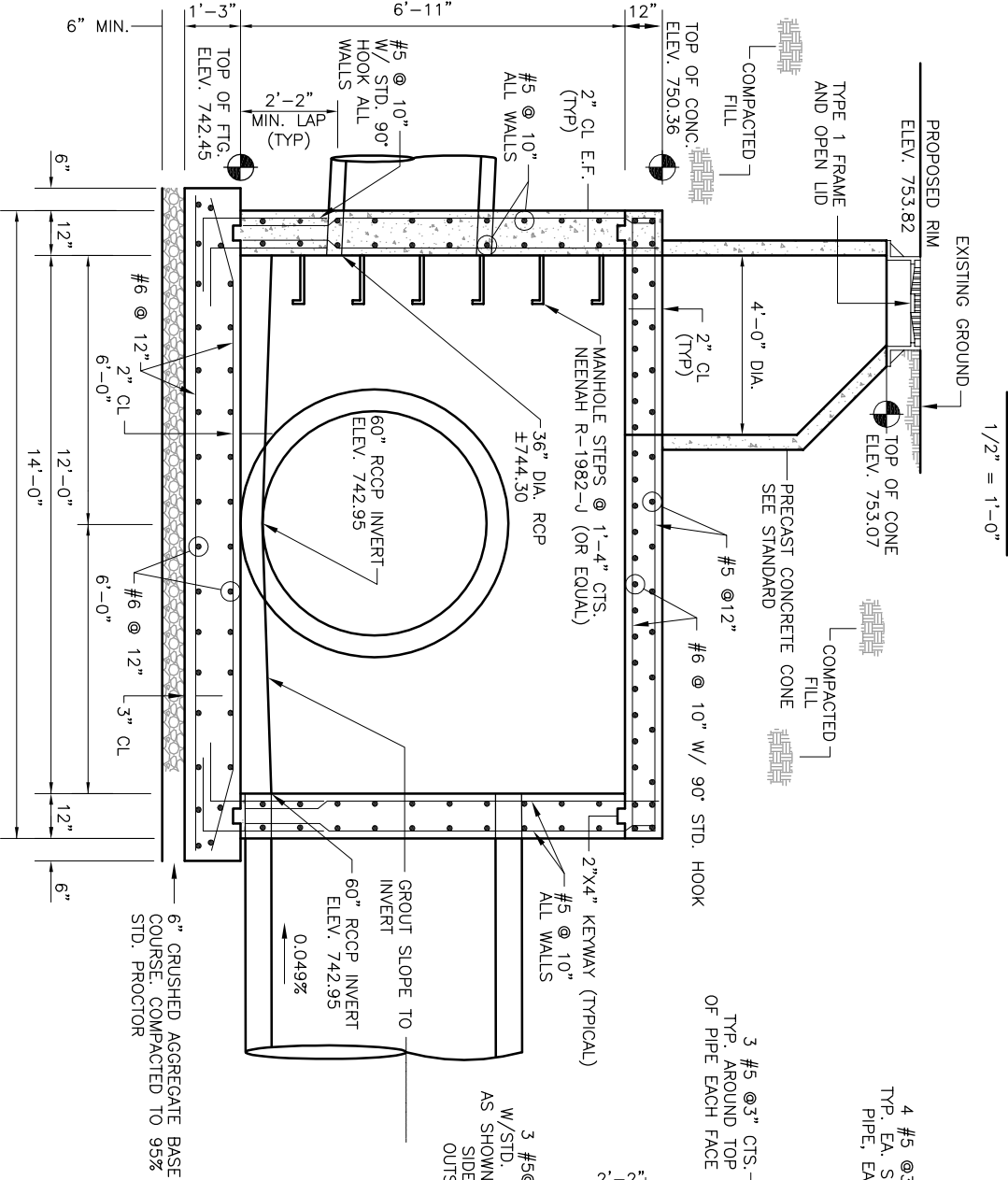


WALL REINFORCEMENT PLAN  
 1/2" = 1'-0"

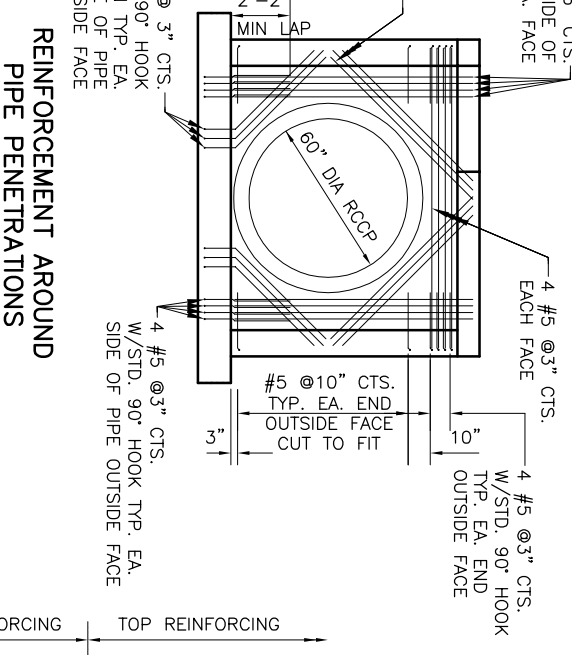
**REFERENCE POINT TABLE**

| REFERENCE POINT    | STATION   | OFFSET    |
|--------------------|-----------|-----------|
| REFERENCE POINT #1 | 150+16.00 | 0.00'     |
| REFERENCE POINT #2 | 150+18.42 | 4.00' RT. |

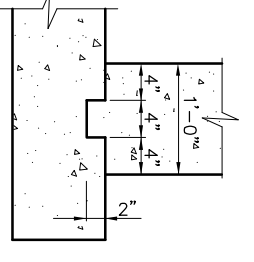
- GENERAL NOTES**
1. ALL REINFORCEMENT BARS SHALL CONFORM TO ASTM A615 GRADE 60 AND SHALL BE CLEAN AND FREE OF GREASE, SCALING RUST AND OTHER FOREIGN MATERIALS.
  2. INLET MAY BE CONSTRUCTED BY CAST-IN-PLACE CONCRETE OR PRECAST CONCRETE. PRECAST CONCRETE INLETS SHALL BE CONSTRUCTED TO THE LINES, DIMENSIONS AND DETAILS SHOWN ON THIS SHEET.
  3. CAST-IN-PLACE CONCRETE AND PRECAST CONCRETE FOR THE INLETS SHALL HAVE A MINIMUM 28 DAYS COMPRESSIVE STRENGTH OF 4000 PSF.
  4. THE CONTRACTOR SHALL VERIFY THE LOCATION AND SIZE OF PIPE OPENINGS AS SHOWN ON THIS SHEET.
  5. ALL FOOTING EXCAVATIONS SHALL BE CLEAN FREE OF DEBRIS, STANDING WATER AND LOOSE SOIL AND SHALL BE INSPECTED BY THE ENGINEER PRIOR TO THE PLACEMENT OF CONCRETE OR SUBBASE.
  6. CONCRETE SHALL NOT BE PLACED OVER FROZEN OR MUDDY SOIL.
  7. ADDITIONAL REINFORCEMENT FOR PIPE PENETRATIONS NOT REQUIRED FOR 6" DIA. PIPES OR LESS.



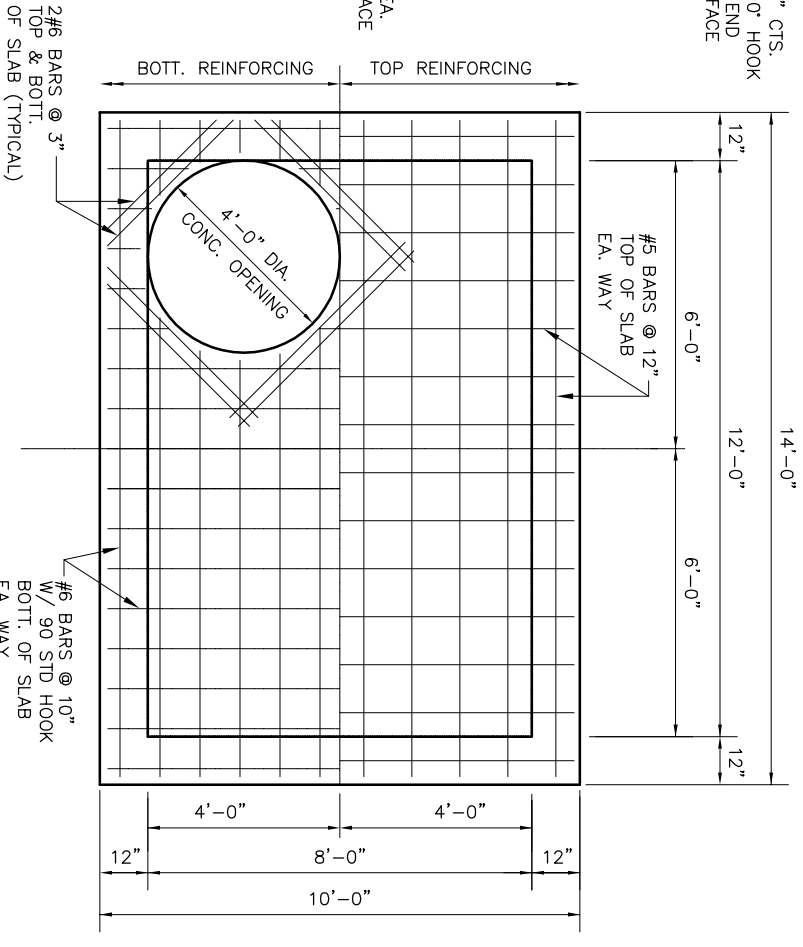
SECTION A  
 N.T.S.



REINFORCEMENT AROUND PIPE PENETRATIONS  
 N.T.S.



KEYWAY DETAILS  
 N.T.S.



TOP SLAB REINFORCEMENT PLAN  
 1/2" = 1'-0"

DRAINAGE STRUCTURE B

**DESIGN CRITERIA**  
 L.L. = AASHTO HS 20-44  
 NET ALLOWABLE SOIL BEARING PRESSURE = 3000 PSF

**DUPAGE AIRPORT WEST CHICAGO, ILLINOIS NE QUADRANT DRAINAGE IMPROVEMENTS**

**DRAINAGE DETAILS - SHEET 3**

DESIGN BY: ARM  
 DRAWN BY: JRO  
 CHECKED BY: ARM  
 APPROVED BY: XXX

DATE: 07/23/2010  
 JOB No: 08257-03-00

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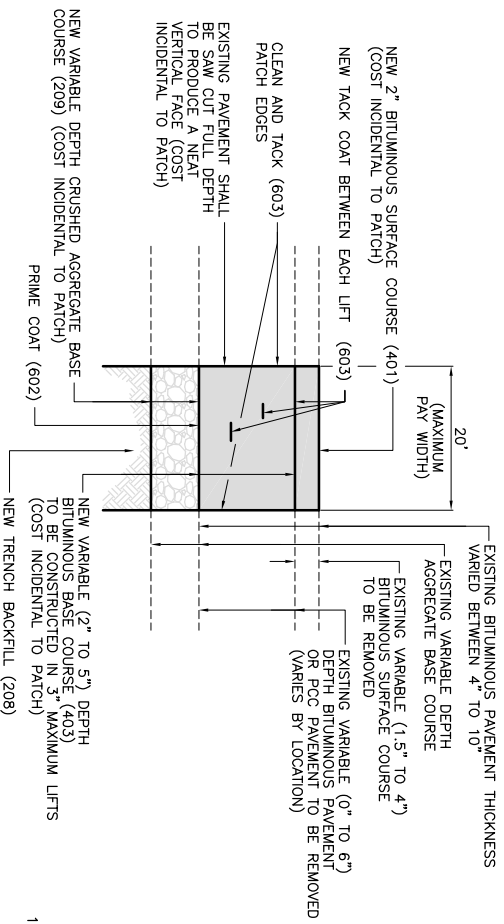
SURVEY BOOK # ###  
 REVISIONS  
 NUMBER BY DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

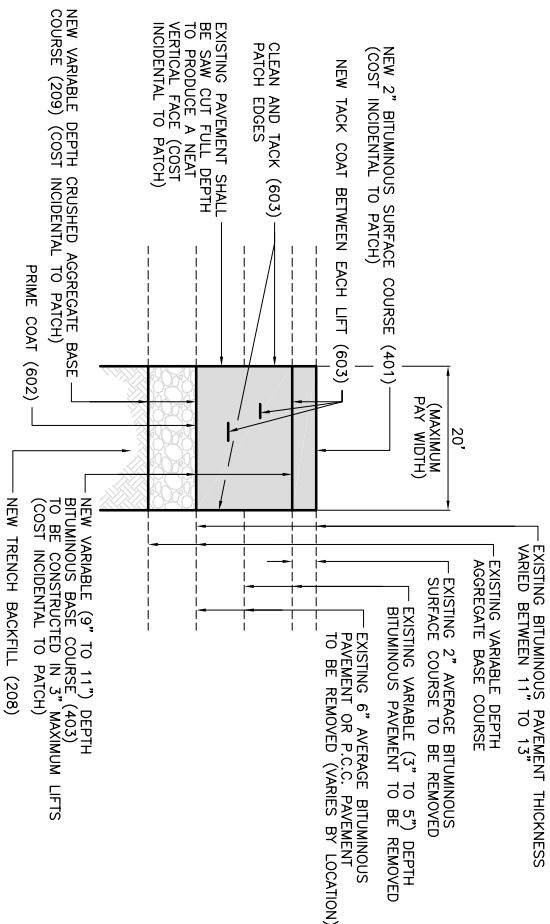
SHEET 21 OF 25 SHEETS

IL CONTRACT: **DU079**  
 IL LETTING ITEM: **1A**  
 IL PROJECT: **DPA-3891**  
 A.I.P. PROJECT: **3-17-0017-824**

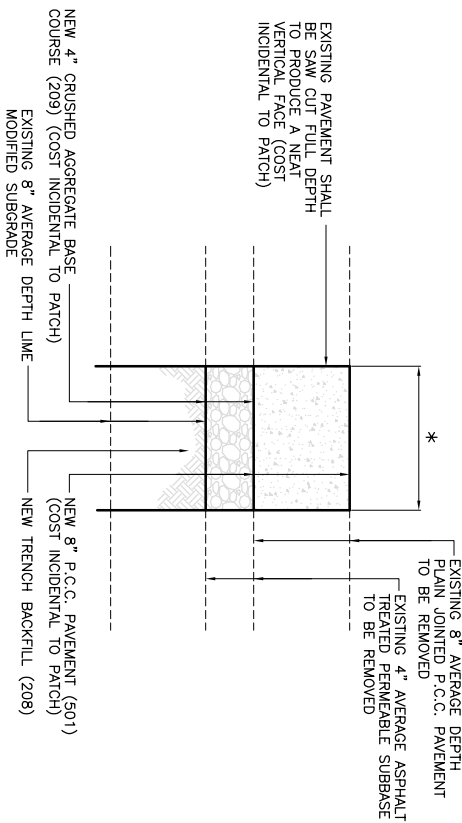
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**PROPOSED REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A**  
 NOT TO SCALE



**PROPOSED REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B**  
 NOT TO SCALE



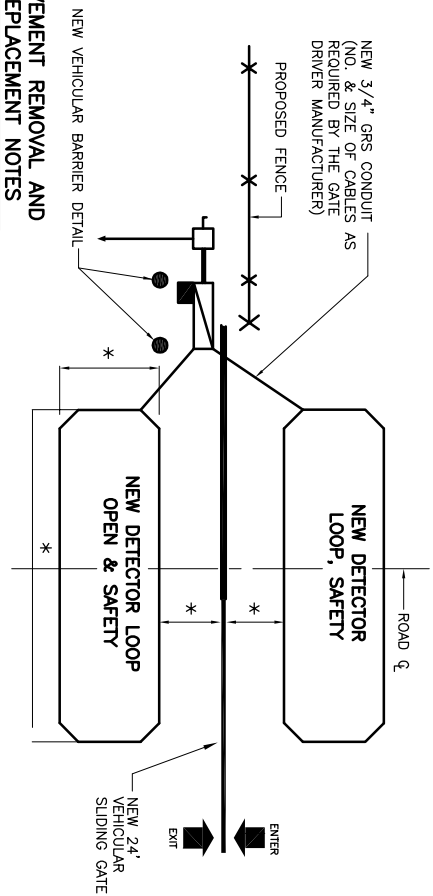
**PROPOSED REMOVE AND REPLACE P.C.C. PAVEMENT**  
 NOT TO SCALE

\* SEE PAVEMENT JOINTING/GRADING PLAN - TAXIMWAY G SHEET FOR REMOVAL LIMITS.

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.

**PAVEMENT REMOVAL AND REPLACEMENT NOTES**

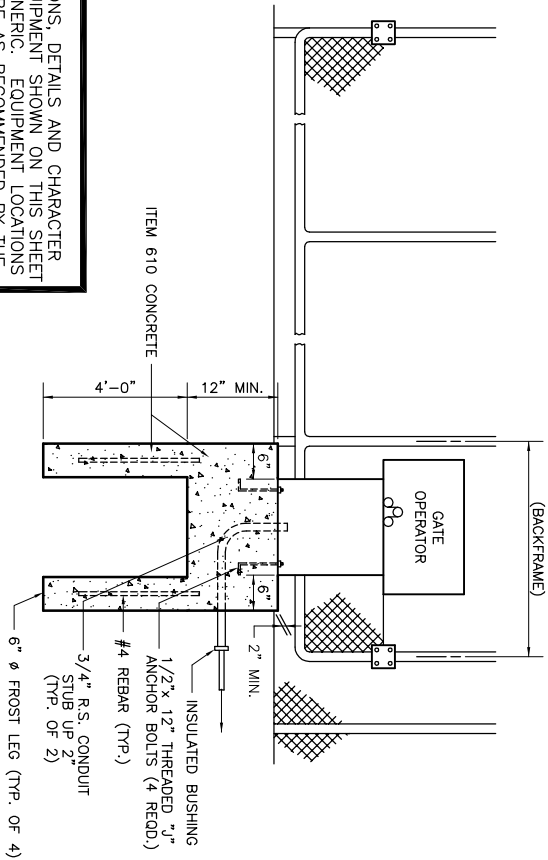
**DETECTOR LOOP LAYOUT**  
 NOT TO SCALE



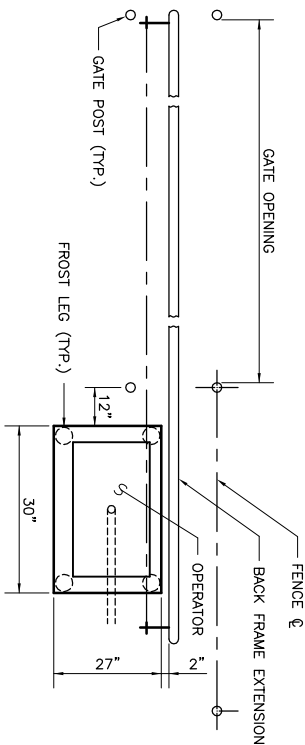
\* PER MANUFACTURERS RECOMMENDATION, CONTRACTOR SHALL CORRODATE THE WORK WITH ENGINEER.

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

**SECTION**



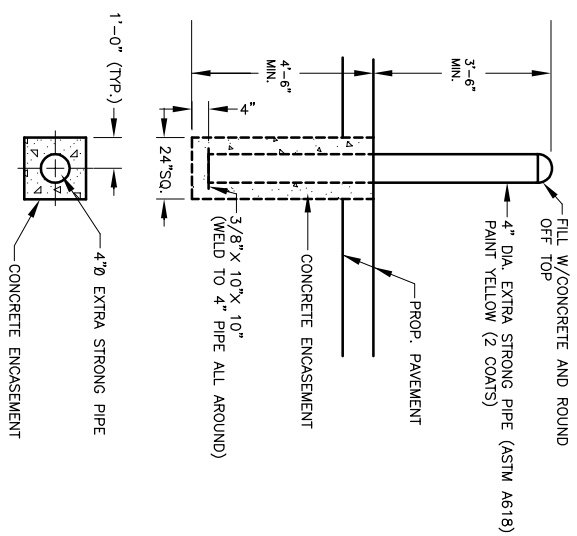
**PLAN**



**GATE OPERATOR DETAIL**  
 NOT TO SCALE

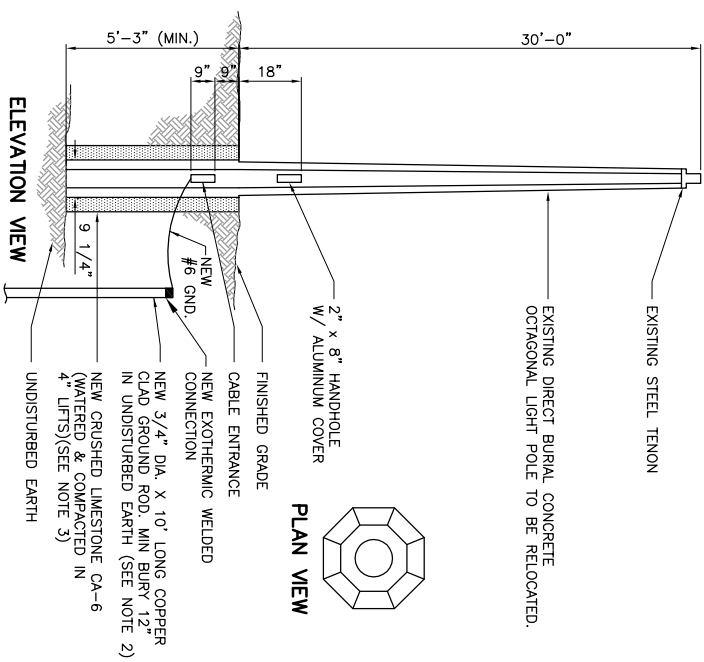
- NOTES:**
1. THE LOCATION OF THE GATE DRIVER AND DETECTOR LOOPS ARE FOR INFORMATION ONLY AND SHALL BE FIELD ADJUSTED PER THE MANUFACTURER RECOMMENDATION.
  2. THE MINIMUM BURIAL DEPTH FOR GRS CONDUIT IS 30" BELOW FINISHED GRADE.
  3. NO DIRECT BURIED CABLE WILL BE ALLOWED IN THE INSTALLATION OF THE GATE DRIVER.

**VEHICULAR BARRIER DETAIL**  
 NOT TO SCALE



- NOTES:**
1. THE VEHICULAR BARRIERS WILL NOT BE MEASURED SEPARATELY FOR PAVEMENT BUT SHALL BE CONSIDERED INCIDENTAL TO RELOCATED GATE PAY ITEM.
  2. LOCATION OF UNDERGROUND ELECTRICAL ITEMS SHALL BE COORDINATED WITH VEHICULAR BARRIERS TO AVOID ANY CONFLICTS.
  3. CONTRACTORS SHALL INSTALL TWO VEHICULAR BARRIERS ADJACENT TO THE GATE OPERATOR.

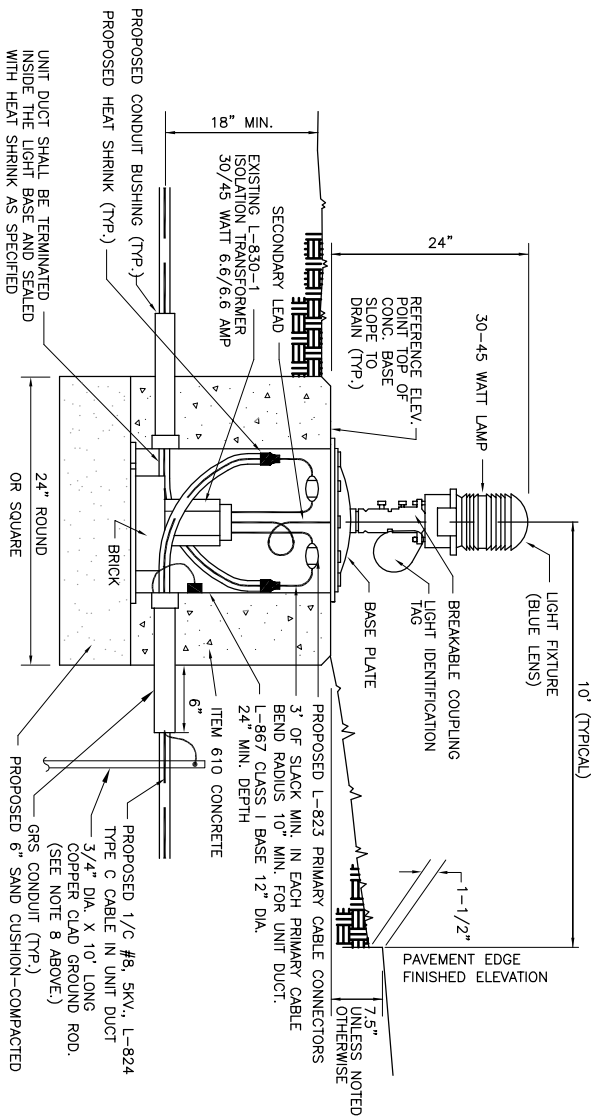
**EXISTING DIRECT BURIED CONCRETE TYPE A LIGHT POLE TO BE RELOCATED DETAIL**  
 NOT TO SCALE



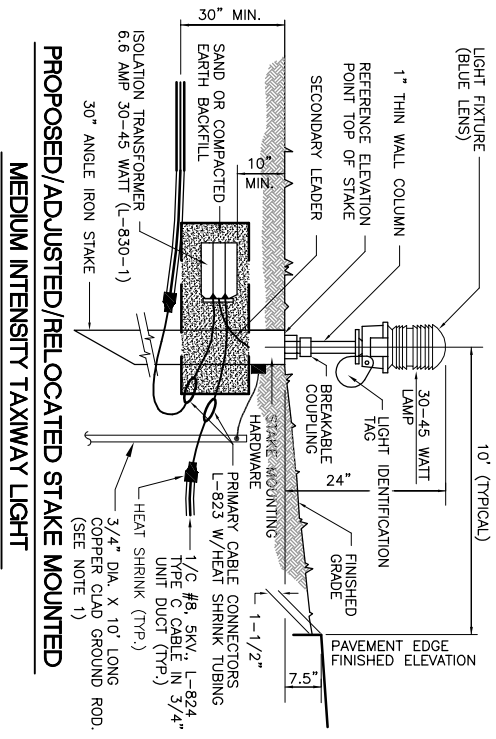
**LIGHT RELOCATION NOTES**

1. CONTRACTOR SHALL DISCONNECT EXISTING CIRCUIT, PULL BACK UNIT-DUCT AND RE-CONNECT CABLES AFTER THE INSTALLATION OF LIGHT POLE.
2. CONTRACTOR SHALL INSTALL NEW 3/4" DIA. x 10'-0" GROUND ROD, CAD WELDED TO NEW GROUND CONDUCTOR AS SHOWN.
3. INSTALL NEW CRUSHED LIMESTONE, COMPACTED IN 4" LIFTS AS SHOWN.

| <p>IL. CONTRACT: <b>DU079</b><br/>                 IL. LETTING ITEM: <b>1A</b><br/>                 A.I.P. PROJECT: <b>DPA-3891</b><br/>                 A.I.P. PROJECT: <b>3-17-0017-824</b></p>  |                              |      |      |  |  |  |   |
|--|------------------------------|------|------|--|--|--|---|
| <p>SURVEY BOOK # ###</p>   |                              |      |      |  |  |  |   |
| <p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>  | NUMBER                       | BY   | DATE |  |  |  | <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p> |
| NUMBER   | BY                           | DATE |      |  |  |  |   |
|  |                              |      |      |  |  |  |   |
| <p><b>DUPAGE AIRPORT</b><br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</p> <p><b>PAVEMENT REMOVAL AND REPLACEMENT DETAILS / MISCELLANEOUS DETAILS</b></p>                                     |                              |      |      |  |  |  |   |
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| <p>DESIGN BY: ARV</p> <p>DRAWN BY: JRO</p> <p>CHECKED BY: ARV</p> <p>APPROVED BY: DLP</p> <p>DATE: 07/23/2010</p> <p>JOB No: 08257-03-00</p>   | <p>SHEET 22 OF 25 SHEETS</p> |      |      |  |  |  |   |



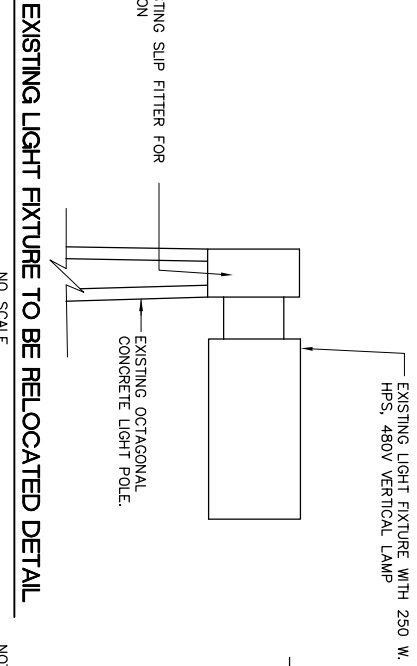
**PROPOSED/ADJUSTED/RELOCATED BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT**  
 NOT TO SCALE



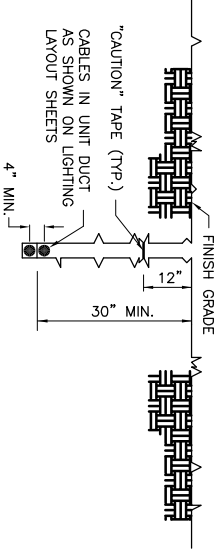
**PROPOSED/ADJUSTED/RELOCATED STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT**  
 NOT TO SCALE

**GENERAL NOTES:**

1. INSTALL 1/2" #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD.

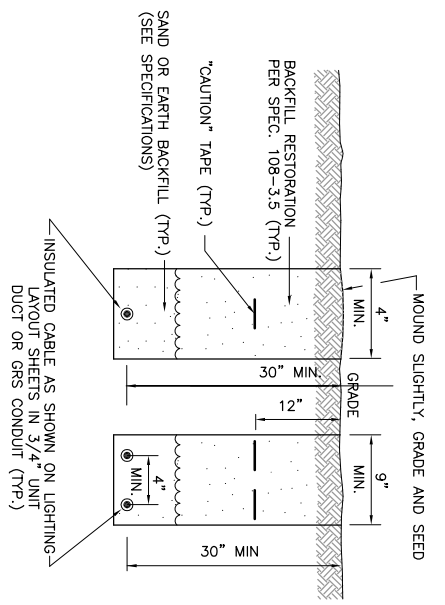


**EXISTING LIGHT FIXTURE TO BE RELOCATED DETAIL**  
 NO SCALE



**CABLE IN UNIT DUCT - PLOWED**  
 NOT TO SCALE

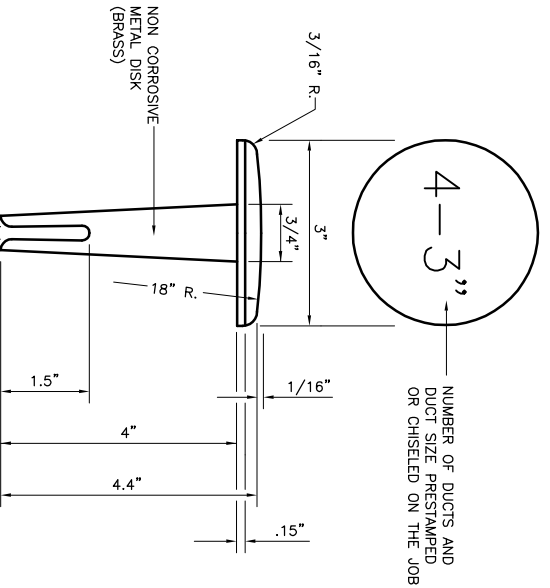
NOTE: CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



**TURF AREA CABLE TRENCH DETAIL**  
 NOT TO SCALE

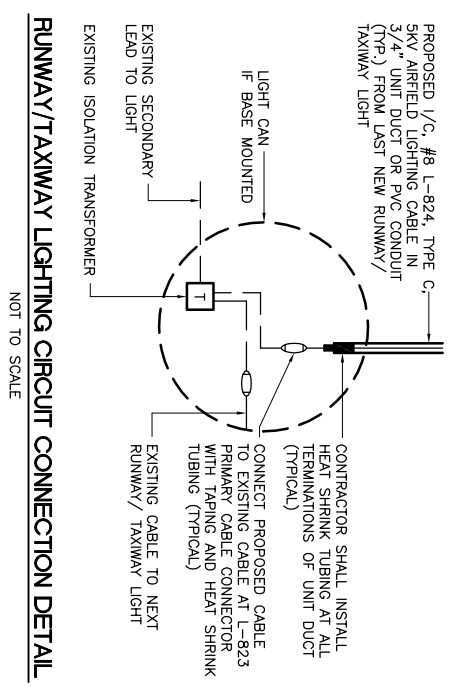
**NOTES**

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

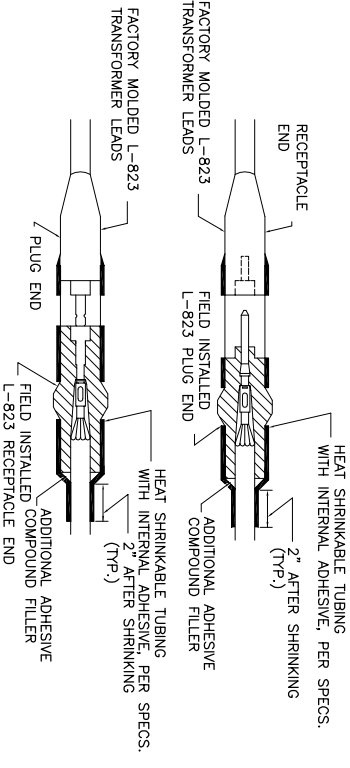


**DUCT MARKER DETAIL**  
 NOT TO SCALE

NEW DUCT MARKER SHALL BE INSTALLED AT ALL DUCTS LOCATIONS PROPOSED AND EXISTING AS SHOWN ON THE CABLING AND DUCT PLAN. (COST INCIDENTAL) CONTRACTOR SHALL LOCATE EXISTING DUCT LOCATIONS IN FIELD AND MARK IN FIELD BEFORE OVERLAY BEGINS IN ORDER TO REPLACE OVERLAY MARKERS AT CORRECT LOCATIONS. (COST INCIDENTAL TO CONTRACT)



**RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL**  
 NOT TO SCALE



**TYPE C AND D - CABLE SPLICE**  
 FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS  
 NOT TO SCALE

**CABLE SPLICE NOTES**

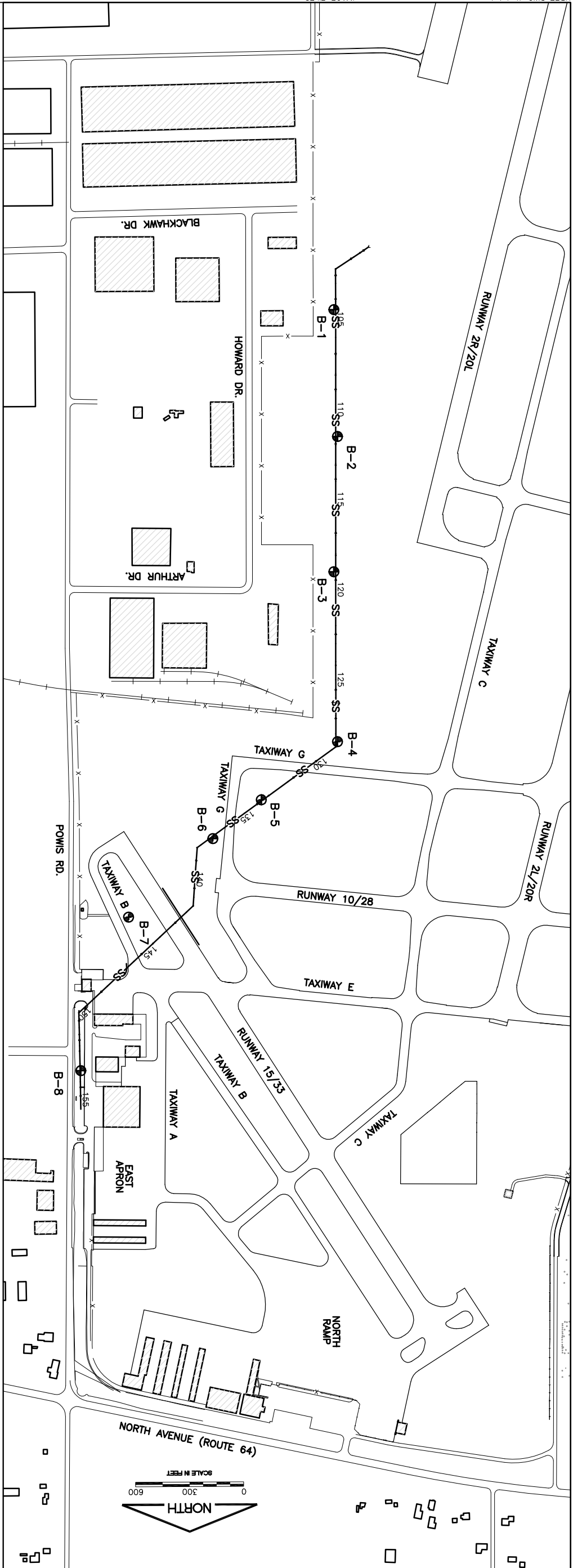
1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
3. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
4. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

**GENERAL NOTES:**

1. THE CONCRETE BASE FOR BASE MTD. LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
2. TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
3. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
4. ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 3). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/2" #8, S.W. L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
5. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
6. ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST.
7. TAXIWAY LIGHTS SHALL HAVE A BLUE LENS. RUNWAY LIGHTS SHALL HAVE A CLEAR OR 180° AMBER/CLEAR LENS AS DESIGNATED ON PLANS.
8. DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
9. CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

|   |   |
|---|---|
| <p>IL. CONTRACT: <b>DU079</b><br/>                 IL. LETTING ITEM: <b>1A</b><br/>                 I.L. PROJECT: <b>DPA-3891</b><br/>                 A.I.P. PROJECT: <b>3-17-0017-824</b></p> |   |
| <p>SURVEY BOOK # ###<br/>                 REVISIONS<br/>                 NUMBER BY DATE</p>   |   |
| <p>0 1 2<br/>                 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).</p>  |   |
| <p>DUPAGE AIRPORT<br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</p>  |   |
| <p><b>ELECTRICAL DETAILS</b></p>  |   |
| <p>DESIGN BY: ARM<br/>                 DRAWN BY: JRO<br/>                 CHECKED BY: ARM<br/>                 APPROVED BY: DLP</p>   | <p>DATE: 07/23/2010<br/>                 JOB No: 08257-03-00<br/>                 SHEET 23 OF 25 SHEETS</p> |

IMAGE FILES:



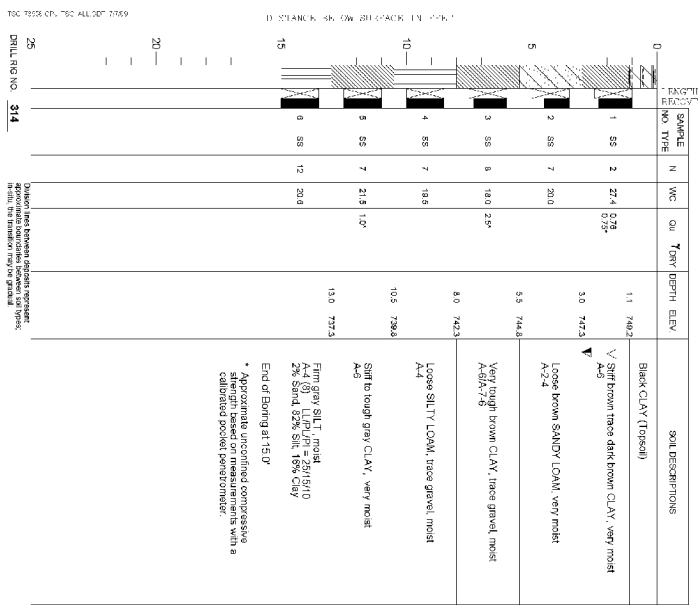
| SOIL BORING LOCATIONS (NAD 83) |                             |             |             |           |
|--------------------------------|-----------------------------|-------------|-------------|-----------|
| BORING#                        | LOCATION                    | NORTHING    | EASTING     | ELEVATION |
| B-1                            | STA. 104+46.08, 9.45' RT.   | 1907382.291 | 1008694.599 | 750.3     |
| B-2                            | STA. 111+50.00, 10.47' LT.  | 1908086.396 | 1008677.833 | 750.8     |
| B-3                            | STA. 119+00.00, 10.66' RT.  | 1908836.880 | 1008702.333 | 752.5     |
| B-4                            | STA. 128+43.55, 9.67' LT.   | 1909779.501 | 1008685.465 | 753.0     |
| B-5                            | STA. 133+79.40, 5.31' RT.   | 1910100.403 | 1009110.638 | 752.0     |
| B-6                            | STA. 137+23.60, 10.87' LT.  | 1910314.504 | 1009380.740 | 752.7     |
| B-7                            | STA. 144+52.38, 199.44' RT. | 1910748.097 | 1009850.409 | 754.5     |
| B-8                            | STA. 153+37.76, 4.28' LT.   | 1911599.259 | 1010119.802 | 752.5     |

**LEGEND**  
 B-1 SOIL BORING  
 SS NEW STORM SEWER

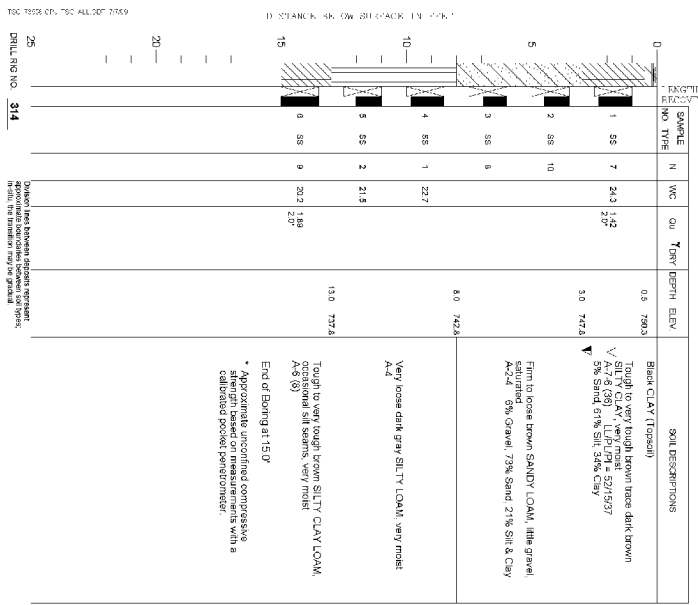
|  |  |   |  |
|--|--|---|--|
| <p>IL. CONTRACT: <b>DU079</b><br/>                 IL. LETTING ITEM: <b>1A</b><br/>                 IL. PROJECT: <b>DPA-3891</b><br/>                 A.I.P. PROJECT: <b>3-17-0017-824</b></p> |  | <p>SURVEY BOOK # ###<br/>                 REVISIONS<br/>                 NUMBER BY DATE</p> |  |
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| <p><b>DUPAGE AIRPORT</b><br/>                 WEST CHICAGO, ILLINOIS<br/>                 NE QUADRANT DRAINAGE IMPROVEMENTS</p>  |  | <p><b>ENGINEERING INFORMATION - SHEET 1</b></p>   |  |
| <p>DESIGN BY: ARV<br/>                 DRAWN BY: JRO<br/>                 CHECKED BY: ARV<br/>                 APPROVED BY: DLP</p>  |  | <p>DATE: 07/23/2010<br/>                 JOB No: 08257-03-00</p>                            |  |
| <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p>  |  | <p>SHEET 24 OF 25 SHEETS</p>  |  |



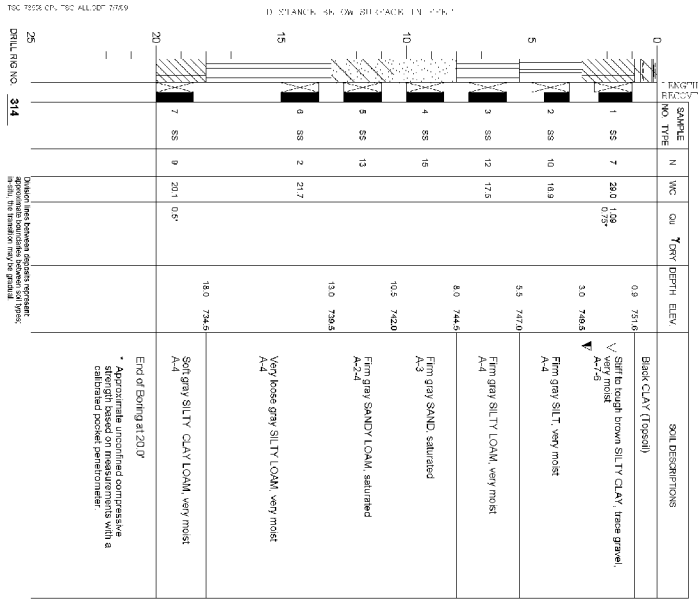
PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **1**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7803**  
GROUND SURFACE: **7803**  
END OF BORING: **7803**  
WATER LEVEL OBSERVATIONS: **20'**  
WHILE DRILLING: **30'**  
AT END OF BORING: **20'**  
HOURS: **24 HOURS**



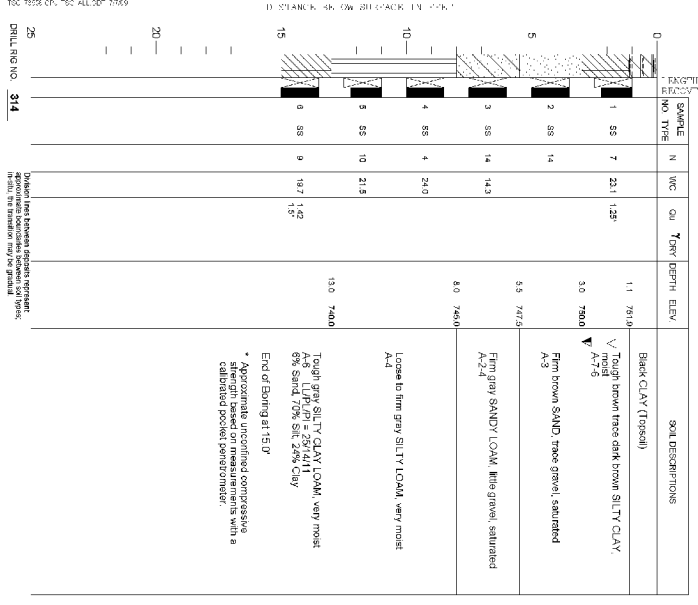
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CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **2**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7808**  
GROUND SURFACE: **7808**  
END OF BORING: **7808**  
WATER LEVEL OBSERVATIONS: **30'**  
WHILE DRILLING: **30'**  
AT END OF BORING: **20'**  
HOURS: **24 HOURS**



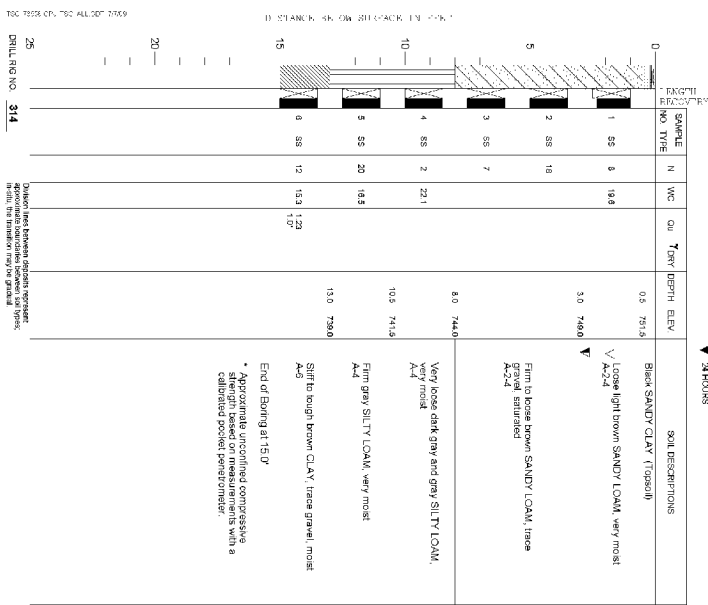
PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **3**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7826**  
GROUND SURFACE: **7826**  
END OF BORING: **7826**  
WATER LEVEL OBSERVATIONS: **30'**  
WHILE DRILLING: **30'**  
AT END OF BORING: **20'**  
HOURS: **24 HOURS**



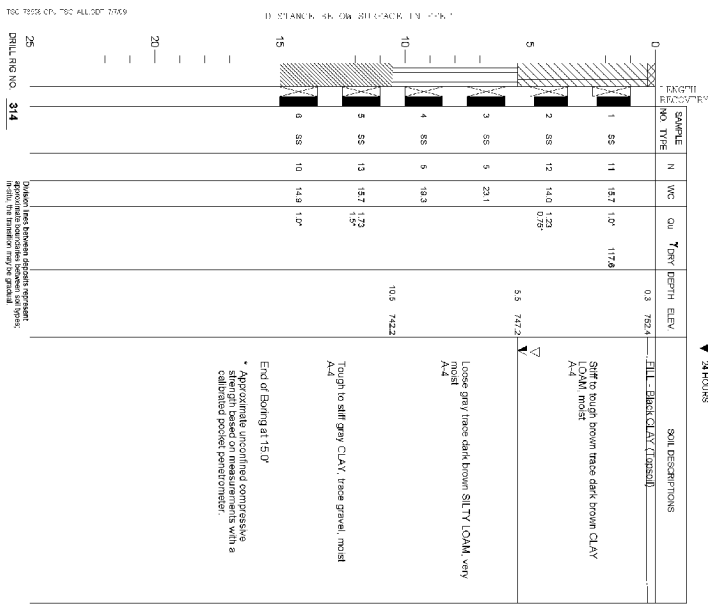
PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **4**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7800**  
GROUND SURFACE: **7800**  
END OF BORING: **7800**  
WATER LEVEL OBSERVATIONS: **30'**  
WHILE DRILLING: **30'**  
AT END OF BORING: **20'**  
HOURS: **24 HOURS**



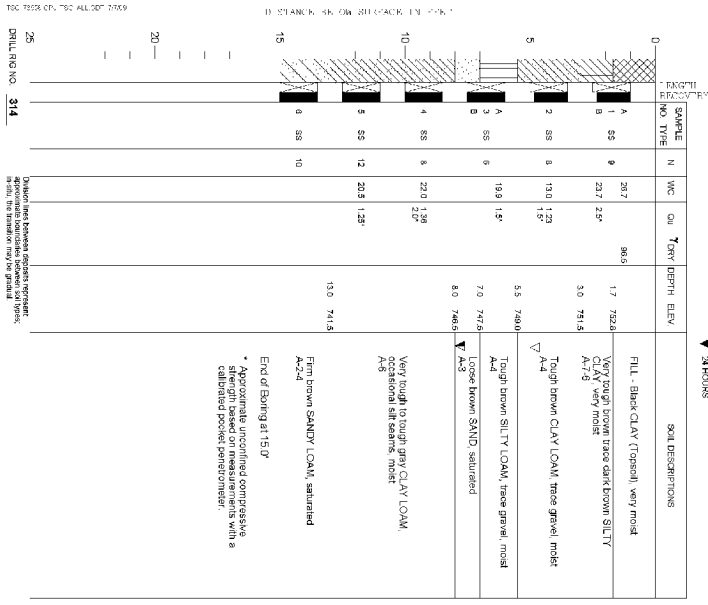
PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **5**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7820**  
GROUND SURFACE: **7820**  
END OF BORING: **7820**  
WATER LEVEL OBSERVATIONS: **30'**  
WHILE DRILLING: **30'**  
AT END OF BORING: **20'**  
HOURS: **24 HOURS**



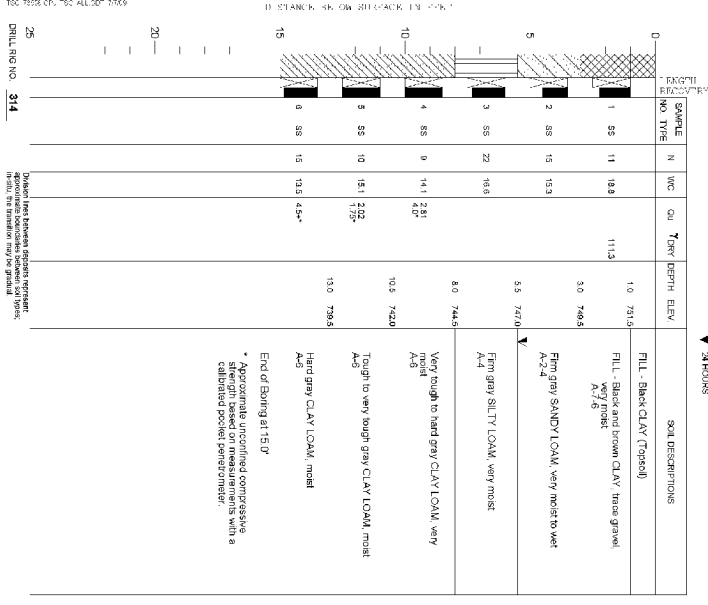
PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **6**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7827**  
GROUND SURFACE: **7827**  
END OF BORING: **7827**  
WATER LEVEL OBSERVATIONS: **50'**  
WHILE DRILLING: **50'**  
AT END OF BORING: **50'**  
HOURS: **24 HOURS**



PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **7**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7848**  
GROUND SURFACE: **7848**  
END OF BORING: **7848**  
WATER LEVEL OBSERVATIONS: **50'**  
WHILE DRILLING: **50'**  
AT END OF BORING: **50'**  
HOURS: **24 HOURS**



PROJECT: **DuPage Airport Authority, East Side Drainage System, West Chicago, IL**  
CLIENT: Crawford, Murphy & Tilly, Inc., Aurora, IL  
BORING: **8**  
DATE STARTED: **6-24-09**  
DATE COMPLETED: **6-24-09**  
JOB: **L-73588**  
EQUATIONS: **7825**  
GROUND SURFACE: **7825**  
END OF BORING: **7825**  
WATER LEVEL OBSERVATIONS: **55'**  
WHILE DRILLING: **55'**  
AT END OF BORING: **55'**  
HOURS: **24 HOURS**



IL CONTRACT: **DU079**  
IL LETTING ITEM: **1A**  
IL PROJECT: **DPA-3891**  
A.I.P. PROJECT: **3-17-0017-824**

SURVEY BOOK # **###**

| NUMBER | BY | DATE |
|--------|----|------|
|        |    |      |

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**ENGINEERING INFORMATION - SHEET 2**

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**DPA**  
DuPage Airport

DESIGN BY: **ARM**  
DRAWN BY: **JRO**  
CHECKED BY: **ARM**  
APPROVED BY: **DLP**

DATE: **07/23/2010**  
JOB No: **08257-03-00**  
SHEET **25** OF **25** SHEETS