

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1503	08-00260-01-PV	KANE	176	41
STA. N/A		TO STA. N/A		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

**MAINTENANCE OF TRAFFIC GENERAL NOTES**

- THE SUGGESTED SEQUENCE OF OPERATIONS AND SUMMARY FOR CONSTRUCTION STAGING DOES NOT, NOR IS IT INTENDED TO, DEPICT ALL WORK THAT WILL BE REQUIRED BY THE CONTRACTOR FOR STAGING OPERATIONS DURING THE CONTRACT, THE SEQUENCE OF OPERATIONS IS GIVEN AS AN AIDE AND GUIDE FOR THE CONTRACTOR'S USE TO ESTABLISH THE NECESSARY GUIDELINES FOR EFFICIENT TRAFFIC OPERATION DURING THE DURATION OF THE CONTRACT.
- THE CONTRACTOR MAY WISH TO MAKE REVISIONS OR MODIFICATIONS TO THE SEQUENCE OF CONSTRUCTION OR THE MAINTENANCE OF TRAFFIC PLANS. ALL CHANGES MUST BE SUBMITTED IN WRITING TO THE ENGINEER FOR APPROVAL. IF REVISIONS IN THE PHASING OF CONSTRUCTION OR MAINTENANCE OPERATIONS, REQUESTED BY THE CONTRACTOR, MAY REQUIRE TRAFFIC CONTROL TO BE INSTALLED IN ACCORDANCE WITH STANDARDS AND/OR DESIGNS OTHER THAN THOSE INCLUDED IN THE PLANS, REVISIONS IN THE PHASING OF CONSTRUCTION OR MAINTENANCE OPERATIONS REQUESTED BY THE CONTRACTOR REQUIRES ADDITIONAL SIGNS, FLAGGERS, BARRICADES OR OTHER TRAFFIC CONTROL DEVICES OVER AND ABOVE THOSE SPECIFIED WILL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PROTECT ANY OPENING GREATER THAN 3 FOOT IN DEPTH WITHIN 6 FOOT OF ANY DRIVING LANE. CONTRACTOR SHALL PROTECT OPENINGS WITH THE USE OF STEEL PLATES, CONCRETE BARRIER WALL, OR BY BACKFILLING OPENINGS TO WITHING THE 3 FOOT MINIMUM. PROTECTION OF THE OPENING SHALL BE CONSIDERED INCIDENTAL TO THE TRAFFIC CONTROL ITEM.
- SIDE STREETS SHALL REMAIN OPEN AT ALL TIMES. SPECIAL CONSIDERATION MAY BE GIVEN TO A SHORT-TERM CLOSURE ON AN AS NEEDED BASES. THESE CLOSURES WILL BE COORDINATED WITH THE RESIDENT ENGINEER.
- ACCESS TO FIELD, PRIVATE AND COMMERCIAL ENTRANCES SHALL REMAIN OPEN AT ALL TIMES. ON PROPERTIES THAT HAVE MORE THAN ONE ACCESS, ONE ENTRANCE MAY BE TEMPORARILY CLOSED, HOWEVER, VEHICULAR ACCESS MUST REMAIN OPEN TO TRAFFIC FOR THE OPPOSITE ENTRANCE.
- ALL MUNICIPAL OWNED MANHOLES, SERVICE VALVES, BOXES AND FIRE HYDRANT SHALL BE KEPT ACCESSIBLE AT ALL TIMES DURING CONSTRUCTION. MANHOLES AND SERVICE BOXES SHALL BE ADJUSTED TO MATCH THE TEMPORARY PAVEMENT AND GRADING ELEVATION. FIRE HYDRANTS SHALL BE MOVED OUTSIDE THE CLEAR ZONE LIMITS OF THE TEMPORARY PAVEMENT. ALL TEMPORARY ADJUSTMENTS TO MANHOLES, SERVICE VALVES, BOXES AND FIRE HYDRANTS REQUIRED AS A RESULT OF THE STAGED MAINTENANCE OF TRAFFIC WILL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE FINAL ADJUSTMENT ITEM. ONLY THE FINAL ADJUSTMENTS TO THESE UTILITIES SHALL BE PAID FOR. THE WORK SHALL BE AS DETAILED ON THE PLAN AND PROFILE SHEETS AND/OR REMOVAL SHEETS.
- EXISTING INLETS AND STORM SEWER SYSTEMS THAT FALL UNDER TEMPORARY PAVEMENT WIDENING AND NEED TO REMAIN ACTIVE DURING STAGE 1, SHALL BE PLATED OR COVERED, BUT NOT REMOVED. TEMPORARY DRAINAGE CONNECTIONS SHALL BE INCIDENTAL.
- THE CONTRACTOR SHALL USE TEMPORARY TYPE III MARKING TAPE (BLACK) TO COVER ANY EXISTING PAVEMENT MARKINGS.
- EXCAVATION REQUIRED TO CONSTRUCT THE TEMPORARY PAVEMENT WIDENING IN STAGE 1A SHALL BE MEASURED FOR PAYMENT AND SHALL BE PAID FOR AS "EARTH EXCAVATION". QUANTITIES FOR THIS WORK HAVE BEEN INCLUDED IN THE EARTHWORK ITEMS FOR THE STAGE.
- NO INTERMITTENT LANE CLOSURES WILL BE ALLOWED. CONTRACTOR SHALL FOLLOW IDOT STANDARDS AND TIE CLOSURES INTO ANY CLOSURE WEST OF THE WORK ZONE.
- ALL OBSTRUCTIONS SHALL BE REMOVED PRIOR TO CONSTRUCTING THE TEMPORARY PAVEMENT AND SWITCHING TRAFFIC TO IT. REMOVALS SHALL BE DETAILED ON THE REMOVAL SHEETS. REMOVAL SHALL BE PAID FOR ONCE AND SHALL BE PAID FOR AS INDICATED ON THE REMOVAL SHEETS. THERE WILL BE NO ADDITIONAL COMPENSATION FOR PARTIAL REMOVALS REQUIRED FOR THE STAGED MAINTENANCE OF TRAFFIC.
- THE USE OF A TRAILER MOUNTED ARROW BOARD WILL BE REQUIRED WHEN FULL LANE SHIFTS ARE REQUIRED. THE ARROW BOARDS WILL REMAIN IN PLACE FOR THE FULL DURATION OF THE LANE SHIFT. THE LOCATIONS OF THE ARROW BOARD(S) ARE DETAILED IN THE MAINTENANCE OF TRAFFIC PLANS OR AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW BOARDS WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

**SEQUENCE OF CONSTRUCTION**

IN GENERAL, THE STAGING OF CONSTRUCTION FOR THIS SECTION SHALL BE AS FOLLOWS:

**STAGE 1A**

- INSTALL EROSION CONTROL DEVICES AS REQUIRED PRIOR TO BEGINNING CONSTRUCTION ACTIVITY.
- CONSTRUCT PROPOSED TEMPORARY STORM SEWERS, DRAINAGE STRUCTURES, OUTLETS, DRAINAGE SWALES, AND ADJUSTMENTS FROM AT STA. 39+48.92, RT. TO STA. 72+80.91, RT. THE EXACT LOCATION OF THE WORK IS ILLUSTRATED IN THE STAGE 1 MOT PLAN SHEETS. THE WORK SHALL BE COMPLETED UTILIZING TRAFFIC CONTROL STD 701201 AND 701406.
- CONSTRUCT TEMPORARY PAVEMENT WIDENING FROM STA 39+48.92, RT. TO STA. 72+80.91, RT. WORK TO BE COMPLETED UTILIZING TRAFFIC CONTROL STANDARD 701406.
- SET TEMPORARY BARRIER WALL, GLARE SCREEN, AND ATTENUATORS FROM STA. 52+93 TO STA. 54+24.
- SET TEMPORARY TRAFFIC SIGNALS AT CHURCH ROAD INTERSECTION.

**STAGE 1**

- SET UP STAGE 1 TRAFFIC CONTROL AND TEMPORARY SIGNAL AND PRE-TIMED SIGNAL PHASING AT CHURCH STREET. MAINTAIN ACCESS AT AUTUMN LANE AND TRASK ROAD EXCEPT AS ALLOWED BY DETOUR PLAN TO COMPLETE WORK AT TRASK ROAD.
- PLACE EROSION CONTROL.
- REMOVE EXISTING PAVEMENT, DRIVEWAYS, AND UTILITIES LEFT OF CENTERLINE (SEE MOT PLANS, SHEETS) STA. 46+00 TO STA. 72+80.91. REMOVE CURBS AND PARTIAL PAVEMENT FROM STA. 39+48.92 TO STA. 46+00.
- CONSTRUCT WATER MAIN IN ITS ENTIRETY STA. 39+48.92 TO STA. 72+80.91, LT.
- CONSTRUCT STAGE 1 STORM SEWERS TRUNK LINE AND NORTH LATERALS WITHIN THE STAGE 1 LIMITS STA. 46+00 TO STA. 72+80.91. CONNECT TO EXISTING OUTLET DRAINAGE STRUCTURE APPROXIMATELY STA. 72+00, LT.
- CONSTRUCT SEWERS, SUBGRADE, AGGREGATE BASES, PAVEMENTS WITHOUT BITUMINOUS SURFACE COURSE (BINDER ONLY) AND CURB AND GUTTERS LEFT OF CENTERLINE.
- CONSTRUCT CONCRETE DRIVEWAYS IN THEIR ENTIRETY AND ASPHALT DRIVEWAY WITH BINDER COURSE.
- CONSTRUCT BRICK PAVER ENTRANCE AND SIDEWALK AT THE POLICE STATION IN ITS ENTIRETY.
- CONSTRUCT MEDIAN CURB AND GUTTER LEFT OF CENTERLINE.
- CONSTRUCT PARTIAL TRAFFIC SIGNAL INSTALLATION AT OHIO STREET.
- CONSTRUCT STREET LIGHTING LEFT OF CENTERLINE.
- CONSTRUCT BIKE PATH IN IT ENTIRETY.
- PLACE TOPSOIL, FINAL TURF, AND LANDSCAPING (INCLUDING TREE AND SHRUBS) LEFT OF CENTERLINE IN ITS ENTIRETY

**STAGE 1B**

- SET UP STAGE 1B TRAFFIC CONTROL AND TEMPORARY SIGNAL AND PRE-TIMED SIGNAL PHASING AT CHURCH STREET.
- PLACE EROSION CONTROL.
- REMOVE EXISTING PAVEMENT, DRIVEWAYS, AND UTILITIES LEFT OF CENTERLINE (SEE MOT PLANS, SHEETS) FROM STA. 39+48.92 TO STA. 46+00, LT.
- CONSTRUCT STAGE 1 STORM SEWERS TRUNK LINE AND NORTH LATERALS WITHIN THE STAGE 1 LIMITS STA. 39+48.92 TO STA. 46+00, LT.
- CONSTRUCT SEWERS, SUBGRADE, AGGREGATE BASES, PAVEMENTS WITHOUT BITUMINOUS SURFACE COURSE AND CURB AND GUTTERS LEFT OF CENTERLINE.

THESE FOLLOWING ITEMS TO BE COMPLETED IN STAGE 1B IF NOT PREVIOUSLY COMPLETED AS PART OF STAGE 1.

- CONSTRUCT CONCRETE DRIVEWAYS IN THEIR ENTIRETY AND ASPHALT DRIVEWAY WITH BINDER COURSE.
- CONSTRUCT BRICK PAVER ENTRANCE AND SIDEWALK AT THE POLICE STATION IN ITS ENTIRETY.
- CONSTRUCT MEDIAN CURB AND GUTTER LEFT OF CENTERLINE.
- CONSTRUCT STREET LIGHTING LEFT OF CENTERLINE.
- CONSTRUCT BIKE PATH IN IT ENTIRETY.
- PLACE TOPSOIL, FINAL TURF, AND LANDSCAPING (INCLUDING TREE AND SHRUBS) LEFT OF CENTERLINE IN ITS ENTIRETY

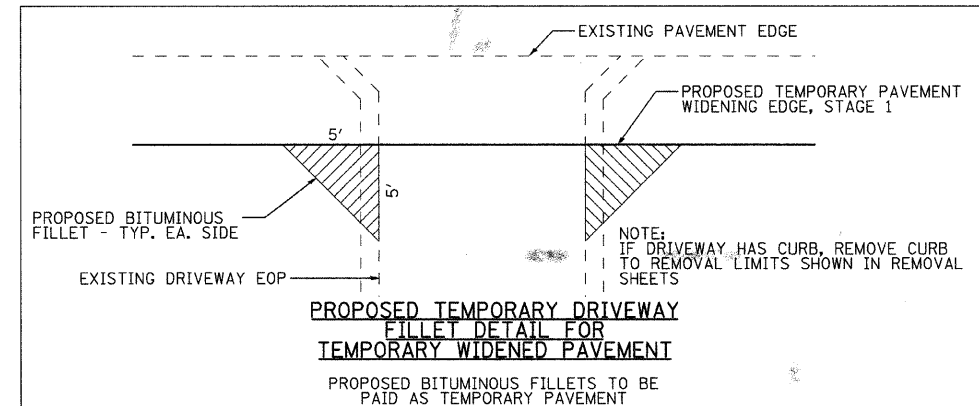
STAGING NOTE: IF CONTRACTOR COMPLETES WORK WITHIN STAGE 1B PRIOR TO COMPLETING ALL WORK IN STAGE 1, THE STAGE 1 TRAFFIC CONTROL AND INTERSECTION AT TEMPORARY PRE-TIMED STAGE 1 SIGNAL PHASING AT CHURCH ROAD SHALL BE RE-ESTABLISHED. IT IS THE INTENT OF THE TRAFFIC CONTROL PLAN TO LIMIT THE AMOUNT OF TIME THAT LEFT TURN LANES WILL BE OUT OF SERVICE AT THIS INTERSECTION. THERE WILL BE NO ADDITIONAL COMPENSATION FOR RE-ESTABLISHING THE STAGE 1 TRAFFIC CONTROL PLAN, WITH THE EXCEPTION THAT TEMPORARY STRIPING WILL BE MEASURED FOR SEPARATELY FOR PAYMENT.

**STAGE 2**

- SET UP STAGE 2 TRAFFIC CONTROL AND TEMPORARY PRE-TIMED STAGE 2 SIGNAL PHASING AT CHURCH ROAD. MAINTAIN ACCESS AT OHIO STREET, AUTUMN LANE, AND TRASK ROAD EXCEPT AS ALLOWED BY DETOUR PLAN TO COMPLETE WORK AT TRASK ROAD.
- PLACE EROSION CONTROL.
- REMOVE, TEMPORARY PAVEMENT WIDENING, AND SURFACE OVERLAY RIGHT OF CENTERLINE.
- REMOVE EXISTING PAVEMENT, DRIVEWAYS, AND UTILITIES LEFT OF CENTERLINE (SEE MOT PLANS, SHEETS) FROM STA. 39+48.92 TO STA. 72+80.91, RT.
- CONSTRUCT TRAFFIC SIGNAL INTERCONNECT IN ITS ENTIRETY STA. 39+48.92 TO STA. 72+80.91, RT.
- CONSTRUCT STAGE 2 STORM SEWERS SOUTH LATERALS WITHIN THE STAGE 2 LIMITS STA. 39+48.92 TO STA. 72+80.91.
- CONSTRUCT SUBGRADE, AGGREGATE BASES, PAVEMENTS WITHOUT BITUMINOUS SURFACE COURSE (BINDER ONLY) AND CURB AND GUTTERS RIGHT OF CENTERLINE.
- CONSTRUCT CONCRETE DRIVEWAYS IN THEIR ENTIRETY AND ASPHALT DRIVEWAY WITH BINDER COURSE.
- CONSTRUCT BRICK PAVER ENTRANCE AT THE PARK IN ITS ENTIRETY.
- CONSTRUCT MEDIAN CURB AND GUTTER RIGHT OF CENTERLINE. BACKFILL MEDIAN WITH TOPSOIL.
- CONSTRUCT CONCRETE AND MODULAR PAVER MEDIANS.
- CONSTRUCT PARTIAL TRAFFIC SIGNAL INSTALLATION AT OHIO STREET.
- CONSTRUCT STREET LIGHTING RIGHT OF CENTERLINE.
- CONSTRUCT SIDEWALK AND BIKE PATH IN THEIR ENTIRETY.
- PLACE TOPSOIL, FINAL TURF, AND LANDSCAPING (INCLUDING TREE AND SHRUBS) MEDIAN AND RIGHT OF CENTERLINE IN ITS ENTIRETY.

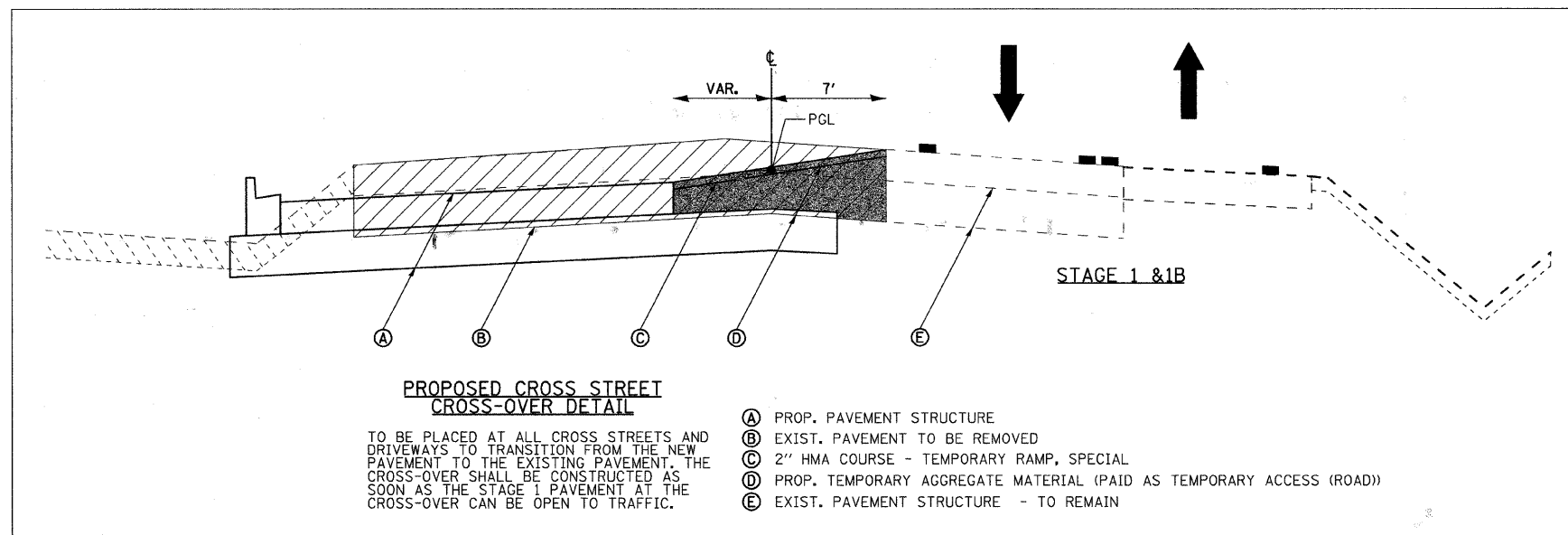
**STAGE 3**

- OPEN ALL LANES TO TRAFFIC FROM STA. 39+48.92 TO STA. 72+80.91.
- CONSTRUCT PAVEMENT SURFACE MILLING STA. 34+84.29 TO STA. 39+76.11, STA. 39+29.76 TO STA. 40+97 AND STA. 72+80.91 TO STA. 75+13.53. MAINTENANCE OF TRAFFIC FOR THE MILLING AND OVERLAY OPERATION SHALL BE COMPLETED UTILIZING TRAFFIC CONTROL STD. 701306.
- CONSTRUCT BITUMINOUS LEVELING (AS REQUIRED) IN MILLED AREAS. APPLY BITUMINOUS TACK COAT.
- CONSTRUCT HMA SURFACE COURSE FROM STA. 34+84.29 TO STA. 75+13.53. THIS WORK SHALL BE COMPLETED UTILIZING TRAFFIC CONTROL STANDARDS 701306.
- CONSTRUCT TRAFFIC CONTROL DETECTOR LOOPS IN MILLED AREAS, RE-SET TRAFFIC SIGNAL TO ACTUATED PHASING AT CHURCH ROAD INTERSECTIONS.
- ACTIVATE TRAFFIC SIGNAL AT OHIO STREET.
- PLACE ALL PERMANENT PAVEMENT MARKINGS AND SIGNAGE STA. 34+84.29 TO STA. 75+13.53.
- REMOVE REMAINING TRAFFIC CONTROL DEVICES AND SIGNAGE.



**LEGEND - TYPICALS**

	DRUM WITH MONO DIRECTIONAL STEADY BURNING LIGHT
	VERTICAL SIGN PANELS (GROUND MOUNTED)
	TYPE III BARRICADE
	DIRECTION OF TRAFFIC FLOW
	DIRECTIONAL BARRICADES
	ARROW BOARD
	TRAFFIC CONTROL SIGN
	TEMPORARY CONCRETE BARRIER
	TEMPORARY PAVEMENT MARKING
	PAVEMENT MARKING REMOVAL
	ATTENUATOR END TREATMENT
	WORK ZONE
	TEMPORARY HMA PAVEMENT WIDENING



- (A) PROP. PAVEMENT STRUCTURE
- (B) EXIST. PAVEMENT TO BE REMOVED
- (C) 2" HMA COURSE - TEMPORARY RAMP, SPECIAL
- (D) PROP. TEMPORARY AGGREGATE MATERIAL (PAID AS TEMPORARY ACCESS (ROAD))
- (E) EXIST. PAVEMENT STRUCTURE - TO REMAIN

REVISIONS	
NAME	DATE

**RHA&A**  
 Robert H. Anderson & Associates, Inc.  
 Consulting Engineers & Land Surveyors  
 License No. 044-029281  
 Phone No. 630-584-3530

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 MAINTENANCE OF TRAFFIC - GENERAL NOTES  
 INDIAN TRAIL ROAD  
 SHEET 1 OF 22

SCALE: VERT. N/A  
 DATE 1/7/09  
 DRAWN BY TC  
 CHECKED BY TC/SBP

PLOT DATE = 8/12/2009  
 PLOT SCALE = USER  
 PLOT USER = USER