~~9-18-09 Letting, Item 082~~

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	1
		CONTRACT NO. 63231		
	ILLINOIS FED. AID PROJECT			

INDEX OF SHEETS

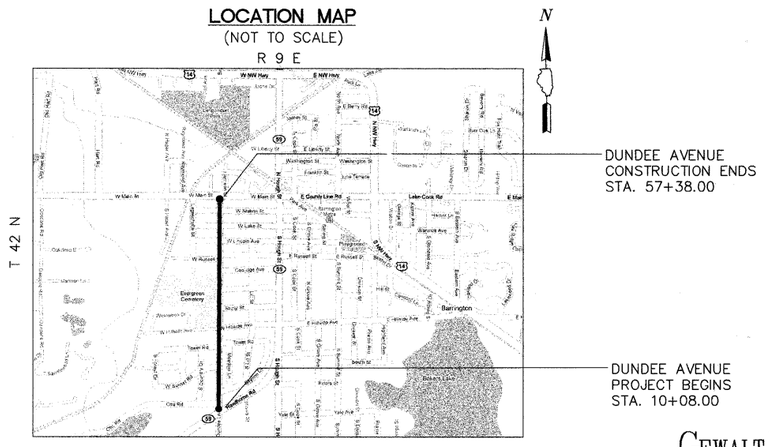
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

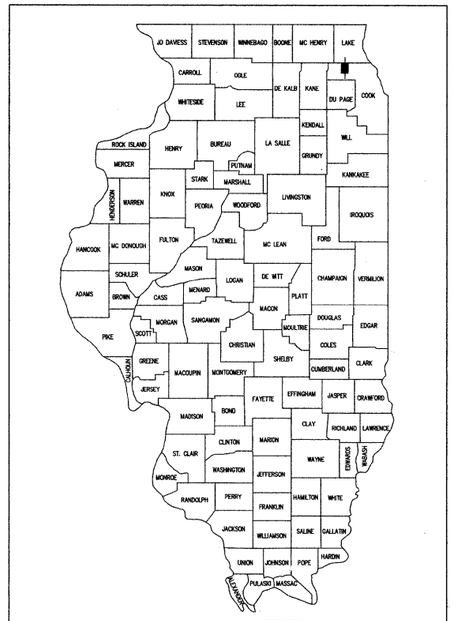
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 2555/ DUNDEE AVENUE
IL ROUTE 59 TO LAKE COOK ROAD
SECTION: 09-00085-00-RS
JOB NO. 91-644-09
PROJECT NO. ARA-9003(329)

VILLAGE OF BARRINGTON, ILLINOIS
COOK COUNTY

LOCATION MAP
(NOT TO SCALE)





LOCATION OF SECTION INDICATED THUS: ■

PROJECT INFORMATION

LENGTH OF PROJECT = 4757.80 FT (0.901 MI)
ADT = 7000 VPD (2006)
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED LIMIT = 35 MPH
ROAD CLASSIFICATION = URBAN COLLECTOR

GEWALT HAMILTON ASSOCIATES, INC.
Consulting Engineers & Surveyors
850 Forest Edge Drive
Vernon Hills, IL 60061
847-478-9700
FAX 847-478-9701

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: June 26 2009
Karen Darch
VILLAGE OF BARRINGTON

PASSED: July 16 2009
CHRISTOPHER HOLT
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: JULY 16, 2009
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

Leo X. Morand
LEO X. MORAND
EXPIRES: 11-30-09

LEO X. MORAND 062-060808 LICENSED PROFESSIONAL ENGINEER STATE OF ILLINOIS
SEAL

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

J.U.L.I.E
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
CALL 811

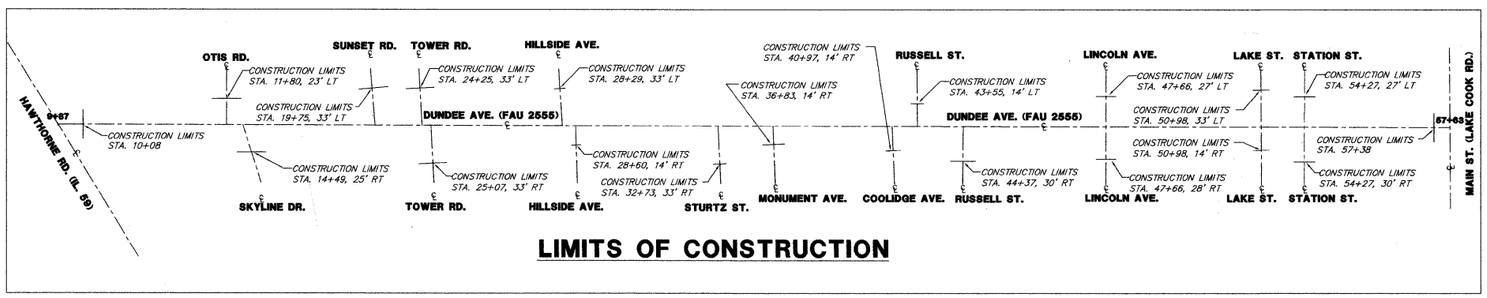
811 Know what's below. Call before you dig.

EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES, DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

CONTRACTOR IS RESPONSIBLE FOR CONTACTING J.U.L.I.E. AT 1-800-892-0123 AND MUST ACQUIRE A DIG NUMBER A MINIMUM OF 72 HOURS PRIOR TO ANY WORK BEING DONE.

CONTRACT NO: 63231

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847) 705-4169



LIMITS OF CONSTRUCTION

GENERAL NOTES

A-1 THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, MUNICIPALITY, ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERE SHALL GOVERN THIS WORK.

A-2 ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.

A-3 NO CONSTRUCTION PLANS SHALL BE USED FOR CONSTRUCTION UNLESS SPECIFICALLY MARKED 'FOR CONSTRUCTION'. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEERS' LINE AND GRADES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

A-4 IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.

A-5 EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHTS-OF-WAY ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION IN THE FIELD OF THESE UTILITY LINES AND THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

A-6 SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

A-7 THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

A-8 OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

A-9 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

A-10 QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD.

A-11 WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSTIED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC. SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATION, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

STORM SEWERS, WATER MAINS, AND UTILITIES

B-1 EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PROCESS OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

B-2 THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AS WELL AS LOCAL CABLE TV COMPANIES AND THE VILLAGE. THE FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK WITHIN ANY EASEMENT, R.O.W, OR SUSPECTED UTILITY LOCATION.

B-3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THROUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFICATION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.

B-4 ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.

B-5 THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVE VAULTS, AND SANITARY SEWER MANHOLES REMIAN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISABLE AT ALL TIMES.

SIGNING AND STRIPING

C-1 SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13, AND REFERENCE PLAN SHEETS.

TRAFFIC CONTROL

D-1 SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.

DEBRIS REMOVAL
MATERIALS RESULTING FROM THE REMOVAL OF ASPHALT SURFACES, UTILITY ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE CITY, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE CITY WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.

WATER SUPPLY
THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE. IF THE WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.

DRIVEWAY CLOSING
IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT, AND/OR DRIVEWAY REPLACEMENT. AT LOCATIONS WHERE THE DRIVEWAY IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB, SIDEWALK, OR DRIVE APPROACH. EVERY EFFORT SHALL BE MADE TO ACCOMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 8 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.

STREET SWEEPING AND PREPARATION
THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.

HIGHWAY STANDARDS:

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424101-07	CURB RAMPS FOR SIDEWALK
424001-05	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATION
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY

SUMMARY OF QUANITIES

	PAYCODE	DESCRIPTION	UNIT	ROADWAY 100% FEDERAL I000
	40201000	Aggregate for Temporary Access	TON	50.00
	40600100	Bituminous Materials (Prime Coat)	GAL	1715.00
	40600300	Aggregate (Prime Coat)	TON	53.00
	40600625	Leveling Binder (Machine Method), N50	TON	407.00
	40600982	Hot-Mix Asphalt Surface Removal - Butt Joint	SY	327.00
	40603310	HMA Surface Course, Mix "C", N50	TON	46.00
	40603335	HMA Surface Course, Mix "D", N50	TON	1727.00
	42300300	Portland Cement Concrete Driveway Pavement, 7"	SY	30.00
	44000155	HMA Surface Removal, 1.5"	SY	542.00
	44000157	HMA Surface Removal, 2"	SY	15420.00
	44000200	Driveway Pavement Removal	SY	60.00
	44000400	Gutter Removal	FT	32.00
	44001700	Combination Curb and Gutter Removal and Replacement	FT	471.00
	44201737	Class D Patches, Type I, 8 Inch	SY	10.00
	44201741	Class D Patches, Type II, 8 Inch	SY	15.00
	44201761	Class D Patches, Type I, 10 Inch	SY	78.00
	44201765	Class D Patches, Type II, 10 Inch	SY	78.00
	44201769	Class D Patches, Type III, 10 Inch	SY	78.00
	44201771	Class D Patches, Type IV, 10 Inch	SY	78.00
	44201773	Class D Patches, Type I, 11 Inch	SY	125.00
	44201777	Class D Patches, Type II, 11 Inch	SY	125.00
	44201781	Class D Patches, Type III, 11 Inch	SY	125.00
	44201783	Class D Patches, Type IV, 11 Inch	SY	125.00
	60300310	Frames and Lids To Be Adjusted - SPECIAL	EA	30.00
	60602800	Concrete Gutter, Type B	FT	32.00
	67100100	Mobilization	LS	1.00
	70102620	Traffic Control and Protection - Standard 701501	LS	1.00
	70300100	Short Term Pavement Marking,	LF	1395.00
*	78000100	Thermoplastic Pavement Marking - Letters and Symbols	SF	150.00
*	78000200	Thermoplastic Pavement Marking - Line 4"	LF	4764.00
*	78000400	Thermoplastic Pavement Marking - Line 6"	LF	975.00
*	78000600	Thermoplastic Pavement Marking - Line 12"	LF	88.00
*	78000650	Thermoplastic Pavement Marking - Line 24"	LF	103.00
*	88600600	Detector Loop Replacement	LF	260.00

* DENOTES SPECIALTY ITEM

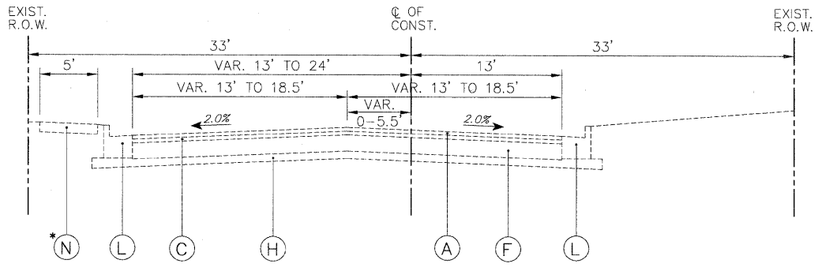
FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED - LXM 7/8/09
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 7/8/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

GENERAL NOTES AND SUMMARY OF QUANITIES

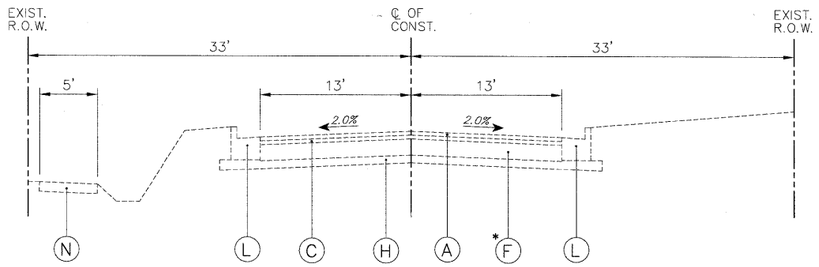
SCALE: NONE | SHEET NO. 2 OF 13 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	2
		CONTRACT NO. 63231		
		ILLINOIS FED. AID PROJECT		



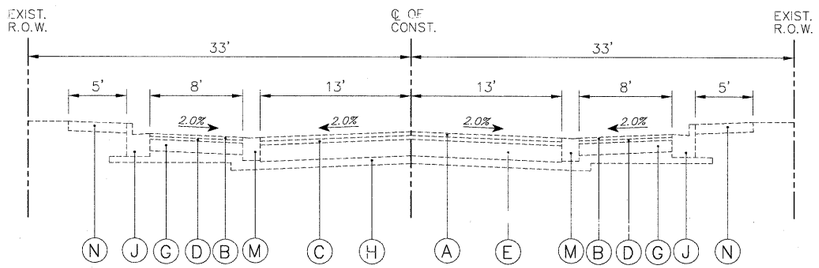
* SIDEWALK BEGINS AT STA. 12+00

EXISTING TYPICAL CROSS SECTION
STA. 10+00 (IL ROUTE 59) TO STA. 13+67 (SKYLINE DR.)

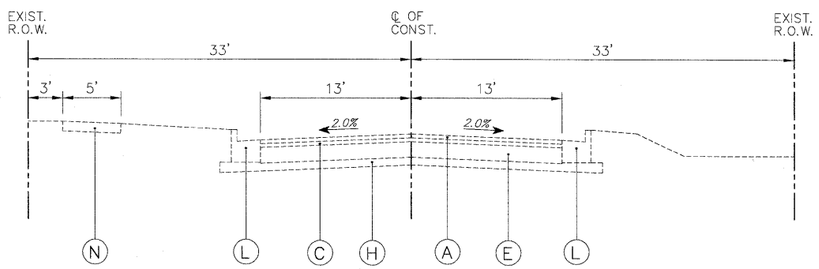


* (E) NORTH OF STA. 27+83

EXISTING TYPICAL CROSS SECTION
STA. 13+67 TO STA. 29+00 (HILLSIDE AVE.)



EXISTING TYPICAL CROSS SECTION
STA. 29+00 TO STA. 32+50 (STURTZ ST.)



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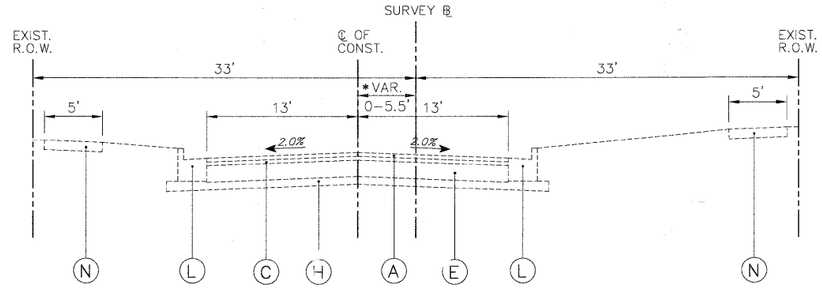

* FROM STA. 48+86 TO STA. 51+36

EXISTING TYPICAL CROSS SECTION
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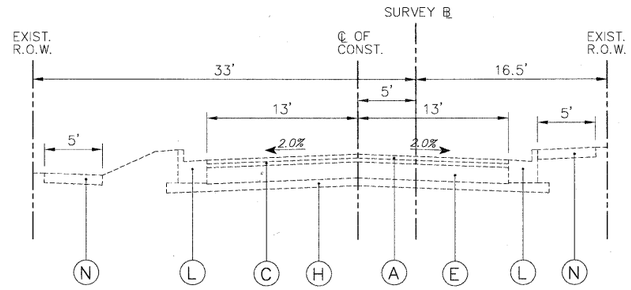

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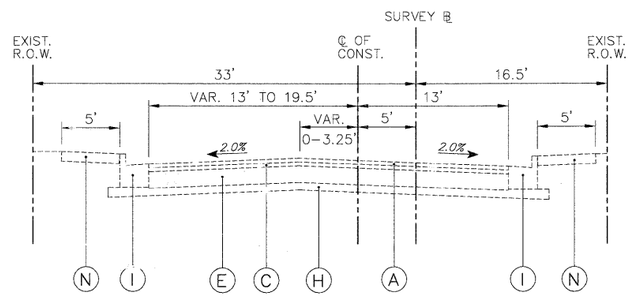


EXISTING TYPICAL CROSS SECTION
STA. 53+82.5 TO STA. 57+37 (MAIN ST.)

EXISTING LEGEND

- (A) HMA SURFACE REMOVAL, 2"
- (B) HMA SURFACE REMOVAL, 1.5"
- (C) EXISTING HMA BINDER COURSE, 2"
- (D) EXISTING HMA BINDER COURSE, 1.5"
- (E) EXISTING BIT. BASE COURSE, 8.5"
- (F) EXISTING BIT. BASE COURSE, 7.5"
- (G) EXISTING BIT. BASE COURSE, 6"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL, 4"
- (I) EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.12 TO BE REMOVED WHERE MARKED IN FIELD
- (J) EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.18 TO BE REMOVED WHERE MARKED IN FIELD
- (L) EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.24 TO BE REMOVED WHERE MARKED IN FIELD
- (M) CONCRETE GUTTER, TYPE B TO BE REMOVED WHERE MARKED IN FIELD
- (N) EXISTING P.C. CONCRETE 5"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS	MAX RAP%
HOT MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 1.5"	PG 64 -22	4% @ 50 GYR	15 %
HOT MIX ASPHALT SURFACE COURSE, MIX 'D', N50, 2.0"	PG 64 -22	4% @ 50 GYR	15 %
LEVELING BINDER (MACHINE METHOD), N50	PG 64 -22 *	4% @ 50 GYR	25 %
CLASS D PATCHES (HMA BINDER IL-19 mm)	PG 64 -22 *	4% @ 50 GYR	25 %

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUALTITIES IS 112 LBS/SQ/IN
*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

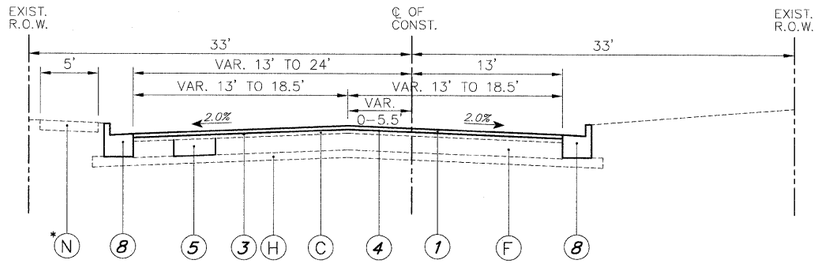
EXISTING TYPICAL SECTIONS

SCALE: NONE | SHEET NO. 3 OF 12 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	3

CONTRACT NO: 63231

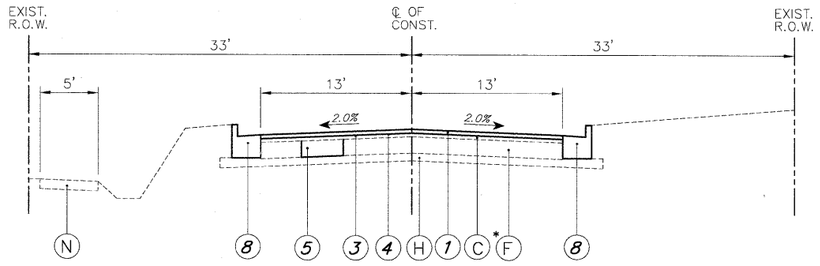
ILLINOIS | FED. AID PROJECT



* SIDEWALK BEGINS AT STA. 12+00

PROPOSED TYPICAL CROSS SECTION

STA. 10+00 (IL ROUTE 59) TO STA. 13+67 (SKYLINE DR.)



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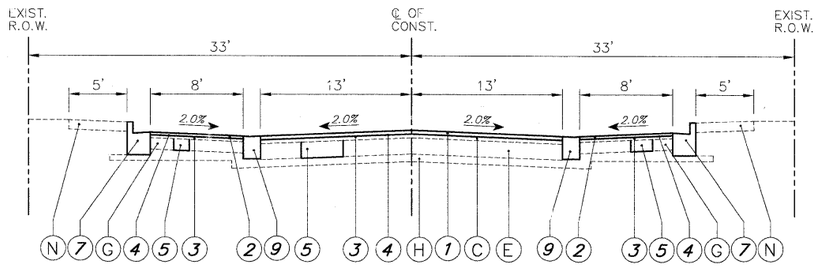
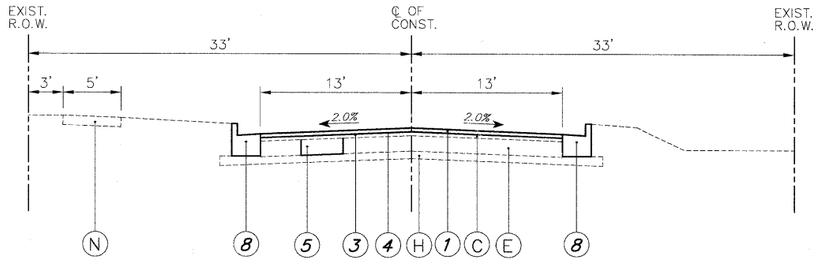


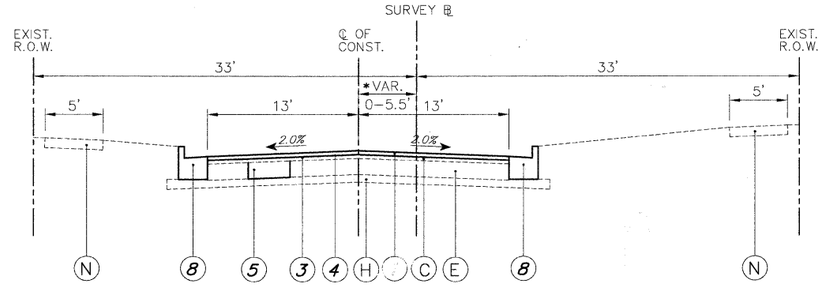
PROPOSED TYPICAL CROSS SECTION

STA. 29+00 TO STA. 32+50 (STURTZ ST.)



PROPOSED TYPICAL CROSS SECTION

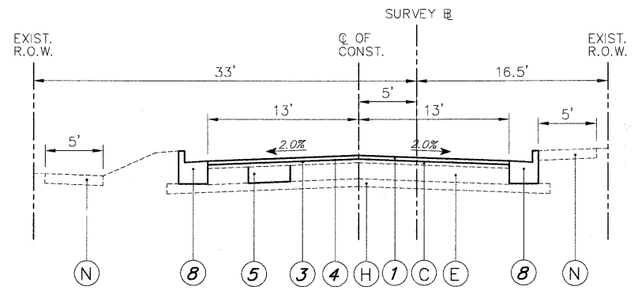
STA. 32+50 TO STA. 41+00 (COOLIDGE AVE.)



* FROM STA. 48+86 TO STA. 51+36

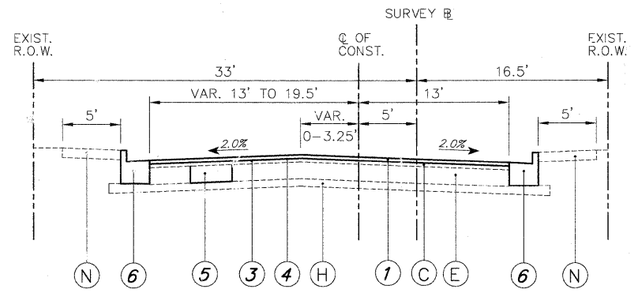
PROPOSED TYPICAL CROSS SECTION

STA. 41+00 TO STA. 51+36 (LAKE ST.)



PROPOSED TYPICAL CROSS SECTION

STA. 51+36 TO STA. 53+82.5 (STATION ST.)



PROPOSED TYPICAL CROSS SECTION

STA. 53+82.5 TO STA. 57+37 (MAIN ST.)

PROPOSED LEGEND

1. *HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50, 2"*
2. *HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 1.5"*
3. *LEVELING BINDER (MACHINE METHOD), N50, VARIES*
4. *BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)*
5. *CLASS D PATCHING AS MARKED IN FIELD*
6. *COMB. CONC. CURB & GUTTER, TYPE B-6.12 WHERE MARKED IN THE FIELD*
7. *COMB. CONC. CURB & GUTTER, TYPE B-6.18 WHERE MARKED IN THE FIELD*
8. *COMB. CONC. CURB & GUTTER, TYPE B-6.24 WHERE MARKED IN THE FIELD*
9. *CONCRETE GUTTER, TYPE B WHERE MARKED IN THE FIELD*

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS	MAX RAP%
HOT MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 1.5"	PG 64 -22	4% @ 50 GYR	15 %
HOT MIX ASPHALT SURFACE COURSE, MIX 'D', N50, 2.0"	PG 64 -22	4% @ 50 GYR	15 %
LEVELING BINDER (MACHINE METHOD), N50	PG 64 -22 *	4% @ 50 GYR	25 %
CLASS D PATCHES (HMA BINDER IL-19 mm)	PG 64 -22 *	4% @ 50 GYR	25 %

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUALTITIES IS 112 LBS/SQ/IN

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

PROPOSED TYPICAL SECTIONS

SCALE: NONE | SHEET NO. 4 OF 12 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	15	4

CONTRACT NO. 63231

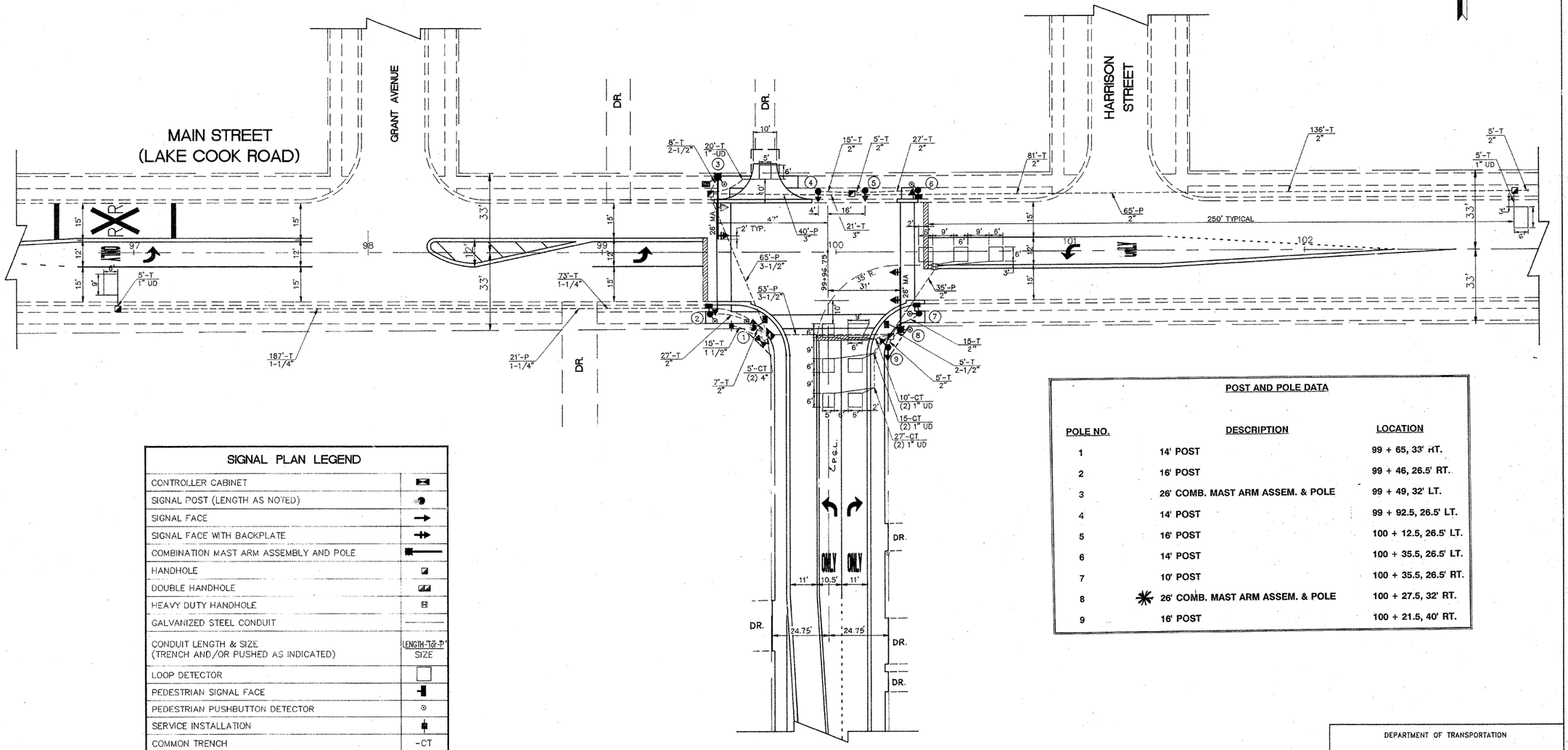
ILLINOIS | FED. AID PROJECT

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	*	COOK & LAKE	41	29
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT- M-6003(883)		

* BARRINGTON 90-00059-00-WR
STATE 70-WRS-90

✱ Note: Combination Steel Mast Arm Assembly & Pole Shall Include a 10' Nominal Length Luminaire Mast Arm @ a 45' Mounting Height.

THIS SHEET INCLUDED FOR REFERENCE ONLY. DETECTOR LOOPS SHALL BE REPLACED ON DUNDEE AVENUE ONLY IN ACCORDANCE WITH THE SPECIAL PROVISION "DETECTOR LOOP REPLACEMENT".



SIGNAL PLAN LEGEND

CONTROLLER CABINET	
SIGNAL POST (LENGTH AS NOTED)	
SIGNAL FACE	
SIGNAL FACE WITH BACKPLATE	
COMBINATION MAST ARM ASSEMBLY AND POLE	
HANDHOLE	
DOUBLE HANDHOLE	
HEAVY DUTY HANDHOLE	
GALVANIZED STEEL CONDUIT	
CONDUIT LENGTH & SIZE (TRENCH AND/OR PUSHED AS INDICATED)	LENGTH-"T" OR "P" SIZE
LOOP DETECTOR	
PEDESTRIAN SIGNAL FACE	
PEDESTRIAN PUSHBUTTON DETECTOR	
SERVICE INSTALLATION	
COMMON TRENCH	-CT
LOUVERED SIGNAL FACE	"L"
OPTICALLY PROGRAMMED SIGNAL FACE	"P"

POST AND POLE DATA

POLE NO.		DESCRIPTION	LOCATION
1		14' POST	99 + 65, 33' RT.
2		16' POST	99 + 46, 26.5' RT.
3		26' COMB. MAST ARM ASSEM. & POLE	99 + 49, 32' LT.
4		14' POST	99 + 92.5, 26.5' LT.
5		16' POST	100 + 12.5, 26.5' LT.
6		14' POST	100 + 35.5, 26.5' LT.
7		10' POST	100 + 35.5, 26.5' RT.
8	✱	26' COMB. MAST ARM ASSEM. & POLE	100 + 27.5, 32' RT.
9		16' POST	100 + 21.5, 40' RT.

REVISIONS	
NAME	DATE
F.R.B.	04-28-92

DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION

MAIN ST. AND DUNDEE AVE.

SCALE : 1" = 20'

DATE 3/10/92
CHECKED BY DMH

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

DETECTOR LOOP REPLACEMENT

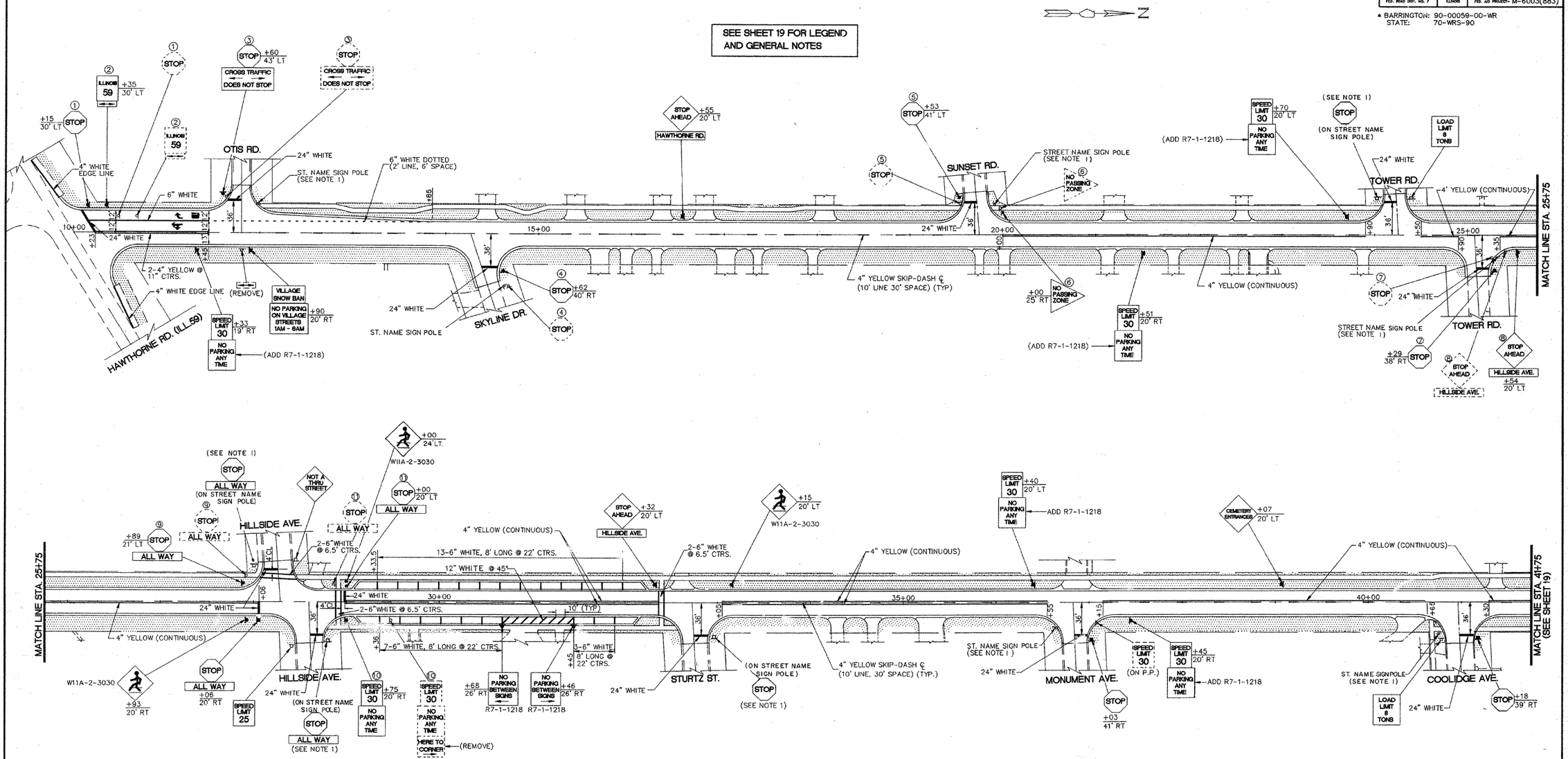
SCALE: NONE | SHEET NO. 5 OF 12 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00089-00-RS	COOK	13	5
		CONTRACT NO. 63231		
	ILLINOIS FED. AID PROJECT			

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	*	COOK & LAKE	41	18
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT- M-6003(883)		

* BARRINGTON: 90-00059-00-WR
STATE: 70-WRS-90

SEE SHEET 19 FOR LEGEND AND GENERAL NOTES



THIS SHEET INCLUDED FOR REFERENCE ONLY. PAVEMENT MARKINGS SHALL BE REPLACED WITHIN THE LIMITS OF CONSTRUCTION AND PAID FOR AT THE CONTRACT UNIT PRICES FOR PAVEMENT MARKINGS.

REVISIONS	
NAME	DATE

DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING, SIGNING AND LANDSCAPING

DUNDEE AVENUE

STA. 10+00 TO STA. 41+75

50 0 50

DATE 3/10/92
CHECKED BY RJK

FILE NAME =	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
4167-000_Dundee.dwg		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

PAVEMENT MARKING

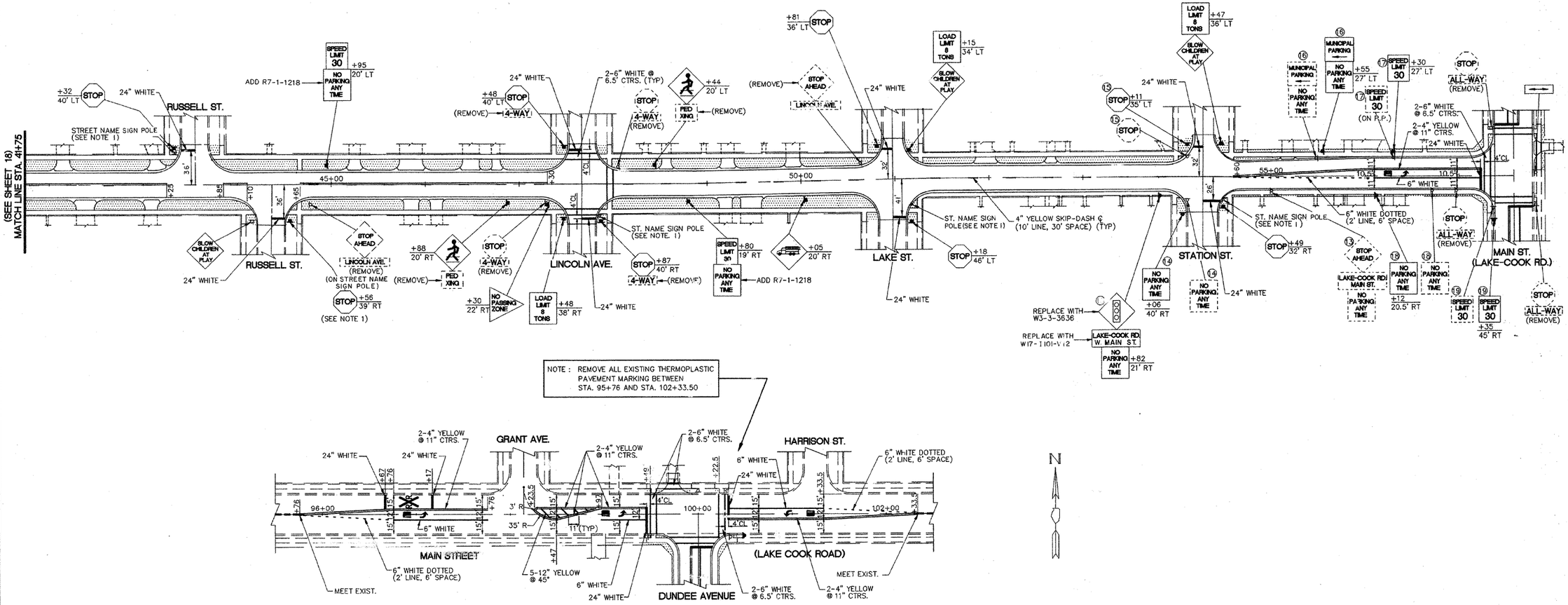
SCALE: NONE | SHEET NO. 6 OF 12 SHEETS | STA. 10+00 TO STA. 41+75

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00089-00-RS	COOK	12	6
		CONTRACT NO. 63231		
		ILLINOIS	FED. AID PROJECT	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	*	COOK & LAKE	41	19
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT- M-6003(883)		

* BARRINGTON: 90-00059-00-WR
STATE: 70-WRS-90

THIS SHEET INCLUDED FOR REFERENCE ONLY. PAVEMENT MARKINGS SHALL BE REPLACED WITHIN THE LIMITS OF CONSTRUCTION AND PAID FOR AT THE CONTRACT UNIT PRICES FOR PAVEMENT MARKINGS.



LEGEND (SHEETS 18 & 19)

- EXISTING SIGN PANEL ASSEMBLY TO BE REMOVED, OR RELOCATED ON NEW TELESCOPING STEEL SIGN SUPPORT, UNLESS OTHERWISE NOTED.
- NEW SIGN PANEL ASSEMBLY ON NEW TELESCOPING STEEL SIGN SUPPORT, OR RELOCATED SIGN PANEL ASSEMBLY
- EXISTING SIGN PANEL ASSEMBLY TO REMAIN IN PLACE
- EXISTING SIGN PANEL ASSEMBLY TO BE TEMPORARILY REMOVED AND RE-SET IN APPROXIMATELY THE SAME LOCATION ON NEW TELESCOPING STEEL SIGN SUPPORT
- 4" TOP SOIL AND SODDING

GENERAL NOTES (SHEETS 18 & 19)

1. STREET NAME SIGN POLES SHALL BE TEMPORARILY REMOVED AND RESET AT LOCATIONS AS DIRECTED BY THE ENGINEER, AND THE COST THEREOF SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. STOP SIGNS MOUNTED ON STREET NAME SIGN POLES SHALL REMAIN SO MOUNTED, BUT THEY MUST BE VISIBLE AT ALL TIMES DURING CONSTRUCTION.
2. ALL NEW AND EXISTING SIGNS SHALL CONFORM TO THE 'ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS'. THOSE EXISTING SIGNS THAT DO NOT CONFORM SHALL BE REPLACED AS DIRECTED BY THE ENGINEER, AND PAYMENT WILL BE FOR 'SIGN PANEL - TYPE 1'.
3. MOUNTING HEIGHT FOR SIGNS IS 7'.
4. ALL PERMANENT PAVEMENT MARKINGS ON DUNDEE AVENUE SHALL BE PREFORMED PLASTIC AS MANUFACTURED BY 3 M CO., OR APPROVED EQUAL, AND SHALL BE PLACED IN ACCORDANCE WITH ARTICLE T 503.03(c) OF THE 'TRAFFIC SPECIFICATIONS'. IN THE EVENT THE ROAD IS OPENED TO THRU TRAFFIC FOR THE WINTER BEFORE THE FINAL SURFACE COURSE CAN BE PLACED, IDENTICAL PAINTED MARKINGS SHALL BE IN PLACE ON THE BASE OR BINDER COURSE PRIOR TO OPENING, AND PAYMENT WILL BE FOR 'PAINT PAVEMENT MARKING - LINE OR LETTERS AND SYMBOLS'. IF THE PAINTED MARKINGS ARE NOT REQUIRED, THEY SHALL BE DEDUCTED FROM THE CONTRACT.
5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE, UNLESS OTHERWISE SPECIFIED.
6. TURN LANE MARKINGS (ARROW & "ONLY") SHALL BE LARGE SIZE.
7. ALL TELESCOPING STEEL SIGN SUPPORTS SHALL BE GROUND MOUNTED, AND SHALL BE EITHER THE "QWIK-PUNCH SYSTEM" AS MANUFACTURED BY ALLIED TUBE AND CONDUIT CO. OF HARVEY, ILLINOIS, OR AN APPROVED EQUAL.
8. ALL PERMANENT PAVEMENT MARKINGS ON MAIN STREET SHALL BE THERMOPLASTIC, PLACED IN ACCORDANCE WITH ARTICLE T 501.03 OF THE 'TRAFFIC SPECIFICATIONS'. IN THE EVENT THE ROAD IS READY FOR PERMANENT MARKINGS BETWEEN NOVEMBER 1 AND APRIL 25, IDENTICAL PAINTED MARKINGS SHALL BE INSTALLED, AND PAYMENT WILL BE FOR 'PAINT PAVEMENT MARKING - LINE OR LETTERS AND SYMBOLS'. IN THIS CASE THE REMOVAL OF THE PAINTED MARKINGS PRIOR TO THE INSTALLATION OF FINAL THERMOPLASTIC MARKINGS, IF NECESSARY, WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

REVISIONS	
NAME	DATE

DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING, SIGNING AND LANDSCAPING

DUNDEE AVENUE
STA. 41+75 TO STA. 57+57.80

MAIN STREET
STA. 95+76 TO STA. 102+33.50

50 0 50

DATE 3/10/92
CHECKED BY RJK

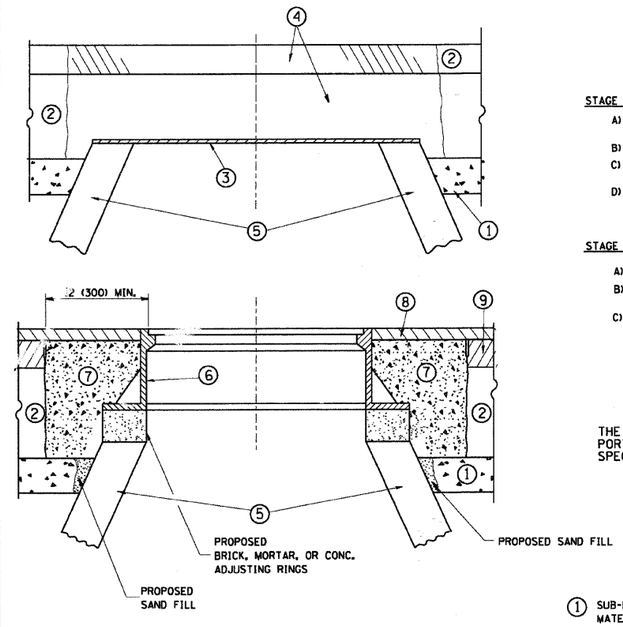
FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE RESURFACING PROGRAM

PAVEMENT MARKING

SCALE: NONE | SHEET NO. 7 OF 12 SHEETS | STA. 41+75 TO STA. 57+57.80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00089-00-RS	COOK	13	7
		CONTRACT NO. 63231		
		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\bd88.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD600-03 (BD-8)	CONTRACT NO.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

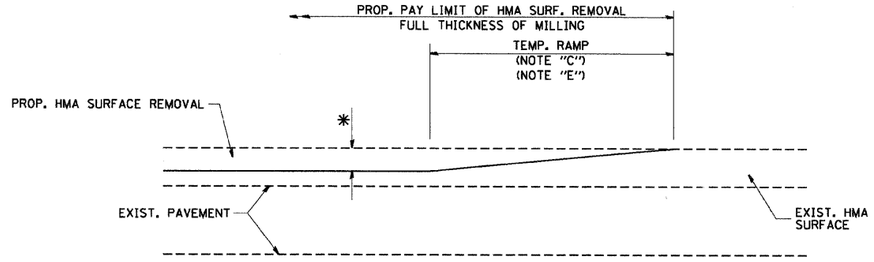
FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE
RESURFACING PROGRAM

DETAILS

SCALE: NONE | SHEET NO. 8 OF 12 SHEETS | STA. | TO STA.

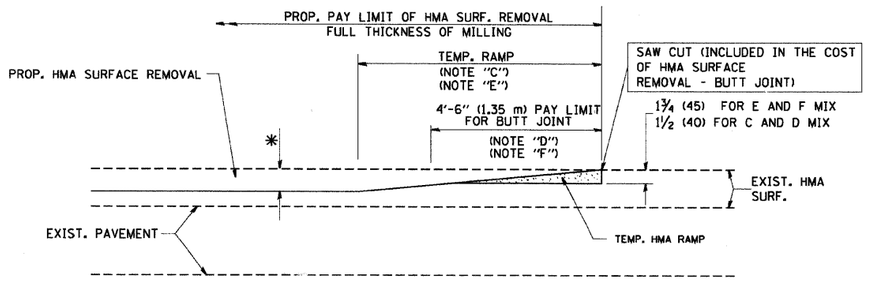
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	8
		CONTRACT NO. 63231		
	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

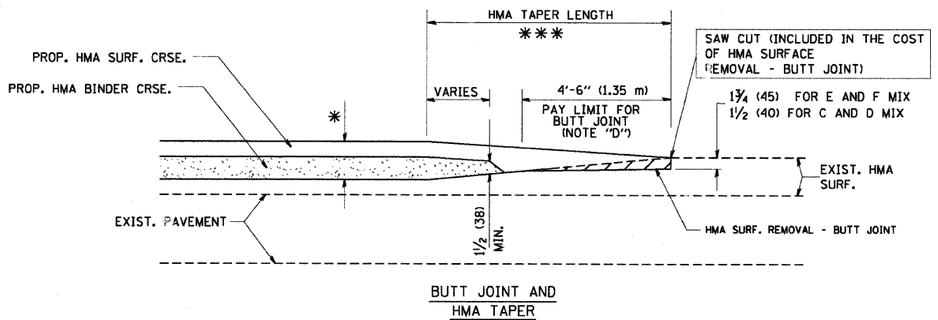


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

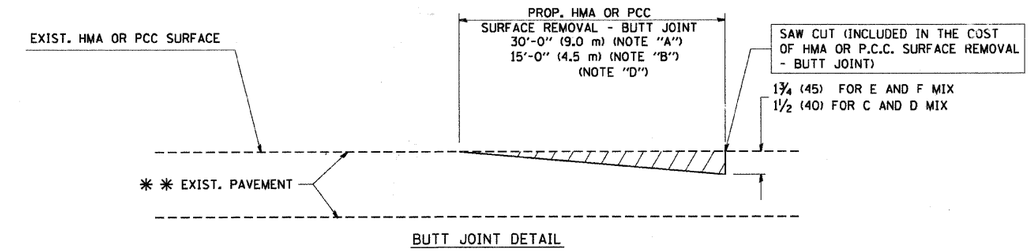
OPTION 2

TYPICAL TEMPORARY RAMP

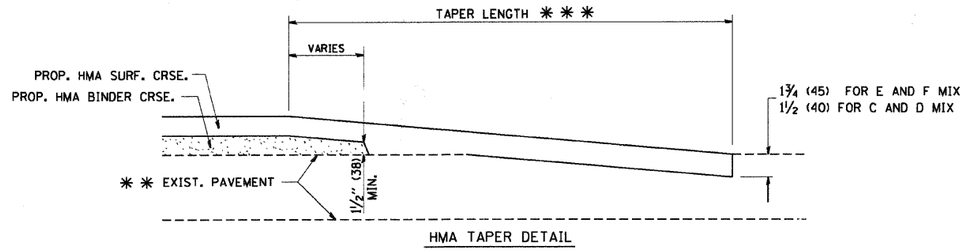


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = geglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD400-05 BD32	CONTRACT NO.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

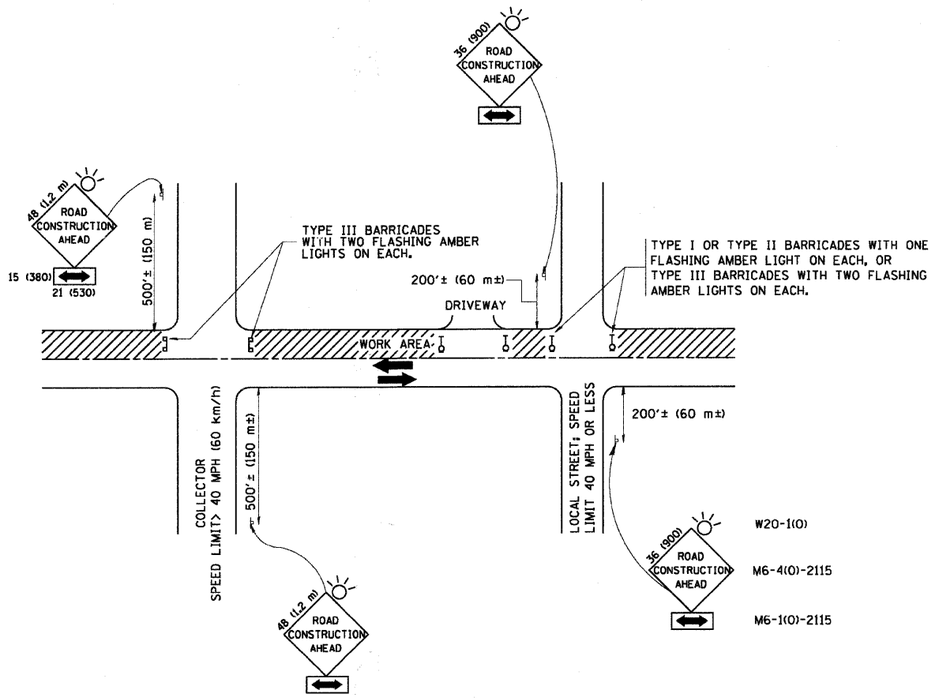
FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE
RESURFACING PROGRAM

DETAILS

SCALE: NONE | SHEET NO. 9 OF 12 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	9
		CONTRACT NO. 63231		
	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-10	CONTRACT NO.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

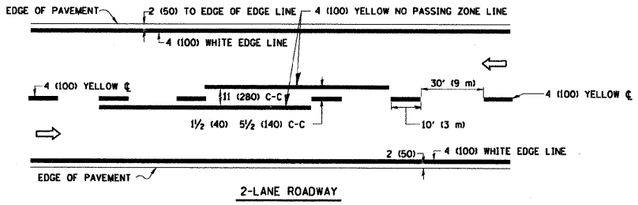
FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED -
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE
RESURFACING PROGRAM

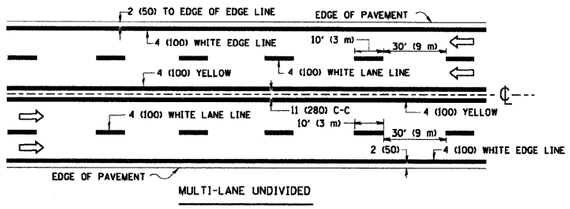
DETAILS

SCALE: NONE | SHEET NO. 10 OF 12 SHEETS | STA. | TO STA.

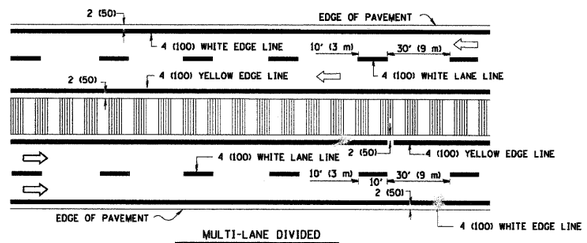
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	10
		CONTRACT NO. 63231		
	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



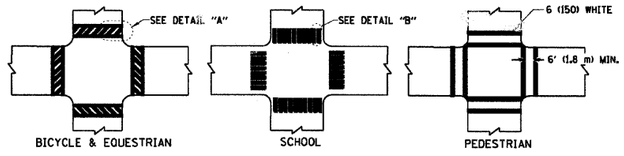
MULTI-LANE UNDIVIDED

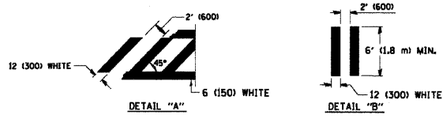


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

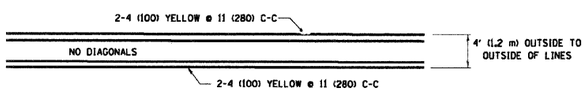
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

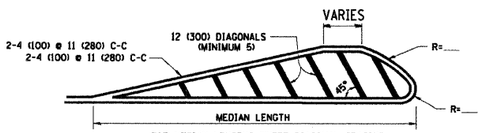




TYPICAL CROSSWALK MARKING



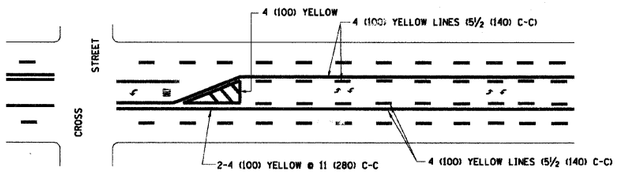
4' (1.2 m) WIDE MEDIANS ONLY



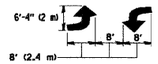
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

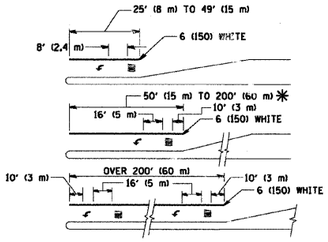


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

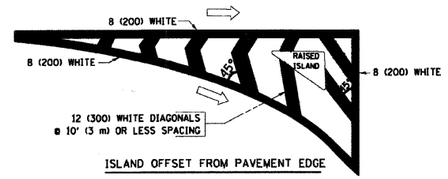


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

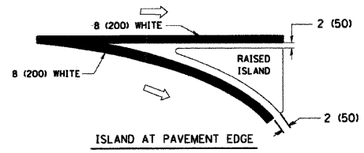
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

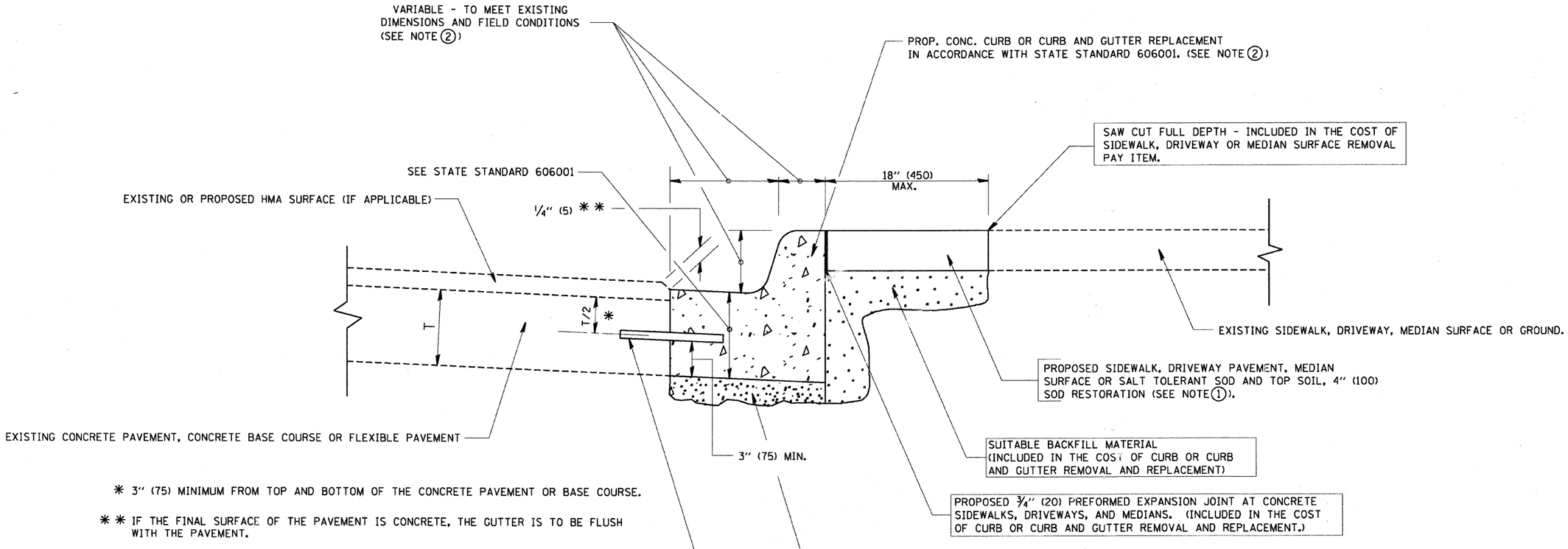
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tc13.dgn	USER NAME = geglienobt	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
		DRAWN -	REVISED -A. HOUSEH 10-09-96		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -A. HOUSEH 10-17-96		
	PLOT DATE = 1/4/2000	DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13	CONTRACT NO.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09	DUNDEE AVENUE RESURFACING PROGRAM	DETAILS
		DRAWN - LXM	REVISED -		
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -		SCALE: NONE | SHEET NO. 11 OF 12 SHEETS | STA. | TO STA.
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	11
		CONTRACT NO. 63231		
	ILLINOIS | FED. AID PROJECT			



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(2) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(3) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(4) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(5) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(7) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

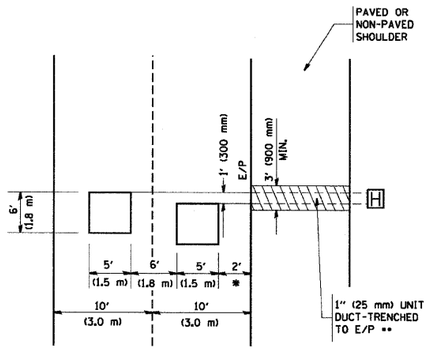
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd24.dgn	USER NAME = gaglienobt	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97								
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01						BD600-06 (BD-24)	CONTRACT NO.	
	PLOT DATE = 1/4/2008	DATE - 03-11-94	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09	DUNDEE AVENUE RESURFACING PROGRAM	DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - LXM	REVISED -					2555	09-00085-00-RS	COOK	13 / 12
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -							CONTRACT NO. 63231	
	PLOT DATE = 6/30/09	DATE - 5/26/09	REVISED -		SCALE: NONE	SHEET NO. 12 OF 12 SHEETS	STA. TO STA.		ILLINOIS	FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

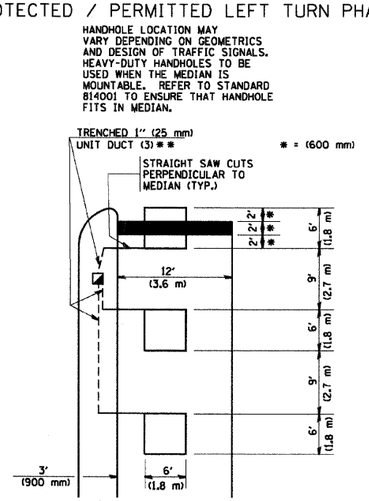


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

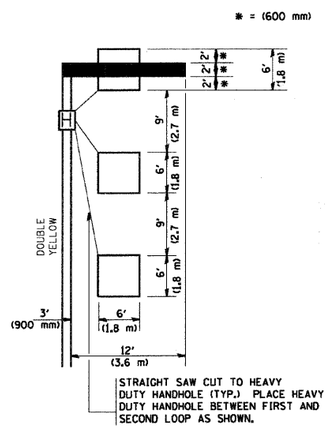


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

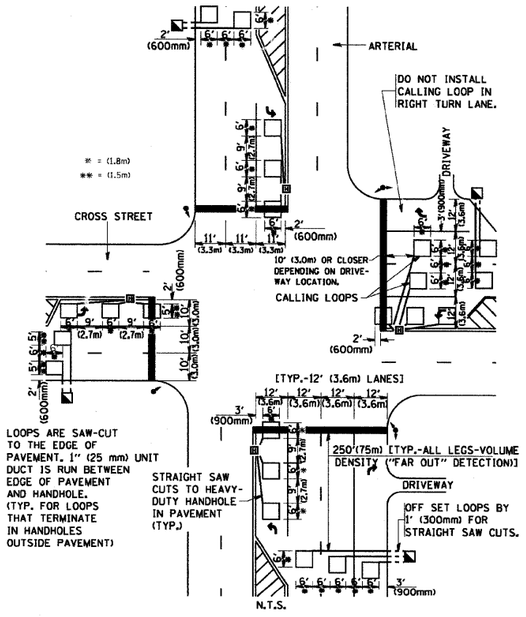
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



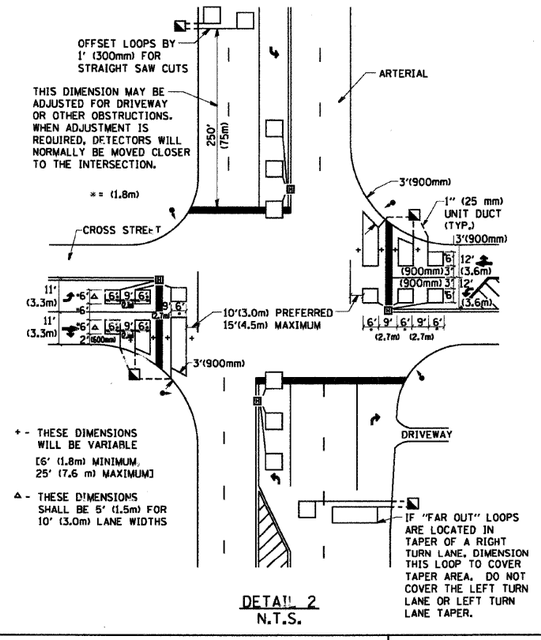
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TS-07	CONTRACT NO.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FILE NAME = 4167-000_Dundee.dwg	USER NAME =	DESIGNED - LXM	REVISED - LXM 6/30/09
		DRAWN - LXM	REVISED - LXM 7/8/09
	PLOT SCALE = N.T.S	CHECKED - DEM	REVISED -
	PLOT DATE = 7/8/09	DATE - 5/26/09	REVISED -

DUNDEE AVENUE
RESURFACING PROGRAM

DETAILS

SCALE: NONE | SHEET NO. 13 OF 13 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2555	09-00085-00-RS	COOK	13	13
		CONTRACT NO. 63231		
	ILLINOIS	FED. AID PROJECT		