

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	1
ILLINOIS PROJECT ARA-9003(324)				
CONTRACT NO. 63257				



LOCATION OF SECTION INDICATED THIS:

INDEX OF SHEETS ON SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID PROJECT
LOCAL AGENCY PAVEMENT PRESERVATION (LAPP)
FAU ROUTE 2713 (WESTCHESTER BOULEVARD)
CANTERBURY STREET TO ROOSEVELT ROAD
SECTION NO. 09-00074-00-RS
PROJECT ARA-9003(324)
VILLAGE OF WESTCHESTER
COOK COUNTY
JOB NO. C-91-631-09

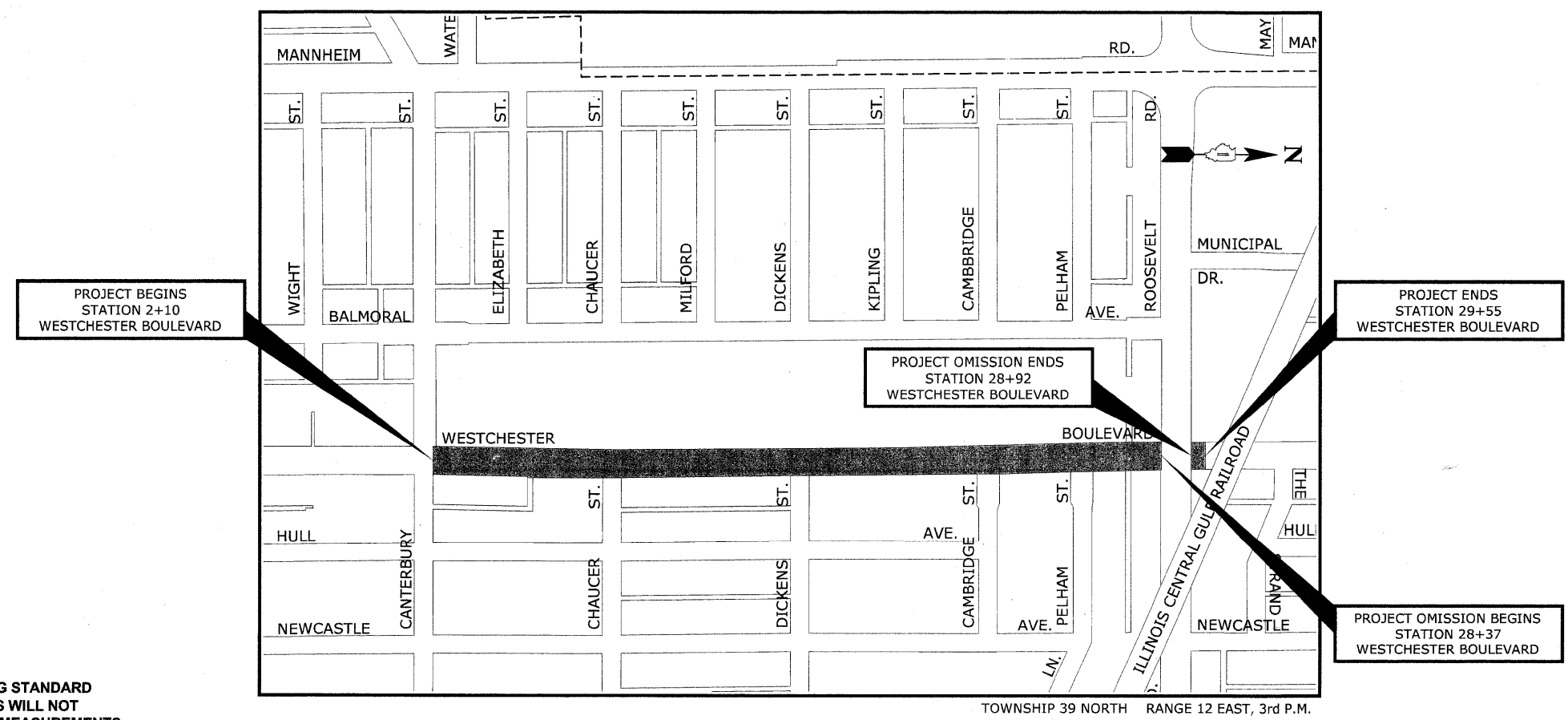
TRAFFIC DATA
ADT (2009) = 3,300
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED LIMIT = 30 MPH

DESIGN DESIGNATION
COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF WESTCHESTER



Know what's below.
Call before you dig.

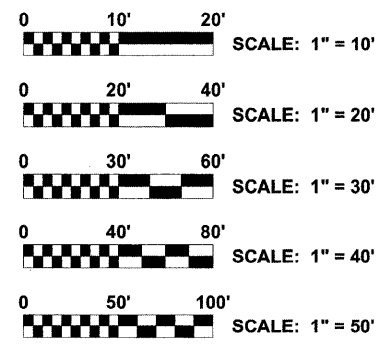


PROJECT BEGINS STATION 2+10 WESTCHESTER BOULEVARD

PROJECT OMISSION ENDS STATION 28+92 WESTCHESTER BOULEVARD

PROJECT ENDS STATION 29+55 WESTCHESTER BOULEVARD

PROJECT OMISSION BEGINS STATION 28+37 WESTCHESTER BOULEVARD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 63257

- AREA OF IMPROVEMENT
NOT TO SCALE
GROSS LENGTH OF IMPROVEMENT = 2,745 FT = 0.52 MI.
NET LENGTH OF IMPROVEMENT = 2,690 FT = 0.51 MI.

STATE OF ILLINOIS
ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED _____ 20
VILLAGE OF WESTCHESTER, PRESIDENT

PASSED July 16 2009
Christopher Hart
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW JULY 16, 2009
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS,
REGION 1 ENGINEER

(PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS)



DATE SIGNED: 07-14-09
LICENSE EXPIRES: 11-30-09

EDWIN HANCOCK ENGINEERING COMPANY
9933 ROOSEVELT ROAD PHONE: (708) 865-0300
WESTCHESTER, ILLINOIS 60154

FIELD ENGINEER: MARLIN SOLOMON, (847)705-4407

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, LEGEND OF SYMBOLS, AND I.D.O.T. STANDARD DRAWINGS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
6	DETAILS
7-8	PAVING PLANS
9	STAGING PLAN AND TRAFFIC SIGNAL DETECTOR LOOP REPLACEMENT
10	DETOUR FOR THROUGH TRAFFIC
11	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
12	CURB AND GUTTER REMOVAL AND REPLACEMENT (BD 24)
13	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
15	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)
16	TRAFFIC CONTROL AND PROTECTIONS AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC 14)
17	ARTERIAL ROAD INFORMATION SIGN (TC 22)
18	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS 07)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442101-07	CLASS B PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701301-03	LANE CLOSURE, 2-LANE, 2-WAY, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701606-06	URBAN LANE CLOSURE, MULTILANE, 2-WAY, WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

LEGEND OF SYMBOLS

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-05)

SYMBOL	DESCRIPTION
	EXISTING HOT-MIX ASPHALT AREA
	EXISTING CONCRETE AREA
	EXISTING GRASS AREA
	PROPOSED HOT-MIX ASPHALT BUTT JOINT
	EXISTING CONCRETE SIDEWALK OR DRIVEWAY REMOVAL
	PROPOSED CONCRETE AREA, 5" SIDEWALK, 7" DRIVEWAY
	PROPOSED HOT-MIX ASPHALT PAVING AREA
	PROPOSED CLASS C PATCHES
	PROPOSED DETECTABLE WARNINGS
A	STRUCTURE TO BE ADJUSTED
A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
1C	NEW FRAME AND LID, TYPE 1, CLOSED LID
1P	NEW FRAME AND LID, TYPE 1, OPEN LID
	EXISTING DOMESTIC WATER SERVICE BOX
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE BOX
	EXISTING WATER MAIN VALVE VAULT
	EXISTING STORM SEWER INLET
	EXISTING STORM SEWER CATCH BASIN
	EXISTING SEWER MANHOLE
	EXISTING STREET LIGHT POLE
	EXISTING POWER POLE
	EXISTING TRAFFIC SIGNAL POLE
	EXISTING TRAFFIC SIGNAL MAST ARM
	EXISTING HANDHOLE
	DOUBLE HANDHOLE
	EXISTING TRAFFIC SIGNAL OR STREET LIGHT CONTROLLER
	EXISTING TRAFFIC SIGNAL MANHOLE
	EXISTING CURB AND GUTTER
	PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

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9010 Riverside Blvd
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Phone: 708.942.4300
Fax: 708.942.4322

USER NAME	DESIGNED	TG	REVISED
	DRAWN	TG	REVISED
	CHECKED	JG	REVISED
	DATE	07/14/09	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, AND LEGEND OF SYMBOLS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	2
CONTRACT NO. 63257			ILLINOIS FED. AID PROJECT ARA-9003(324)	

E.H.E. PROJECT NO. 838-09-10201

GENERAL NOTES

STANDARDS

REFERENCES TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS IN THE PLANS. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2009, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE "DETAILS" IN THE PLANS, AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

UNDERGROUND UTILITIES

BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES IF SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF WESTCHESTER, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXACT LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF WESTCHESTER.

FRAMES AND LIDS

THE TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF WESTCHESTER AND BE SALVAGED. THESE ITEMS SHALL BE DELIVERED TO THE VILLAGE OF WESTCHESTER PUBLIC WORKS DEPARTMENT.

MANHOLE OR VALVE COVERS

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN DURING THE CONSTRUCTION OPERATIONS, LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS DUE TO CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

THIS ITEM ONLY PERTAINS TO STRUCTURES LOCATED IN THE CONCRETE OR HOT-MIX ASPHALT ROADWAY PAVEMENT AREAS THAT WILL REQUIRE CONCRETE OR HOT-MIX SURFACE REMOVAL. THE ENGINEER WILL MARK IN THE FIELD ALL STRUCTURES TO BE DONE UNDER THIS ITEM. SEE DETAIL SHEET FOR "FRAMES AND LIDS ADJUSTMENT WITH MILLING."

PRIME COAT

PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT.

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY ROW WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR THE VILLAGE.

BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

BUTT JOINTS

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

MILLED PAVEMENT OPEN TO TRAFFIC

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)

PAVING OPERATIONS

FROM STATION 2+10 TO STATION 26+15 THE CONTRACTOR MUST PAVE THE ROADWAY (19' E-E) ON EACH SIDE OF THE LANDSCAPED MEDIAN IN ONE PASS.

PAVEMENT PATCHING

LOCATIONS OF PATCHES ON PLANS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN FIELD BY ENGINEER. HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE (PP 3200PSI IN FORTY-EIGHT (48) HOURS) SHALL BE USED.

<p>MAINTENANCE OF SEWER FLOWS</p> <p>THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.</p>
<p>MAINTENANCE OF EXISTING DRAINAGE STRUCTURES</p> <p>WHEN DURING THE CONSTRUCTION OPERATIONS, LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS DUE TO CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.</p>
<p>SAW CUTTING</p> <p>THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.</p>

USER NAME	DESIGNED	TG	REVISED
	DRAWN	TG	REVISED
PLOT SCALE	CHECKED	JG	REVISED
PLOT DATE	DATE	07/14/09	REVISED

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	3
CONTRACT NO. 63257			ARA-903(324)	

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	1000 TOTAL QUANTITY	1000 100% FEDERAL 0% LOCAL
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	300	300
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5	5
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5
25200100	SODDING	SQ YD	300	300
25200200	SUPPLEMENTAL WATERING	UNIT	10	10
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	40	40
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	1,450	1,450
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	20	20
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	900	900
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	500	500
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,700	1,700
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	5	5
42101300	PROTECTIVE COAT	SQ YD	200	200
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	40	40
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	850	850
⊛ 42400800	DETECTABLE WARNINGS	SQ FT	60	60
⊛ 44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	13,400	13,400
⊛ 44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	40	40
⊛ 44000600	SIDEWALK REMOVAL	SQ FT	850	850
⊛ 44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	400	400
⊛ 44200924	CLASS B PATCHES, TYPE III, 7 INCH	SQ YD	50	50
⊛ 44200926	CLASS B PATCHES, TYPE IV, 7 INCH	SQ YD	350	350
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1
⊛ 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	31	31
⊛ 60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5
⊛ 60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5
67100100	MOBILIZATION	L SUM	1	1
⊛ 70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	1
70300200	TEMPORARY PAVEMENT MARKING	FOOT	1,000	1,000
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	85	85
★ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	85	85
★ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5,400	5,400
★ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	700	700
★ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100
★ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	150
★⊛ 88600600	DETECTOR LOOP REPLACEMENT	FOOT	700	700
⊛ Z0004900	BITUMINOUS MIXTURE FOR PATCHING POTHoles (HOT MIX)	TON	20	20

- ★ DENOTES SPECIALTY ITEM
 ⊛ DENOTES THAT A SPECIAL PROVISION HAS BEEN PROVIDED

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9913 Research Blvd
 Westchester, Illinois 60151-2798
 Phone: 708.968.0200
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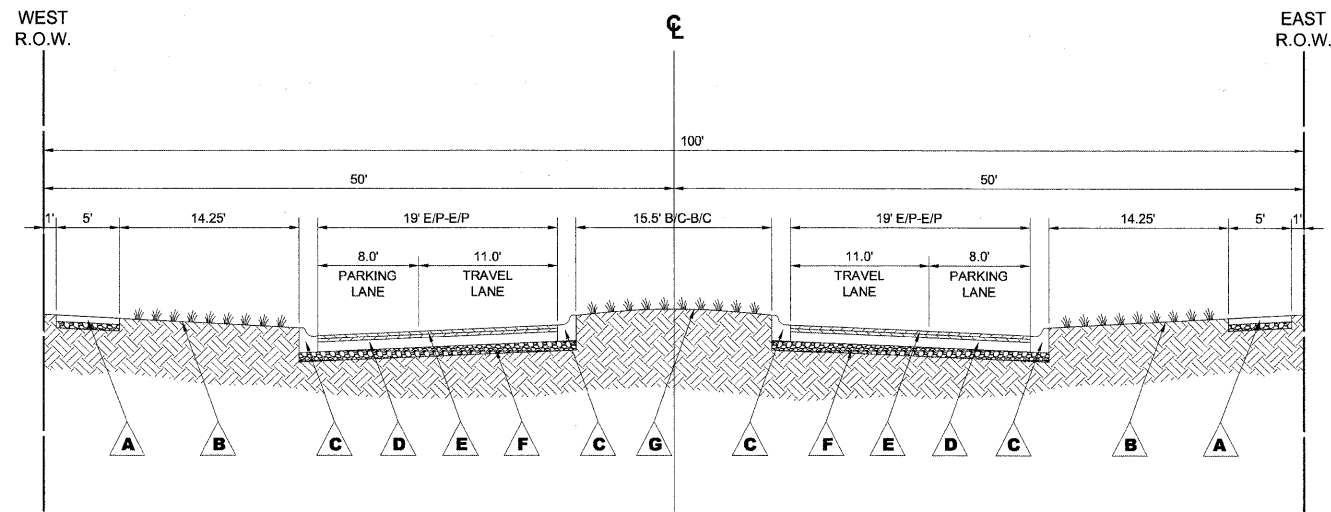
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	DRAWN	TG	REVISED
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

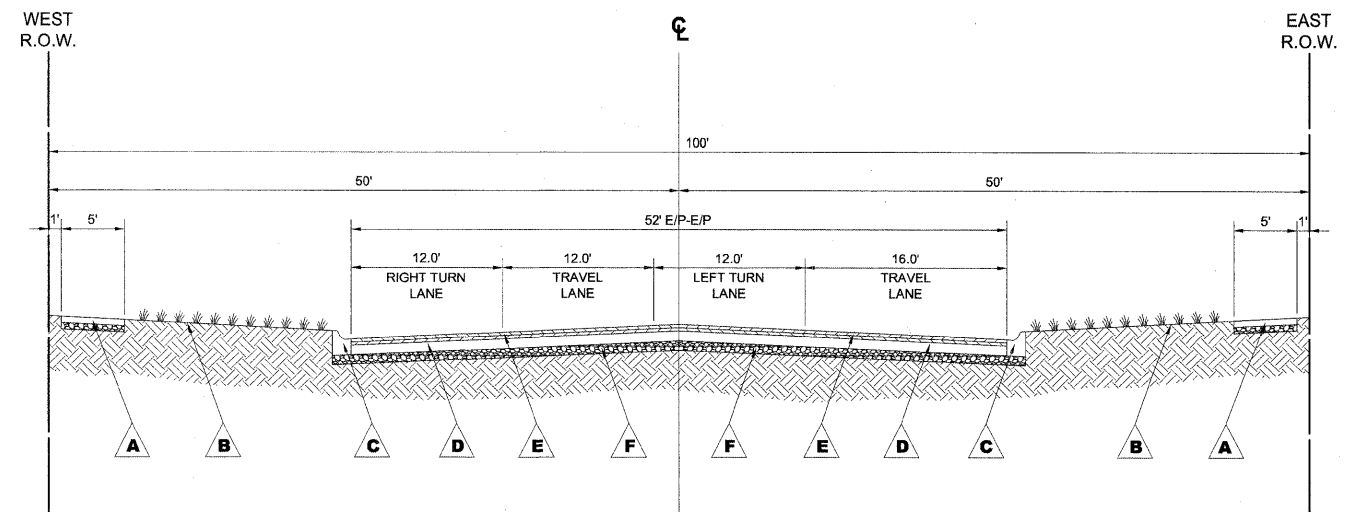
SUMMARY OF QUANTITIES

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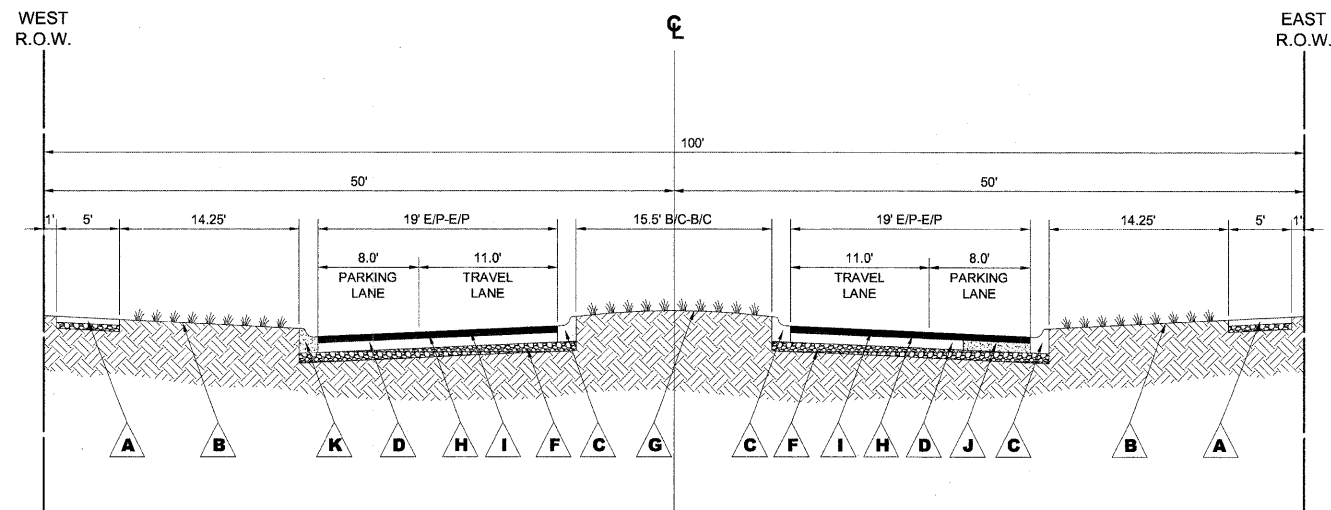
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FAU 2713	09-00074-00-RS	COOK	18	4
CONTRACT NO. 63257			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(324)	



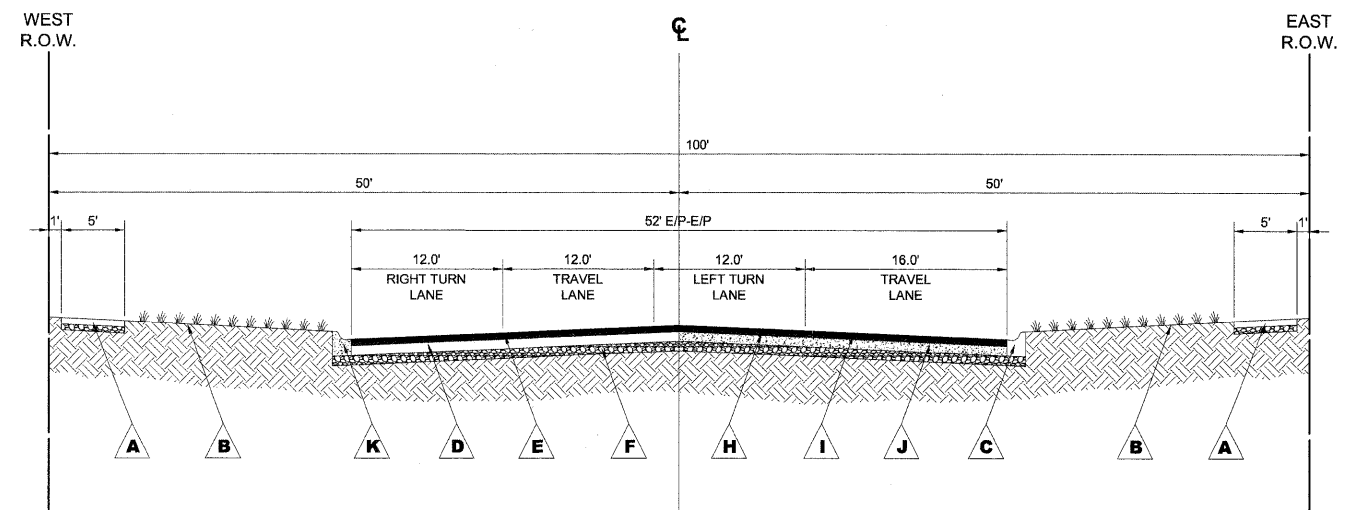
**EXISTING TYPICAL SECTION
WESTCHESTER BOULEVARD
STATION 2+10 TO STATION 24+93**



**EXISTING TYPICAL SECTION
WESTCHESTER BOULEVARD
STATION 24+93 TO STATION 28+37
STATION 28+92 TO STATION 29+55**



**PROPOSED TYPICAL SECTION
WESTCHESTER BOULEVARD
STATION 2+10 TO STATION 24+93**



**PROPOSED TYPICAL SECTION
WESTCHESTER BOULEVARD
STATION 24+93 TO STATION 28+37
STATION 28+92 TO STATION 29+55**

LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	G	EXISTING LANDSCAPED MEDIAN
B	EXISTING SODDED PARKWAY	H	PROPOSED LEVELING BINDER, 1"
C	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12	I	PROPOSED HOT-MIX ASPHALT COURSE, 2"
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE, 7"	J	PROPOSED CLASS B PATCH, 7"
E	EXISTING HOT-MIX ASPHALT BINDER & SURFACE COURSES, 3"	K	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
F	EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B, 4"		

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS

ITEM	A C TYPE	VOIDS
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL - 9.5 mm)	PG 64 - 22	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50 (IL - 9.5 mm)	PG 64 - 22 *	4% @ 50 GYR.
PATCHING		
BITUMINOUS MIXTURE FOR PATCHING POTHOLES (HOT MIX), MIX "C", N50	PG 64 - 22	4% @ 50 GYR.
INCIDENTAL HOT-MIX ASPHALT SURFACING, MIX "C", N50	PG 64 - 22	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE COURSE IS 112 LBS/SQYD/IN.
* WHEN RAP EXCEEDS 20 %, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58 - 22.

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	DRAWN TG	REVISED
PLOT SCALE	CHECKED JG	REVISED
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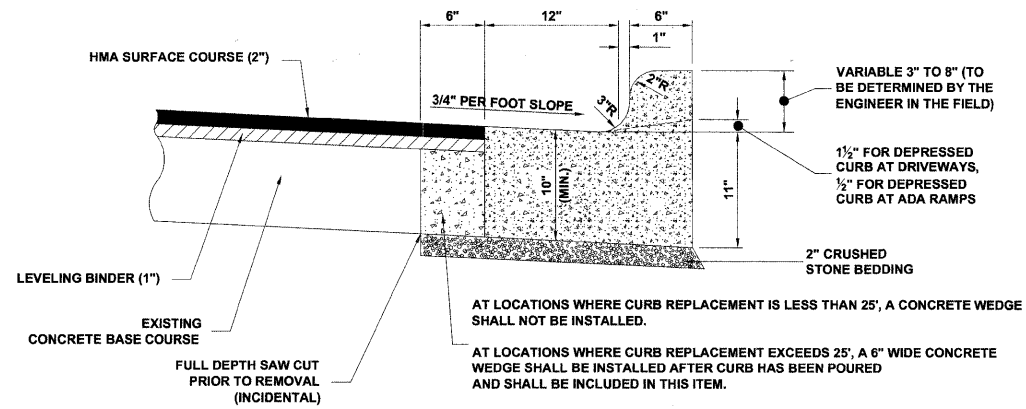
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED TYPICAL CROSS SECTIONS

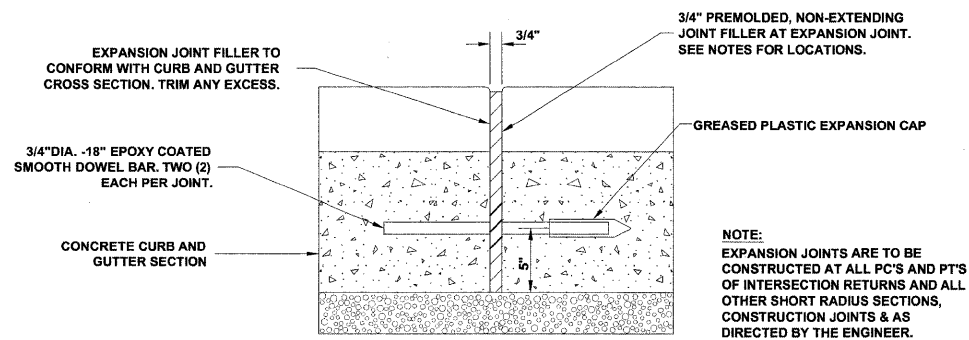
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ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	5
CONTRACT NO. 63257			ILLINOIS FED. AID PROJECT ARA-9003(324)	

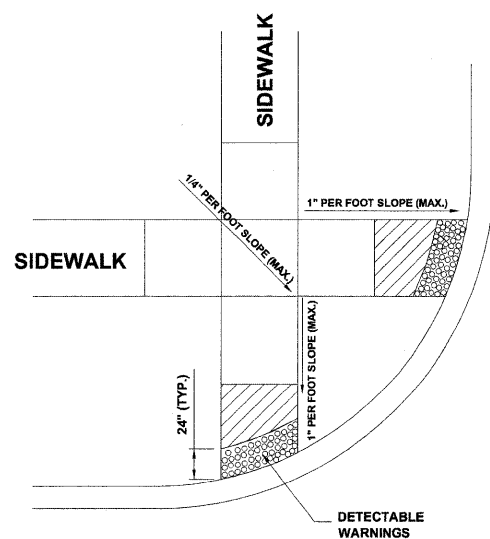
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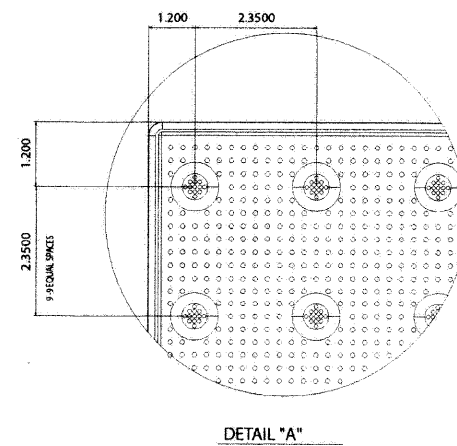
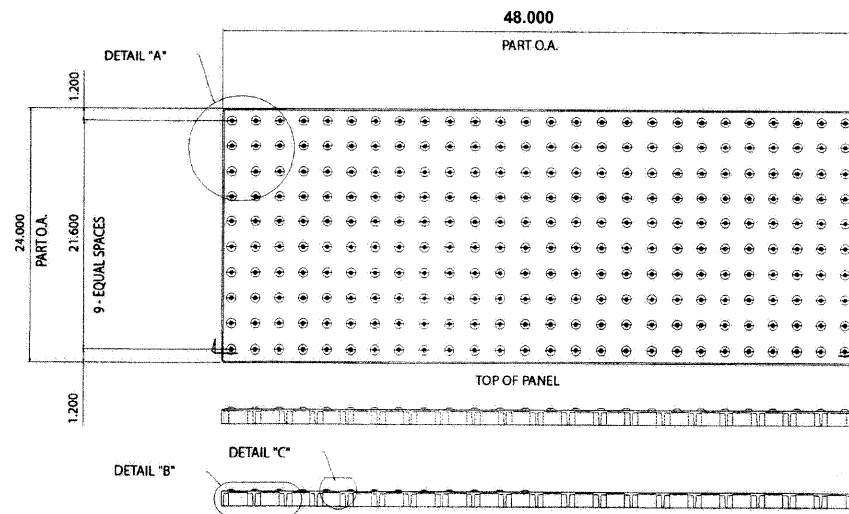
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TYPE B-6.12 (MODIFIED)**



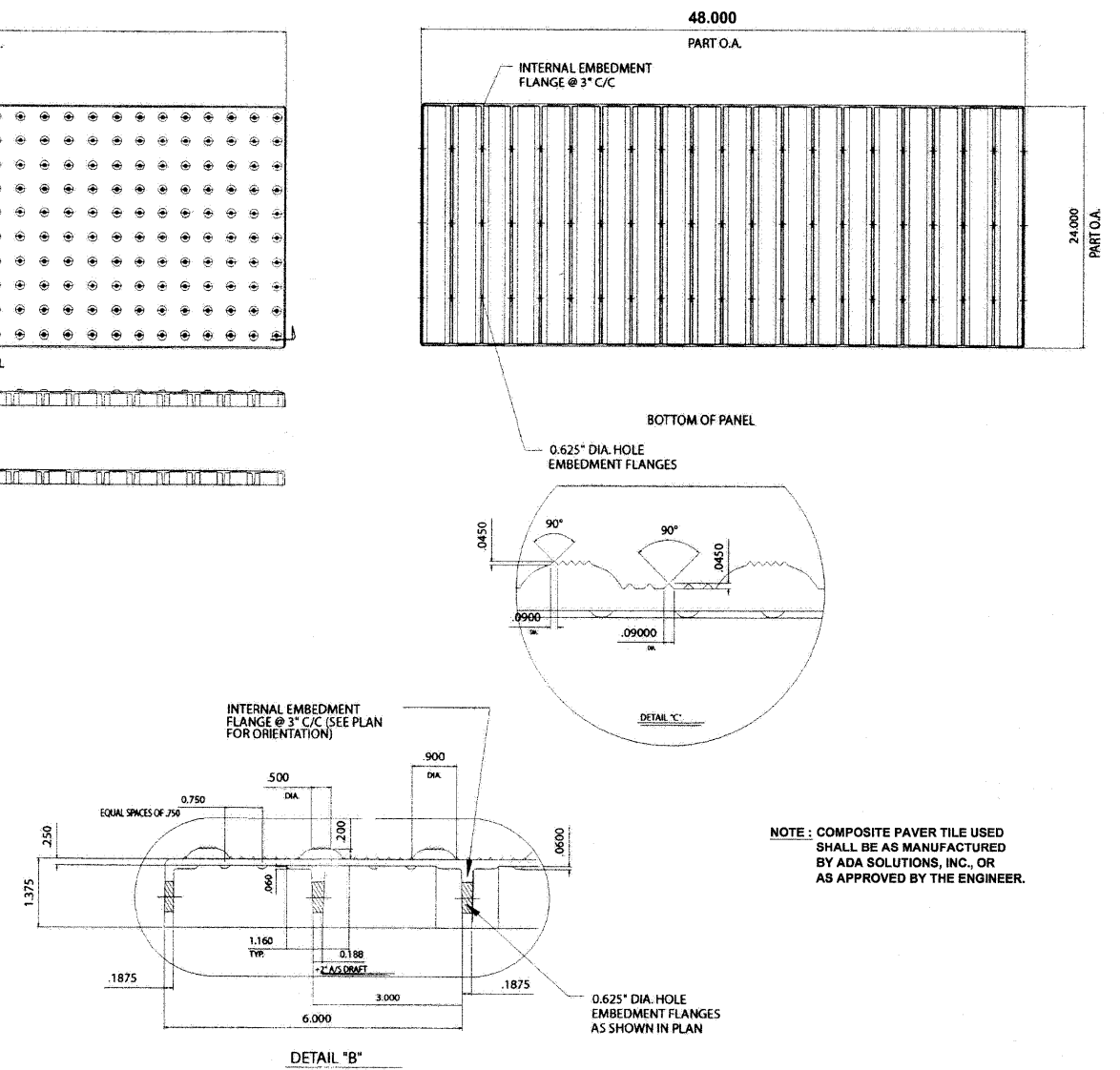
**TYPICAL CURB AND GUTTER
EXPANSION JOINT**



A.D.A. RAMP



CAST-IN-PLACE DETECTABLE WARNINGS



NOTE: COMPOSITE PAVER TILE USED SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC., OR AS APPROVED BY THE ENGINEER.

Drawing file: W:\Projects\32809102 - Westchester Blvd Resurfacing\Detail-WestchesterBld.dwg Jul 14, 2009 - 9:54am



USER NAME	DESIGNED TG	REVISED
PLOT SCALE	DRAWN TG	REVISED
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	DATE 07/14/09	REVISED

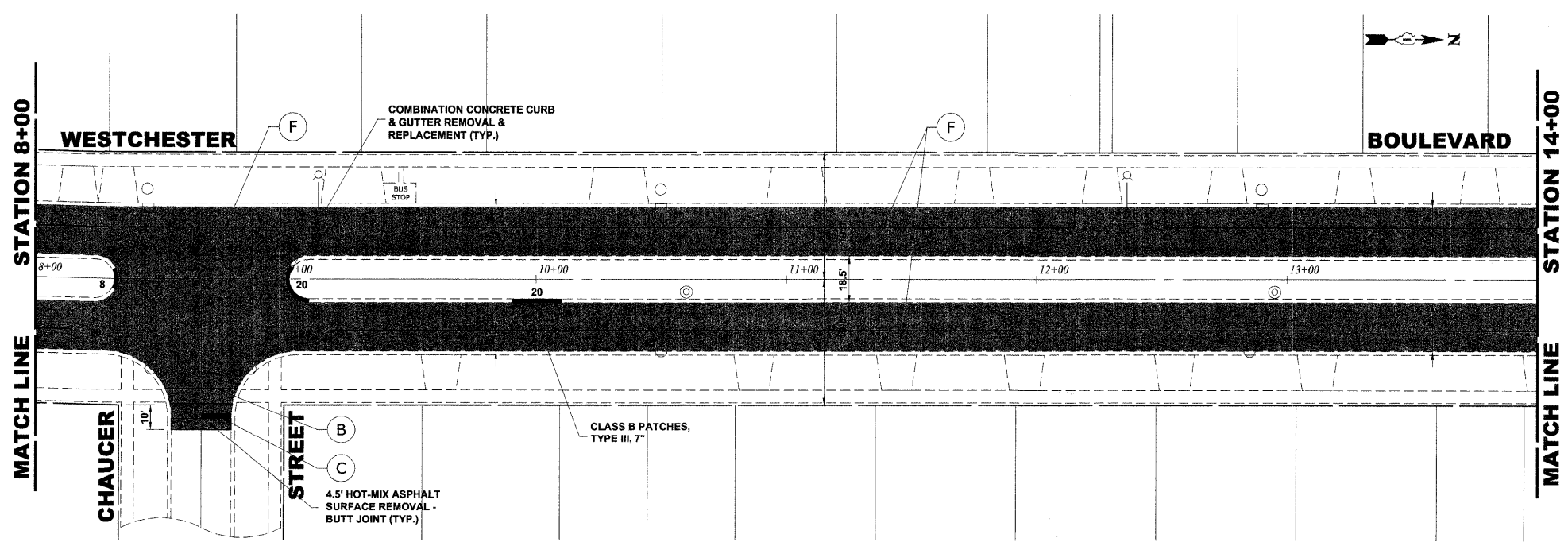
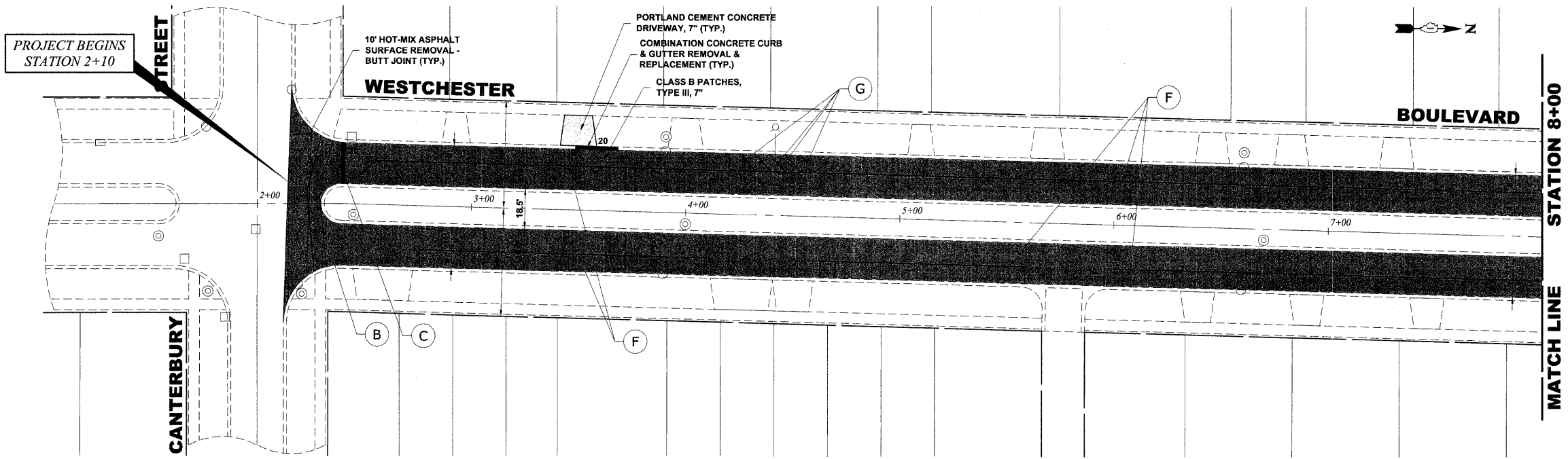
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	6
CONTRACT NO. 63257			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(324)	

E.H.E. PROJECT NO. 838-09-10201



PAVEMENT MARKING LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARALLEL PARKING LINE, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARKING LINE, YELLOW	(G)
THERMOPLASTIC PAVEMENT MARKING LINE 12", MEDIAN DIAGONALS @ 45°, YELLOW	(H)

Drawing file: W:\Projects\83509102 - Westchester Blvd Resurfacing\WestchesterBld.dwg
 Jul 14, 2009 - 9:54am

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

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PLOT SCALE	CHECKED JG	REVISED
PLOT DATE	DATE 07/14/09	REVISED

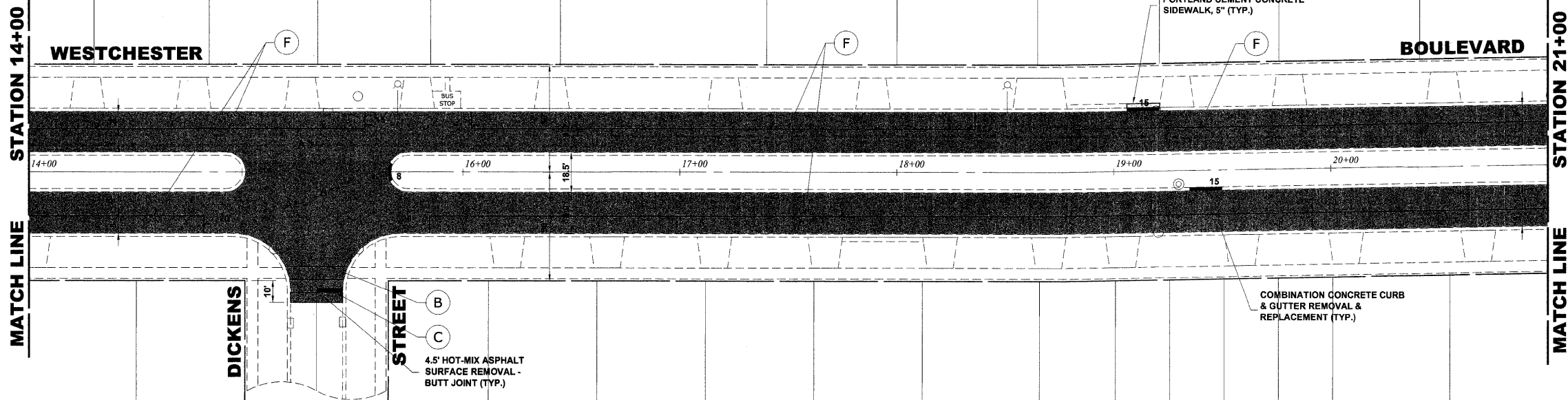
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVING PLAN

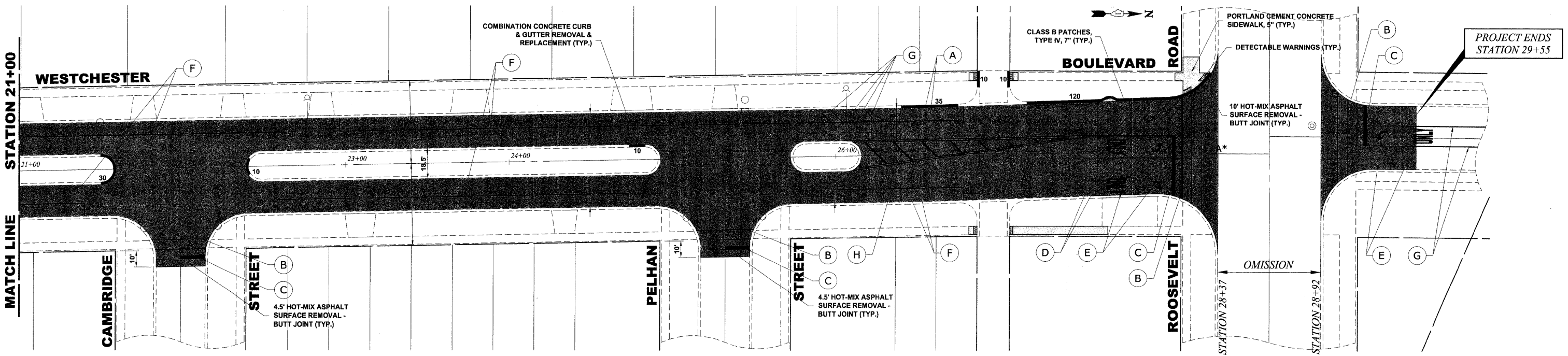
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FAU 2713	09-00074-00-RS	COOK	18	7
CONTRACT NO. 63257			ILLINOIS FED. AID PROJECT ARA-9003(324)	

E.H.E. PROJECT NO. 838-09-10201



PAVEMENT MARKING LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARALLEL PARKING LINE, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARKING LINE, YELLOW	(G)
THERMOPLASTIC PAVEMENT MARKING LINE 12", MEDIAN DIAGONALS @ 45°, YELLOW	(H)



Drawing file: W:\Projects\3309102 - Westchester Blvd Resurfacing\WestchesterBld.dwg Jul 14, 2009 - 9:54am

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

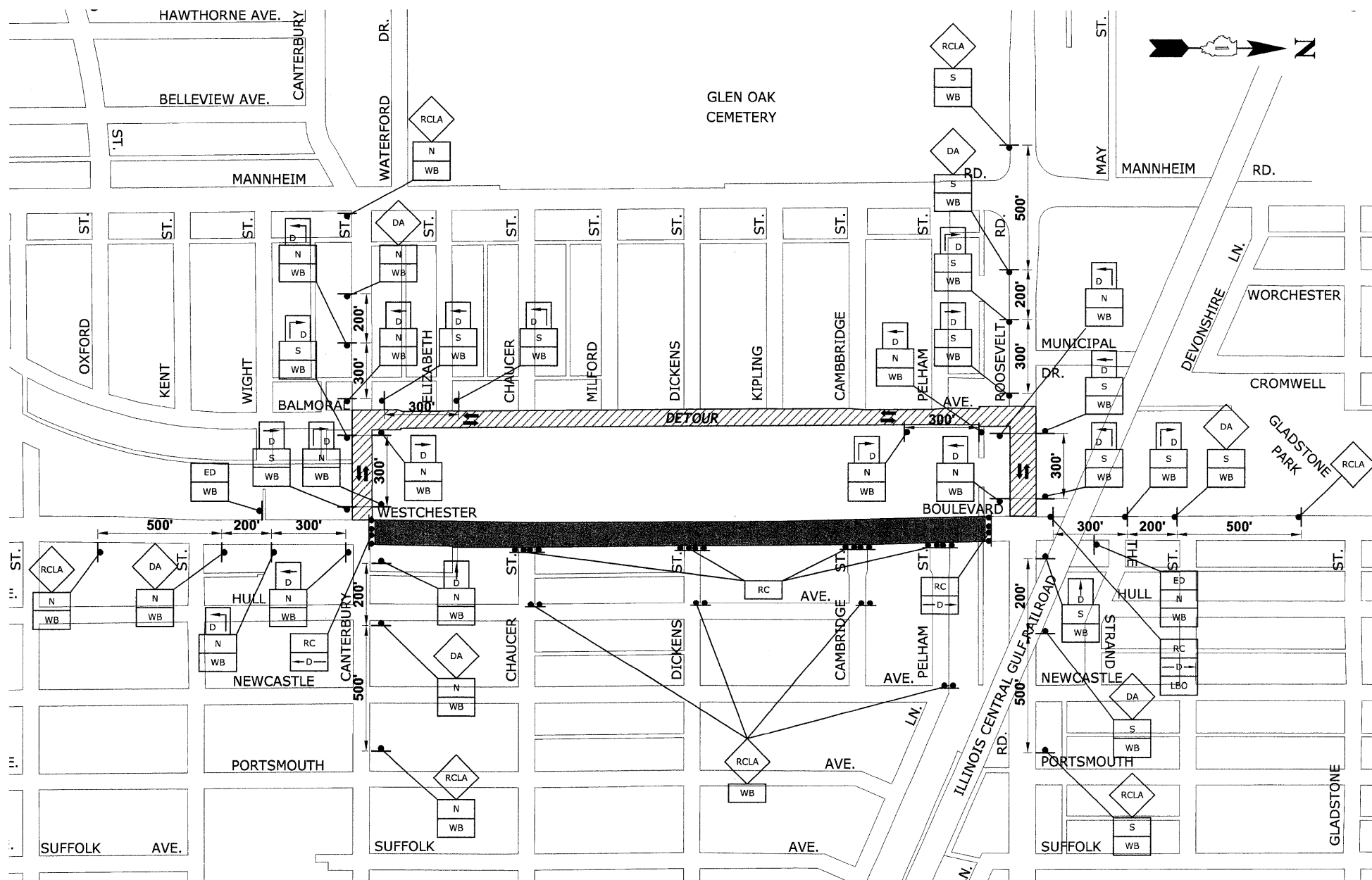
9533 Roosevelt Road
 Westchester, Illinois 60591-2709
 Phone: 708.962.4100
 Fax: 708.962.4232

USER NAME	DESIGNED TG	REVISED
PLOT SCALE	DRAWN TG	REVISED
PLOT DATE	CHECKED JG	REVISED
	DATE 07/14/09	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVING PLAN			
SCALE: 1"=30"	SHEET NO. OF SHEETS	STA. TO STA.	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63257	
			ARA-9003(324)	
E.H.E. PROJECT NO. 838-09-10201				



LEGEND OF SYMBOLS					
SYMBOL	DESCRIPTION	CODE & SIZE	SYMBOL	DESCRIPTION	CODE & SIZE
	ROAD CLOSED AHEAD	W20-3 48"x48"		END DETOUR	M4-8 24"x18"
	DETOUR AHEAD	W20-2 48"x48"		LOCAL BUSINESSES OPEN	M4-8 36"x24"
		W1-2 24"x24"		DETOUR	M4-9 30"x24"
		W1-2 24"x24"		DETOUR	M4-9 30"x24"
		W1-2 24"x24"		DETOUR	M4-9R 24"x30"
		R8-3 24"x24"		DETOUR	M4-9L 24"x30"
	ROAD CLOSED	R11-4 60"x30"		DETOUR	M4-10 48"x18"
	ROAD CLOSED TO THRU TRAFFIC	R11-4 60"x30"		DETOUR	M4-10 48"x18"
	NORTH	M3-1 24"x12"		NO PARKING	R8-3 12"x18" R8-3a 12"x6"
	SOUTH	M3-1 24"x12"		NO PARKING	R8-3 12"x18" R8-3a 12"x6"
	WESTCHESTER BLVD	M4-8 36"x12"			

NOTES:

1. ANY LOCAL DETOURS SHALL BE APPROVED BY THE VILLAGE AT LEAST 48 HOURS PRIOR TO INSTALLATION.
2. CONTRACTOR TO NOTIFY IDOT ARTERIAL TRAFFIC CONTROL SUPERVISOR (847)705-4470, SEVENTY-TWO (72) HOURS IN ADVANCE OF SETTING UP DETOUR ROUTE.

Drawing File: W:\Projects\33509102 - Westchester Blvd Resurfacing\Cover-WestchesterBld.dwg Jul 14, 2009 - 9:55am



1913 Riverside Road
Westchester, Illinois 60591-2700
Phone: 708-665-8300
Fax: 708-665-5132

USER NAME	DESIGNED	TG	REVISED
	DRAWN	TG	REVISED
PLOT SCALE	CHECKED	JG	REVISED
PLOT DATE	DATE	07/14/09	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR FOR THROUGH TRAFFIC

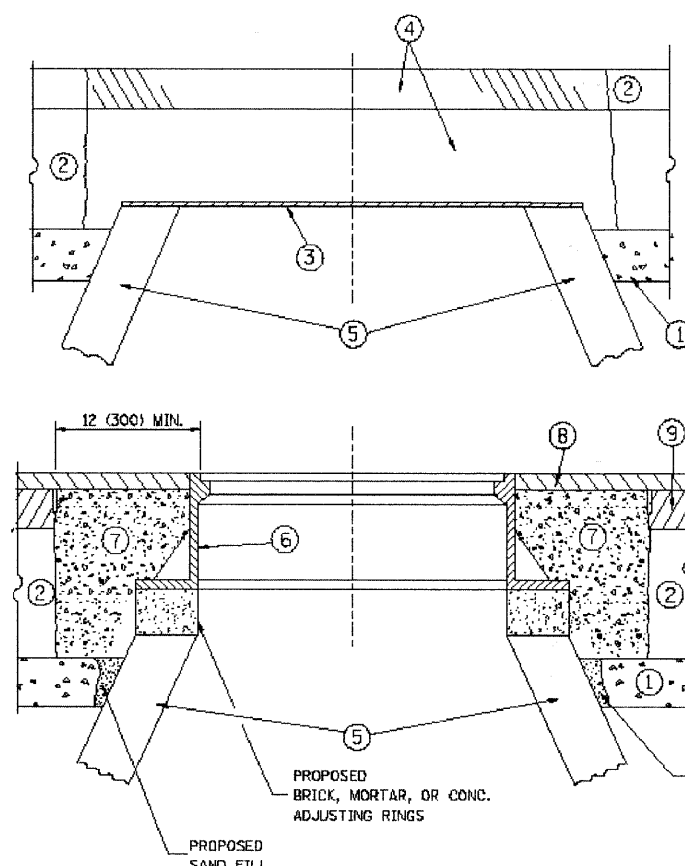
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ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2713	09-00074-00-RS	COOK	18	10
CONTRACT NO. 63257			PROJECT AR-9003(324)	

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT AR-9003(324)

E.H.E. PROJECT NO. 838-09-10201

Drawing file: W:\Projects\83809102 - Westchester Blvd Resurfacing\Drawings\Westchester\bd08.dgn Jul 14, 2009 - 10:18am



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- | | |
|--|--|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gogierobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
W:\data\td\22x34\bd08.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 / IN	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2713	09-00074-00-RS	COOK	18	11
8000-01 (BD-8)		CONTRACT NO. 63257		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 838-09-10201

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = geglertobt

DESIGNED - A. HOUSEH

REVISED - R. SHAH 10-03-96

DRAWN -

REVISED - A. ABBAS 03-21-97

PLOT SCALE = 66.000 1/4" IN.

REVISOR - M. GOMEZ 01-22-01

PLOT DATE = 1/4/2008

REVISOR - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE

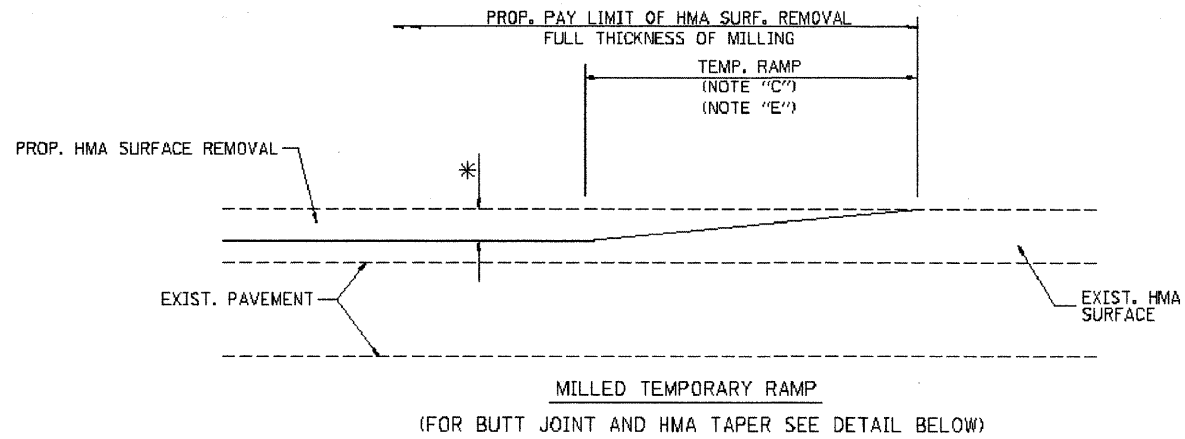
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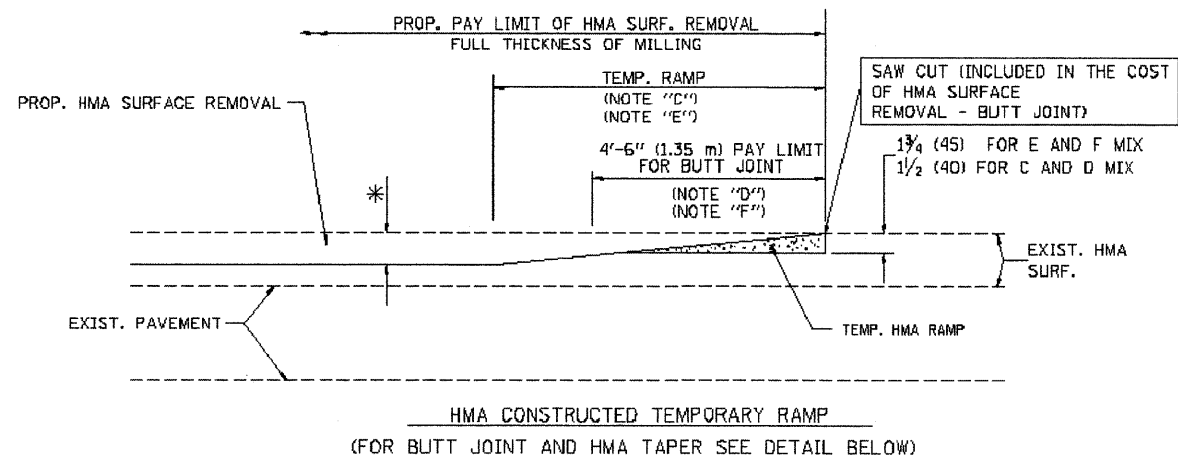
TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD00-06 (BD-24)			CONTRACT NO. 63257	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

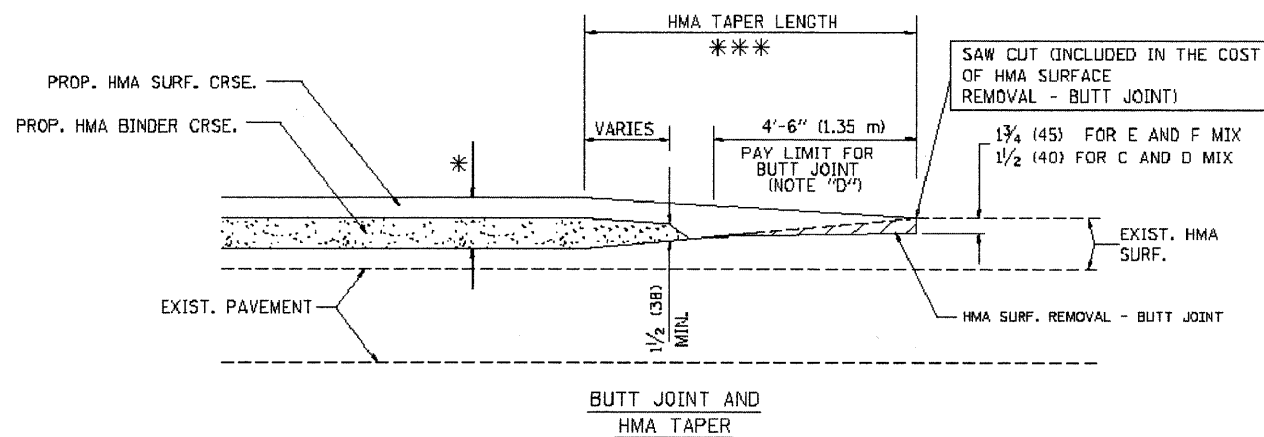
E.H.E. PROJECT NO. 838-09-10201



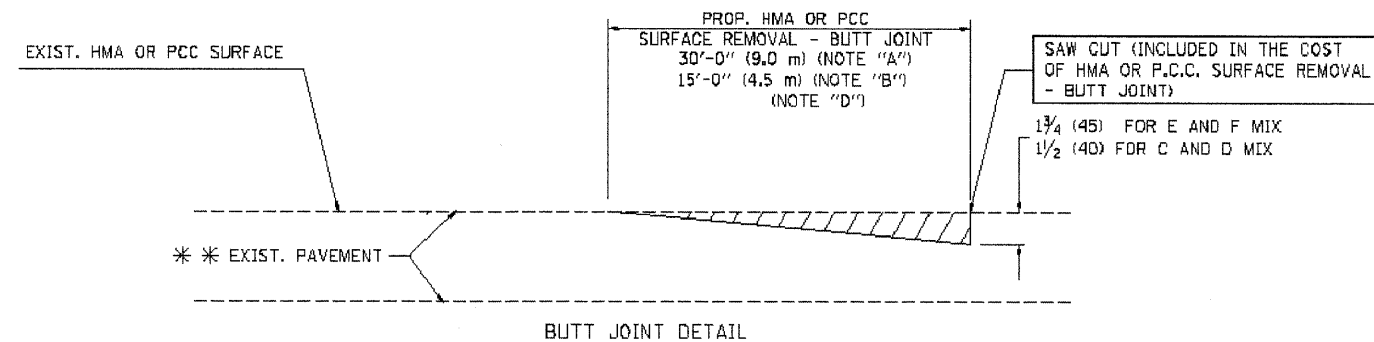
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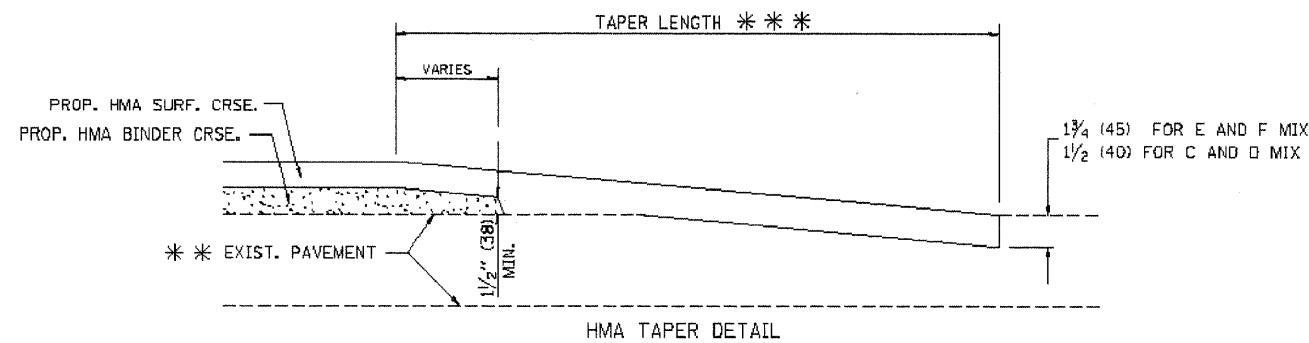
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Drawing file: W:\Projects\62809102 - Westchester Blvd Resurfacing\Workings-WestchesterBvd.dwg Jul 14, 2009 - 10:20am

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USER NAME = gajlambert	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / IN	CHECKED -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R. BORO 01-01-07

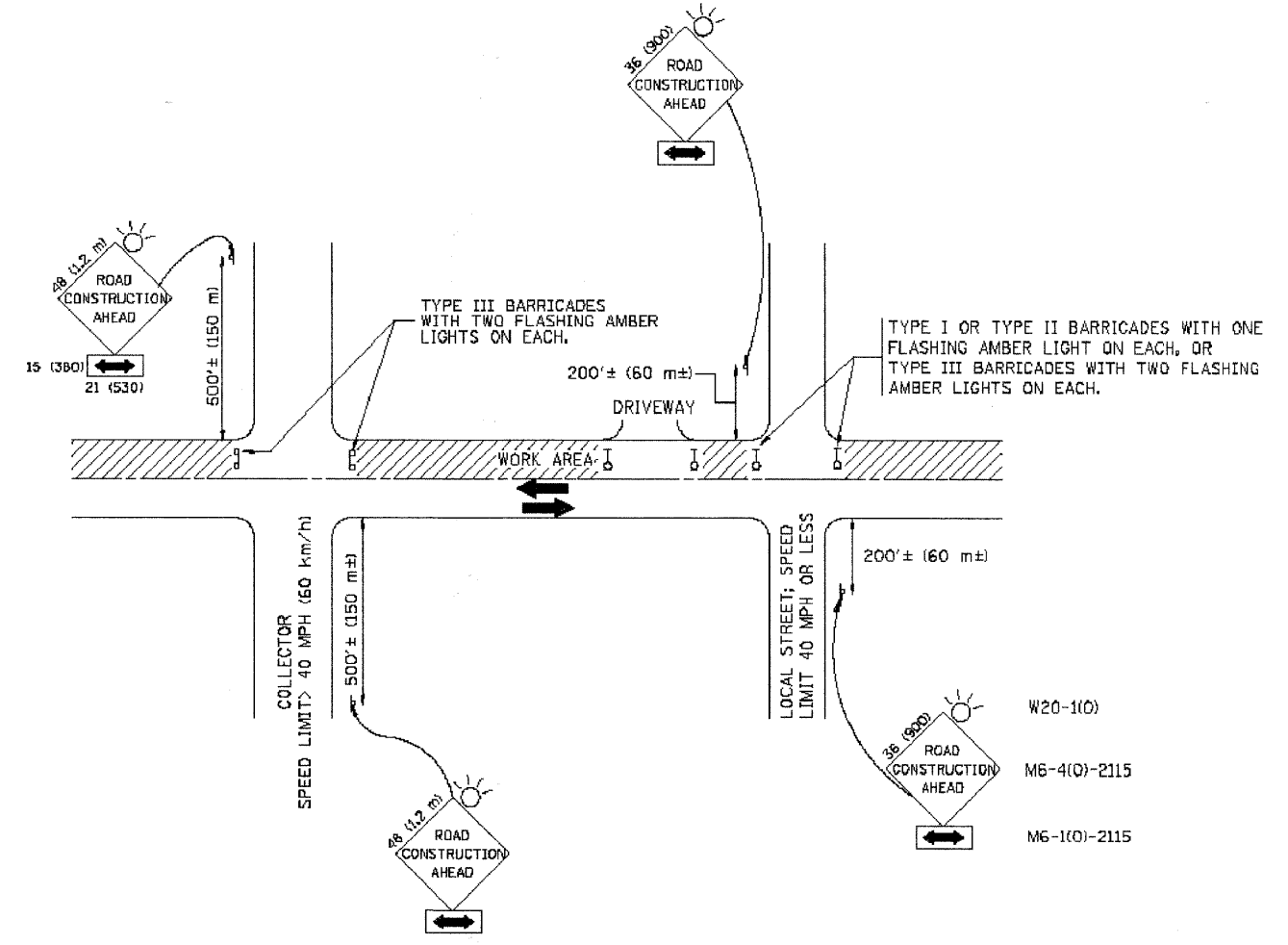
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2713	09-0074-00-RS	COOK	18	13
RD400-05 RD32			CONTRACT NO. 63257	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 838-09-10201

Drawing file: W:\Projects\83809102 - Westchester Blvd Resurfacing\Markings-WestchesterBvd.dgn Jul 14, 2009 - 10:21am



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701605 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

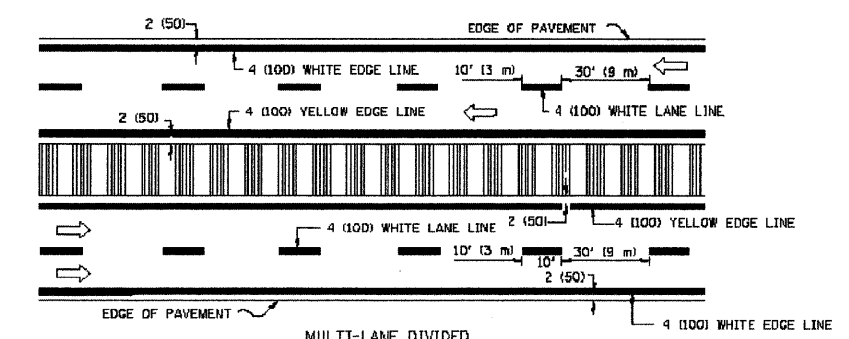
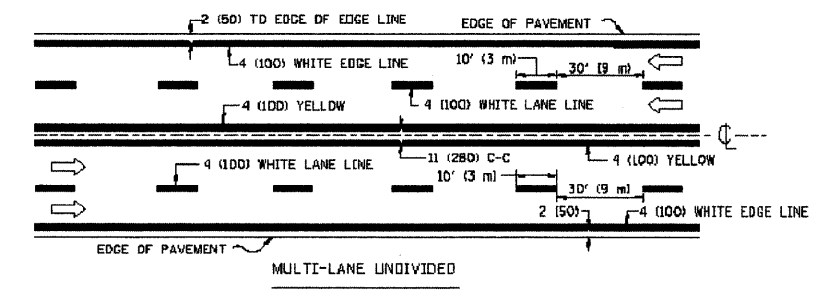
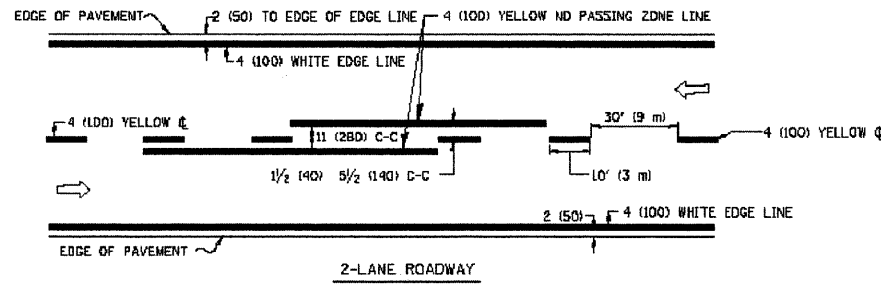
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

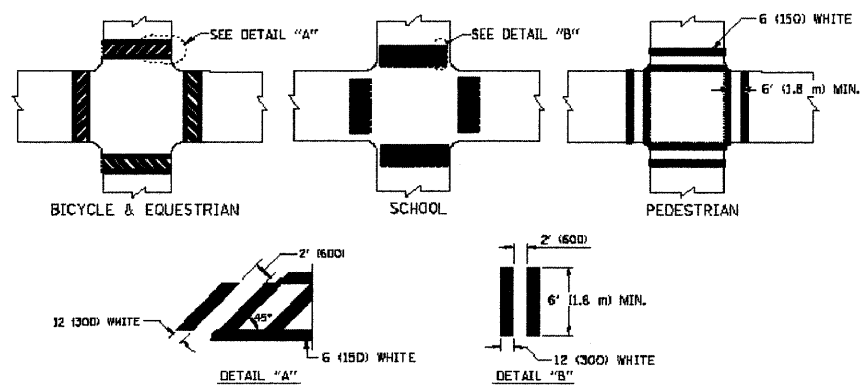
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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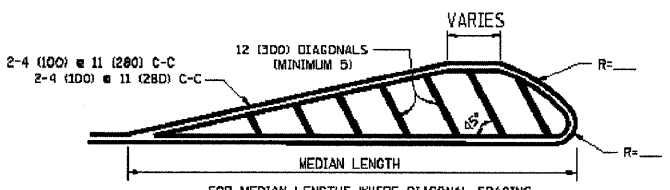
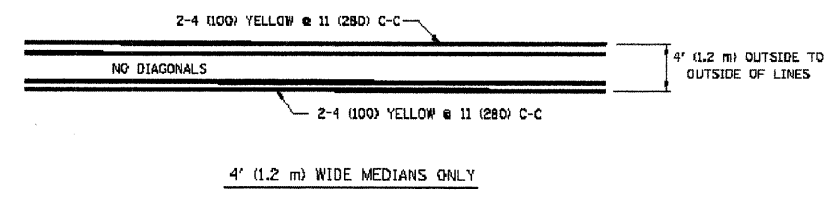
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TC-10			CONTRACT NO. 63257	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



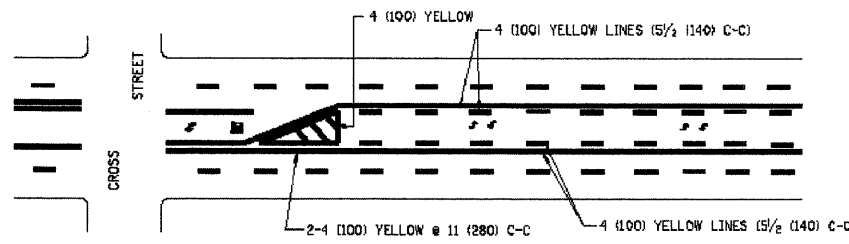
TYPICAL LANE AND EDGE LINE MARKING



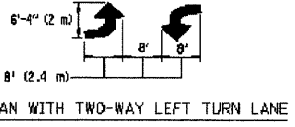
TYPICAL CROSSWALK MARKING



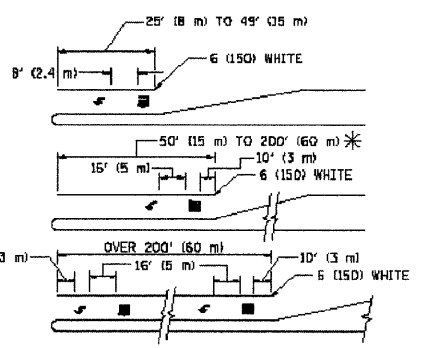
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPADED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

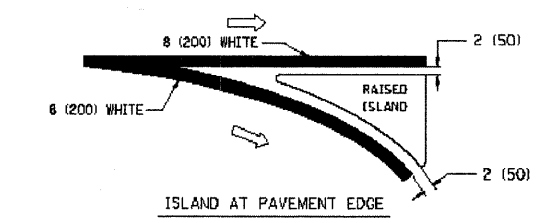
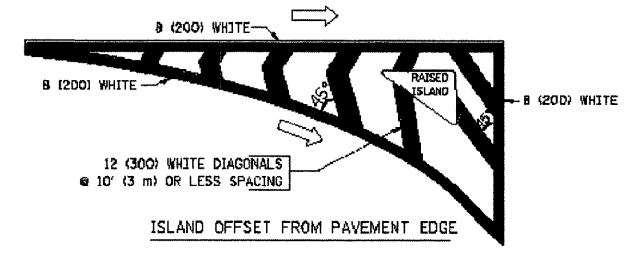


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 6" (1.5 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	30' (9 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (6" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
DORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78000 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000.

All dimensions are in inches (millimeters) unless otherwise shown.

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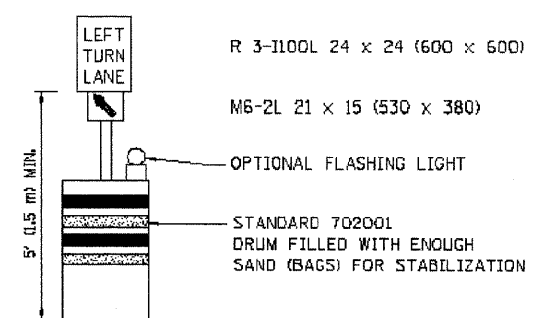
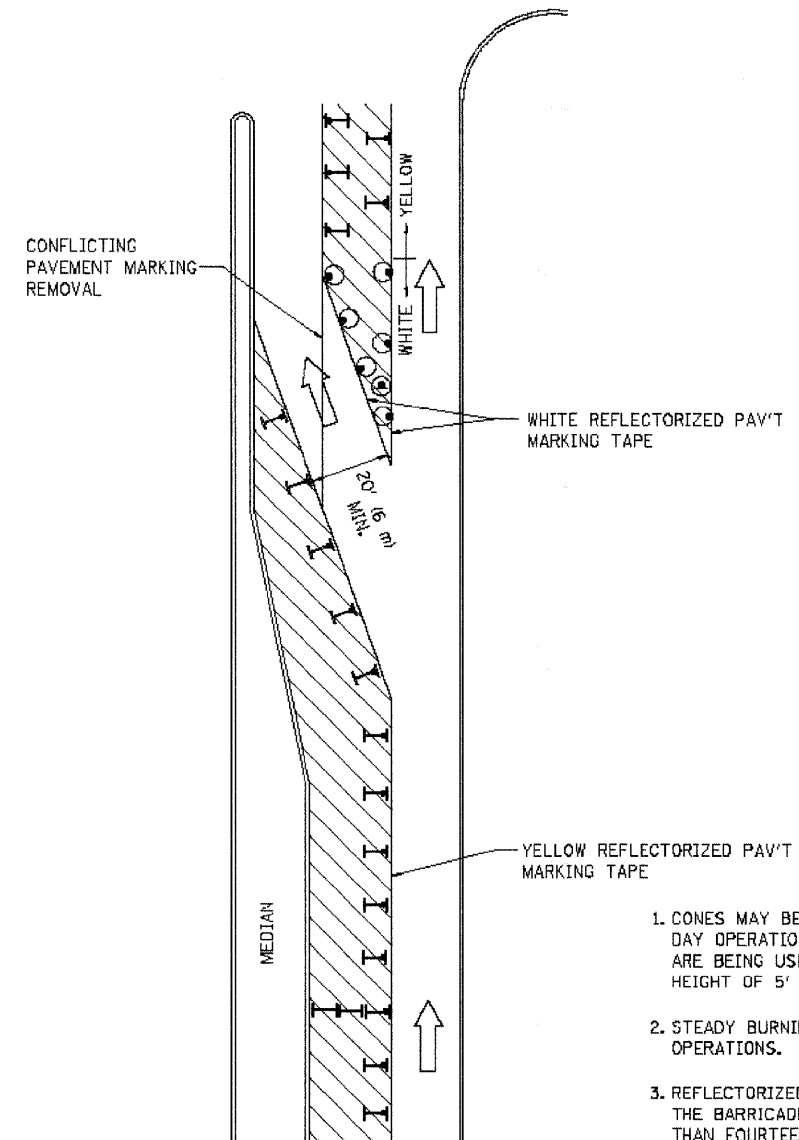
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2713	09-0074-00-RS	COOK	18	15
TC-18		CONTRACT NO. 63257		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

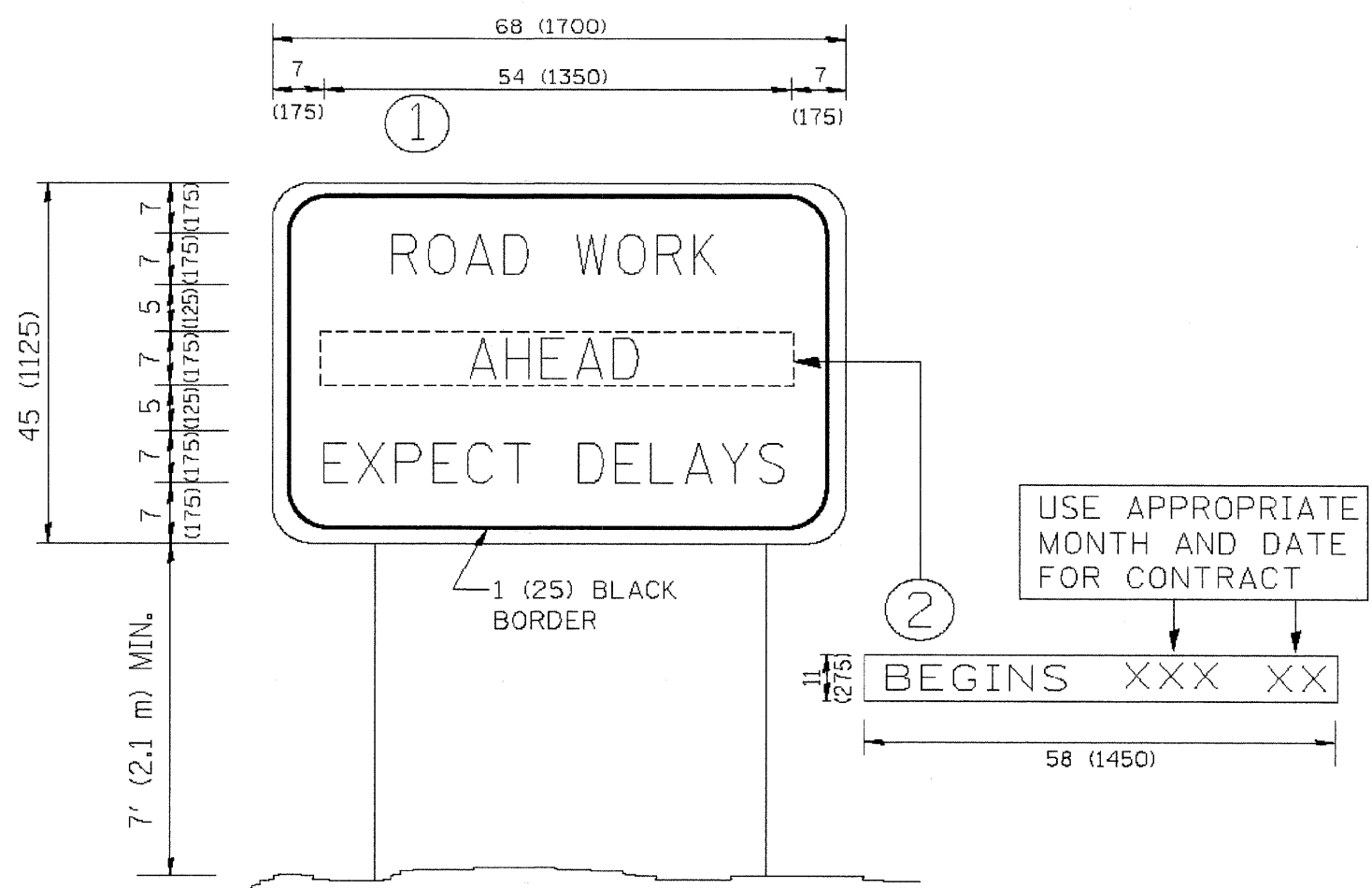
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2713	09-00074-00-RS	COOK	18	16
TC-14			CONTRACT NO. 63257	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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PLOT DATE = 1/4/2008		DATE -	REVISED -T. RAMMACHER 01-06-00

Drawing file: W:\Projects\8309102 - Westchester Blvd Resurfacing\Markings-WestchesterBvd.dwg Jul 14, 2009 - 10:22am



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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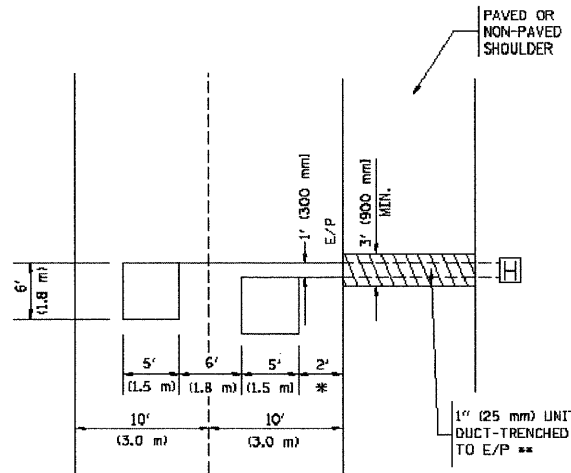
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2713	SECTION 09-0074-00-RS	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 17
TC-22			CONTRACT NO. 63257	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

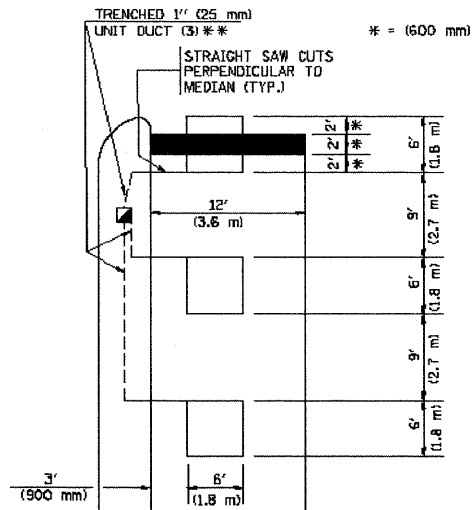


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

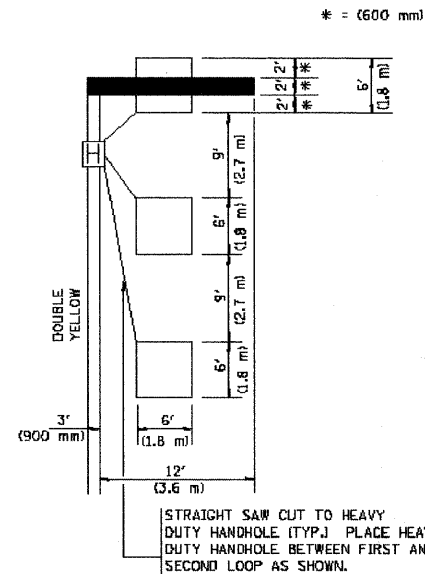
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
B1400L TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

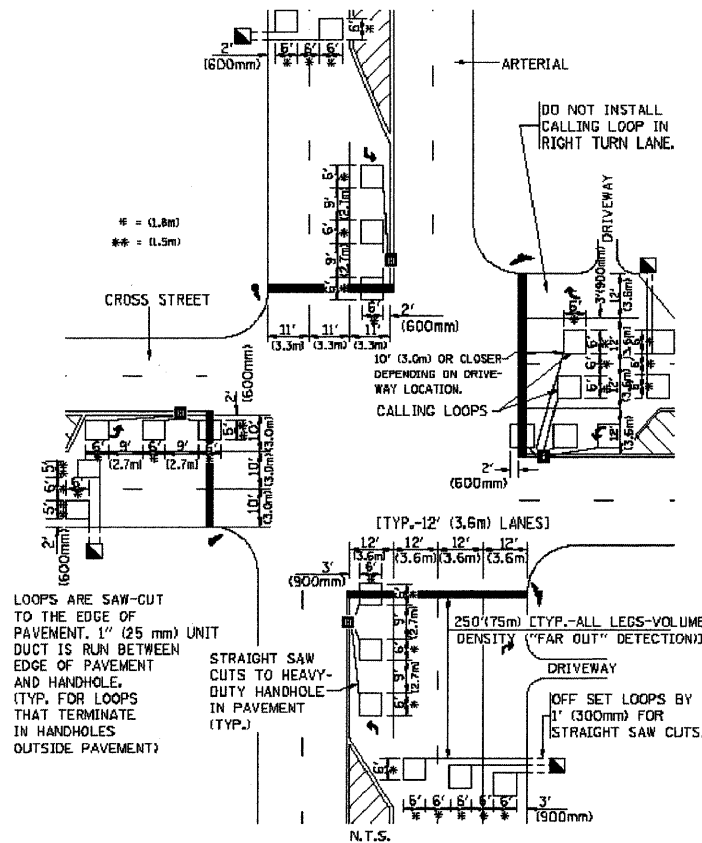
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



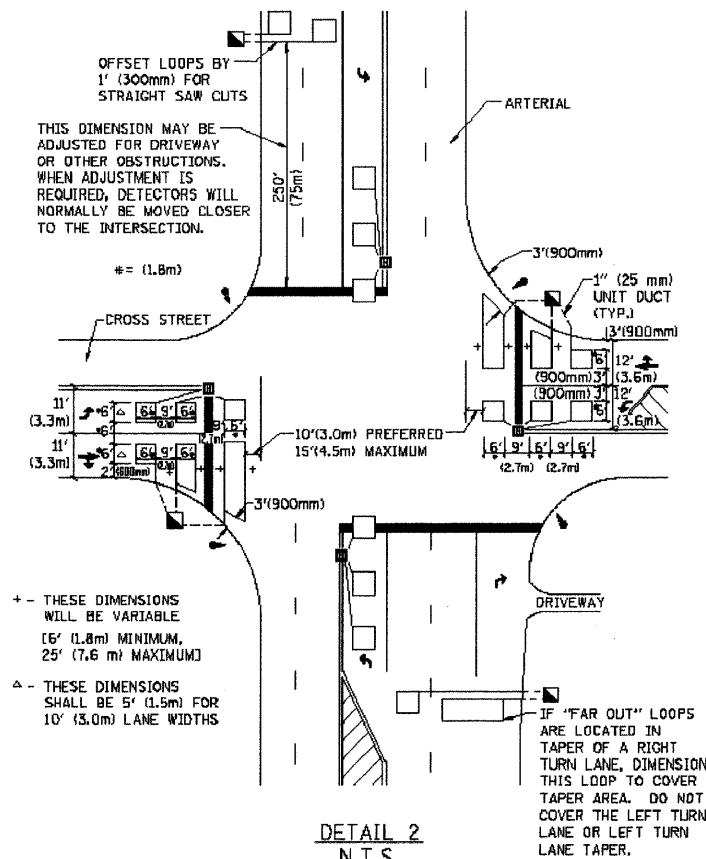
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSS STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

Drawing file: W:\Projects\31809102 - Westchester Blvd Resurfacing\Markings-WestchesterBvd.dwg Jun 14, 2009 - 10:23am

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PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2713	08-00074-00-RS	COOK	18	18
TS-07		CONTRACT NO. 63257		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 838-09-10201