

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	1
		ILLINOIS	CONTRACT NO. 62L13	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN  
VILLAGE OF WAUCONDA

PROPOSED  
HIGHWAY PLANS

ROUTE FAU 234: BARRINGTON ROAD  
US 12 (RAND ROAD) TO ILL 176 (LIBERTY STREET)

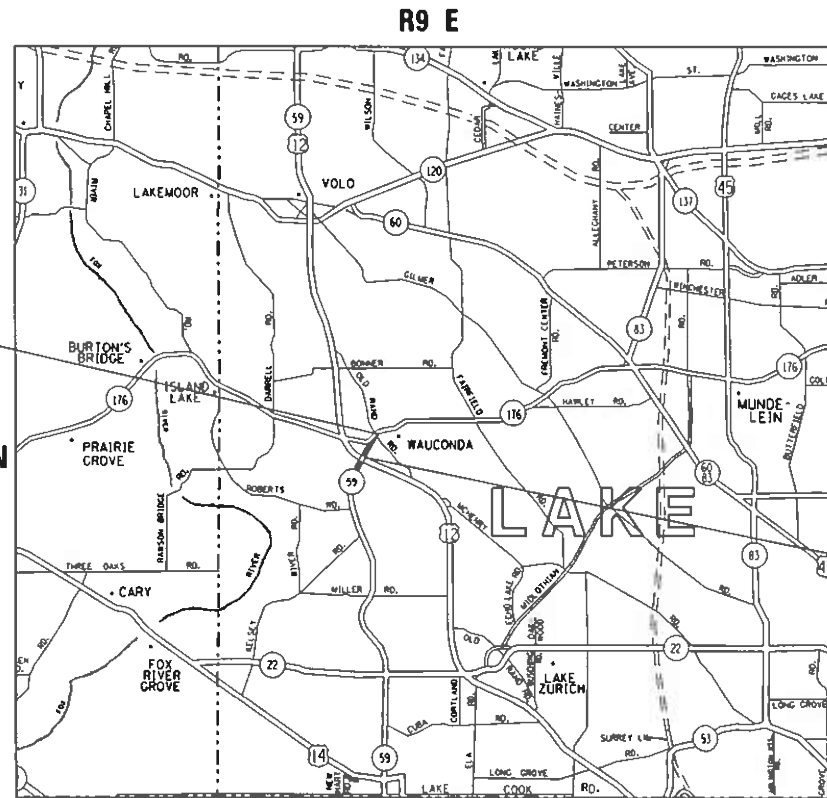
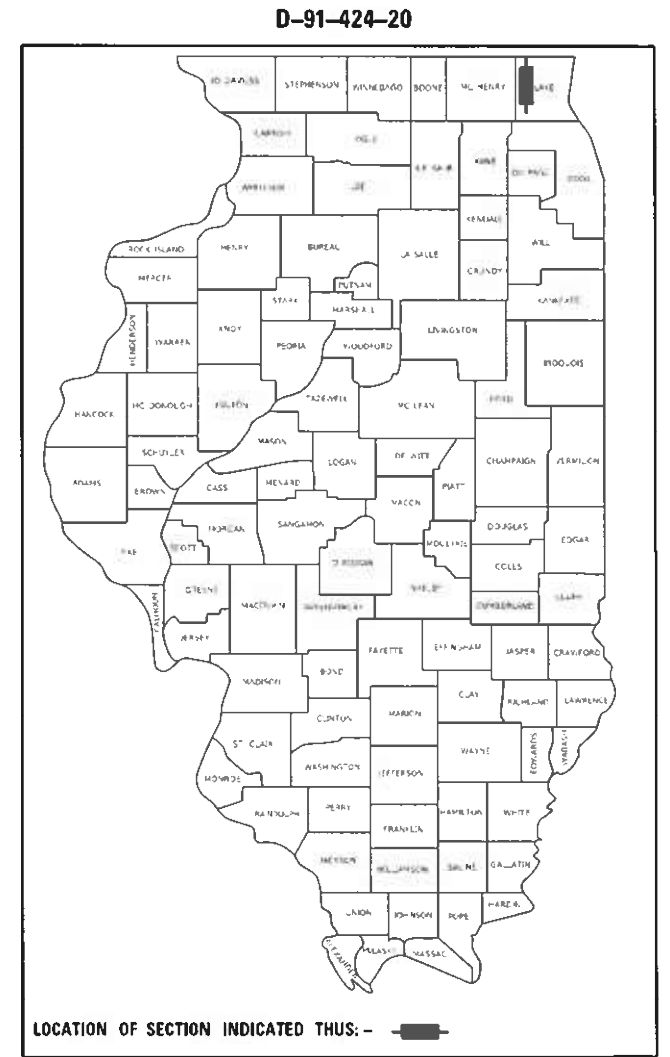
SECTION: 2020-041-RS&SW  
PROJECT: STP-RQRN(023)

STANDARD OVERLAY, ADA IMPROVEMENTS  
LAKE COUNTY

C-91-231-20

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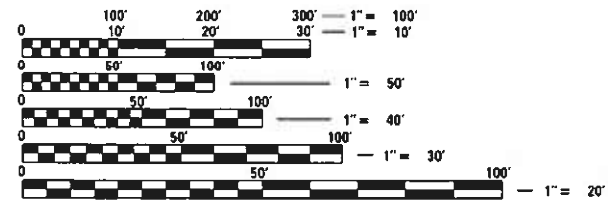
PROJECT BEGINS TO IL 176 (LIBERTY ST.)  
ADT (2019) = 4950  
SPEED LIMIT = 35 MPH



LOCATION MAP  
(NOT TO SCALE)

PROJECT ENDS  
STA. 43 + 15

PROJECT BEGINS  
STA. 17 + 14



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

GROSS & NET LENGTH = 2,600 FT. = 0.5 MILE

CONTRACT NO. 62L13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED June 18, 2020

*Anthony J. Quinlan*  
REGIONAL ENGINEER

August 14, 2020

*Scott A. Elk*  
ENGINEER OF DESIGN AND ENVIRONMENT

August 14, 2020

*James J. Quinlan*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

**STATE STANDARDS**

**GENERAL NOTES**

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	COVER SHEET	000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
3-4	SUMMARY OF QUANTITIES	424021-04	DEPRESSED CORNER FOR SIDEWALKS
5-6	TYPICAL SECTIONS	442201-03	CLASS C AND D PATCHES
7-8	ROADWAY AND PAVEMENT MARKING PLANS	482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
9	BD-01: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB OR EDGE OF SHOULDER GREATER THAN OR EQUAL TO 15' (4.5 M)	701101-05	OffRdOp-MultiLn-LessThan15ftToEOP
10	BD-02: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB IS LESS THAN 15' (4.5 M)	701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
11	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
12	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
13	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
14	BD-32: BUTT JOINT AND HMA TAPER DETAILS	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
15	TC-8: ENTRANCE_AND_EXIT_RAMP CLOSURE_DETAILS	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
16	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
17	TC-11: TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	701901-08	TRAFFIC CONTROL DEVICES
18	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS	814001-03	HANDHOLES
19	TC-14: CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)		
20	TC-16: PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		
21	TC-17: TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		
22	TC-22: ARTERIAL ROAD INFORMATION SIGN		
23	TC-26: DRIVEWAY ENTRANCE SIGNING		
24	PD-01A: SINGLE PERPENDICULAR CURB RAMPS		
25	PD-06A: PARALLEL CURB RAMPS		

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGE OF WAUCONDA.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT WATER CZARNY, AREA TRAFFIC FIELD ENGINEER, VIA E-MAIL AT WALTER.CZARNY@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BARRINGTON RD (US 12 (RAND RD) TO ILL 176 (LIBERTY ST)  
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-R5&SW	LAKE	25	2
				CONTRACT NO. 62L13
		ILLINOIS	FED. AID PROJECT	

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% ST	0005 100% STATE					CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% ST	0005 100% STATE				
* 20100110	TREE REMOVAL ( 6 TO 15 UNITS DIAMETER)	UNIT	15	15						44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	10610	10610					
20200100	EARTH EXCAVATION	CU YD	6	6						44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	30	30					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	13	13						44000600	SIDEWALK REMOVAL	SO FT	150	150					
* 25200110	SODDING, SALT TOLERANT	SO YD	13	13						44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	205	205					
25200200	SUPPLEMENTAL WATERING	UNIT	1	1						44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	40	40					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	30	30						44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	220	220					
40600290	BITUMINOUS MATERIALS ( TACK COAT)	POUND	7200	7200						48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	40	40					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	16	16						60250200	CATCH BASINS TO BE ADJUSTED	EACH	3	3					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	102	102					* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	6	6						
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	460	460					* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2						
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1045	1045					* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1						
42001300	PROTECTIVE COAT	SO YD	82	82					* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1						
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	150	150					* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2						
42400800	DETECTABLE WARNINGS	SO FT	34	34					67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						
									67100100	MOBILIZATION	L SUM	1	1						

REV-SEP  
 \* = SPECIALTY ITEMS  
 △ = NON-PARTICIPATING WORK ( 100% STATE)

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% ST	0005 100% STATE			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	645	645				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	215	215				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5030	5030				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	260	260				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	100	100				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	25	25				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	5670	5670				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5030	5030				
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	260	260				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	25	25				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	70	70				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	70	70				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% ST	0005 100% STATE			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	11	11				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	50		50			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	5415	5415				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	220	220				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	11		11			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52				
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	2700	2700				
* Z0064800	SELECTIVE CLEARING	UNIT	20	20				

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

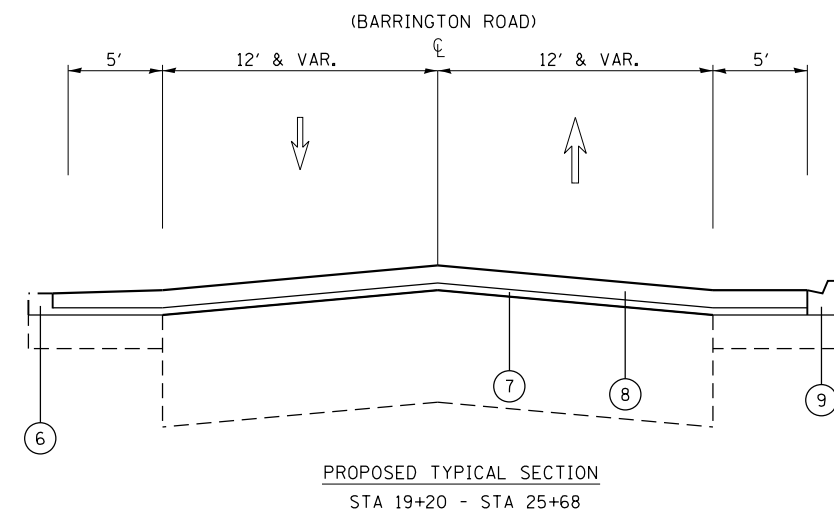
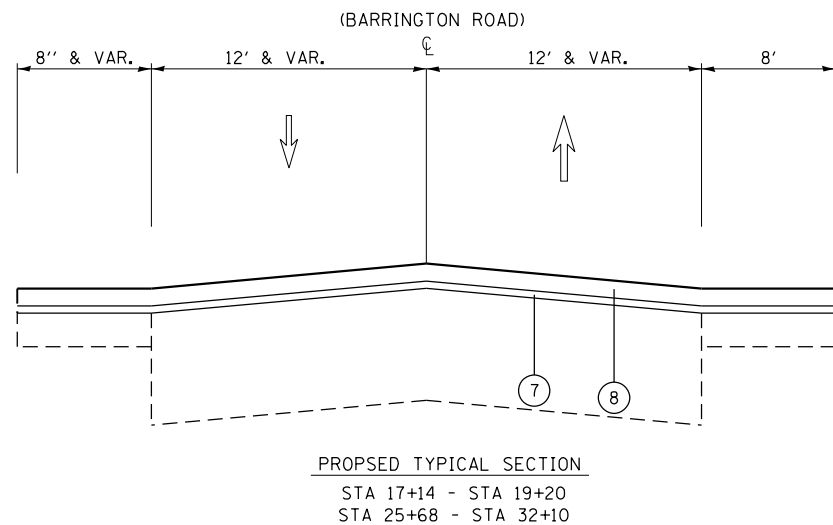
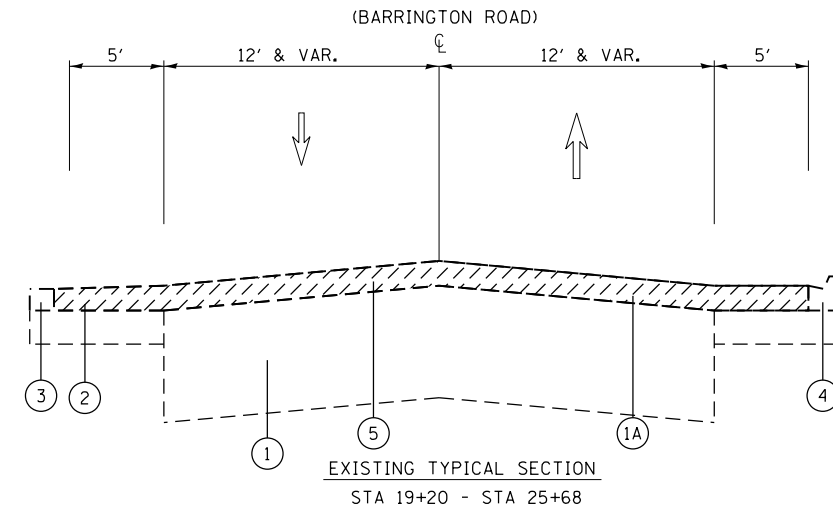
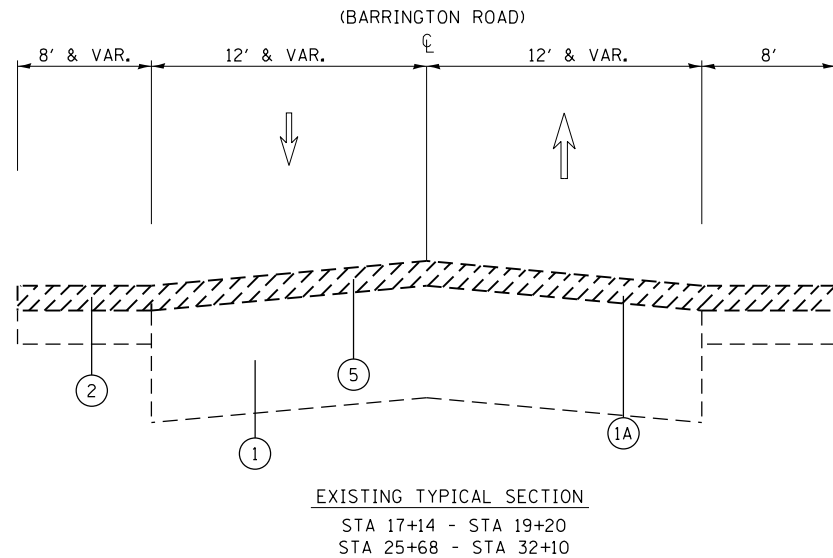
**BARRINGTON RD (US 12 (RAND RD) TO ILL 176 (LIBERTY ST))  
SUMMARY OF QUANTITIES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-R5&SW	LAKE	25	4
CONTRACT NO. 62L13				
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

REV-SEP  
\* = SPECIALTY ITEMS  
△ = NON-PARTICIPATING WORK (100% STATE)

**LEGEND**

- 1. EXISTING PCC BASE COURSE, (+/-) 8.5"
- 1A. EXISTING HMA, 2 1/2" (+/-)
- 2. EXISTING HMA SHOULDER
- 3. EXISTING AGGREGATE SHOULDER
- 4. EXISTING CONCRETE CURB AND GUTTER
- 5. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2"
- 6. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE 8
- 7. PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- 8. PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 3/4"
- 9. PROPOSED COMB. CONC. CURB & GUTTER REMOVAL & REPLACEMENT



HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (OMP)
MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
<b>PAVEMENT</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 1 3/4"	4% @ 50 GYR.	QC/OA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 3/4" -1"	3.5% @ 50 GYR.	QC/OA
<b>DRIVEWAYS</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 2"	4% @ 50 GYR.	QC/OA
HOT-MIX ASPHALT BASE COURSE, 6"	4% @ 50 GYR.	QC/OA
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYR.	QC/OA
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

**NOTE**

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- NOTE 3: "QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE"
- NOTE 4: THE CONTRACTOR SHALL PATCH AFTER MILLING
- NOTE 5: "THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BC IL-4.75 N50"

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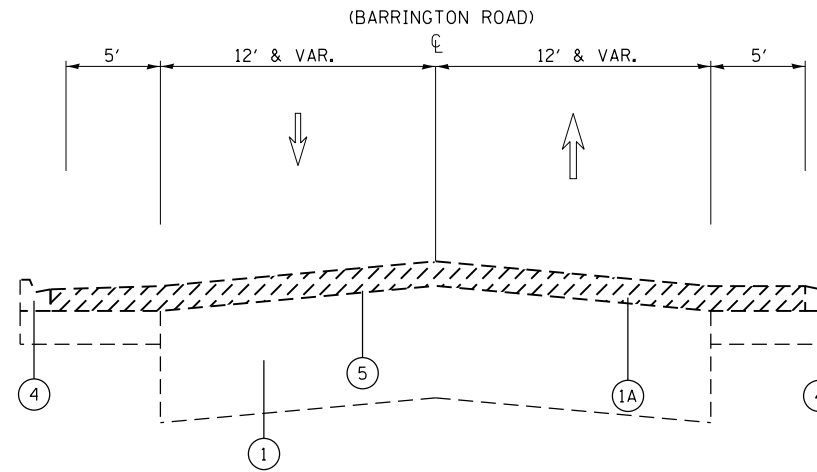
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

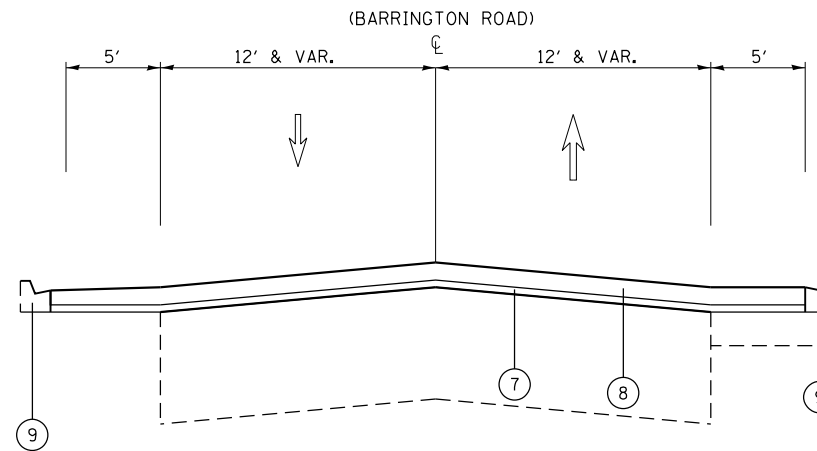
<b>EXISTING TYPICAL SECTION</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>BARRINGTON RD (US 12 (RAND RD) TO ILL 176 (LIBERTY ST))</b>		234	2020-041-R5&SW	LAKE	25	5
SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 62L13				
		ILLINOIS FED. AID PROJECT				

LEGEND

- |   |   |
|---|---|
| <ul style="list-style-type: none"> <li>1. EXISTING PCC BASE COURSE, (+/-) 8.5"</li> <li>1A. EXISTING HMA, 2 1/2" (+/-)</li> <li>2. EXISTING HMA SHOULDER</li> <li>3. EXISTING AGGREGATE SHOULDER</li> <li>4. EXISTING CONCRETE CURB AND GUTTER</li> <li>5. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2"</li> <li>6. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE 8</li> </ul> | <ul style="list-style-type: none"> <li>7. PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"</li> <li>8. PROPOSED HOT-MIX ASPALT SURFACE COURSE, MIX "D", N50, 1 3/4"</li> <li>9. PROPOSED COMB. CONC. CURB &amp; GUTTER REMOVAL &amp; REPLACEMENT</li> </ul> |
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EXISTING TYPICAL SECTION  
STA 32+10 - STA 43+15



PROPOSED TYPICAL SECTION  
STA 32+10 - STA 43+15

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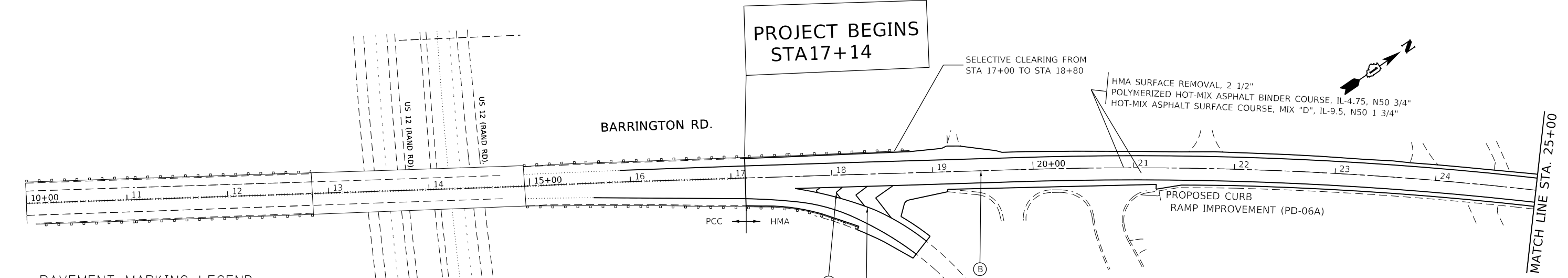
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING TYPICAL SECTION  
BARRINGTON RD (US 12 (RAND RD) TO ILL 176 (LIBERTY ST))**

SCALE: SHEET OF SHEETS STA. TO STA.

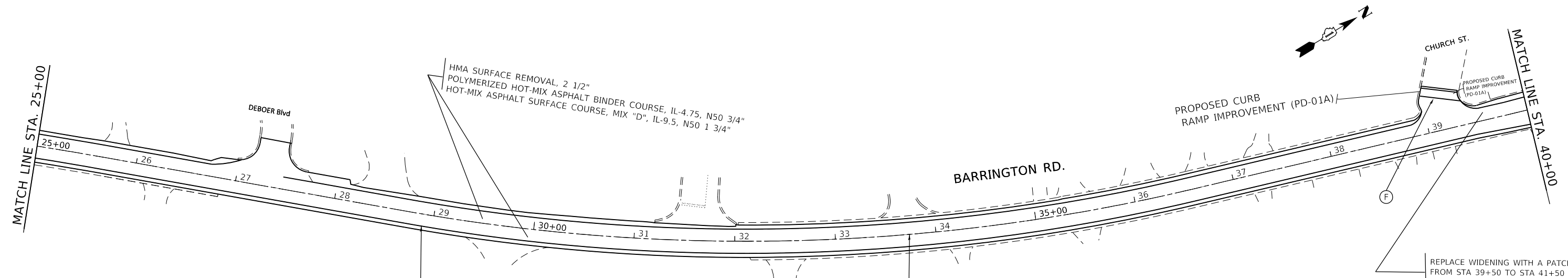
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-R5&SW	LAKE	25	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L13	



- PAVEMENT MARKING LEGEND**
- (A) • DOUBLE LINES 4", YELLOW (TYP.)
  - (B) • LINE 4", 10' DASH-30'SKIP, YELLOW (TYP.)
  - (C) • LINE 4", WHITE (TYP.)
  - (D) • LINE 8", WHITE (TYP.)
  - (E) • LINE 12", WHITE (TYP.)
  - (F) • STOP LINE 24", WHITE (TYP.)

**NOTES:**

- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.



- PAVEMENT MARKING LEGEND**
- (A) • DOUBLE LINES 4", YELLOW (TYP.)
  - (B) • LINE 4", 10' DASH-30'SKIP, YELLOW (TYP.)
  - (C) • LINE 4", WHITE (TYP.)
  - (D) • LINE 8", WHITE (TYP.)
  - (E) • LINE 12", WHITE (TYP.)
  - (F) • STOP LINE 24", WHITE (TYP.)

**NOTES:**

- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

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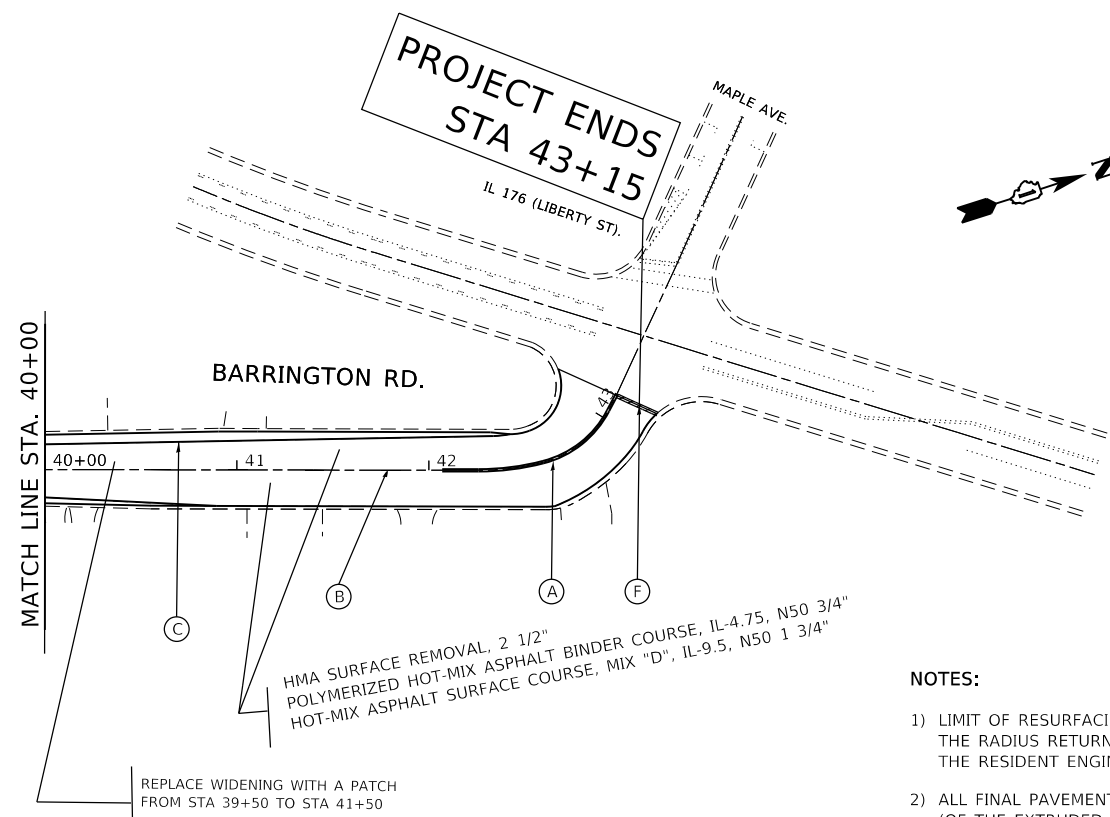
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
BARRINGTON RD. US 12 (RAND RD) TO ILL 176 (LIBERTY ST)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-R5&SW	LAKE	25	7
CONTRACT NO. 62L13				
ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- (A) • DOUBLE LINES 4", YELLOW (TYP.)
- (B) • LINE 4", 10' DASH-30'SKIP, YELLOW (TYP.)
- (C) • LINE 4", WHITE (TYP.)
- (D) • LINE 8", WHITE (TYP.)
- (E) • LINE 12", WHITE (TYP.)
- (F) • STOP LINE 24", WHITE (TYP.)

**NOTES:**

- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

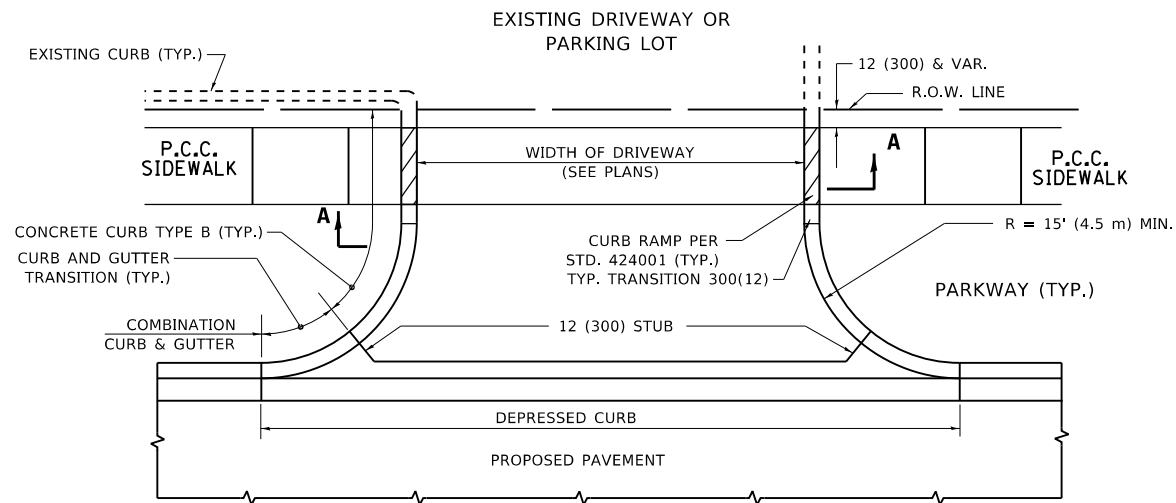
**ROADWAY PLAN  
BARRINGTON RD. US 12 (RAND RD) TO ILL 176 (LIBERTY ST)**

SCALE: SHEET OF SHEETS STA. TO STA.

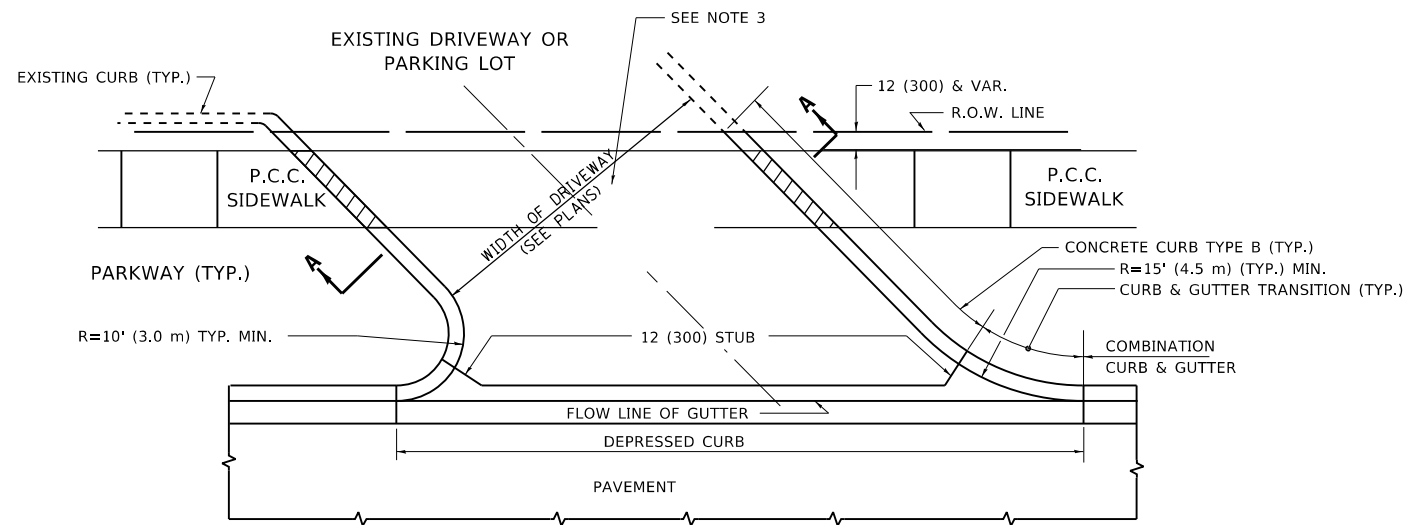
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234	2020-041-R5&SW	LAKE	25	8
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 62L13

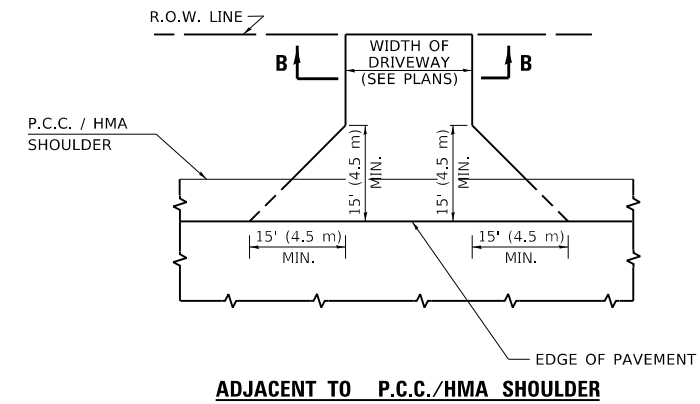
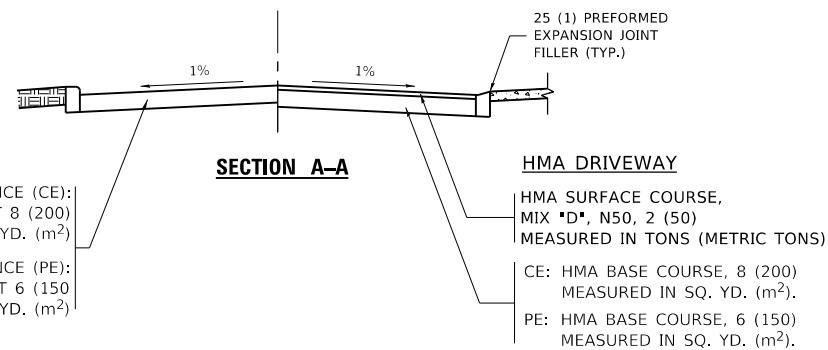




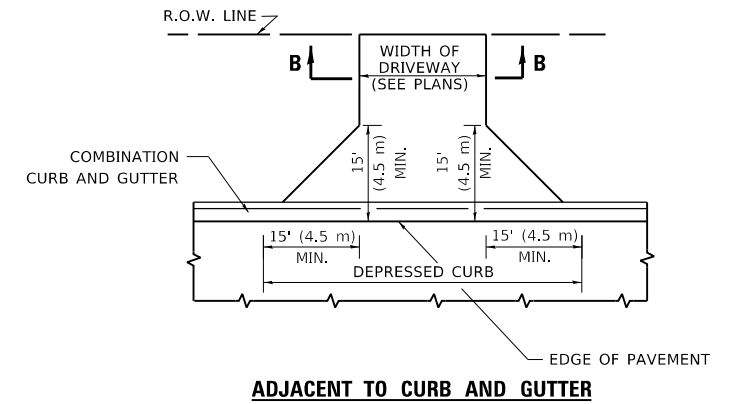
**WITH CONCRETE CURB, TYPE B**



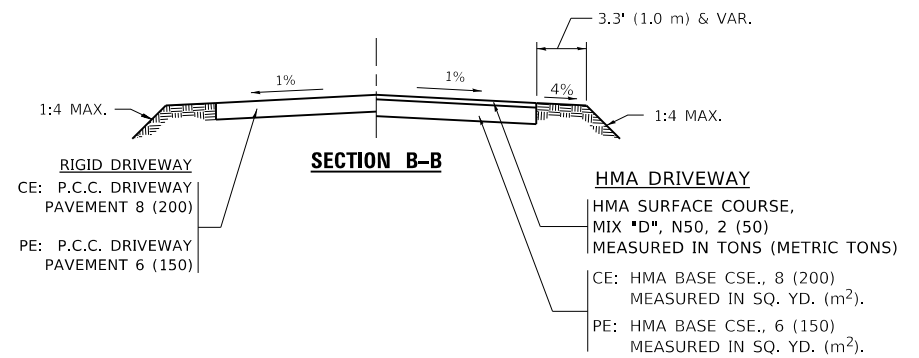
**WITH CONCRETE CURB, TYPE B**



**ADJACENT TO P.C.C./HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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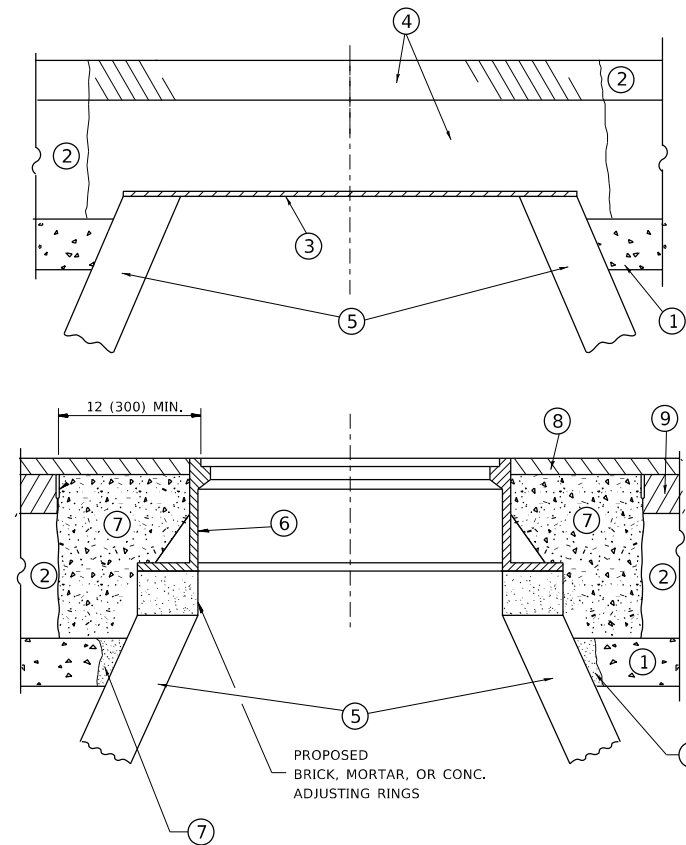
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PLOT DATE = 6/24/2020	DATE - 11-04-95	REVISED - R. BORO 06-11-08
		REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	9
BD400-01 (BD-01)			CONTRACT NO. 62L13	
ILLINOIS FED. AID PROJECT				





**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1 \*CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).\*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

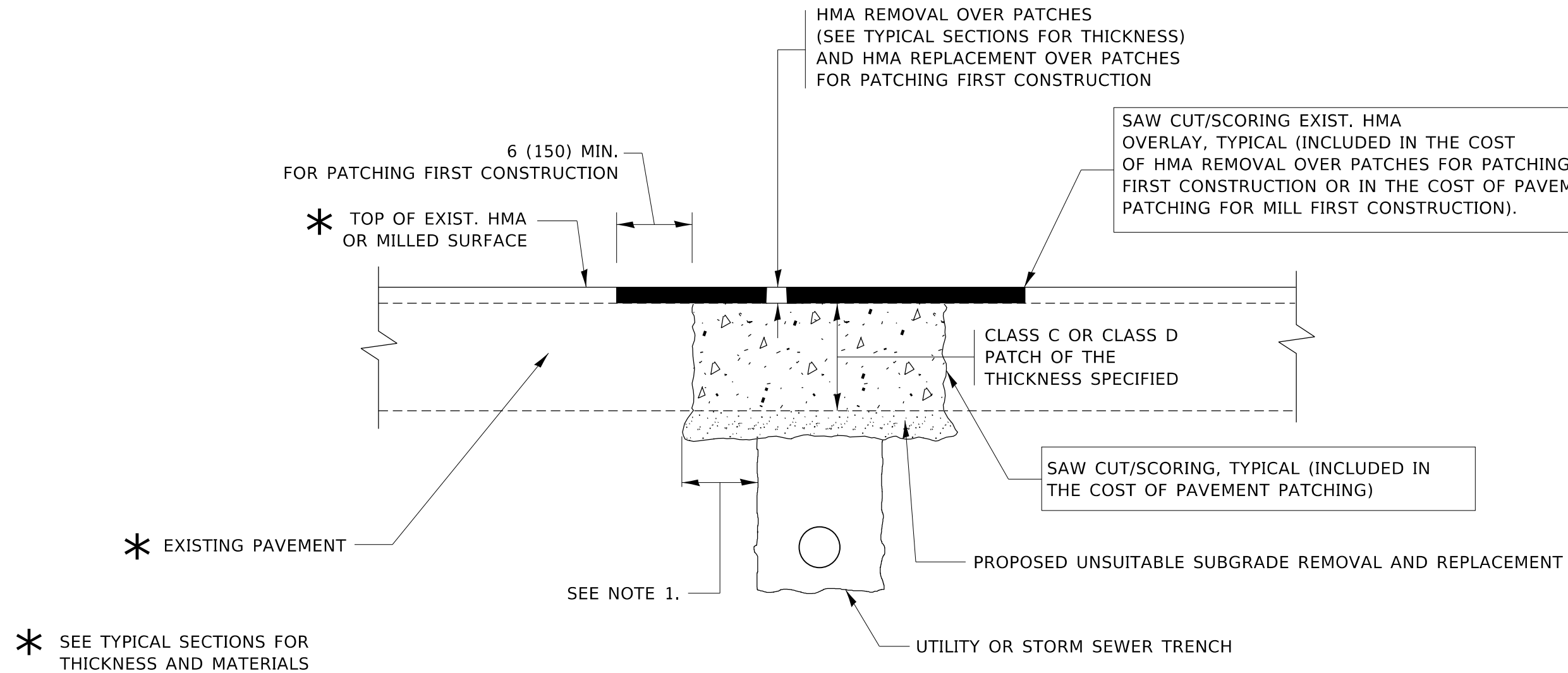
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	11
BD600-03 (BD-8)		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 6/24/2020	DATE - 10-25-94	REVISED - R. BORO 12-06-11



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 6/24/2020	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	12
BD400-04 (BD-22)		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE 2)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE 2)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

¼" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE 1).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED ¾" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

**NOTE:**

- SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
- CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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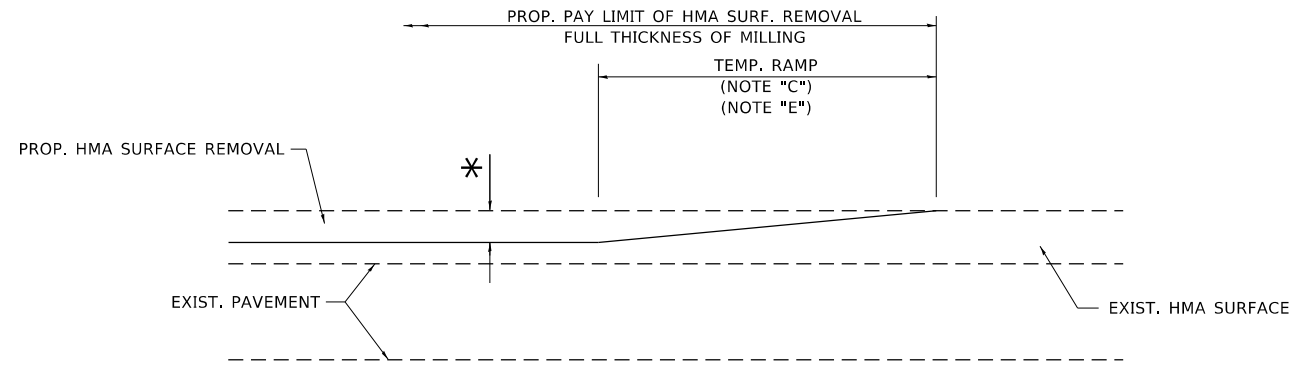
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PLOT DATE = 6/24/2020	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01
		REVISED - R. BORO 12-15-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

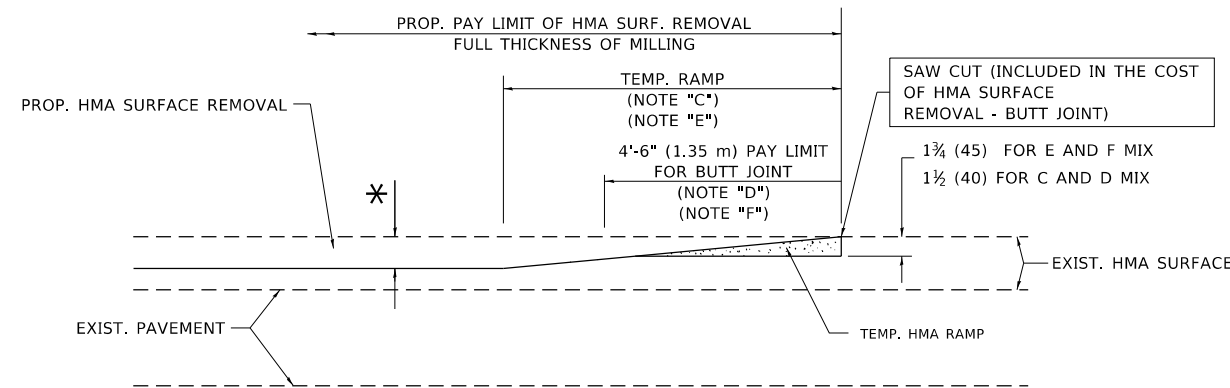
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234	2020-041-RS&SW	LAKE	25	13
BD600-06 (BD-24)			CONTRACT NO. 62L13	
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

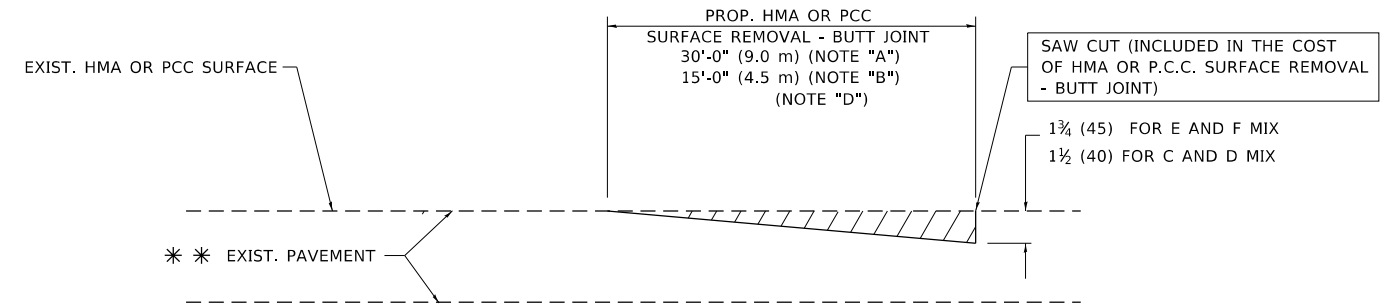


**HMA CONSTRUCTED TEMPORARY RAMP**

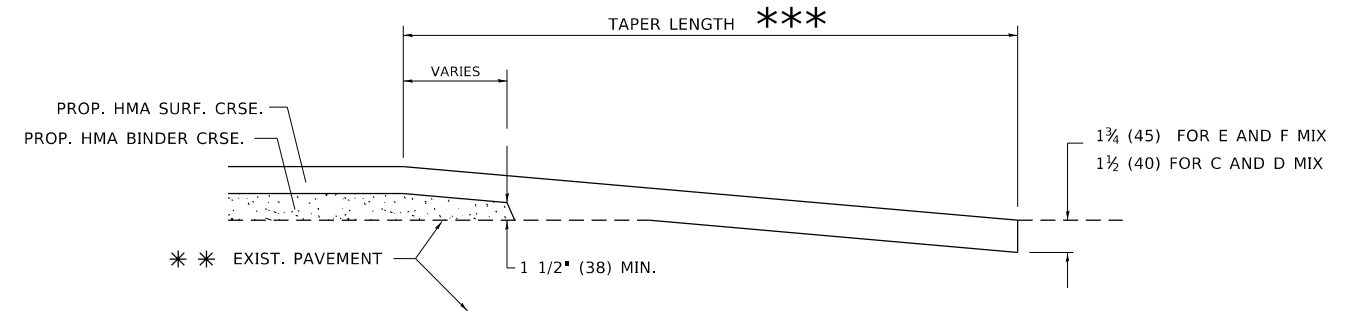
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

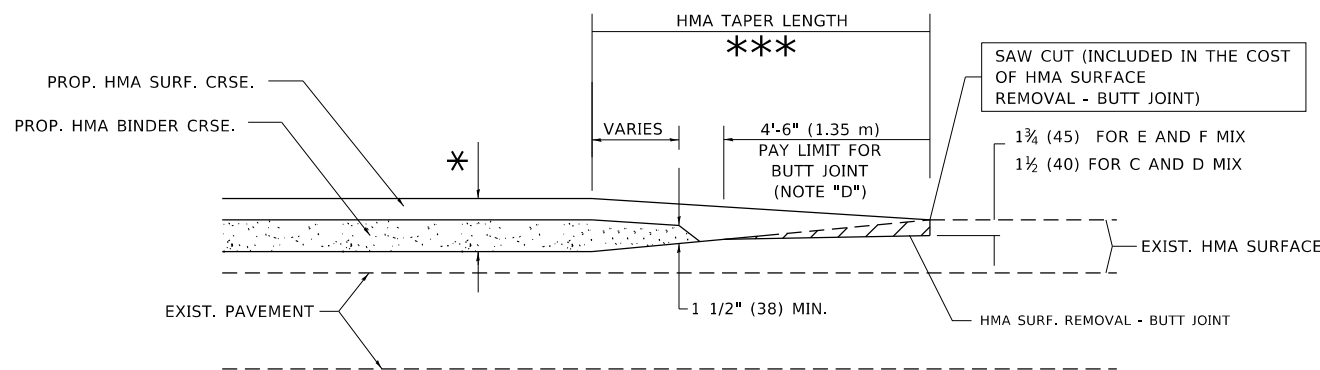
**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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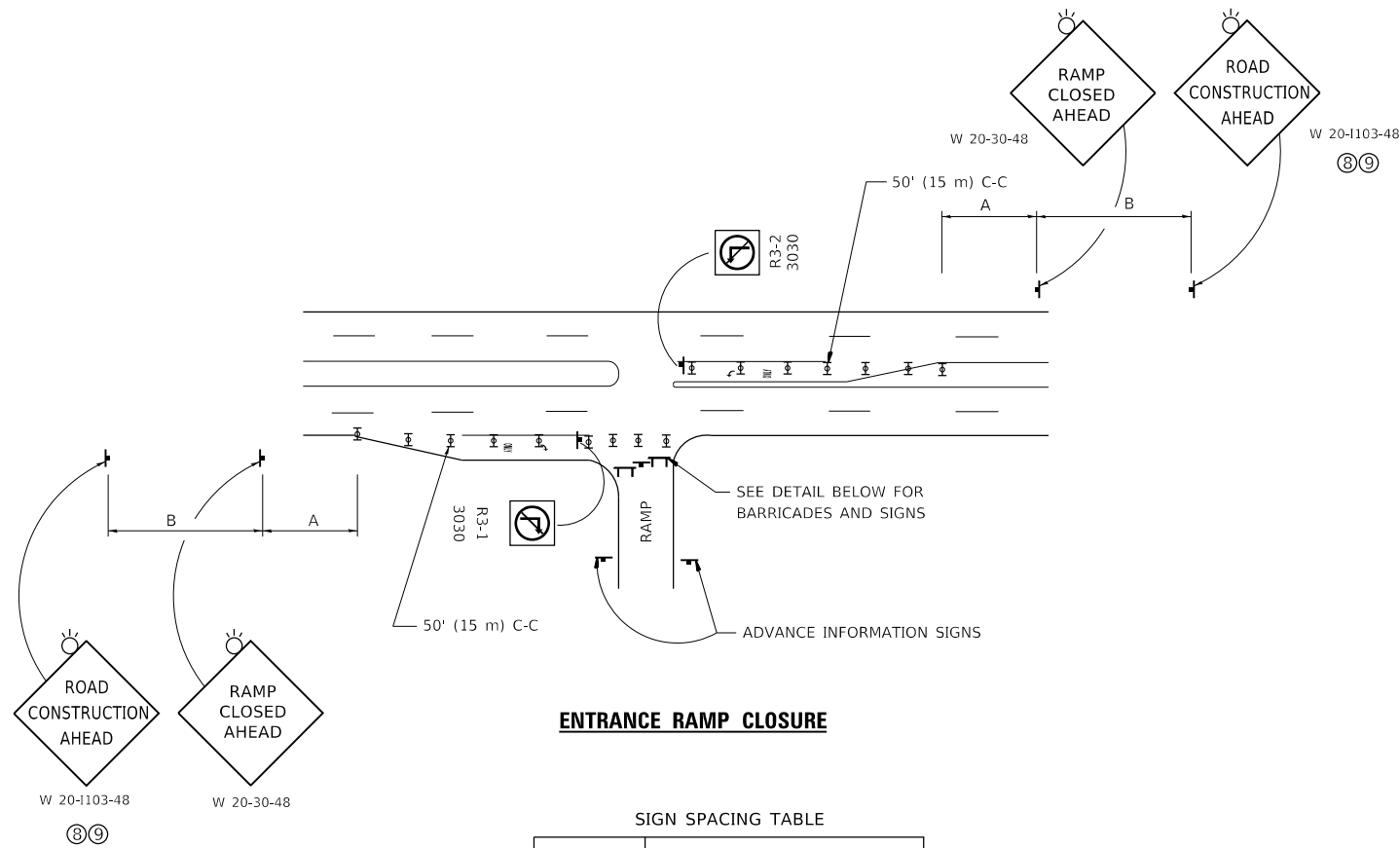
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PLOT DATE = 6/24/2020	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD32		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

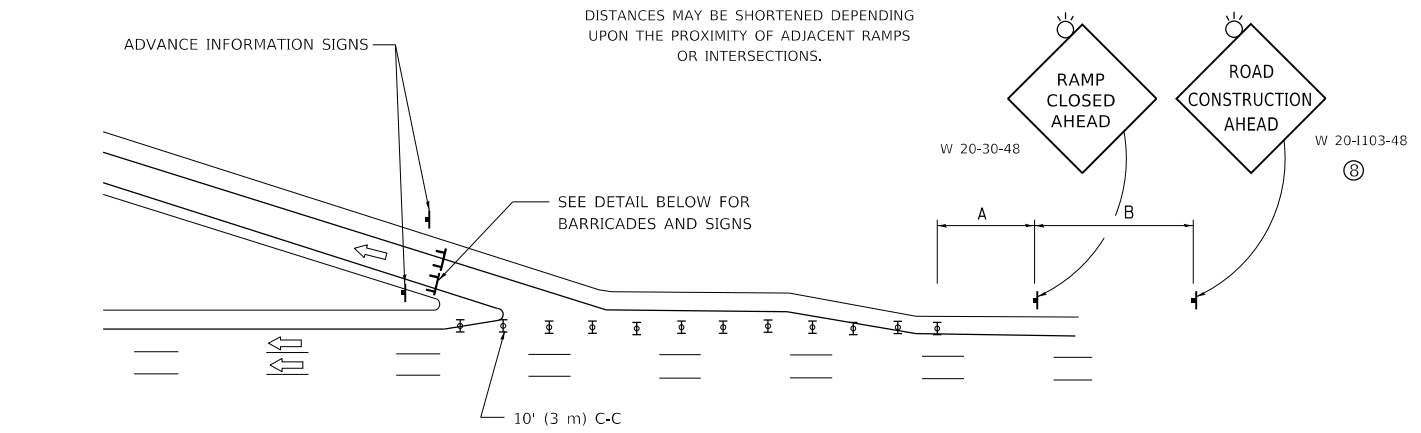


**ENTRANCE RAMP CLOSURE**

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

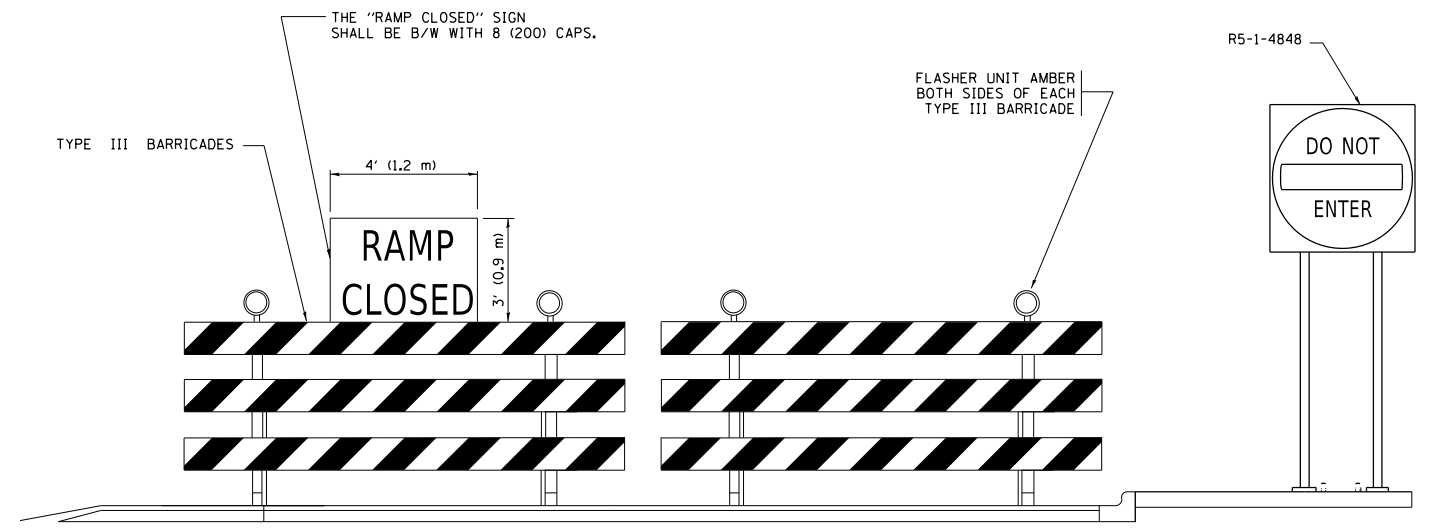
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

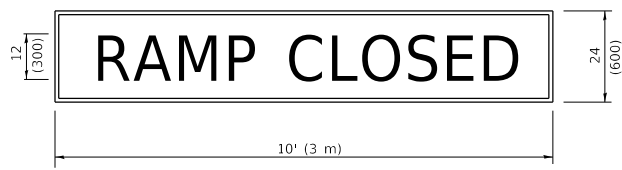
**SYMBOLS**

- ☐ TYPE II BARRICADE OR DRUM
- ☐ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



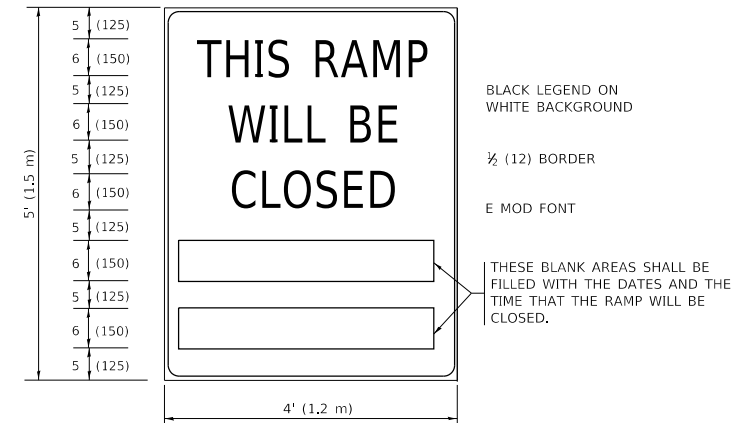
**DETAIL FOR REQUIRED BARRICADES & SIGNS**

**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER  
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.  
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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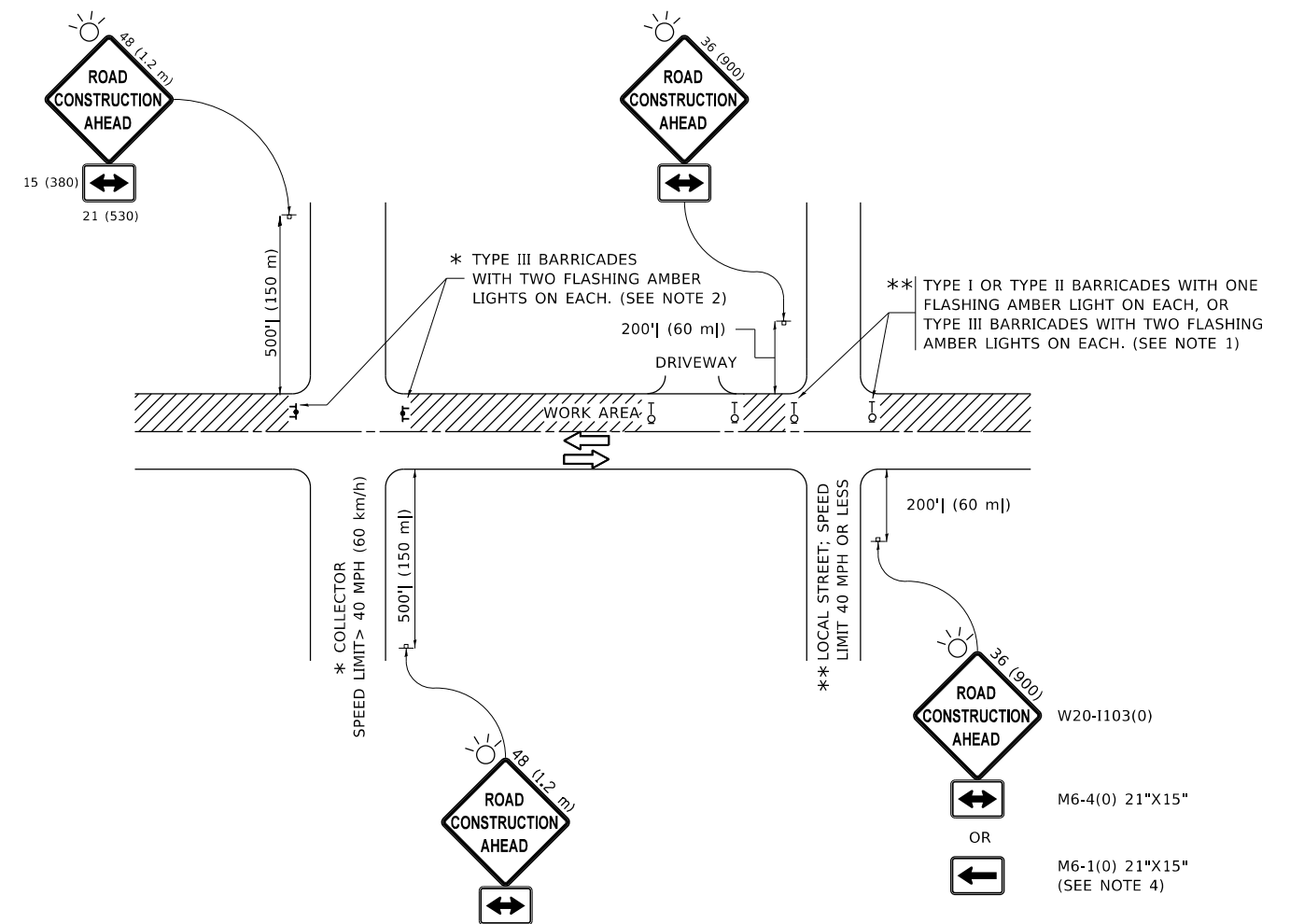
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		REVISED - M.D._01-18

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE\_AND\_EXIT\_RAMP  
CLOSURE\_DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	15
TC-08			CONTRACT NO. 62L13	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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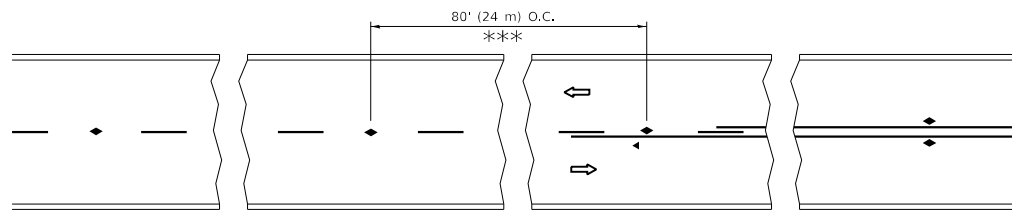
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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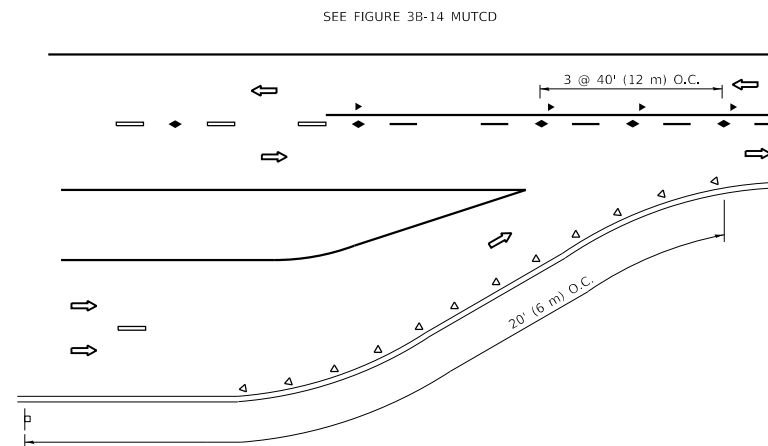
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TC-10			CONTRACT NO. 62L13	
ILLINOIS FED. AID PROJECT				



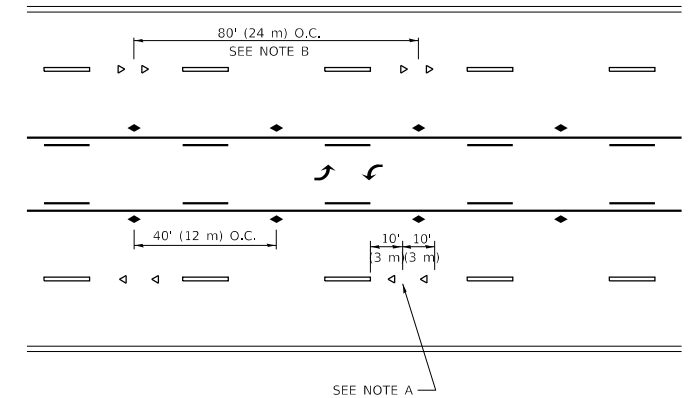


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

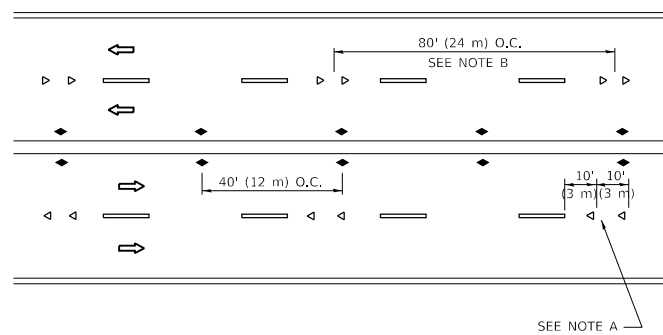
**TWO-LANE/TWO-WAY**



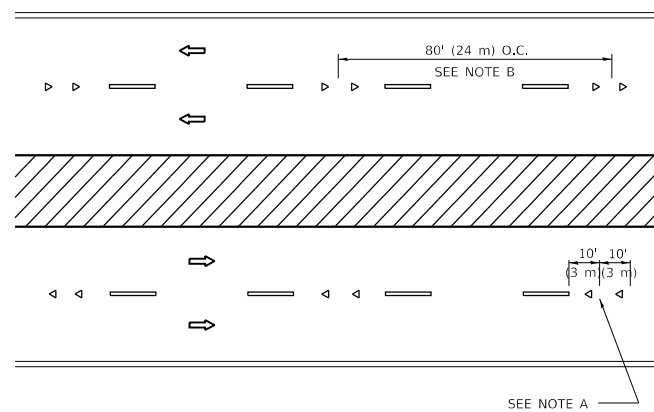
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

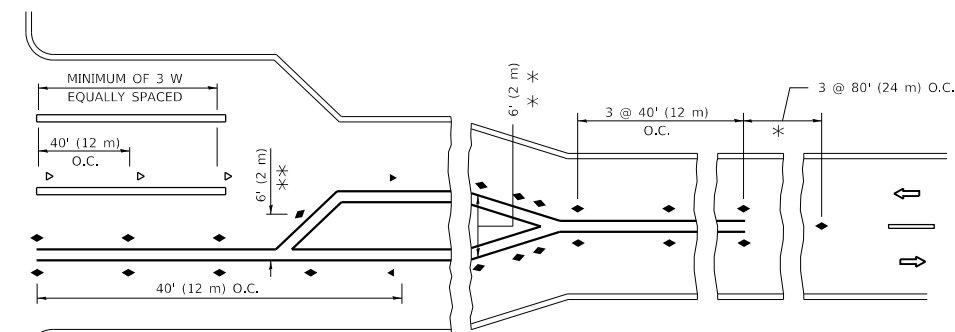
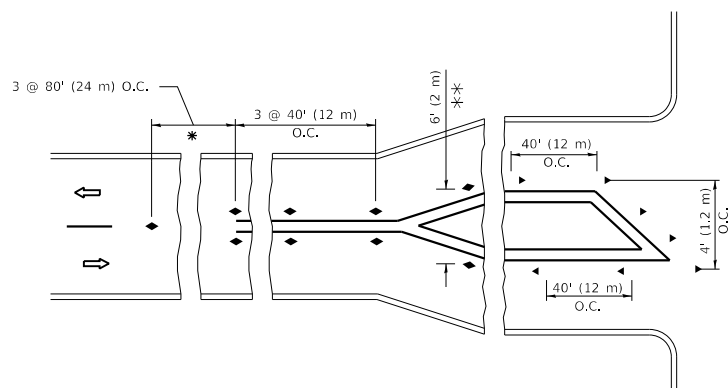
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

**TURN LANES**

All dimensions are in inches (millimeters) unless otherwise shown.

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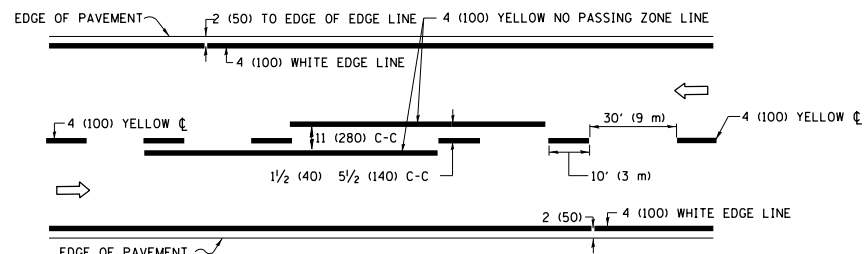
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

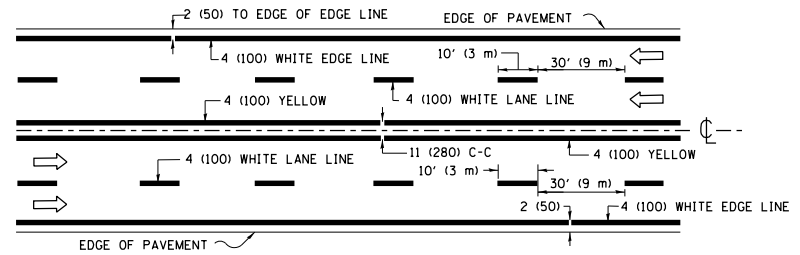
**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

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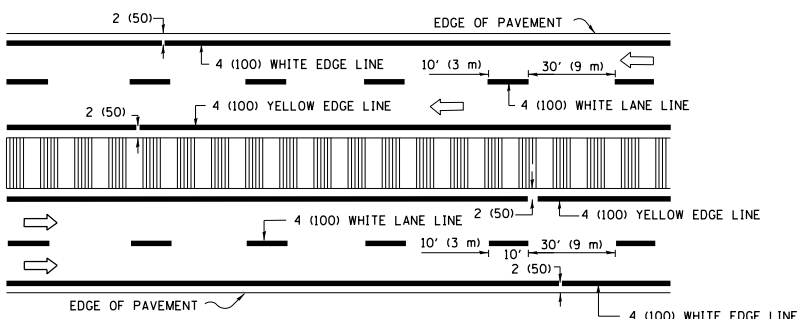
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	17
TC-11		CONTRACT NO. 62L13		
ILLINOIS		FED. AID PROJECT		



**2-LANE ROADWAY**

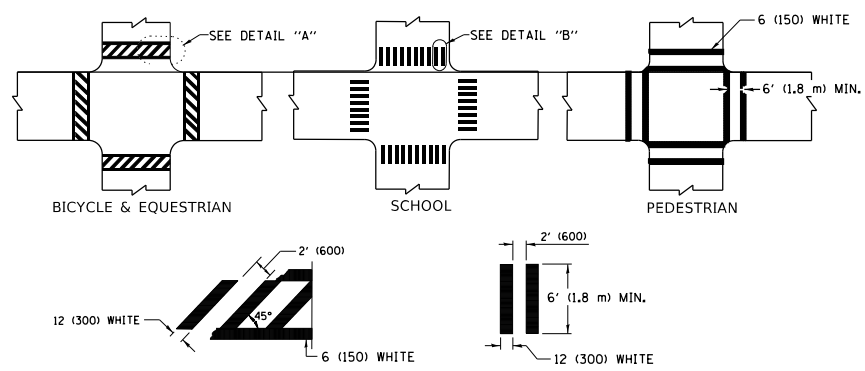


**MULTI-LANE UNDIVIDED**



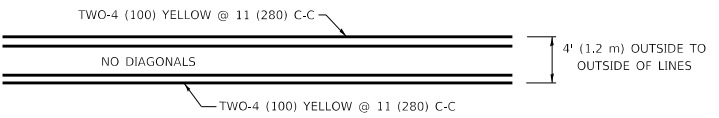
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

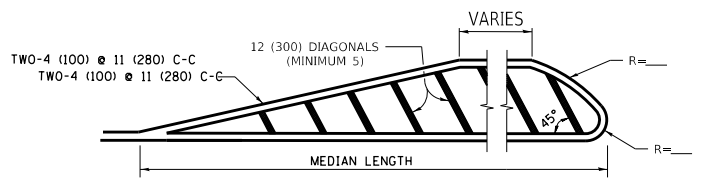


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

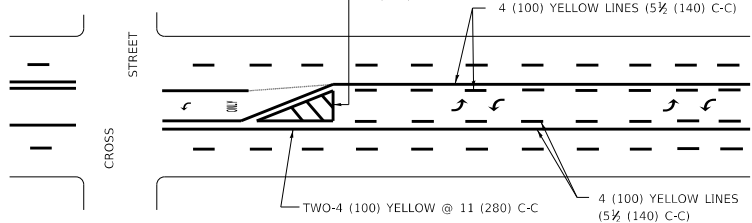


**4' (1.2 m) WIDE MEDIANS ONLY**

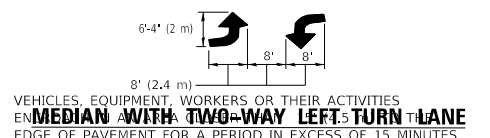


**MEDIANS OVER 4' (1.2 m) WIDE**

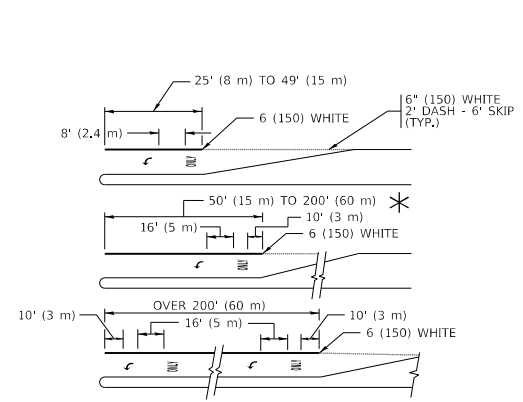
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



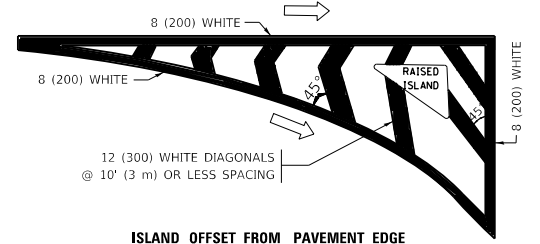
**TYPICAL PAINTED MEDIAN MARKING**



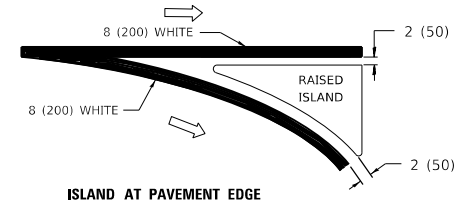
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

**TYPICAL LEFT (OR RIGHT) TURN LANE**

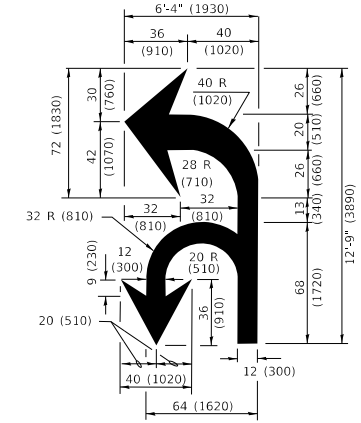
**TYPICAL TURN LANE MARKING**



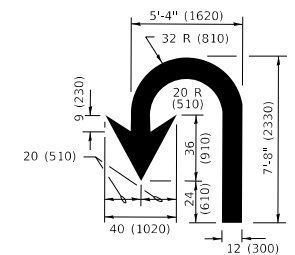
**ISLAND OFFSET FROM PAVEMENT EDGE**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**  
\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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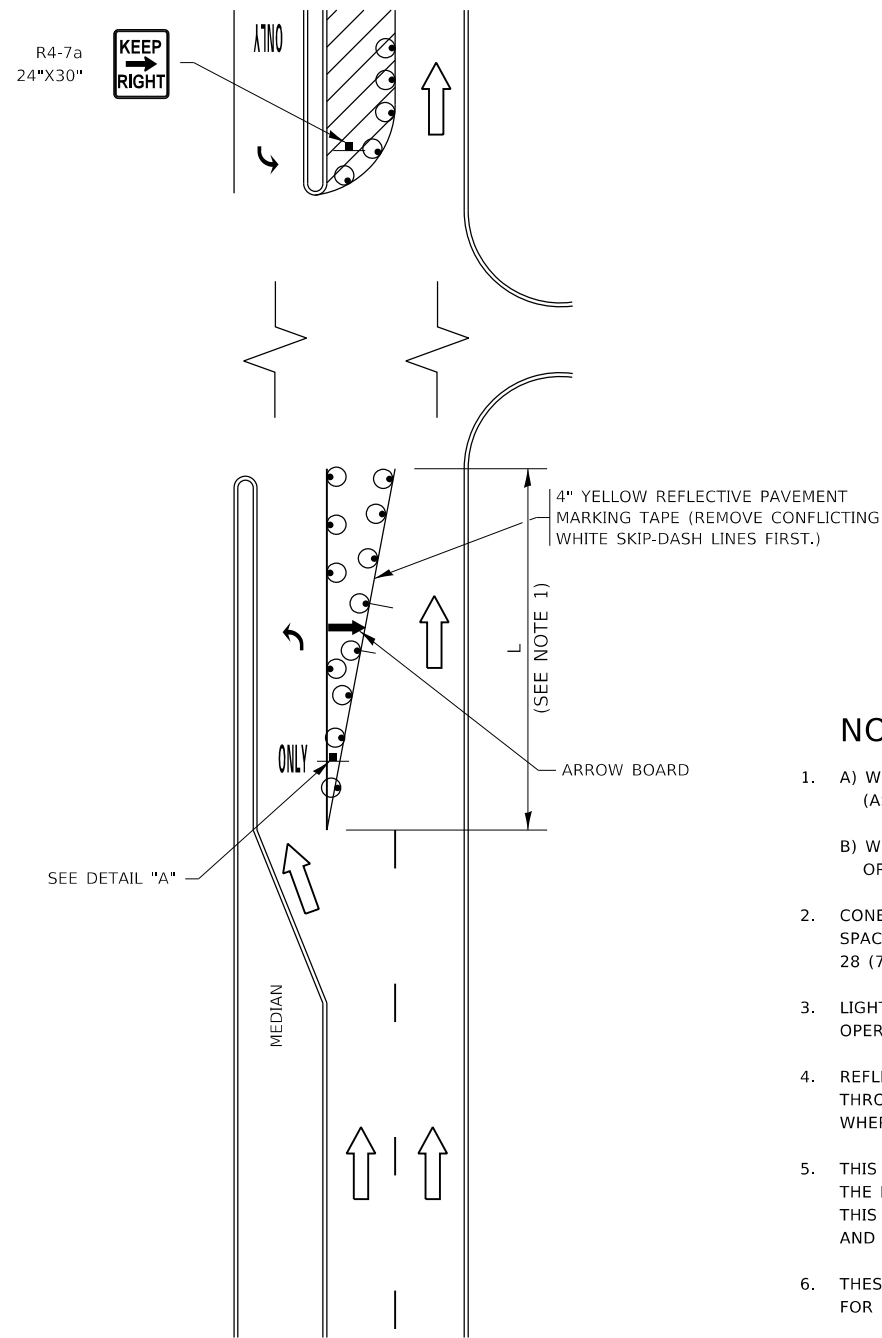
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		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1 OF 2 SHEETS	STA. TO STA.	

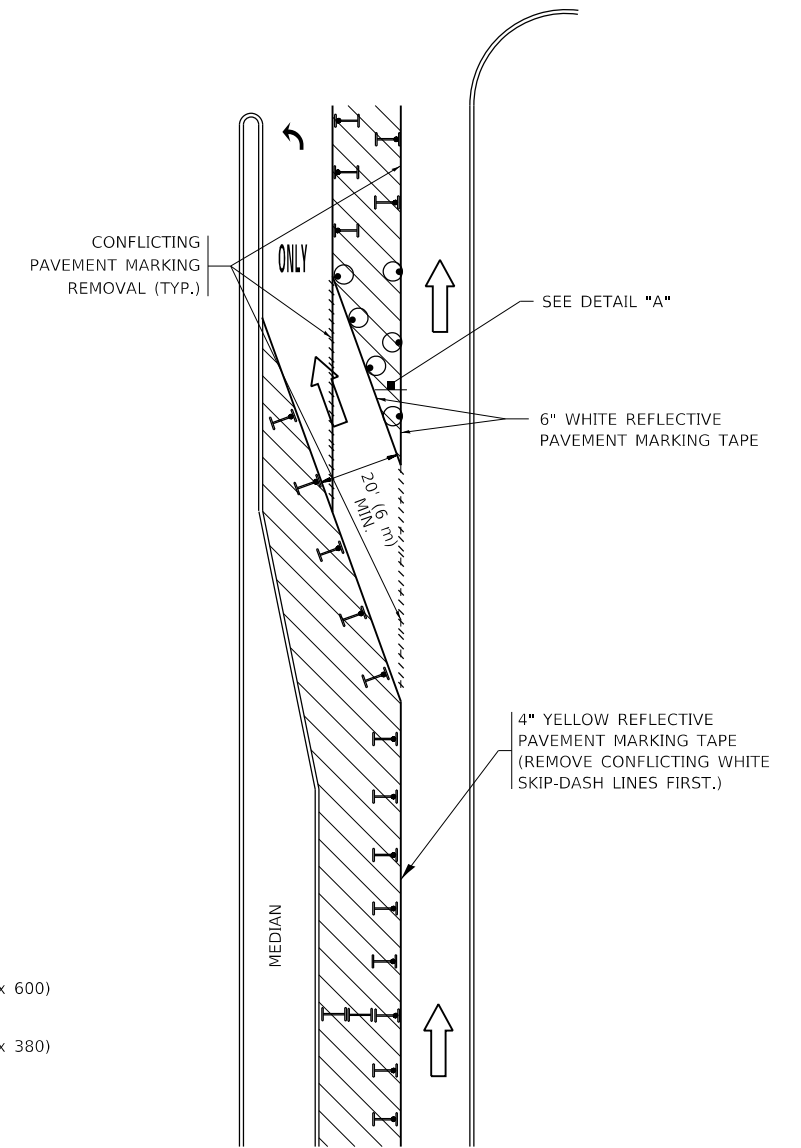
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234	2020-041-RS&SW	LAKE	25	18
TC-13		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

## TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

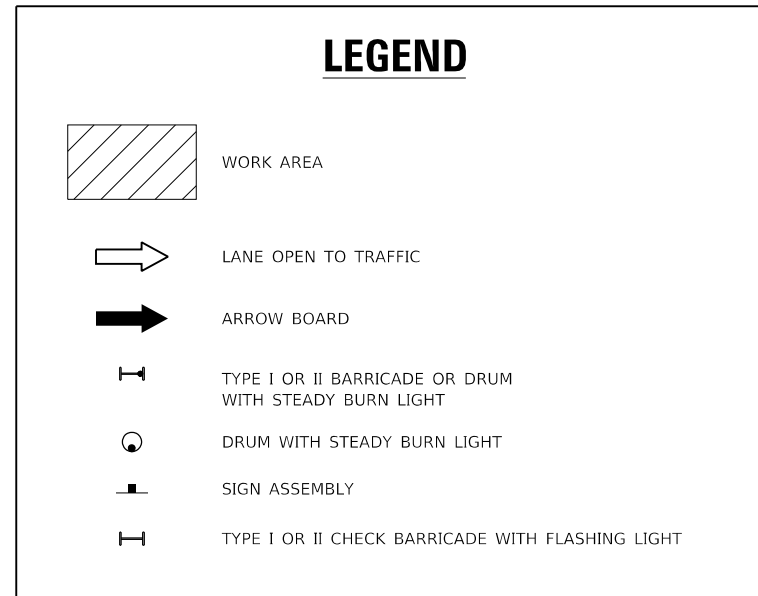


**FIGURE 1**

## TURN BAY ENTRANCE WITHIN A LANE CLOSURE

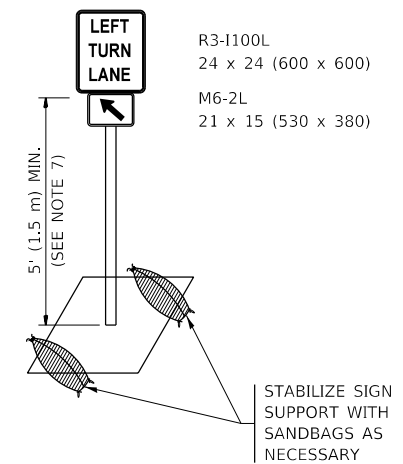


**FIGURE 2**



### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
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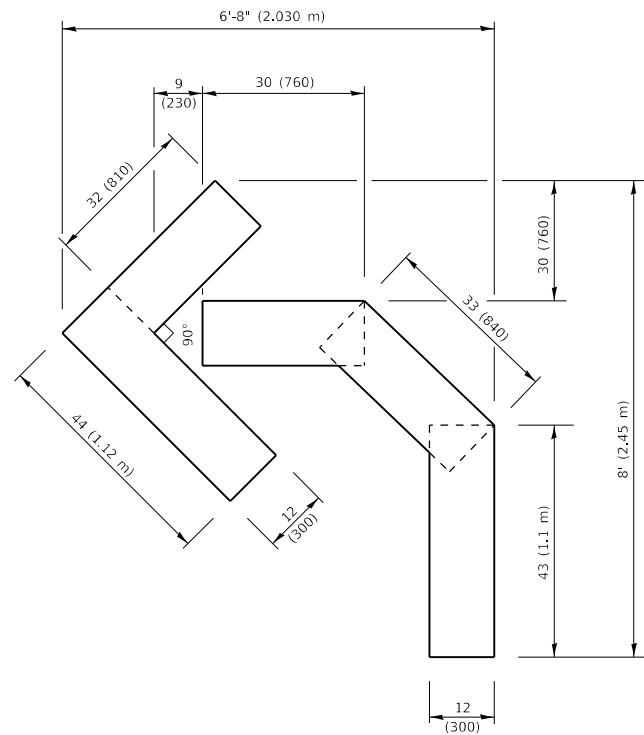
USER NAME = husselc	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
DRAWN - A. HOUSEH 11-07-95	REVISOR - A. SCHUETZE 07-01-13	
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISOR - A. SCHUETZE 09-15-16
PLOT DATE = 6/24/2020	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

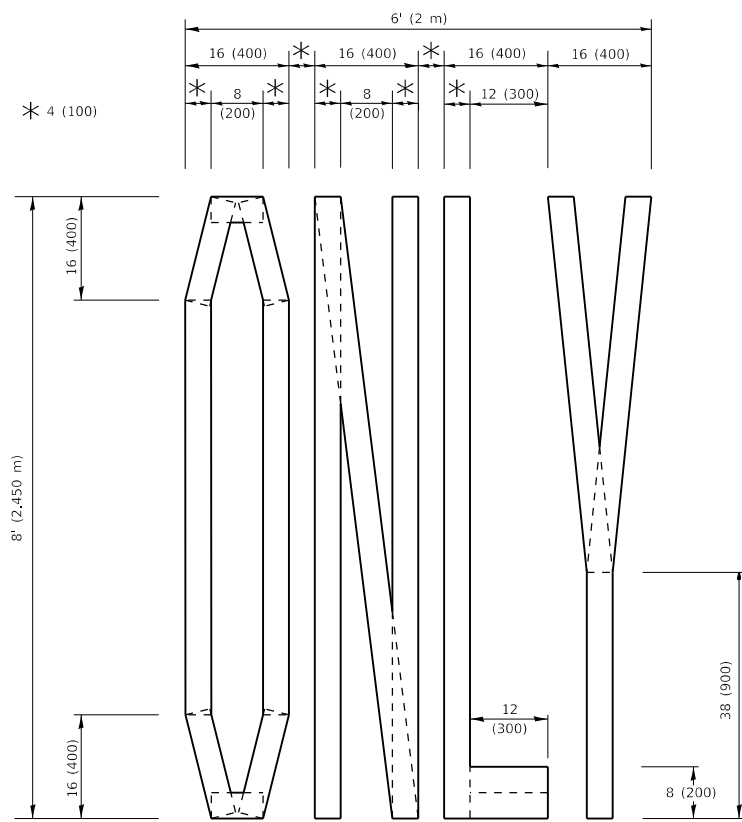
SCALE: NONE    SHEET 1 OF 1 SHEETS    STA.    TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	19
TC-14		CONTRACT NO. 62L13		
ILLINOIS   FED. AID PROJECT				



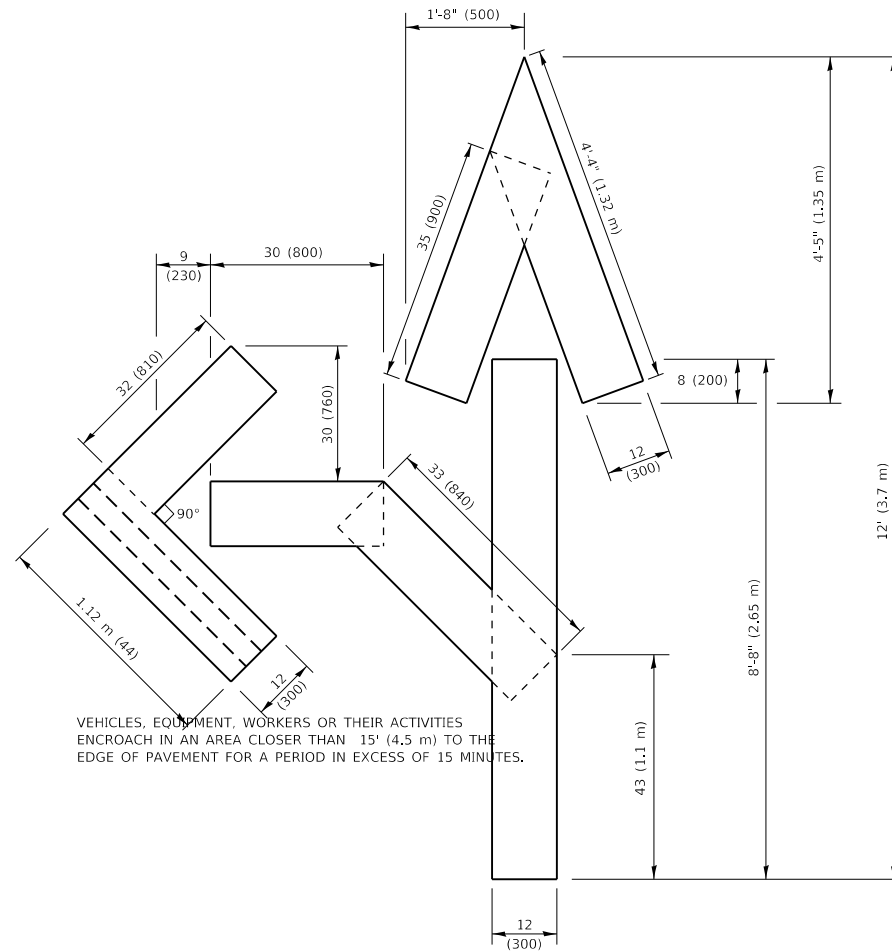
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)



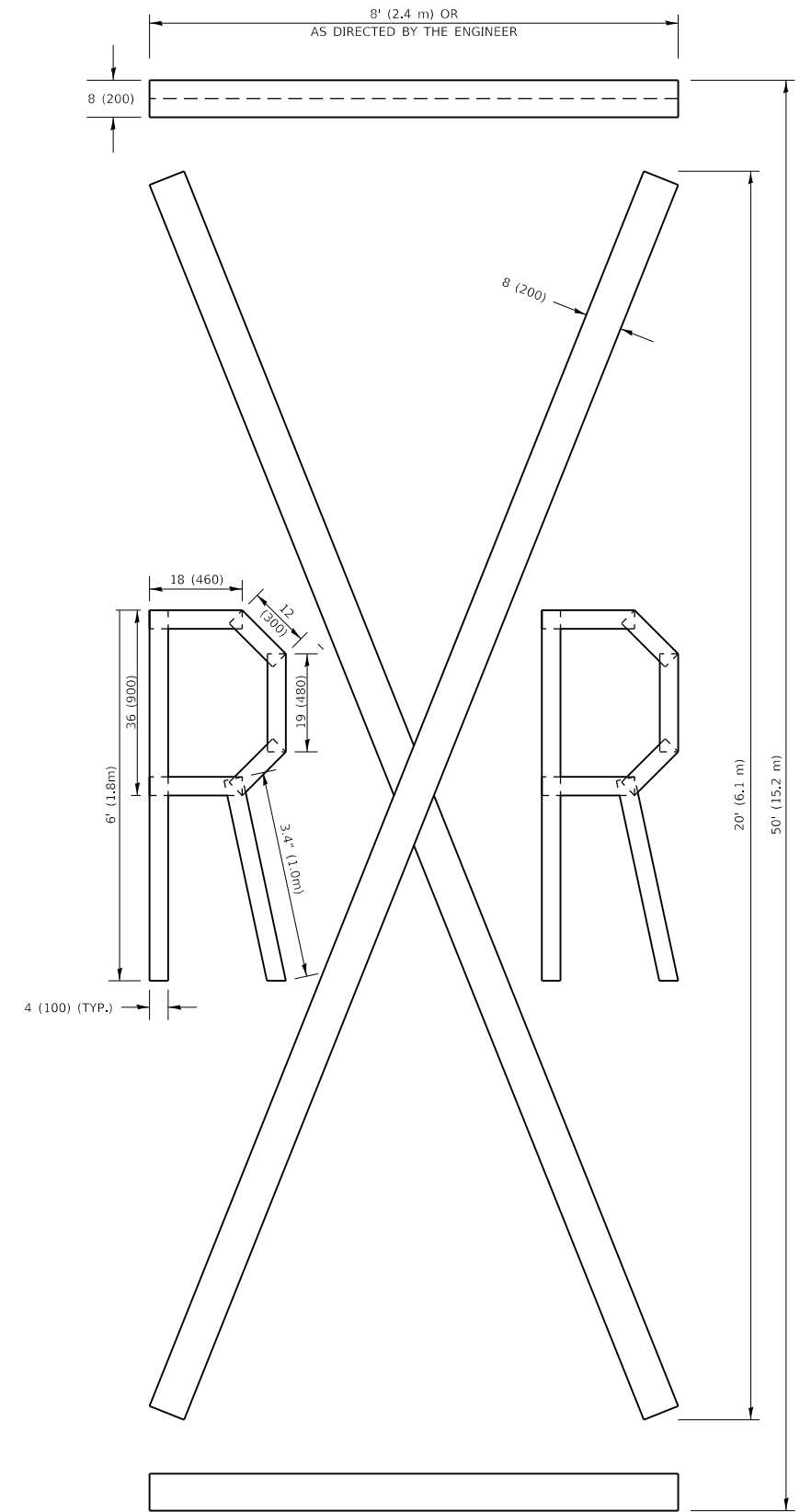
VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
FILE NAME: p:\pub\barroom.dwg  
PROJECT: 142420\CADD\Design\Design\DESIGN.dwg

USER NAME = hussein	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 6/24/2020	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

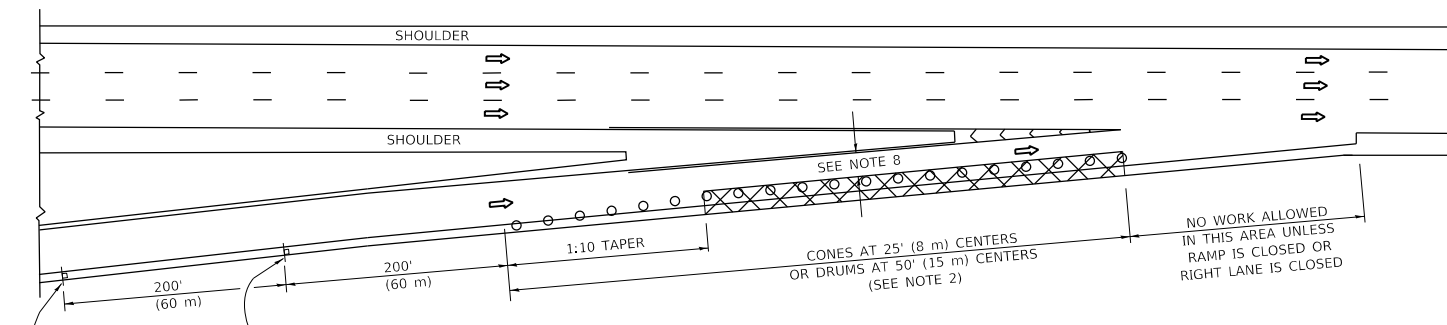
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

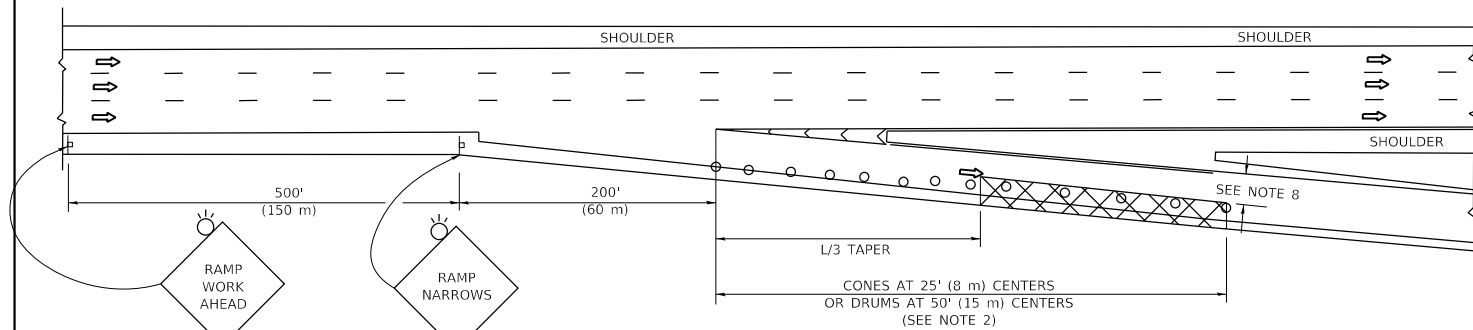
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	20
TC-16		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

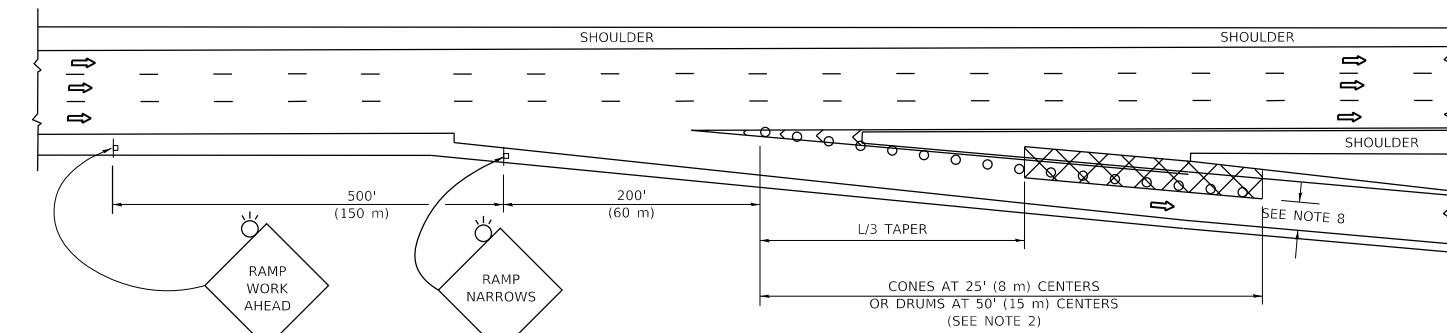
**PARTIAL RAMP CLOSURE DETAILS**



**TYPICAL ENTRANCE RAMP**



**TYPICAL EXIT RAMP**



**TYPICAL EXIT RAMP**

**SYMBOLS**

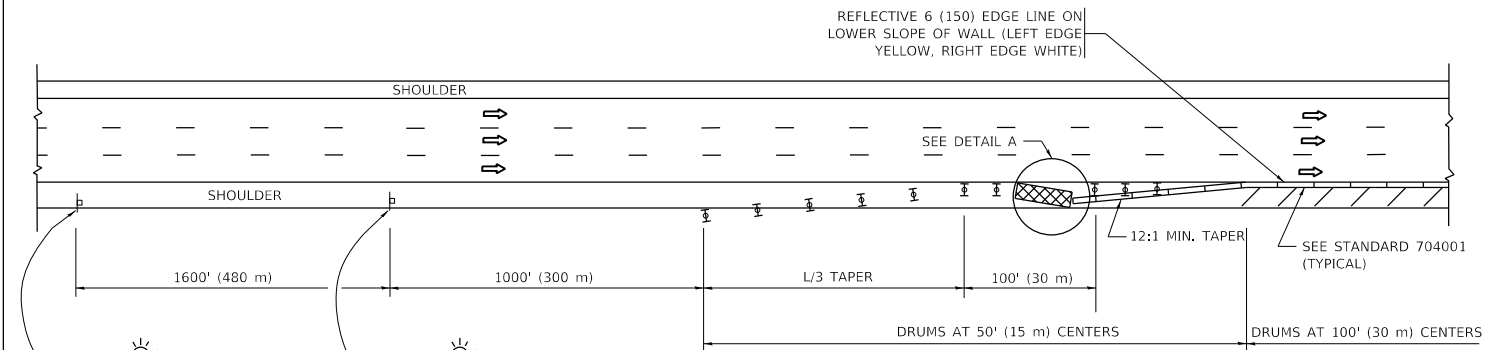
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

**GENERAL NOTES:**

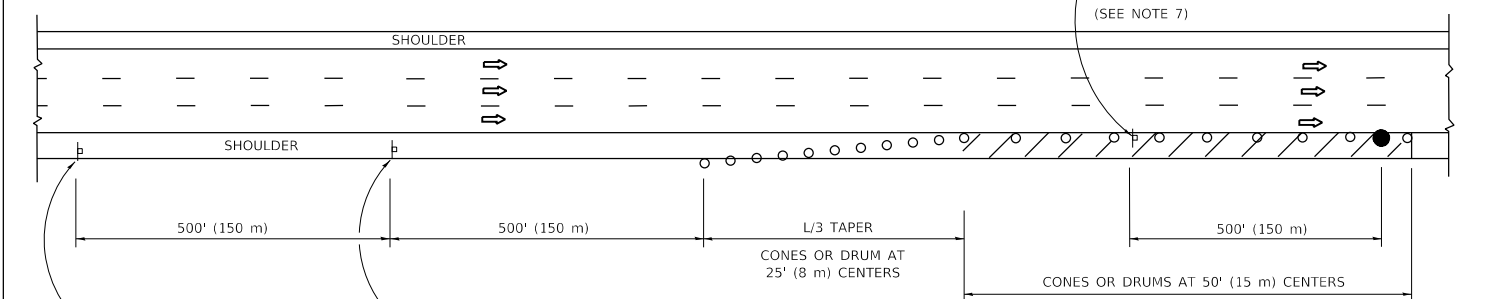
1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(WXS)$ $L=(WXS)$
	$W =$ WIDTH OF OFFSET IN FEET (METERS)
	$S =$ NORMAL POSTED SPEED MPH (KM/H)
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

**SHOULDER CLOSURE DETAILS**



**PERMANENT SHOULDER CLOSURE**



**DAYTIME SHOULDER CLOSURE**

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"  
 IMPACT ATTENUATOR, TEMPORARY  
 (SEE NOTE 5)**

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
 16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default  
 FILE: \\nchrp\pub\pubroom\dat\illinois\gov\PIWIDOT\Documents\IBOT\_Offices\Illinet\1\Projects\ID142420\CADDData\Design\DRS\4.dgn

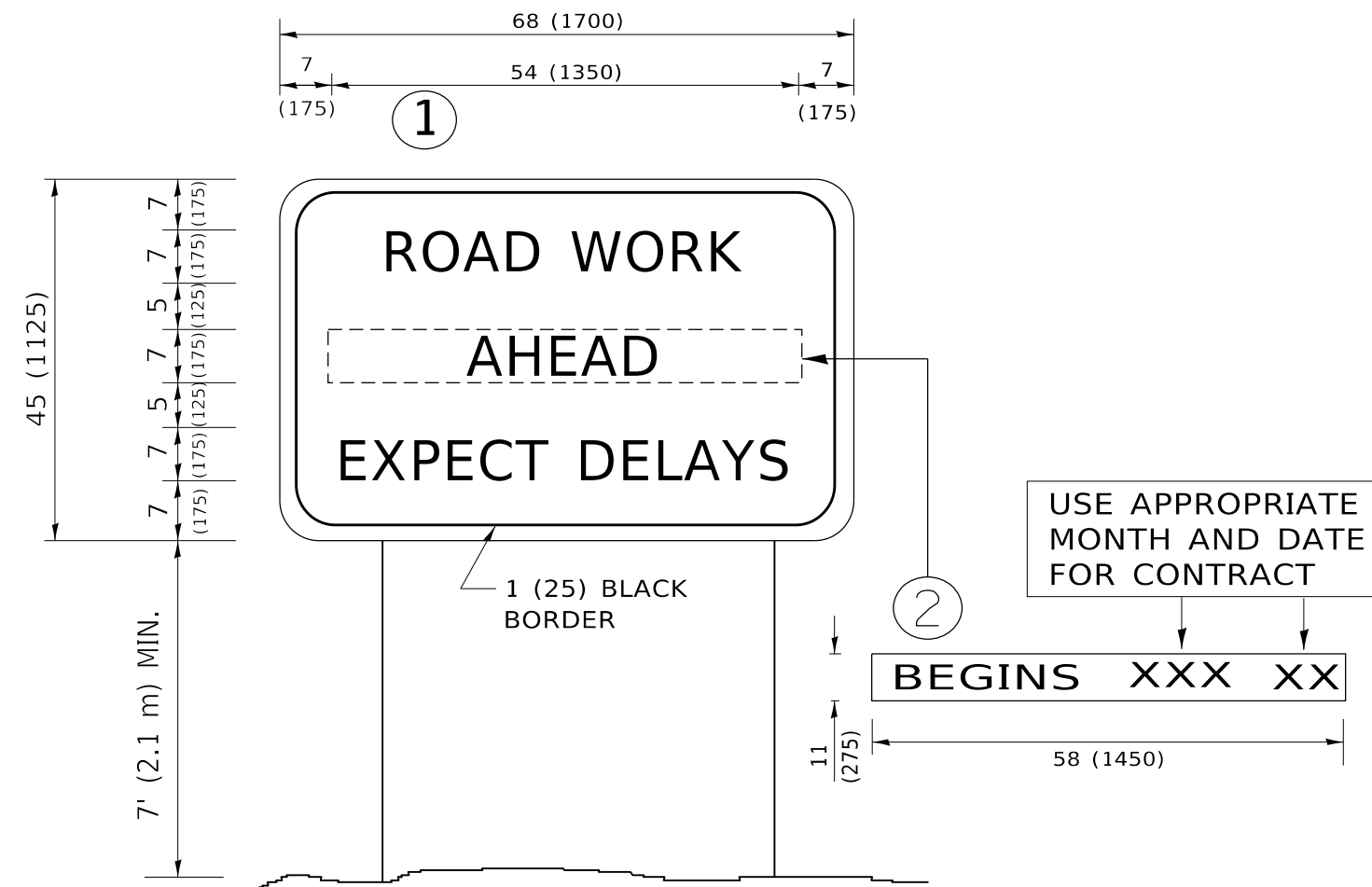
USER NAME = hussein	DESIGNED -	REVISED - S.P.B. 01-07
	DRAWN - D.W.S.	REVISED - S.P.B. 12-09
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M.D. 06-13
PLOT DATE = 6/24/2020	DATE - 11-96	REVISED - M.D. 01-18

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR FREEWAY  
 SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	21
TC-17		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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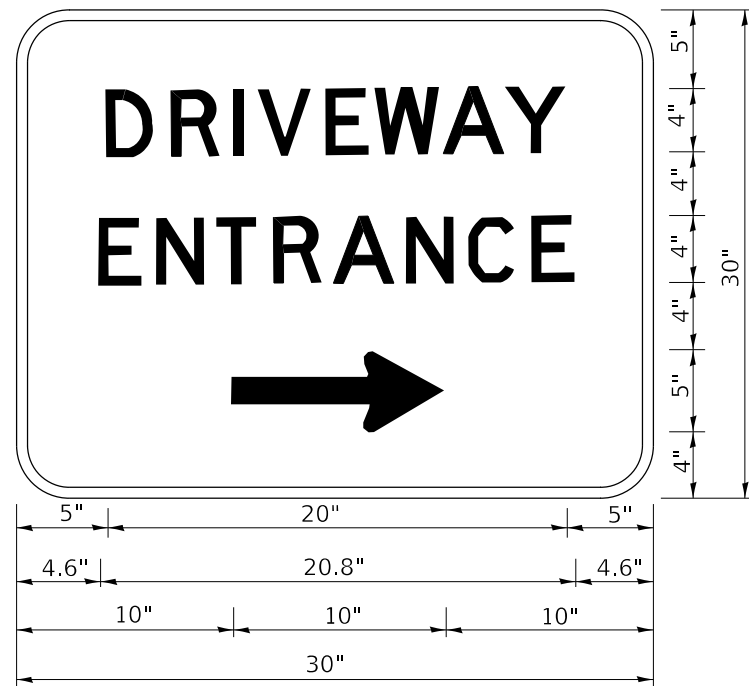
USER NAME = husseinaa	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 6/24/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	22
TC-22		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default  
 FILE: Model: ProjectRoom.dwg  
 PROJECT: P:\Projects\142420\CADD\Drawings\DOT\Office\Drawings\1\ProjectRoom.dwg

USER NAME = husseinaa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/24/2020	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

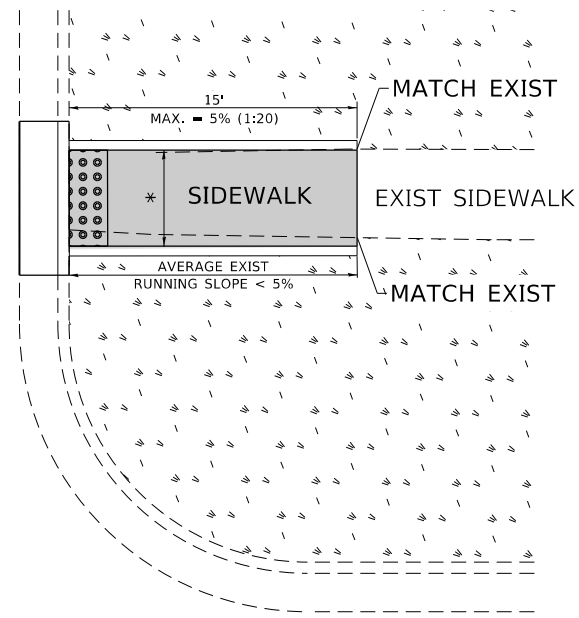
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

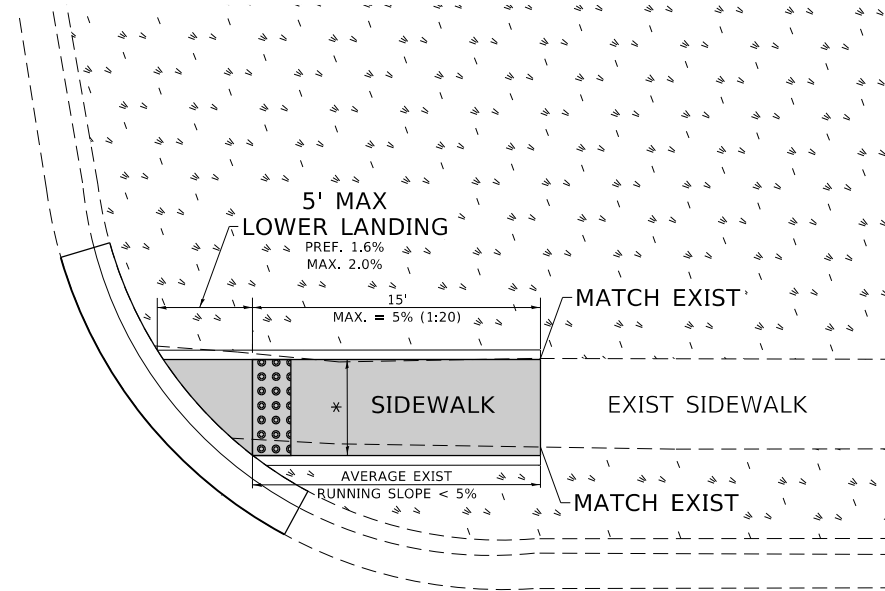
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	23
TC-26		CONTRACT NO. 62L13		
		ILLINOIS	FED. AID PROJECT	

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

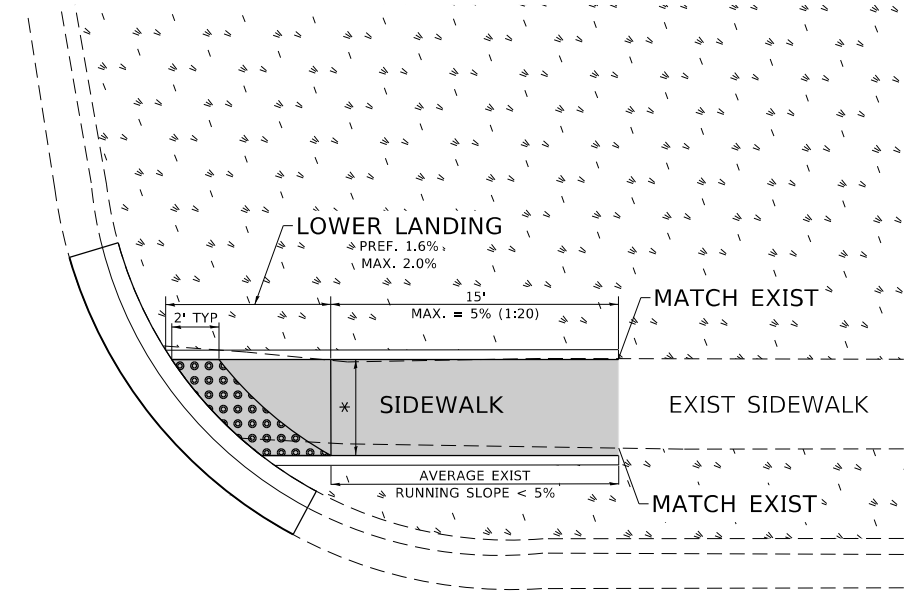
**PD-01A**







**PD-01B**



**PD-01C**



**LEGEND**

-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS
-  PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
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USER NAME = husseinaa	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/24/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-01)**

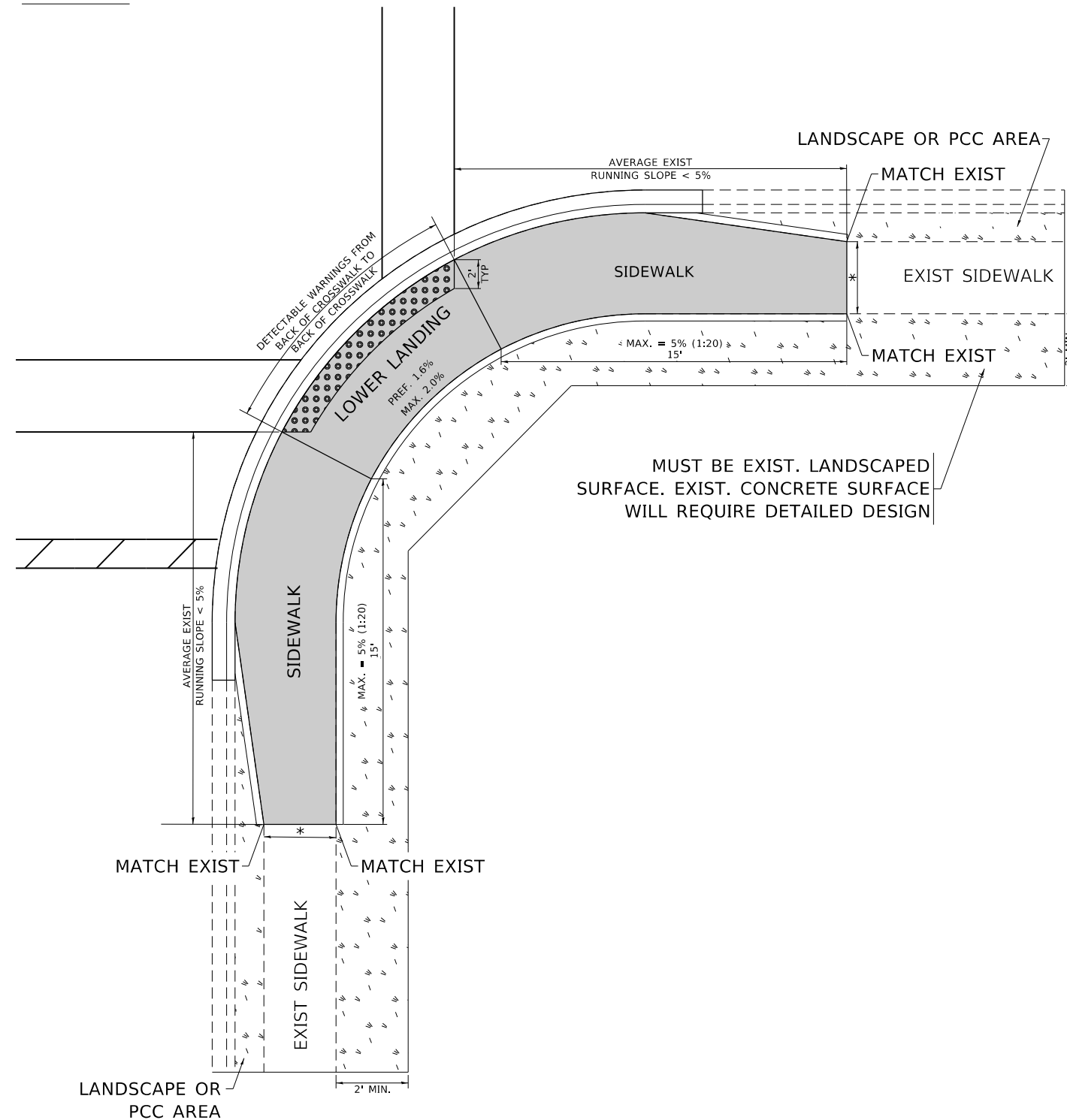
SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	24
PD-01		CONTRACT NO. 62L13		
ILLINOIS FED. AID PROJECT				

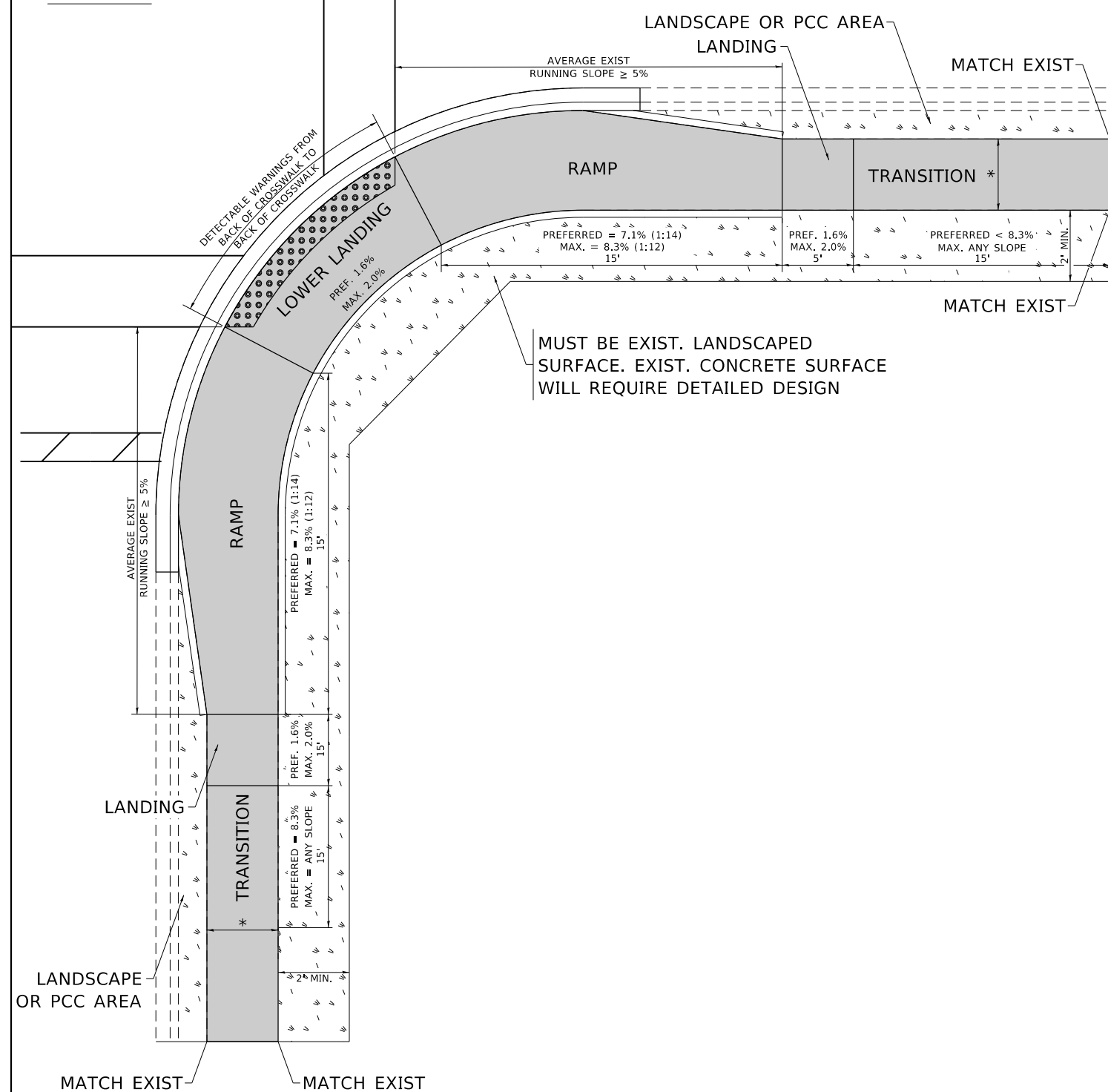


# ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

**PD-06A**



**PD-06B**



**LEGEND**

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
FILE: hussaina.pwd\pwworkroom\ada\illinois.gov\PWD\DOT\Documents\DOT\_Offices\Illinet\1\Projects\142420\CADD\ata\Design\DES06.dgn

USER NAME = hussaina	DESIGNED -	REVISED -
DRAWN - R. LEDEZMA	REVISOR -	REVISOR -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 6/24/2020	DATE - 10/02/2019	REVISOR -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS  
(PD-06)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
234	2020-041-RS&SW	LAKE	25	25
PD-06		CONTRACT NO. 62L13		
ILLINOIS		FED. AID PROJECT		