

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	1
FED. ROAD DIST. NO.	ILLINOIS CONTRACT NO.		60D86	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
SECTION 111 B-2
BRIDGE BEAM REPLACEMENT, NEW DECK

WILL COUNTY
C-91-120-08

FOR INDEX OF SHEETS SEE SHEET NUMBER 2

THIS IMPROVEMENT IS LOCATED
IN CUSTER TOWNSHIP WITHIN
UNINCORPORATED WILL COUNTY

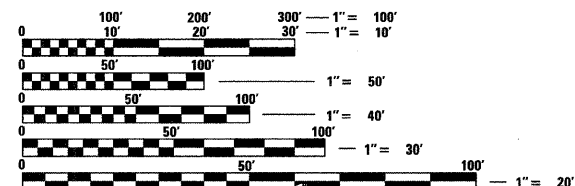
TRAFFIC DATA

2005 ADT - 4500
POSTED SPEED LIMIT - 55 MPH

IMPROVEMENT LOCATION
SN: 099-0172

IL 113 OVER TERRY CREEK

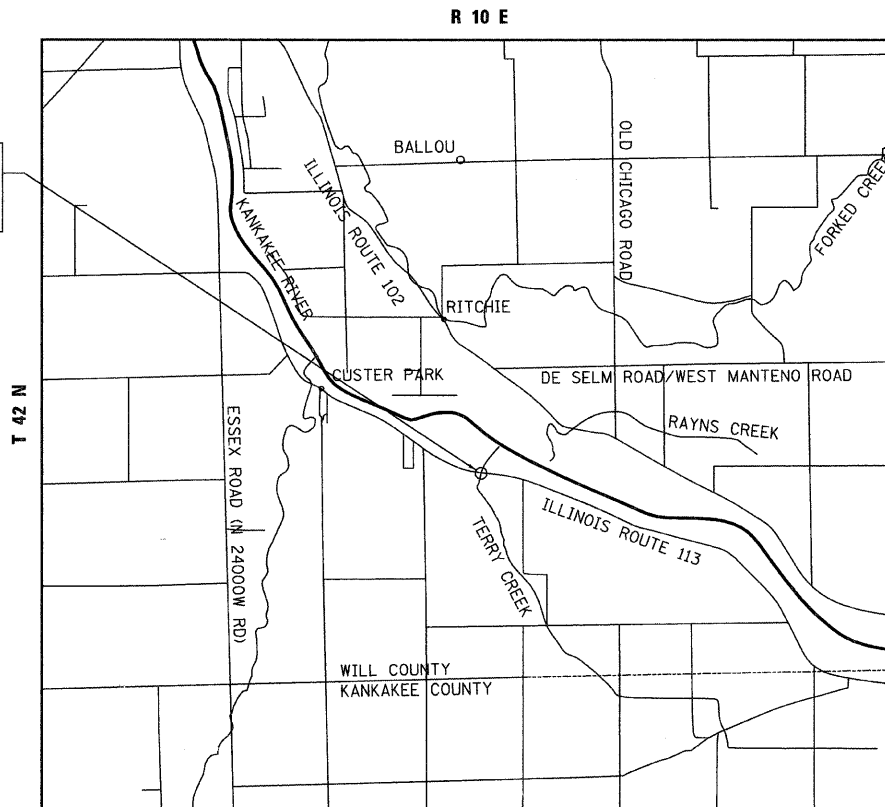
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SINGLE SPAN PPC DECK
BEAM BRIDGE ON CLOSED ABUTMENTS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

Ciorba Group, Inc.
DESIGN FIRM
REGISTRATION NUMBER
184-001016
CONSULTING ENGINEERS
SUITE 402, 5507 NORTH CUMBERLAND AVE
CHICAGO, ILLINOIS 60656 :: (773) 775-4009



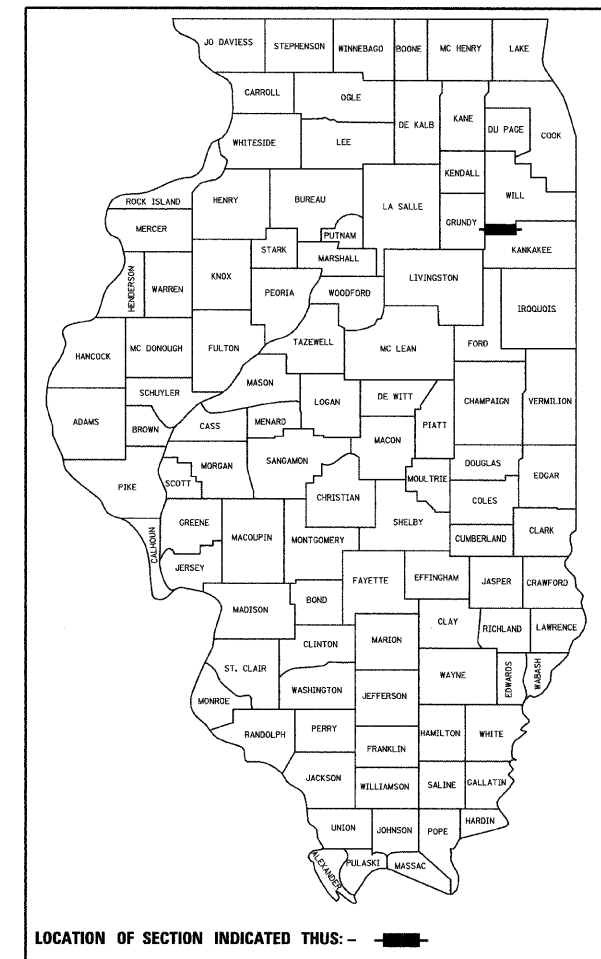
CUSTER TOWNSHIP

LOCATION MAP
1:5000

GROSS AND NET LENGTH OF PROJECT = 451 FT = 0.09 MI



D-91-120-08



LOCATION OF SECTION INDICATED THUS: -



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED July 7, 2008

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

August 15, 2008
Eric E. Haran
INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

August 15, 2008
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

CONTRACT NO. 60D86

DISTRICT 1 DESIGN PLAN PREPARATION ENGINEER: K. ENG (847) 705-4247

INDEX OF SHEETS

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31	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
32	ARTERIAL ROAD INFORMATION SIGN (TC-22)

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
420401-00	BRIDGE APPROACH PAVEMENT
515001-02	NAME PLATE FOR BRIDGES
630001-07	STEEL PLATE BEAM GUARDRAIL
630301-04	SHOULDER WIDENING FOR TYPE 1 GUARDRAIL TERMINALS
631032-03	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-02	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-01	REFLECTOR MARKER AND MOUNTING DETAILS
701201-02	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701321-04	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701901	TRAFFIC CONTROL DEVICES
704001-04	TEMPORARY CONCRETE BARRIER
805001	ELECTRICAL SERVICE INSTALLATION DETAILS
857001	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
880001	SPAN WIRE MOUNTED SIGNAL AND FLASHING LIGHT BEACON INSTALLATION

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE PAVEMENT MARKING LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT MS. CORA MATHIS, AREA TRAFFIC FIELD ENGINEER @ 815-485-6475.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- CHANGEABLE MESSAGE SIGNS SHALL BE PLACED IN ADVANCE OF THE PROJECT LIMITS AT LEAST ONE WEEK PRIOR TO LANE CLOSURE AND TEMPORARY TRAFFIC SIGNAL OPERATIONS.

COMMITMENTS

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATIONS	MIXTURE TYPE	AC TYPE	PERCENT AIR VOIDS
ROADWAY RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 1 1/2"	PG 64-22	4% @ 70 GYR
	LEVELING BINDER (MACHINE METHOD), N70, 3/4" MIN.	PG 64-22 *	4% @ 70 GYR
BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5), 2"	PG 64-22	4% @ 70 GYR
	HOT-MIX ASPHALT BINDER COURSE, IL 19.0 mm, N70, 10" to 13"	PG 64-22 *	4% @ 70 GYR
TEMPORARY PAVEMENT, 10"	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL-9.5 mm), 1 1/2"	PG 64-22	4% @ 50 GYR
	HOT-MIX ASPHALT BINDER COURSE, IL 19.0 mm, N50, 8 1/2"	PG 64-22 *	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

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PLOT DATE = 7/8/2008	DATE - 07/09/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

F.A.S. RTE. 1317	SECTION 111 B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 2
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D86	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITY 100% STATE	CONSTRUCTION CODE	
CODE NO.	DESCRIPTION	UNIT		ROADWAY 1000	BRIDGE X081-2A
20200100	EARTH EXCAVATION	CU YD	195	195	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	125	125	
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1	
25100630	EROSION CONTROL BLANKET	SQ YD	125	125	
31100300	SUB-BASE GRANULAR MATERIAL, TYPE A 4"	SQ YD	250	250	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.1	0.1	
40600300	AGGREGATE (PRIME COAT)	TON	1	1	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	10	10	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	30	30	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	20	20	
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	220	220	
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	46	46	
44000100	PAVEMENT REMOVAL	SQ YD	200	200	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	225	225	
44000700	APPROACH SLAB REMOVAL	SQ YD	131		131
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	320	320	
50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1		1
50102400	CONCRETE REMOVAL	CU YD	4.0		4.0
50300225	CONCRETE STRUCTURES	CU YD	5.0		5.0
50300255	CONCRETE SUPERSTRUCTURE	CU YD	5.0		5.0
50300260	BRIDGE DECK GROOVING	SQ YD	139		139
50300300	PROTECTIVE COAT	SQ YD	155		155
50301200	CONCRETE WEARING SURFACE	SQ YD	155		155
50400405	PRECAST PRESTRESSED CONCRETE DECK BEAMS (21" DEPTH)	SQ FT	1397		1397
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3090		3090
50800515	BAR SPLICERS	EACH	55		55
50901050	STEEL RAILING, TYPE SM	FOOT	92		92
51500100	NAME PLATES	EACH	1		1
52000110	PREFORMED JOINT STRIP SEAL	FOOT	33		33
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	200	200	
* 63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4	4	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	580	580	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	2	4
67100100	MOBILIZATION	L SUM	1	0.2	0.8
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.2	0.8

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITY 100% STATE	CONSTRUCTION CODE	
CODE NO.	DESCRIPTION	UNIT		ROADWAY 1000	BRIDGE X081-2A
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,000	1,000	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	350	350	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	300	300	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,300	2,300	
* 78008210	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4"	FOOT	410	410	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	16	16	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	4	4	
78200450	MONODIRECTIONAL GUARDRAIL REFLECTORS	EACH	8	8	
78200530	BARRIER WALL MARKERS, TYPE C	EACH	26	26	
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	900	900	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	18	18	
* 80400100	ELECTRIC SERVICE INSTALLATION	EACH	1	1	
* 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1	1	
* 82103400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 400 WATT	EACH	6	6	
* 83057355	LIGHT POLE, WOOD, 60 FOOT, CLASS 4, WITH 15FT MAST ARM	EACH	6	6	
* 84100110	REMOVAL OF TEMPORARY LIGHTING UNITS	EACH	6	6	
* 84500120	REMOVAL OF ELECTRIC SERVICE INSTALLATION	EACH	1	1	
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
* X0323574	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	6	6	
X0325239	TEMPORARY PAVEMENT 10"	SQ YD	65	65	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	50		50
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III 4 INCH	FOOT	2,500	2,500	
X0325841	WET REFLECTIVE TEMPORARY TAPE, TYPE III 24 INCH	FOOT	55	55	
* X8900005	TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION	EACH	1	1	
* XX006937	GROUND ROD, 5/8" DIA. X 10 FT.	EACH	3	3	
Z0001900	ASBESTOS BEARING PAD REMOVAL	EACH	24		24
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.2	0.8
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
X0326133	TEMPORARY WOOD POLE, .45 FT. CLASS 5	EACH	1	1	
* X0326135	ELECTRIC SERVICE DISCONNECT, LIGHTING AND TRAFFIC SIGNAL	EACH	1	1	
* X0326170	AERIAL CABLE, 3-1/8 NO. 4, ALUMINUM, WITH MESSENGER WIRE	FOOT	1,150	1,150	

* SPECIALTY ITEM

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PLOT DATE = 7/9/2008	DATE - 07/09/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
SUMMARY OF QUANTITIES**

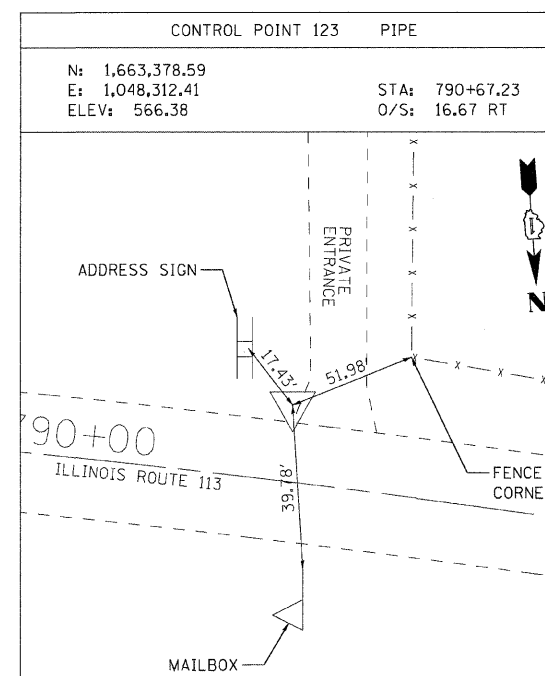
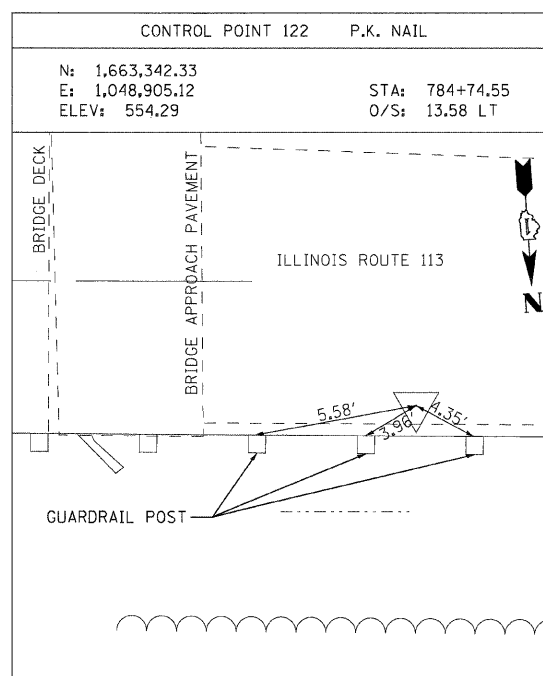
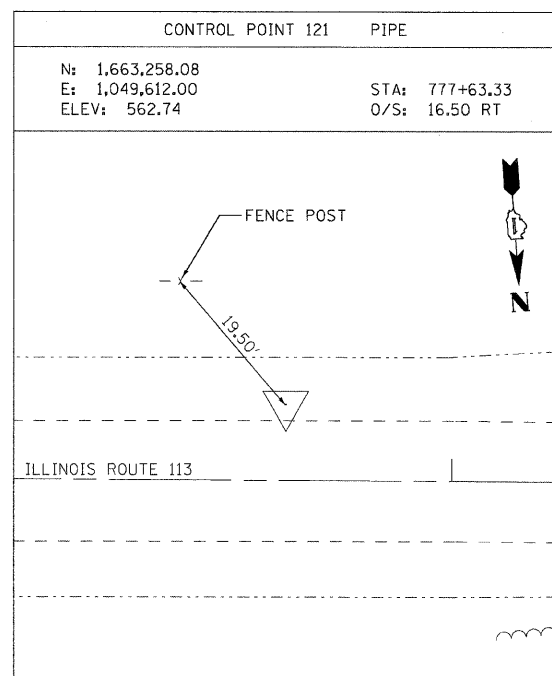
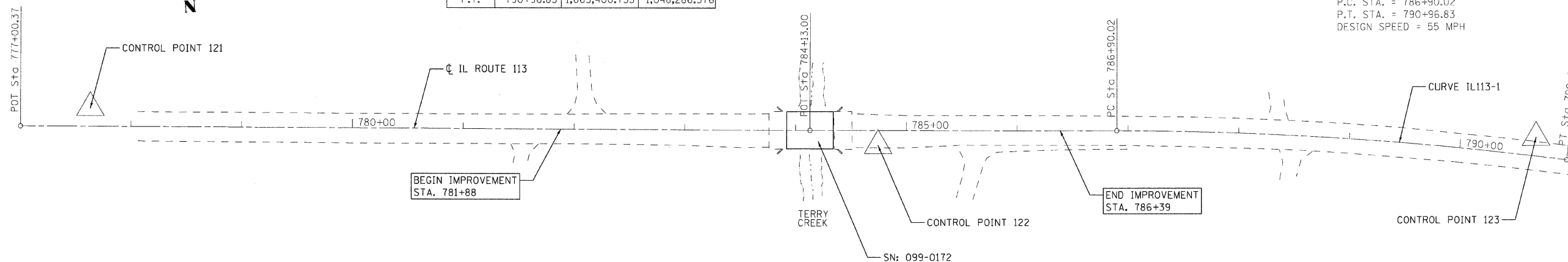
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FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 60D86	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

PROJECT COORDINATES

ILLINOIS ROUTE 113 CENTERLINE			
DESCRIPTION	NORTHING	EASTING	
P.O.T.	777+00.37	1,663,269.717	1,049,676.034
P.O.T.	784+13.00	1,663,324.216	1,048,965.490
P.C.	786+90.02	1,663,344.790	1,048,689.231
P.T.	790+96.83	1,663,400.753	1,048,286.576

EXIST. CURVE IL113-1
 PI STA. = 788+93.70
 $\Delta = 7^\circ 18' 25''$ (RT)
 $D = 1^\circ 47' 46''$
 $R = 3,189.82'$
 $T = 203.68'$
 $L = 406.80'$
 $E = 6.50'$
 P.C. STA. = 786+90.02
 P.T. STA. = 790+96.83
 DESIGN SPEED = 55 MPH



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	DATE - 07/09/2008	REVISED -




**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

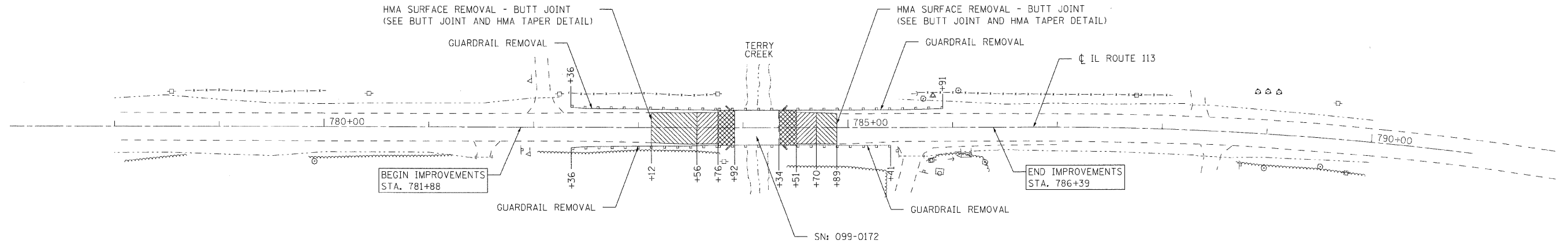
**FAS 1317 / ILLINOIS ROUTE 113
 OVER TERRY CREEK
 ALIGNMENT AND CONTROL POINTS**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	4
CONTRACT NO. 60D86			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

LEGEND

-  APPROACH SLAB REMOVAL
-  PAVEMENT REMOVAL
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"



NOTE: THE EXISTING PAVEMENT CONSISTS OF 4 INCHES OF HOT-MIX ASPHALT OVER 9 INCHES OF PCC BASE COURSE.

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

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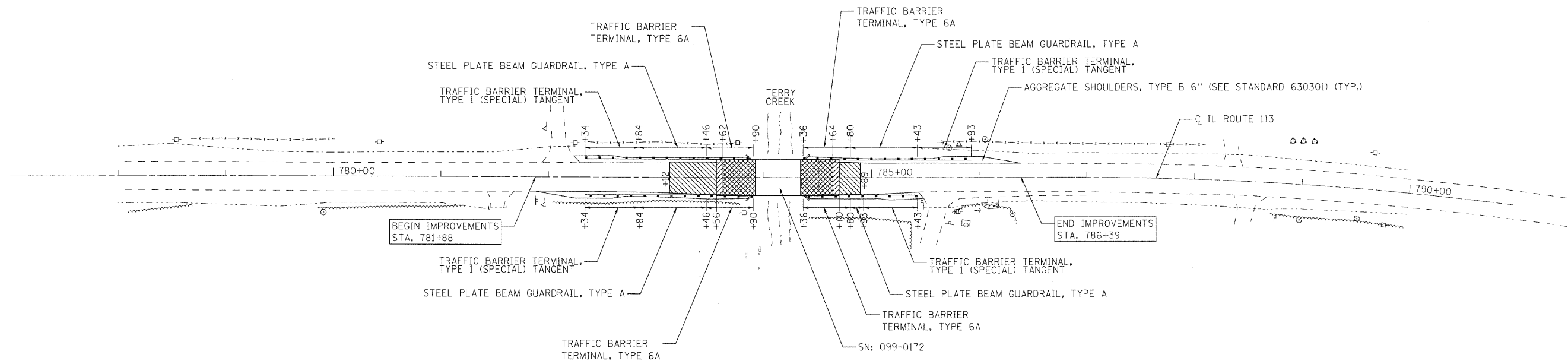
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAS 1317 / ILLINOIS ROUTE 113
 OVER TERRY CREEK
 EXISTING CONDITIONS & REMOVAL PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.S. RTE. 1317	SECTION 111 B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 5
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 60D86				

- LEGEND**
-  SUB-BASE GRANULAR MATERIAL, TYPE A 4"
 -  BRIDGE APPROACH PAVEMENT (SEE STANDARD 420401)
 -  BRIDGE APPROACH CONNECTOR (FLEXIBLE):
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5), 2"
HOT-MIX ASPHALT BINDER COURSE, IL 19.0 mm, N70, 10" TO 13" (IN 4 LIFTS)
 -  ROADWAY RESURFACING:
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5), 1 1/2"
LEVELING BINDER (MACHINE METHOD), N70, 3/4" MIN.



- NOTES:**
- EARTH EXCAVATION QUANTITIES HAVE BEEN INCLUDED FOR THE FOLLOWING LOCATIONS:

FOR BRIDGE APPROACH PAVEMENT AND BRIDGE APPROACH CONNECTOR (FLEXIBLE)	140 CU YD
FOR AGGREGATE SHOULDERS, TYPE B	55 CU YD
 - SEE SHEET NO. 16 FOR PROFILE GRADE

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	DATE - 07/09/2008	REVISED -









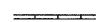




**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAS 1317 / ILLINOIS ROUTE 113
 OVER TERRY CREEK
 PROPOSED ROADWAY PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	6
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D86	

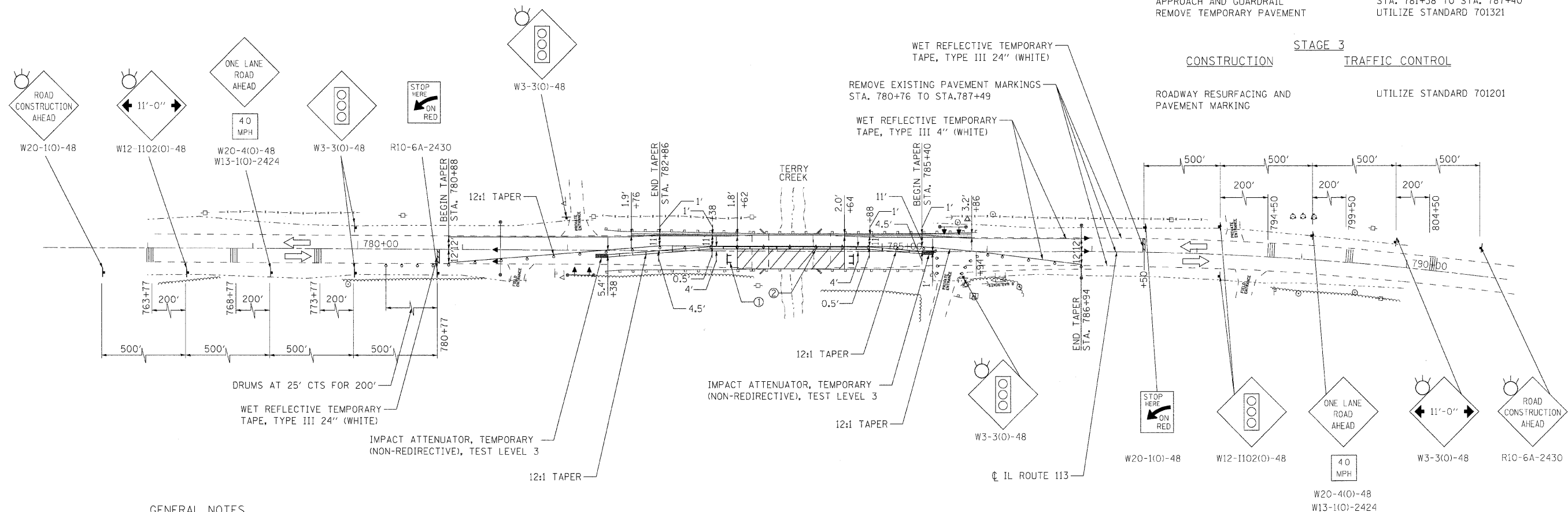
SYMBOLS

-  Work area
-  Sign
-  Type III barricade
-  Traffic signal
-  Detector loops
-  Impact attenuator
-  Non-directional flashing beacon
-  Drum with steady burning light 25' cts.
-  Temporary concrete barrier
-  Temporary rumble strip (when specified)
-  Double vertical panel (see detail)
-  Crystal, bidirectional barrier wall/guardrail marker
-  Temporary Pavement:
Hot-mix asphalt surface course, mix "C", N50 1 1/2"
Hot-mix asphalt binder course, IL-19.0, N50 8 1/2" (in 4 lifts)

- ① Type III barricade to be placed when no work is being performed.
- ② Barrier wall/guardrail markers at 7.6 m (25') cts. See Standards 704001 & 635011.

CONSTRUCTION SEQUENCE

PRESTAGE	
CONSTRUCTION TEMPORARY PAVEMENT	TRAFFIC CONTROL UTILIZE STANDARD 701201
STAGE 1	
CONSTRUCTION WESTBOUND DECK BEAM REPLACEMENT, BRIDGE APPROACH AND GUARDRAIL	TRAFFIC CONTROL ONE-WAY TRAFFIC ON EASTBOUND LANE FROM STA. 780+88 TO STA. 786+90 UTILIZE STANDARD 701321
STAGE 2	
CONSTRUCTION EASTBOUND DECK BEAM REPLACEMENT, BRIDGE APPROACH AND GUARDRAIL REMOVE TEMPORARY PAVEMENT	TRAFFIC CONTROL ONE-WAY TRAFFIC ON WESTBOUND LANE FROM STA. 781+38 TO STA. 787+40 UTILIZE STANDARD 701321
STAGE 3	
CONSTRUCTION ROADWAY RESURFACING AND PAVEMENT MARKING	TRAFFIC CONTROL UTILIZE STANDARD 701201



GENERAL NOTES

Traffic signals shall be operational only when all traffic controls are in place. When traffic signals are not in operation, flaggers shall be used and traffic control shall conform to Standard 701201.

Temporary concrete barrier shall be according to Standard 704001.

When existing pavement markings and raised pavement markers are in conflict with the traffic control and protection plan, existing pavement marking and raised pavement markers shall be removed and paid for as pavement marking removal or as raised reflective pavement marking removal.

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Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = jjolesman	DESIGNED - JCC	REVISED -
PLDT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLDT DATE = 7/8/2008	CHECKED - MJL	REVISED -
	DATE - 07/09/2008	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TRAFFIC CONTROL AND PROTECTION - STAGE 1

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.S. RTE. 1317	SECTION 111 B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 7
CONTRACT NO. 60D86			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

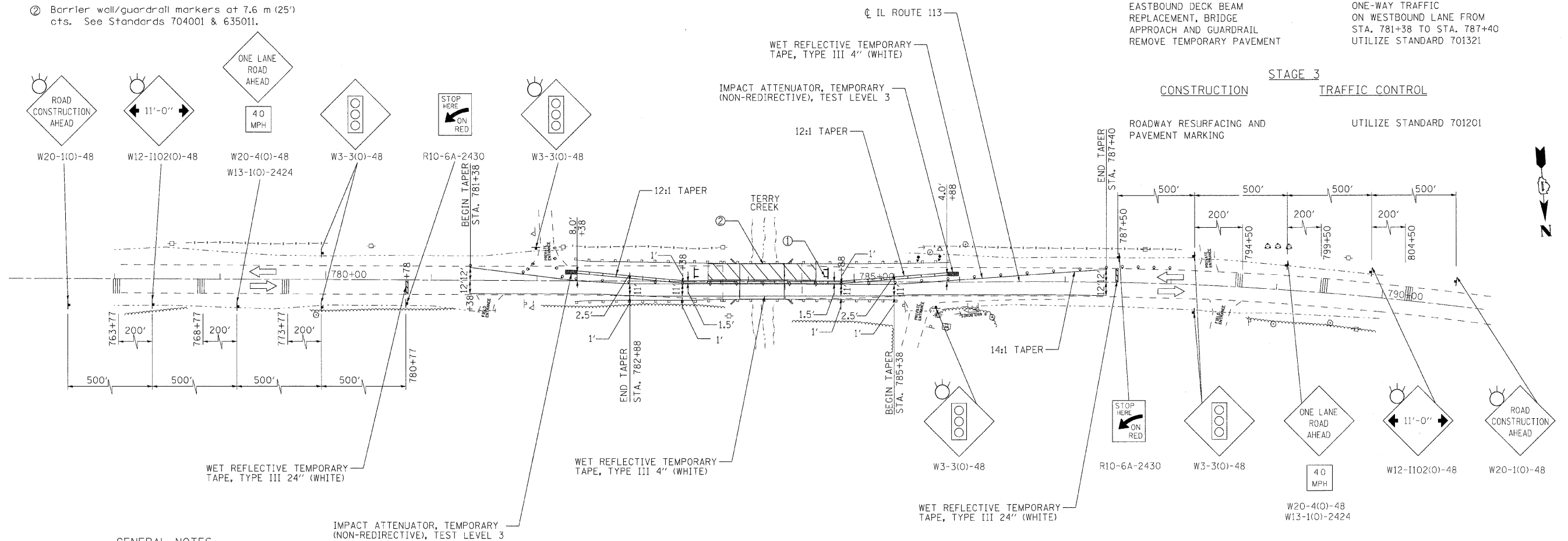
SYMBOLS

- Work area
- Sign
- Type III barricade
- Traffic signal
- Detector loops
- Impact attenuator
- Non-directional flashing beacon
- Drum with steady burning light 25' cts.
- Temporary concrete barrier
- Temporary rumble strip (when specified)
- Double vertical panel (see detail)
- Crystal, bidirectional barrier wall/guardrail marker
- Temporary Pavement:
Hot-mix asphalt surface course, mix "C", N50 1 1/2"
Hot-mix asphalt binder course, IL-19.0, N50 8 1/2" (in 4 lifts)

- ① Type III barricade to be placed when no work is being performed.
- ② Barrier wall/guardrail markers at 7.6 m (25') cts. See Standards 704001 & 635011.

CONSTRUCTION SEQUENCE

PRESTAGE	
CONSTRUCTION	TRAFFIC CONTROL
TEMPORARY PAVEMENT	UTILIZE STANDARD 701201
STAGE 1	
CONSTRUCTION	TRAFFIC CONTROL
WESTBOUND DECK BEAM REPLACEMENT, BRIDGE APPROACH AND GUARDRAIL	ONE-WAY TRAFFIC ON EASTBOUND LANE FROM STA. 780+88 TO STA. 786+40 UTILIZE STANDARD 701321
STAGE 2	
CONSTRUCTION	TRAFFIC CONTROL
EASTBOUND DECK BEAM REPLACEMENT, BRIDGE APPROACH AND GUARDRAIL REMOVE TEMPORARY PAVEMENT	ONE-WAY TRAFFIC ON WESTBOUND LANE FROM STA. 781+38 TO STA. 787+40 UTILIZE STANDARD 701321
STAGE 3	
CONSTRUCTION	TRAFFIC CONTROL
ROADWAY RESURFACING AND PAVEMENT MARKING	UTILIZE STANDARD 701201



GENERAL NOTES

Traffic signals shall be operational only when all traffic controls are in place. When traffic signals are not in operation, flaggers shall be used and traffic control shall conform to Standard 701201.

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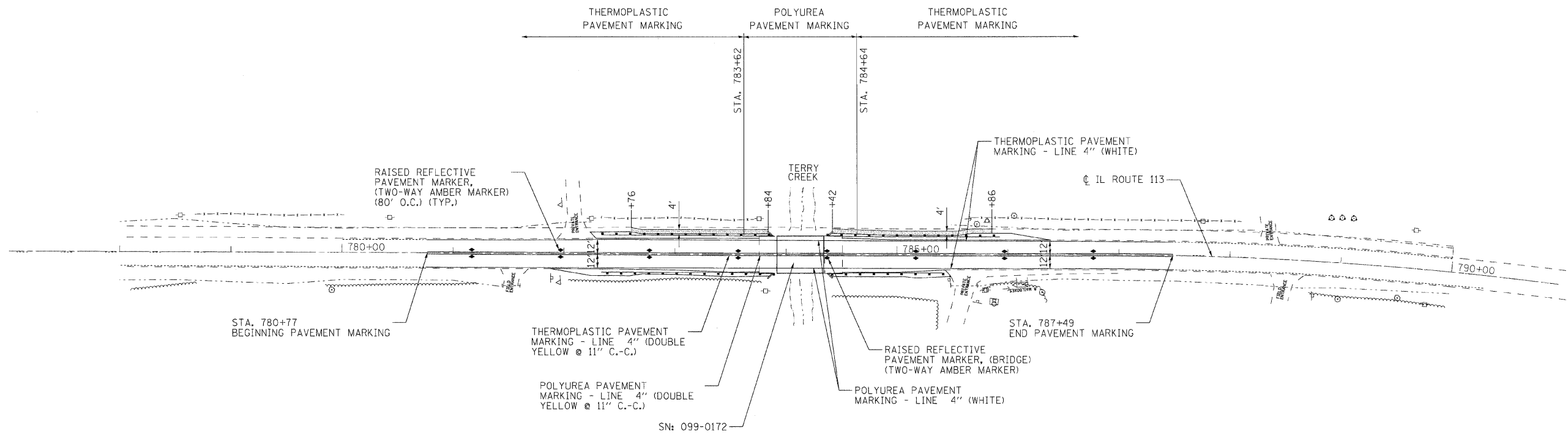
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PLOT DATE = 7/8/2008	CHECKED - MJL	REVISED -
	DATE - 07/09/2008	REVISED -


**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TRAFFIC AND PROTECTION - STAGE 2**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.S. RTE. 1317	SECTION 111 B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 8
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D86	




 TOPSOIL FURNISH AND PLACE, 4"
 SEEDING CLASS 2A
 EROSION CONTROL BLANKET

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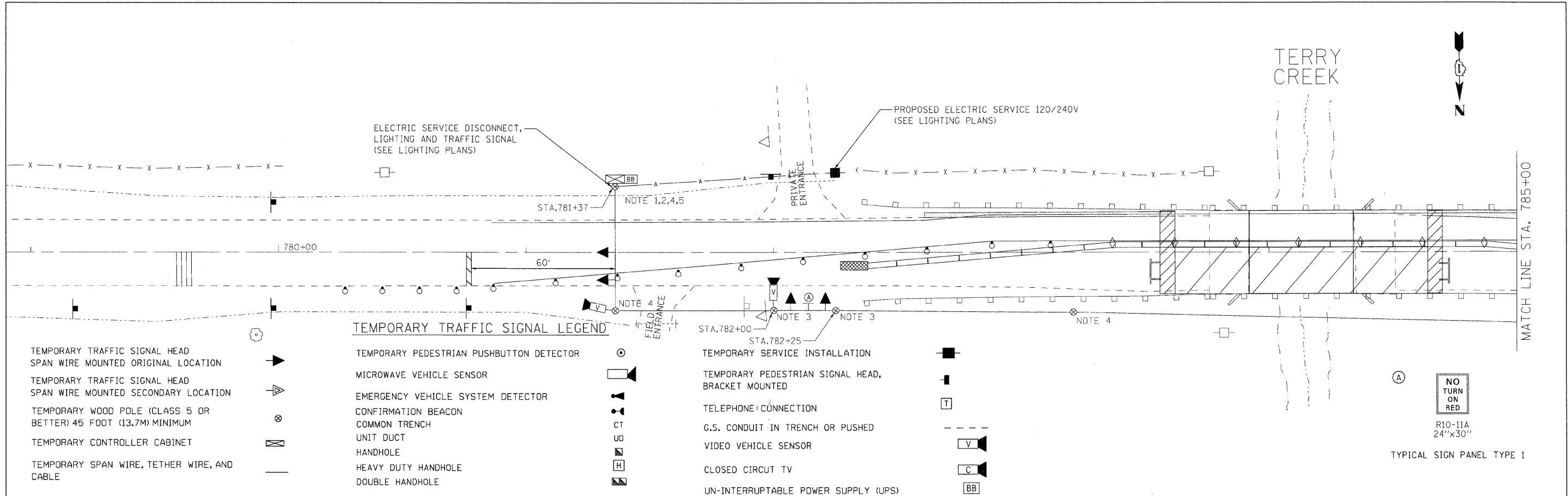
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PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 7/8/2008	CHECKED - MJL	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
PAVEMENT MARKING & LANDSCAPING PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

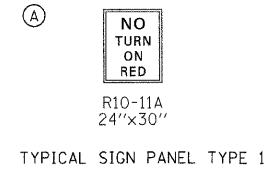
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	9
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D86	



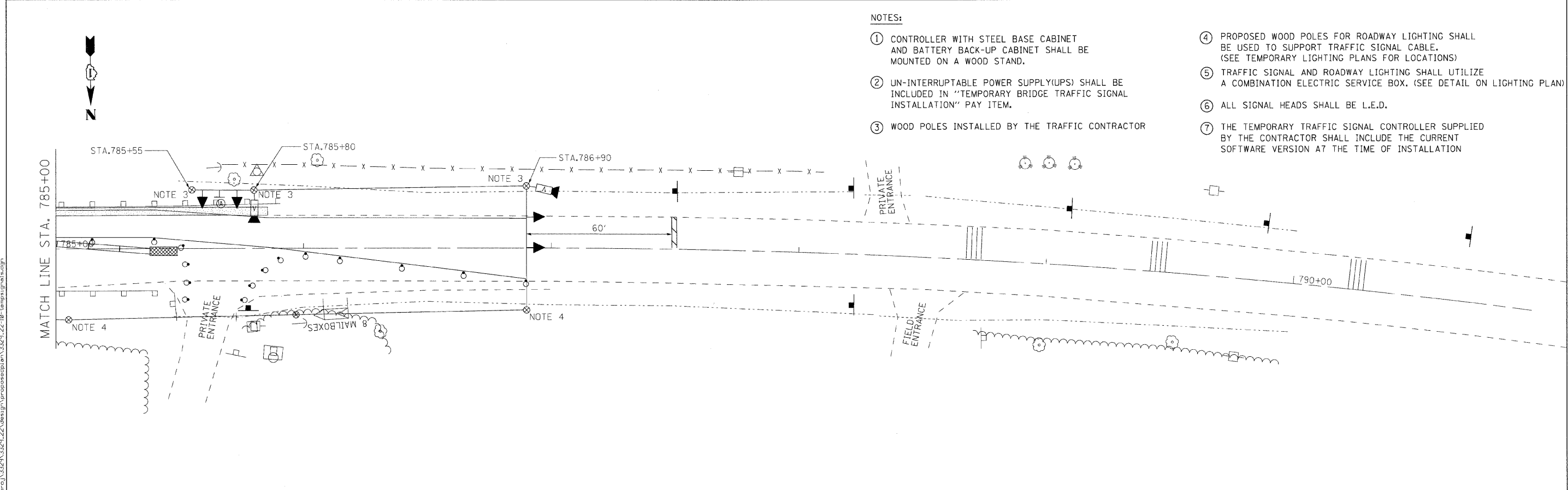
- TEMPORARY TRAFFIC SIGNAL LEGEND**
- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
 - TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
 - TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7M) MINIMUM
 - TEMPORARY CONTROLLER CABINET
 - TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE

- TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR
- MICROWAVE VEHICLE SENSOR
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- COMMON TRENCH
- UNIT DUCT
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE

- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- TELEPHONE / CONNECTION
- G.S. CONDUIT IN TRENCH OR PUSHED
- VIDEO VEHICLE SENSOR
- CLOSED CIRCUIT TV
- UN-INTERRUPTABLE POWER SUPPLY (UPS)



- NOTES:**
- 1 CONTROLLER WITH STEEL BASE CABINET AND BATTERY BACK-UP CABINET SHALL BE MOUNTED ON A WOOD STAND.
 - 2 UN-INTERRUPTABLE POWER SUPPLY(UPS) SHALL BE INCLUDED IN "TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION" PAY ITEM.
 - 3 WOOD POLES INSTALLED BY THE TRAFFIC CONTRACTOR
 - 4 PROPOSED WOOD POLES FOR ROADWAY LIGHTING SHALL BE USED TO SUPPORT TRAFFIC SIGNAL CABLE. (SEE TEMPORARY LIGHTING PLANS FOR LOCATIONS)
 - 5 TRAFFIC SIGNAL AND ROADWAY LIGHTING SHALL UTILIZE A COMBINATION ELECTRIC SERVICE BOX. (SEE DETAIL ON LIGHTING PLAN)
 - 6 ALL SIGNAL HEADS SHALL BE L.E.D.
 - 7 THE TEMPORARY TRAFFIC SIGNAL CONTROLLER SUPPLIED BY THE CONTRACTOR SHALL INCLUDE THE CURRENT SOFTWARE VERSION AT THE TIME OF INSTALLATION



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USER NAME = jcoleman	DESIGNED -	REVISED -
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PLDT DATE = 7/8/2008	CHECKED -	REVISED -
	DATE = 07/09/2008	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TEMPORARY TRAFFIC SIGNAL PLAN

SCALE: 1"=20'

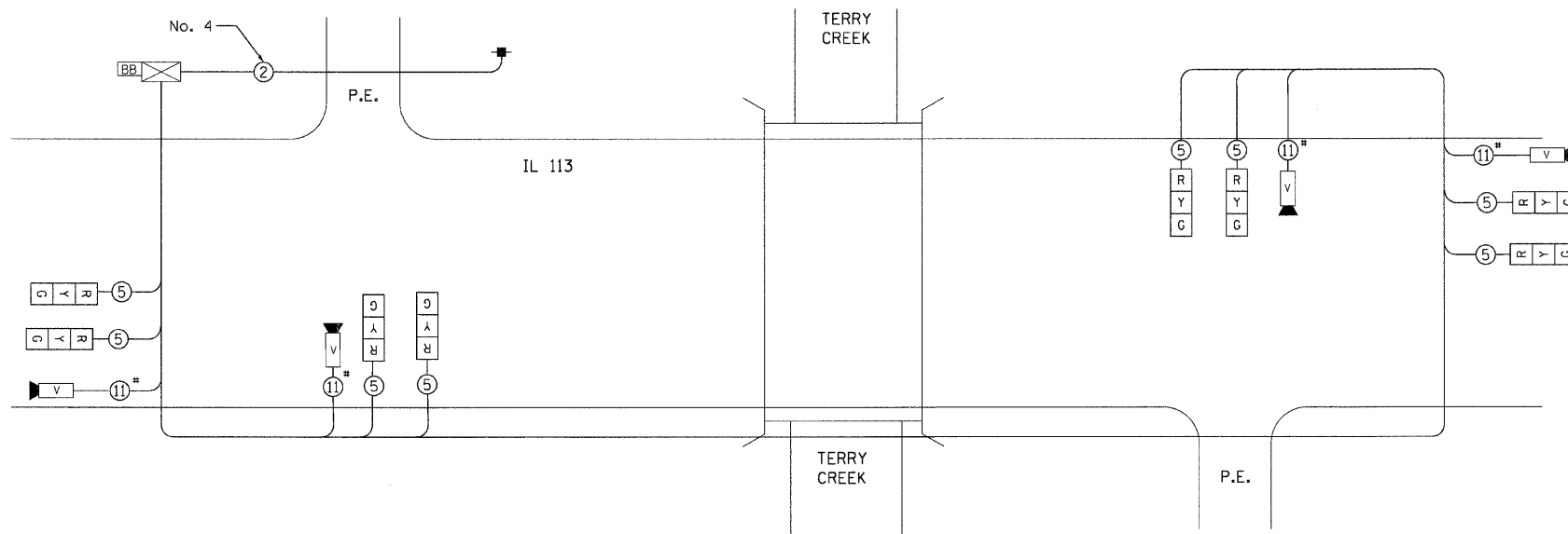
SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.S. RTE. 1317	SECTION III B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 10
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 60D86				

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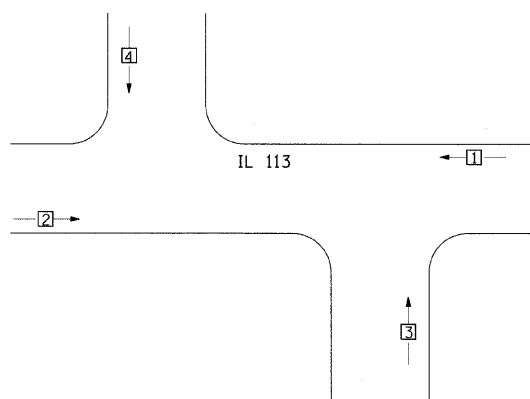
NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR, EVP WILL BE PAID FOR SEPARATELY.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET, ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT. A REPRESENTATIVE OF THE TRAFFIC SIGNAL CONTROLLER/CABINET VENDOR/SUPPLIER MUST BE PRESENT AT THE TRAFFIC SIGNAL TURN ON.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm), HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATED HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS, EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 24" WHITE STOP BAR TO BE INSTALLED AFTER THE INSTALLATION AND IMPLEMENTATION OF THE TEMPORARY TRAFFIC SIGNALS.
- THE VIDEO CAMERA VENDOR/SUPPLIER REPRESENTATIVE WILL ASSIST THE CONTRACTOR IN THE EQUIPMENT SETUP/PLACEMENT OF CAMERAS AND WILL BE PRESENT AT THE TRAFFIC SIGNAL TURN ON.



NOTE: OR AS SPECIFIED BY CAMERA VENDOR

CONTROLLER SEQUENCE



PHASE DESIGNATION

- LEGEND**
- SINGLE ENTRY PHASE
 - ◇ OVERLAP
 - PEDESTRIAN PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE

SUMMARY OF QUANTITIES			
ITEM	DESCRIPTION	UNIT	QUANTITY
X8900005	TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION	EACH	1

TEMPORARY CABLE DIAGRAM LEGEND

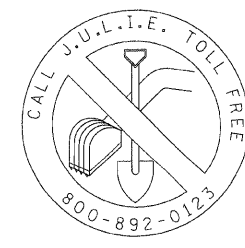
- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- ⑤ INDICATES NUMBER OF CONDUCTORS IN CABLE, ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- ▶ EMERGENCY VEHICLE LIGHT DETECTOR
- ◀ CONFIRMATION BEACON
- PEDESTRIAN PUSHBUTTON DETECTOR
- VEHICLE DETECTOR, INDUCTION LOOP
- ⊠ 12" (300mm) PEDESTRIAN SIGNAL SECTION
- ⊠ MICROWAVE VEHICLE SENSOR
- ⊠ VIDEO DETECTOR SENSOR
- ⊠ CLOSED CIRCUIT TV
- ⊠ BATTERY - BACK UP
- ⊠ TELEPHONE CONNECTION

NOTE:
EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	WATTAGE (LED)	% OPERATION	
SIGNAL (RED)	8	135	17	0.50	68
(YELLOW)	8	135	25	0.25	50
(GREEN)	8	135	15	0.25	30
ARROW		135	12	0.10	
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		84		0.05	
VIDEO CAMERA	4		45	1.0	180
ENERGY COSTS TO:					TOTAL = 428

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: KATHY NYSTROM
PHONE: (847) 816-5489
COMPANY: COM. ED.

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE
E - M. ARM POLE		SIGNAL POST	2 (1.0)	20' ± L-2'
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND
				POST MOUNTED



48 - HOURS BEFORE DIGGING

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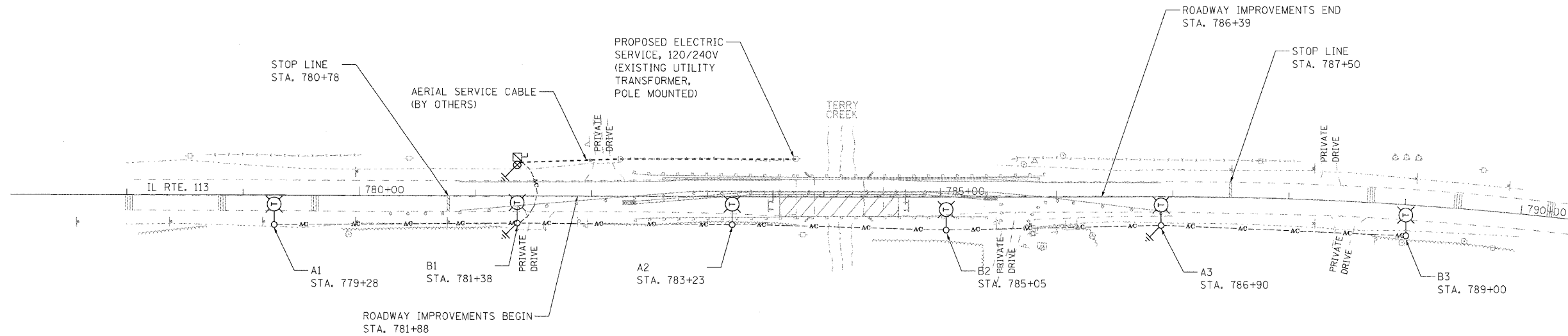
USER NAME	DESIGNED	REVISIONS
* joolemon	- RBG	-
	DRAWN - JAG	REVISED -
	CHECKED - MJL	REVISED -
	DATE - 07/09/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**







**FAS 1317 /ILLINOIS ROUTE 113
OVER TERRY CREEK
TEMPORARY CABLE PLAN & TEMPORARY PHASE DESIGNATION DIAGRAM**

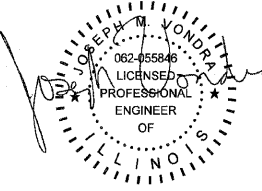
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	11
CONTRACT NO. 60D86				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

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LEGEND:

-  TEMPORARY LIGHTING UNIT
50 FT. MOUNTING HEIGHT, 15 FT. MAST ARM
400 WATT, 120 VOLT LUMINAIRE WITH PHOTOCELL
ON EACH LUMINAIRE
-  AERIAL CABLE, 3-1/8" NO. 4, ALUMINUM, WITH MESSENGER WIRE
-  GROUND ROD, 5/8" DIAMETER X 10 FT.
-  ELECTRIC SERVICE DISCONNECT, LIGHTING
AND TRAFFIC SIGNAL
-  EXISTING UTILITY POLE
-  TEMPORARY WOOD POLE, 45 FT. CLASS 5


 DATE: 7/8/2008
 SEAL EXPIRES: 11/30/2009

NOTES

1. TEMPORARY LIGHTING SHALL BE INSTALLED AND OPERATIONAL BEFORE THE START OF CONSTRUCTION.
2. POLE SHALL BE SET BACK 18 FT. FROM THE EDGE OF THE TRAVELLED PAVEMENT, UNLESS OTHERWISE REQUIRED BY THE FIELD CONDITIONS.
3. TEMPORARY LIGHTING SHALL REMAIN OPERATIONAL FOR BOTH STAGE I AND STAGE II. STAGE I SHOWN.
4. ALL POLES SHOWN ON THIS PLAN SHALL BE INSTALLED BY THE LIGHTING CONTRACTOR.

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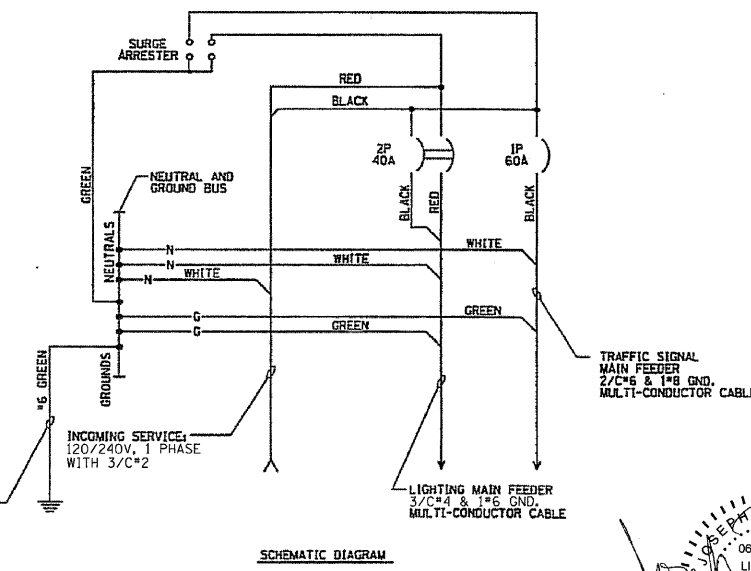
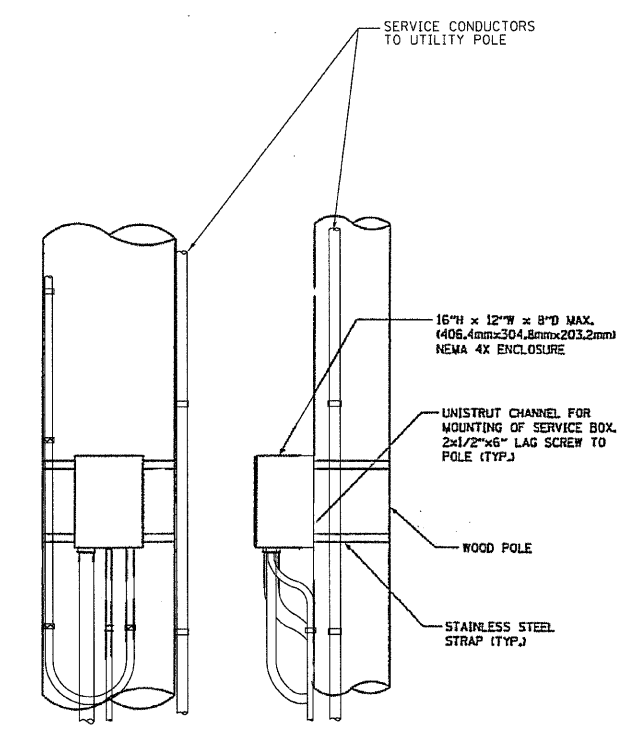
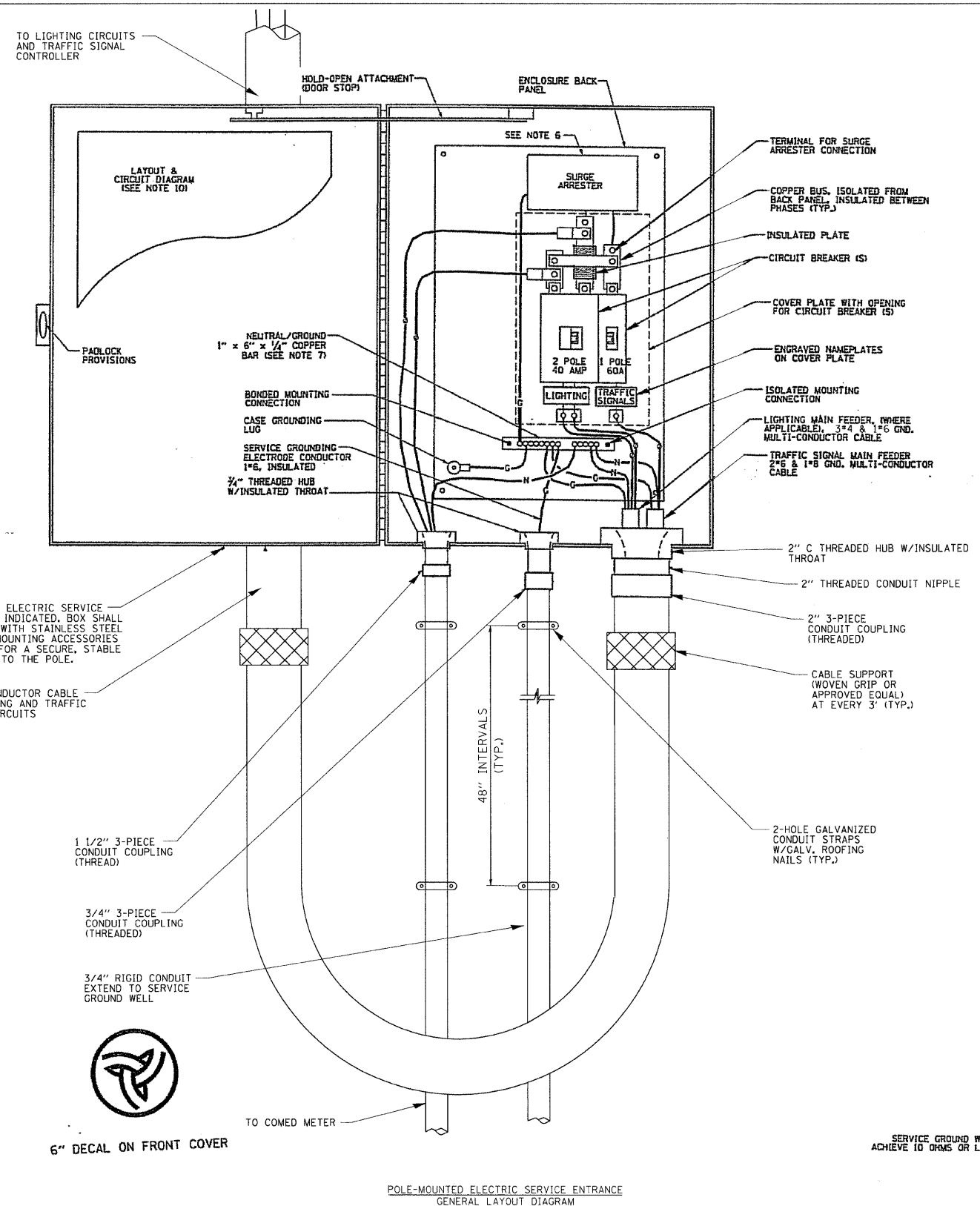
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PLOT DATE = 7/8/2008	CHECKED - JMV	REVISED -
	DATE - 07/09/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

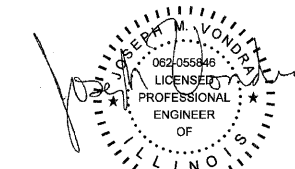
**FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TEMPORARY LIGHTING PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.S. RTE. 1317	SECTION III B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 12
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT CONTRACT NO. 60DB6		



- NOTES:**
- ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED OR DESIGNATED BY THE ENGINEER, AND SERVICE DROP CABLE SHALL BE COMPATIBLE WITH THE SERVICE ACCORDINGLY. SOME INSTALLATIONS MAY CALL FOR SERVICE ENTRANCE EQUIPMENT SUITABLE FOR 3-WIRE SERVICE EVEN THOUGH INITIALLY WIRED FOR 2-WIRE SERVICE.
 - THE POLE-MOUNTED ELECTRIC SERVICE BOX DETAIL DEPICTS THE BASIC CONSTRUCTION OF THE EQUIPMENT. SLIGHT MODIFICATIONS APPLY FOR DIFFERING SERVICES AND APPLICATIONS AS FOLLOWS:
 - TYPE A FULLY EQUIPPED FOR 240/120V, 3W SERVICE, COMPLETE WITH LIGHTING MAIN BREAKER
 - TYPE A1 FULLY EQUIPPED FOR 240/120V, 3W SERVICE, BLANK COVER IN LIEU OF LIGHTING MAIN BREAKER
 - TYPE B EQUIPPED FOR 120V, SERVICE, COMPLETE WITH 1P, 60A, TRAFFIC SIGNALS MAIN BREAKER
 - TYPE B1 EQUIPPED FOR 120V, SERVICE, COMPLETE WITH 1P, 40A, TRAFFIC SURVEILLANCE MAIN BREAKER
 - THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LISTED AS SUITABLE FOR USE AS SERVICE ENTRANCE EQUIPMENT.
 - THE ELECTRIC SERVICE EQUIPMENT ENCLOSURE SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12" W X 16" H X 8" D, WITH A PIANO-HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS AND DOOR STOP, HOFFMAN CATALOG NO. A-16H120SS56P/A-16 P12/A-DSTOP/C-PW12, OR APPROVED EQUAL.
 - CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
 - THE SURGE PROTECTOR SHALL BE SUITABLE FOR 240/120 VOLT SINGLE PHASE AC ELECTRICAL SERVICE WITH A SURGE ENERGY CAPABILITY OF 2160 JOULES OR BETTER AT 8/20 MICRO-SECONDS, RATED -40 TO 60 DEGREES C, WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CM0V230L065XST OR APPROVED EQUAL.
 - BUS BARS, CONNECTORS, AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED, AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS, OR THE ASSEMBLY SHALL BE A MANUFACTURED SPECIALTY PANELBOARD, CUTLER-HAMMER PRL2A OR APPROVED EQUAL.
 - THE COMBINATION GROUND AND NEUTRAL BAR SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN, THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE, THE SERVICE NEUTRAL AND SERVICE GROUNDING ELECTRODE CONDUCTOR SHALL BE TERMINATED ADJACENT TO EACH OTHER AT THE DIVIDE BETWEEN THE SECTIONS AND WIRING SHALL BE TERMINATED ONLY UPON THE APPROPRIATE SECTION.
 - THE WIRING TERMINALS, INCLUDING THE GROUND/NEUTRAL BAR SHALL BE ARRANGED TO PROVIDE ADEQUATE ROOM FOR PERFORMING FIELD TERMINATIONS.
 - A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE MECHANICALLY SECURED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
 - A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
 - LUGS AND CONNECTORS SHALL BE RATED FOR 75°C CONDUCTOR.
 - THE EXACT MOUNTING HEIGHT OF THE BOX SHALL BE FIELD DETERMINED TO AVOID OBSTRUCTIONS AND PUBLIC ACCESS. TYPICAL HEIGHT SHALL BE APPROXIMATELY 10 FEET ABOVE GRADE.



DATE: 7/8/2008
SEAL EXPIRES: 11/30/2009

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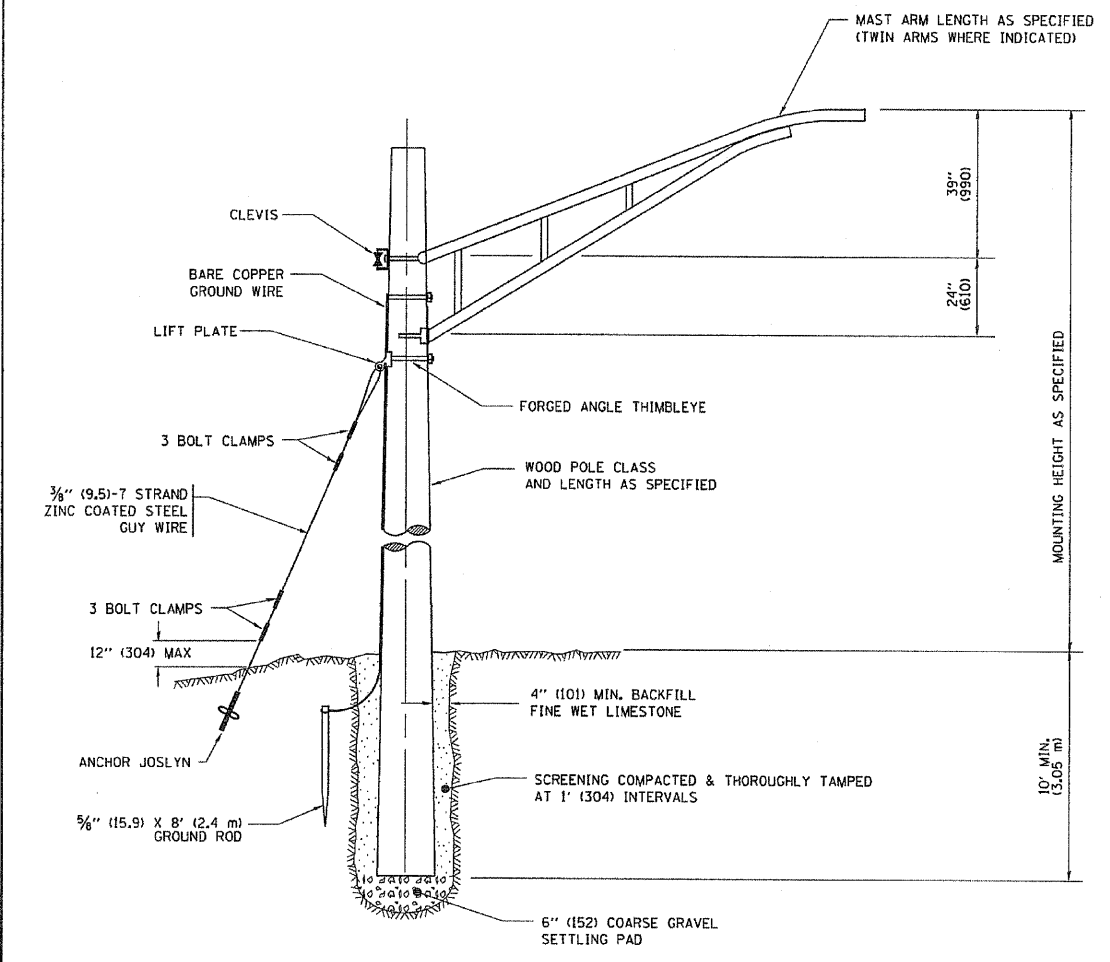
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	DRAWN - MB	REVISED -
PLOT SCALE = 1:8000 1/4" IN.	CHECKED - JMV	REVISED -
PLOT DATE = 7/8/2008	DATE - 07/09/2008	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

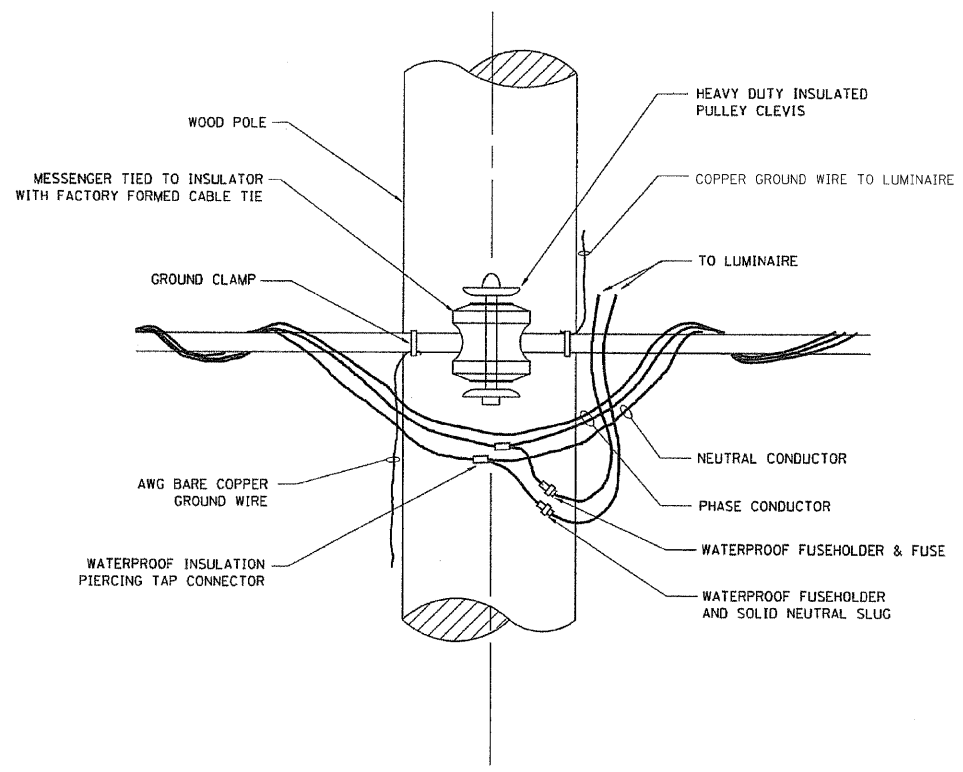
FAS 1317 / ILLINOIS ROUTE 113 OVER TERRY CREEK
ELECTRIC SERVICE DISCONNECT,
LIGHTING AND TRAFFIC SIGNAL
SCALE: NONE SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.S. RTE. 1317	SECTION 111 B-2	COUNTY WILL	TOTAL SHEETS 32	SHEET NO. 13
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 60D86				

CONTRACT NO.			
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

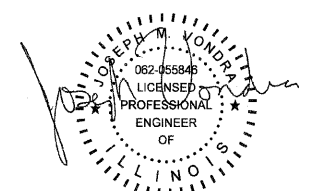


TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

NOTES:
1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED



DATE: 7/8/2008
SEAL EXPIRES: 11/30/2009

REVISIONS	
NAME	DATE
	08/08/03

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY LIGHT POLE DETAILS
SCALE: VERT. NONE
HORIZ. DRAWN BY
CHECKED BY
BE-800

FILE NAME = m:\proj\3329\3329_22\design\lighting\3329_22-1.tg03.dwg
PLOT DATE = 4/18/2007
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

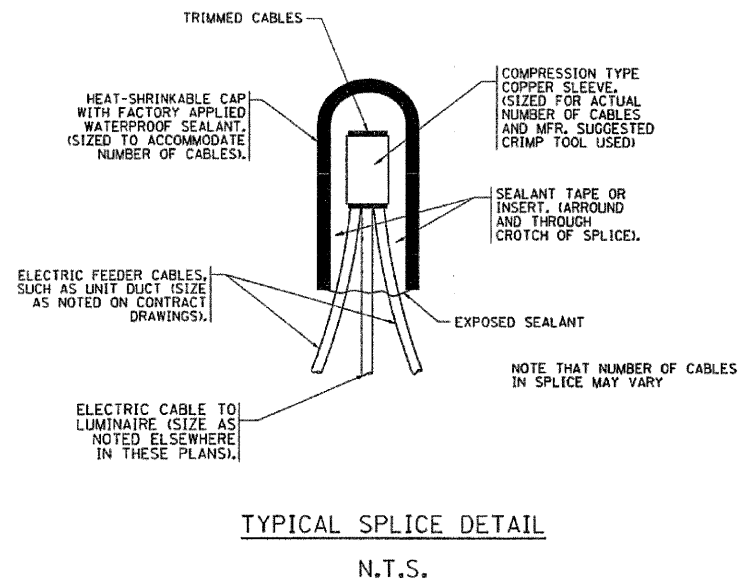
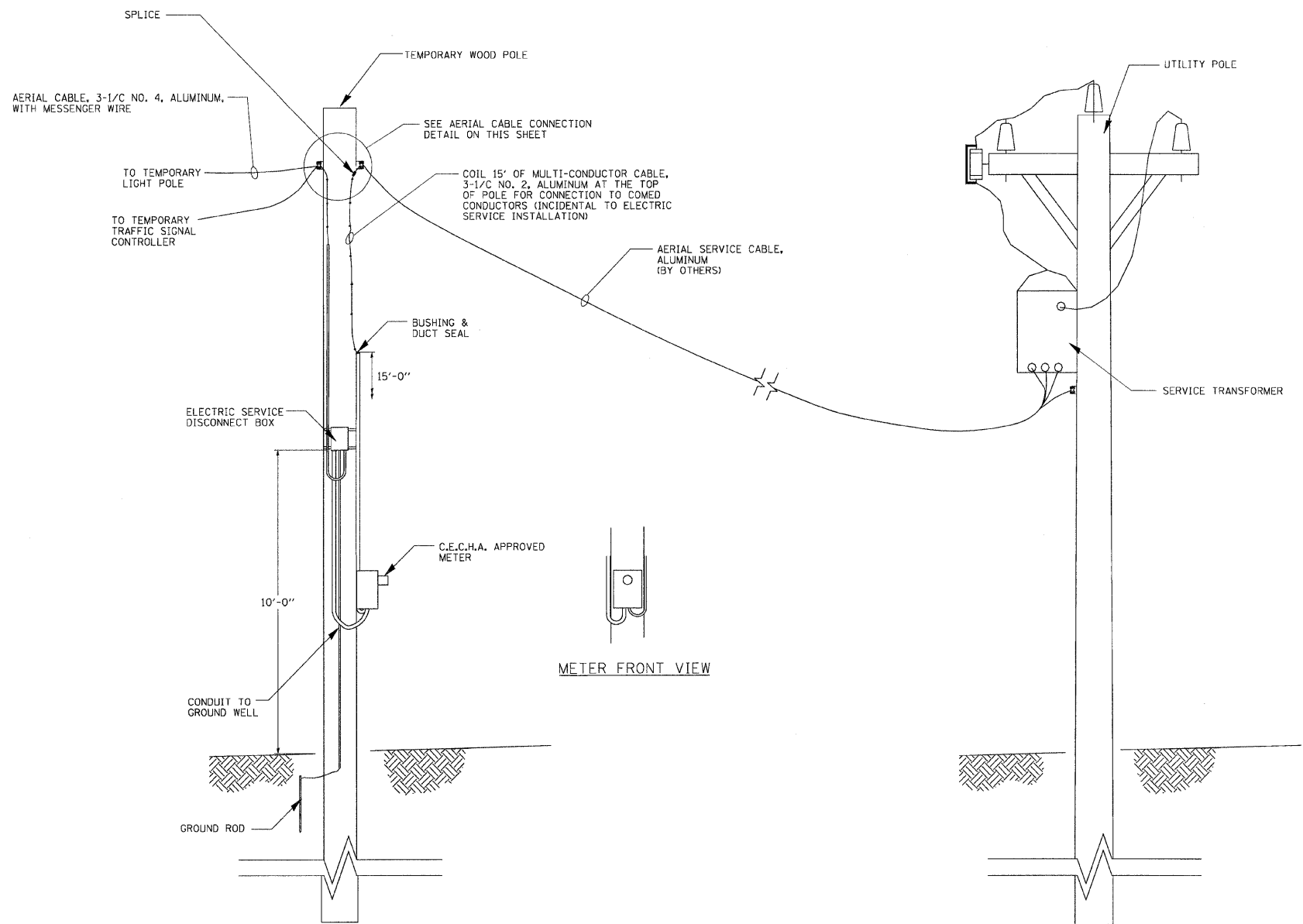
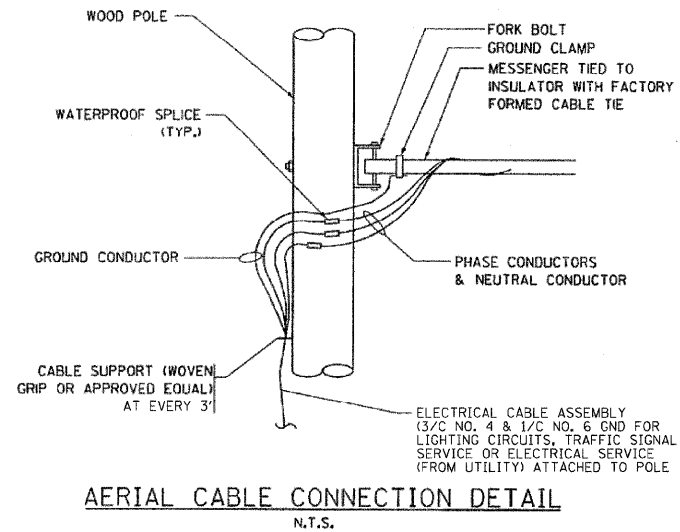
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	DRAWN - MB	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TEMPORARY LIGHTING DETAILS

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	14
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT
			CONTRACT NO. 60DB6	



DATE: 7/15/2008
 SEAL EXPIRES: 11/30/2009

NOTE:
COMMONWEALTH EDISON SHALL BE CONTACTED BEFORE THE INSTALLATION WORK BEGINS FOR THE ELECTRIC SERVICE INSTALLATION.

(7-17-08)

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 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = jvondol	DESIGNED - JMV	REVISED -
	DRAWN - MB	REVISED -
PLOT SCALE = 1:2000' / IN.	CHECKED - JMV	REVISED -
PLOT DATE = 7/15/2008	DATE - 07/09/2008	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAS 1317 / ILLINOIS ROUTE 113
OVER TERRY CREEK
TEMPORARY LIGHTING DETAILS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	15
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 60D86	

SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

Benchmark: Pipe in NW Shoulder Sta. 784+74.54 Offset 13ft Lt El. 554.29

Existing Structure: SN 099-0172, @ Sta. 784+13 built in 1927 as S.B.I. RT. 113 Section III. Existing R.C. Deck Girder was removed and replaced in 1973 with a single span 21" x 36" PPC Deck Beam Bridge with 4" concrete wearing surface on closed abutments on spread footing. Abutments were widened to accommodate the new Superstructure. The structure measures 43'-0" Bk. to Bk. Abutments and 33'-0" Out to Out Deck. The bridge was rehabilitated in 2000 with two exterior beams replaced. Traffic is to be maintained utilizing stage construction. One lane for both directions will be provided by using temporary traffic signals.

Salvage: None.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

Reinforcement bars shall conform to the requirements of ASTM A706 Grade 60 (IL Modified). See Special Provisions.

The Contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

After the removal of the existing beams for stage I removal, the Contractor shall re-connect or re-engage the transverse ties in the existing beams for stage I traffic.

Burn or cut the existing dowel rods flush with existing bearing seat. Grind the existing dowel rods smooth and seal with epoxy. The cost of this work shall be included with "Removal of Existing Superstructures"

Attach new Name Plate to the inside face of steel rail as shown. Existing name plate is to be removed, cleaned and relocated adjacent to new name plate. Cost included in the cost of Name Plates.

Reinforcement Bars designated (E) shall be epoxy coated.

No in-stream work will be allowed on this project.

The minimum thickness of the concrete overlay shall be 5" and varies as required to adjust for the new profile grade and beam camber.

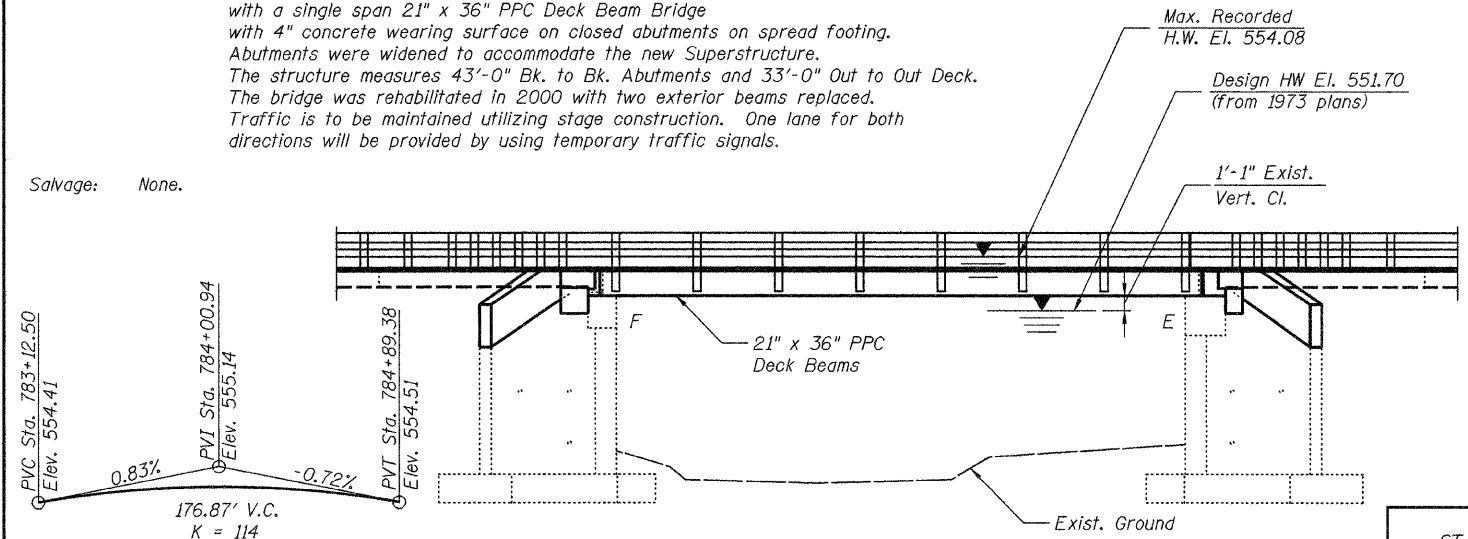
Repair of the substructure shall be completed prior to placement of the new deck beams.

INDEX OF SHEETS

- S-1 General Plan & Elevation
- S-2 Stage Construction Details
- S-3 Temporary Concrete Barrier
- S-4 21"x36" PCC Deck Beam
- S-5 21"x36" PCC Deck Beam Details
- S-6 Superstructure Details 1
- S-7 Superstructure Details 2
- S-8 Steel Railing, Type SM with Concrete Wearing Surface and Curb
- S-9 East Abutment Repairs
- S-10 East Abutment
- S-11 West Abutment Repairs
- S-12 West Abutment
- S-13 Bar Splicer Details

SCOPE OF WORK

1. Total superstructure removal and replacement.
2. Substructure repairs.
3. Approach slab removal and replacement. See Roadway sheets for details.



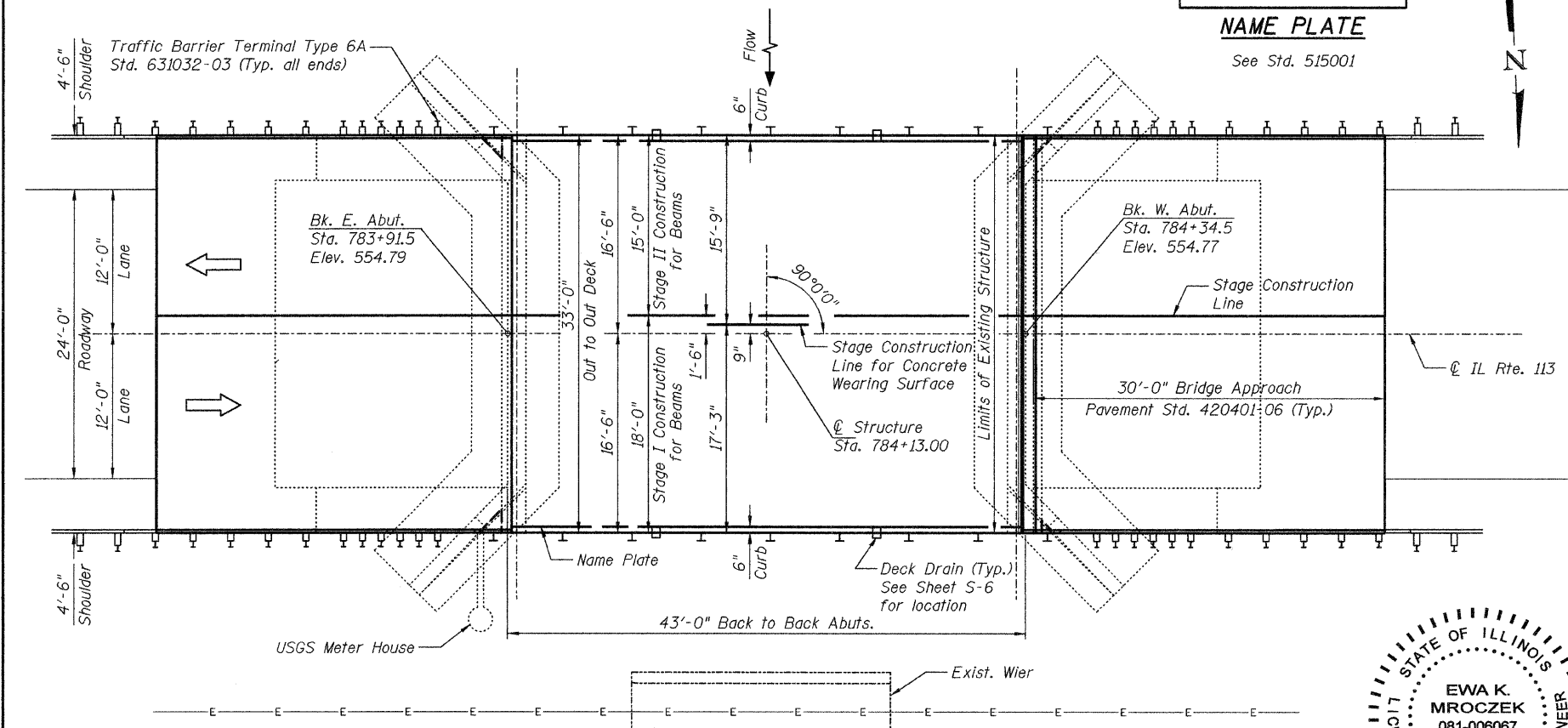
PROPOSED PROFILE

ELEVATION

STATION 784+13.00
REBUILT 20 BY
STATE OF ILLINOIS
IL RT. 113 SEC. 111-B-2
LOADING HL 93
STR. NO. 099-0172

NAME PLATE

See Std. 515001



PLAN

DESIGN STRESSES

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi

PRESTRESSED UNITS
f'c = 6,000 psi
f'ci = 5,000 psi
f's = 270,000 psi (1/2" ϕ low lax, strands)
f'si = 201,900 psi (1/2" ϕ low lax, strands)

LOADING HL - 93

No future wearing surface allowed

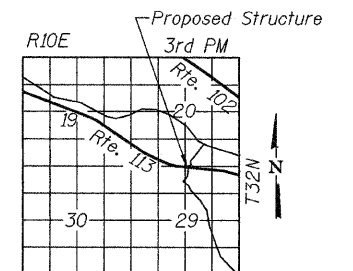
DESIGN SPECIFICATIONS

AASHTO LRFD Bridge Design Specifications
(4th Edition, 2007)

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Removal of Existing Superstructures	Each	1
Concrete Removal	Cu. Yd.	4.0
Concrete Structures	Cu. Yd.	5.0
Concrete Superstructure	Cu. Yd.	5
Bridge Deck Grooving	Sq. Yd.	139
Protective Coat	Sq. Yd.	155
Concrete Wearing Surface	Sq. Yd.	155
Precast Prestressed Concrete Deck Beams (21" Depth)	Sq. Ft.	1,397
Reinforcement Bars, Epoxy Coated	Pound	3,090
Bar Splicers	Each	55
Steel Railing, Type SM	Foot	92
Name Plates	Each	1
Preformed Joint Strip Seal	Foot	33
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	50
Asbestos Bearing Pad Removal	Each	24

* Special Provision



LOCATION SKETCH

GENERAL PLAN AND ELEVATION

FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S-1	1317	111 B-2	WILL	32	16
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		



APPROVED
FOR STRUCTURAL ADEQUACY ONLY

Ralph E. Anderson
ENGINEER OF BRIDGES AND STRUCTURES

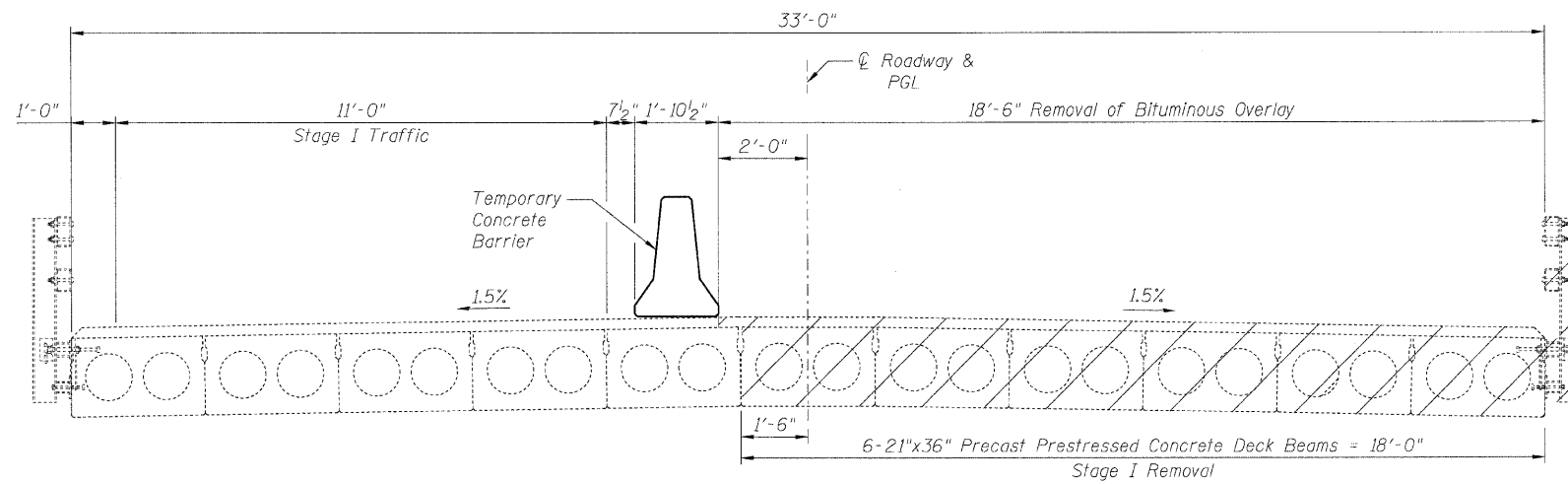
Ewa K. Mroczek
DATE: 7/18/2008
SEAL EXPIRES: 11/30/2008

B. Sauter
E. Mroczek
R. Danley
B. Sauter

CG **Ciorba Group, Inc.**
CONSULTING ENGINEERS
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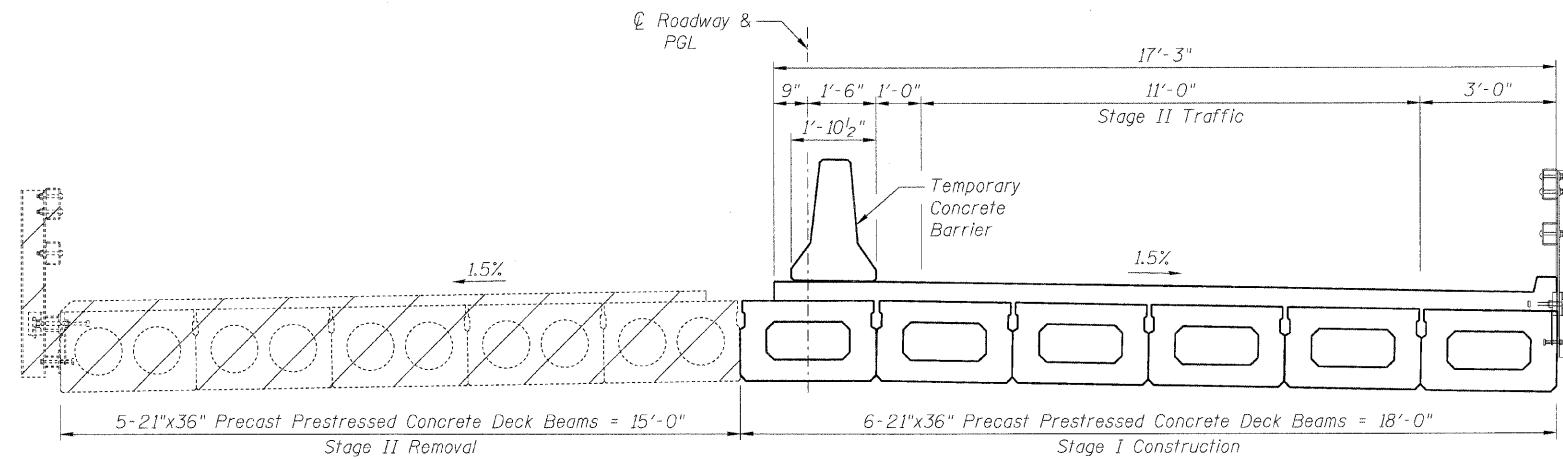
7/18/2008 rdanley N:\PROJ\3329\3329_22\Design\Structural\CAD\Sheet\3329_22_01_GPI.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



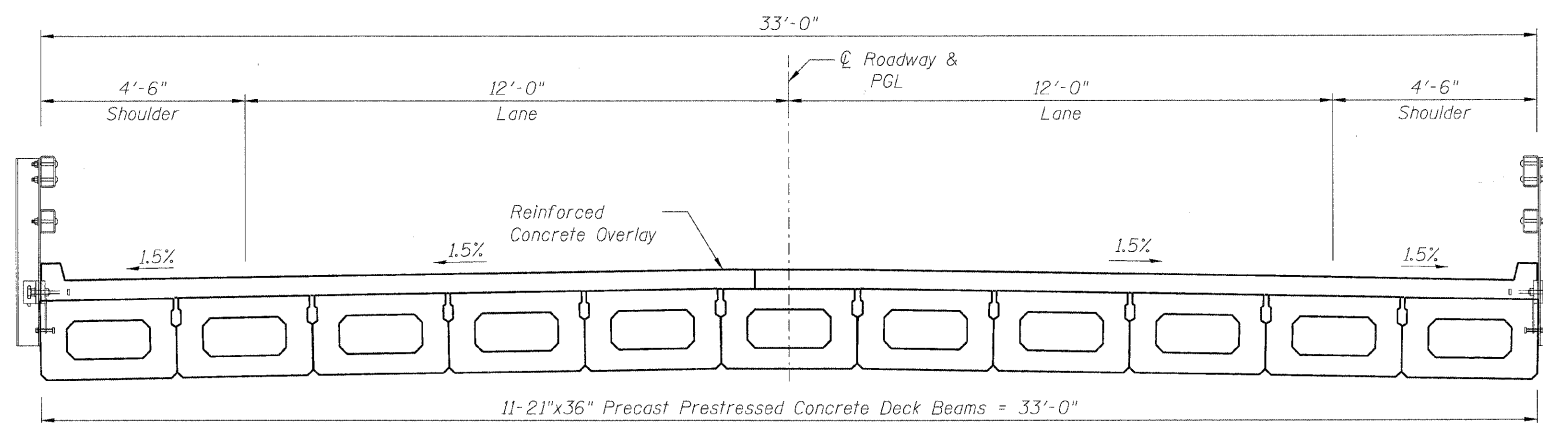
STAGE I REMOVAL

(Looking West)



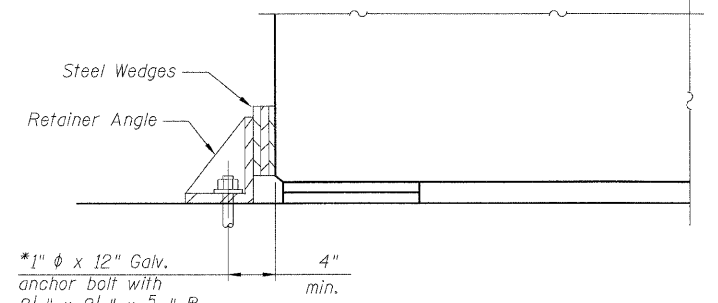
STAGE I CONSTRUCTION & STAGE II REMOVAL

(Looking West)

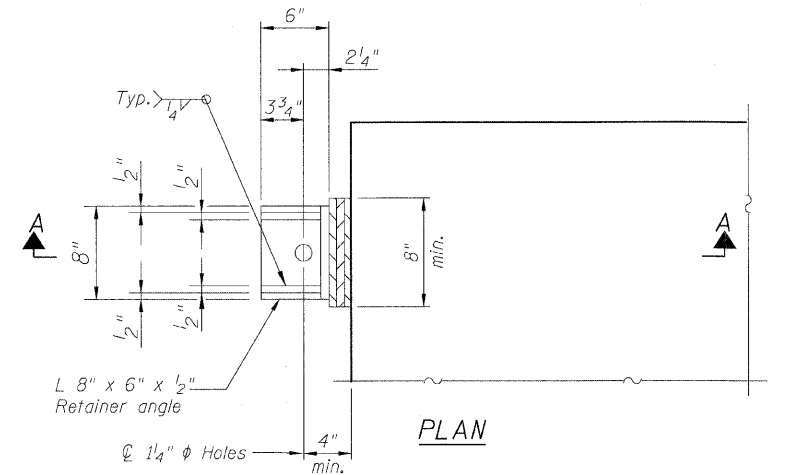


FINAL

(Looking West)



SECTION A-A

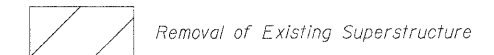


PLAN

TEMPORARY RETAINER ANGLE

* Retainer angle to be placed adjacent to center beam at West Abutment right after Stage I center beam is removed. Retainer angle shall be removed right before installation of the new stage I beams. Repeat procedure for stage II. Cost of retainer and accessories are included with Precast Prestressed Concrete Deck Beams.

LEGEND:



NOTES:

1. See Sheet S-3 for Temporary Concrete Barrier Details.
2. The Contractor is ultimately responsible of means and methods to ensure the complete stability of the structural members during construction.
3. Existing Bearing removal to be paid for under "Asbestos Bearing Pad Removal".

STAGE CONSTRUCTION DETAILS

FAS 1317 (IL RTE 113)

OVER TERRY CREEK

STA. 784+13.00

S.N. 099-0172

B. Sauter
E. Mroczek
R. Danley
B. Sauter



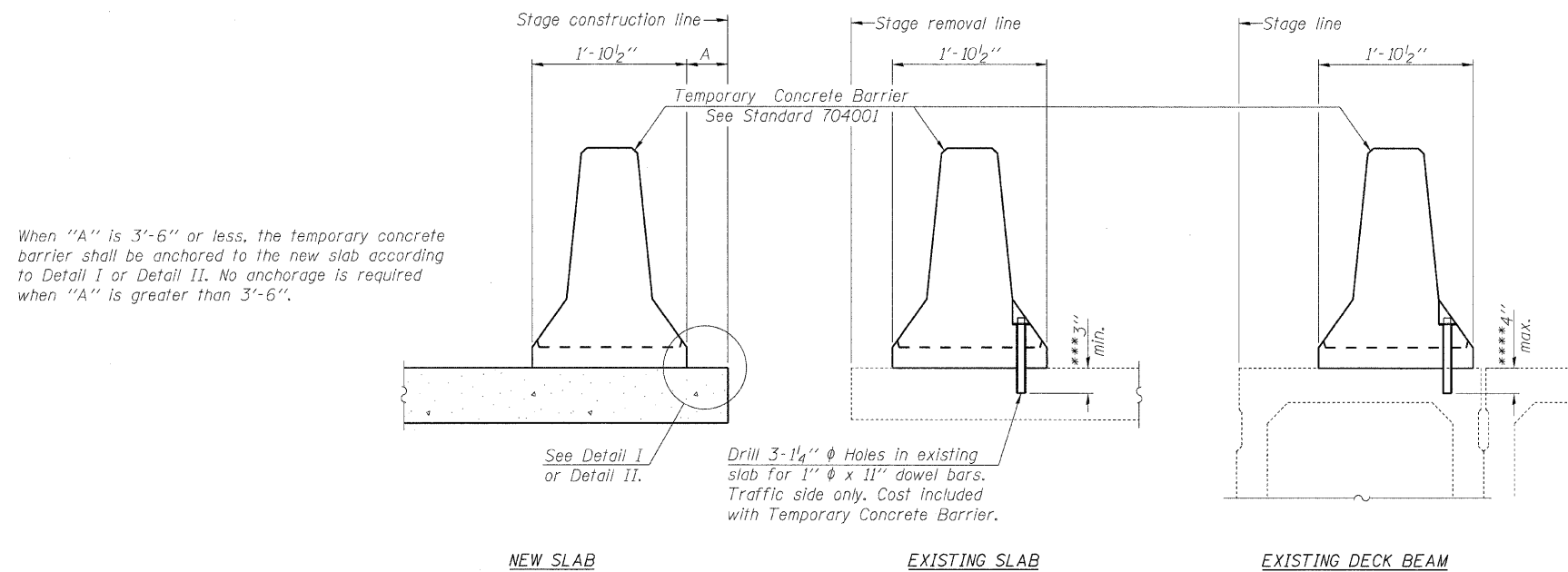
Giorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@giorba.com

SHEET NO. S-2	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	17
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

rdm/ley

7/8/2008

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

NEW SLAB

Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

EXISTING SLAB

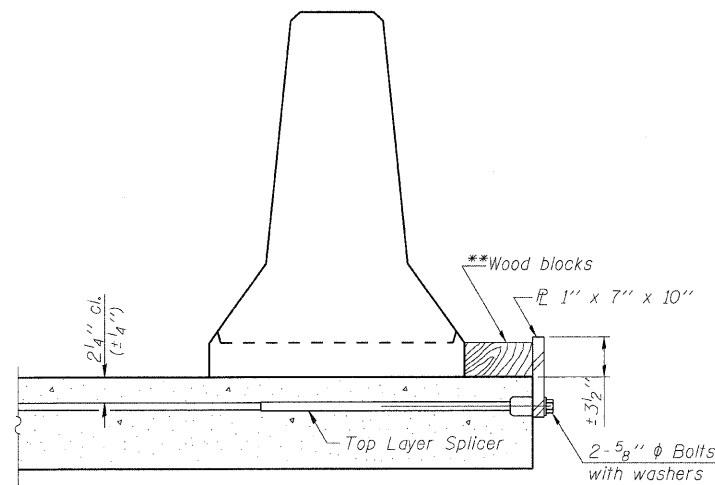
EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

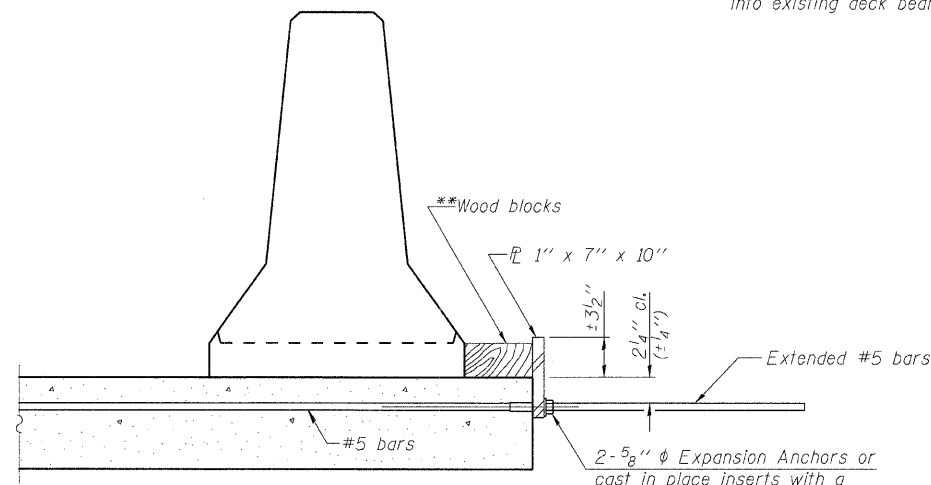
NOTES

- Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{r} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{c} of each barrier panel.
- Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{r} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{c} of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

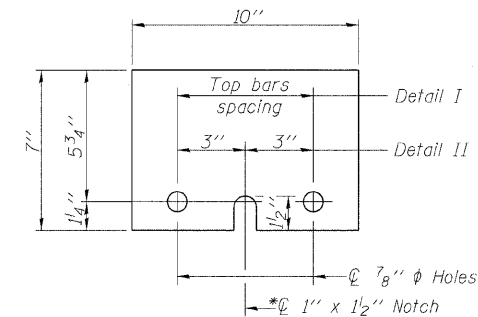
- ***Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.
- ***If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER 1" x 7" x 10"

*Required only with Detail II

TEMPORARY CONCRETE BARRIER

FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

**Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

B. Sauter
E. Mroczek
R. Danley
B. Sauter

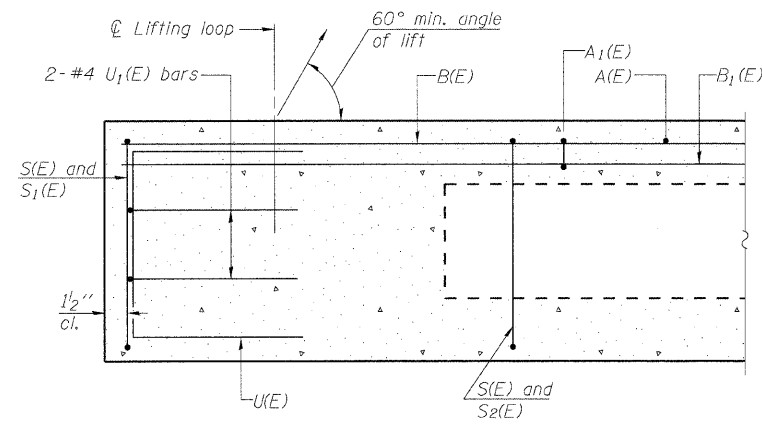


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CONSULTING ENGINEERS

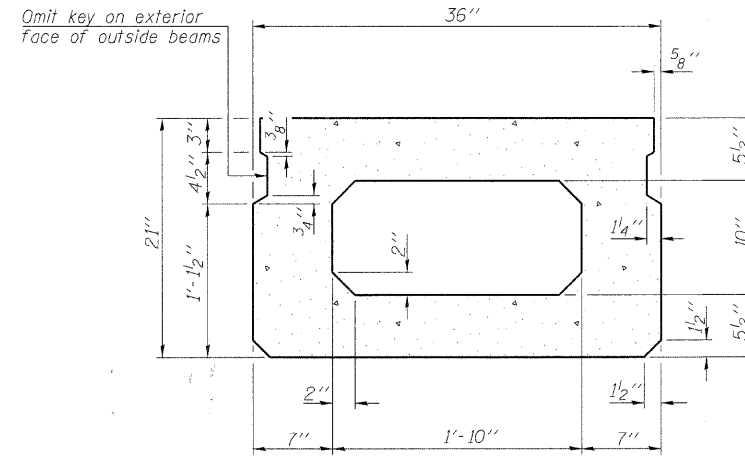
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

SHEET NO. S-3	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

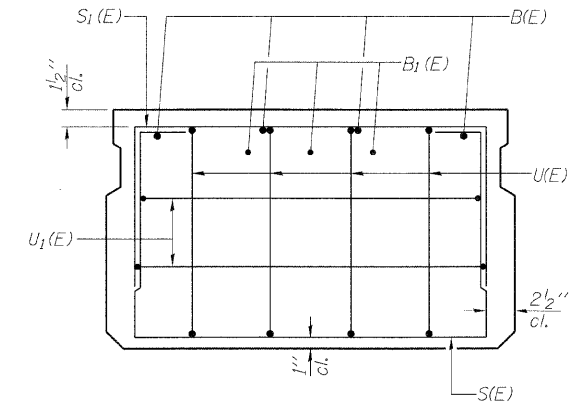
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



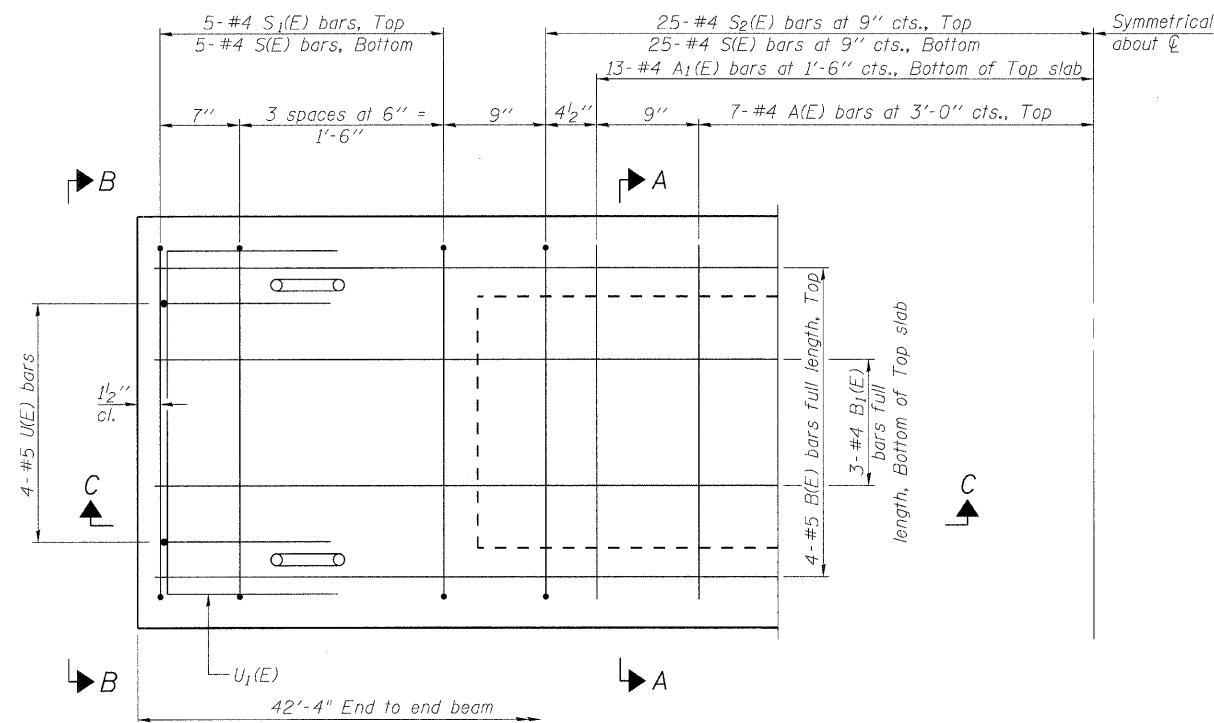
SECTION C-C



SECTION A-A
(Showing dimensions)

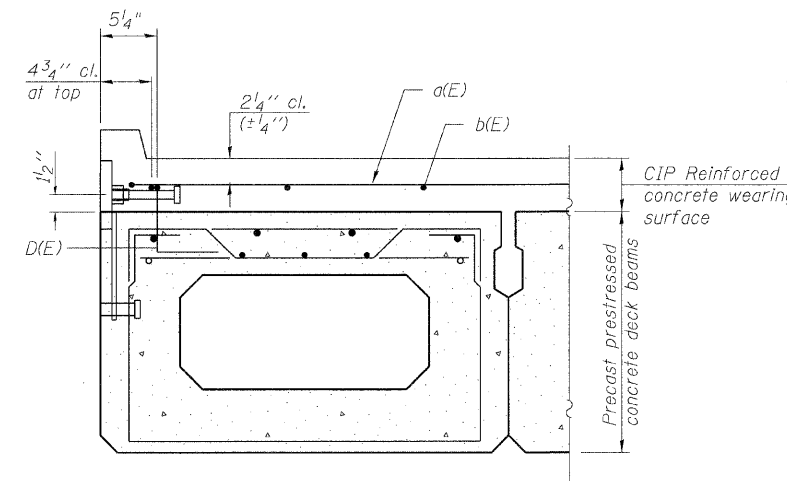


VIEW B-B



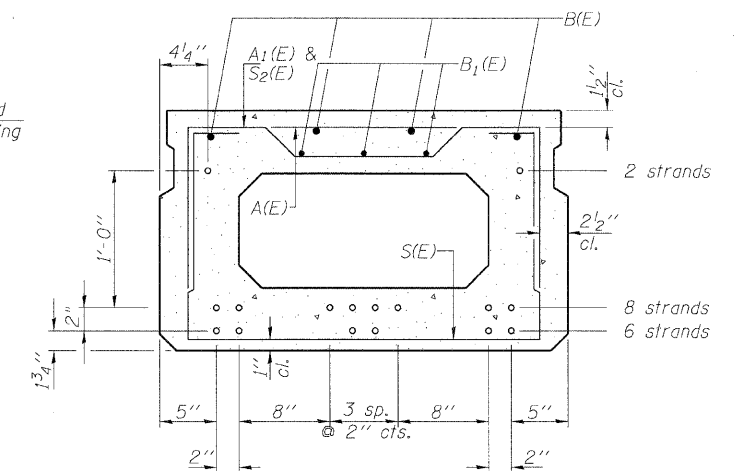
PLAN VIEW

Note: Spacing of S(E) and S₂(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.



SECTION THRU EXTERIOR BEAM

See Typical Section Thru Interior Beam for strand pattern, dimensions and bar call outs. CWS and Curb shall be poured in the field.



SECTION A-A

(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

BAR LIST
ONE BEAM ONLY

(For information only)

Bar	No.	Size	Length	Shape
A(E)	14	#4	2'-7"	—
A ₁ (E)	26	#4	2'-11"	—
B(E)	8	#5	22'-0"	—
B ₁ (E)	5	#4	22'-0"	—
D(E)	32	#4	2'-9"	U
S(E)	60	#4	6'-5"	U
S ₁ (E)	10	#4	4'-11"	U
S ₂ (E)	50	#4	5'-2"	U
U(E)	8	#5	4'-0"	U
U ₁ (E)	4	#4	5'-0"	U

Note: See sheet S-5 for additional details and Bill of Material.

* Exterior Beams only

21" x 36" PPC DECK BEAM
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

B. Sauter
E. Mroczek
R. Danley
B. Sauter

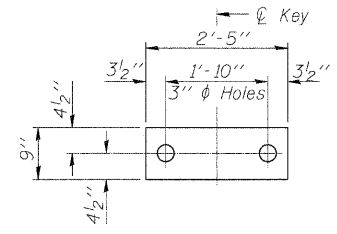


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5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

PD-2136-0 5-16-08

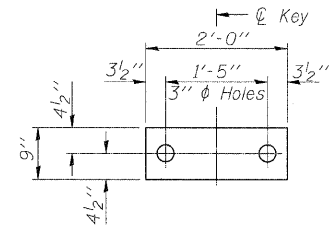
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	1317	111 B-2	WILL	32	19
S-13 SHEETS	CONTRACT NO. 60D86				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



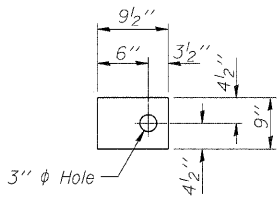
FABRIC BEARING PAD
(Interior)
(8 Required)

FIXED

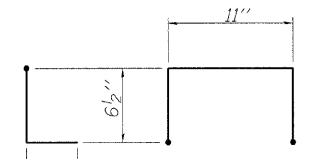


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(Interior)
(2 Required)

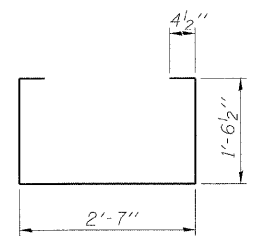
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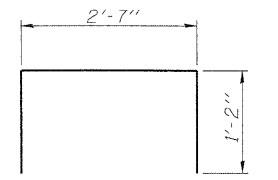
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(Exterior)
(2 Required)



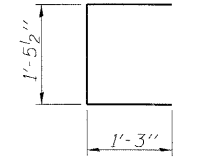
BAR D(E)



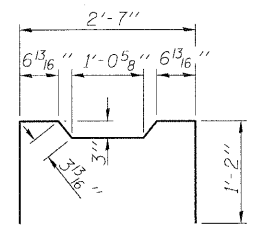
BAR S(E)



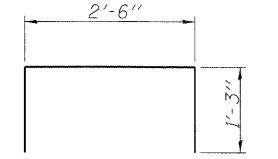
BAR S1(E)



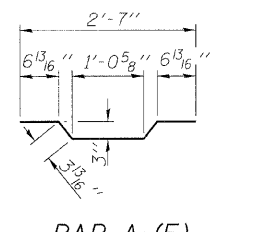
BAR U(E)



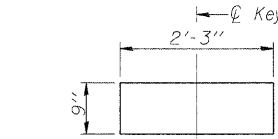
BAR S2(E)



BAR U1(E)

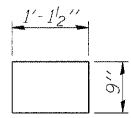


BAR A1(E)

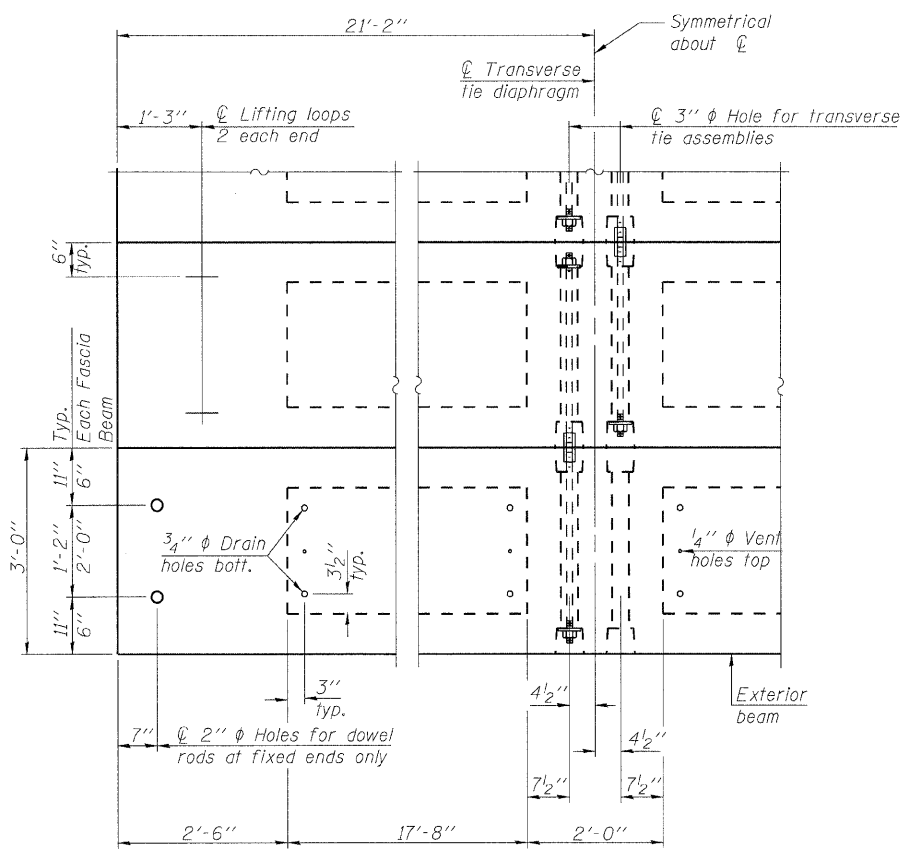


FABRIC BEARING PAD
(Interior)
(10 Required)

EXPANSION



FABRIC BEARING PAD
(Exterior)
(2 Required)



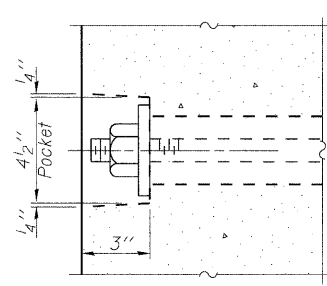
PLAN VIEW

Note: Connect beams in pairs with the transverse tie configuration shown.

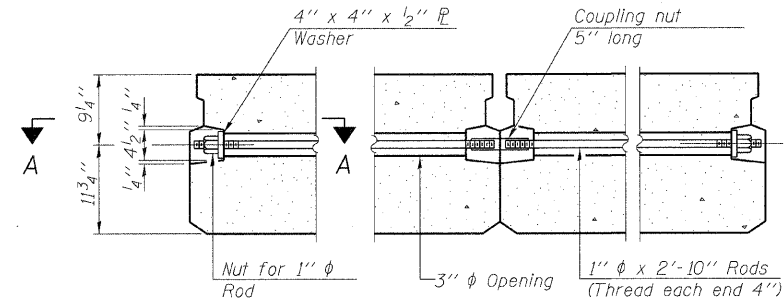
B. Sauter
E. Mroczek
R. Danley
B. Sauter

CG **Giorba Group, Inc.**
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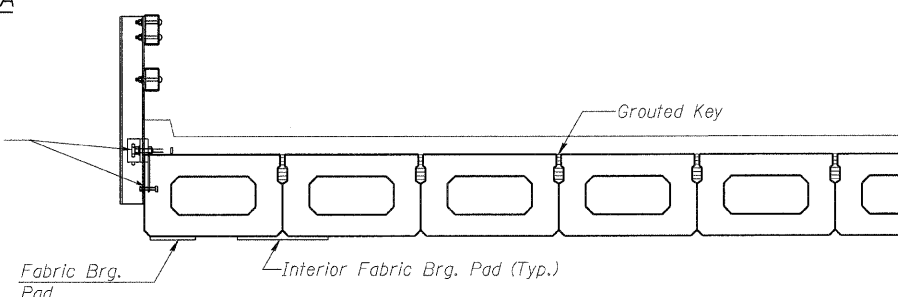
PD-2136-OD 5-16-08



SECTION A-A



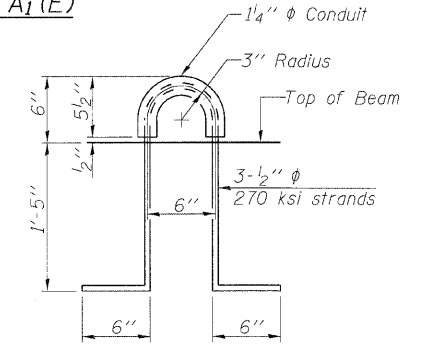
TYPICAL TRANSVERSE TIE ASSEMBLY



PARTIAL CROSS SECTION

NOTES

- 1. Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. The 1" diameter rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
- 2. Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions).
- 3. Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
- 4. A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.
- 5. Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.06 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
- 6. Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
- 7. Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.



LIFTING LOOP DETAIL

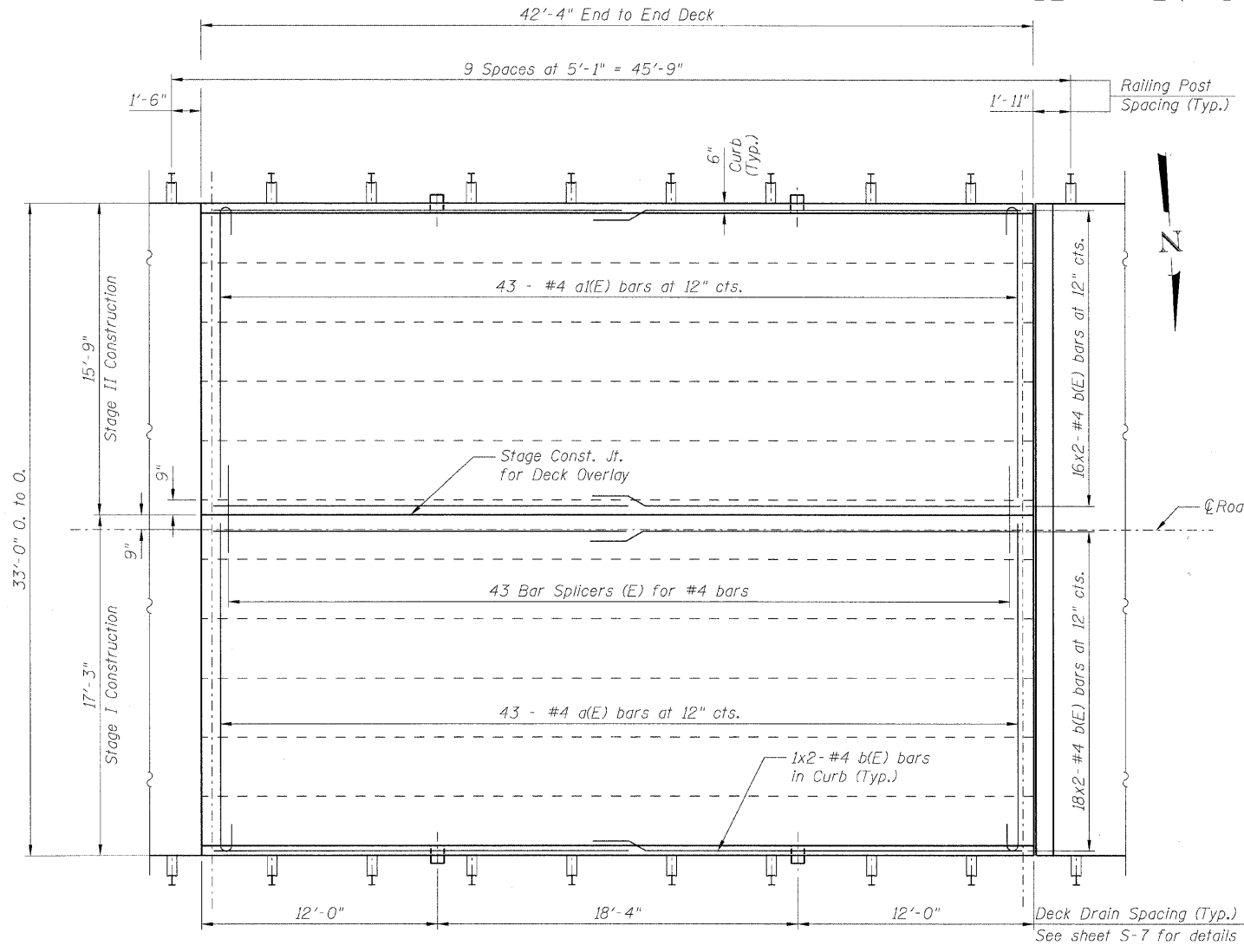
BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (21" depth)	Sq. Ft.	1,397
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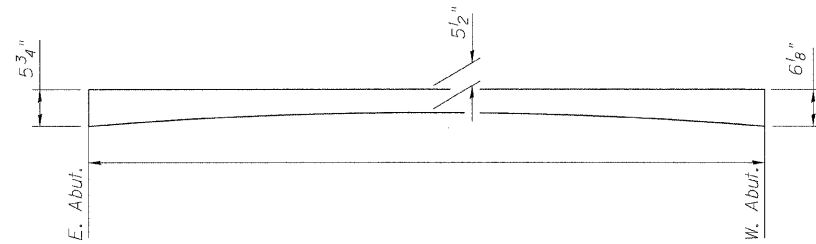
21" X 36" PPC DECK BEAM DETAILS
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

SHEET NO. S-5	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	20
S-13 SHEETS	CONTRACT NO.			60D86	
	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

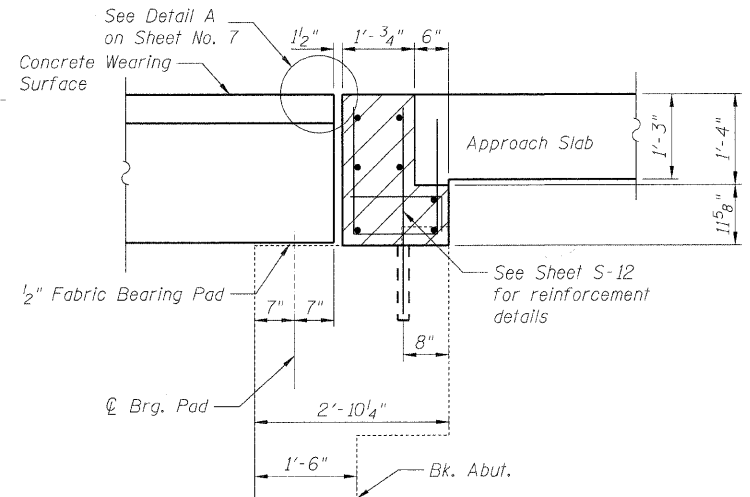
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



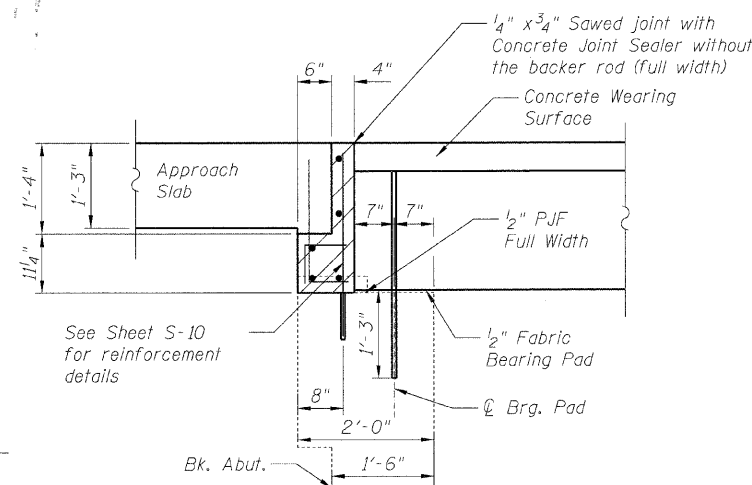
CONCRETE WEARING SURFACE - PLAN



REINFORCED CONCRETE WEARING SURFACE PROFILE

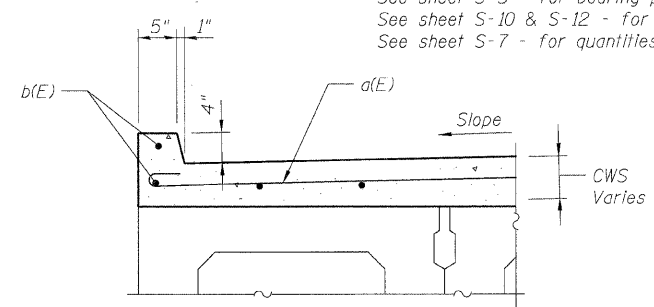


SECTION THRU WEST ABUTMENT
(Looking South)



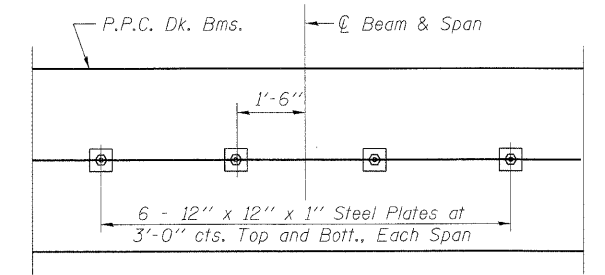
SECTION THRU EAST ABUTMENT

Notes:
After beams have been erected, holes shall be drilled into substructure and anchor dowels placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.
Hatched area to be poured after concrete wearing surface is in place.
All horizontal dimensions are at right angles to beam ends.
See sheet S-5 - for bearing pad details.
See sheet S-10 & S-12 - for Abutment reinforcement details.
See sheet S-7 - for quantities.

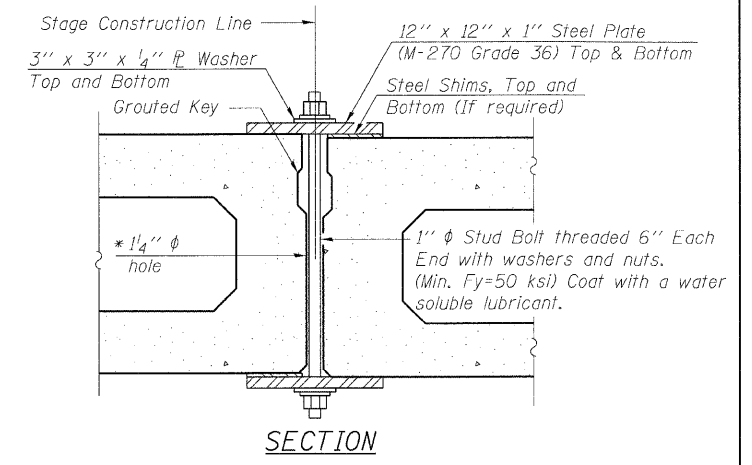


SECTION THRU CURB
Curbs shall be poured in the field.

LAP LENGTH
4 bars - 1'-8"

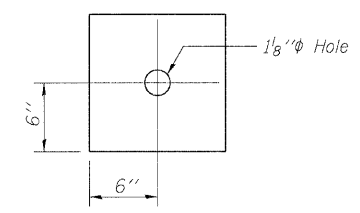


PLAN



SECTION

* The fabricator shall cast 2" diameter semi-circular recesses in the sides of each beam adjacent to the stage construction line. These recesses shall align to form a hole at the appropriate locations for the clamping device bolts. The Contractor shall show the details on the shop drawings.



CLAMPING PLATE

SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

Cost included with Precast Prestressed Concrete Deck Beams.
See Stage Construction Details for traffic lanes.

NOTES:

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Bars indicated thus 2x3-#4 etc. indicates 2 lines of bars with 3 lengths per line.

SUPERSTRUCTURE
DETAILS 1
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

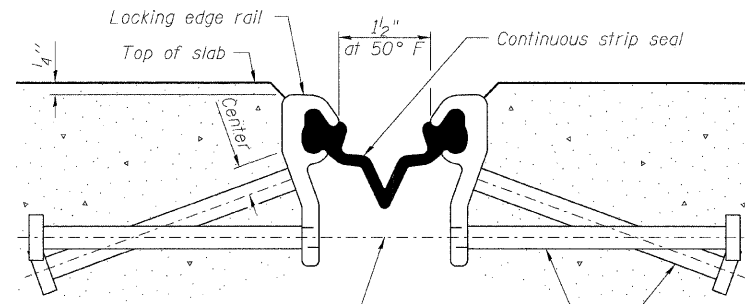
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SHEET NO. S-6	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	21
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

7/8/2008 rdanley n:\proj\3329\3329_22\design\structure\3329_22_06_Superstructure_Details\sheet

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

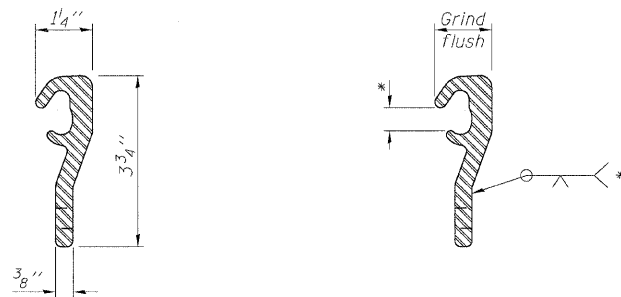


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" int. cts.

SECTION THRU STRIP SEAL JOINT
FOR OVERLAY OVER DECK BEAMS

DETAIL A



LOCKING EDGE RAIL

LOCKING EDGE RAIL SPLICE

Notes:

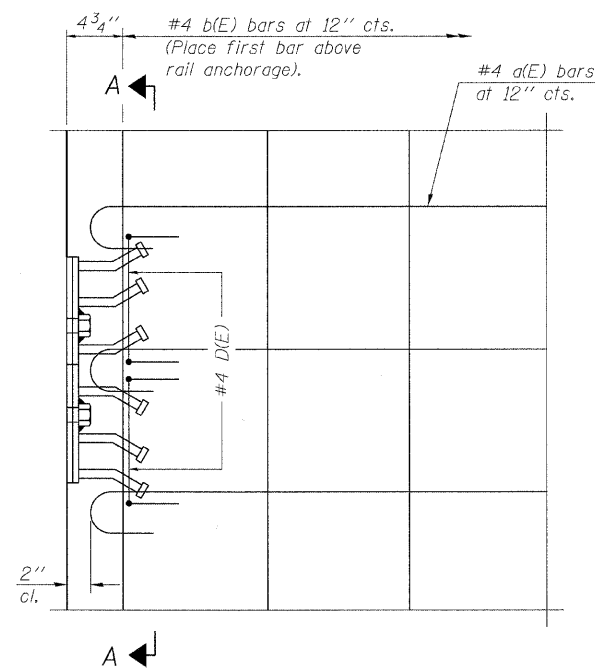
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue.

Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed.

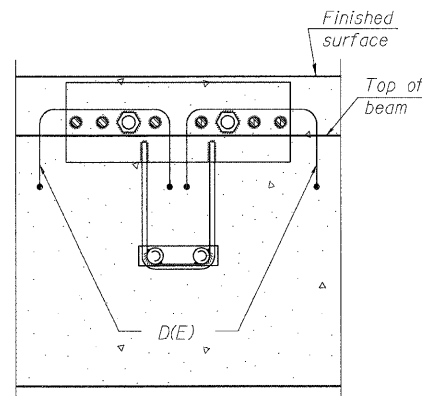


PLAN

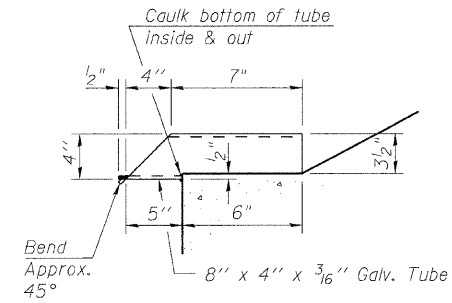
Notes:

The rail anchorage shall be cast with the beam and the wearing surface shall be cast in the field. Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam. Drilling into the beam will not be permitted.

For the rail posts location see sheet S-6.

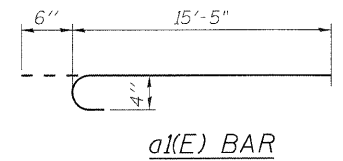


SECTION A-A

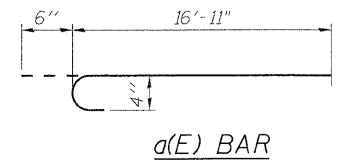


DRAIN DETAIL

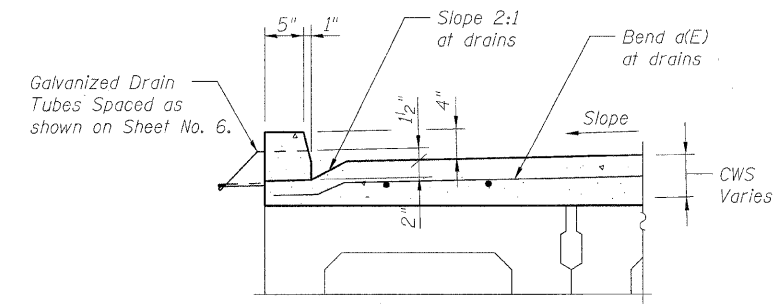
Cost of drains is included with Concrete Wearing Surface



a(E) BAR



a(E) BAR



SECTION THRU CURB

Curbs shall be poured in the field.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a(E)	43	#4	17'-5"	—	
a(E)	43	#4	15'-11"	—	
b(E)	72	#4	21'-8"	—	
Reinforcement Bars, Epoxy Coated				Pound	2,010
Concrete Wearing Surface				Sq. Yd.	155
Bar Splicers				Each	43
Bridge Deck Grooving				Sq. Yd.	139
Protective Coat				Sq. Yd.	155
Preformed Joint Strip Seal				Foot	33

NOTE:

1. Bars indicated thus 1x2- #5 etc. indicates 1 line of bars with 2 lengths per line.

SUPERSTRUCTURE
DETAILS 2
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

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E. Mroczek
R. Danley
B. Sauter



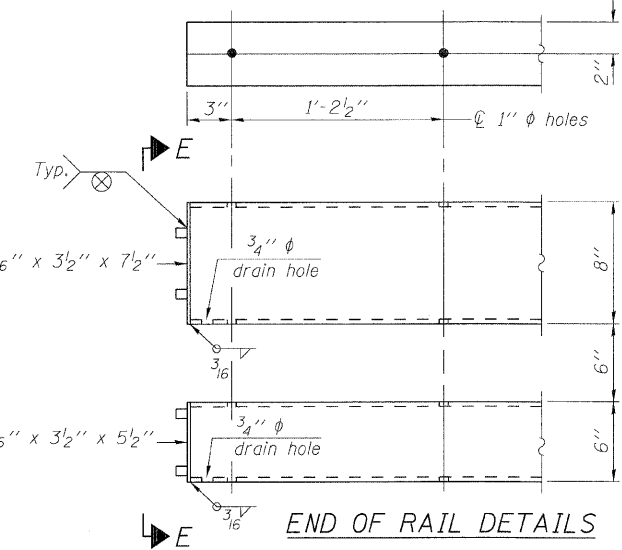
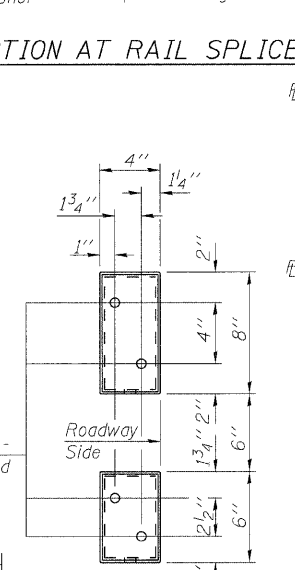
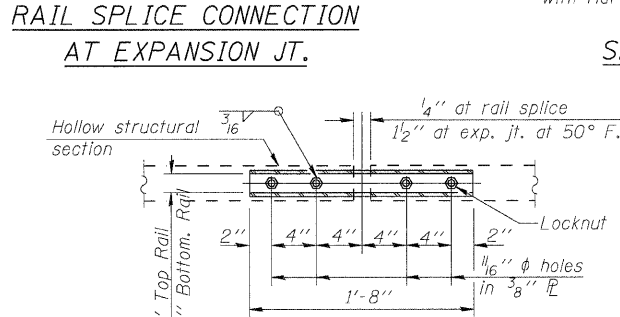
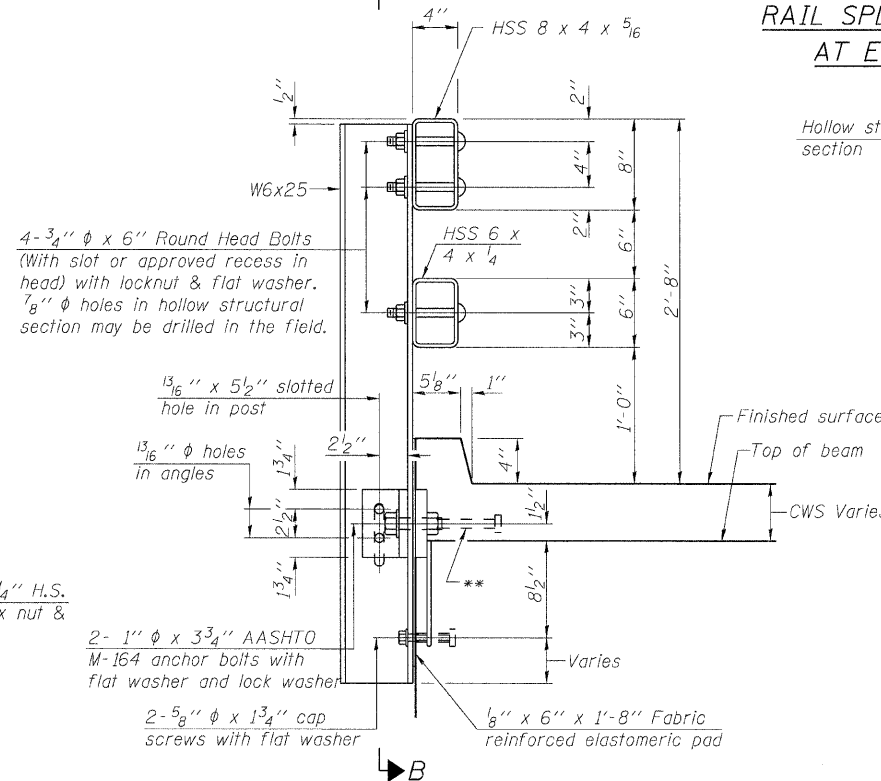
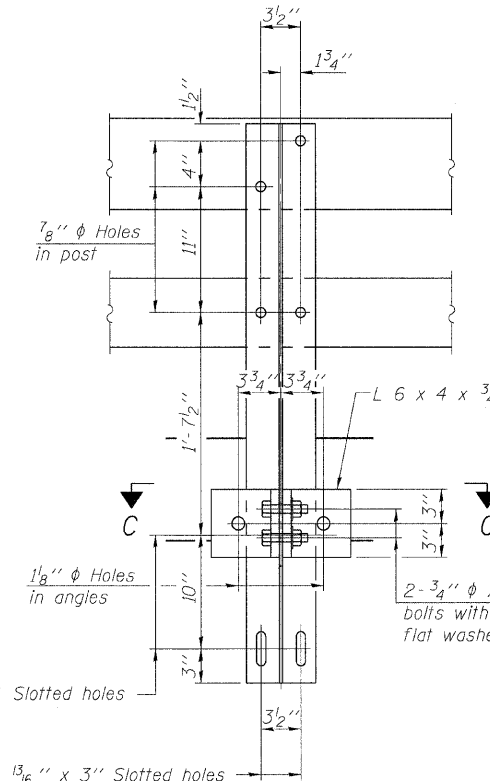
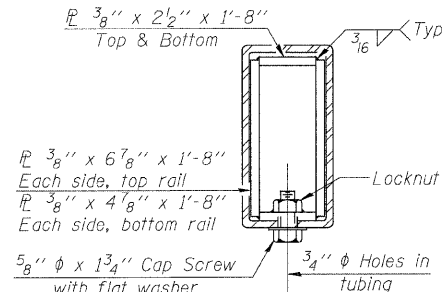
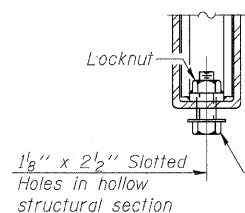
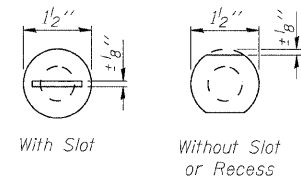
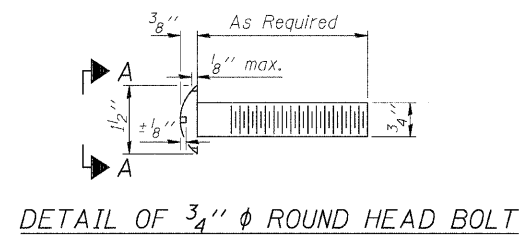
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SHEET NO. S-7	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	22
S-13 SHEETS		CONTRACT NO.		60D86	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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Notes:
All field drilled holes shall be coated with an approved zinc rich paint before erection.
For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM.
All steel railing elements shall be galvanized according to Article 509.05 of the Standard Specifications.
**The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	92

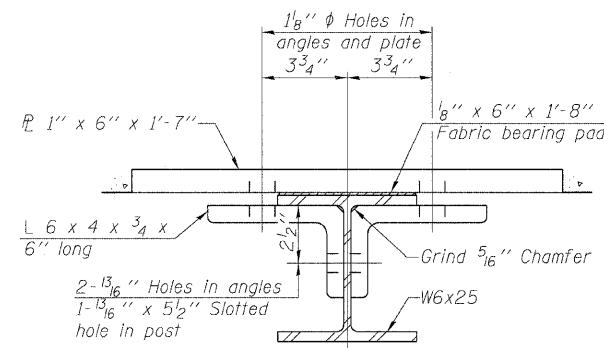
SECTION B-B

SECTION AT RAIL POST

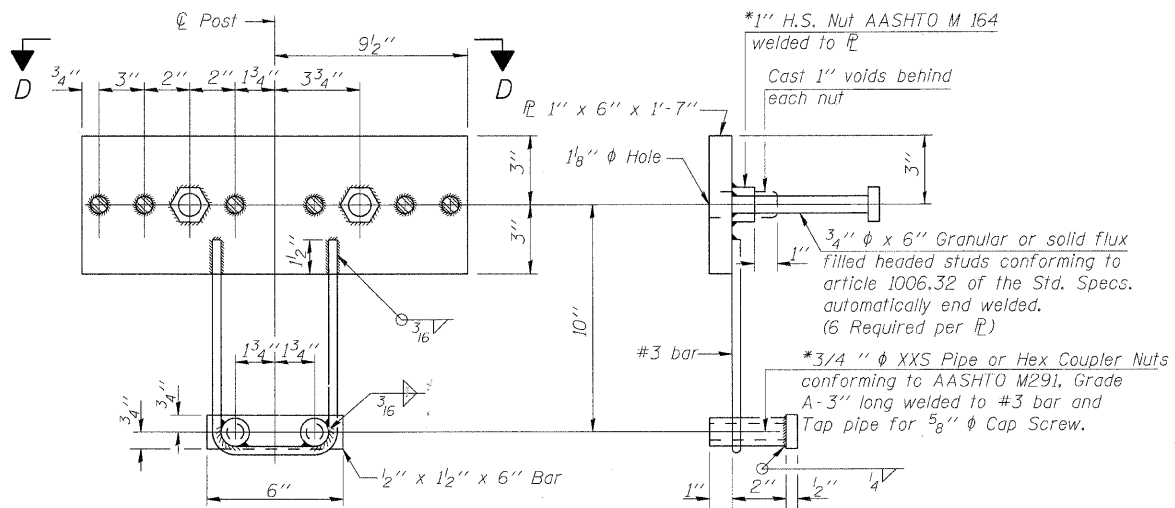
PLAN-BOTTOM SPLICE R TYPICAL

VIEW E-E

STEEL RAILING, TYPE SM
WITH CONCRETE
WEARING SURFACE AND CURB
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172



SECTION C-C



ANCHOR DEVICE

*Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

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E. Mroczek
R. Danley
B. Sauter

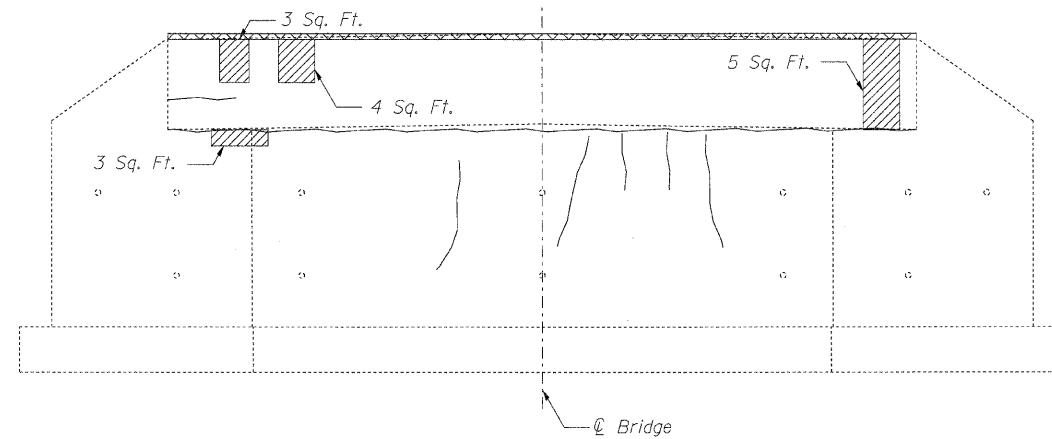
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R-34CWSC 5-16-08 (6'-3" Maximum Post Spacing) (5" minimum to 7 1/8" maximum CWS thickness)

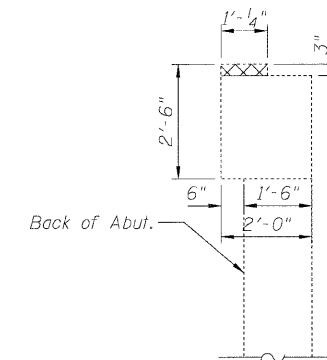
SHEET NO. S-8	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	23
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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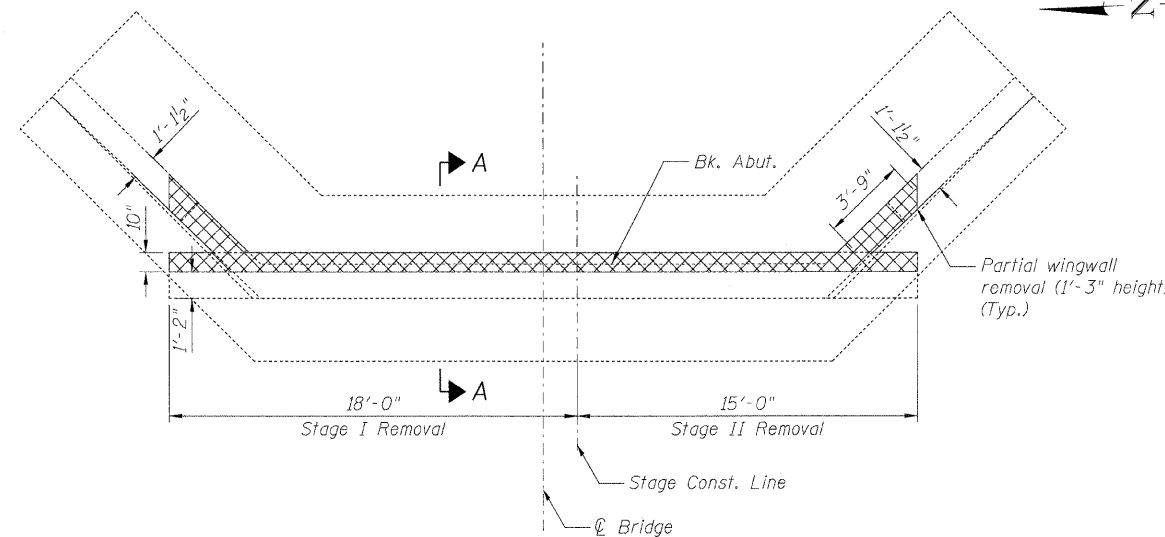
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION
Looking East



SECTION A-A



PLAN

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	15
Concrete Removal	Cu. Yd.	0.7

NOTE:

Repairs of the existing abutments shall include but not be limited to the areas shown. The actual areas to be determined by the engineer at the time of construction.

**EAST ABUTMENT
REPAIRS
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172**

LEGEND

- Structural Repair of Concrete ($\leq 5"$)
- Hairline Crack
- Concrete Removal

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E. Mroczek
R. Danley
B. Sauter



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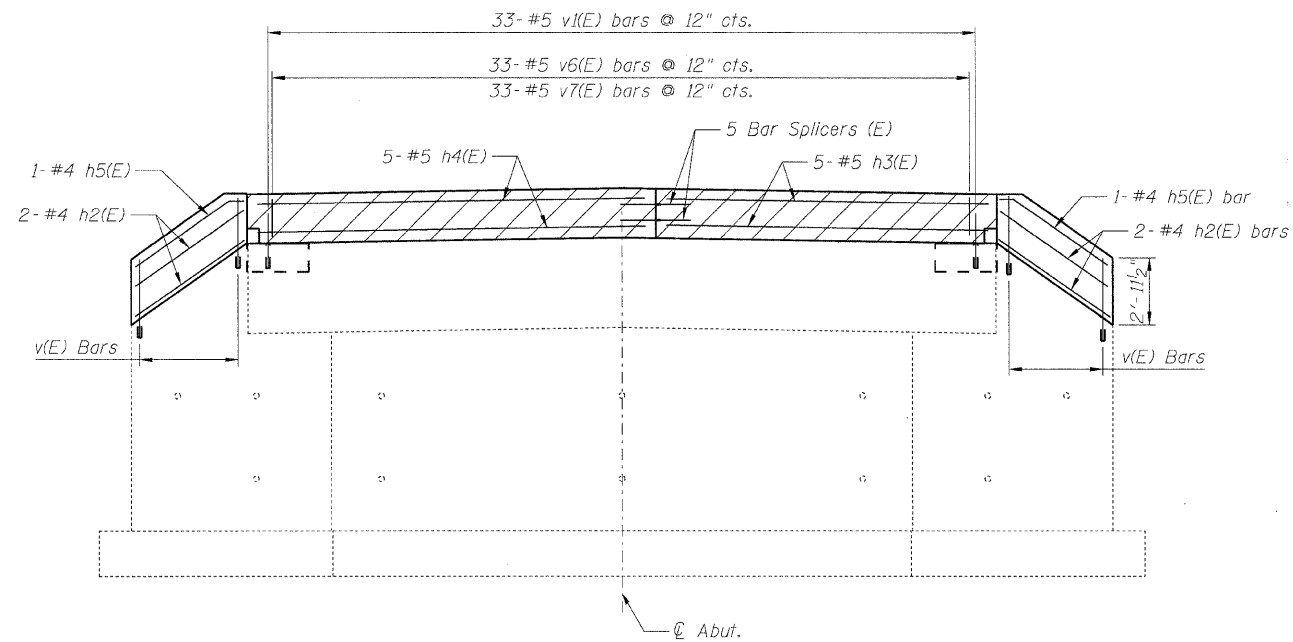
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	1317	111 B-2	WILL	32	24
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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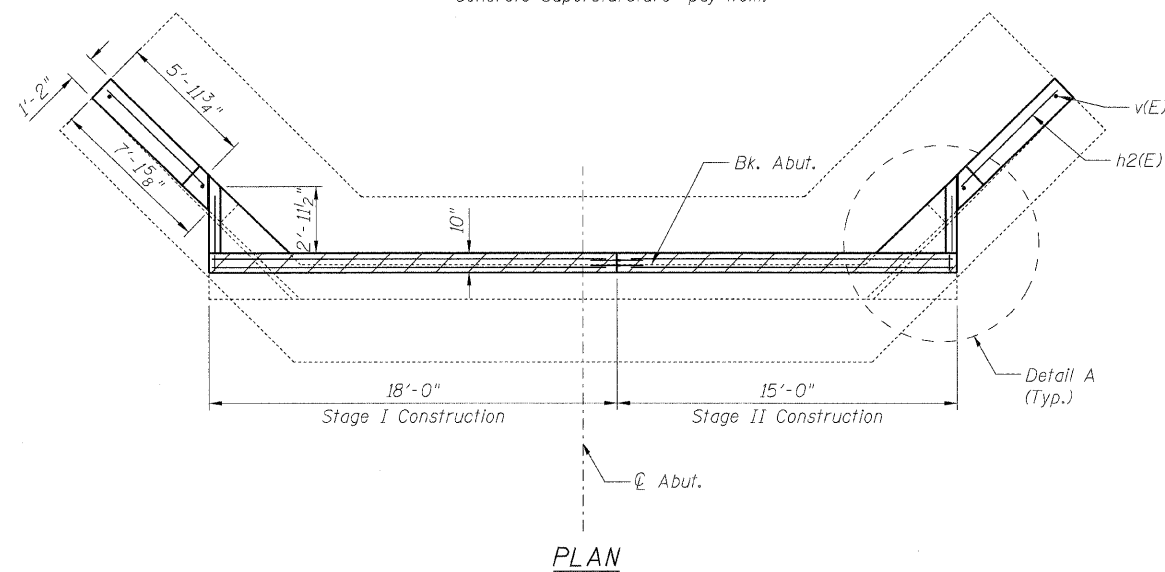
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

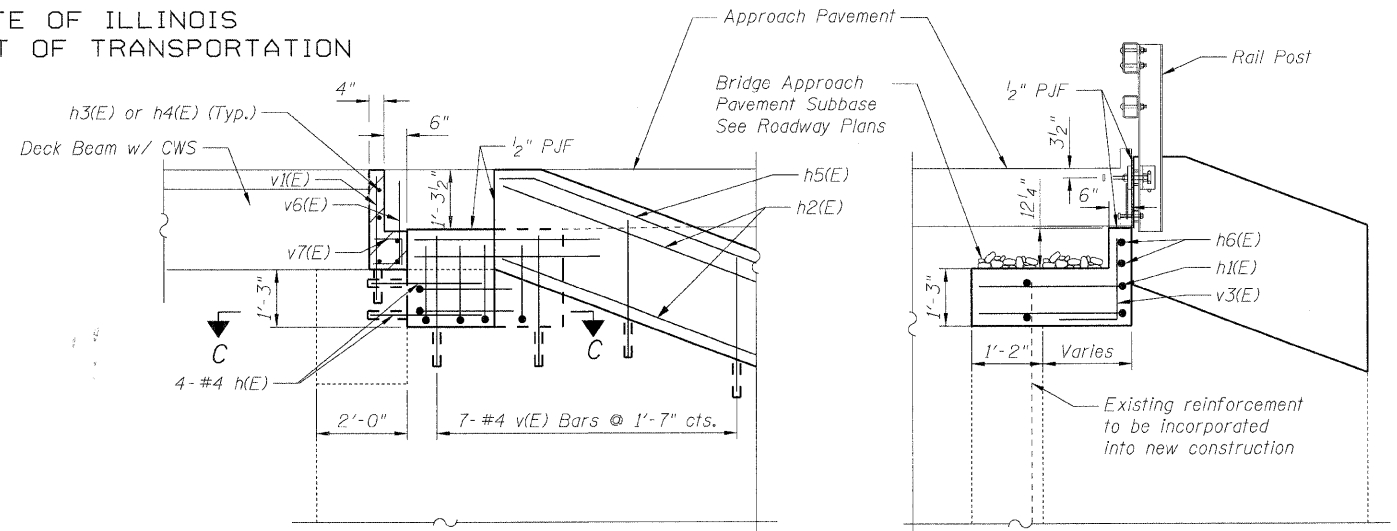


ELEVATION
Looking East

Hatched areas to be poured after concrete wearing surface is in place. The cost of Concrete is included with "Concrete Superstructure" pay item.

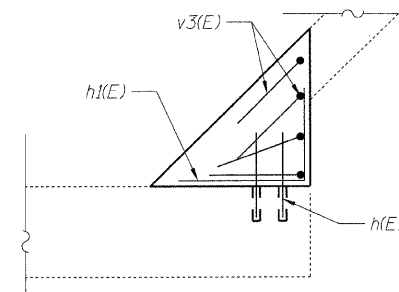


PLAN

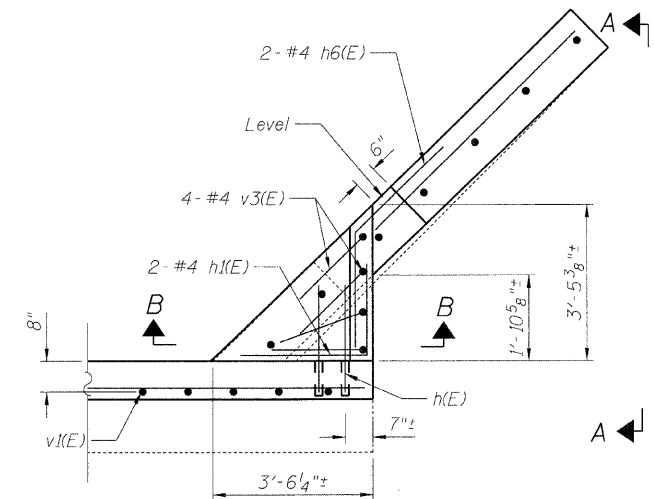


VIEW A-A

SECTION B-B



SECTION C-C



DETAIL A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	8	#4	1'-9"	—
h1(E)	4	#4	5'-2"	┘
h2(E)	4	#4	7'-0"	—
h3(E)	5	#5	13'-8"	—
h4(E)	5	#5	18'-8"	—
h5(E)	2	#4	7'-3"	—
h6(E)	4	#4	6'-0"	—
v(E)	14	#4	3'-8"	—
v1(E)	33	#5	2'-10"	—
v3(E)	8	#4	4'-1"	┘
v6(E)	33	#5	2'-4"	┘
v7(E)	33	#5	1'-0"	┘
Concrete Structures		Cu. Yd.	2.3	
Reinforcement Bars, Epoxy Coated		Pound	500	
Bar Splicers		Each	5	
Concrete Superstructures		Cu. Yd.	1.5	

NOTES:

1. Drill and grout h(E) and v(E) bars 9" min., and v1(E) bars 12" min. in accordance with Article 584 of the Standard Specifications. Cost included with Reinforcement Bars Epoxy Coated.
2. For rail anchorage details see sheet S-8.
3. For rail post location, see sheet S-6.
4. For typical section thru E. Abutment, see sheet S-6.

EAST ABUTMENT
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

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B. Sauter



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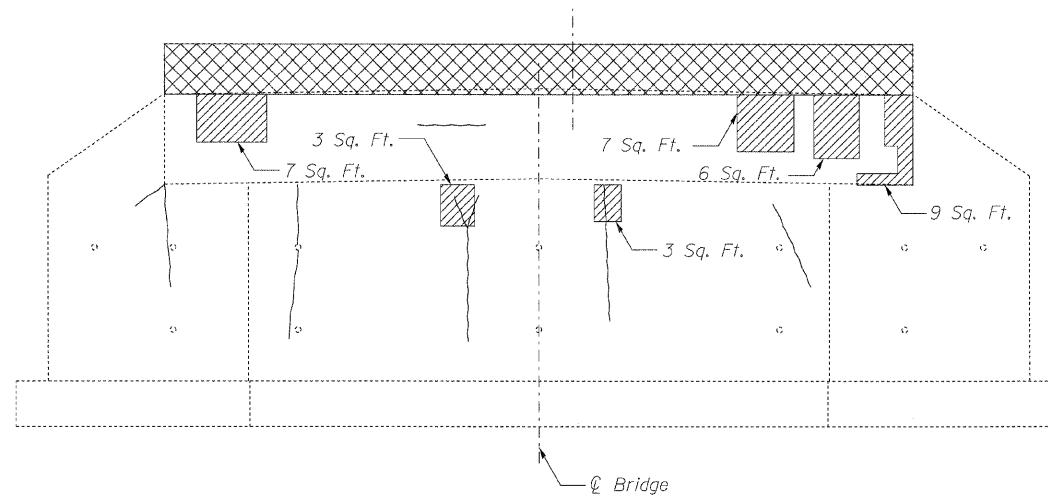
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	1317	111 B-2	WILL	32	25
S-13 SHEETS	CONTRACT NO. 60D86				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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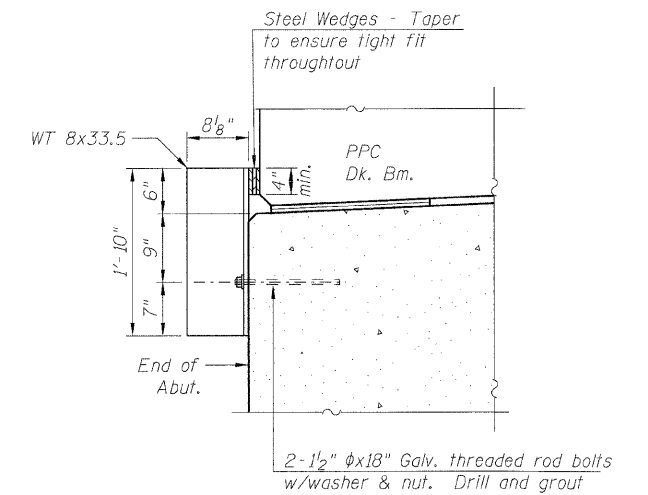
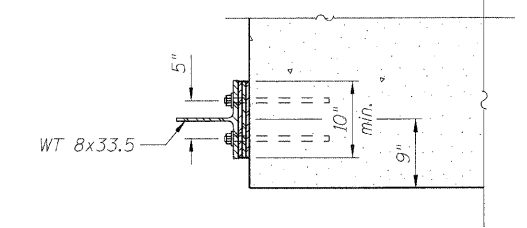
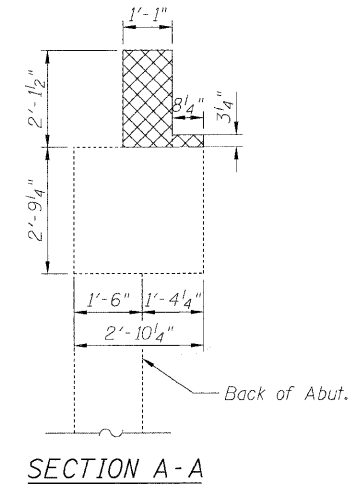
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

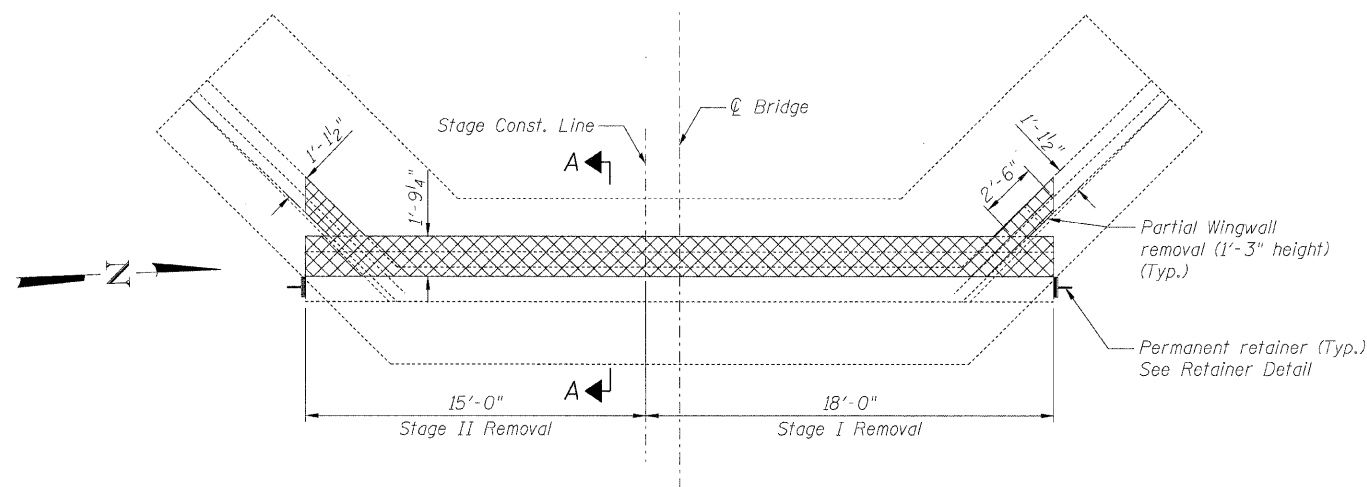


ELEVATION
Looking West



RETAINER DETAIL

Cost of retainer and accessories are included with Precast Prestressed Concrete Deck Beams.



PLAN

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	35
Concrete Removal	Cu. Yd.	3.3

**WEST ABUTMENT
REPAIRS
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172**

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NOTE:
Repairs of the existing abutments shall include but not be limited to the areas shown. The actual areas to be determined by the engineer at the time of construction.

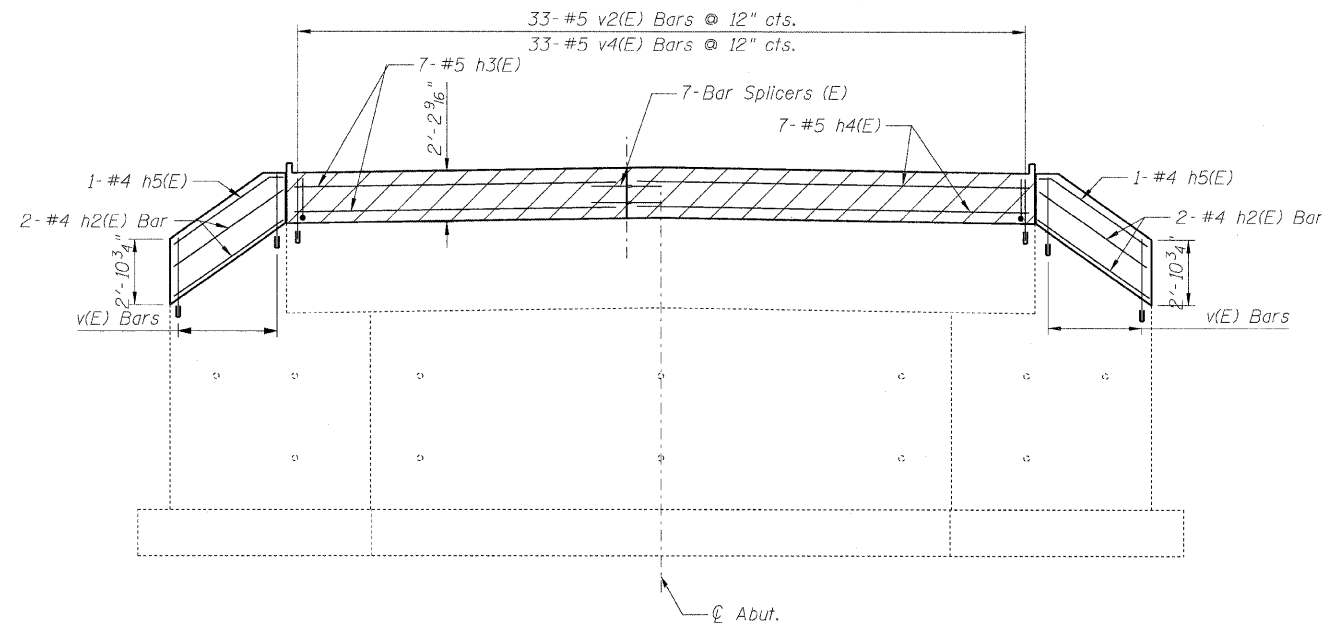
LEGEND

- Structural Repair of Concrete (≤ 5")
- Hairline Crack
- Concrete Removal

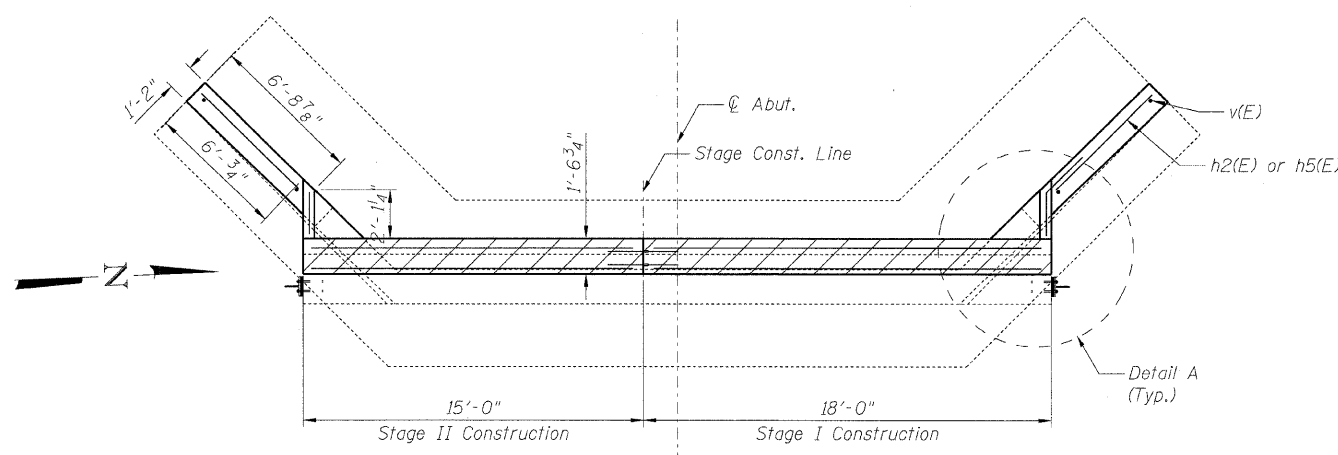
SHEET NO. S-11	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	26
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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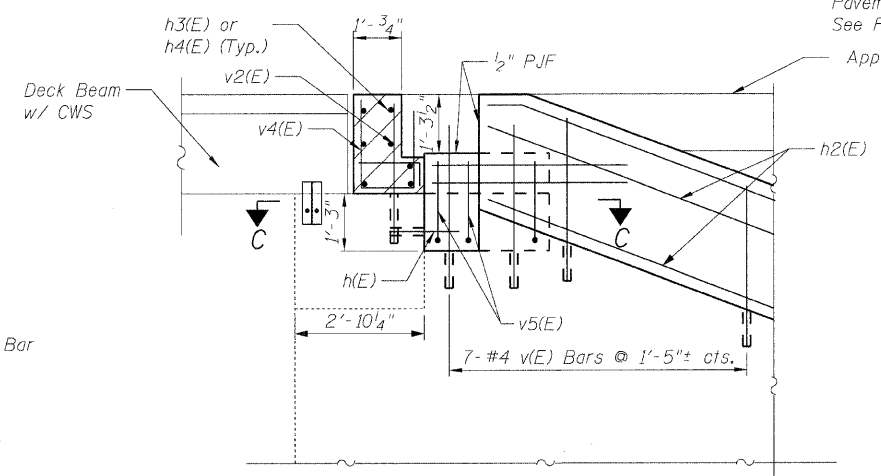
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



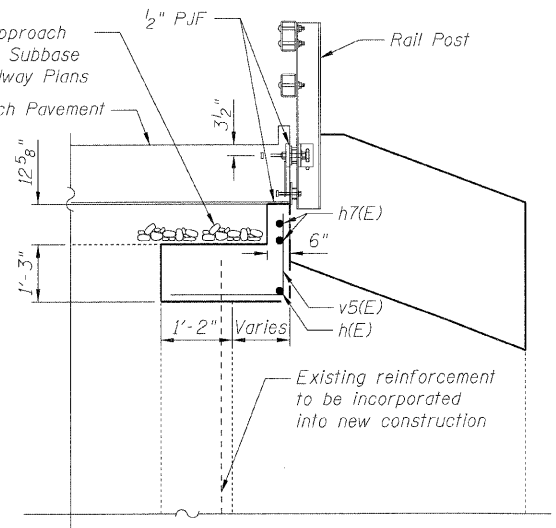
ELEVATION
Looking West



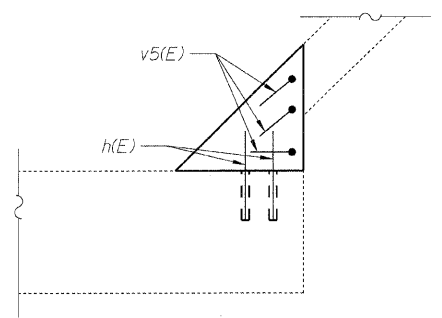
PLAN



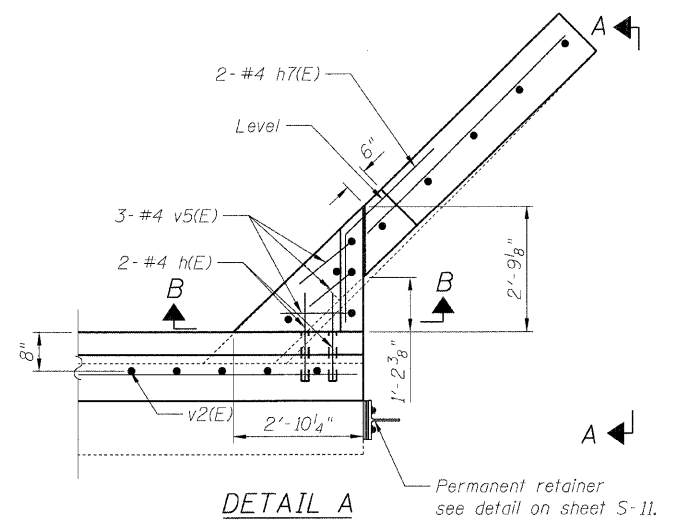
VIEW A-A



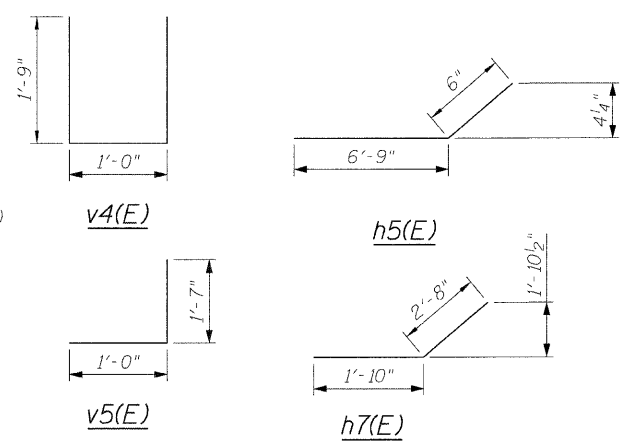
SECTION B-B



SECTION C-C



DETAIL A



NOTES:

1. Drill and grout h(E) and v(E) bars 9" min., and v2(E) bars 12" min. in accordance with Article 584 of the Standard Specifications. Cost included with "Reinforcement, Bars Epoxy Coated."
2. For rail anchorage details see sheet S-6.
3. For rail post location, see sheet S-6.
4. For typical section thru W. Abutment, see sheet S-6.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	4	#4	1'-9"	—
h2(E)	4	#4	7'-0"	—
h3(E)	7	#5	14'-8"	—
h4(E)	7	#5	17'-8"	—
h5(E)	2	#4	7'-3"	—
h7(E)	4	#4	4'-6"	—
v(E)	14	#4	3'-8"	—
v2(E)	33	#5	2'-10"	—
v4(E)	33	#5	4'-6"	┌
v5(E)	6	#4	2'-7"	└
Concrete Structures			Cu. Yd.	2.7
Reinforcement Bars, Epoxy Coated			Pound	580
Bar Splicers			Each	7
Concrete Superstructure			Cu. Yd.	3.5

WEST ABUTMENT
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

B. Sauter
E. Mroczek
R. Danley
B. Sauter

CG Giorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@giorba.com

SHEET NO. S-12	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	27
S-13 SHEETS			CONTRACT NO. 60D86		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

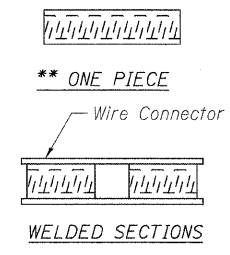
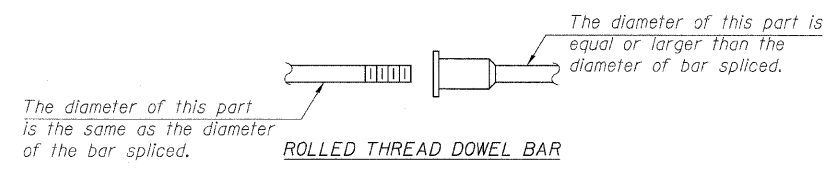
7/8/2008 rdanley na:\proj\3329\3329_22\design\structural\acad\shht\3329_22_12_West Abutment2.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOTES

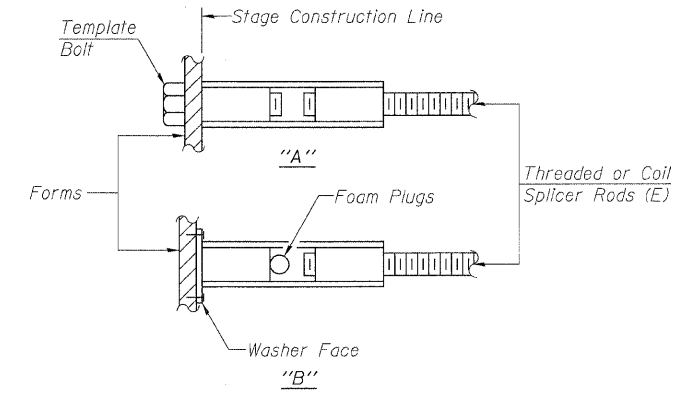
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_l$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_l$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_l = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete



BAR SPLICER ASSEMBLY ALTERNATIVES

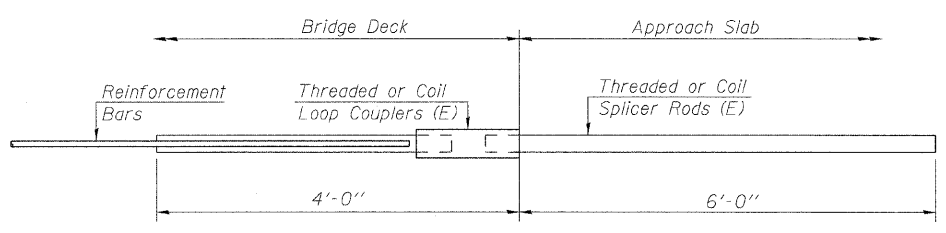
**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



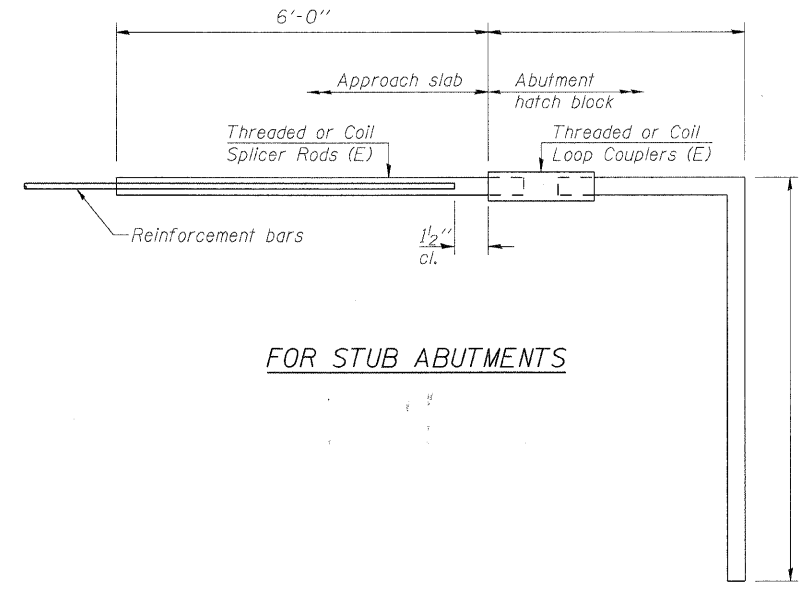
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
"B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.

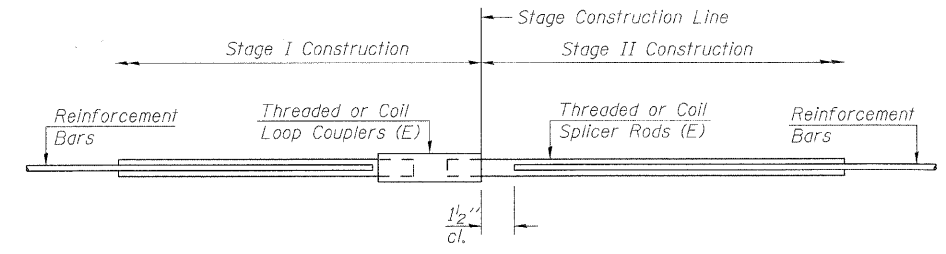
BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS



FOR STUB ABUTMENTS



STANDARD

Bar Size	No. Assemblies Required	Location
#4	43	Deck Overlay
#5	12	Abut. Backwall

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =

B. Sauter
E. Mroczek
R. Danley
B. Sauter

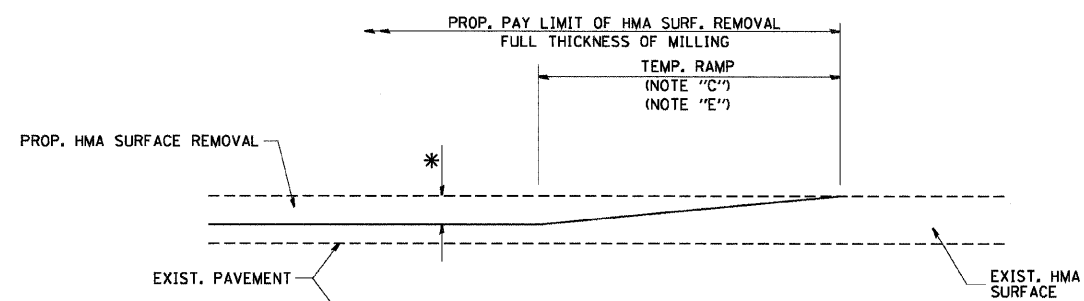
CG Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

BAR SPLICER DETAILS
FAS 1317 (IL RTE 113)
OVER TERRY CREEK
STA. 784+13.00
S.N. 099-0172

SHEET NO. S-13	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1317	111 B-2	WILL	32	28
S-13 SHEETS	CONTRACT NO. 60D86				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

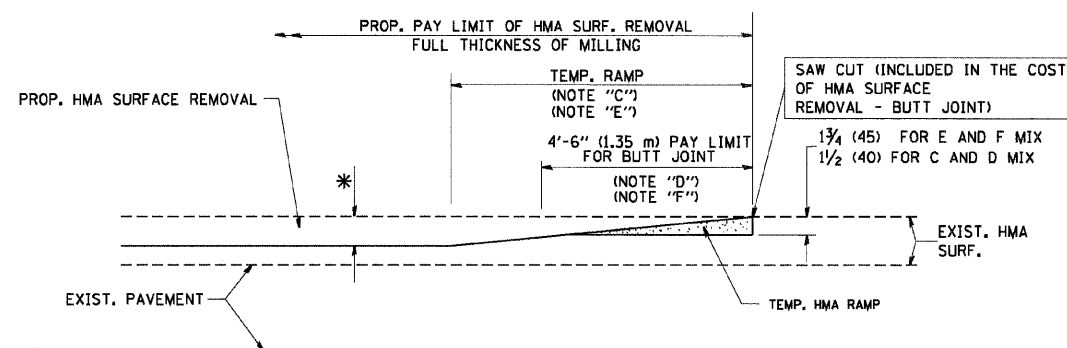
7/8/2008 rdanley 7/8/2008 rdanley

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	29
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



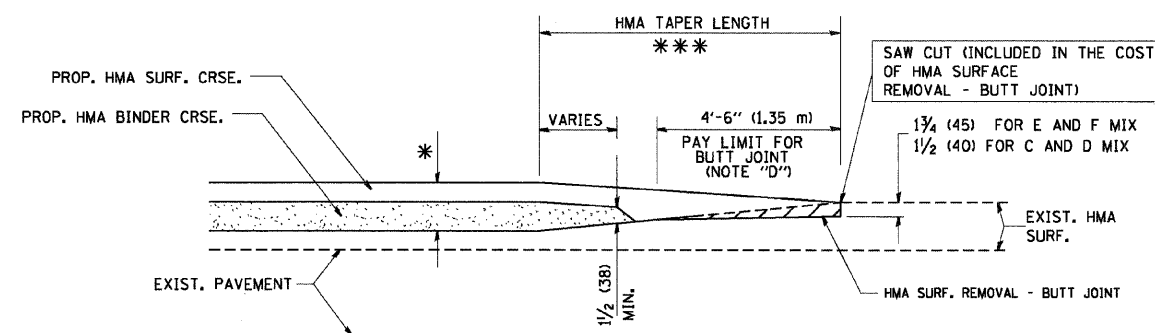
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

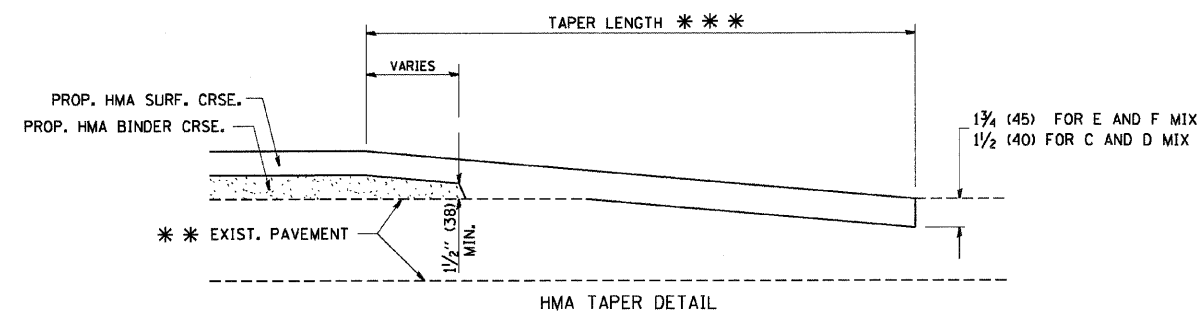
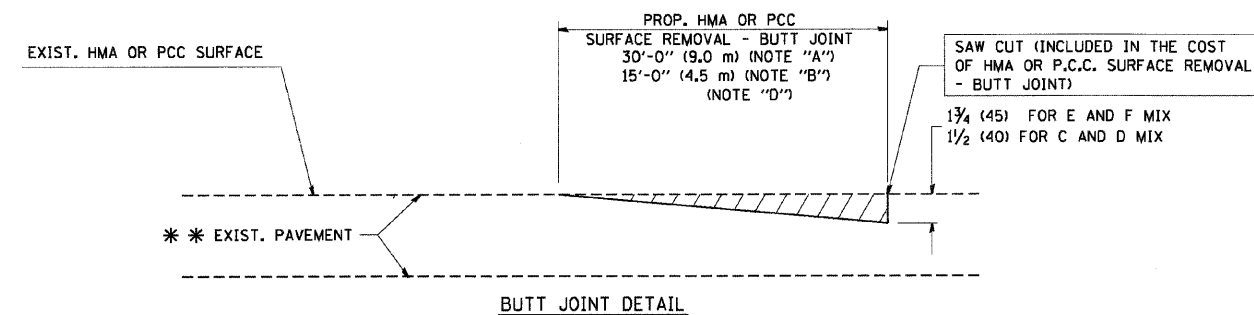


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

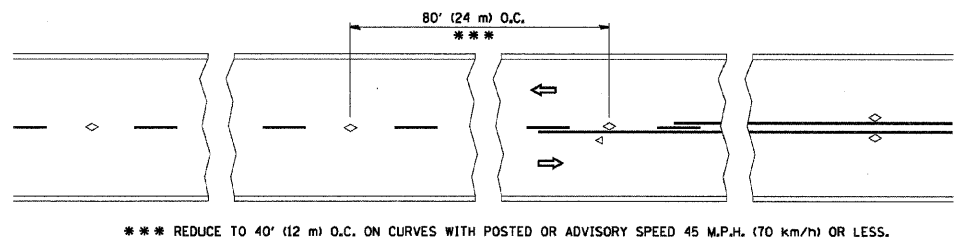
BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
HORIZ.

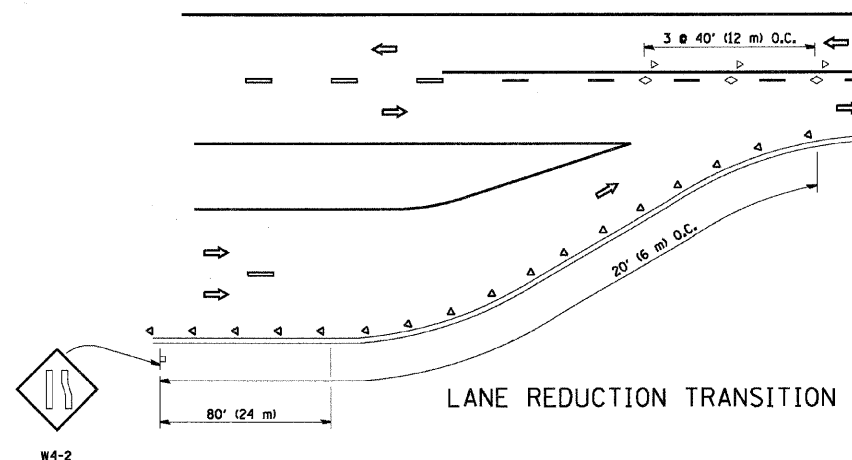
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CHECKED BY

BD400-05 (VI-BD32)

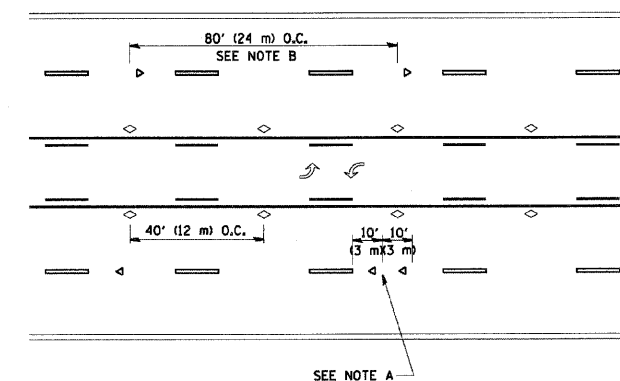
CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	30
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



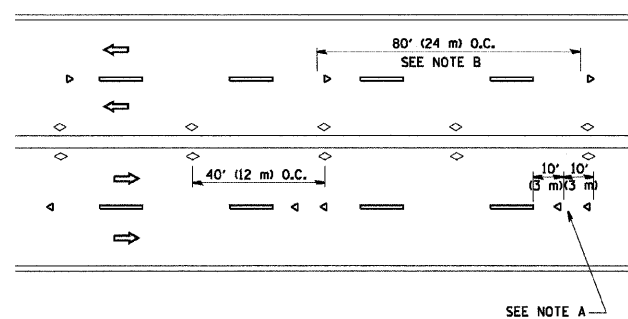
TWO-LANE/TWO-WAY



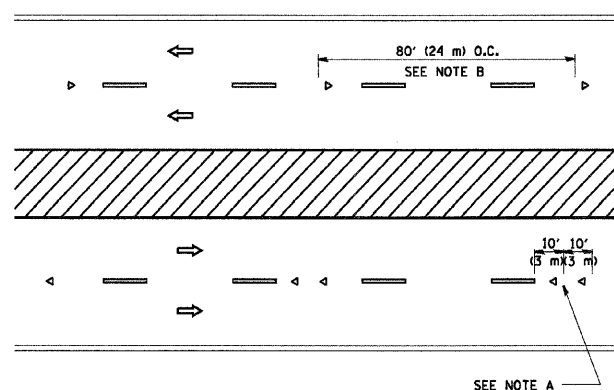
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

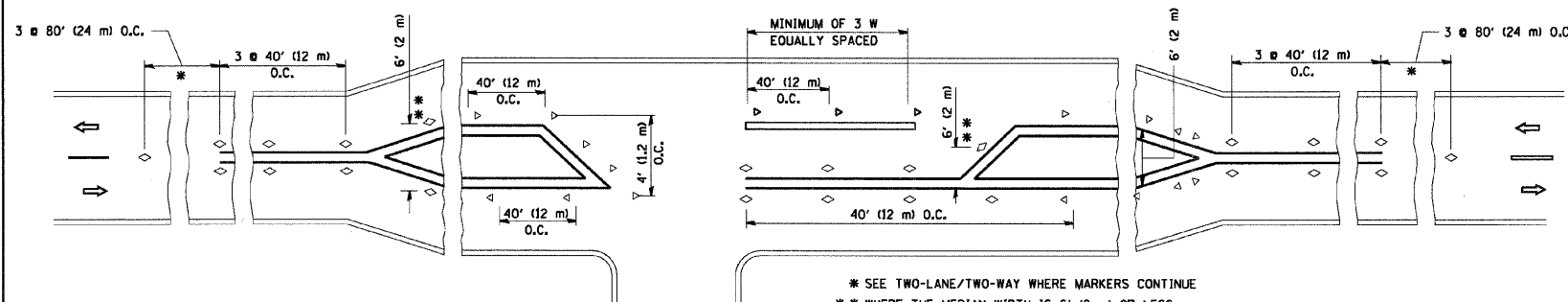
LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.



LEFT TURN

- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT
MARKERS (SNOW-PLOW RESISTANT)

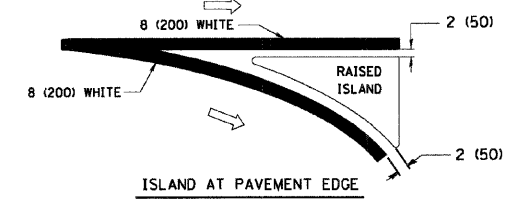
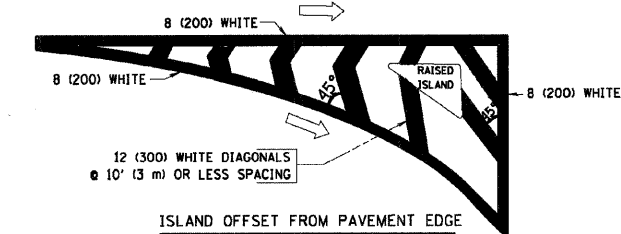
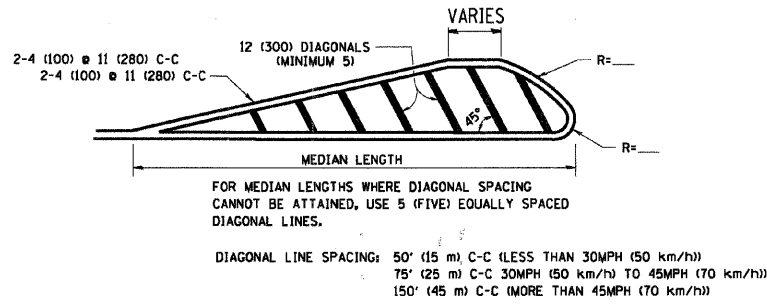
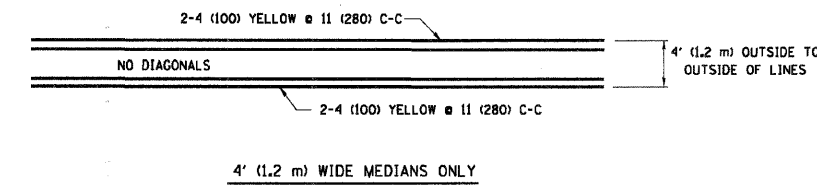
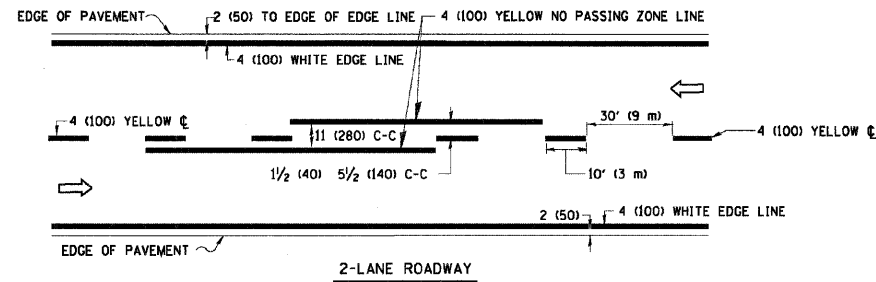
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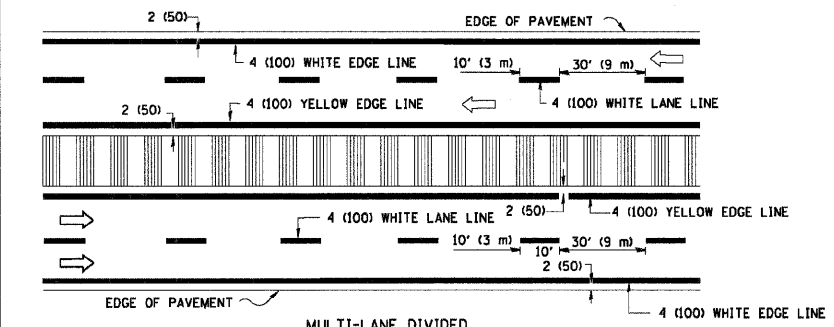
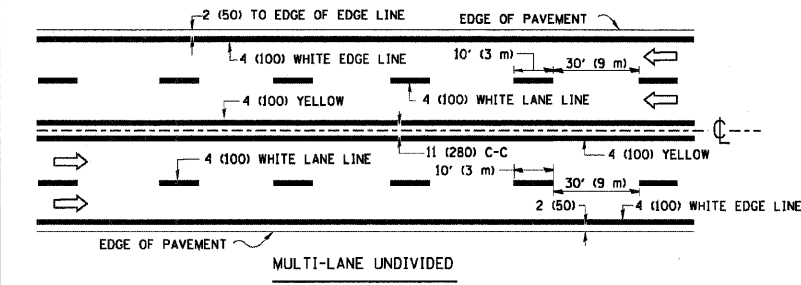
TC-11

PLOT DATE = 3/6/2007
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USER NAME = bbarndt

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1317	111 B-2	WILL	32	31
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

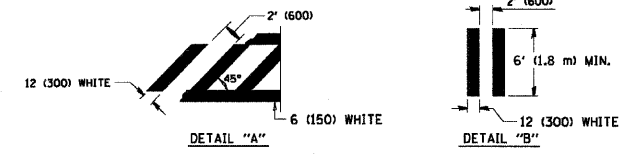
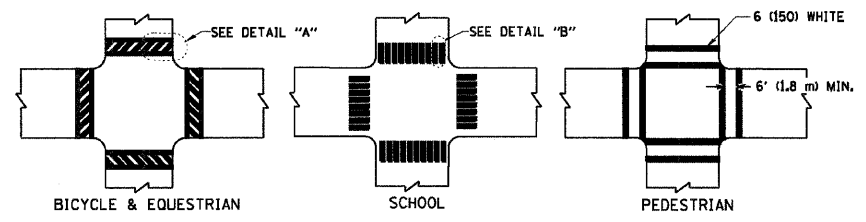


TYPICAL ISLAND MARKING

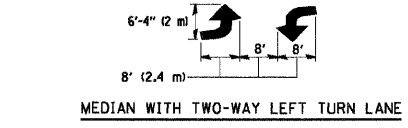
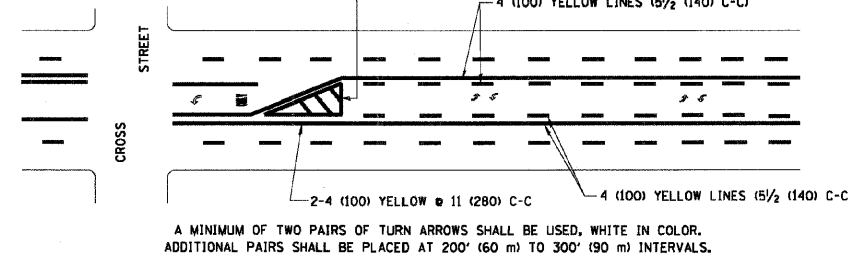


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

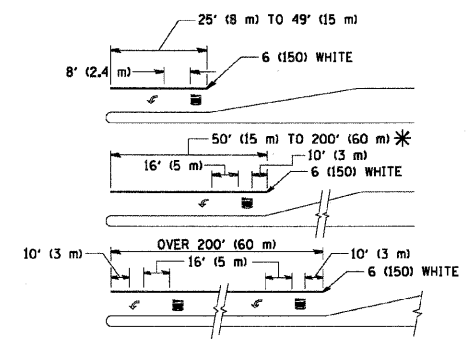
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

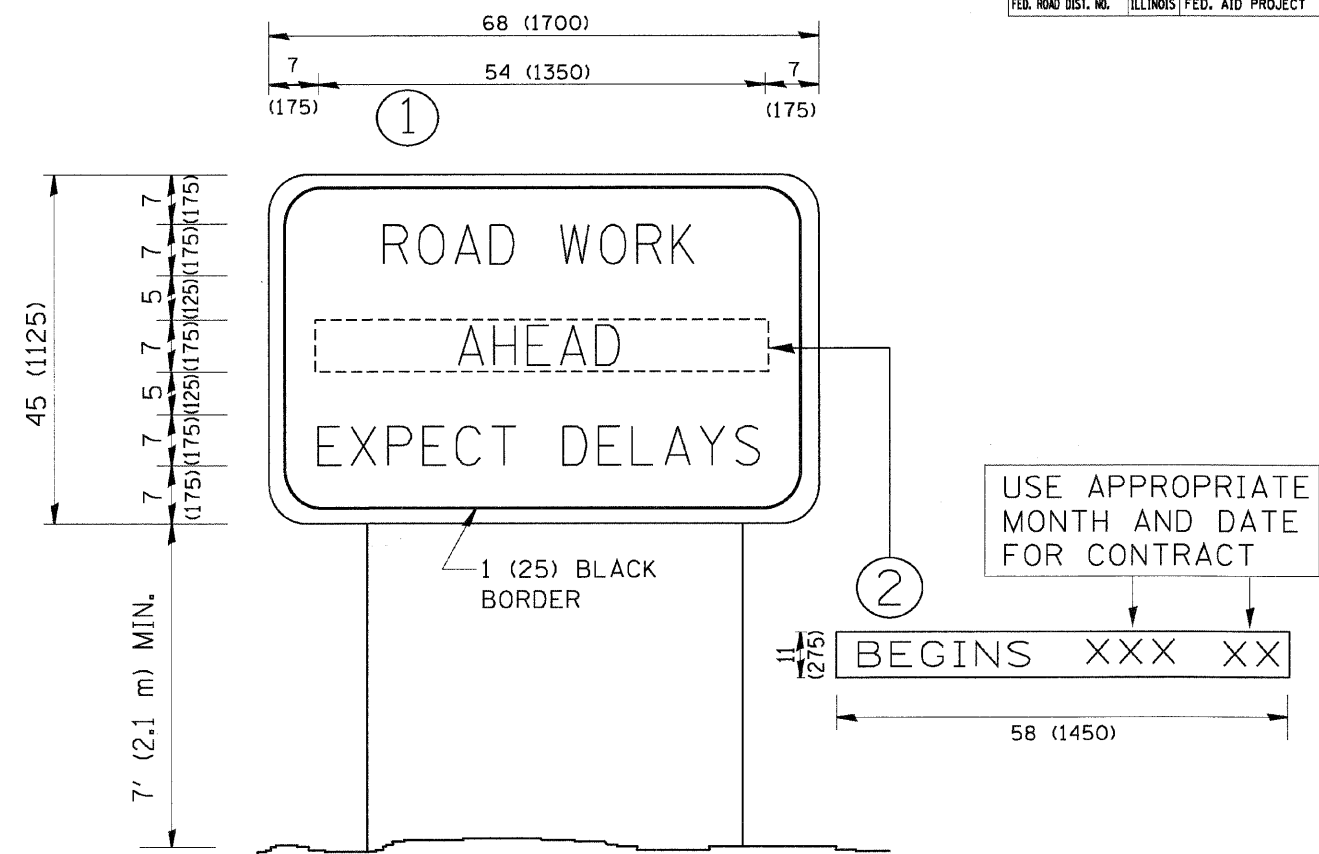
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE

DRAWN BY CADD
CHECKED BY

CONTRACT NO.			
F.A. RYE	SECTION	COUNTY	TOTAL SHEET NO.
1317	111 B-2	WILL	32 32
STA.		TO STA.	
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN

CHECKED BY

TC22

PLOT DATE = 3/18/2007
FILE NAME = K:\hst\td\1022.dgn
PLOT SCALE = 1/8" = 1' / IN.
USER NAME = bauerfd