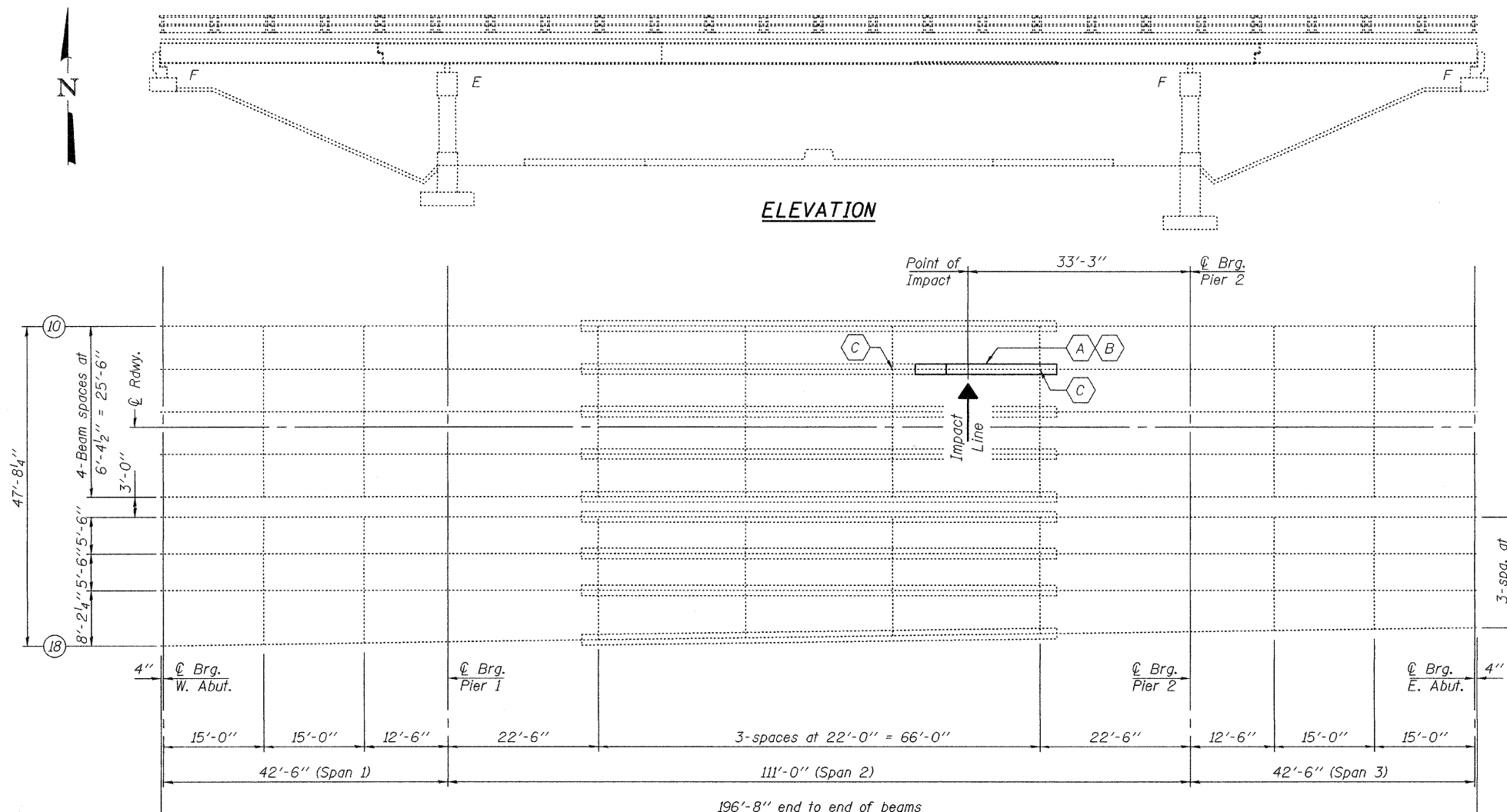


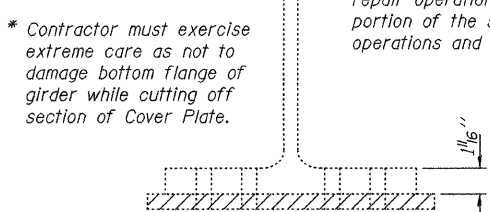
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

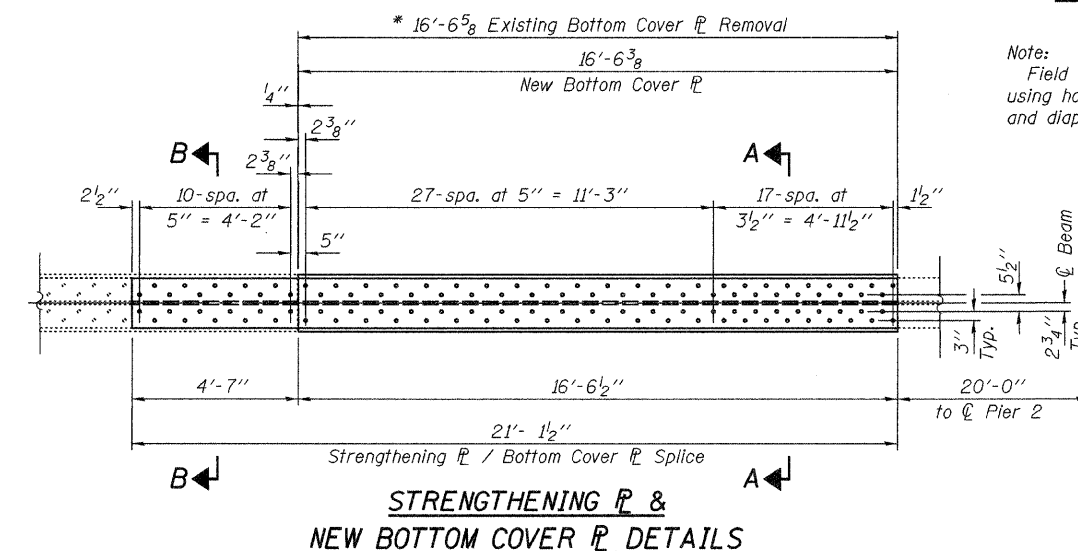
All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
Fasteners shall be high strength bolts. Bolts $\frac{7}{8}$ " ϕ , open holes $\frac{15}{16}$ " ϕ , unless otherwise noted.
After the new beam is in its final position and/or beam straightening operations have been completed, the Engineer in the field shall check to see that the top flange is tight against the slab. If not, the Contractor shall inject epoxy between the existing concrete deck and the top flange of the beam. See Special Provision "Epoxy Injection".
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Structural Steel Repair.
The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all steel surfaces shall be gray, Munsell No. 5B 7/1. See Special Provision "Cleaning and Painting New Metal Structures".
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.
Grind existing nicks, gouges and shallow cracks in the damaged beams as detailed. Grinding shall be done parallel to the longitudinal axis of the member. Ground surfaces shall be inspected for cracks using dye penetrant or magnetic particle testing prior to initiating any beam straightening operations. Any cracks that cannot be removed by grinding approximately $\frac{1}{4}$ " deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. Ground surfaces shall be spot cleaned and painted with an aluminum epoxy mastic primer followed by a finish coat to match the color of the existing beam. Cost of grinding, testing and spot painting included with Beam Straightening.
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
Diaphragm connection holes shall be $\frac{15}{16}$ " ϕ for $\frac{3}{4}$ " ϕ bolts. Two hardened washers shall be required at diaphragm connections.



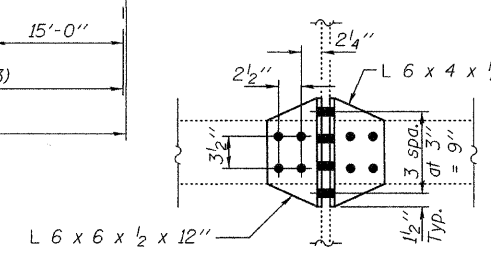
- Notes:
Beam Straightening shall be done after portion of Bottom Cover \bar{P} has been removed and prior to erecting the new Cover \bar{P} and Strengthening \bar{P} 's.
Traffic shall be removed from the beam to be repaired prior to commencing the repair operations and shall be kept off that portion of the structure during the repair operations and until all repairs are complete.
- (A) - Existing Beam to be straightened & strengthened
 - (B) - Replace portion of Bottom Cover \bar{P}
 - (C) - Replace Bottom Clip Angles on both sides of beam



SECTION A-A
* (Showing Removal)

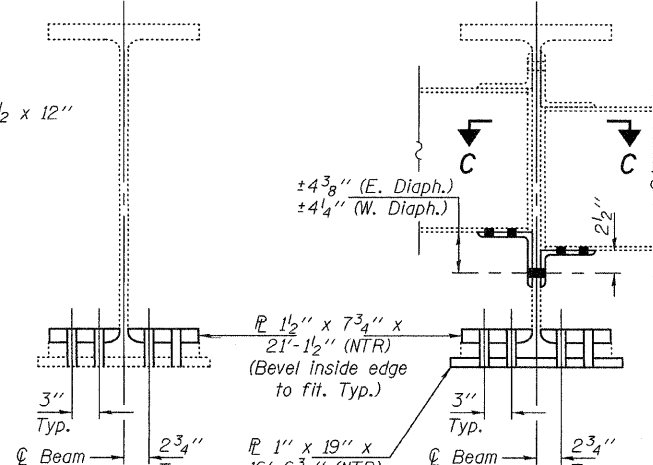


STRENGTHENING \bar{P} & NEW BOTTOM COVER \bar{P} DETAILS



SECTION C-C

Note: Field drill holes in angles using holes in existing beam and diaphragms as template.



SECTION B-B
(Diaphragms & Clip Angles not shown for clarity)

SECTION A-A
(Showing Repairs)

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Beam Straightening	L.S.	1
Structural Steel Repair	Pounds	2960

**PLAN & ELEVATION
US ROUTE 20**

DESIGNED *Adrian T. Holloway*
CHECKED *[Signature]*
DRAWN *Kyle M. Steffen*
CHECKED *MB*

AUGUST 11, 2008

EXAMINED *[Signature]*
PASSED *[Signature]*

ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES



Expires: November 30, 2008

SHEET NO. 1	F.A. RTE. 301	SECTION (3HB & 3HB-2)M	COUNTY WINNEBAGO	TOTAL SHEETS 13	SHEET NO. 6A
2 SHEETS	SN 101-0056		CONTRACT NO. 64E16		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		