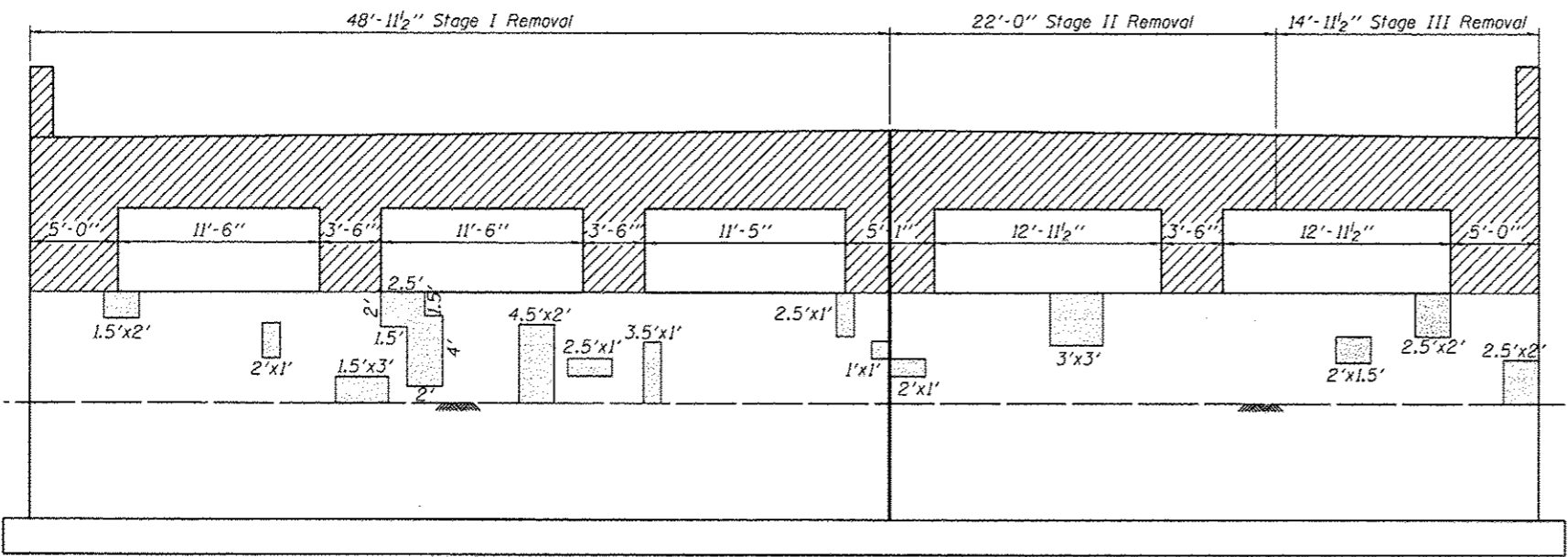


PIER 3 ELEVATION
(Looking East)



PIER 3 ELEVATION
(Looking West)

BILL OF MATERIAL

Item	Unit	Quantity
Concrete Removal	cu. yd.	47
Structural Repair of Concrete (Depth Equal to or Less than 5")	sq. ft.	189

LEGEND

- Concrete Removal
- Structural Repair of Concrete (Depth Equal to or Less than 5")

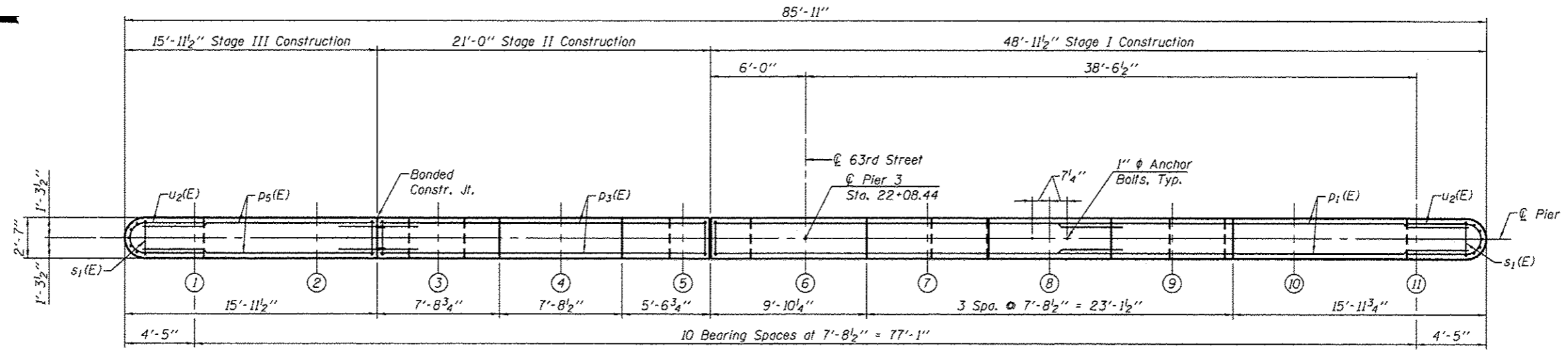
NOTES

- Clean and reuse 8'-7" length of existing #9 vertical column bars at all columns. Cost included with Concrete Removal.

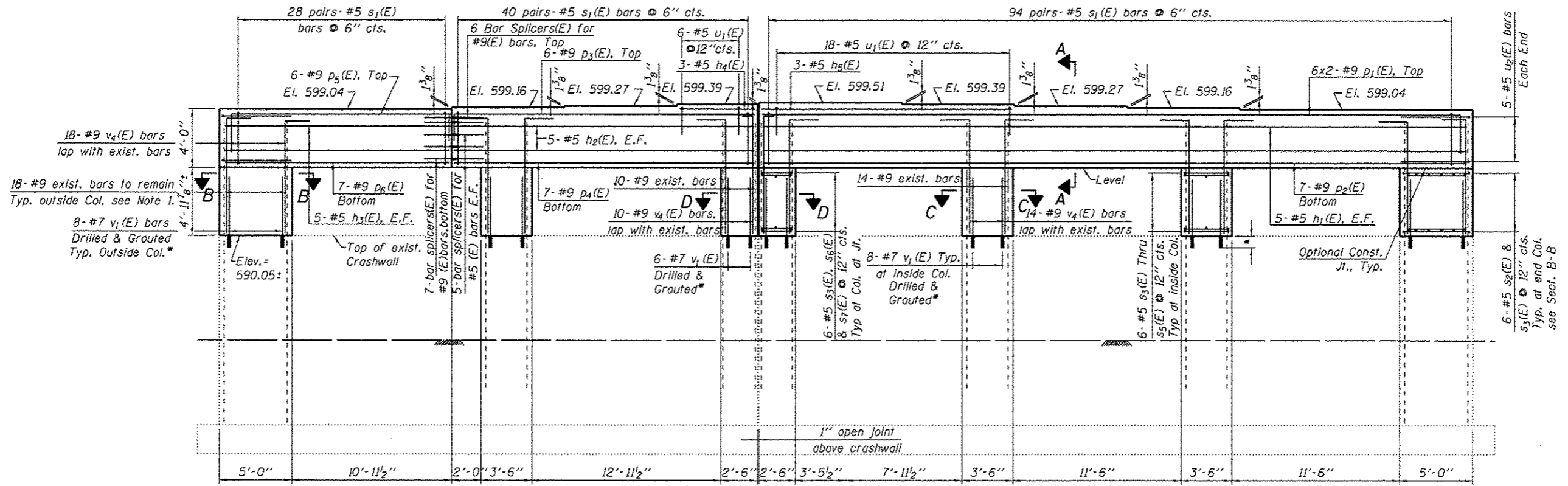
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	DATE - 5/17/2013	REVISED -

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	CODK	137	101
CONTRACT NO. 60J15				
ILLINOIS FED. AID PROJECT				



TOP PLAN



PIER 3 ELEVATION
(Looking East)

NOTES

1. Clean and reuse 8'-7" length of existing #9 vertical column bars at all columns. Cost included with Concrete Removal.
2. *Epoxy grout v₁(E) bars in 13" (min.) drilled holes according to Article 584 of the Standard Specifications.
3. See Sheets S46 and S47 for sections and Bill of Material.
4. Space reinforcement in cap to miss anchor bolts.
5. Pour steps monolithically with cap.

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	DATE - 5/17/2013	REVISIONS -

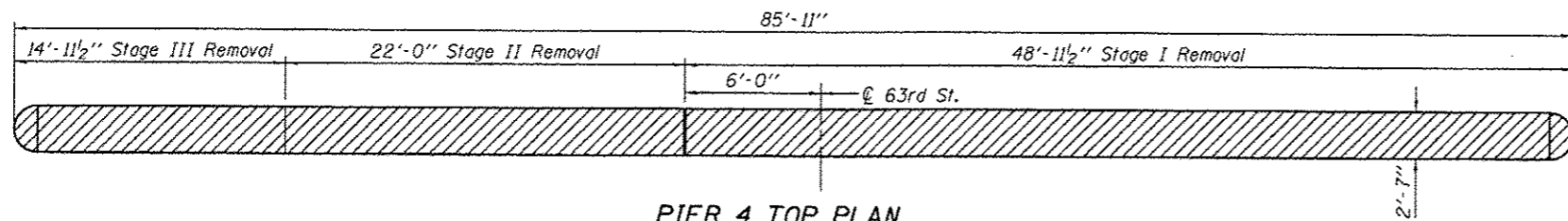
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 3 PLAN & ELEVATION
STRUCTURE NO. 016-1149

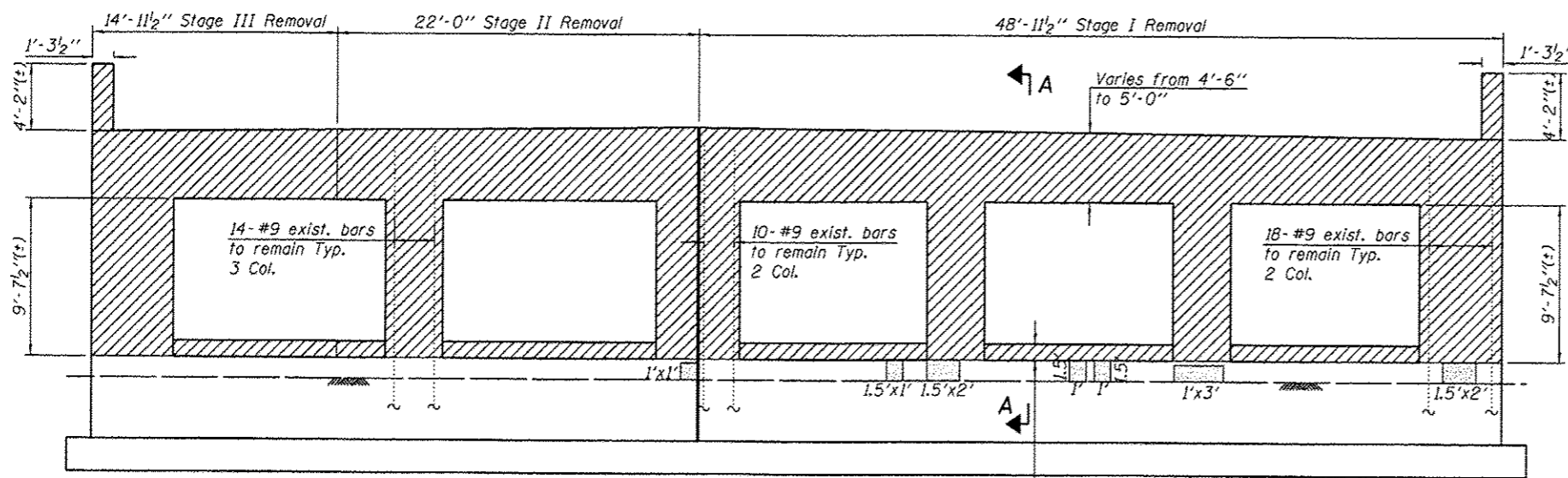
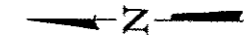
SHEET NO. S43 OF 50 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	102
CONTRACT NO. 60J15				

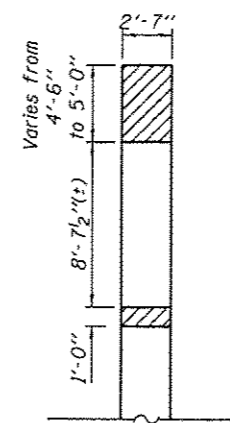
ILLINOIS FED. AID PROJECT



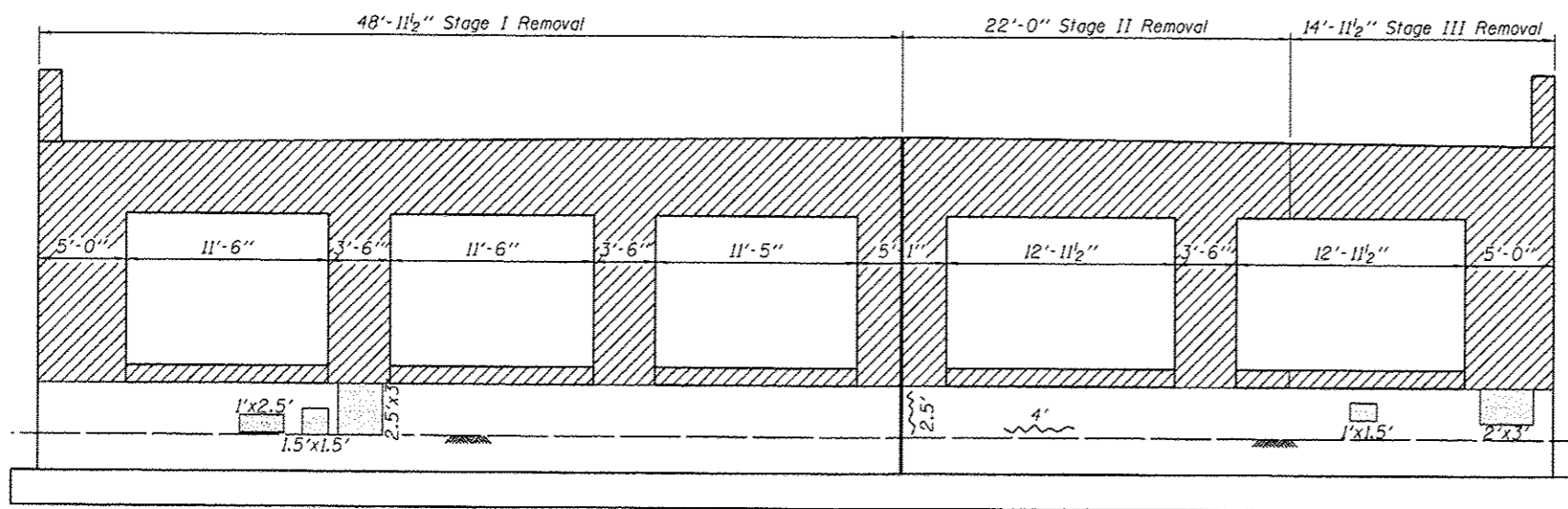
PIER 4 TOP PLAN



PIER 4 ELEVATION
(Looking East)



SECTION A-A



PIER 4 ELEVATION
(Looking West)

BILL OF MATERIAL

Item	Unit	Quantity
Concrete Removal	cu. yd.	63
Epoxy Crack Injection	ft.	7
Structural Repair of Concrete (Depth Equal to or Less than 5")	sq. ft.	34

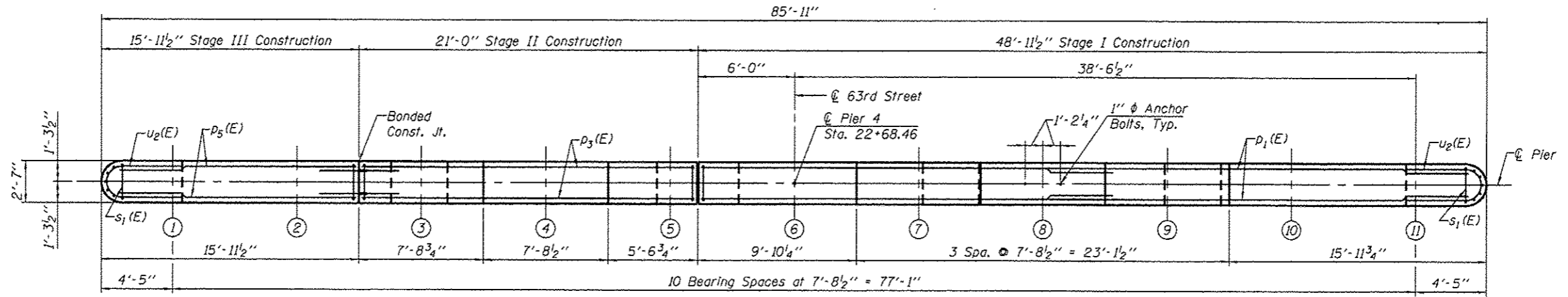
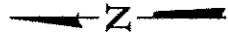
LEGEND

- Concrete Removal
- Structural Repair of Concrete (Depth Equal to or Less than 5")
- Epoxy Crack Injection

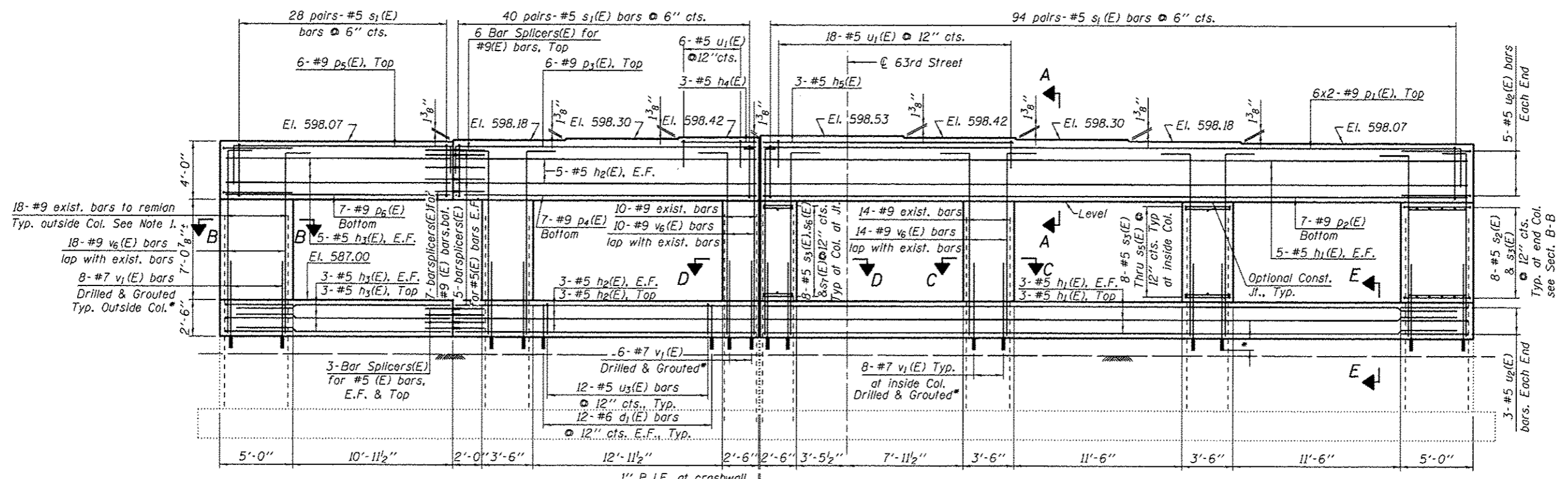
NOTES

- Clean and reuse 8'-7" length of existing #9 vertical column bars at all columns. Cost included with Concrete Removal.

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PIER 4 TOP PLAN



PIER 4 ELEVATION

(Looking East)

NOTES

1. Clean and reuse 8'-7" length of existing #9 vertical column bars at all columns. Cost Included with Concrete Removal.
2. *Epoxy grout v1(E) bars in 13" (min.) drilled holes according to Article 584 of the Standard Specifications.
3. See Sheets S46 and S47 for sections and Bill of Material.
4. Space reinforcement in cap to miss anchor bolts.
5. Pour steps monolithically with cap.

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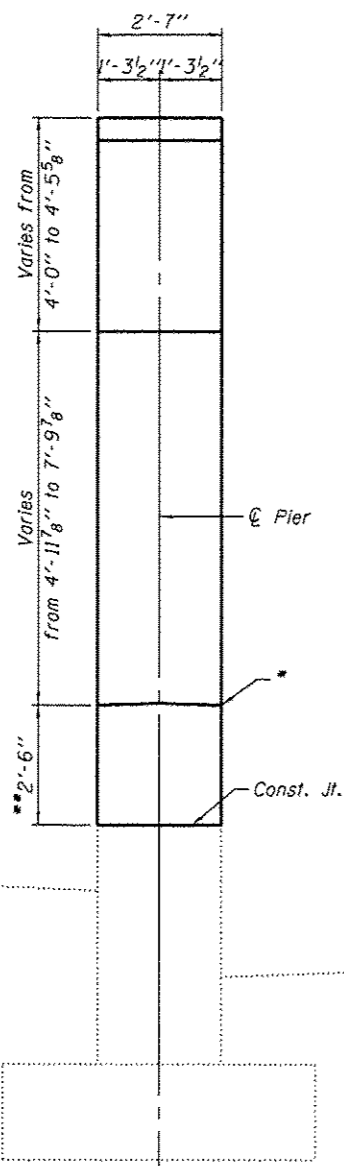


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	DATE - 5/17/2013	REVISED -

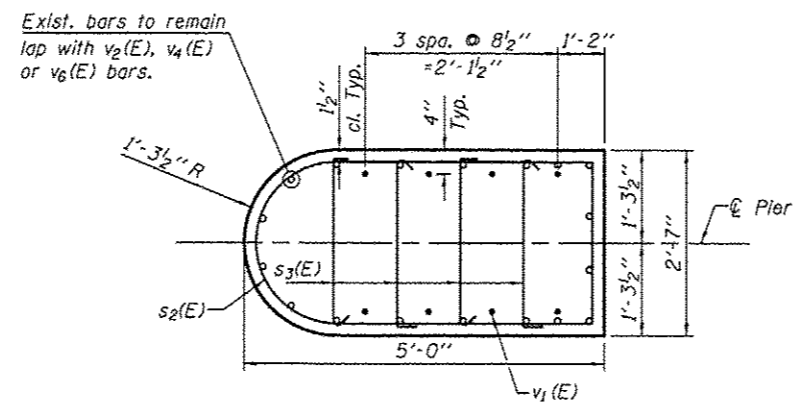
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 4 PLAN & ELEVATION
STRUCTURE NO. 016-1149
SHEET NO. S45 OF 50 SHEETS

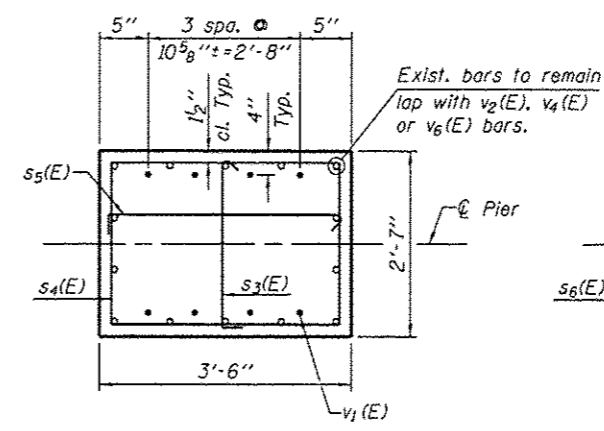
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	104
CONTRACT NO. 60J15				
ILLINOIS FED. AID PROJECT				



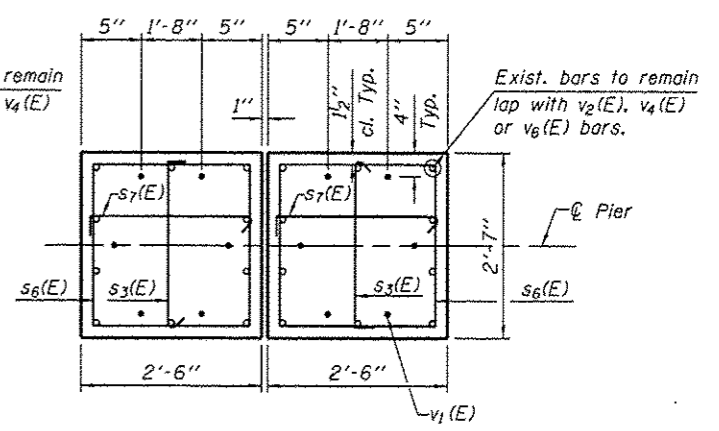
END VIEW



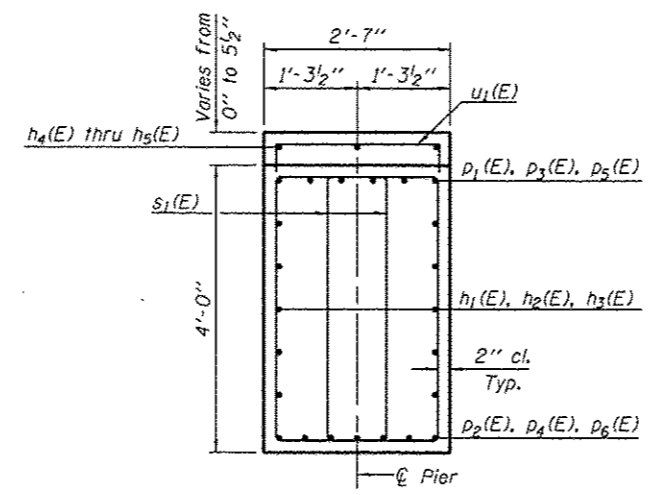
SECTION B-B



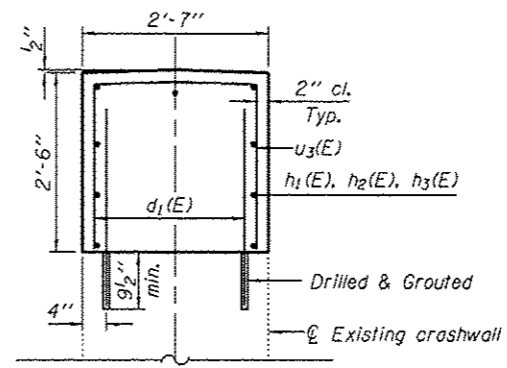
SECTION C-C



SECTION D-D



SECTION A-A



SECTION E-E

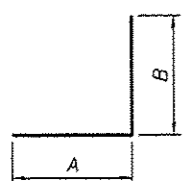
* The Location of the elevation at the top of crashwall shown in the Pier Elevation view.

**For Piers 1 & 4 only.

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	DATE - 5/17/2013	REVISED -

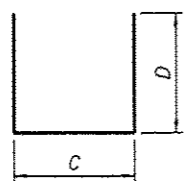
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	105
CONTRACT NO. 60J15				
ILLINOIS FED. AID PROJECT				



A & B DIMENSIONS

Bar	A	B
p1(E)	29'-2"	1'-7"
v2(E)	11'-6"	1'-7"
v4(E)	8'-9"	1'-7"
v6(E)	10'-10"	1'-7"

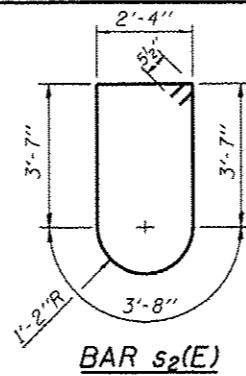
BARS p1(E), v2(E), v4(E) & v6(E)



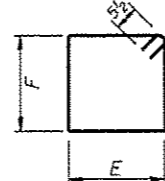
C & D DIMENSIONS

Bar	C	D
p3(E)	20'-8"	1'-7"
p5(E)	14'-5"	1'-7"
u1(E)	2'-3"	1'-11"
u3(E)	2'-3"	2'-3"

BARS p3(E), p5(E), u1(E) & u3(E)



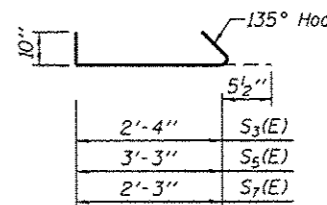
BAR s2(E)



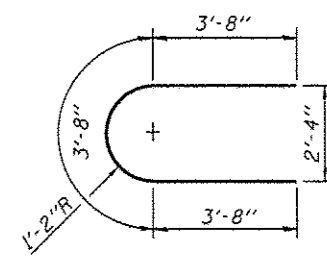
E & F DIMENSIONS

Bar	E	F
s1(E)	1'-6"	3'-8"
s4(E)	3'-3"	2'-4"
s6(E)	2'-3"	2'-4"

BARS s1(E), s4(E) & s6(E)



BARS s3(E), s5(E) & s7(E)



BAR u2(E)

**PIER 1
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d1(E)	120	#6	3'-1"	—
h1(E)	19	#5	47'-4"	—
h2(E)	19	#5	20'-8"	—
h3(E)	19	#5	14'-5"	—
h4(E)	3	#5	5'-2"	—
h5(E)	3	#5	17'-3"	—
p1(E)	12	#9	30'-9"	┘
p2(E)	7	#9	47'-3"	—
p3(E)	6	#9	23'-10"	┘
p4(E)	7	#9	20'-8"	—
p5(E)	6	#9	17'-7"	┘
p6(E)	7	#9	14'-5"	—
s1(E)	324	#5	11'-3"	□
s2(E)	16	#5	14'-1"	□
s3(E)	104	#5	3'-8"	┘
s4(E)	24	#5	12'-1"	□
s5(E)	24	#5	4'-7"	┘
s6(E)	16	#5	10'-1"	□
s7(E)	16	#5	3'-7"	┘
u1(E)	24	#5	6'-1"	┘
u2(E)	16	#5	11'-0"	┘
u3(E)	60	#5	6'-9"	┘
v1(E)	52	#7	6'-3"	—
v2(E)	98	#9	13'-1"	—
Concrete Structures			Cu. Yd.	71.8
Reinforcement Bars, Epoxy Coated			Pound	17,180
Concrete Sealer			Sq. Ft.	2,166

**PIER 2
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h1(E)	10	#5	47'-4"	—
h2(E)	10	#5	20'-8"	—
h3(E)	10	#5	14'-5"	—
h4(E)	3	#5	5'-2"	—
h5(E)	3	#5	17'-3"	—
p1(E)	12	#9	30'-9"	┘
p2(E)	7	#9	47'-3"	—
p3(E)	6	#9	23'-10"	┘
p4(E)	7	#9	20'-8"	—
p5(E)	6	#9	17'-7"	┘
p6(E)	7	#9	14'-5"	—
s1(E)	324	#5	11'-3"	□
s2(E)	12	#5	14'-1"	□
s3(E)	78	#5	3'-8"	┘
s4(E)	18	#5	12'-1"	□
s5(E)	18	#5	4'-7"	┘
s6(E)	12	#5	10'-1"	□
s7(E)	12	#5	3'-7"	┘
u1(E)	24	#5	6'-1"	┘
u2(E)	10	#5	11'-0"	┘
v1(E)	52	#7	6'-3"	—
v4(E)	98	#9	10'-4"	—
Concrete Structures			Cu. Yd.	45.9
Reinforcement Bars, Epoxy Coated			Pound	14,130
Concrete Sealer			Sq. Ft.	1,371

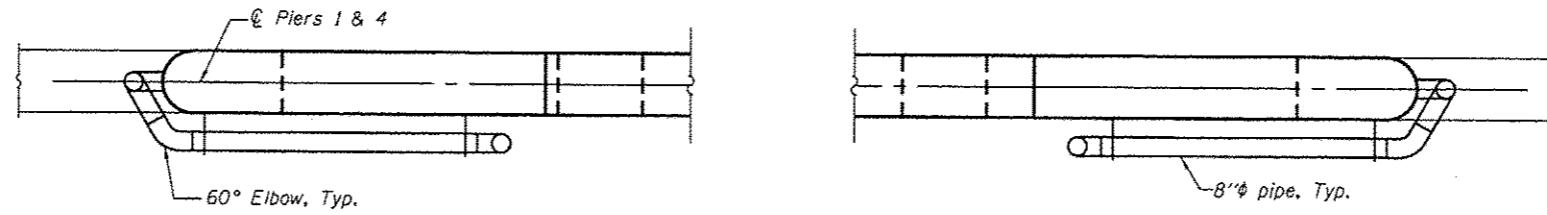
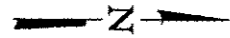
**PIER 3
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h1(E)	10	#5	47'-4"	—
h2(E)	10	#5	20'-8"	—
h3(E)	10	#5	14'-5"	—
h4(E)	3	#5	5'-2"	—
h5(E)	3	#5	17'-3"	—
p1(E)	12	#9	30'-9"	┘
p2(E)	7	#9	47'-3"	—
p3(E)	6	#9	23'-10"	┘
p4(E)	7	#9	20'-8"	—
p5(E)	6	#9	17'-7"	┘
p6(E)	7	#9	14'-5"	—
s1(E)	324	#5	11'-3"	□
s2(E)	12	#5	14'-1"	□
s3(E)	78	#5	3'-8"	┘
s4(E)	18	#5	12'-1"	□
s5(E)	18	#5	4'-7"	┘
s6(E)	12	#5	10'-1"	□
s7(E)	12	#5	3'-7"	┘
u1(E)	24	#5	6'-1"	┘
u2(E)	10	#5	11'-0"	┘
v1(E)	52	#7	6'-3"	—
v4(E)	98	#9	10'-4"	—
Concrete Structures			Cu. Yd.	45.3
Reinforcement Bars, Epoxy Coated			Pound	14,130
Concrete Sealer			Sq. Ft.	1,348

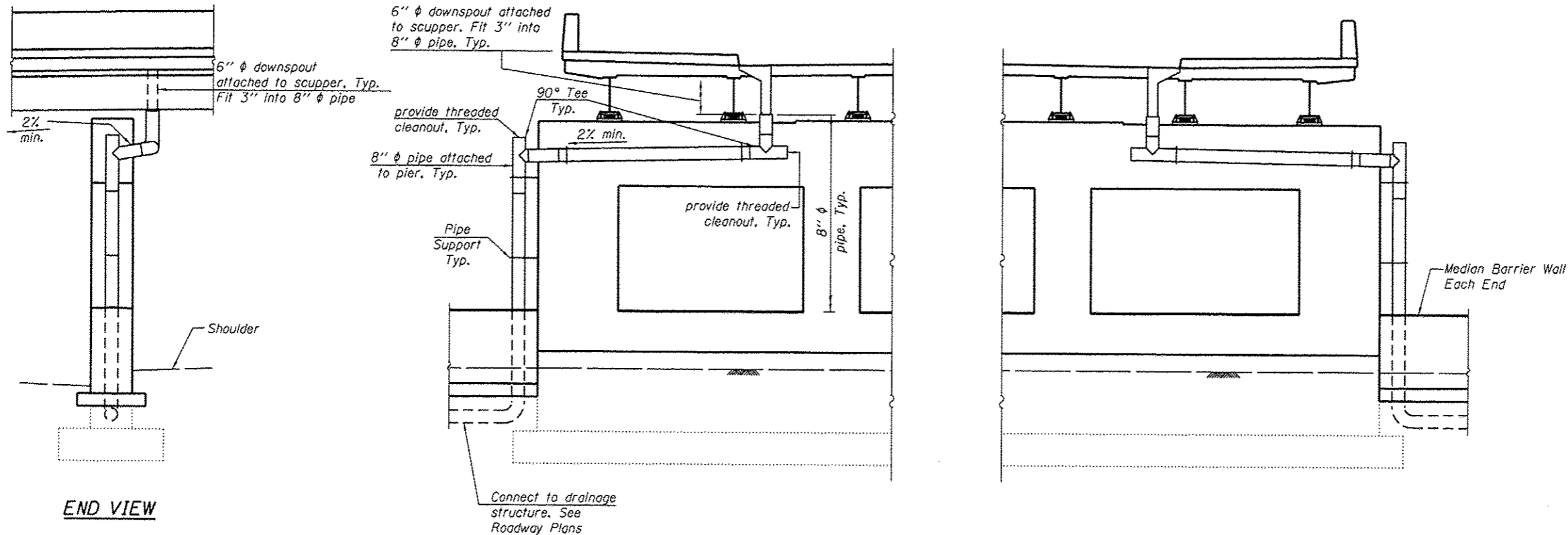
**PIER 4
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d1(E)	120	#6	3'-1"	—
h1(E)	19	#5	47'-4"	—
h2(E)	19	#5	20'-8"	—
h3(E)	19	#5	14'-5"	—
h4(E)	3	#5	5'-2"	—
h5(E)	3	#5	17'-3"	—
p1(E)	12	#9	30'-9"	┘
p2(E)	7	#9	47'-3"	—
p3(E)	6	#9	23'-10"	┘
p4(E)	7	#9	20'-8"	—
p5(E)	6	#9	17'-7"	┘
p6(E)	7	#9	14'-5"	—
s1(E)	324	#5	11'-3"	□
s2(E)	16	#5	14'-1"	□
s3(E)	104	#5	3'-8"	┘
s4(E)	24	#5	12'-1"	□
s5(E)	24	#5	4'-7"	┘
s6(E)	16	#5	10'-1"	□
s7(E)	16	#5	3'-7"	┘
u1(E)	24	#5	6'-1"	┘
u2(E)	16	#5	11'-0"	┘
u3(E)	60	#5	6'-9"	┘
v1(E)	52	#7	6'-3"	—
v6(E)	98	#9	12'-5"	—
Concrete Structures			Cu. Yd.	70.2
Reinforcement Bars, Epoxy Coated			Pound	16,960
Concrete Sealer			Sq. Ft.	2,112

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PARTIAL PIER PLAN VIEW



PARTIAL PIERS 1 & 4 ELEVATION
(Looking West)

BILL OF MATERIAL

Item	Unit	Total
Drainage System	L. Sum	1

NOTES

- The pipe, fittings, pipe supports, inserts and bolts above the median barrier wall for the 4 locations shown are included in the pay item DRAINAGE SYSTEM. The 6" diameter downspouts attached to scuppers are included with DRAINAGE SCUPPERS, DS-12.

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GRÖEF
 8501 W. Higgins Road Suite 280
 Chicago, Illinois 60631 (773) 399-0112

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	DATE - 5/17/2013	REVISED -

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 DEPARTMENT OF TRANSPORTATION

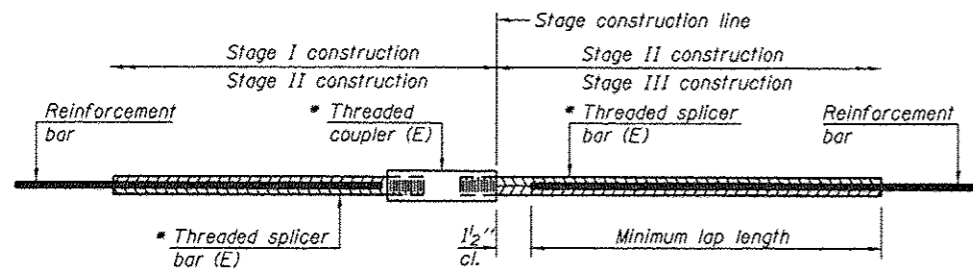
BRIDGE DRAINAGE SYSTEM
 STRUCTURE NO. 016-1149

SHEET NO. 548 OF 50 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	107

CONTRACT NO. 60J15

ILLINOIS FED. AID PROJECT



STANDARD BAR SPLICER ASSEMBLY

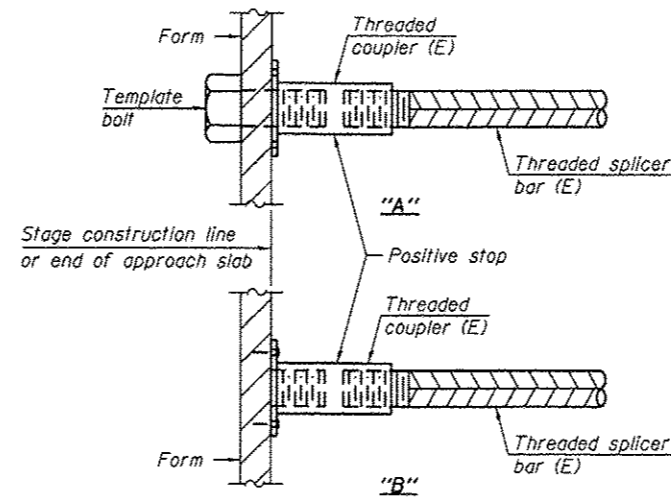
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar lap, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

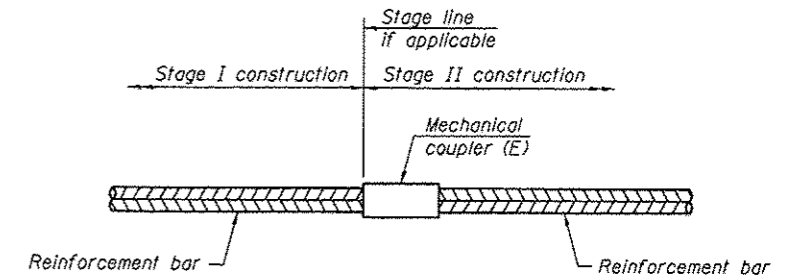
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Deck	#5	2,152	5
Approach Slab	#4	100	6
Approach Slab	#5	344	5
West Abutment	#5	19	6
East Abutment	#5	19	6
Pier 1 Cap, Top	#9	6	6
Pier 1 Cap, Bottom	#9	7	5
Pier 1 Cap, Side	#5	10	6
Pier 1 Crashwall	#5	9	6
Pier 2 Cap, Top	#9	6	6
Pier 2 Cap, Bottom	#9	7	5
Pier 2 Cap, Side	#5	10	6
Pier 3 Cap, Top	#9	6	6
Pier 3 Cap, Bottom	#9	7	5
Pier 3 Cap, Side	#5	10	6
Pier 4 Cap, Top	#9	6	6
Pier 4 Cap, Bottom	#9	7	5
Pier 4 Cap, Side	#5	10	6
Pier 4 Crashwall	#5	9	6



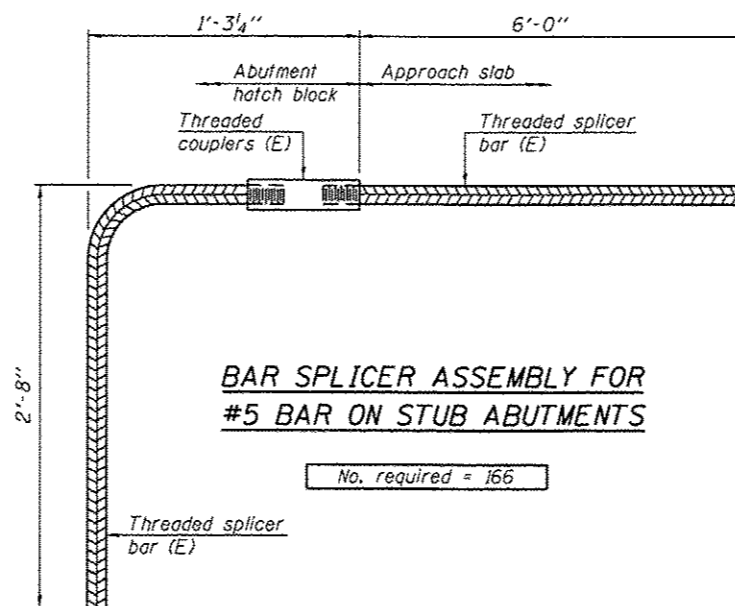
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required = 166

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 8-31-12

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	DATE - 5/17/2013	REVISED -

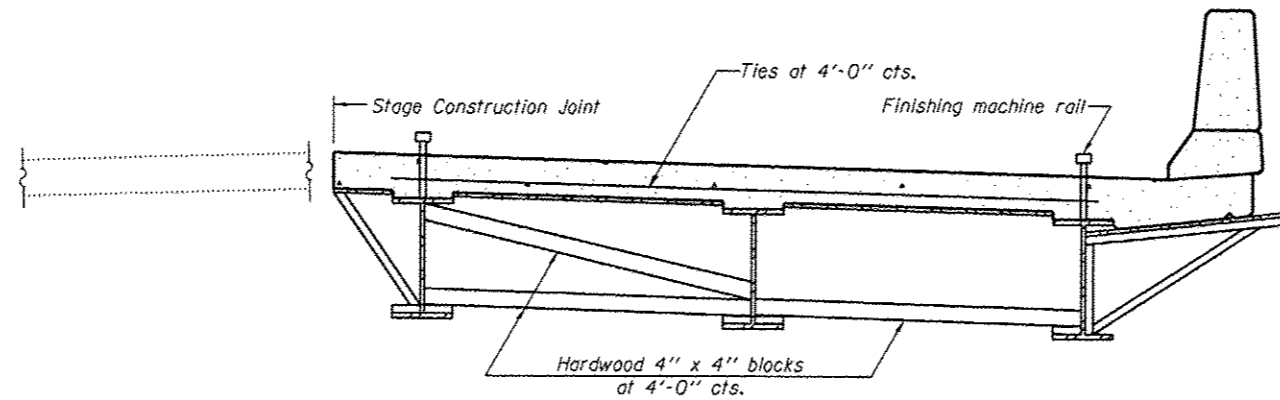
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY DETAILS
 STRUCTURE NO. 016-1149

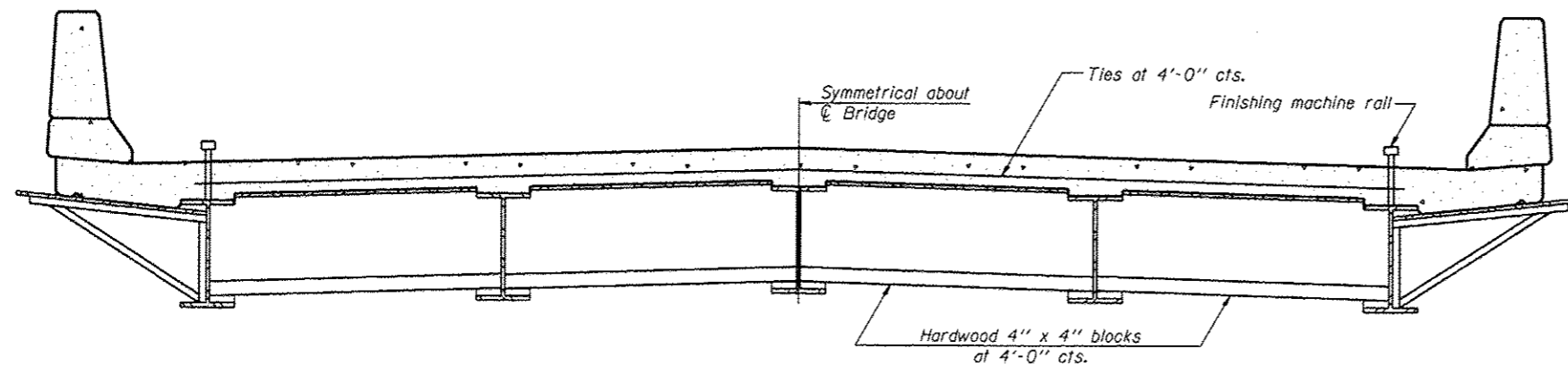
SHEET NO. 549 OF 50 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-6	COOK	137	108
CONTRACT NO. 60J15				
ILLINOIS FED. AID PROJECT				

When cantilever forming brackets are used, the work shall be done according to Article 503.06(b) of the Standard Specifications, except as modified below and in the details shown on this sheet.
 The finishing machine rails shall be placed on the top flange of the exterior beams.
 The beams or girders, supporting cantilever forming brackets, shall be tied together at 4 foot intervals.
 For Standard construction, or Stage Construction the Hardwood bracing materials shall be placed as shown between webs of beams in each bay.



**FORM BRACES FOR
STAGE CONSTRUCTION**



**FORM BRACES FOR
STANDARD CONSTRUCTION**

H:\Jobs\2013\20130222\CAD\Structural\Drawings\016-1149-Final\00\06015-SS0-cent.dgn
 5/20/2013 10:24:19 AM

SB-1

7-1-10

GRAF
 8501 W. Higgins Road, Suite 280
 Chicago, Illinois 60631 (773) 399-0112

USER NAME :	DESIGNED - J.Z.	REVISED -
	CHECKED - J.A.Z.	REVISED -
PLOT SCALE :	DRAWN - E.E.J.	REVISED -
PLOT DATE :	DATE - 5/17/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CANTILEVER FORMING BRACKETS FOR SUPERSTRUCTURE
STRUCTURE NO. 016-1149**

SHEET NO. 550 OF 50 SHEETS

P.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	109
				CONTRACT NO. 60J15

ILLINOIS FED. AID PROJECT

INDEX OF SHEETS

SHEET NO.

- 1 TITLE PAGE
- 2 GENERAL PLAN
- 3 EXCAVATION, FOUNDATION LAYOUT & DRAINAGE
- 4 EAST AND WEST ABUTMENTS
- 5 WINGWALLS
- 6 RETAINING WALLS
- 7 PIERS NO. 1 & 4
- 8 PIERS NO. 2 & 3
- 9 SUPERSTRUCTURE
- 10 SUPERSTRUCTURE-SECTION & DETAILS
- 11 BEAM DETAILS
- 12 HANDRAIL LAYOUT
- 13 HANDRAIL DETAILS
- 14 APPROACH SLAB
- 15 GENERAL PLAN-ALTERATION TO CTA STRUCTURE
- 16 GIRDER DETAILS-ALTERATION TO CTA STRUCTURE
- 17 FRAMING PLAN & DETAILS
ALTERATION TO CTA STRUCTURE
- 18 FALSEWORK
- 19 TEST BORING LOGS & LAB DATA
- 20 FIELD OFFICE AND LABORATORY
- 21 FOUNDATION REMOVAL & PLUGGING
EXISTING DRAINS & SEWERS
- BILL OF MATERIAL-PAVING
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- NOSING DETAIL & MODIFIED TYPE 4 MEDIAN
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NAME PLATES
- 23 TYPICAL CROSS SECTIONS
- 24 STAGES OF CONSTRUCTION
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- 26-27 PAVEMENT DETAILS
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- 32-36 CROSS SECTIONS
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CONSTRUCTION OF PARKWAYS
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- 44 CURB & GUTTER, FRAME & GRATE-TYPE 31, 32 & 34
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FIELD JOINT
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- 48 ELECTRICAL PLANS

STATE OF ILLINOIS
COUNTY OF COOK
DEPARTMENT OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

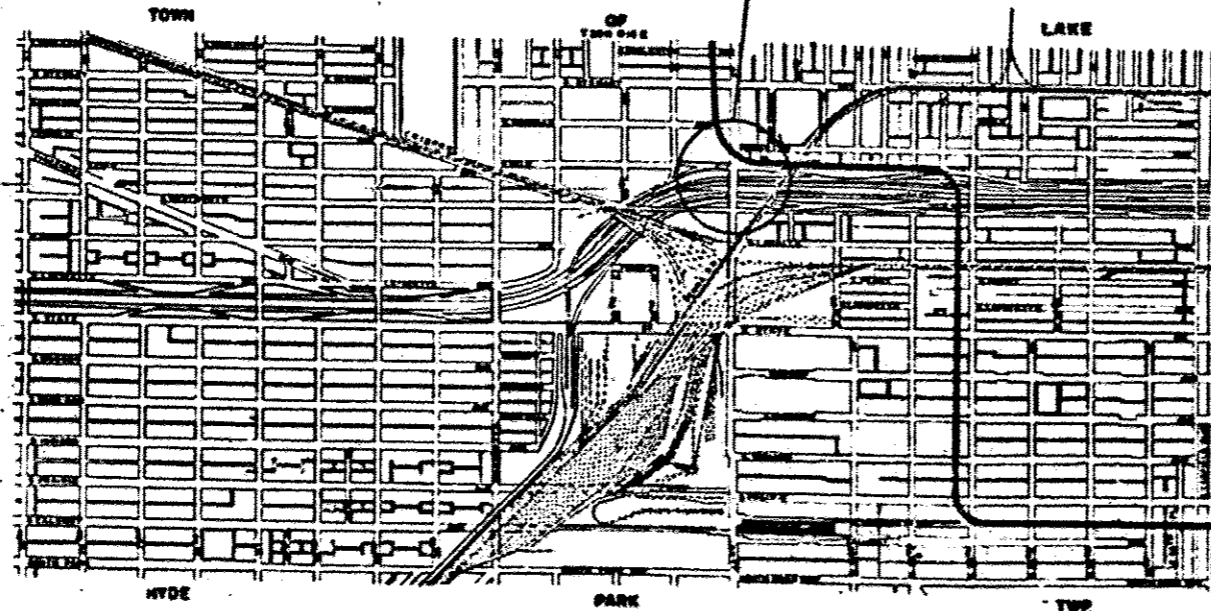
F. A. INTERSTATE ROUTE-94-PROJECT EBU-94-3(55)56
63RD STREET GRADE SEPARATION
OVER
SOUTH ROUTE EXPRESSWAY
SECTION 066-1920-G.F.

PLAN 1"=30'
PROFILE 1"=10'
CROSS SECTION 1"=10'
SCALE: 1"=30' 1"=10' 1"=10'

FOR INFORMATION ONLY

LENGTH OF STRUCTURE - 302.24 FT. (8.0540) MI.
LENGTH OF ROADWAY - 541.00 FT. (1.6270) MI.
LENGTH OF CONTRACT - 1212.02 FT. (3.2295) MI.
LENGTH ALONG EXPRESSWAY - 0.00 FT. (0.00) MI.

SECTION 066-1920-G.F. INCLUDES THE CONSTRUCTION OF A PRECAST
PRESTRESSED CONCRETE BOX GIRDER BRIDGE, 5 SIMPLE SPANS AT
52° 14'00", 58° 10", 62° 7'-0", 58° 11", 58° 7'-50"
ON 63RD STREET OVER SOUTH ROUTE EXPRESSWAY IN THE CITY OF CHICAGO,
LAKE TOWNSHIP AT STATION 249+47.11 ON SOUTH ROUTE EXPRESSWAY, A
POINT APPROXIMATELY 220 FEET WEST OF THE INTERSECTION OF
WESTNORTH AVENUE & 63RD STREET.



SUMMARY OF QUANTITIES

QUANTITY	UNIT	ITEM
1,122.1	CU. YDS.	CLASS "A" CONCRETE
124.300	YDS.	
20	YDS.	
32	YDS.	
20	YDS.	
120.000	YDS.	
800	YDS.	
154	YDS.	
1,025	YDS.	
1	YDS.	
650	YDS.	
650	YDS.	
2,130	YDS.	
2,000	YDS.	
21,983	YDS.	
127	YDS.	
2,820	YDS.	
1	YDS.	
2	YDS.	
2	YDS.	
2,800	YDS.	
20	YDS.	
520	YDS.	
2,200	YDS.	
180	YDS.	
4,750	YDS.	
120	YDS.	
104	YDS.	
100	YDS.	
1,541	YDS.	
11	YDS.	
619	YDS.	
700	YDS.	
631	YDS.	
5	YDS.	
640	YDS.	
300	YDS.	
500	YDS.	
700	YDS.	
100	YDS.	
19	YDS.	CLASS "A" CONCRETE
20	YDS.	PAVEMENT REPLACEMENT - SURFACE COURSE
20	YDS.	PAVEMENT REPLACEMENT - ENTIRE
0.2	ACRES	IRREGULARLY SHAPED
2	EACH	SECTION MARKERS
22	IN. DIAM.	TREE REMOVAL SPECIAL (6 IN. TO 15 IN. DIAM.)
00	IN. DIAM.	TREE REMOVAL SPECIAL (OVER 15 IN. DIAM.)
15	CU. YDS.	TOP SOIL
0.01	ACRES	COMPLETE REPAIRING
800	SQ. YDS.	SOULING
0.02	TONS	FERTILIZER MATERIALS
0	UNITS	SUPPLEMENTAL MATERIAL
7	EACH	FILLING EXISTING HOLES
0	EACH	FILLING EXISTING CATCH BASINS
2	EACH	PLUMBING EXISTING DRAINS & SEWERS (OVER 12 IN.)
1	EACH	CATCH BASIN TO BE ABUTTER
0	EACH	MANHOLES TO BE ABUTTER
1	EACH	MANHOLES TO BE ABUTTER
0	EACH	MANHOLES TO BE ABUTTER
1	EACH	MANHOLES TO BE ABUTTER
0	EACH	MANHOLES TO BE ABUTTER
11	EACH	MANHOLES TYPE A, 4" DIA. WITH TYPE 3 FRAME, CLOSED LID
11	EACH	MANHOLES TYPE A, 4" DIA. WITH TYPE 3 FRAME, CLOSED LID
10	EACH	CATCH BASINS TYPE A-2, 4" DIA. WITH TYPE 3 FRAME, OPEN LID
0	EACH	CATCH BASINS, TYPE A-2, 4" DIA. WITH TYPE 3 FRAME & GRATE
0	EACH	CATCH BASINS, TYPE A-2, 4" DIA. WITH TYPE 3 FRAME & GRATE
0	EACH	TABLETS TYPE A WITH TYPE 3 FRAME, OPEN LID
0	EACH	CAST IRON FRAME, TYPE 5, INCLUDING CLOSED LID
12	EACH	PLUMBING EXISTING DRAINS & SEWERS (12" OR LESS)
000	CU. YDS.	TRENCH BACKFILL
22	LIN. FT.	STORM SEWER, TYPE 2 (R.C.P. CLASS III) 10 IN.
500	LIN. FT.	STORM SEWER, TYPE 2 (R.C.P. CLASS III) 12 IN.
0	LIN. FT.	STORM SEWER, TYPE 2 (R.C.P. CLASS III) 20 IN.
0	LIN. FT.	STORM SEWER, TYPE 2 (R.C.P. CLASS III) 20 IN.
201	LIN. FT.	STORM SEWER, TYPE 2 (ASTM C200-577) 8 IN.
2	LIN. FT.	STORM SEWER, TYPE 2 (ASTM C200-577) 8 IN.
120	LIN. FT.	STORM SEWER, TYPE 2 (ASTM C200-577) 15 IN.
650	LIN. FT.	STORM SEWER, TYPE 2 (ASTM C200-577) 20 IN.
20	LIN. FT.	STORM SEWER, TYPE 2 (ASTM C200-577) 20 IN.
220	LIN. FT.	CAST IRON PIPE (8 & 6, CLASS 150) 8 IN.
150	LIN. FT.	2 INCH GALVANIZED STEEL CONDUIT, GALVANIZED
700	LIN. FT.	2 INCH GALVANIZED STEEL CONDUIT IN STRUCTURE
120	LIN. FT.	2 1/2 IN. GALVANIZED STEEL CONDUIT DIRECT BURIED
1800	LIN. FT.	2 1/2 IN. ASBESTOS CEMENT CONDUIT IN SIGNAL SLAB
520	LIN. FT.	8-21" IN. ASBESTOS CEMENT CONDUIT, ENCASED
220	LIN. FT.	8-21" IN. ASBESTOS CEMENT CONDUIT, ENCASED
20	LIN. FT.	1-6 IN. FIBRE CEMENT, ENCASED
0	EACH	CAST IRON JUNCTION BOX - 6" DIA. x 18"
0	EACH	FOUNDATION FOR BALLAST INSULATED BASE LIGHT STANDARD
0	EACH	FOUNDATION FOR ANCHOR BASE LIGHT STANDARD
0	EACH	FOUNDATION FOR CONTROLLER CABINET
1	EACH	MANHOLE
1	EACH	MANHOLE - STANDARD
0	EACH	MANHOLE - SPECIAL
0	EACH	8" DIA. JUNCTION BOX - 24" x 12" x 10"
40	CU. YDS.	TRENCHING AND BACKFILLING
1000	LUMP SUM	FALSE WORK
1	EACH	BUILDING REMOVAL, PARCEL SB 480
1	EACH	BUILDING REMOVAL, PARCEL SB 482
1	EACH	BUILDING REMOVAL, PARCEL SB 483

RETURN THIS PLAN
TO MAINTENANCE
ROOM 706

DRAWER

9

APPROVED
JUL 12 1960

Dept. of Public Works & Buildings
E.A. Rosenstone
Director

PLANS APPROVED
BY STATE DIVISION OF HIGHWAYS

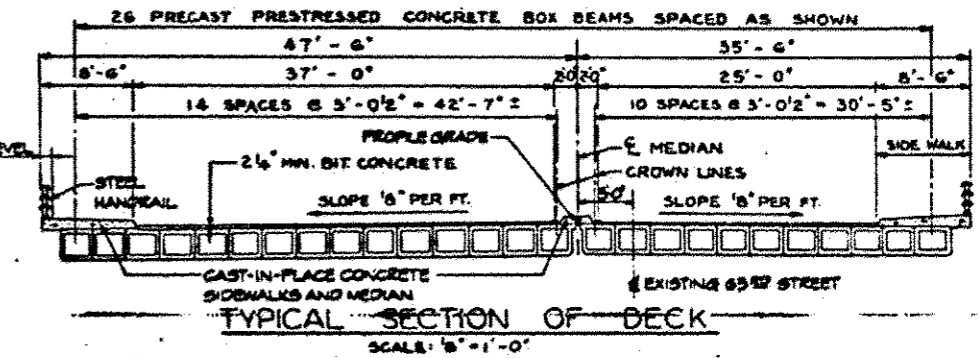
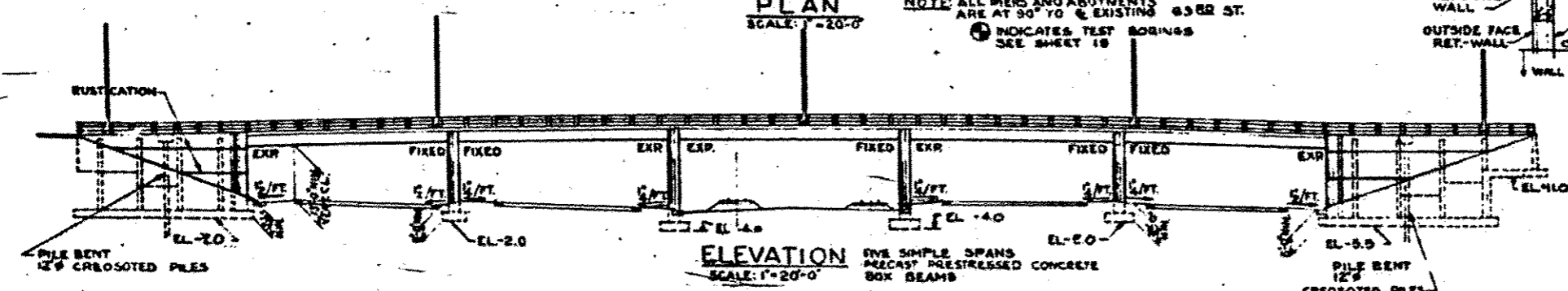
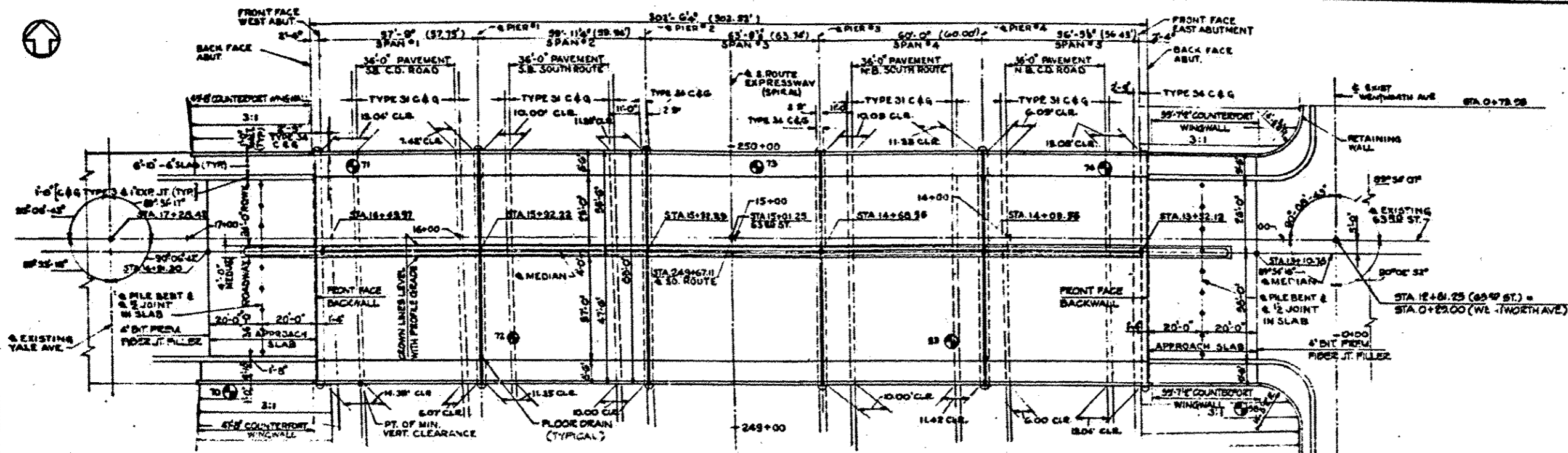
COUNTY OF COOK
DEPARTMENT OF HIGHWAYS
APPROVED: *March 16 1960*
W.C. Ryan
APPROVED: *March 17 1960*
George J. Smith
APPROVED: *March 17 1960*
William J. ...

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
APPROVED: _____
APPROVED: _____
DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
APPROVED: _____

CONTRACT NO. 60J15
SHEET NO. 110 OF 137

DE LEU, CATHER & COMPANY
CONSULTING ENGINEERS
CHICAGO ILLINOIS

Dist. # 10 Copy



GENERAL NOTES

DESIGN SPECIFICATIONS:
AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 7TH EDITION, DATED 1957, BUREAU OF PUBLIC ROADS PUBLICATION "CRITERIA FOR PRESTRESSED CONCRETE GIRDER BRIDGES", DATED 1959 AND A.S.H.C.E. JOINT COMMITTEE 323 REPORT "TENTATIVE RECOMMENDATIONS FOR PRESTRESSED CONCRETE".

DESIGN LOADING:
A.A.S.H.O., H20-44

FOUNDATIONS:
SEE TEST BORING AND LAB DATA SHEET FOR ALLOWABLE FOOTING PRESSURES.

CONSTRUCTION SPECIFICATIONS:
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, STATE OF ILLINOIS, DEPARTMENT OF PUBLIC WORKS AND BUILDINGS, DIVISION OF HIGHWAYS, DATED JANUARY 2, 1960.

CONCRETE:

REINFORCED CONCRETE	
1" x 4"	3,500 P.S.I. ULTIMATE STRENGTH IN 14 DAYS
1" x 6"	1,400 P.S.I. CONCRETE WITHOUT EARTH PRESSURE (SUPERSTRUCTURE)
1" x 8"	800 P.S.I. CONCRETE WITH EARTH PRESSURE
1" x 10"	75 P.S.I. MAX-SHEAR IN FOOTINGS (SUBSTRUCTURE)
1" x 12"	18
PRESTRESSED CONCRETE	
1" x 12"	5,000 P.S.I. ULTIMATE STRENGTH AT 14 DAYS
PRESTRESSING STEEL 3/8" DIA. 7-WIRE STRESS-RELIEVED STRANDS - MINIMUM TENSILE STRENGTH 250,000 P.S.I.	

ALL CAST IN PLACE CONCRETE SHALL BE CLASS 2.

GENERAL NOTES (CONT'D)

REINFORCEMENT:
MILLET STEEL CONCRETE REINFORCEMENT BARS, INTERMEDIATE GRADE A.S.T.M. DESIGNATION A 15, OR AXLE STEEL CONCRETE REINFORCEMENT BARS, A.S.T.M. DESIGNATION A 190 SHALL BE USED. ALL BARS SHALL BE DEFORMED TYPE IN ACCORDANCE WITH DESIGNATION A 305.

ALLOWABLE TENSILE UNIT STRESS - 1/6 28,000 P.S.I.

BAR BENDING, PLACING, CLEARANCES, ETC., UNLESS OTHERWISE SHOWN, SHALL BE IN ACCORDANCE WITH SECTION 50 OF THE STANDARD SPECIFICATIONS.

FOR INFORMATION ONLY

CONTRACT NO. 60J15
SHEET NO. 111 OF 137

REVISIONS		
DATE	BY	DESCRIPTION

DEPARTMENT OF HIGHWAYS
COOK COUNTY, ILLINOIS

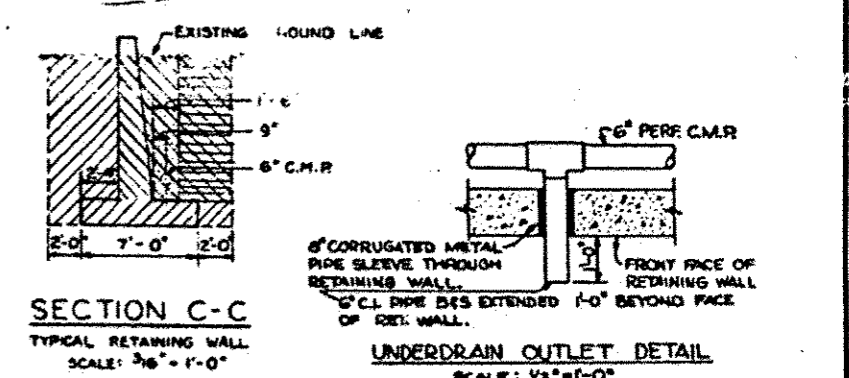
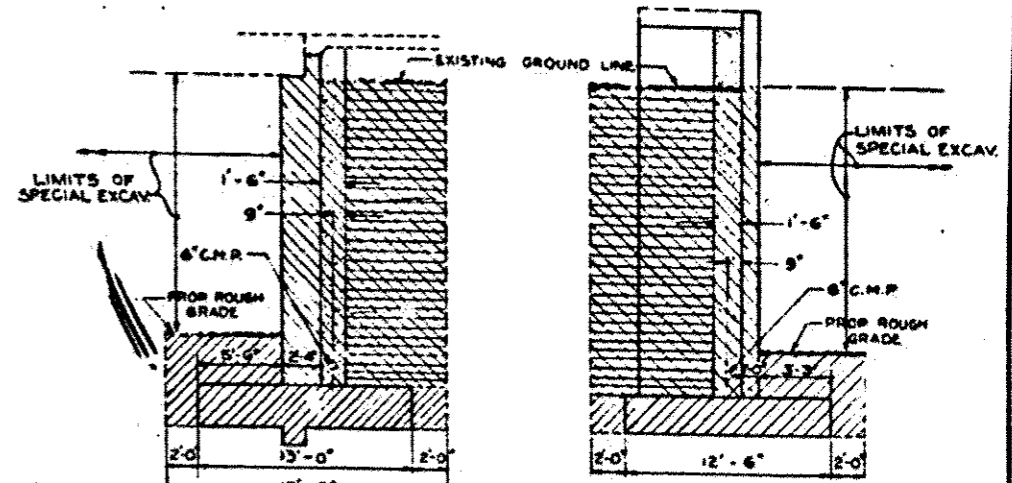
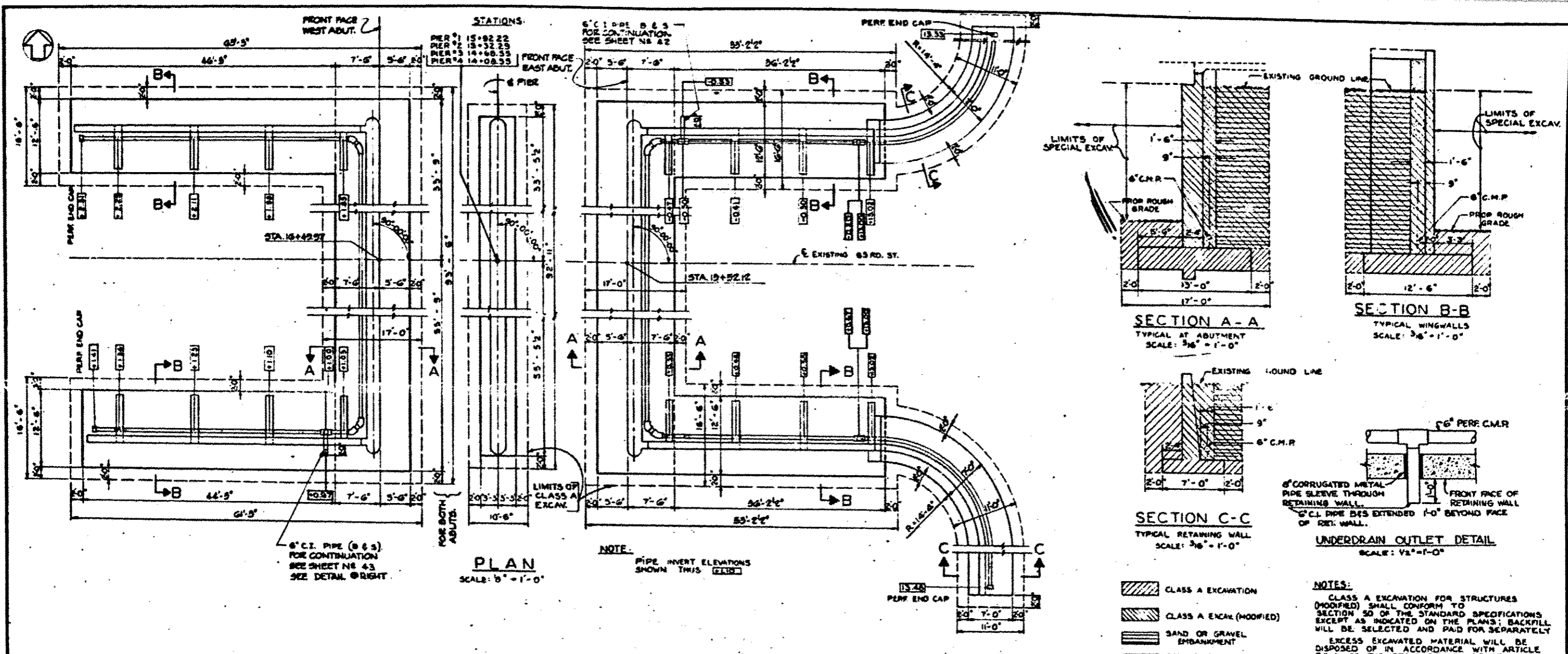
DESIGNED BY: [Signature]
CHECKED BY: [Signature]

GENERAL PLAN
63RD STREET GRADE SEPARATION
OVER
SOUTH ROUTE EXPRESSWAY

COMPUTED: F.M.C. PROJECT: 811-24-3(25)28
DRAWN: L.R. SCALE: AS SHOWN
CHECKED: F.M.C. APPROVED: [Signature]

APPROVED: [Signature]	P.A.I. No. 94	County Highway No. 066-1920-CF	Sheet No. 2	Total Sheets 48	Drawing No. S-1
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DR. LEW, CATHY & COMPANY
CONSULTING ENGINEERS



- CLASS A EXCAVATION
- CLASS A EXCAV. (MODIFIED)
- SAND OR GRAVEL EMBANKMENT
- POROUS GRANULAR EMBANKMENT

NOTES:

CLASS A EXCAVATION FOR STRUCTURES (MODIFIED) SHALL CONFORM TO SECTION 50 OF THE STANDARD SPECIFICATIONS EXCEPT AS INDICATED ON THE PLANS; BACKFILL WILL BE SELECTED AND PAID FOR SEPARATELY.

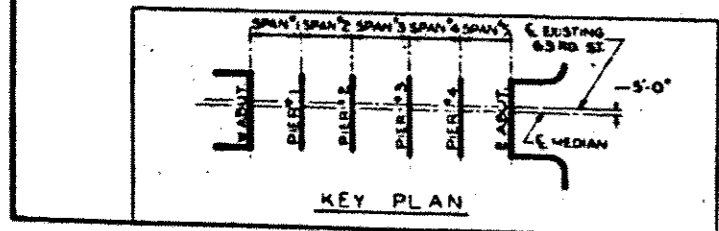
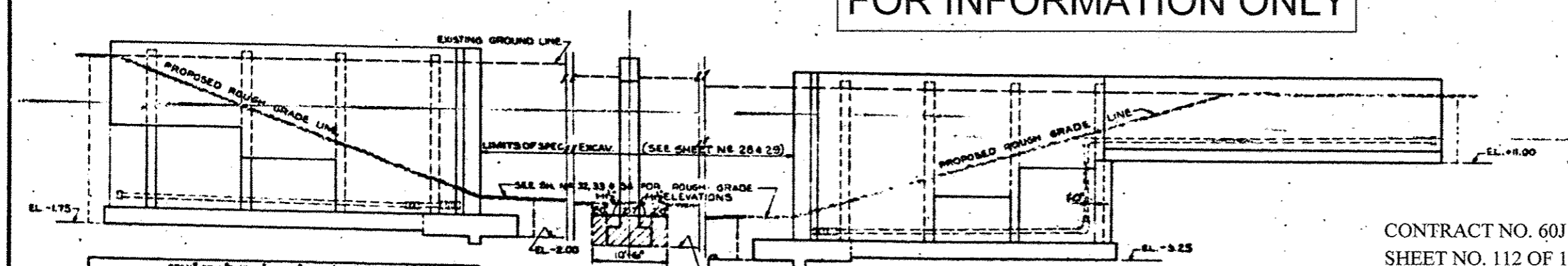
EXCESS EXCAVATED MATERIAL WILL BE DISPOSED OF IN ACCORDANCE WITH ARTICLE 50.8 OF THE STANDARD SPECIFICATION.

ALL HORIZONTAL PIPE TO BE 6" PERFORATED CORRUGATED METAL PIPE UNLESS NOTED.

VERTICAL PIPE (RISERS) TO BE 6" CORRUGATED METAL PIPE (NOT PERFORATED).

COST OF FURNISHING AND INSTALLING END CAPS, 6" CORRUGATED METAL PIPE SLEEVES, 6" CAST IRON (B & S) STD. PIPE AND ALL FITTINGS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER LIN. FT. FOR CORRUGATED METAL PIPE, 6" IN.

FOR INFORMATION ONLY



BILL OF MATERIAL

ITEM	UNIT	QUANT.
CLASS A EXCAVATION FOR STRUCTURES	CU. YDS.	2190
CLASS A EXCAVATION FOR STRUCTURES (MODIFIED)	CU. YDS.	2880
SAND OR GRAVEL EMBANKMENT	TONS	2850
POROUS GRANULAR EMBANKMENT	CU. YDS.	427
CORRUGATED METAL PIPE, 6" IN.	LIN. FT.	450

REVISIONS

DATE	BY	DESCRIPTION

CONTRACT NO. 60J15
 SHEET NO. 112 OF 137

DEPARTMENT OF HIGHWAYS
 COOK COUNTY, ILLINOIS

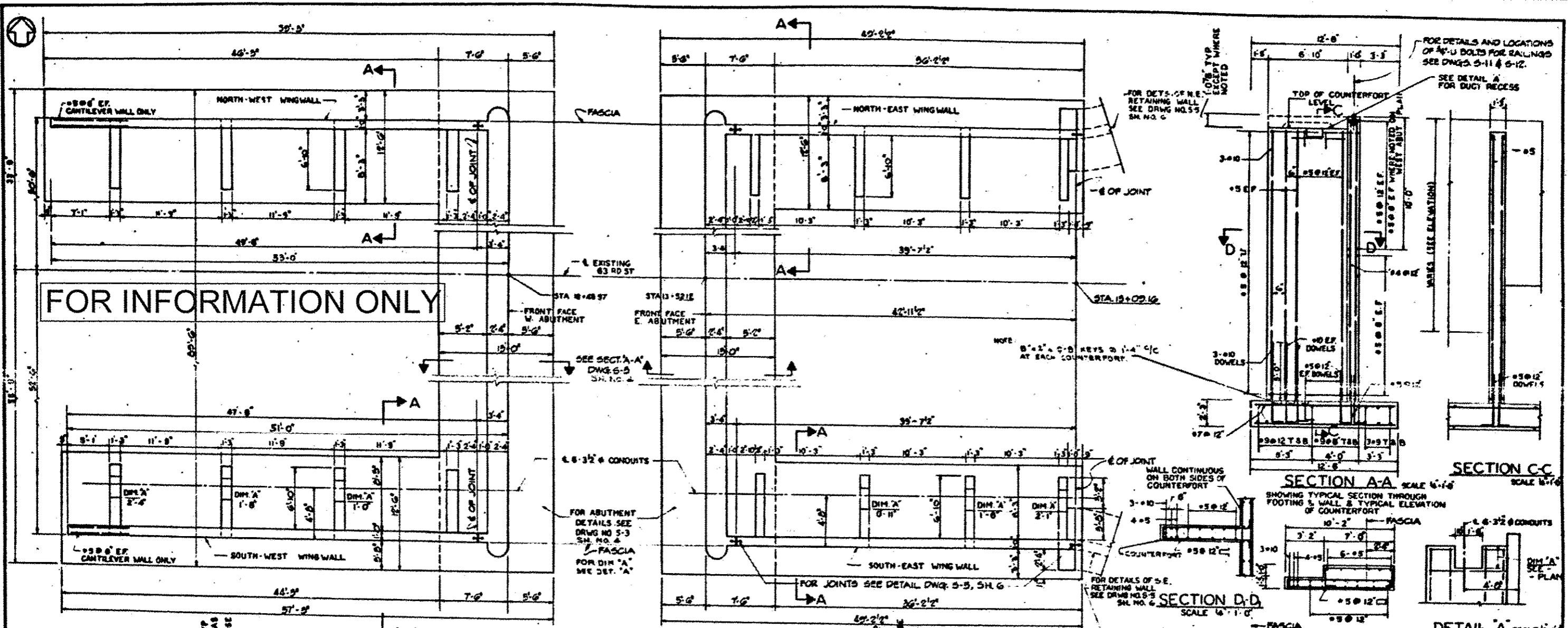
PROJECT: BUI-94-3(15)120
 SCALE: AS SHOWN

EXCAVATION, FOUNDATION LAYOUT & DRAINAGE
 63RD STREET GRADE SEPARATION
 OVER
 SOUTH ROUTE EXPRESSWAY

COMPUTED: E.M.C.
 DRAWN: M.V.
 CHECKED: E.M.C.

APPROVED: *[Signature]*
 COUNTY HIGHWAY: 94
 SHEET NO.: 066-420-CF
 TOTAL SHEETS: 3
 DRAWING NO.: 48 S-2

BE LEUV, CATHEN & COMPANY
 CONSULTING ENGINEERS



PLAN SCALE 3/8" = 1'-0"

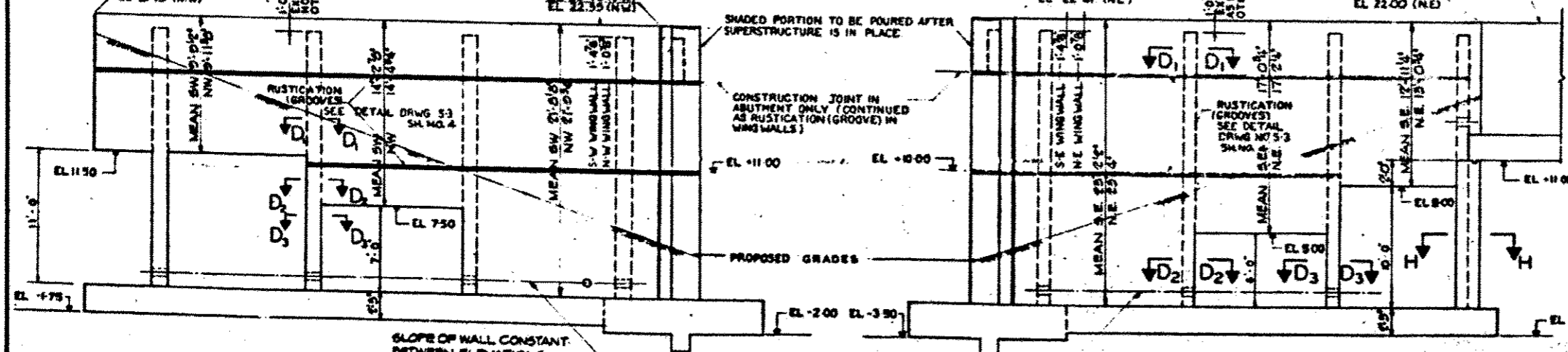
SECTION A-A SCALE 1/4" = 1'-0"

SECTION C-C SCALE 1/4" = 1'-0"

SECTION D-D SCALE 1/4" = 1'-0"

SECTION H-H SCALE 1/4" = 1'-0"

NOTES -
 FOR DRAINAGE DETAILS & PIPE SLEEVES SEE DWG. 5-2, SH. 3
 FOR RAILING DETAILS SEE DWGS. 5-11 & 5-12, SH. 12 & 13
 FOR ELECTRICAL DETAILS SEE SHEET NO. 47 & 48



ELEVATION S-W WING WALL SCALE 3/8" = 1'-0"
 N-W WING WALL (SIMILAR EXCEPT WHERE NOTED)

ELEVATION S-E WING WALL SCALE 3/8" = 1'-0"
 N-E WING WALL (SIMILAR EXCEPT WHERE NOTED)

DEPARTMENT OF HIGHWAYS
 COOK COUNTY, ILLINOIS

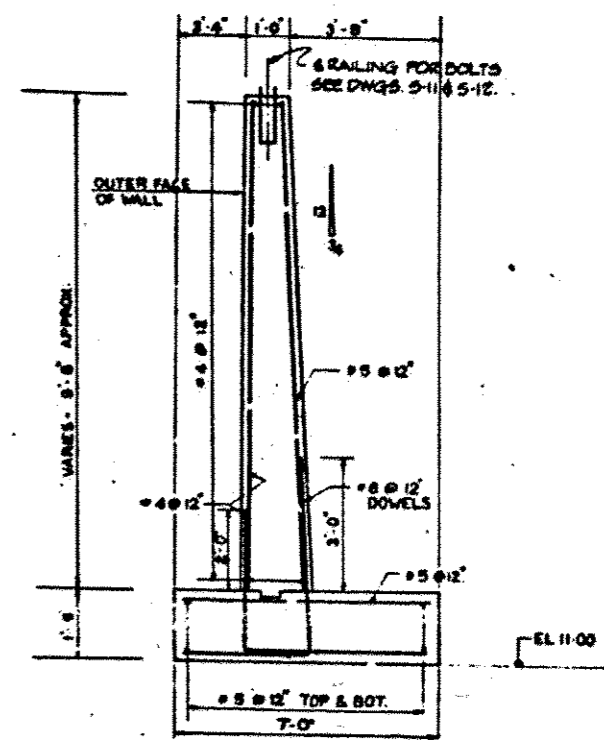
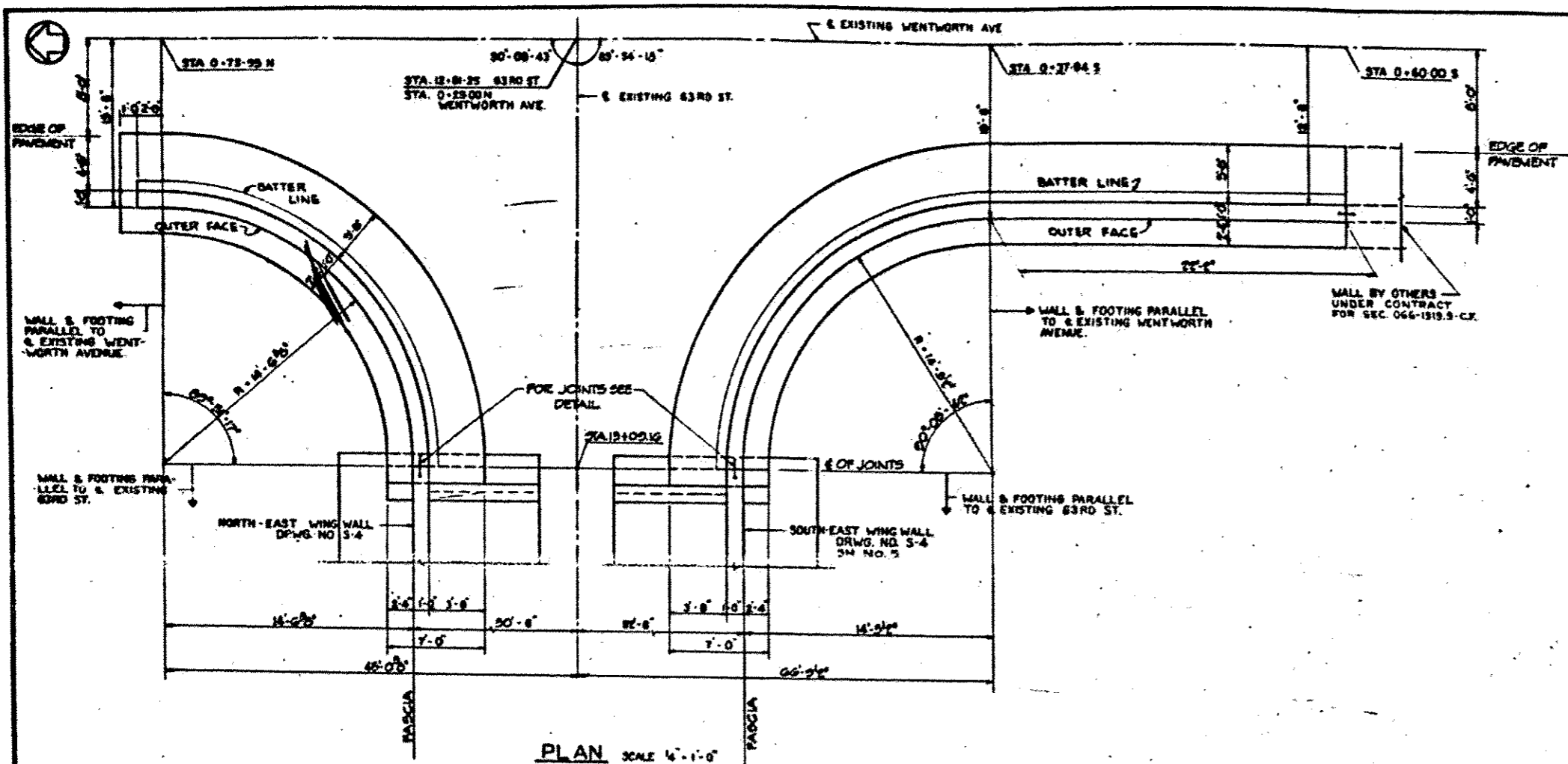
WINGWALLS
 63RD STREET GRADE SEPARATION
 OVER
 SOUTH ROUTE EXPRESSWAY

CONTRACT NO. 60J15
 SHEET NO. 114 OF 137

REVISIONS	
NO.	DESCRIPTION

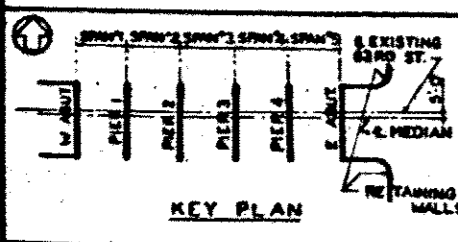
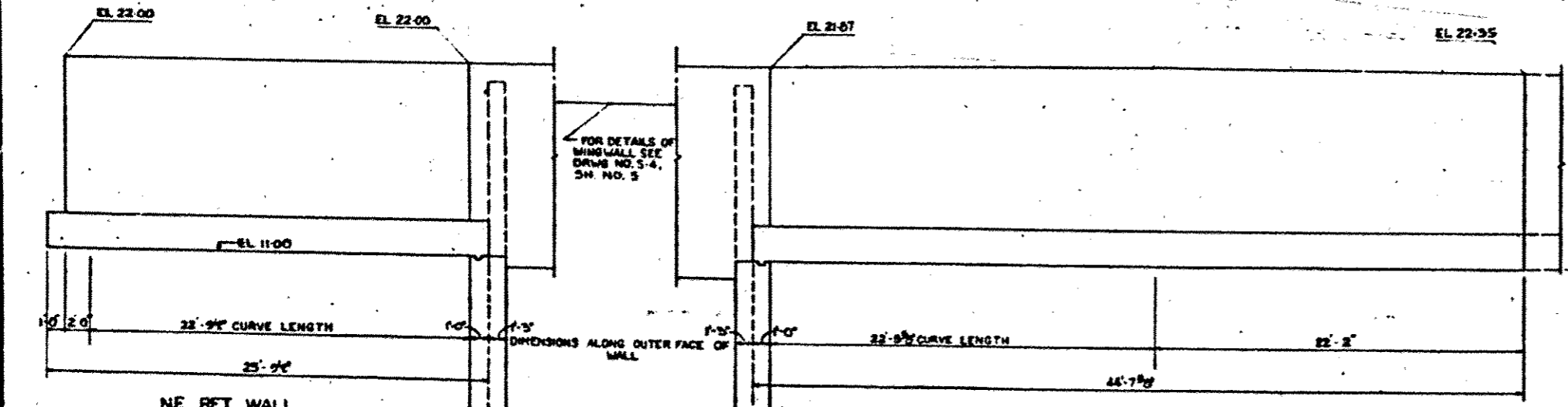
COMPUTED BY H. M. H.	PROJECT 94-114-31(2)150
DRAWN BY E. M. S.	SCALE AS SHOWN
CHECKED BY E. M. C.	APPROVED <i>[Signature]</i>
APPROVED BY <i>[Signature]</i>	F.A.I. No. 94
	County Highway 064-1920-CF
	Sheet No. 5
	Total Sheets 48
	Drawing No. S-4

BE LEW, CATHEN & COMPANY
 CONSULTING ENGINEERS

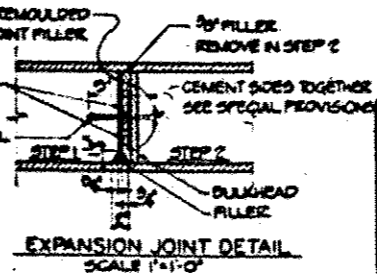


NOTES:
 FOR RAILING DETAILS SEE DRWG. NOS. S-11 & S-12, S-12 & S-13
 FOR DRAINAGE DETAILS SEE DRWG. NO. S-2, SM. NO. 3
 FOR DETAILS OF CHAIN LINK FENCE SEE SHEET NO. 40
 FOR ELECTRICAL DETAILS SEE SHEET NOS. 47 & 48

BILL OF MATERIALS - RETAINING WALLS		
ITEM	UNIT	QUANTITY
CLASS "X" CONCRETE	CU YD	50.4
REINFORCEMENT BARS	LBS	4500



FOR INFORMATION ONLY



CONTRACT NO. 60J15
 SHEET NO. 115 OF 137

REVISIONS		
DATE	BY	DESCRIPTION

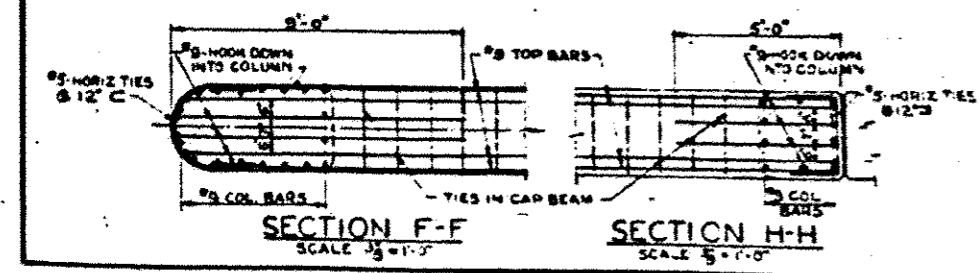
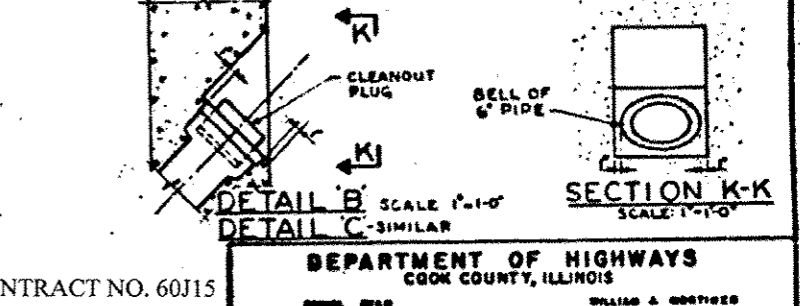
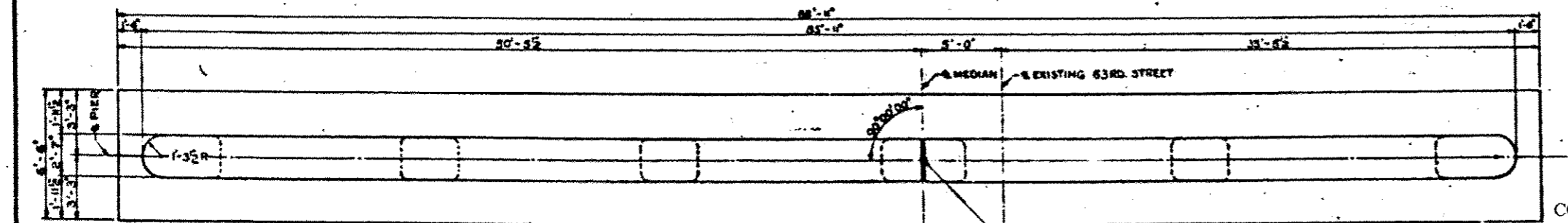
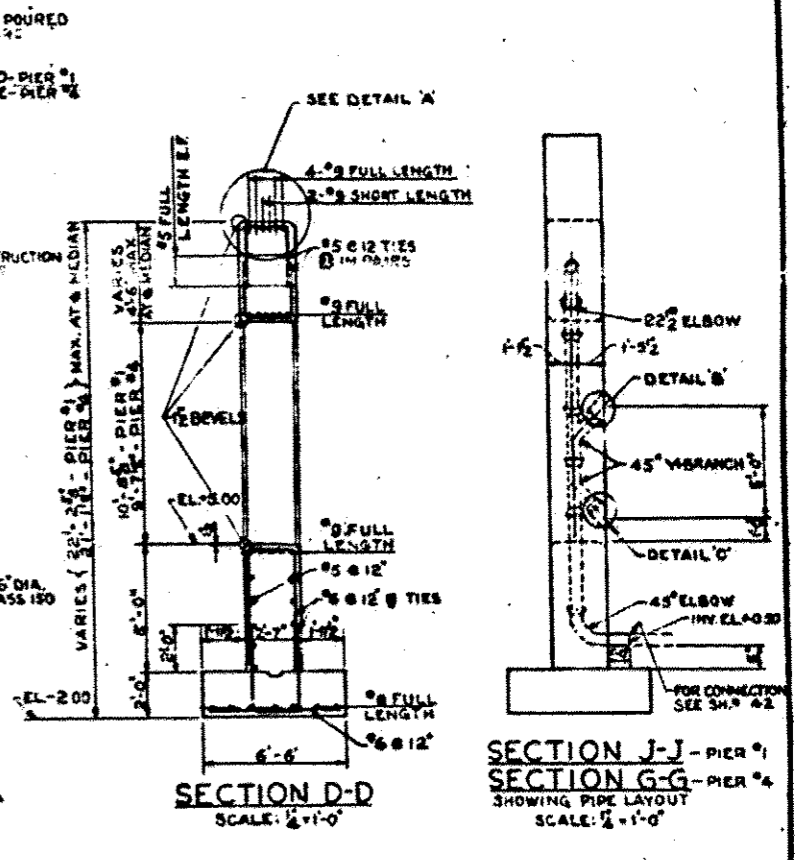
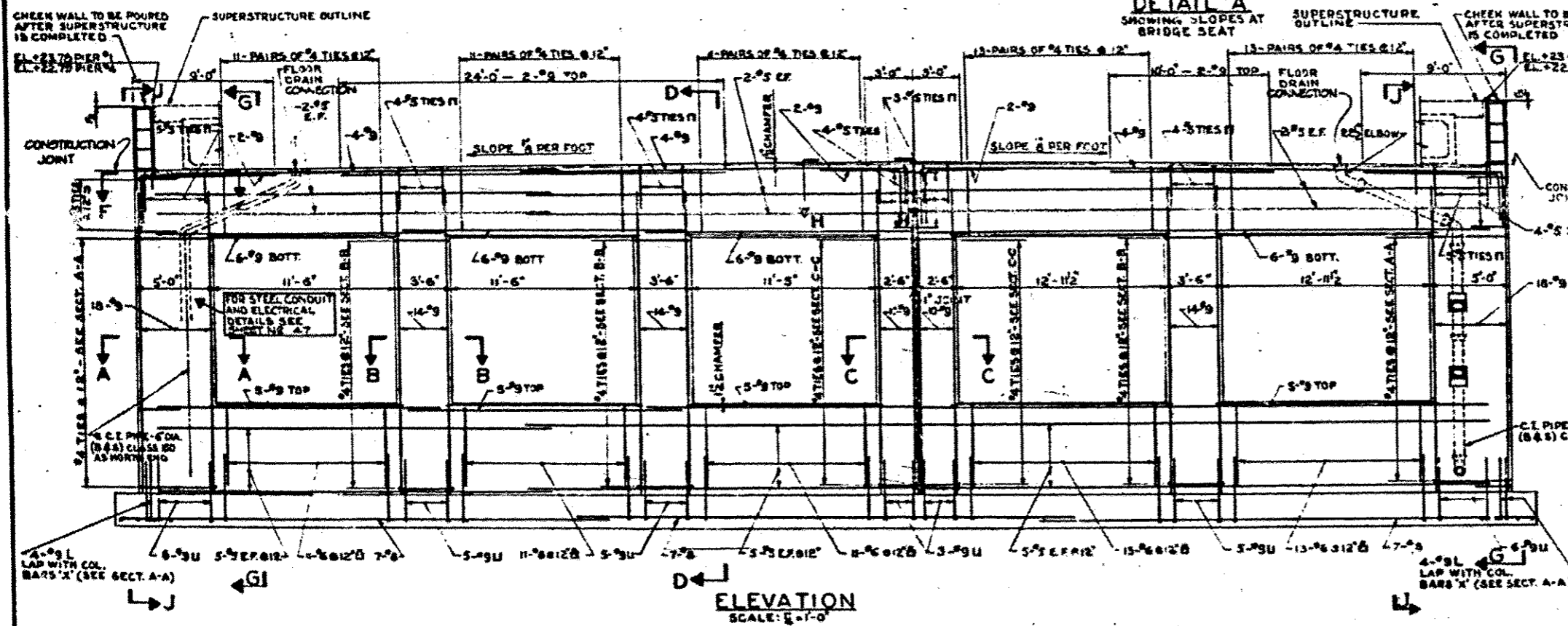
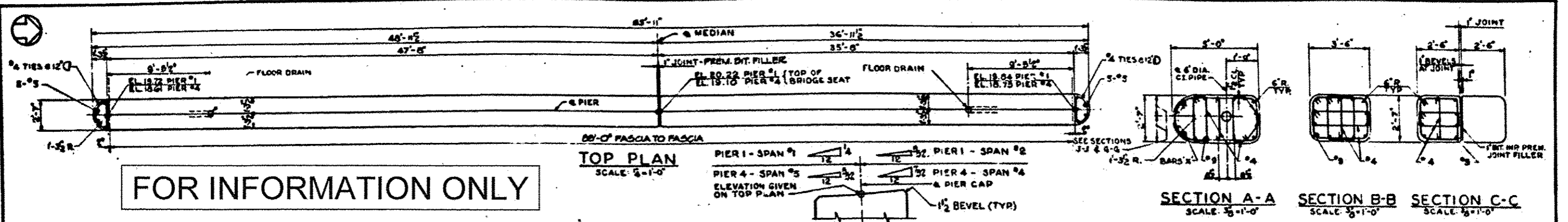
DEPARTMENT OF HIGHWAYS
 COOK COUNTY, ILLINOIS

RETAINING WALLS
 63RD STREET GRADE SEPARATION
 OVER
 SOUTH ROUTE EXPRESSWAY

COMPUTED: H.M.R. PROJECT: ILL-24-3155150
 DRAWN: R.M.S. SCALE: AS SHOWN
 CHECKED: E.N.C. APPROVED: [Signature]

APPROVED: [Signature] F.A.I. COUNTY: 94 SHEET NO.: 6 TOTAL SHEETS: 48 DRAWING NO.: S-5

DE LUYP, CATHY & COMPANY
 CONSULTING ENGINEERS



BILL OF MATERIAL

ITEM	UNIT	PIER #1	PIER #2
CLASS X CONCRETE	CUYDS	144.9	142.2
REINFORCEMENT BARS	LBS	22,200	21,600
CAST IRON PIPE 6" DIA. CLASS 150	LN. FT	45	44

KEY PLAN

CONTRACT NO. 60J15
SHEET NO. 116 OF 137

NOTES:
FOR ELECTRICAL DETAILS SEE SHEET NO. 47 & 48

REVISIONS

DATE	BY	DESCRIPTION

DEPARTMENT OF HIGHWAYS
COOK COUNTY, ILLINOIS

PIERS #1 & 4
63RD STREET GRADE SEPARATION
SOUTH ROUTE EXPRESSWAY

COMPUTED: S.G.
DRAWN: J.R.
CHECKED: F.M.C.

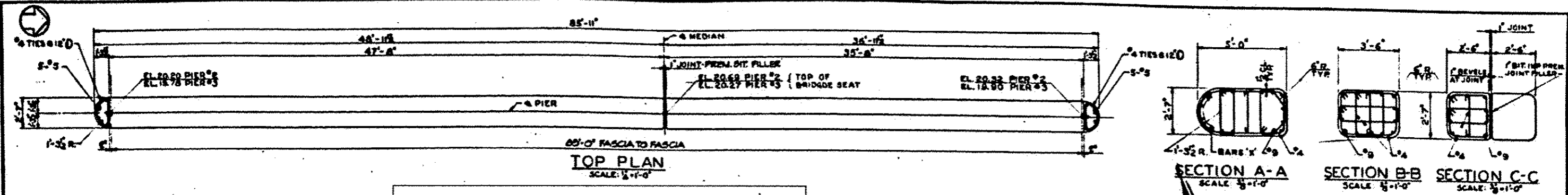
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APPROVED: [Signature] 1/15/69

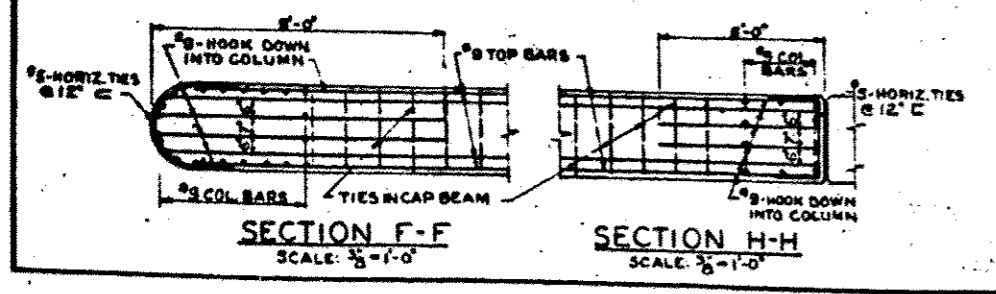
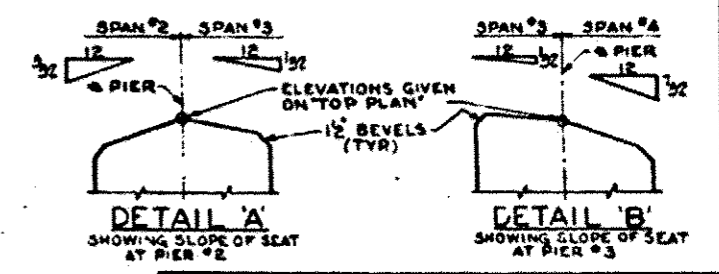
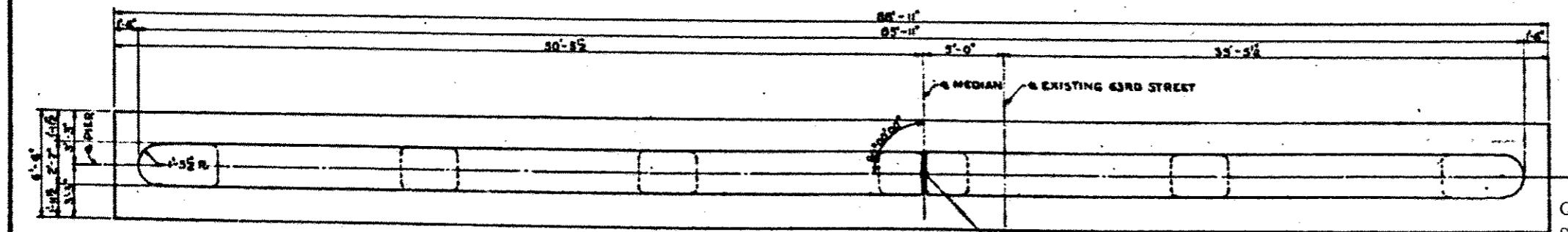
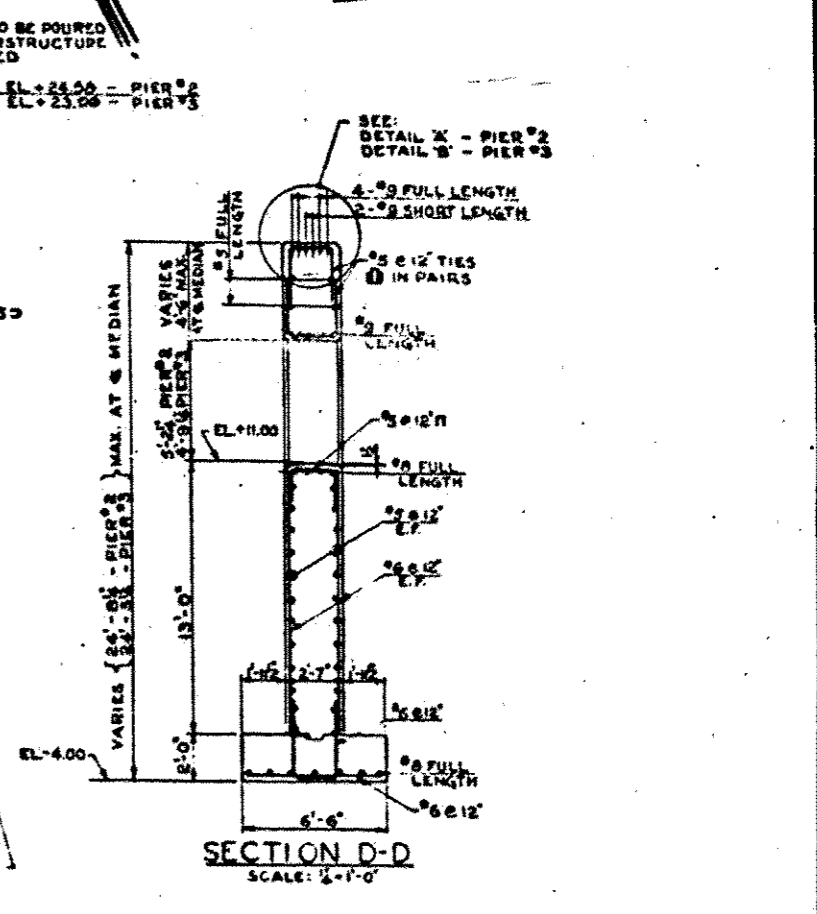
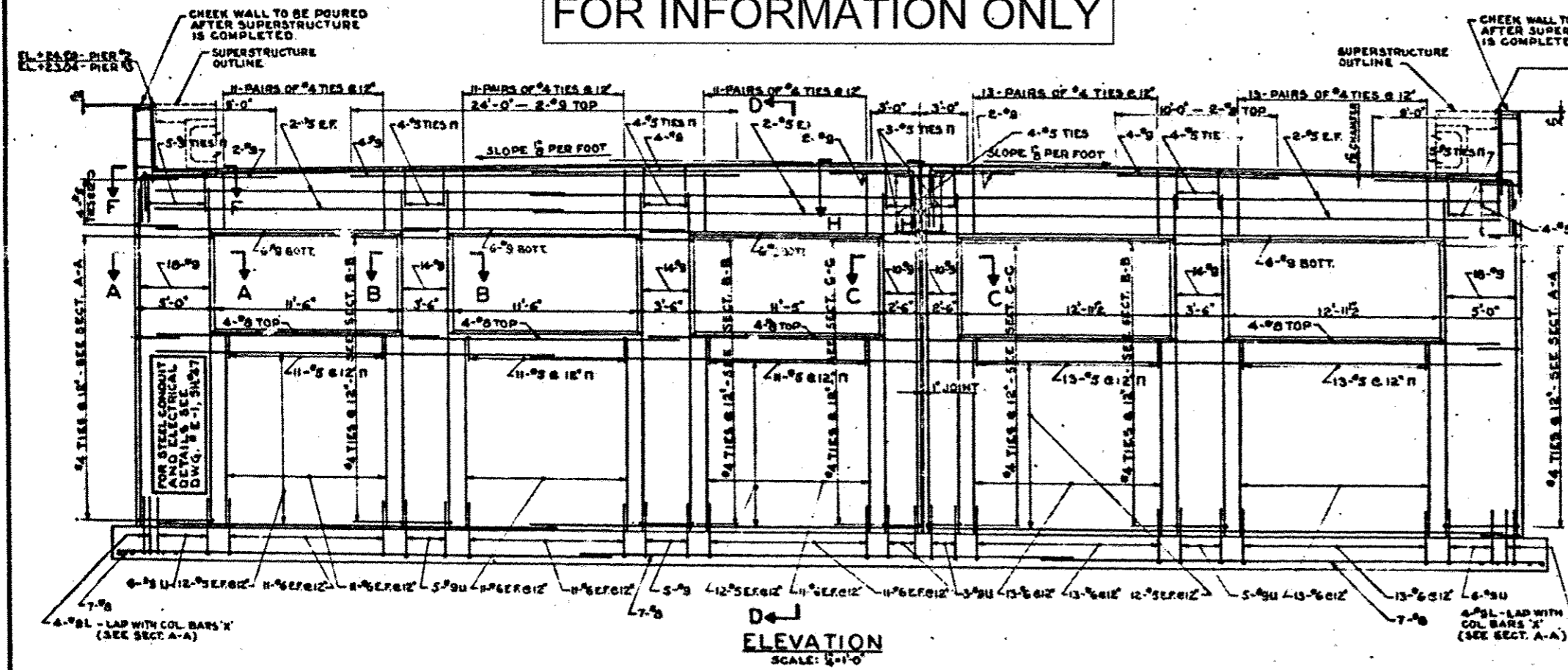
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94 066-120-CF 7 48 S-6

DE LEUW, CATHEN & COMPANY
CONSULTING ENGINEERS



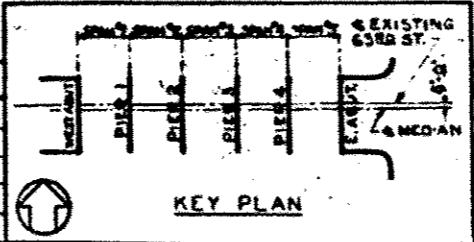
FOR INFORMATION ONLY



FOOTING PLAN SCALE: 1/8"=1'-0"

BILL OF MATERIAL

ITEM	UNIT	PIER #2	PIER #3
CLASS X CONCRETE	CU YDS	197.0	196.0
REINFORCEMENT BARS	LEBS	24,700	24,700



CONTRACT NO. 60J15
SHEET NO. 117 OF 137

NOTES:
FOR ELECTRICAL DETAILS SEE SHEET NO. 47 & 48

REVISIONS

DATE	BY	DESCRIPTION

DEPARTMENT OF HIGHWAYS
COOK COUNTY, ILLINOIS

PIERS NO 2 & 3
63RD STREET GRADE SEPARATION
OVER
SOUTH ROUTE EXPRESSWAY

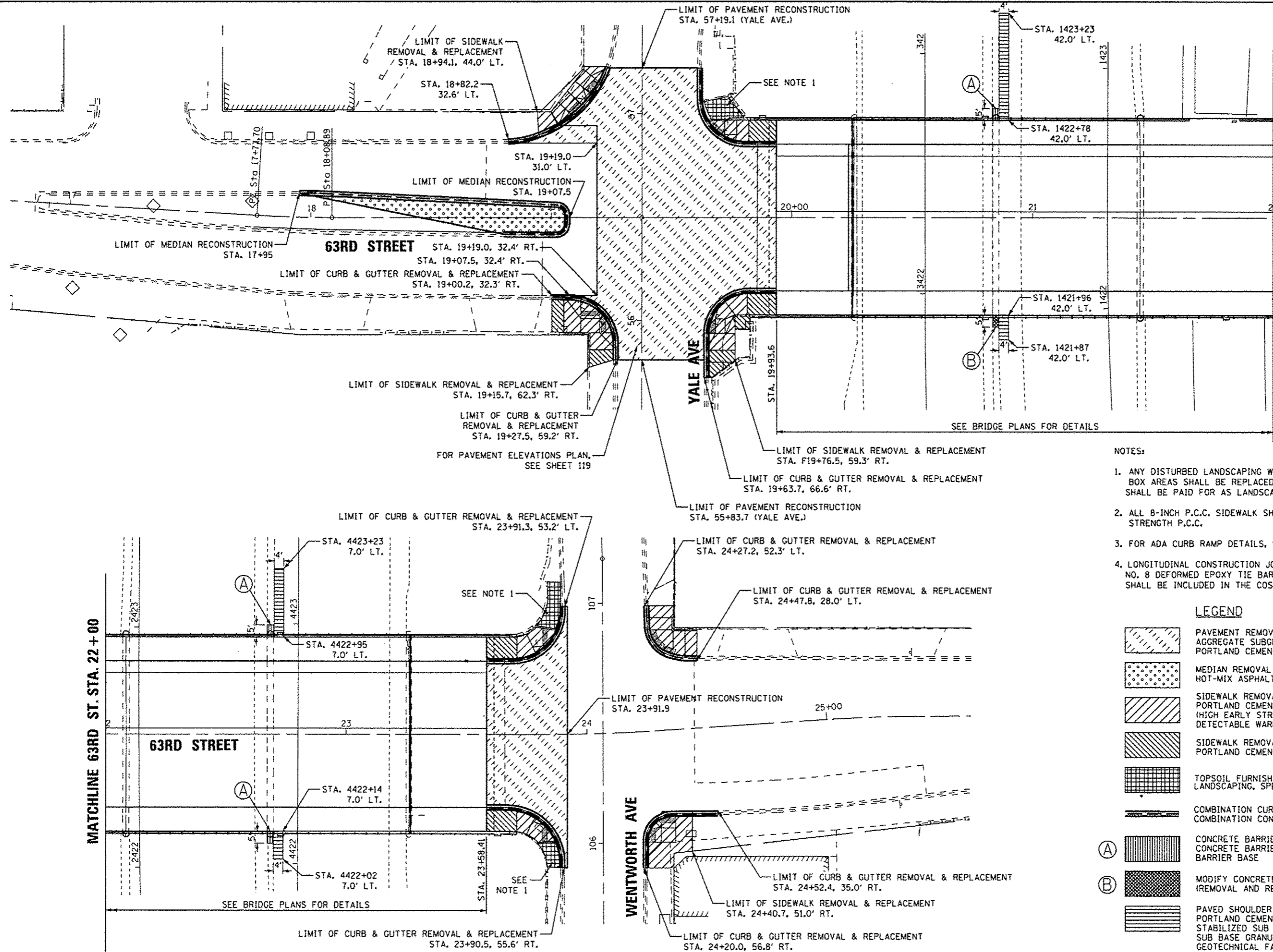
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DRAWN: L.R.
CHECKED: P.M.S.

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APPROVED: [Signature]

APPROVED: [Signature]

94 066-920-CF 8 48 S-7

DE LEUW, CATHY & COMPANY
CONSULTING ENGINEERS



MATCHLINE 63RD ST. STA. 22+00

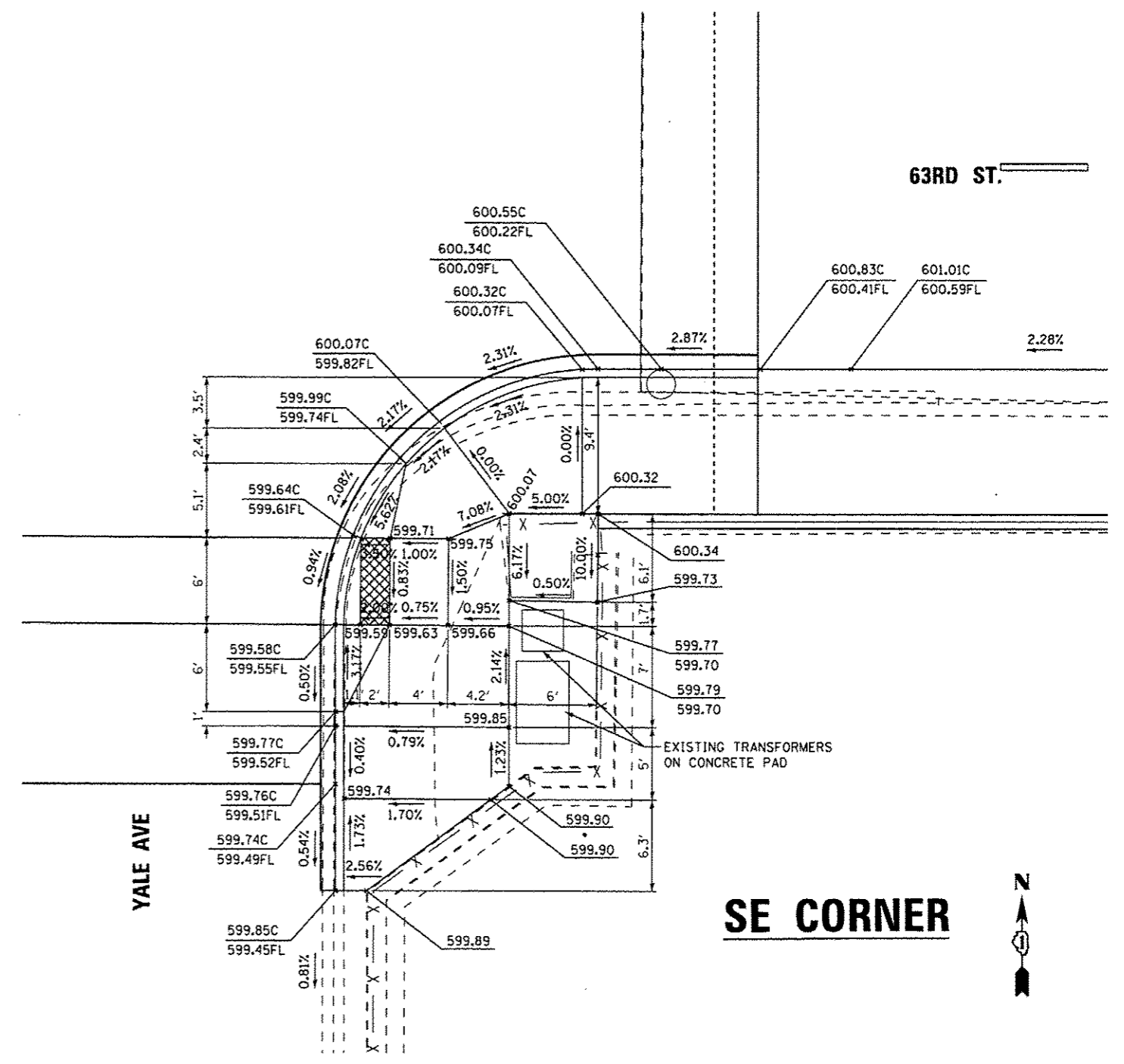
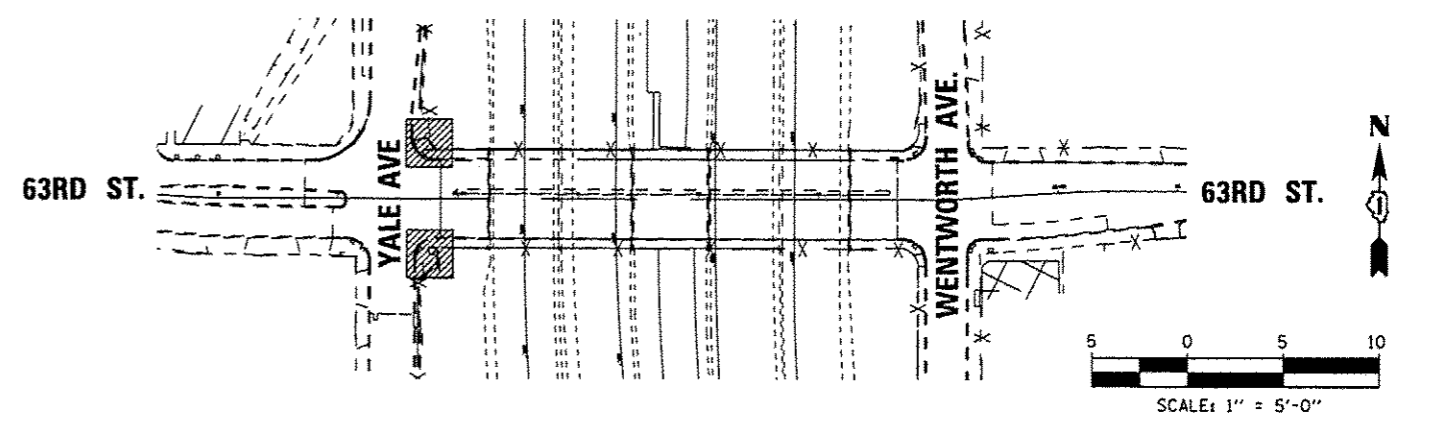
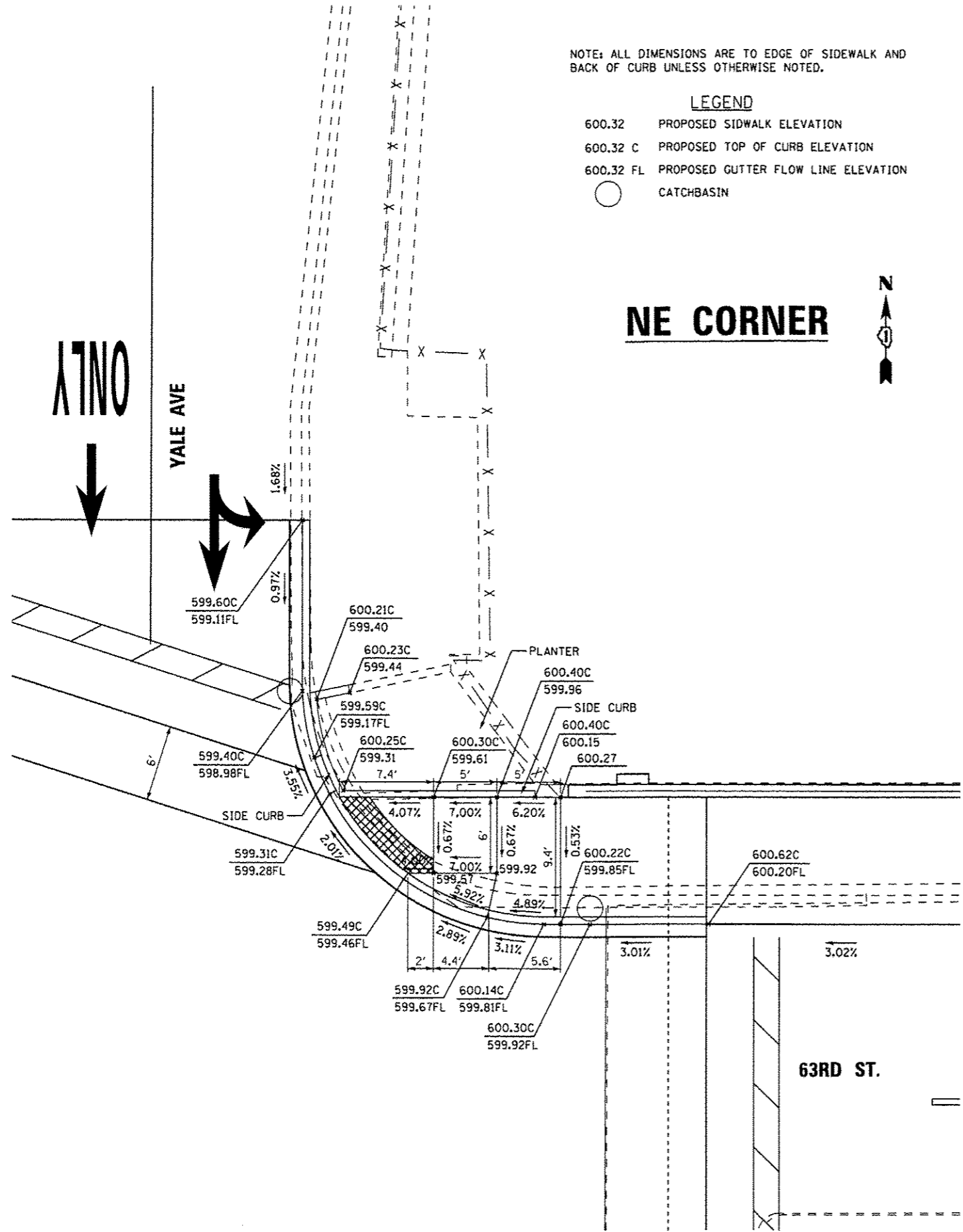
- NOTES:
1. ANY DISTURBED LANDSCAPING WITHIN THE PLANTER BOX AREAS SHALL BE REPLACED IN KIND. THIS WORK SHALL BE PAID FOR AS LANDSCAPING (SPECIAL).
 2. ALL 8-INCH P.C.C. SIDEWALK SHALL BE HIGH EARLY STRENGTH P.C.C.
 3. FOR ADA CURB RAMP DETAILS, SEE SHEETS 120-123.
 4. LONGITUDINAL CONSTRUCTION JOINT. DRILL AND GROUT NO. 8 DEFORMED EPOXY TIE BARS 30" LONG AT 24" C-C SHALL BE INCLUDED IN THE COST OF THE PCC SHOULDER.

LEGEND

- PAVEMENT REMOVAL
AGGREGATE SUBGRADE IMPROVEMENT, 12"
PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- MEDIAN REMOVAL
HOT-MIX ASPHALT MEDIAN SURFACE, 4 INCH
- SIDEWALK REMOVAL
PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH
(HIGH EARLY STRENGTH)
DETECTABLE WARNINGS
- SIDEWALK REMOVAL
PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- TOPSOIL FURNISH AND PLACE, 4"
LANDSCAPING, SPECIAL
- COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12
- CONCRETE BARRIER REMOVAL
CONCRETE BARRIER TRANSITION
BARRIER BASE
- MODIFY CONCRETE BARRIER AND RETAINING WALL
(REMOVAL AND REPLACEMENT)
- PAVED SHOULDER REMOVAL (DAN RYAN) SEE NOTE 4
PORTLAND CEMENT CONCRETE SHOULDERS, 14"
STABILIZED SUB BASE 6"
SUB BASE GRANULAR MATERIAL, TYPE B-24"
GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

MATCHLINE 63RD ST. STA. 22+00

FILE NAME *	USER NAME * #USER#	DESIGNED - KMM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RESTORATION PLAN			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - LV	REVISED -		63RD ST & YALE AVE, 63RD ST & WENTWORTH AVE			94	1920-B	COOK	137	118
PLOT SCALE * #SCALE#		CHECKED - JDH	REVISED -		SCALE: 1"=20'	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60J15		
PLOT DATE * #DATE#		DATE - 02-22-2012	REVISED -		ILLINOIS FED. AID PROJECT							



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PLOT SCALE = #SCALE#	CHECKED - JDH	REVISOR - LV	REVISOR -
PLOT DATE = 5/15/2013	DATE - 02-22-2012	REVISOR - JDH	REVISOR -


**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CURB RAMP DETAILS 63RD ST. AT YALE AVE			
SCALE: 1"=5'	SHEET NO.	OF SHEETS	STA. TO STA.

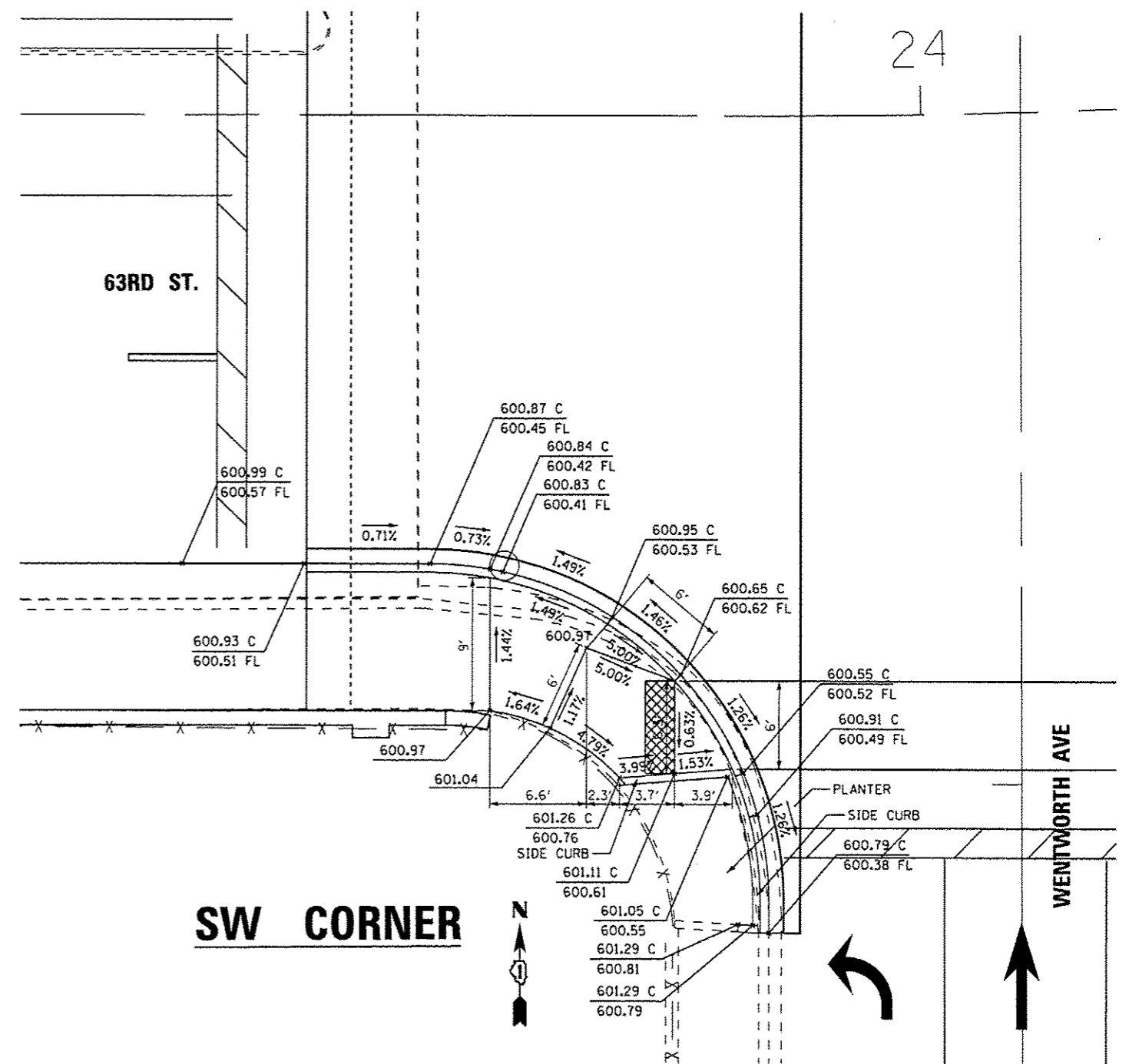
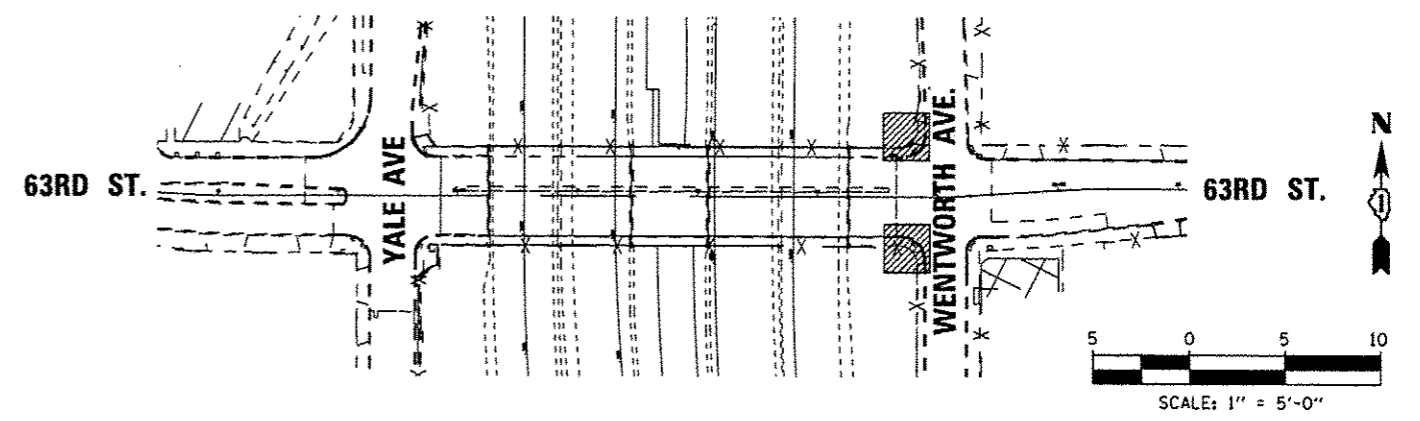
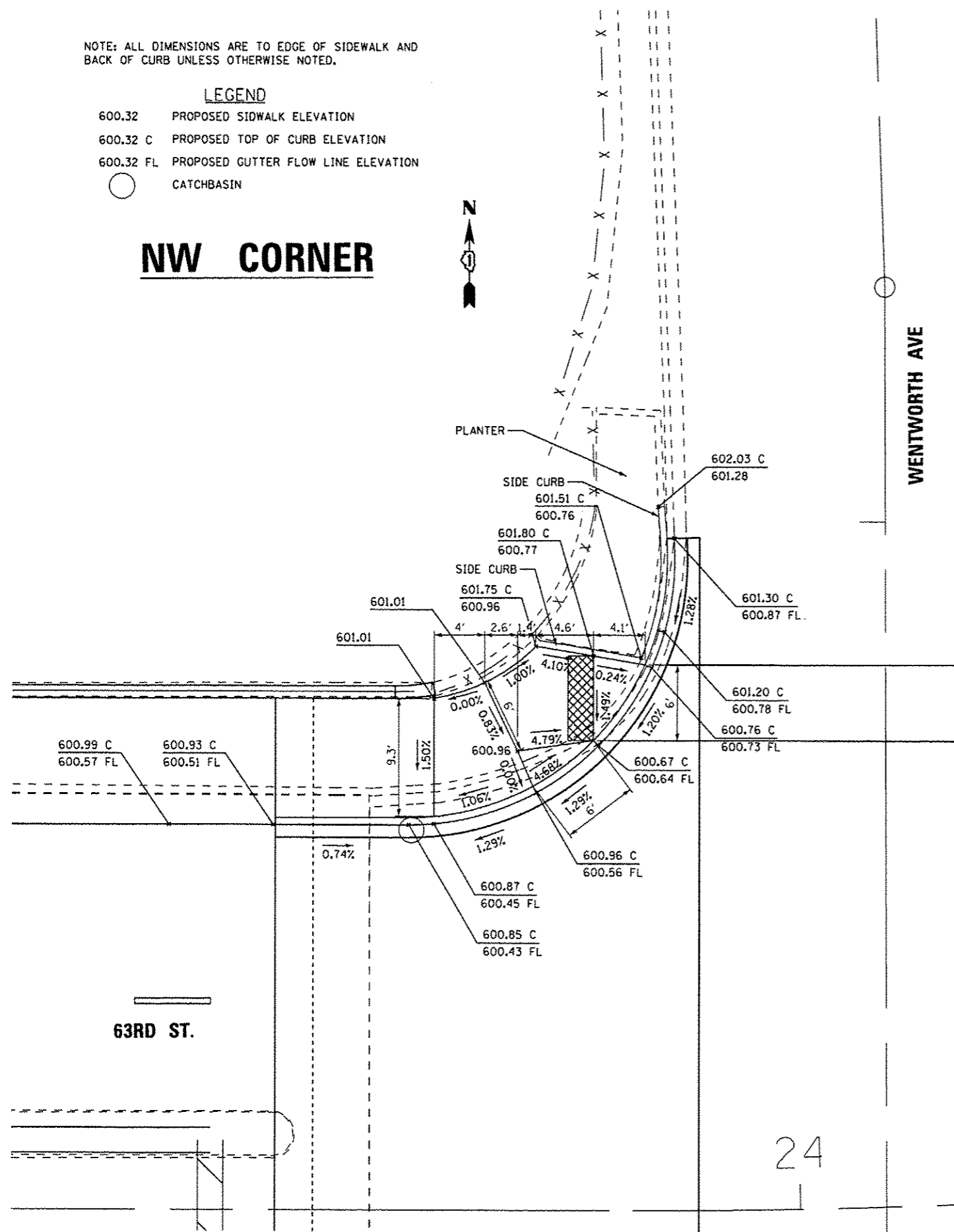
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	121
CONTRACT NO. 60J15				
ILLINOIS FED. AID PROJECT				

NOTE: ALL DIMENSIONS ARE TO EDGE OF SIDEWALK AND BACK OF CURB UNLESS OTHERWISE NOTED.

LEGEND

- 600.32 PROPOSED SIDEWALK ELEVATION
- 600.32 C PROPOSED TOP OF CURB ELEVATION
- 600.32 FL PROPOSED GUTTER FLOW LINE ELEVATION
-  CATCHBASIN

NW CORNER



FILE NAME *	USER NAME * #USER#	DESIGNED - KMM	REVISED -
K:\Projects\11847\Design\CAD\Sheet Files\3dswent.dgn		DRAWN - LV	REVISED -
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PLOT DATE = 5/15/2013		DATE - 02-22-2012	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS
 63RD ST. AT WENTWORTH AVE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	122
CONTRACT NO. 60J15			ILLINOIS FED. AID PROJECT	

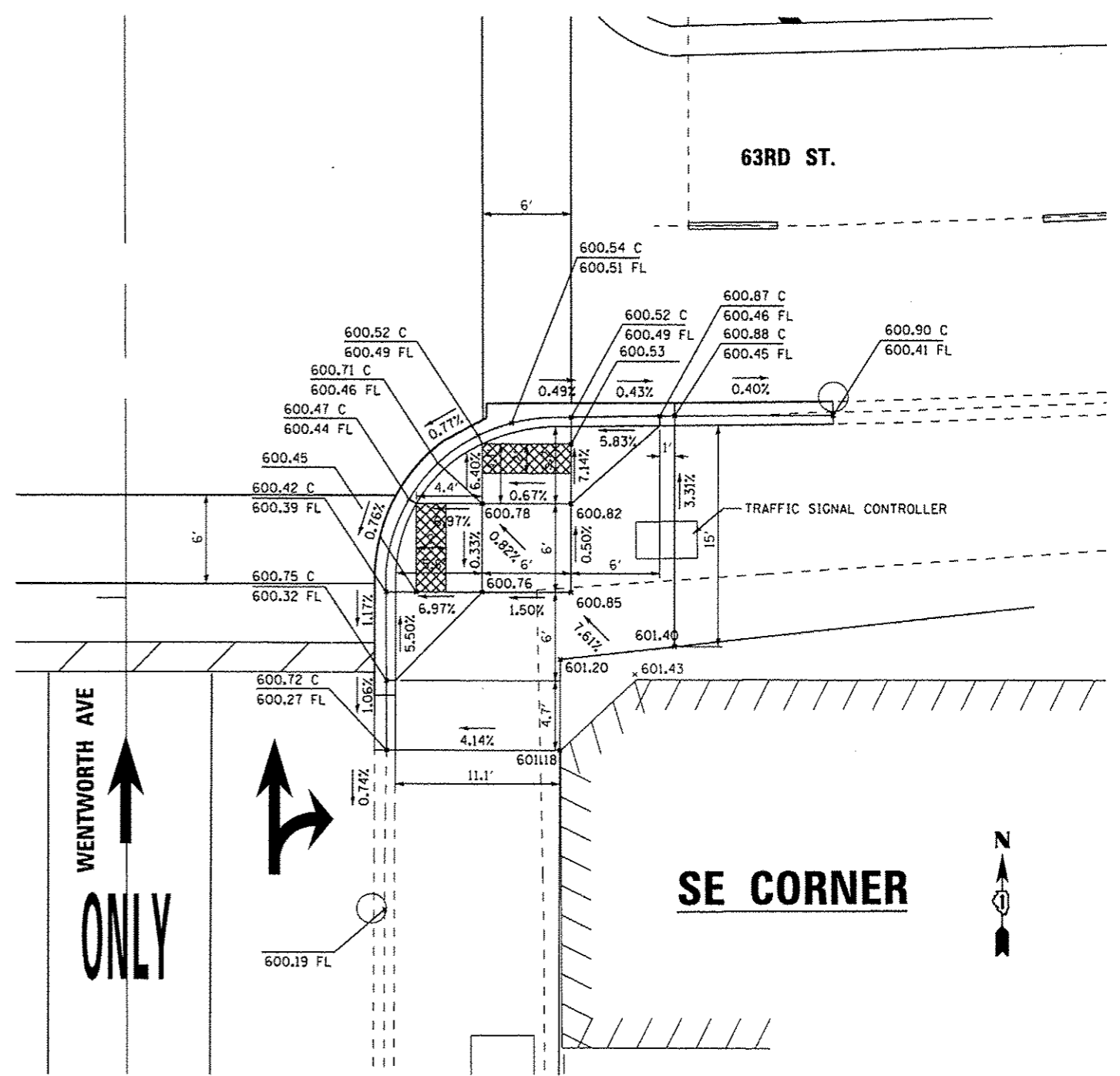
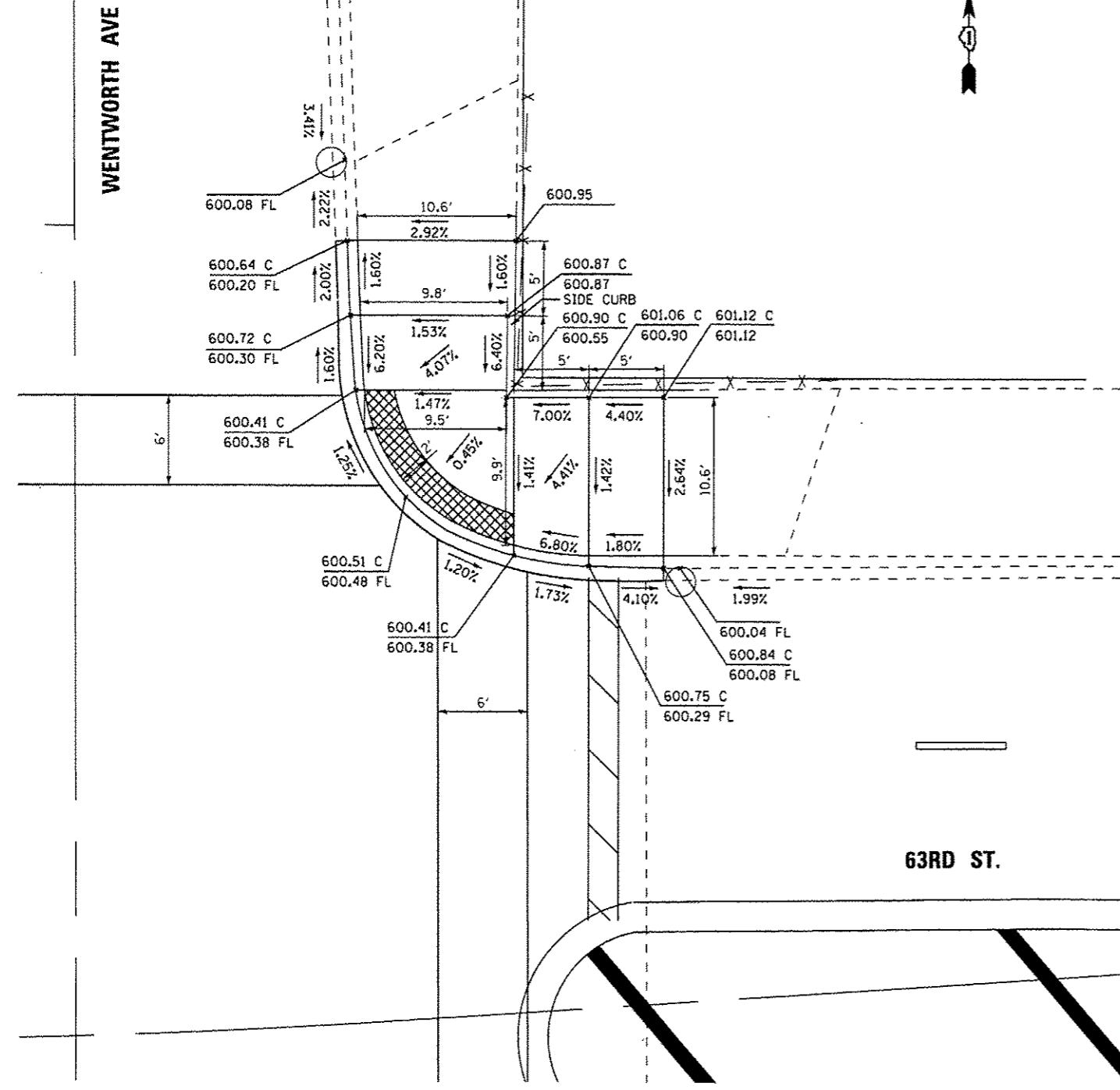
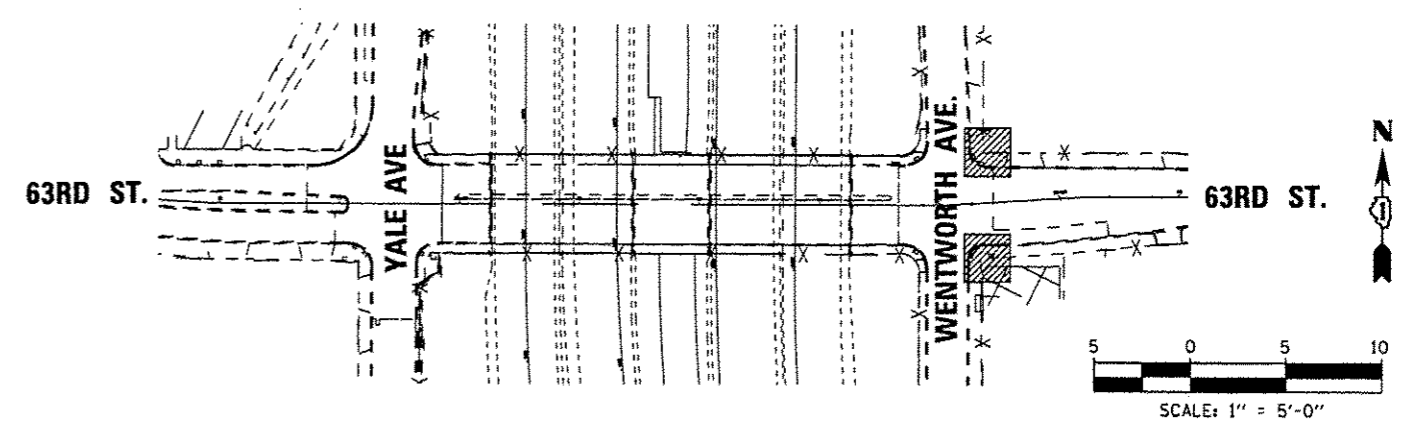
SCALE: 1"=5'
 SHEET NO. OF SHEETS STA. TO STA.

NOTE: ALL DIMENSIONS ARE TO EDGE OF SIDEWALK AND BACK OF CURB UNLESS OTHERWISE NOTED.

LEGEND

- 600.32 PROPOSED SIDEWALK ELEVATION
- 600.32 C PROPOSED TOP OF CURB ELEVATION
- 600.32 FL PROPOSED GUTTER FLOW LINE ELEVATION
- CATCHBASIN

NE CORNER

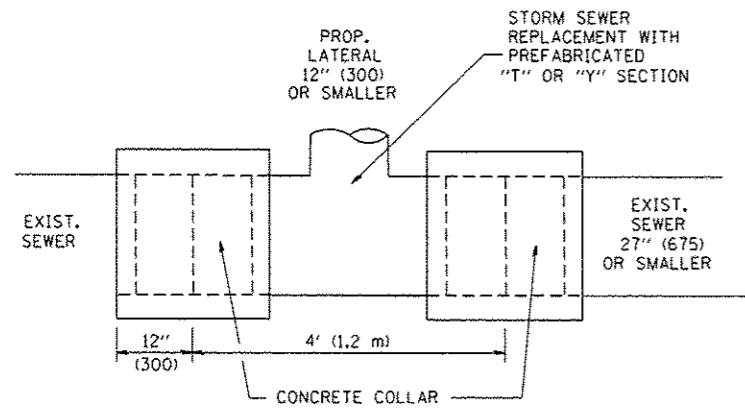


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PLOT DATE = 5/15/2013		DATE - 02-22-2012	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

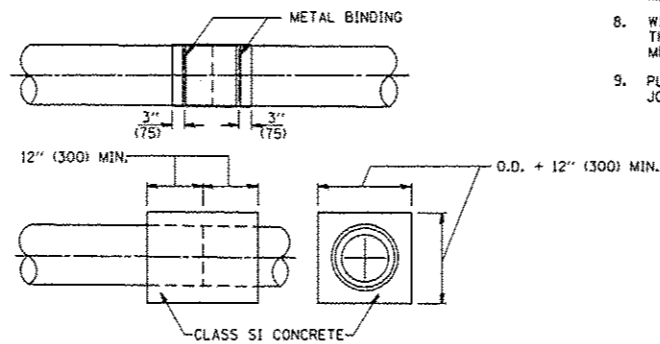
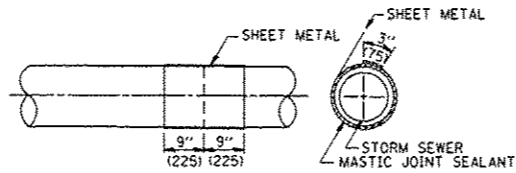
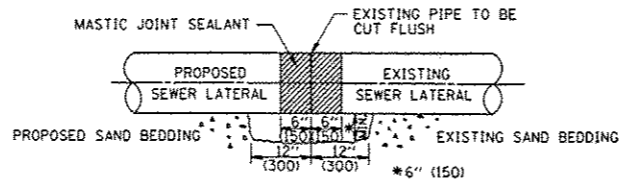
CURB RAMP DETAILS		F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 123
SCALE: 1"=5'	SHEET NO.	OF	SHEETS	STA.	TO	STA.

CONTRACT NO. 60J15
ILLINOIS FED. AID PROJECT



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

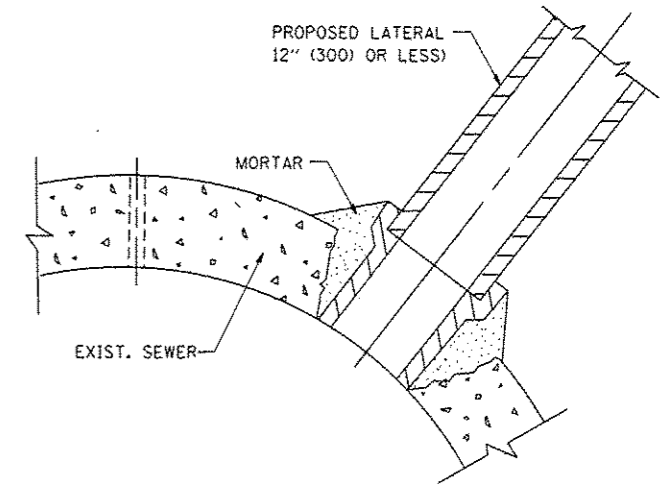


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

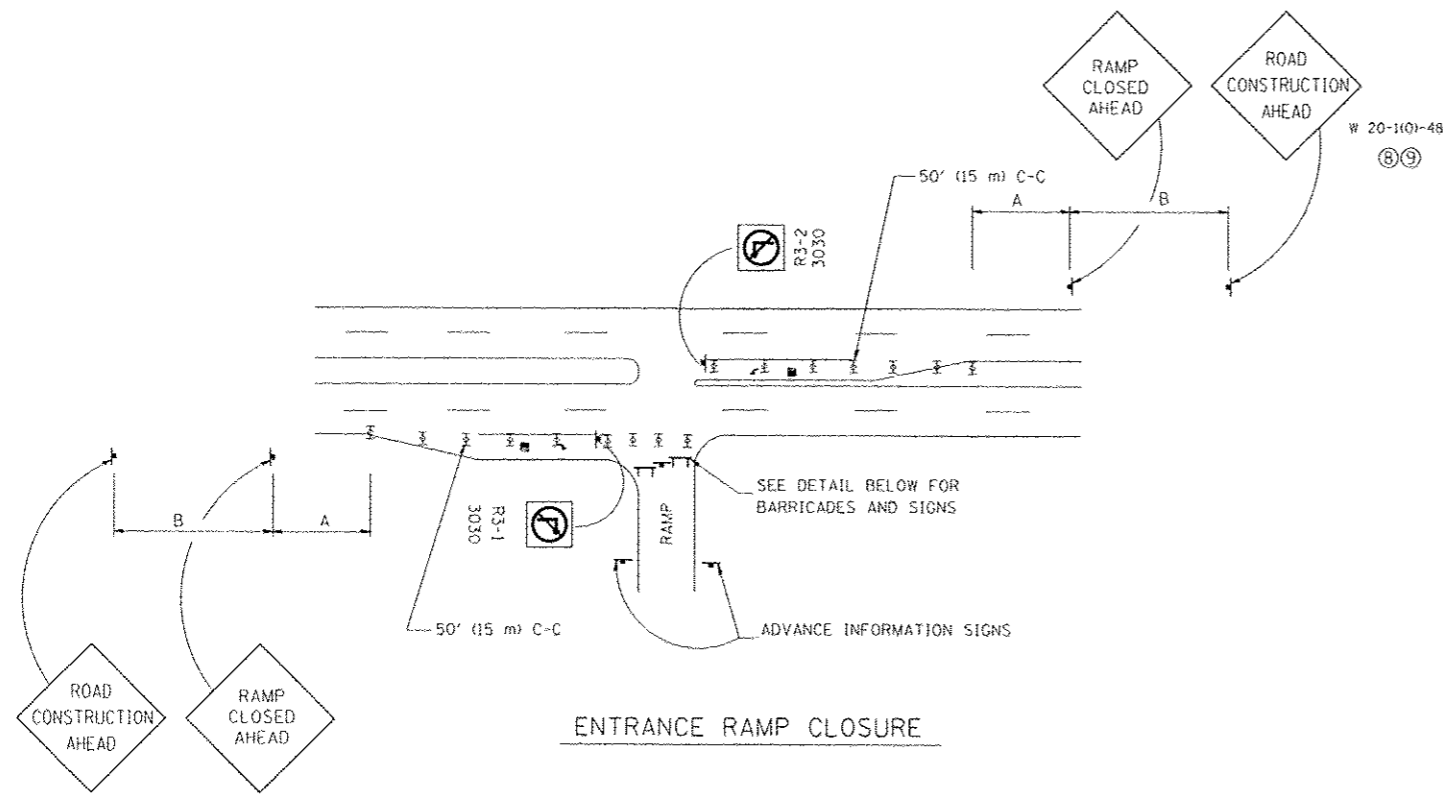
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME : W:\dist\cd\22x34\bd07.dgn	USER NAME : gagtrn0bt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. SHAH 09-09-94		94	1920-B	COOK	137	124			
		PLOT SCALE : 5/8" = 1' / 3/16"	REVISED - R. SHAH 10-25-94		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			BD500-01 (BD-7) CONTRACT NO. 60J15				
		PLOT DATE : 1/4/2008	REVISED - R. SHAH 06-12-96		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



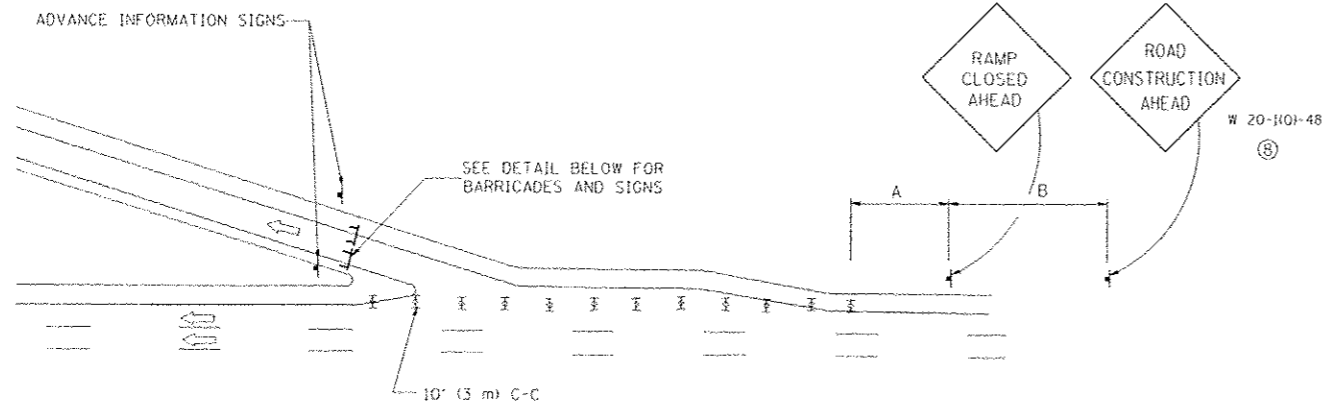
ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

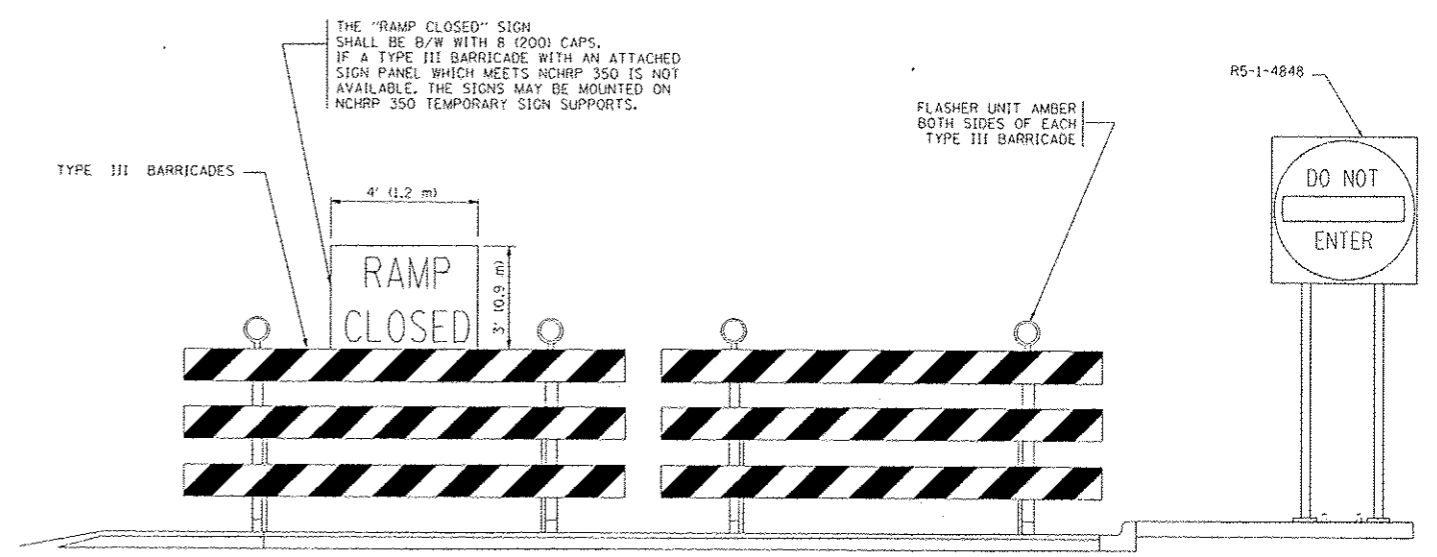
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

W 20-1101-48 (8) (9)

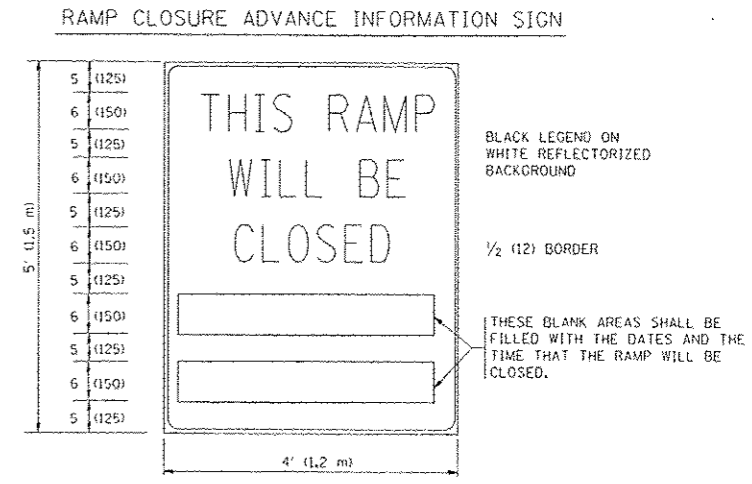
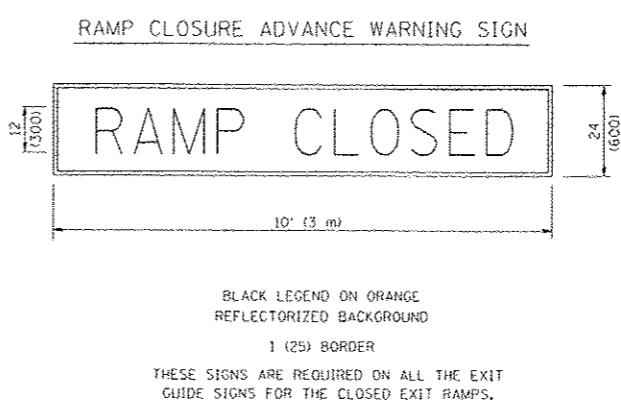


EXIT RAMP CLOSURE

- SYMBOLS**
- ⊞ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊞ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



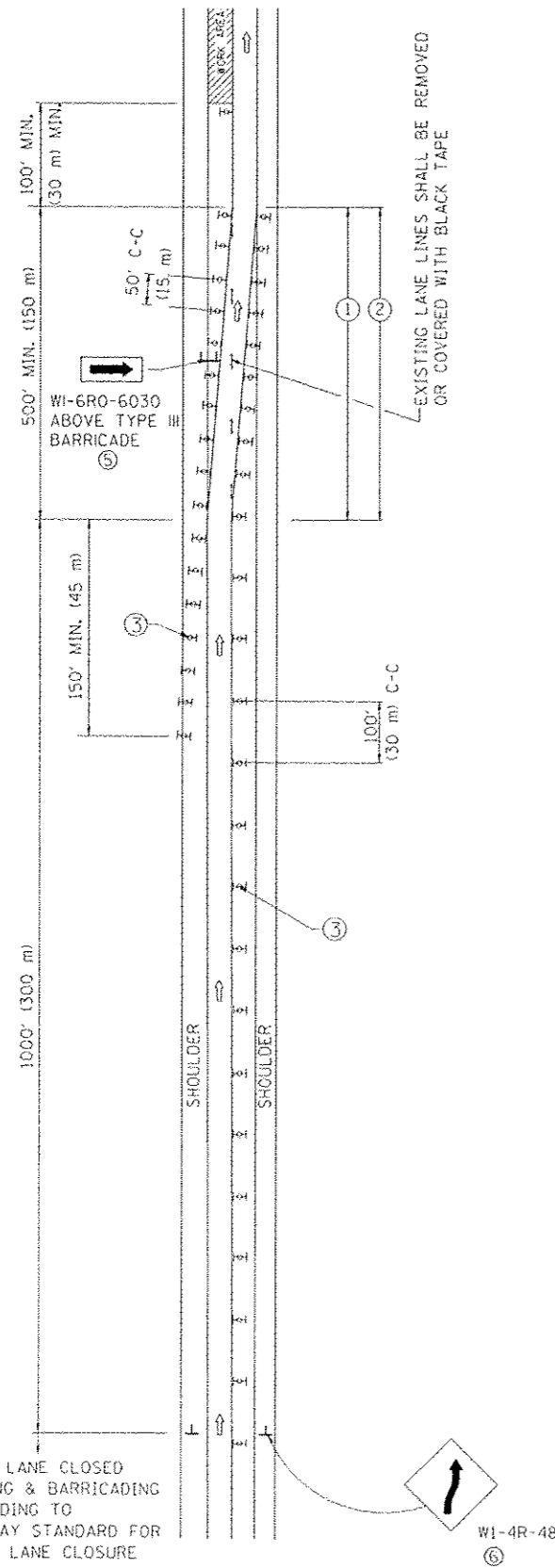
THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

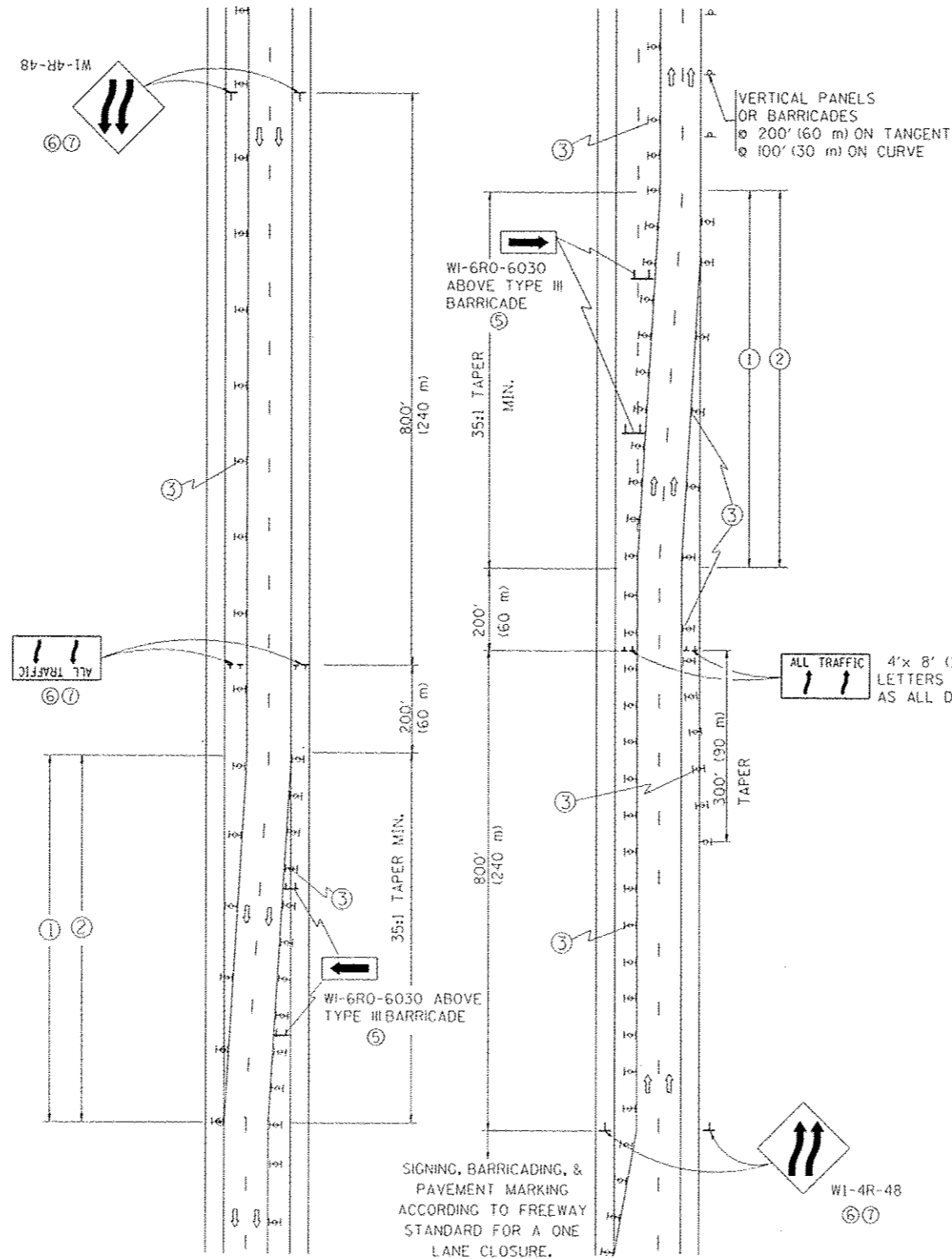
- 1 CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28" (700) HIGH.
- 2 STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- 3 A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- 4 ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- 5 THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- 6 AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- 7 THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- 8 ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- 9 ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

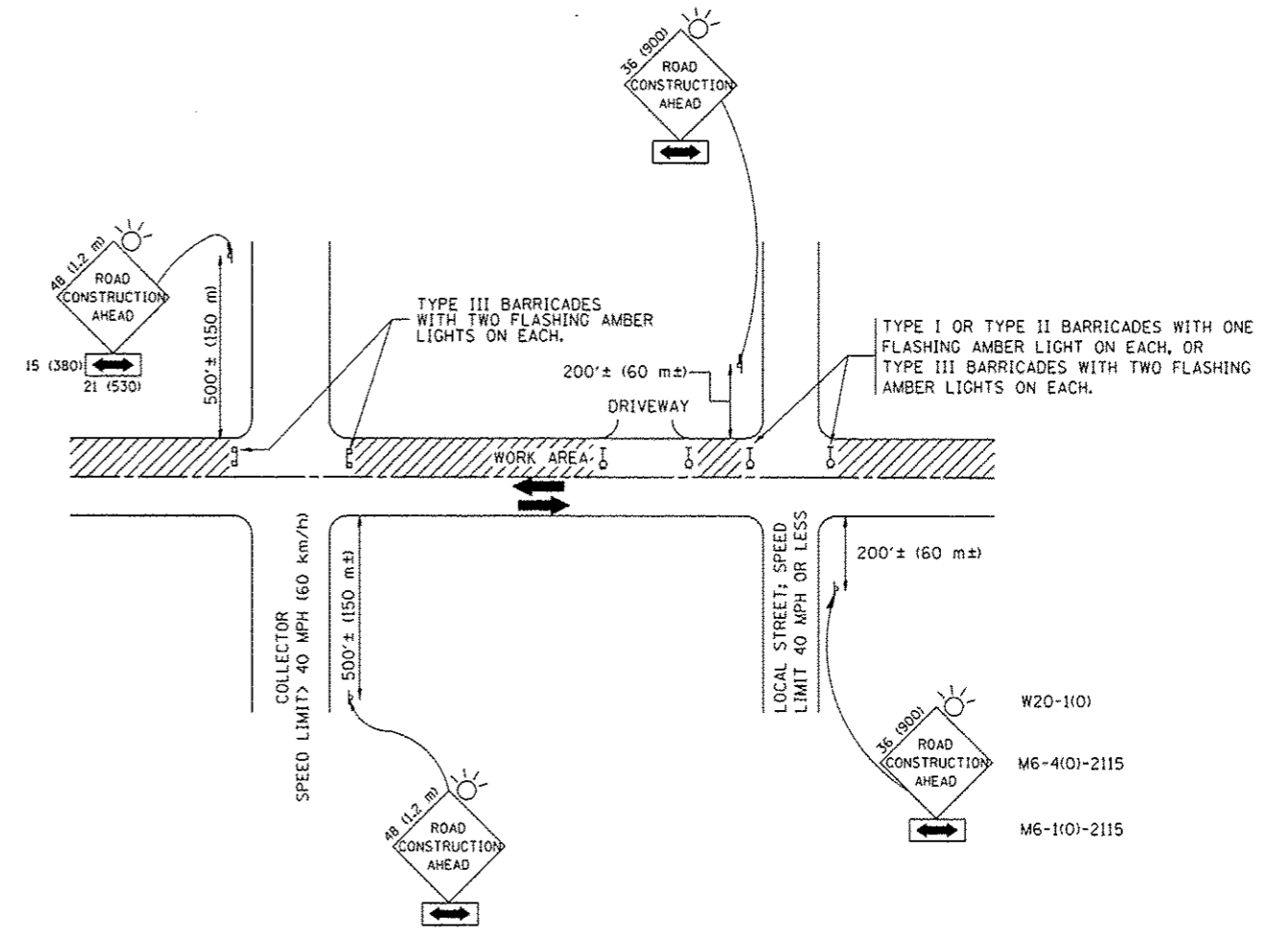
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
 - WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W24-1-48 (7)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME: W:\projects\922\24\1009.dgn	USER NAME: legas	DESIGNED: DWS	REVISED: JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE	F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 126	
		DRAWN:	REVISED: JAF 02-06			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	TC-09	CONTRACT NO. 60J15		
		CHECKED:	REVISED: SPB 01-07								
		DATE: 02-87	REVISED: SPB 12-09								



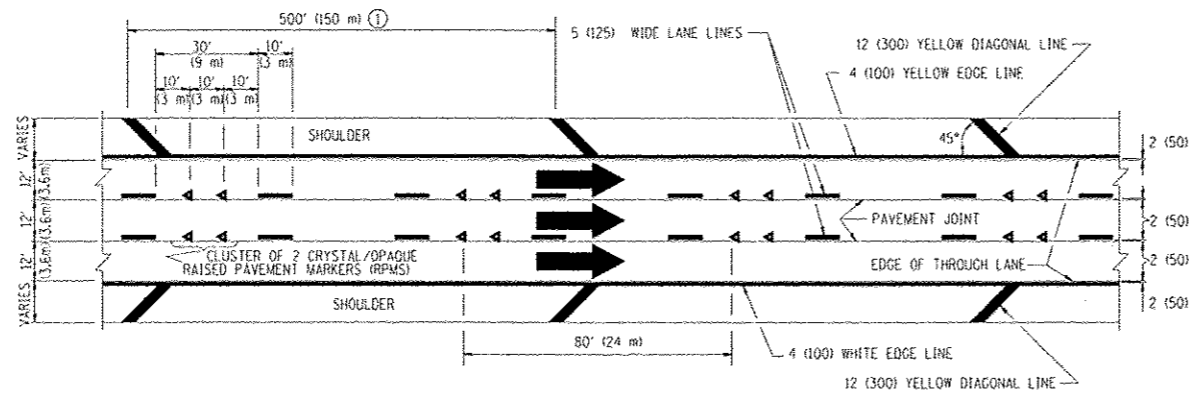
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

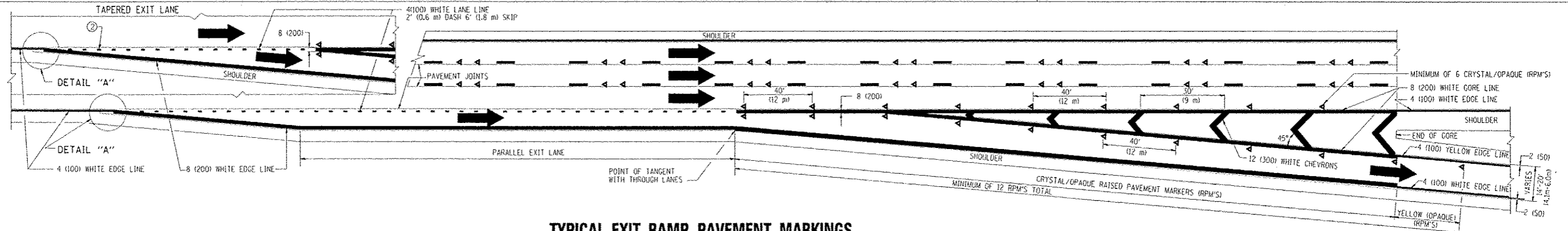
FILE NAME : M:\diststd\22x34\to10.dgn	USER NAME : ggigianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 127
	PLOT SCALE : 50,000 / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-10	
PLOT DATE : 1/4/2000	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



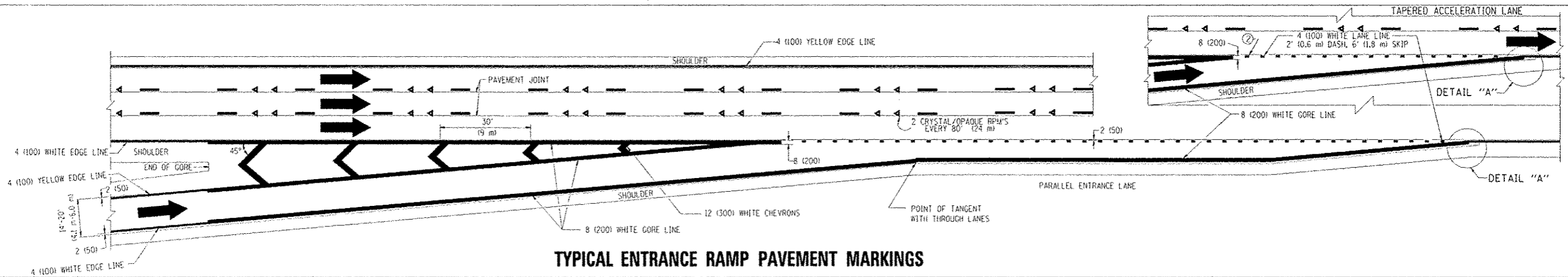
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

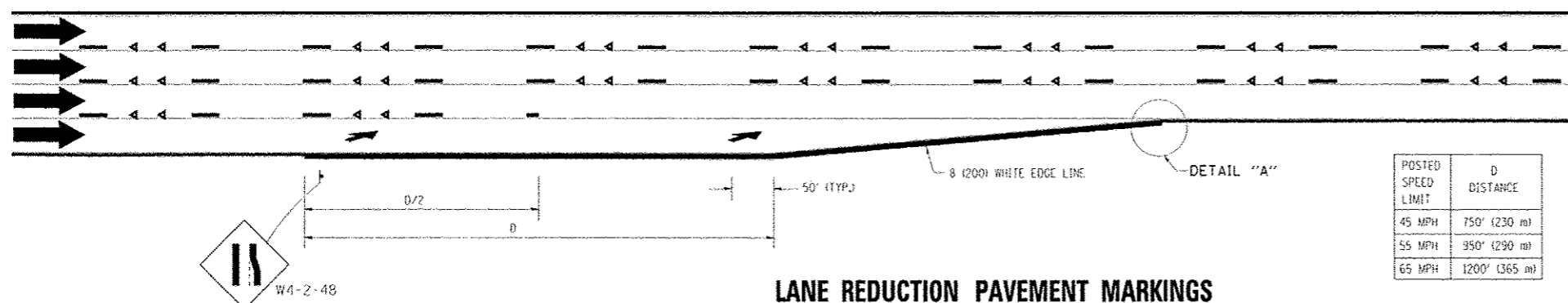
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



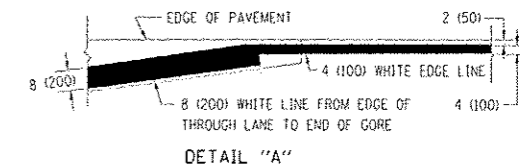
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



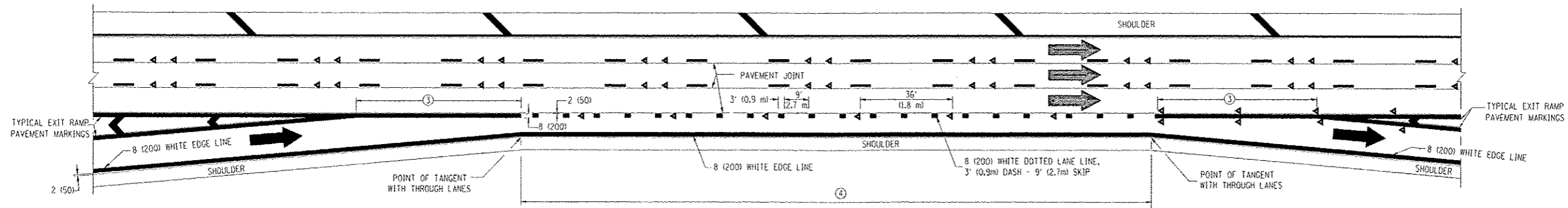
LANE REDUCTION PAVEMENT MARKINGS



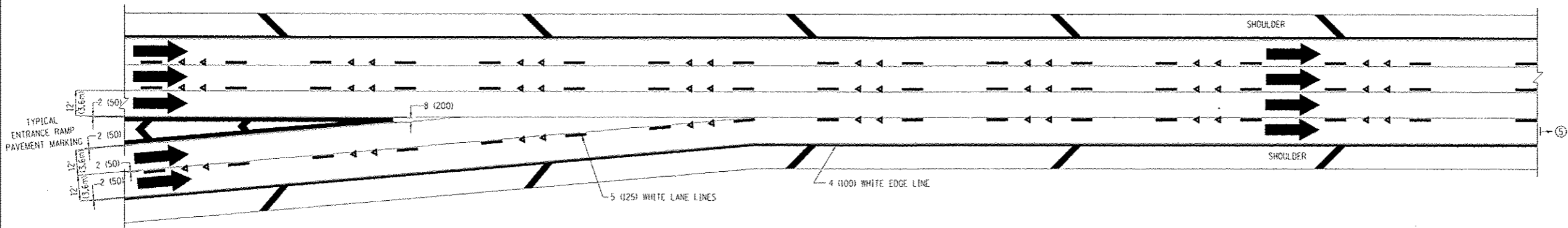
NOTES:

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2" DASH, 6" SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

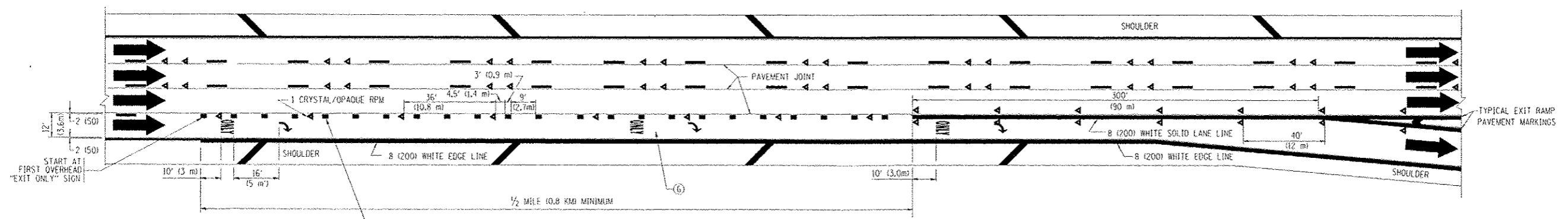
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



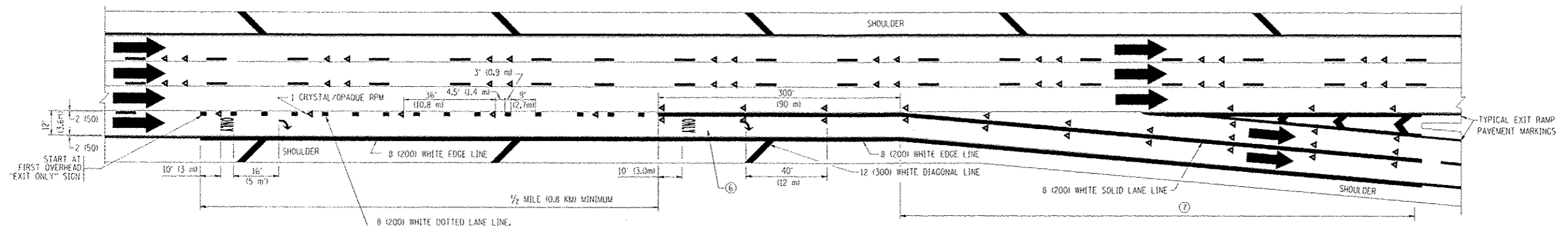
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



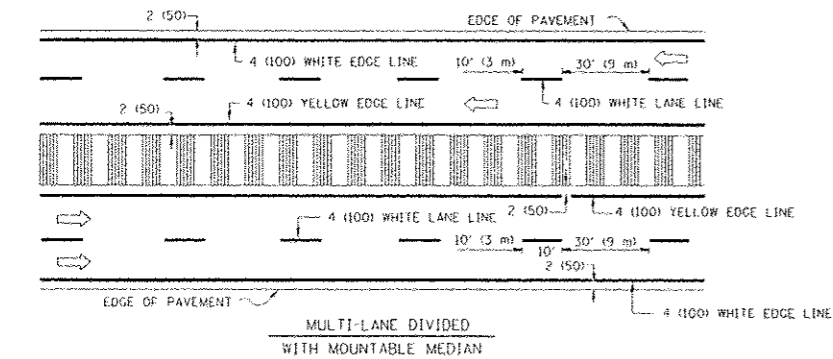
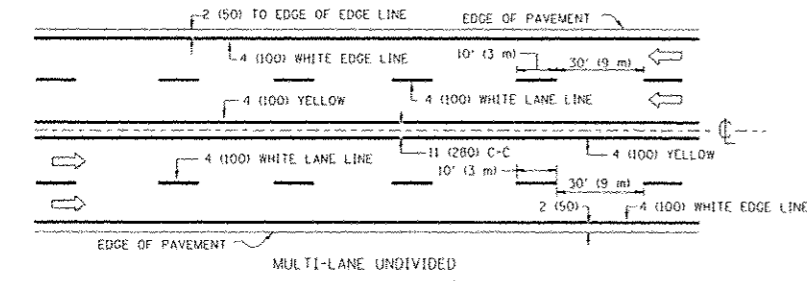
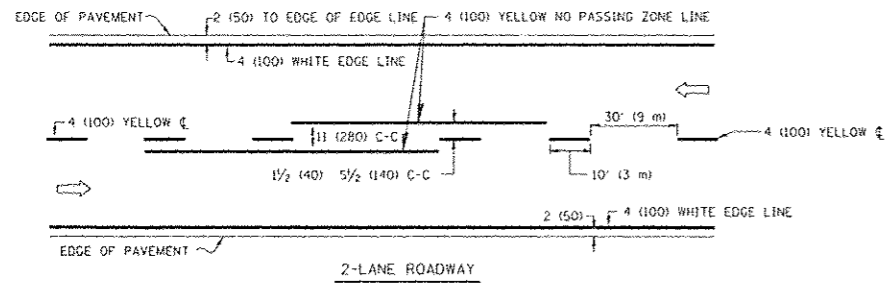
EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

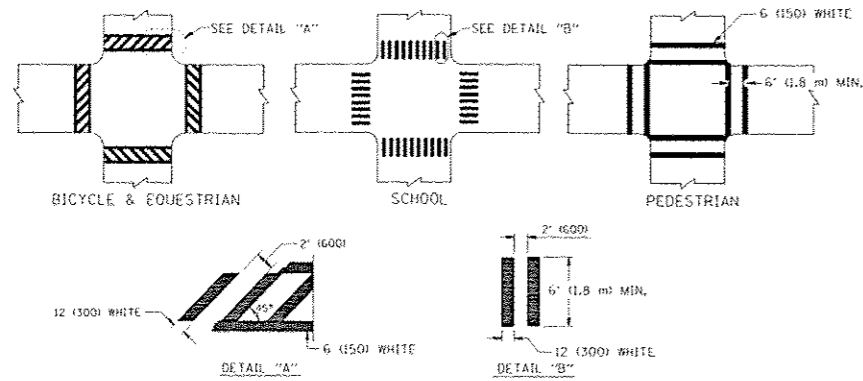
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED CORE.

FILE NAME -	USER NAME - lryse	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS		F.A.T. R.T.E. - 94	SECTION - 1920-B	COUNTY - COOK	TOTAL SHEETS - 137	SHEET NO. - 129	
PROJECT SCALE - 3/8" = 1' IN.	CHECKED -	REVISOR - J.A.F. 02-06	REVISED - S.P.B. 01-07				SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA. TO STA.	TC-12		CONTRACT NO. 60J15
PLOT DATE - 1/22/2010	DATE - 01-90	REVISOR - S.P.B. 01-10	REVISED -									
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												

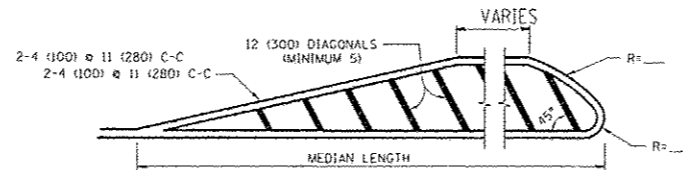
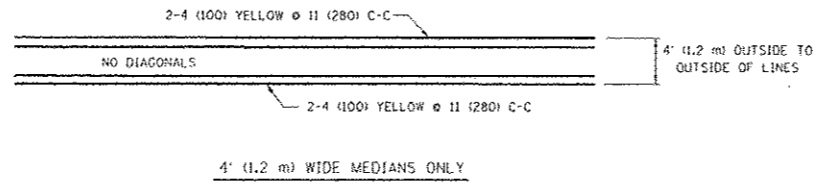


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

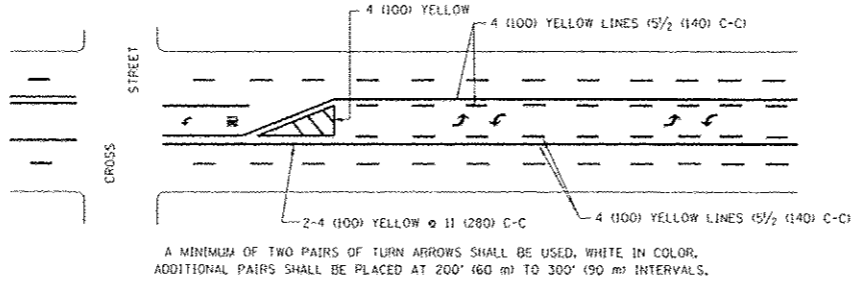


TYPICAL CROSSWALK MARKING

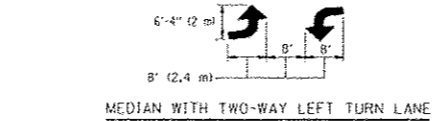


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

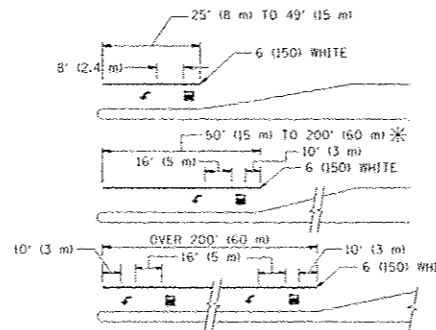
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



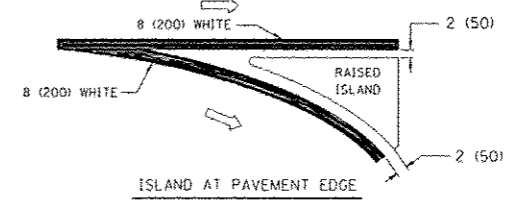
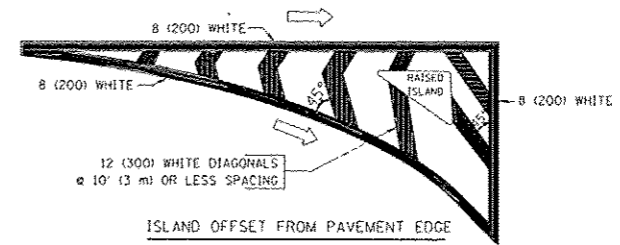
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

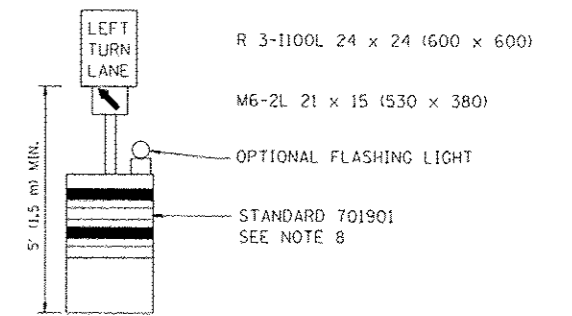
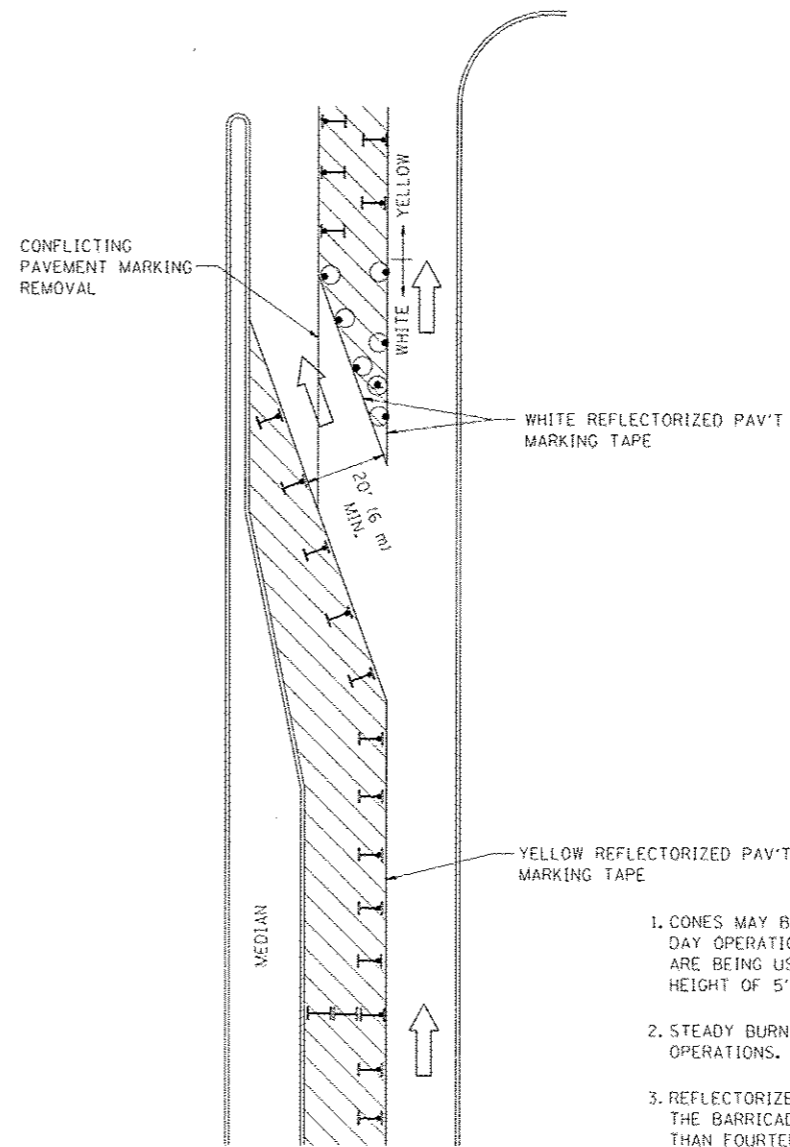


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"-3.6 SQ. FT. (0.33 m²) EACH "X"-54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

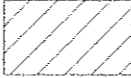







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

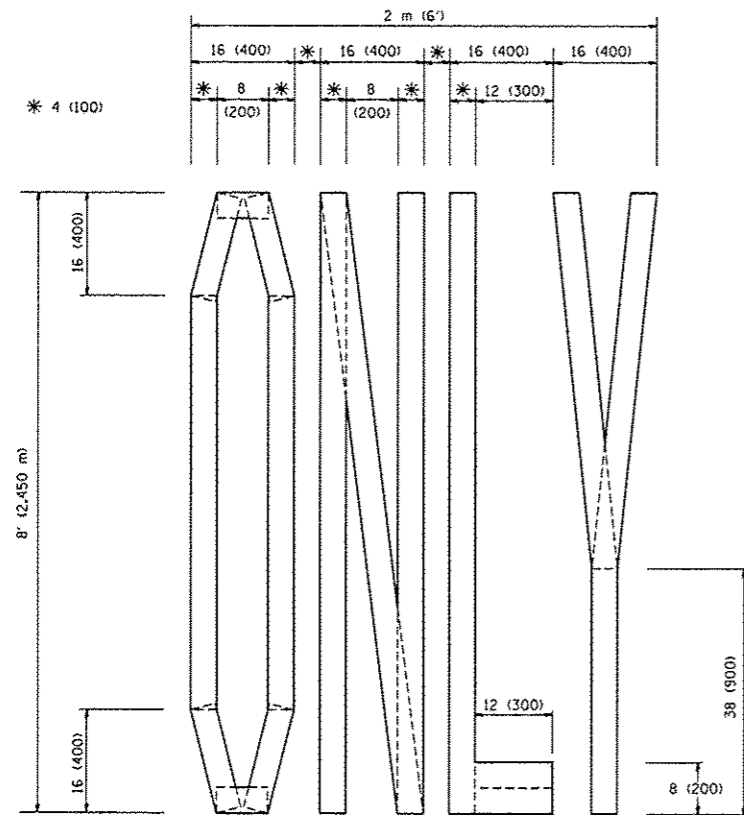
FILE NAME -	USER NAME - drvehozgn	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORG 09-14-09
c:\pwwork\pwe01\NORTWAKOSGN\J01089315\	14.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE - 48/1000 1/2" = 1'	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE - 11/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

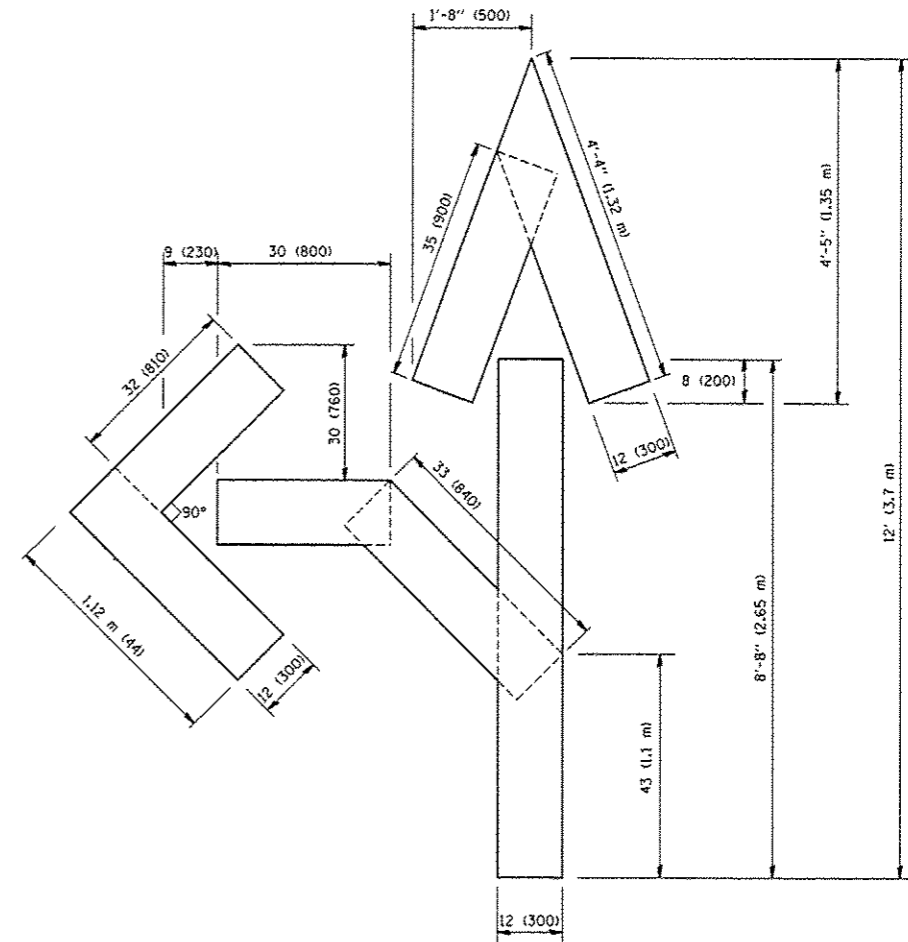
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

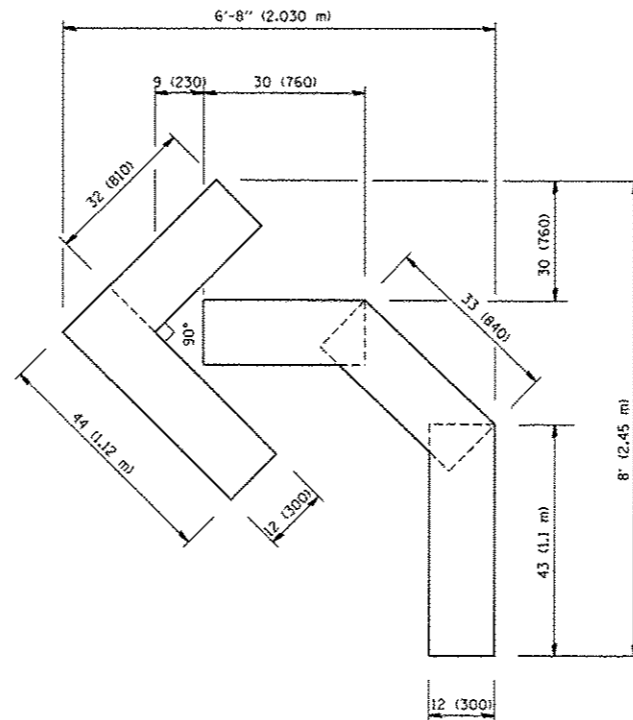
F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 131
TC-14		CONTRACT NO. 60J15		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME : W:\data\td\22x34\col6.dgn	USER NAME : geglianobt	DESIGNED : -	REVISED : -T, RAMMACHER 06-05-96
		DRAWN : -	REVISED : -T, RAMMACHER 11-04-97
		PLOT SCALE : = 50.0000 "/ IN.	REVISED : -T, RAMMACHER 03-02-98
		PLOT DATE : 1/4/2000	REVISED : -E. GOMEZ 08-28-00
		DATE : 09-18-94	

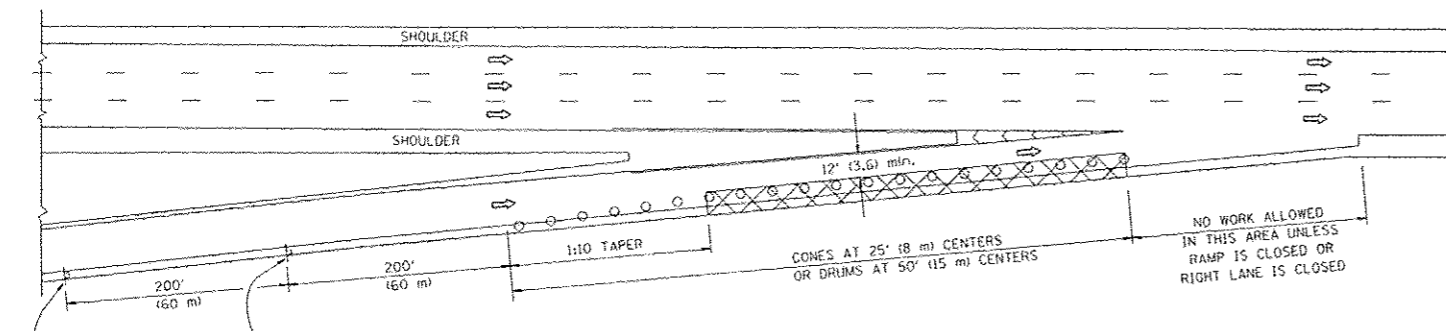
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

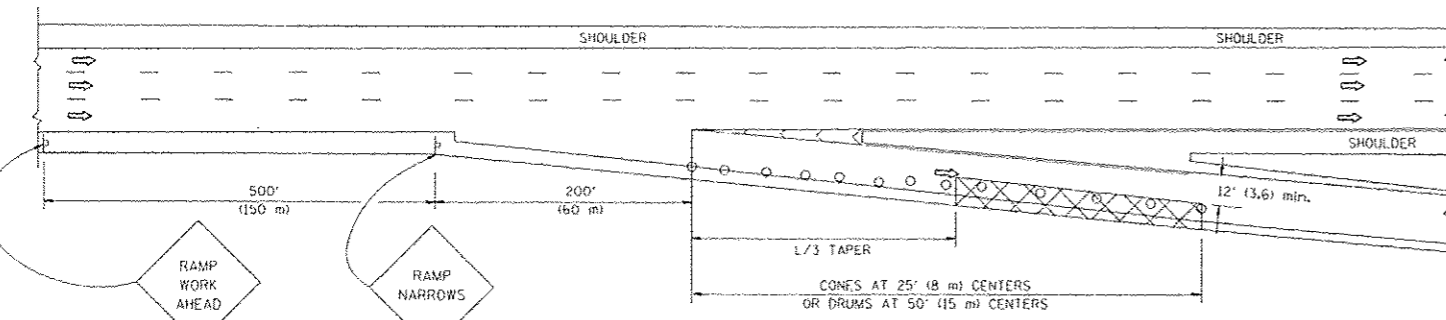
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 132
TC-16			CONTRACT NO. 60J15	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

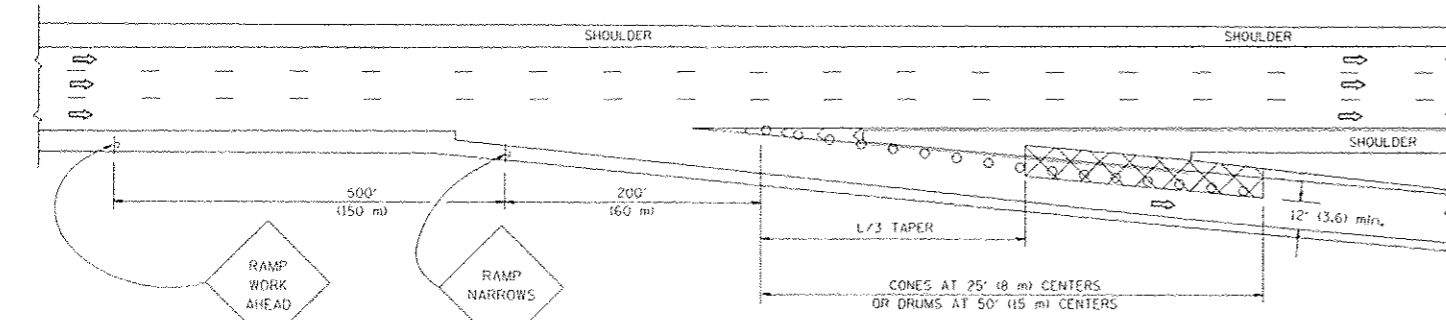
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

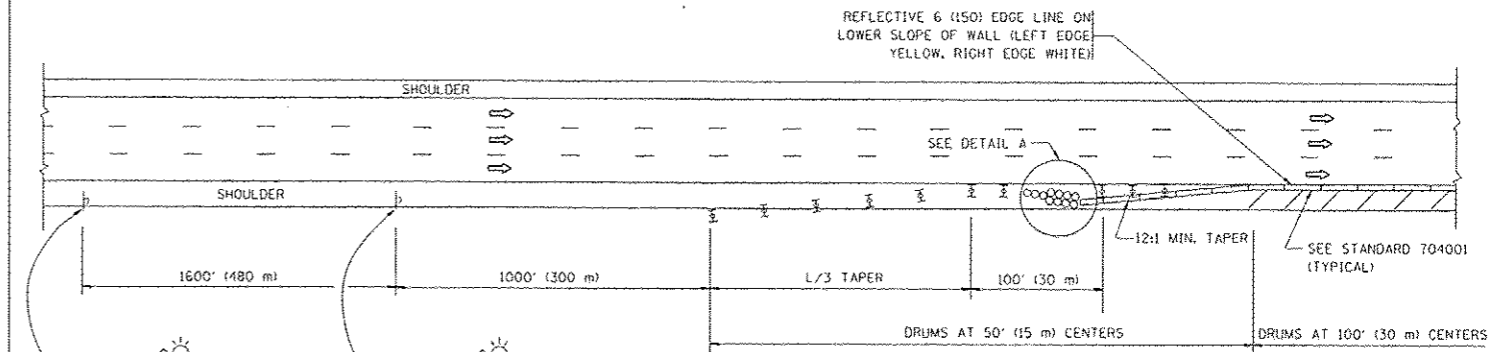
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

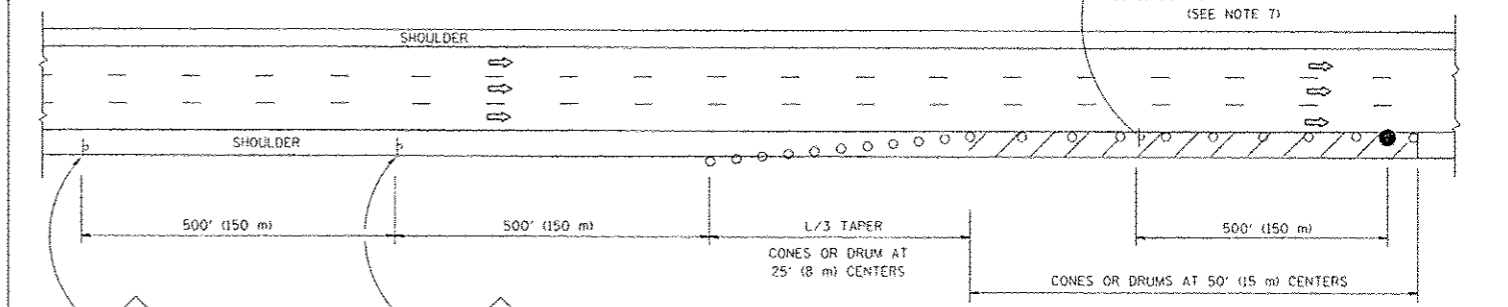
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(W/S)$ $L = (W/S)$
	$W =$ WIDTH OF OFFSET IN FEET (METERS)
	$S =$ NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

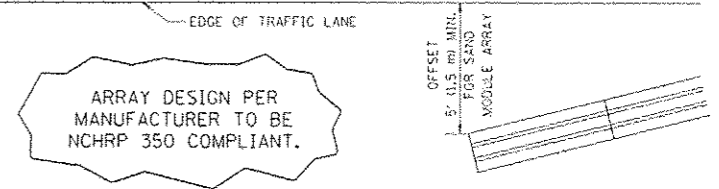


PERMANENT SHOULDER CLOSURE



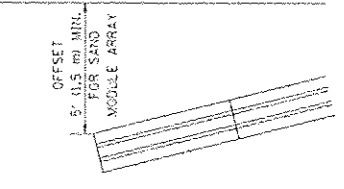
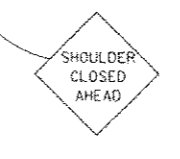
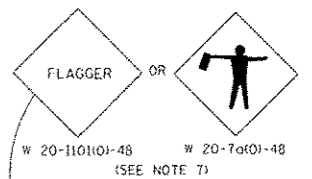
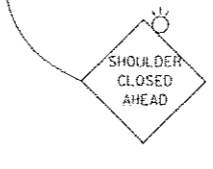
DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRUCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)



ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME: M:\distrad\22x34\1e17.dgn

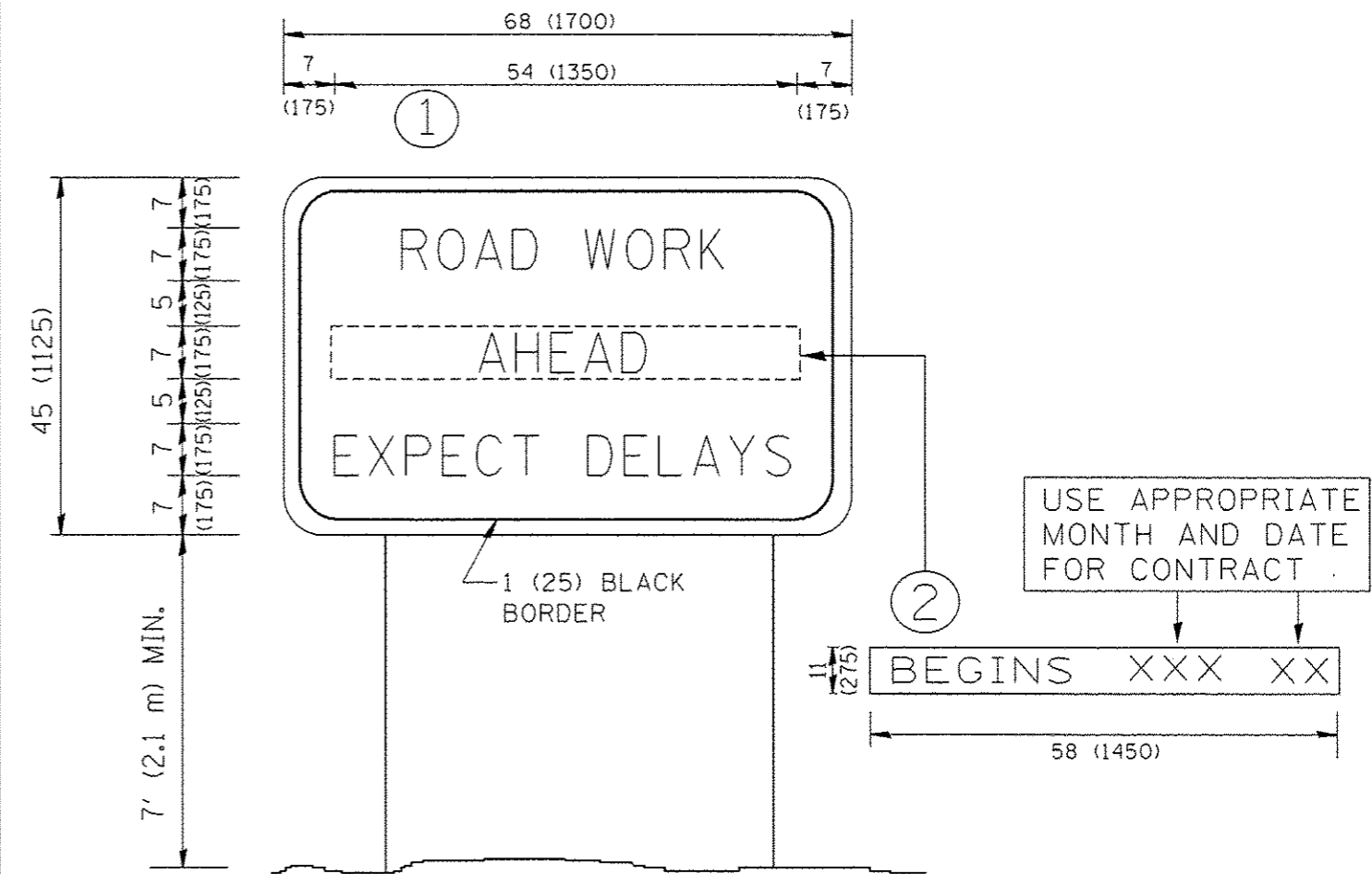
USER NAME: c1m322	DESIGNED: -
PLOT SCALE: 1/8"=1'-0"	DRAWN: D.W.S.
PLOT DATE: 11/26/2010	CHECKED: -
	DATE: 11-96

REVISED: Q4-03
REVISED: J.A.F. 12-06
REVISED: S.P.B. 01-07
REVISED: S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY	
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA. TO STA.	

F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 133
TC-17			CONTRACT NO. 60J15	
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT				

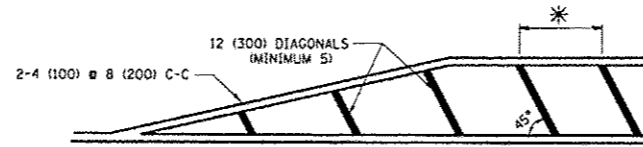
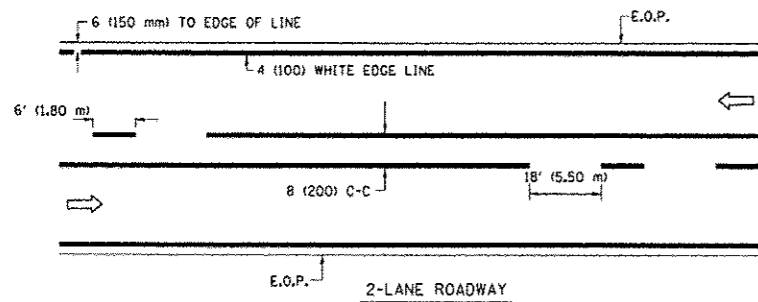


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

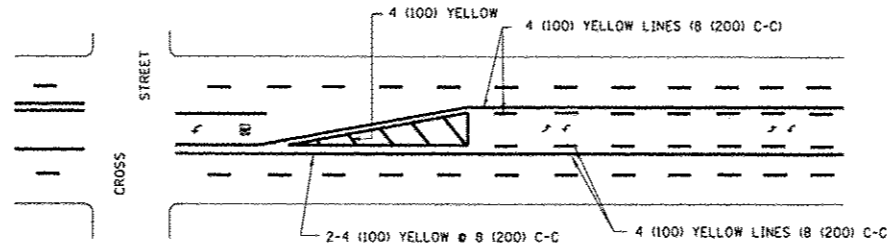
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME : W:\datastd\22x34\to22.dgn	USER NAME : gglienobt	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.I. RTE. 94	SECTION 1920-B	COUNTY COOK	TOTAL SHEETS 137	SHEET NO. 134		
PLOT SCALE : 50.000 / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60J15						
PLOT DATE : 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

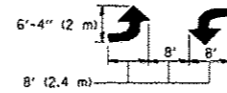


- FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
- DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

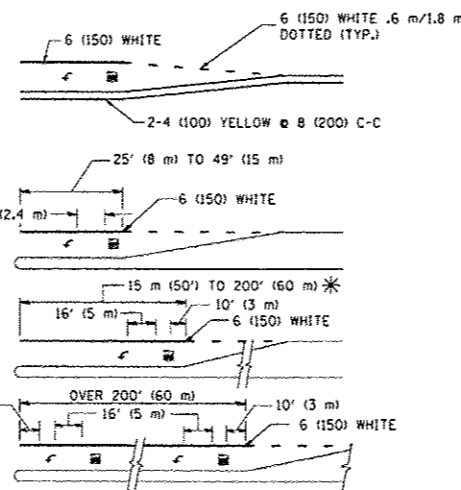


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

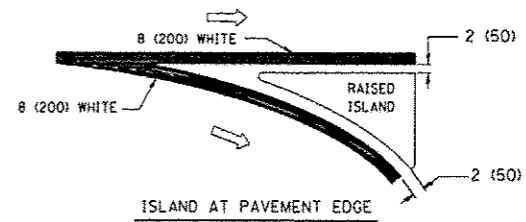
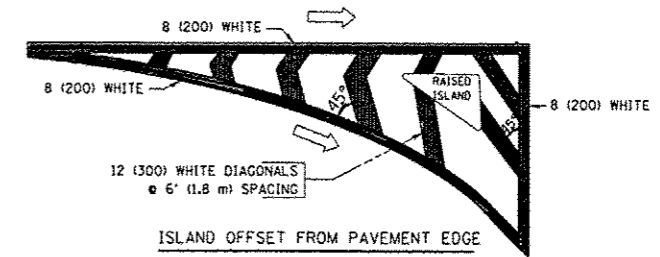


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

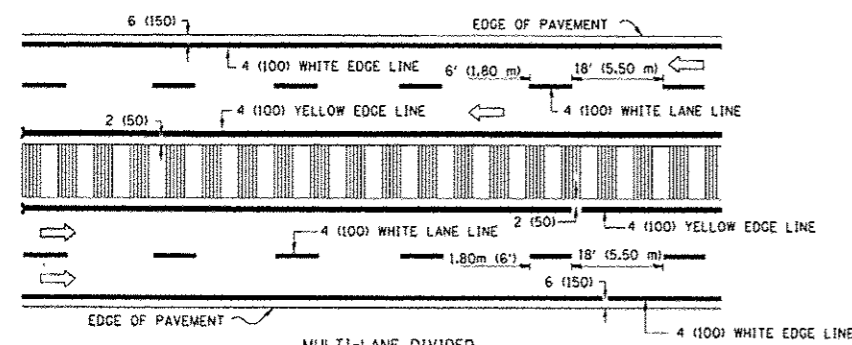
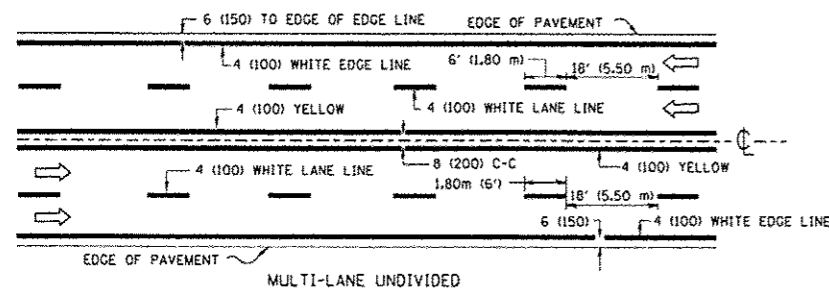


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) 45° 24 (600) 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 4 (100) WITH 12 (300) DIAGONALS 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

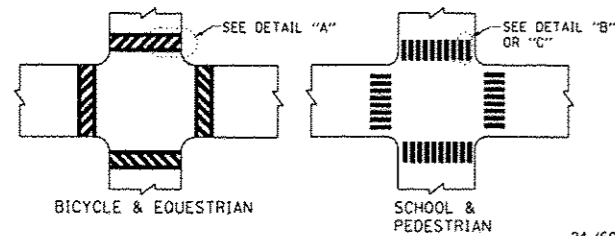
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

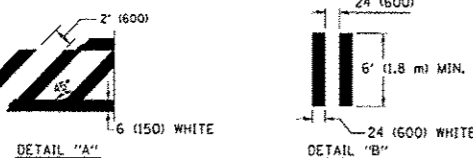


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



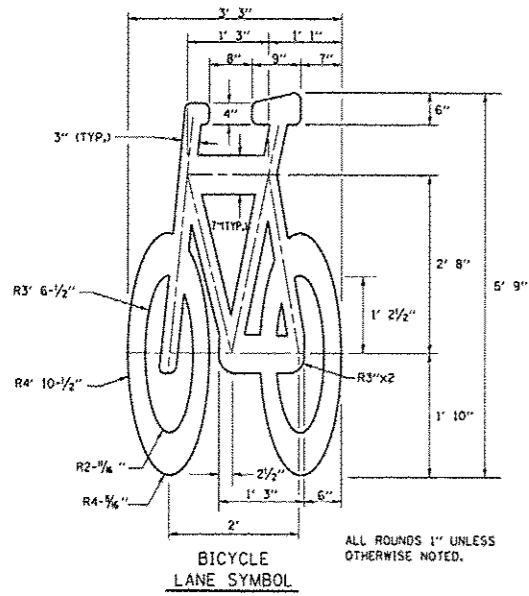
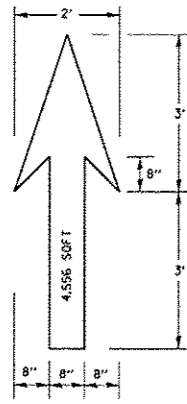
FILE NAME	USER NAME	DESIGNED	REVISED
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		DRAWN	REVISED
		K. ENG 02-28-12	K. ENG 02-28-12
		CHECKED	REVISED
		DATE	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

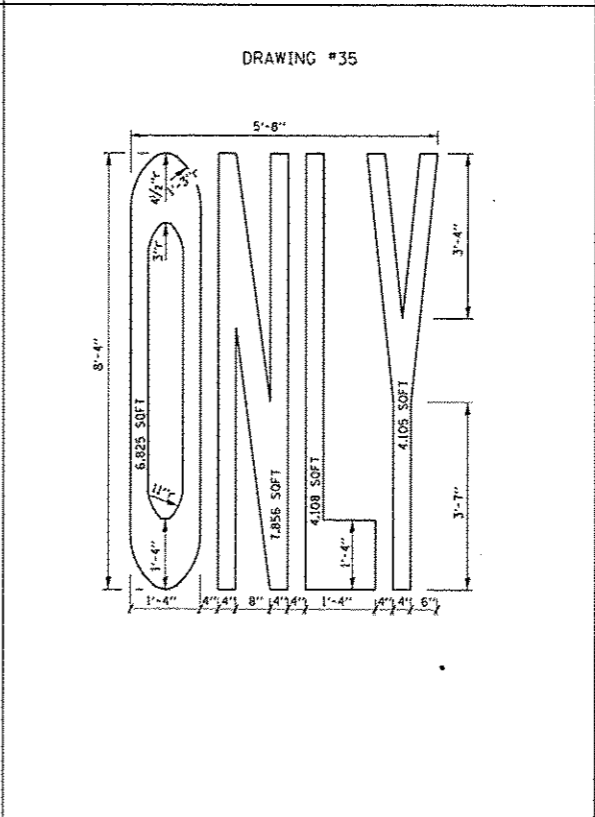
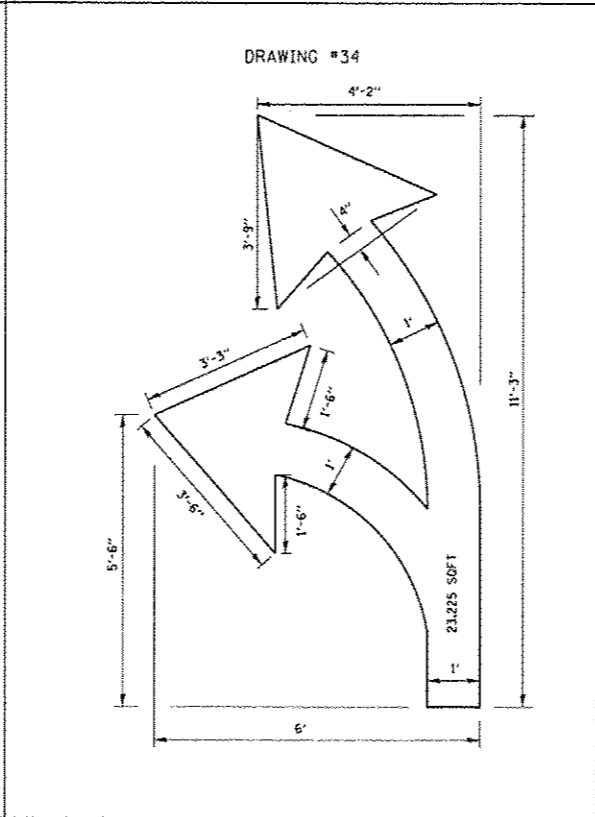
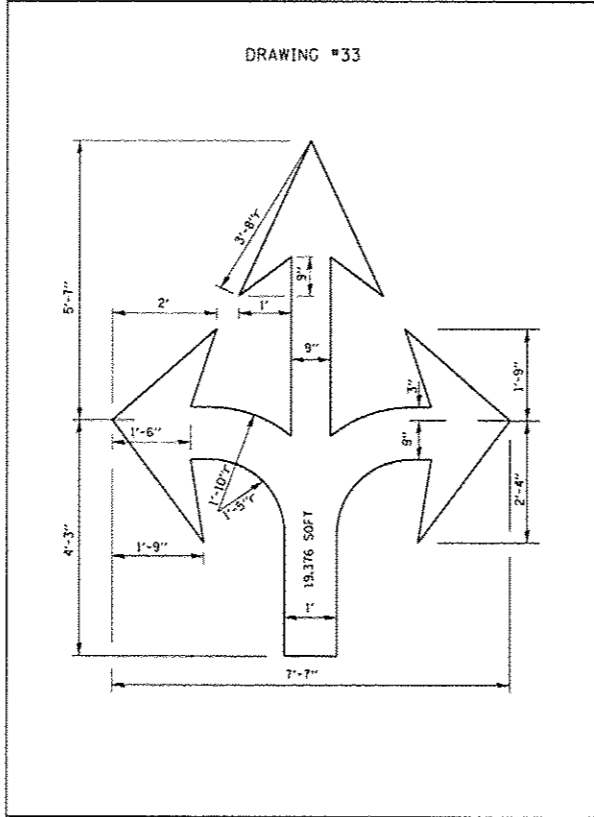
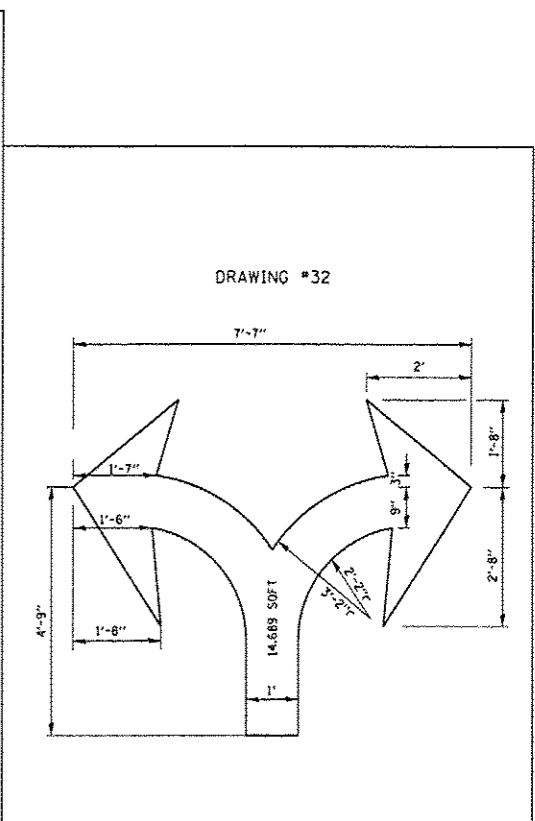
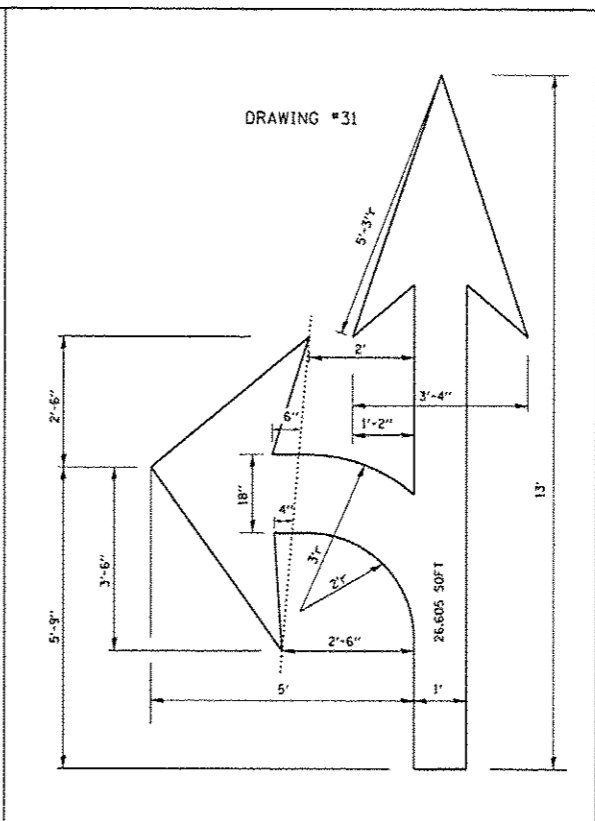
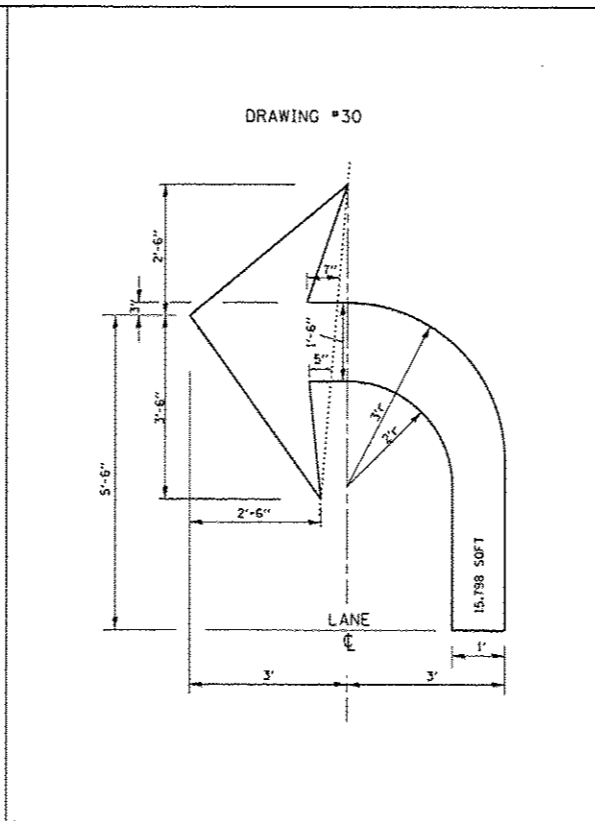
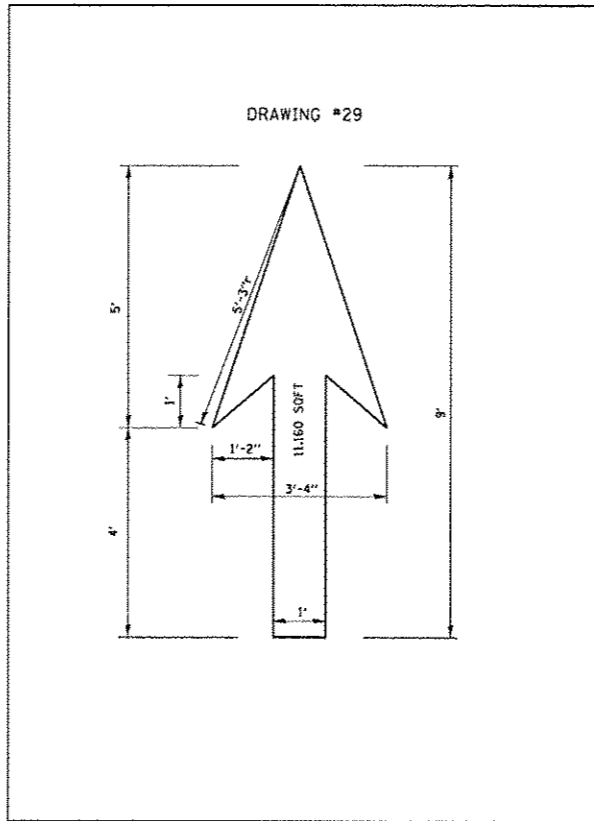
SCALE: NONE SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	135
TC-24		CONTRACT NO. 60J15		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN COMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
 DRAWING #28



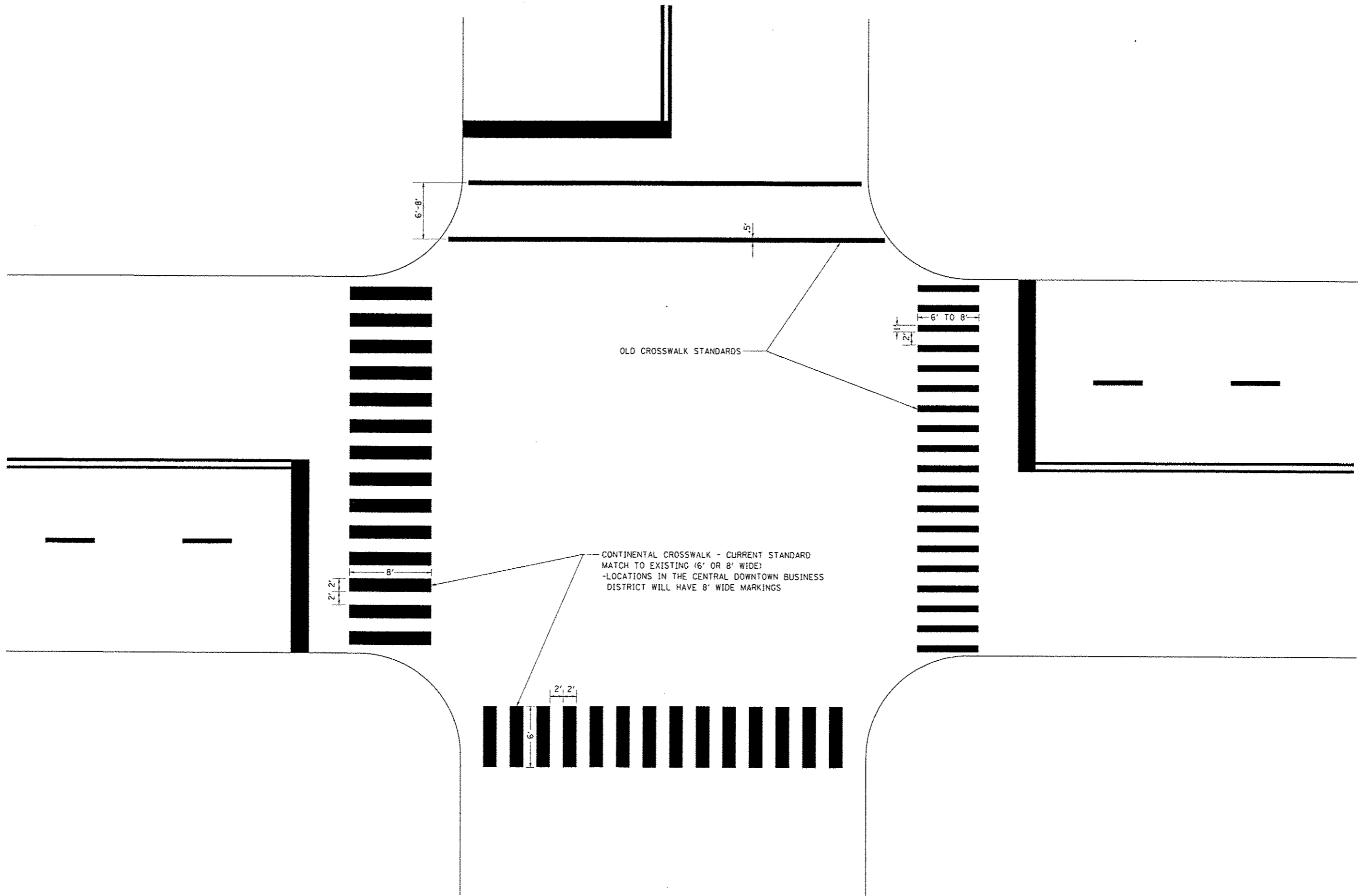
NOTE:
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME	USER NAME	DESIGNED	REVISED
si\p\work\pav\td\drivokosgn\d0128315\td	drivokosgn	-	- T. RAMMACHER 12-07-00
		DRAWN	REVISED - K. ENG 02-28-12
		CHECKED	REVISED -
		DATE	REVISED -
	PLOT SCALE = 5/8" = 1'		
	PLOT DATE = 3/29/2012		

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	136
TC-24			CONTRACT NO. 60J15	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



OLD CROSSWALK STANDARDS

CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
ci\pwwork\p1dot\drivakosgn\101128315\1.dgn		DRAWN -	REVISED - K. ENG 02-28-12
	PLOT SCALE = 1/2" = 1'	CHECKED -	REVISED -
	PLOT DATE = 3/29/2012	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO	
TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1920-B	COOK	137	137
TC-24			CONTRACT NO. 60J15	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				