

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	1
		ILLINOIS	CONTRACT NO. 60W86	

*44 41=45

09-20-13 LETTING ITEM 013

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

VARIOUS ROUTES
VARIOUS LOCATIONS
SECTION 2013-0411
CONCRETE BARRIER WALLS,
CONCRETE NOISE ABATEMENT WALLS AND
BRIDGE PARAPET REPAIRS
VARIOUS COUNTIES

C-91-390-13

D-91-390-13



FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENTS LOCATED IN THE CITIES OF CHICAGO
AND JOLIET AND THE VILLAGES OF CRETE, HINSDALE,
HODGKINS, AND MCCOOK.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3056
PROJECT MANAGER: ISSAM RYYAN (847) 705-4178

CONTRACT NO. 60W86

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED July 9 2013
John F. [Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Aug 16 2013
John D. Baranzoli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Aug 16 2013
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1.	TITLE SHEET
2.	INDEX OF SHEETS, STATE STANDARDS, & GENERAL NOTES
3.	SUMMARY OF QUANTITIES
4.	SCHEDULE OF QUANTITIES
5.	TYPICAL REPAIR FOR FULL DEPTH BARRIER WALL
6.	TYPICAL REPAIR FOR HALF DEPTH BARRIER WALL
7.	TYPICAL BRIDGE PARAPET REPAIR DETAILS
8-9.	REPAIR DETAILS, LOCATION 1 (SN 022-0211)
10-11.	REPAIR DETAILS, LOCATION 2 (SN 022-0211)
12-13.	REPAIR DETAILS, LOCATION 3 (NOISE ABATEMENT WALL REPAIR)
14.	TYPICAL CONCRETE NOISE ABATEMENT WALL REPAIR DETAILS AND NOTES (IL 394)
15.	REPAIR DETAILS, LOCATION 4 (CONCRETE BARRIER WALL REPAIR)
16-18.	REPAIR DETAILS, LOCATION 5 (SN 016-1084)
19-20.	REPAIR DETAILS, LOCATION 6 (SN 016-1083)
21.	REPAIR DETAILS, LOCATION 7 (CONCRETE BARRIER WALL REPAIR)
22.	REPAIR DETAILS, LOCATION 8 (CONCRETE BARRIER WALL REPAIR)
23.	REPAIR DETAILS, LOCATION 9 (NOISE ABATEMENT WALL REPAIR)
24.	REPAIR DETAILS, LOCATION 10 (NOISE ABATEMENT WALL REPAIR)
25.	NOISE ABATEMENT WALL TYPICAL SECTION
26.	TYPICAL CONCRETE NOISE ABATEMENT WALL REPAIR DETAILS AND NOTES (I-55)
27.	REPAIR DETAILS, LOCATION 11 (CONCRETE BARRIER WALL REPAIR)
28.	REPAIR DETAILS, LOCATION 12 (CONCRETE BARRIER WALL REPAIR)
29.	REPAIR DETAILS, LOCATION 13 (CONCRETE BARRIER WALL REPAIR)
30-31.	REPAIR DETAILS, LOCATION 14 (SN 016-1110)
32.	REPAIR DETAILS, LOCATION 15 (STRUCTURAL REPAIR OF CONCRETE)
33.	REPAIR DETAILS, LOCATION 16 (CONCRETE BARRIER WALL REPAIR)
34.	REPAIR DETAILS, LOCATION 17 (CONCRETE BARRIER WALL REPAIR)
35.	REPAIR DETAILS, LOCATION 18 (CONCRETE BARRIER WALL REPAIR)
35A.	REPAIR DETAILS, LOCATION 19 (CONCRETE BARRIER WALL REPAIR)
36-39.	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL (BM-21)
40.	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-8)
41.	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE (TC-9)
42.	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
43.	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
44.	ARTERIAL ROAD INFORMATION SIGN (TC-22)

STD NO.	DESCRIPTION
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
637001-05	CONCRETE BARRIER 32" HEIGHT
637006-03	CONCRETE BARRIER 42" HEIGHT
701101-03	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701400-06	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-07	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-08	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701421-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701446-04	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER, OR CORSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES

GENERAL NOTES (BRIDGE)

THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO INSURE THAT NO DEBRIS WILL ENDANGER OR INTERFERE WITH TRAFFIC ON THE ROADWAY BENEATH THE BRIDGE ACCORDING TO ARTICLE 107.09 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPROPRIATE PAY ITEM INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM, COST INCLUDED WITH CONCRETE REMOVAL.

PLAN DIMENTIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENTIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENT PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

JOINTS ARE TO BE INSTALLED TO MATCH EXISTING.

IF A GAP IN THE PARAPET IS TO OCCUR FOR ANY PERIOD OF TIME, A TEMPORARY RAILING MUST BE INSTALLED SO THAT AT NO TIME SHALL THERE BE AN OPENING IN ANY PORTION OF THE EXISTING SYSTEM. IF THE GAP IS IN A BARRIER WALL SECTION, TEMPORARY RAILING OR BARRELS SHALL BE USED AT THE DISCRETION OF THE ENGINEER.

THE CONTRACTOR SHALL SALVAGE AND RETURN THE EXISTING GUARDRAIL TO:

DISTRICT BRIDGE OFFICE
1101 BIESTERFIELD RD.
ELK GROVE VILLAGE, IL 60007

THIS WORK SHALL INCLUDE REMOVING, TRANSPORTING AND UNLOADING THE EXISTING CARRIER GUARDRAIL TO THE ABOVE YARD WHICH COST IS INCLUDED IN THE COST OF "CONCRETE REMOVAL".

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312)744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITIES OF CHICAGO AND JOLIET AND THE VILLAGES OF CRETE, HINSDALE, HODGKINS, AND MCCOOK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 (FOR ARTERIALS) AND (847)705-4151 (FOR EXPRESSWAYS) A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS.

FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB & CUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.

IN SOME CASES, THERE IS A CONDUIT IN THE BASE OF THE BARRIER WALL AND BRIDGE PARAPET (LOC 5, 13 AND 14). THIS CONDUIT CONTAINS ELECTRICAL AND/OR FIBER OPTIC CABLE. CARE MUST BE TAKEN TO PREVENT DAMAGE TO THIS CONDUIT. IF IT IS DAMAGED, THE CONDUIT SHALL BE REPAIRED OR REPLACED. IF THE ELECTRICAL OR COPPER COMMUNICATIONS CABLE IS DAMAGED, IT SHALL BE REPLACED FROM JUNCTION BOX TO JUNCTION BOX (1500 FT. MIN.). IF THE FIBER OPTIC COMMUNICATIONS CABLE IS DAMAGED, IT SHALL BE REPLACED FROM EXISTING SPLICE ENCLOSURE TO EXISTING SPLICE ENCLOSURE OR TERMINATION POINT, FOR THE LOCATION OF THE CONDUITS, CONTACT ROY LAWSON AT THE ELECTRICAL FIELD OFFICE AT (708) 524-2145.

ANY CONDUIT RUSTED THRU, EXPOSED CABLES, PULL BOXES OR HANDHOLES FOR BRIDGE LIGHTING, TRAFFIC SIGNALS, COUNTERS, SURVEILLANCE EQUIPMENT FOUND IN DISREPAIR THE ENGINEER SHALL TAKE THE APPROPRIATE ACTION:

NO SAW CUTTING OR REMOVAL WILL BE MADE OR CONCRETE POURED UNTIL THE APPROPRIATE ACTION IS TAKEN TO RESOLVE EACH ISSUE;

ALL WORK REQUIRED TO REPAIR/REPLACE SIGNAL SURVEILLANCE OR LIGHTING ITEMS WILL BE DOCUMENTED BY THE ENGINEER AND THE BUREAU OF TRAFFIC WILL BE NOTIFIED.

FOR LOCATION 11, THE CONTRACTOR SHALL COORDINATE WITH ELECTRICAL MAINTENANCE CONTRACTOR "MEADE ELECTRIC" TO INSTALL THE CONDUIT FOR ELECTRICAL LIGHTING SYSTEM.

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\paw\dos\asynorecp\083409210199013-shi-gennote.dgn	SEYMORECP	DRAWN	REVISED								VARIES	2013-0411	VARIES	44	2
Default	PLOT SCALE :	CHECKED :	REVISED :								CONTRACT NO. 60W86				
	PLOT DATE :	DATE :	REVISED :								ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			URBAN 0040 CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
20200100	EARTH EXCAVATION	CU YD	7					
20700220	POROUS GRANULAR EMBANKMENT	CU YD	2					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	16					
25000210	SEEDING, CLASS 2A	ACRE	0.003					
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	16					
44201298	DOWEL BARS 1 1/4"	EACH	9					
44213200	SAW CUTS	FOOT	105					
44213202	TIE BARS 1"	EACH	106					
50102400	CONCRETE REMOVAL	CU YD	32					
50300225	CONCRETE STRUCTURES	CU YD	16					
50300255	CONCRETE SUPERSTRUCTURE	CU YD	8					
50300300	PROTECTIVE COAT	SQ YD	96					
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	578					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6					
67100100	MOBILIZATION	L SUM	1					
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1					

SUMMARY OF QUANTITIES			URBAN 0040 CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1					
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1					
X0326791	NOISE ABATEMENT WALL REPAIRS	L SUM	1					
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1					
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	14					
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	154.2					
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1					

Rev.

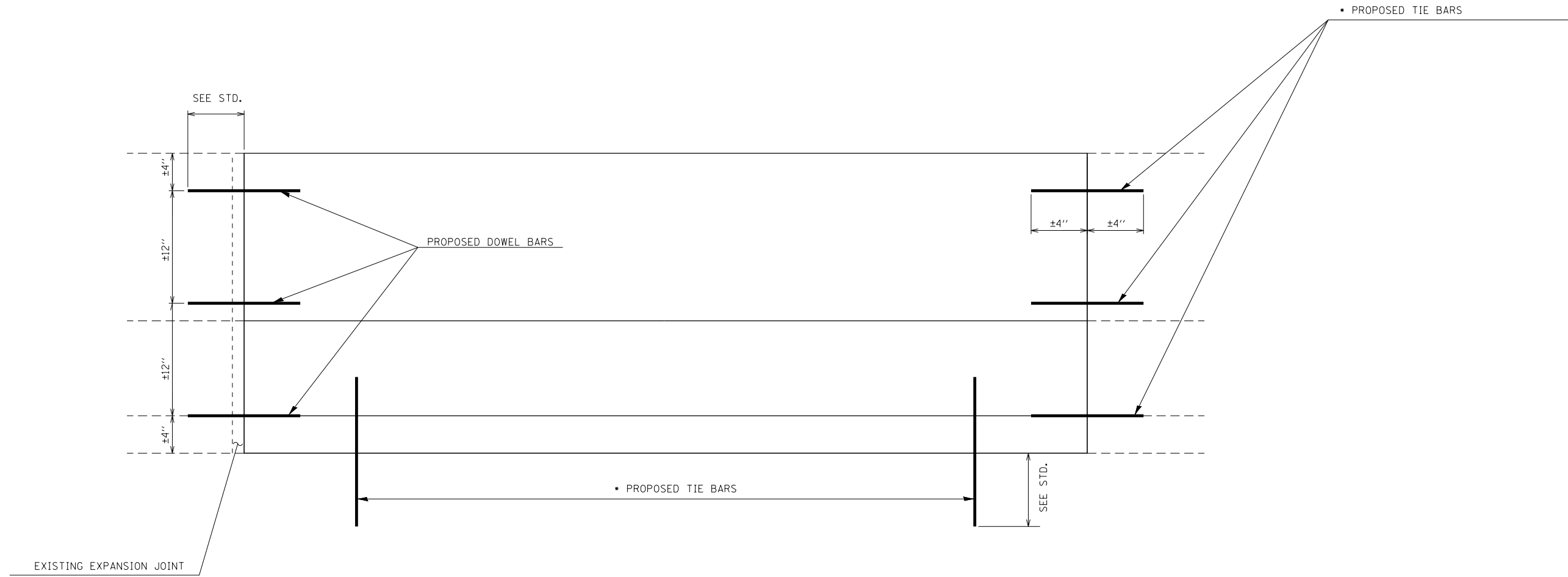
SCHEDULE OF QUANTITIES	COOK COUNTY													WILL COUNTY			DUPAGE COUNTY		
	I-290 EISENHOWER EXPRESSWAY	I-55 STEVENSON EXPRESSWAY					I-90/I-94 KENNEDY EXPRESSWAY		I-90/I-94 DAN RYAN EXPRESSWAY					IL 394 CAULUMET EXPRESSWAY/ BISHOP FORD FREEWAY	I-55 STEVENSON EXPRESSWAY		CHICAGO AVENUE @ IL 83 (KINGERY HIGHWAY)		IL 83 (KINGERY HIGHWAY)
LOCATION	19	4	5	6	7	8	11	12	13	14	15	16	17	3	9	10	1	2	18
TEMPORARY INFORMATION SIGNING (50 FT)														51.4			25.7	25.7	51.4
TRAFFIC CONTROL AND PROTECTION (EXPRESSWAY)	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X			
TRAFFIC CONTROL AND PROTECTION, STANDARD 701421																	X	X	X
TRAFFIC CONTROL AND PROTECTION, STANDARD 701501																	X	X	
TRAFFIC CONTROL AND PROTECTION, STANDARD 701801																		X	
NIGHTTIME WORK ZONE LIGHTING	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X		X	
RAILROAD PROTECTIVE LIABILITY INSURANCE	X																		

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
c:\pw_work\pwl\dd\seymorecp\0348982\0139013-snr-schedule.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES		
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	4
			CONTRACT NO. 60W86	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



FULL DEPTH REPAIR

NOTE:

ALL BARRIER WALL REPAIR SHALL BE COMPLETED IN ACCORDANCE WITH STATE STANDARDS 637001 AND 637006 AND SECTION 637 OF THE STANDARD SPECIFICATIONS AND AS SHOWN ON THE PLANS. ALL PROPOSED DOWEL BARS AND TIE BARS SHALL BE DRILLED AND GROUTED IN PLACE. REINFORCEMENT SHALL BE INCLUDED AS SHOWN IN STANDARDS 637001 AND 637006 AND DESCRIBED IN SECTION 637 OF THE STANDARD SPECIFICATIONS EXCEPT WHEN A REINFORCEMENT CAGE IS ENCOUNTERED. IN SUCH A CASE, THE CONTRACTOR SHALL REMOVE THE DAMAGED PORTIONS OF THE CAGE AND SHALL REPLACE IT WITH BARS OF THE SAME SIZE AND SPACING. THE SCOPE OF REPAIR AT EACH LOCATION SHALL BE AS SHOWN ON THE PLANS AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

* USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS.

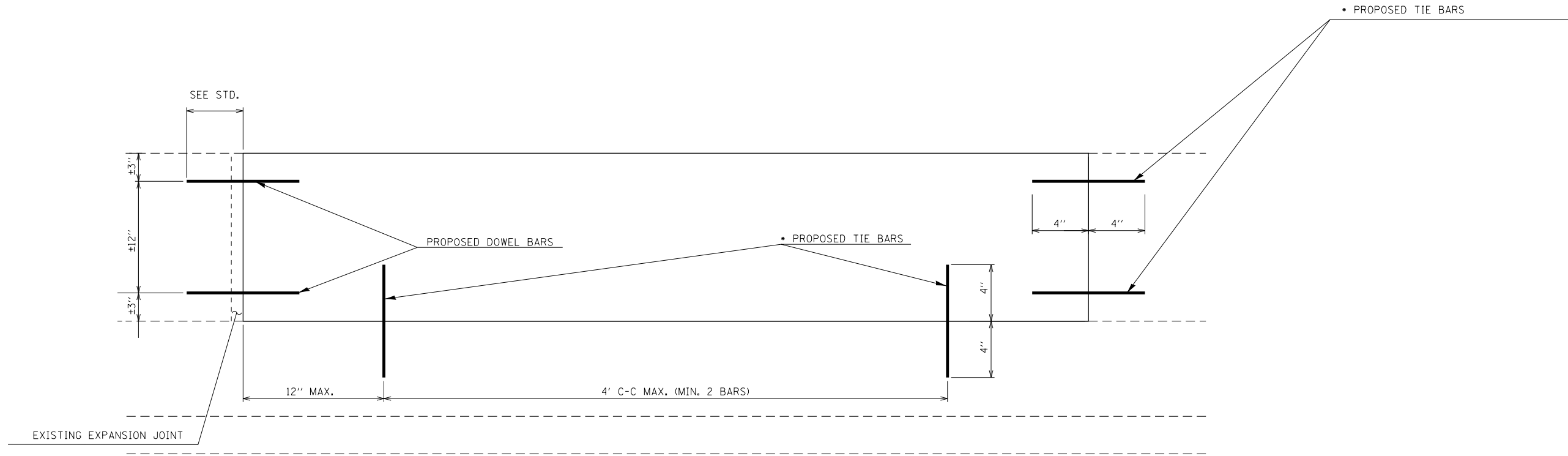
FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
c:\pwork\work\pwork\seymorecp\d0348982\0189013-shit-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL REPAIR FOR FULL DEPTH (BARRIER WALL)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W86	



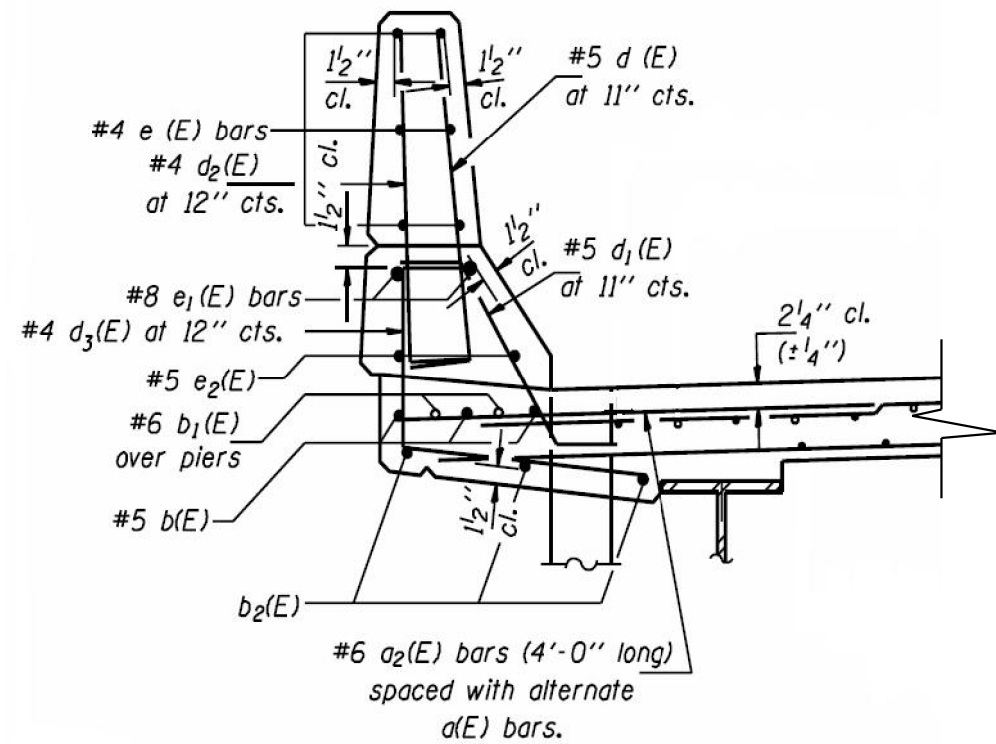
HALF DEPTH REPAIR

NOTE:

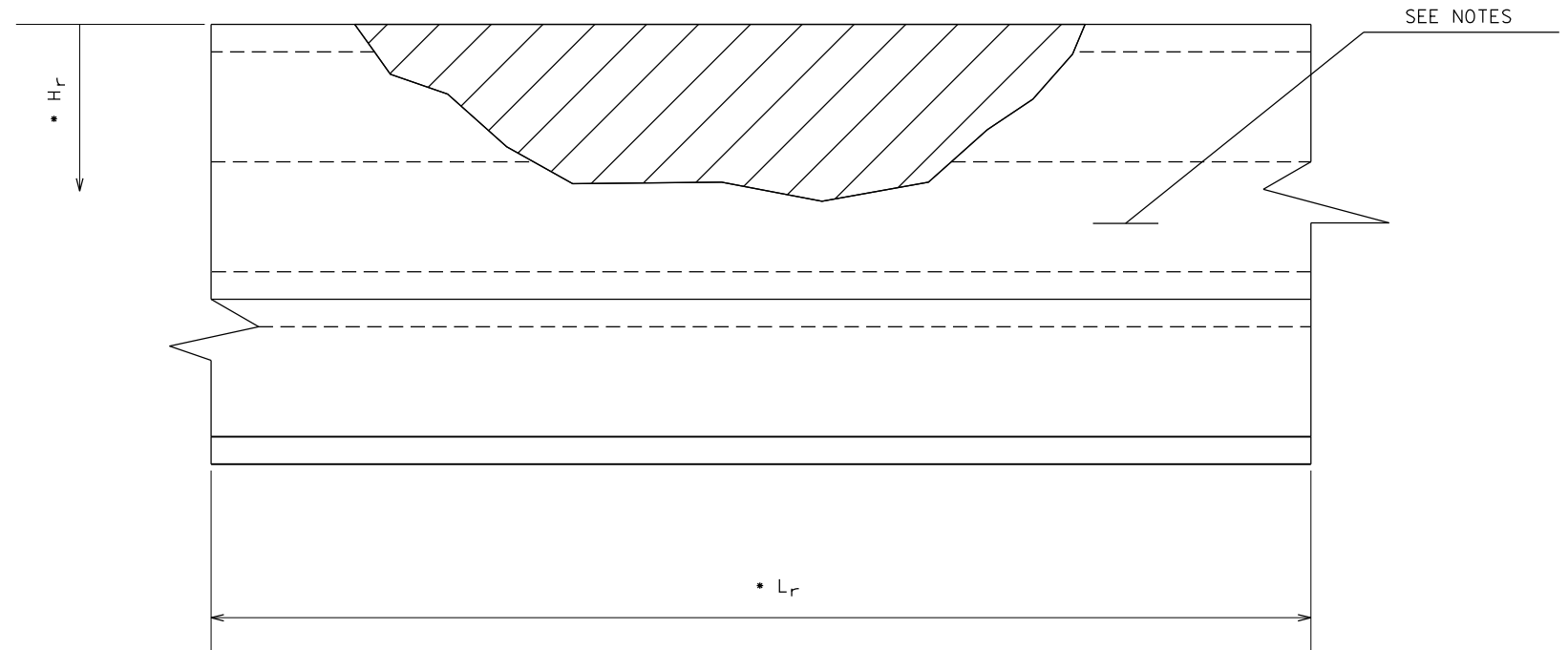
ALL BARRIER WALL REPAIR SHALL BE COMPLETED IN ACCORDANCE WITH STATE STANDARDS 637001 AND 637006 AND SECTION 637 OF THE STANDARD SPECIFICATIONS AND AS SHOWN ON THE PLANS. ALL PROPOSED DOWEL BARS AND TIE BARS SHALL BE DRILLED AND GROUTED IN PLACE. REINFORCEMENT SHALL BE INCLUDED AS SHOWN IN STANDARDS 637001 AND 637006 AND DESCRIBED IN SECTION 637 OF THE STANDARD SPECIFICATIONS EXCEPT WHEN A REINFORCEMENT CAGE IS ENCOUNTERED. IN SUCH A CASE, THE CONTRACTOR SHALL REMOVE THE DAMAGED PORTIONS OF THE CAGE AND SHALL REPLACE IT WITH BARS OF THE SAME SIZE AND SPACING. THE SCOPE OF REPAIR AT EACH LOCATION SHALL BE AS SHOWN ON THE PLANS AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

- USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL REPAIR FOR HALF DEPTH (BARRIER WALL)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Default	DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	6	
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 60W86					
	PLOT DATE = 7/11/2013	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
ILLINOIS FED. AID PROJECT											



SECTION THRU PARAPET



FRONT FACE OF PARAPET

NOTE:

CONCRETE REMOVAL SHALL EXTEND A MINIMUM OF 6" PAST DAMAGE OR TO JOINT IF WITHIN 2'-0".

EXISTING BARS TO BE CLEANED, STRAIGHTENED AND INCORPORATED IN NEW CONSTRUCTION.

IF EXISTING VERTICAL REBAR IS MISSING, BARS d(E) AND d₂(E) SHALL BE STRAIGHTENED AND EPOXY GROUTED INTO EXISTING CONCRETE 9" DEEP. COST SHALL BE INCLUDED IN "CONCRETE REMOVAL".

BARS TO BE EPOXY GROUTED ACCORDING TO ART. 584 OF THE STANDARD SPECIFICATIONS.

* H_r AND L_r INDICATE DIMENSIONS FOR CONCRETE REMOVAL. PERIMETERS OF CONCRETE REMOVAL AREAS SHALL BE SAW CUT 3/4" PRIOR TO REMOVAL OF CONCRETE.

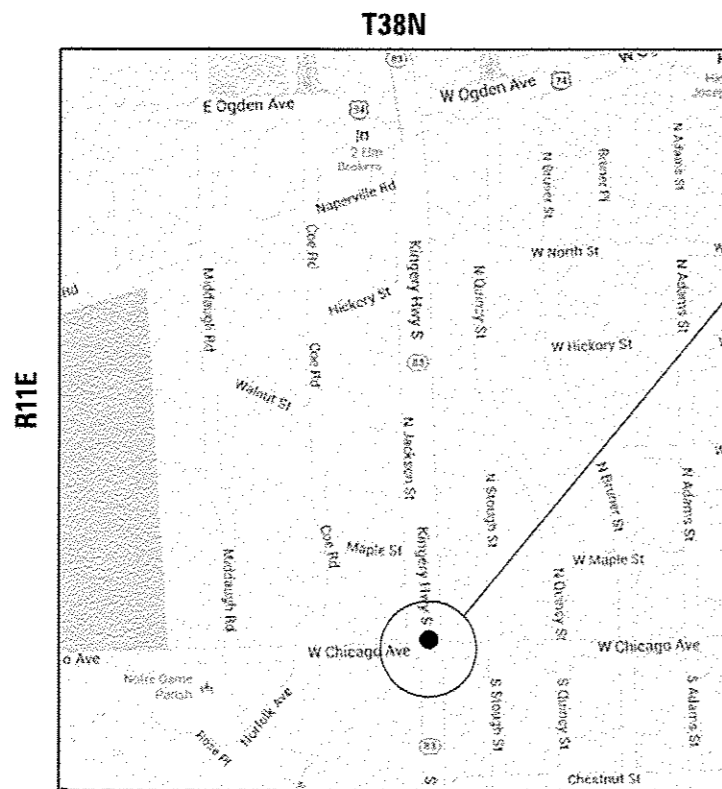
FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
ct:\pw\work\p1dot\seymorecp\d0348982\0189013-sh1-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL BRIDGE PARAPET REPAIR DETAILS

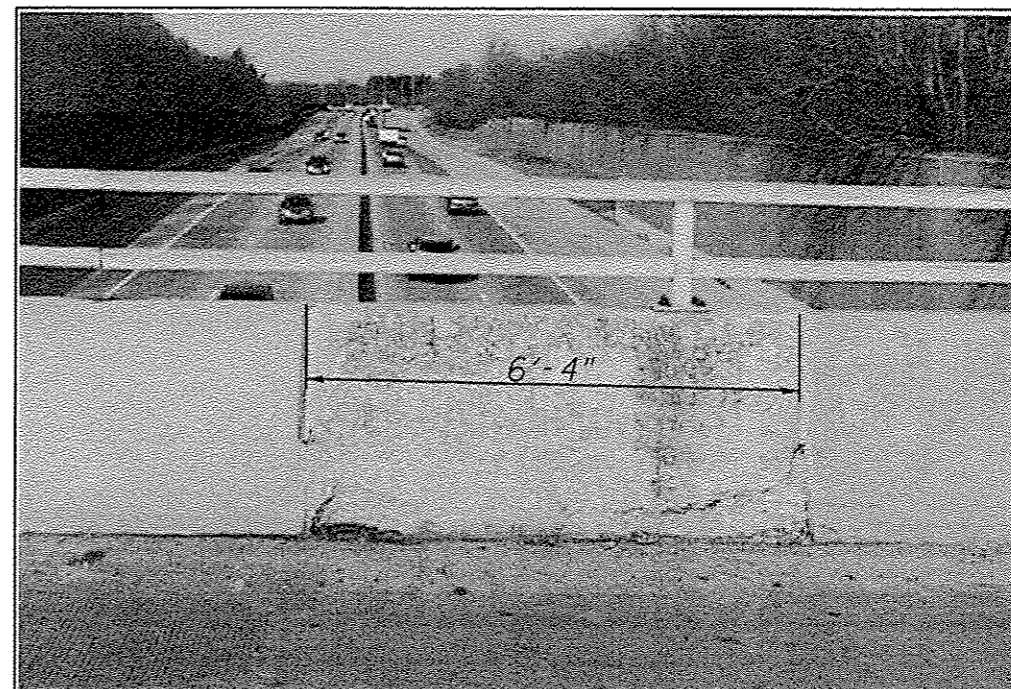
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	7
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				



LOCATION 1: CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY), WB
 5TH PANEL FROM THE WEST OF NORTH PARAPET
 (SN 022-0211)

**VILLAGE OF HINSDALE
 DOWNER'S GROVE TOWNSHIP
 DUPAGE COUNTY**



(022-0211) NORTH PARAPET REPAIR (LOOKING NORTH)
 FOR MORE DETAIL SEE SHEET #7

TRAFFIC DATA:

SPEED LIMIT: 30 MPH
 2012 ADT: 6350



David Carl Puzey 8/7/13
 Expires 11/30/14

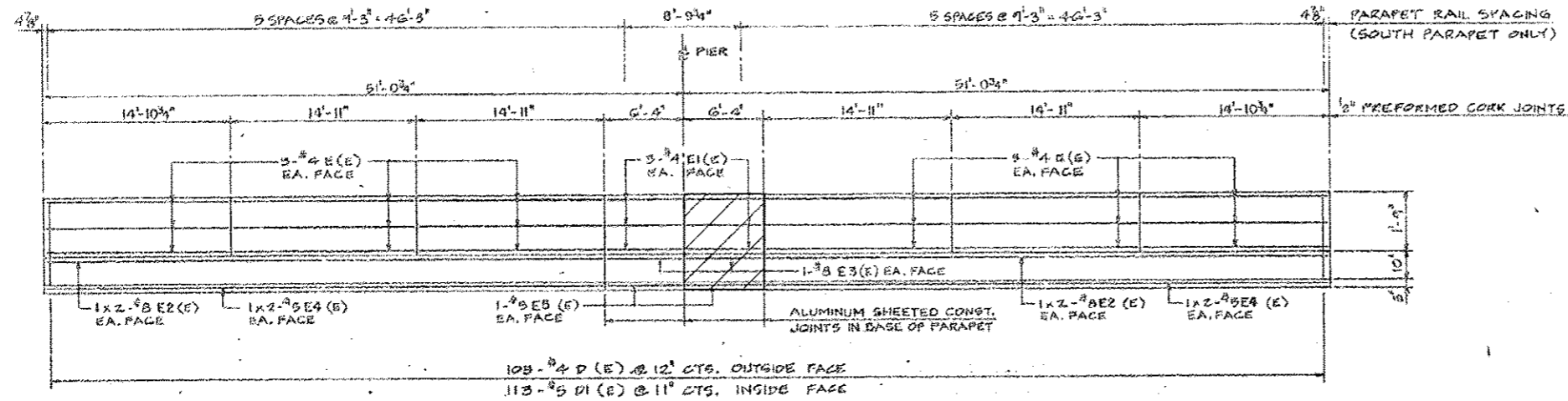
FILE NAME :	USER NAME : SEYMORECP	DESIGNED - <i>JAM</i>	REVISED -
c:\pwwork\pwwork\seymorecp\0346902\01	9013-ahs-plan.dgn	DRAWN -	REVISED -
Default	PLOT SCALE * 100.0000' / in.	CHECKED - <i>VAV</i>	REVISED -
	PLOT DATE * 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LOCATION 1 (SN 022-0211), NORTH PARAPET
 CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY)**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	8
CONTRACT NO. 60W86			ILLINOIS FED. AID PROJECT	



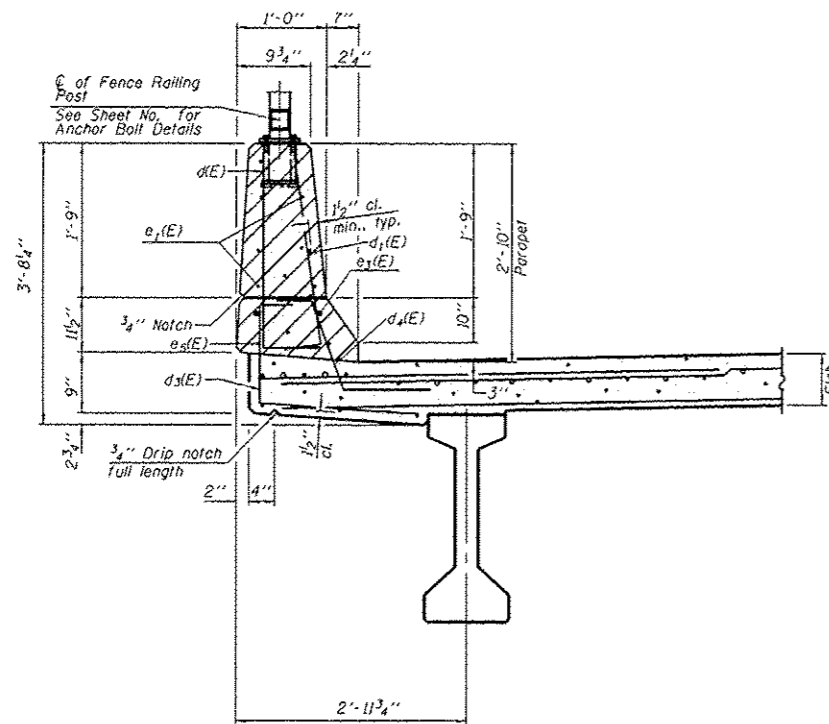
LEGEND

HATCHED AREA INDICATES CONCRETE REMOVAL

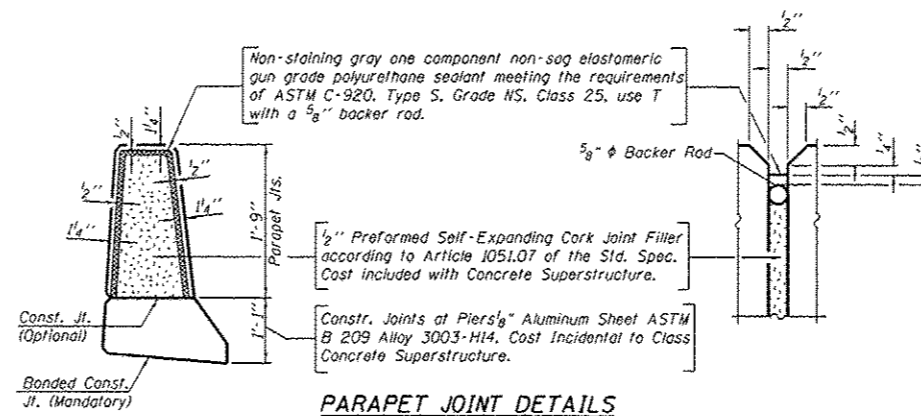
ELEVATION

**SUPERSTRUCTURE
BILL OF MATERIAL**

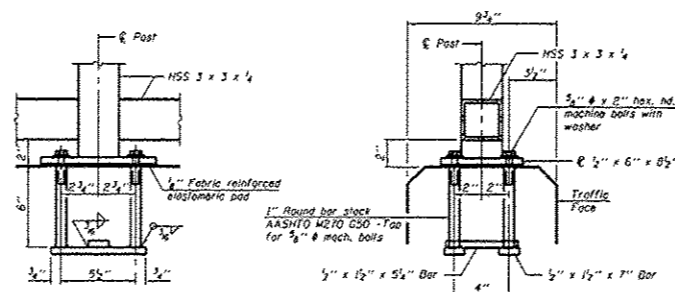
Bar	No.	Size	Length	Shape	
d1(E)	7	#4	3'-0"		
d1(E)	7	#5	3'-0"		
d1(E)	6	#4	6'-1"		
d3(E)	2	#8	6'-1"		
d5(E)	2	#5	6'-1"		
REINFORCEMENT BARS, EPOXY COATED				Pound	106
CONCRETE SUPERSTRUCTURE				Cu. Yds.	1.6
CONCRETE REMOVAL				Cu. Yds.	1.6
SAW CUTS				Foot	6
PROTECTIVE COAT				Sq. Yds.	6.08



EXISTING SECTION THRU PARAPET



PARAPET JOINT DETAILS



EXISTING ANCHOR BOLT DETAILS

NOTES:

- 1) IN LIEU OF THE CAST-IN-PLACE ANCHOR DEVICE SHOWN, THE CONTRACTOR HAS THE OPTION OF DRILLING AND SETTING 3/8" DIAMETER ANCHOR RODS ACCORDING TO ARTICLE 509.05 OF THE STANDARD SPECIFICATIONS. EMBEDMENT SHALL BE ACCORDING TO THE MANUFACTURER SPECIFICATIONS.
- 2) THREADED RODS, NUTS AND WASHERS SHALL CONFORM TO AASHTO M164.
- 3) NUTS FOR 3/8" DIAMETER THREADED ANCHOR RODS CONNECTING THE BASE PLATE TO THE CONCRETE TO BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL 1/8" TURN.
- 4) ALL STEEL ELEMENTS SHALL BE GALVANIZED ACCORDING TO ARTICLE 509.05 OF THE STANDARD SPECIFICATIONS.

**NORTH PARAPET DETAILS
STRUCTURE NO. 022-0211**

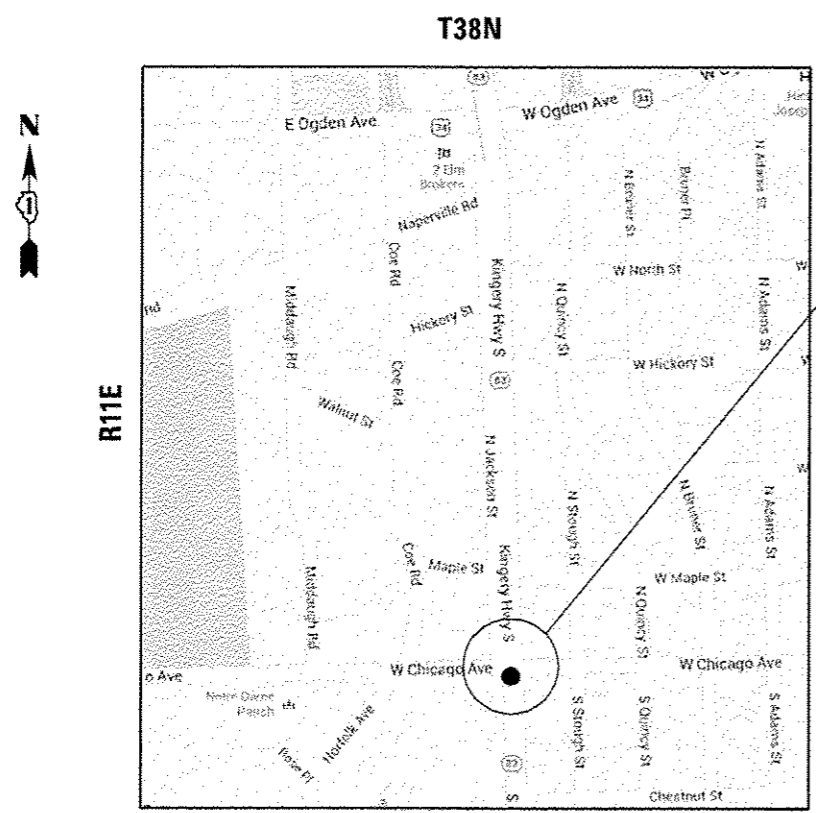
**BAR d1(E) &
BAR d3(E)**

BENT BAR DIAGRAM

NOTES:

- All edges shall have 3/4" chamfer.
- Bar length shall be verified in field prior to ordering the reinforcement.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 1 (SN 022-0211), NORTH PARAPET CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	9813-shr-plan.dgn	DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	9	
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -			SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.					
	PLOT DATE = 7/23/2013	DATE -	REVISED -			CONTRACT NO. 60W86 ILLINOIS FED. AID PROJECT					



LOCATION 2: CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY), EB
 2ND PANEL FROM THE WEST OF SOUTH PARAPET
 (SN 022-0211)

**VILLAGE OF HINSDALE
 DOWNER'S GROVE TOWNSHIP
 DUPAGE COUNTY**



(022-0211) SOUTH PARAPET REPAIR (LOOKING SOUTH)
 FOR MORE DETAIL SEE SHEET #7

TRAFFIC DATA:
 SPEED LIMIT: 30 MPH
 2012 ADT: 6350

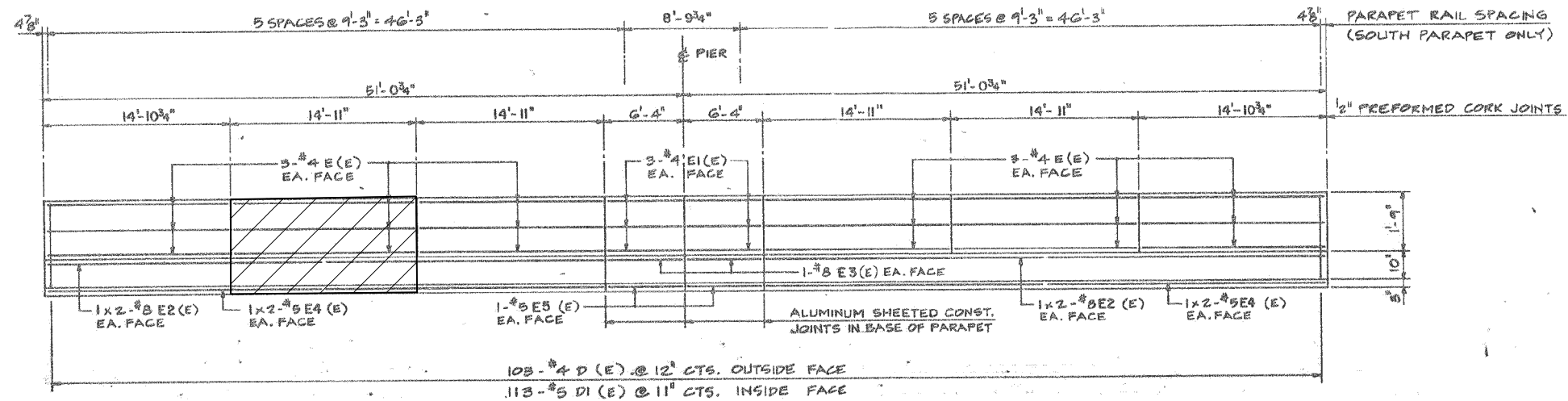


David Carl Puzey 8/7/13
 Expires 11/30/14

Notes:

WORK SHALL BE DONE AT NIGHT. SIDEWALK SHALL BE CLOSED PER 701801 BUT WILL REMAIN OPEN DURING THE DAY.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED - JAM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 2 (SN 022-0211), SOUTH PARAPET CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\p\work\p\idat\seymorecp\0348982\019013-shl-plan.dgn		DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	10	
Default	PLOT SCALE = 100.0000' / 1"	CHECKED - VHV	REVISED -			CONTRACT NO. 60W86					
	PLOT DATE = 7/11/2013	DATE -	REVISED -			SCALE:	SHEET 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



ELEVATION

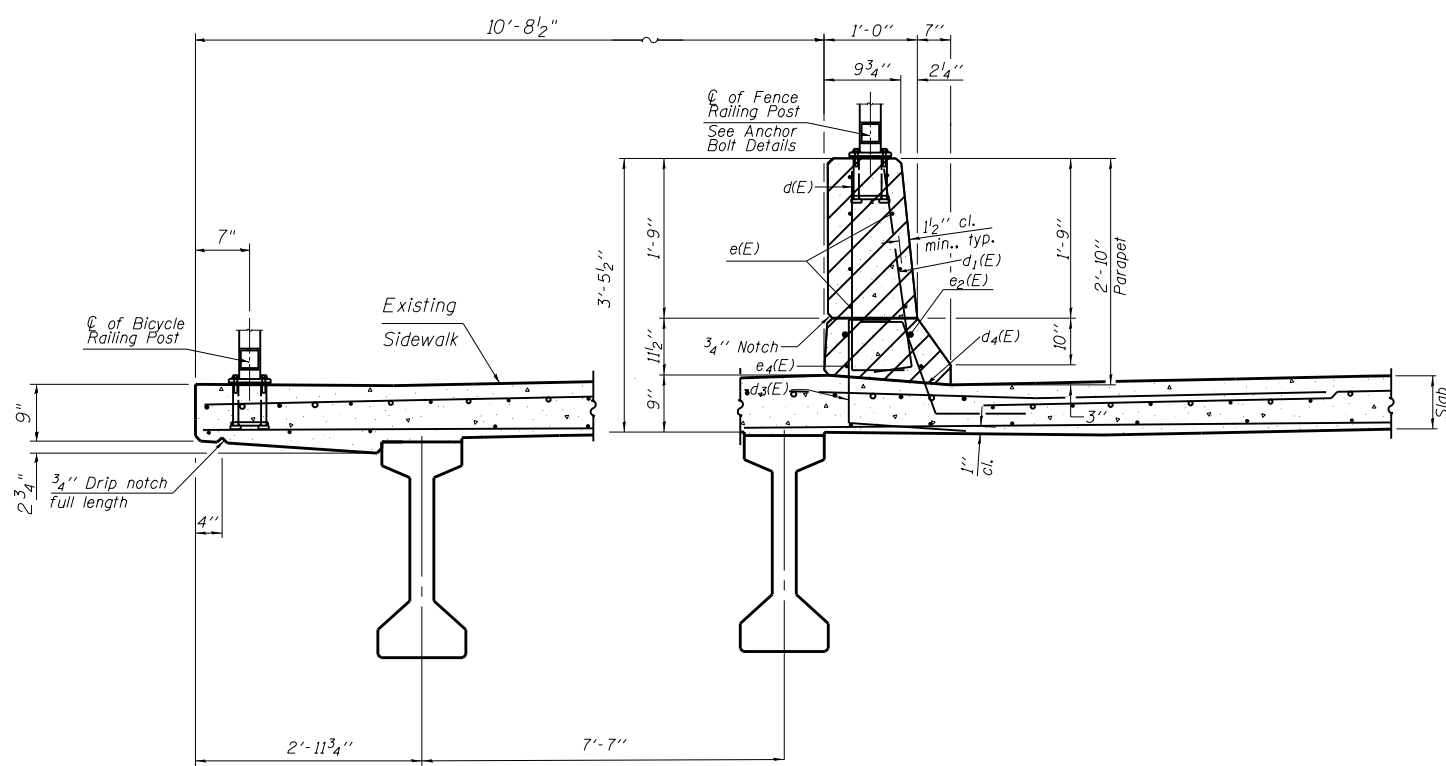
LEGEND



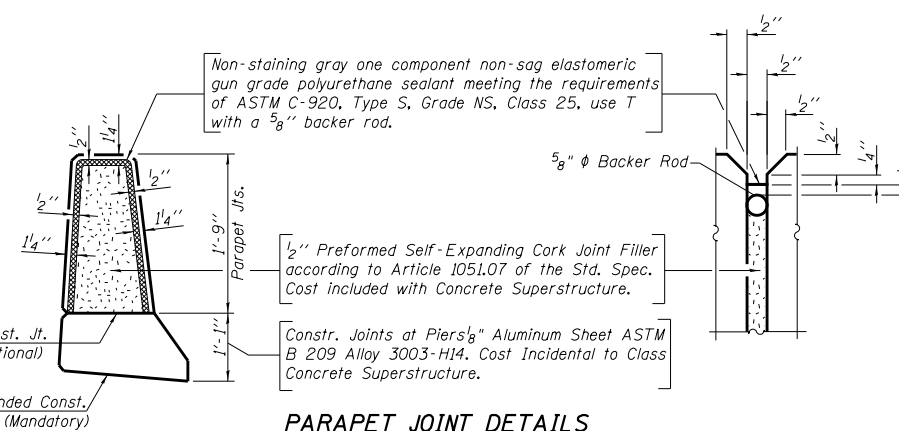
HATCHED AREA INDICATES CONCRETE REMOVAL

SUPERSTRUCTURE BILL OF MATERIAL

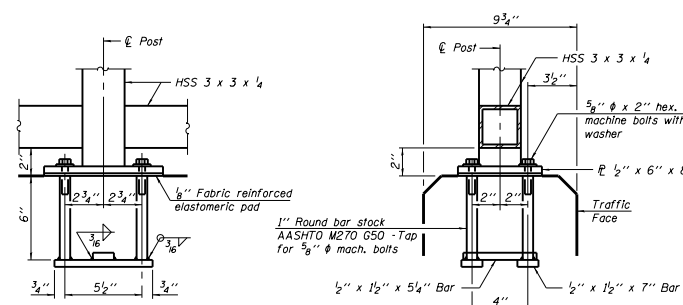
Bar	No.	Size	Length	Shape	
d(E)	15	#4	3'-0"	┌	
d1(E)	17	#5	3'-0"	└	
e(E)	6	#4	14'-7"	—	
e2(E)	2	#8	14'-7"	—	
e4(E)	2	#5	14'-7"	—	
REINFORCEMENT BARS, EPOXY COATED				Pound	250
CONCRETE SUPERSTRUCTURE				Cu. Yds.	1.3
CONCRETE REMOVAL				Cu. Yds.	1.3
SAW CUTS				Foot	6
PROTECTIVE COAT				Sq. Yds.	4.9



EXISTING SECTION THRU PARAPET



PARAPET JOINT DETAILS



EXISTING ANCHOR BOLT DETAILS

NOTES:

- 1) IN LIEU OF THE CAST-IN-PLACE ANCHOR DEVICE SHOWN, THE CONTRACTOR HAS THE OPTION OF DRILLING AND SETTING 5/8" DIAMETER ANCHOR RODS ACCORDING TO ARTICLE 509.06 OF THE STANDARD SPECIFICATIONS. EMBEDMENT SHALL BE ACCORDING TO THE MANUFACTURER SPECIFICATIONS.
- 2) THREADED RODS, NUTS AND WASHERS SHALL CONFORM TO AASHTO M164.
- 3) NUTS FOR 5/8" DIAMETER THREADED ANCHOR RODS CONNECTING THE BASE PLATE TO THE CONCRETE TO BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL 1/8" TURN.
- 4) ALL STEEL ELEMENTS SHALL BE GALVANIZED ACCORDING TO ARTICLE 509.05 OF THE STANDARD SPECIFICATIONS.

**SOUTH PARAPET DETAILS
STRUCTURE NO. 022-0211**

**BAR d(E) &
BAR d1(E)**

BENT BAR DIAGRAM

NOTE:

- All edges shall have 3/4" chamfer.
- Bar length shall be verified in field prior to ordering the reinforcement.
- Work at this location should be done at night due to presence of sidewalk.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 2 (SN 022-0211), SOUTH PARAPET SOUTH PARAPET, CHICAGO AVENUE AT IL 83 (KINGERY HIGHWAY)	F.A. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT DATE = 7/11/2013	DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	11	
		CHECKED -	REVISED -			CONTRACT NO. 60W86					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



LOCATION 3: IL 394 (CALUMET EXPRESSWAY/BISHOP FORD FREEWAY), NB

1800 FEET SOUTH OF THE IL 1 (DIXIE HIGHWAY)/
IL 394 (CALUMET EXPRESSWAY/ BISHOP FORD FREEWAY)
JUNCTION

TRAFFIC DATA:

SPEED LIMIT: 65 MPH
2012 ADT: 75600

PANEL SCHEDULE (TOP)

MARK	PANEL HEIGHT	PANEL WIDTH	QUANTITY	AREA EACH(S.F)	AREA TOTAL(S.F)
T-1	10'-0"	20'-4"	1	203.3	203.3

PANEL SCHEDULE (BOTTOM)

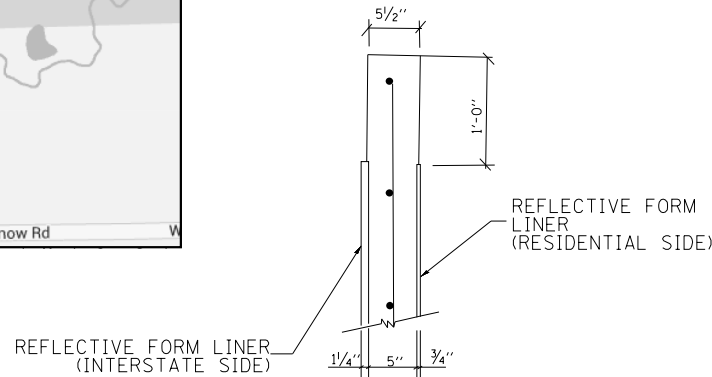
B-4D	6'-11 ⁷ / ₈ "	20'-4"	1	142.2	142.2
------	-------------------------------------	--------	---	-------	-------

BILL OF MATERIAL

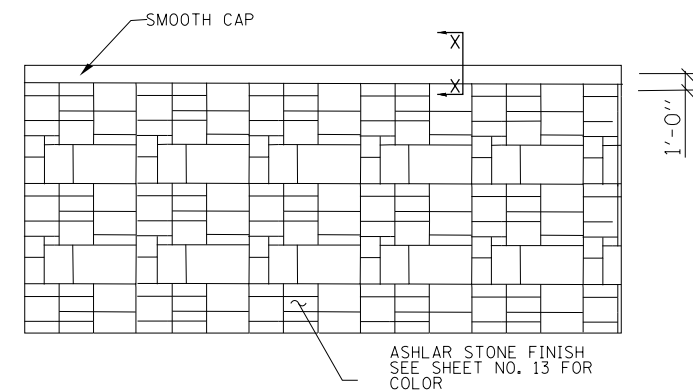
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	2.4
NOISE ABATEMENT WALL REPAIRS	Lump Sum	1
TEMPORARY EROSION CONTROL BLANKET	Sq. Yds.	9.0
TOPSOIL FURNISH AND PLACE, 4"	Sq. Yds.	9.0
SEEDING CLASS 2A	Acre	0.0019

AREA OF REPAIR= 142.2 SQ FT

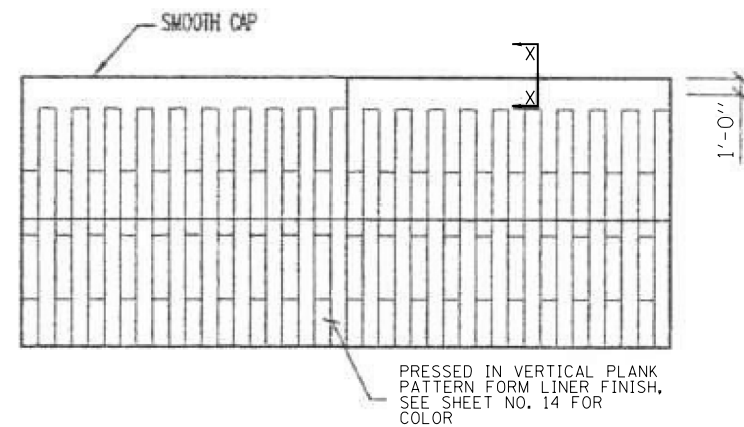
**VILLAGE OF CRETE
CRETE TOWNSHIP
WILL COUNTY**



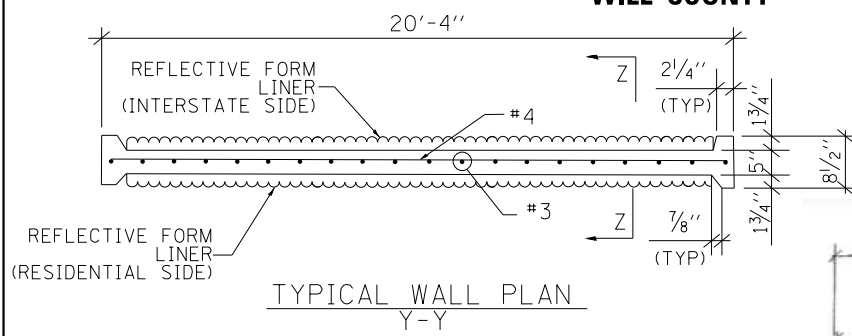
PANEL SECTION W/CAP & FINISH X-X



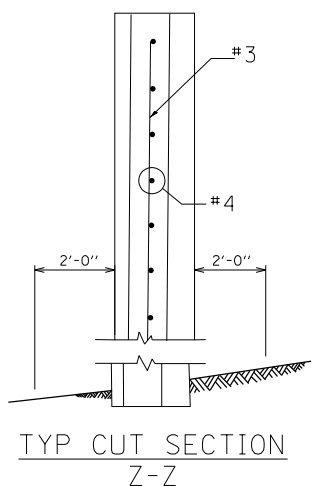
TYPICAL PANEL DETAIL W/ FINISH INTERSTATE SIDE



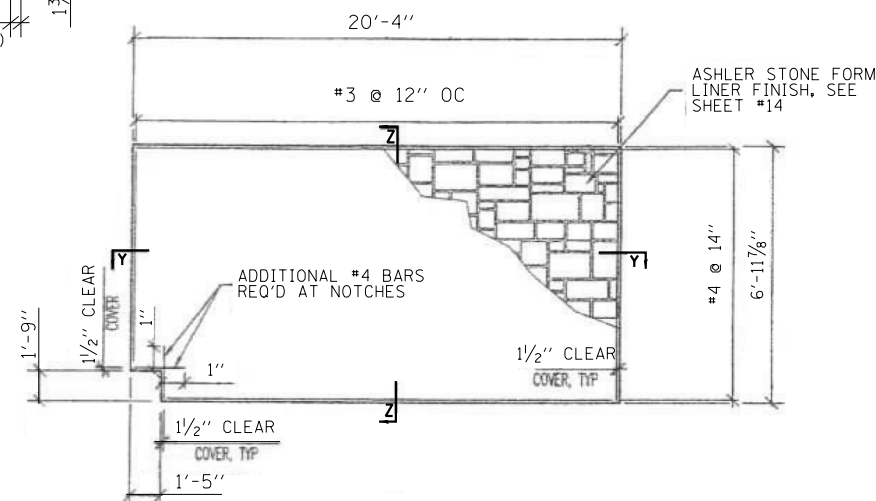
TYPICAL PANEL DETAIL W/ FINISH RESIDENTIAL SIDE



TYPICAL WALL PLAN Y-Y

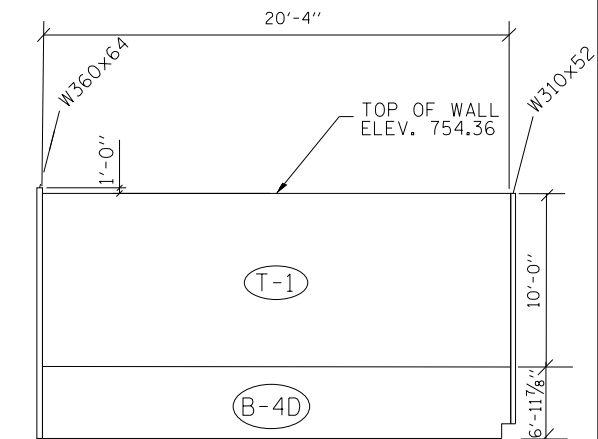


TYP CUT SECTION Z-Z



TYPICAL BOTTOM WALL PANEL REINFORCEMENT DETAIL (W/ NOTCH)

NOTE: NOTCH LOCATED AT THE NORTH END OF THE PANEL.



TYPICAL PANEL DETAIL (LOOKING AT RESIDENTIAL SIDE)

NOTE:
LIFTING AND REPLACEMENT OF TOP PANEL SHALL BE INCLUDED IN THE COST OF "CONCRETE REMOVAL".

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 3 (NOISE WALL) IL 394 (CALUMET EXPRESSWAY/BISHOP FORD FREEWAY)	F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	et:\pw\work\p\midot\seymorecp\d0348982\0189013-shr-plan.dgn	DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	12	
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 60W86					
	PLOT DATE = 7/11/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

GENERAL NOTES

- FIELD TOLERANCE IS IN ACCORDANCE WITH "THE ERECTOR'S MANUAL - STANDARDS & GUIDELINES FOR THE ERECTION OF PRECAST CONCRETE PRODUCTS" PER SECTION 8.2 OF THE PCI MANUAL (CURRENT EDITION).
- ERECTION TOLERANCES IN ACCORDANCE WITH "PCI RECOMMENDED PRACTICE FOR ERECTION OF PRECAST CONCRETE" & SECTION 8.3 OF THE PCI MANUAL (CURRENT EDITION).
- ITEMS PROVIDED BY THE INSTALLATION CONTRACTOR:
 - SHIMS
 - GROUT
 - STRUCTURAL CAULK AND BACKER RODS
 - NON-STRUCTURAL CAULK
 - ANCHOR BOLTS
 - ANCHOR BOLT NUTS
 - ANCHOR BOLT WISHERS
- ITEMS PROVIDED BY PRECAST SUPPLIER:
 - STEEL POSTS
 - PRECAST PANELS
- STAIN SHALL BE A NON-FILM FORMING, PENETRATING CONCRETE STAIN (OR COMPARABLE) APPLIED IN SHOP PER MANUFACTURER'S RECOMMENDATIONS BY SUPPLIER AND PROJECT SPECIFICATIONS. COLOR: FEDERAL STANDARD 595-B. TOUCH UP IN FIELD WITH SAME MATERIAL AS NECESSARY.
- TEST CYLINDERS SHALL BE PREPARED FOR PANELS FORMED PER DOT SPECIFICATIONS.
- REPAIR OF PANELS IS PER MANUFACTURER'S SPECIFICATIONS.
- PANEL CAP SHALL BE SMOOTH FINISH. PANEL WILL BE ASHLAR STONE FORM LINER FINISH ON SIDE FACING I. RTE 394 AND A PRESSED-IN VERTICAL PLANK FORM LINER PATTERN ON SIDE FACING RESIDENTIAL PER THE SPECIFICATIONS. (SEE PANEL DETAILS)
- THE MINIMUM REINFORCEMENT BAR COVER SHALL BE 38 (1 1/2).
- ALL REINFORCEMENT SHALL BE EPOXY COATED.
- PANEL DIMENSIONS SHALL BE WITHIN 6 (1/4).
- ANGULAR DISTORTION WITH REGARD TO PANEL SQUARENESS, DEFINED AS THE DIFFERENCE BETWEEN THE TWO DIAGONALS, SHALL NOT EXCEED 13 (1/2).
- SURFACE DEFECTS ON FORMED SURFACES MEASURED ON A LENGTH OF 1.52m (5'-0") SHALL NOT BE MORE THAN 3 (0.10) PRIOR TO LOADING PANELS ON TRUCK FOR DELIVERY.
- POSTS SHALL BE INSTALLED PLUMB TO WITHIN 13 (1/2) OF VERTICAL FOR EVERY 4.57m (15'-0") OF HEIGHT AND TO WITHIN 13 (1/2) OF THE STATION AND OFFSET INDICATED.
- DRILLED SHAFT FOUNDATIONS SHALL BE PLACED WITHIN 51 (2) OF THE STATION AND OFFSET INDICATED.
- ALL LIFTING INSERTS CAST INTO THE PANELS SHALL BE HOT DIPPED GALVANIZED & SHALL BE INSTALLED INTO THE TOP EDGE OF EACH PANEL AT QUARTER POINTS AS SHOWN IN STEP (3) OF DETAIL 1/52.00.

MATERIAL SPECIFICATIONS (PER DOT SPECIFICATIONS)

- PRECAST CONCRETE (NOISE ABATEMENT WALL, GROUND MOUNTED) CLASS PC
 - $f_c = 41.4 \text{ MPa (6,000 PSI)} @ 28 \text{ DAYS}$
 - DENSITY = $2403 \text{ kg/m}^3 (150 \text{ PCF})$
- GROUT (BY OTHERS)
 - GROUT STRENGTH: $41,369 \text{ kPa} @ 28 \text{ DAYS}$
 - $27,580 \text{ kPa} @ 3 \text{ DAYS}$

(DO NOT ALLOW GROUT TO FREEZE PRIOR TO REACHING 34,474 kPa)
- CONCRETE AND STEEL COLORS PER FEDERAL STANDARD 595-B COLOR COLOR OF NOISE ABATEMENT WALL SYSTEM = COLOR #30340
- CAISSON CONCRETE: CLASS DS, $f_c = 27.5 \text{ MPa} @ 14 \text{ DAYS}$
- WELDED WIRE FABRIC: ASTM A497 (ASHTO M55), $F_y = 483 \text{ MPa MIN. EPOXY COATED}$
- STEEL POSTS: ASHTO M270 GRADE 345 (GRADE 50), $F_y = 345 \text{ MPa}$
- LIFTING INSERTS: UNIVERSAL BUILDING PRODUCTS DFFA58712G 51x7 1/8" DROP FORGED FOOT ANCHOR (RMC LIFT SYSTEM) OR EQUAL.
- SHIMS: VERSA-A-SHIM HIGH IMPACT PLASTIC SHIMS PER ASTM D792 & ASTM D895
- STRUCTURAL CAULK: SIKADUR 51 NS FLEXIBLE EPOXY CONTROL-JOINT SEALER/ADHESIVE OR EQUAL
- BACKER ROD: MILE HIGH FOAM PRODUCT SIZED PER BACKER ROD MANUFACTURING, INC. OR EQUAL
- NON-STRUCTURAL CAULK SEALANT: AC-20 FTR PER PCORNA CORPORATION SPECIFICATIONS AND MANUFACTURING STANDARDS OR EQUAL
- REBAR (CONCRETE REINFORCEMENT): ASTM A706M, GRADE 420

*IF USED IN PLACE OF #10 TEMP/SHRINKAGE EPOXY COATED REINFORCEMENT
 **BASE PLATES AND STEEL POSTS SHALL BE GALVANIZED ACCORDING TO ASTM A305 AND ASHTO M111

INSTALLATION

DRILLED CAISSON SHAFT INSTALLATION

- AUGER HOLE TO REQUIRED DEPTHS. SEE PRECAST SHOP DRAWINGS FOR PIER LOCATIONS. DENAYER AS NECESSARY.
- PLACE REBAR PER DRILLED PIER/CAISSON DETAILS.
- PLACE $f_c = 27,580 \text{ MPa}$ CONCRETE IN AUGURED HOLE TO THE DEPTH AND ELEVATIONS SPECIFIED ON PLAN AND ELEVATION SHEETS AND FOOTING SCHEDULE OF THESE PRECAST SHOP DRAWINGS.
- DRILLED CAISSON FOUNDATIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ARTICLE 516.02 OF THE DOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- IF SOIL IS UNSATURABLE, A CORRUGATED LINER OR SONOTUBE SHALL BE INSTALLED PRIOR TO PLACEMENT OF CONCRETE.

PANEL INSTALLATION (NOISE ABATEMENT WALL, GROUND MOUNTED)

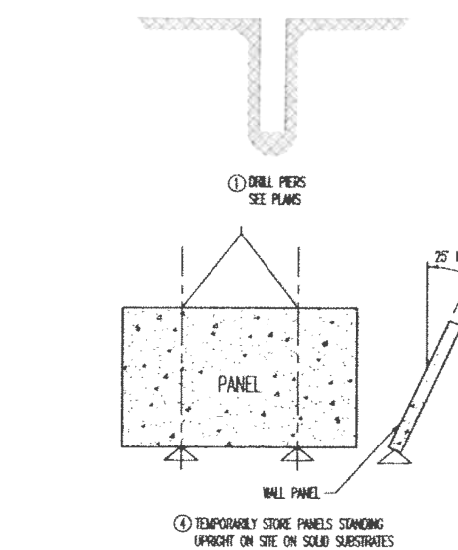
- PRESET STEEL POSTS TO THE NECESSARY ELEVATION TO OBTAIN TOP OF ELEVATIONS SPECIFIED ON THE PLAN AND ELEVATION DRAWINGS. SEE 344/52.00 FOR SHM DETAILS.
- ERECT PRECAST PANEL ON THE SHIMS AND INTO SLOT PROVIDED IN STEEL POST BETWEEN PANELS AND CLIP ANGLES. SEE COLUMN DETAILS ON SHEET 54.00
- FULLY CAULK JOINTS BETWEEN PANELS ALONG CENTERLINE OF PANEL WIDTH SO THAT JOINT FILLER IS NOT VISIBLE TO HINDER THE AESTHETIC APPEARANCE OF THE WALL. CLEAN OUT ANY JOINT FILLER THAT PROTRUDES BEYOND STRUCTURAL WIDTH OF PANEL.
- RELEASE PANEL FROM THE CRANE.

DRILLED CAISSONS

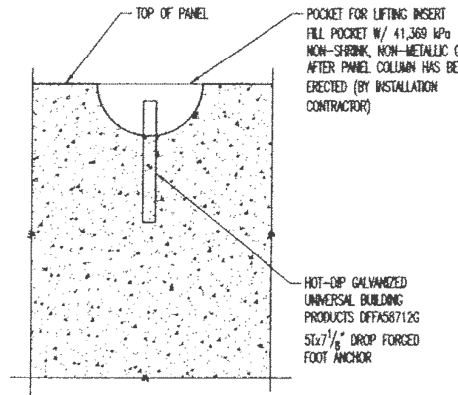
- THIS ITEM SHALL CONSIST OF CONSTRUCTING CONCRETE SHAFT FOUNDATIONS CAST IN DRILLED HOLES IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS, DETAILS SHOWN ON THE PLANS AND FURTHER DESCRIBED HEREIN.
- CAISSON DESIGN IS BASED UPON SOIL TYPES AND PROPERTIES PROVIDED ON SHEETS 4 THRU 10 OF THE NOISE ABATEMENT WALL PLANS (136 THRU 145 OF THE COMPLETE DESIGN DRAWINGS). IF SOIL CONDITIONS IN FIELD DEVIATE FROM THOSE INDICATED IN THE DESIGN DRAWINGS, THE RESIDENT ENGINEER SHALL CONTACT LARSON ENGINEERING, INC PRIOR TO CONTRACTOR COMPLETING THE CAISSON.
- THE DRILLING OF THE HOLES SHALL BE ACCOMPLISHED BY MEANS OF EITHER TRUCK MOUNTED OR CRANE MOUNTED EARTH AUGERS. THE DRILL UNIT USED SHALL BE SUCH THAT THE SHAFTS CAN BE EXCAVATED TO THE DIAMETERS AND ALIGNMENT REQUIRED BY THE PLANS. THE DRILL UNIT SHALL HAVE ADEQUATE CAPACITY TO CREATE A SHAFT EXCAVATION TO A DEPTH OF 20 PERCENT BEYOND THE DEPTHS INDICATED ON THE PLANS.
- IF BOULDERS OR MASONRY FOUNDATIONS SHOULD BE ENCOUNTERED BELOW THE NATURAL GROUND DURING DRILLING, THE HOLES SHOULD CONTINUE THROUGH THESE OBSTRUCTIONS. ALL LOOSE MATERIAL EXISTING AT THE BOTTOM OF THE HOLE AFTER DRILLING OPERATIONS AND EXCAVATION SHALL BE REMOVED BEFORE PLACING CONCRETE IN THE HOLES.
- ALL EXCAVATIONS SHALL BE INSPECTED PRIOR TO CONCRETE PLACEMENT BY AN OWNER'S REPRESENTATIVE TO VERIFY SUITABLE BEARING MATERIAL OF CAPACITY AS SPECIFIED IN THE DESIGN DRAWINGS. THE CONTRACTOR SHALL HAVE AVAILABLE A SUITABLE LIGHT FOR THE INSPECTION OF THE DRILLED HOLES FOR ITS ENTIRE DEPTH. ALL HOLES SHALL BE EXAMINED FOR STRAIGHTNESS. THE ALIGNMENT OF THE SHAFT SHALL NOT VARY FROM VERTICAL BY MORE THAN 1.5X.
- NOTIFY THE OWNER'S REPRESENTATIVE AND LARSON ENGINEERING, INC WHEN ADDITIONAL EXCAVATION IS REQUIRED TO MEET SUITABLE BEARING MATERIAL.
- SURFACE WATER SHALL NOT BE PERMITTED TO ENTER THE HOLE AND ALL WATER WHICH MAY HAVE INFILTRATED INTO THE HOLE SHALL BE REMOVED PRIOR TO PLACING CONCRETE THEREIN. A DRAINAGE SYSTEM OF SUFFICIENT CAPACITY SHALL BE INSTALLED AND OPERATED TO MAINTAIN THE CONSTRUCTION AREA FREE OF WATER AT ALL TIMES. IF DRAINING OF THE HOLE CANNOT BE READILY ACCOMPLISHED WITHOUT LOSS OF GROUND OR CREATING QUICK CONDITIONS, THEN THE HOLE SHALL BE LINED AND TRENCHED CONCRETE SHALL BE USED.
- IF UNSUITABLE SOILS ARE FOUND, IN ORDER TO PREVENT CAVING OF THE HOLE BEFORE CONCRETE IS PLACED THEREIN, DRILLED CAISSON SHAFTS SHALL BE SLEEVED TO A DEPTH OF AT LEAST 10' BELOW TOP OF FINISHED GRADE TO PREVENT CREEP.
- THE METHOD OF PLACING CONCRETE IN THE SHAFTS SHALL BE PRE-APPROVED BY THE RESIDENT ENGINEER. THE METHOD USED SHALL BE ONE THAT WILL PROVIDE A CONTINUOUS FLOW WITH NO SEGREGATION OF THE CONCRETE MATERIALS.

ELECTRICAL

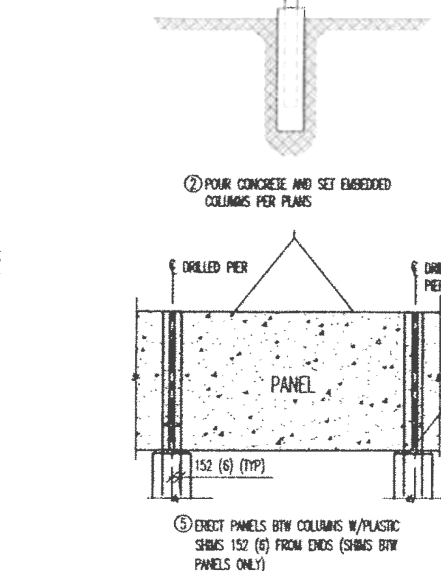
- WHEN WORKING IN THE VICINITY OF COMED'S ELECTRIC TRANSMISSION LINES DURING THE INSTALLATION, OSHA REQUIRES THAT A MIN. 6.10m (20'-0") WORKING CLEARANCE DISTANCE MUST BE MAINTAINED BETWEEN THE BOOMS, ARMS OR OTHER PARTS THAT CAN BE RAISED ON THE EQUIPMENT FOR THE PETITIONER'S CONTRACTOR AND COMED'S EXISTING SOUTHERLY 345,000 VOLT ELECTRIC TRANSMISSION CONDUCTORS. OSHA REQUIRES A MINIMUM OF 4.27m (14'-0") WORKING CLEARANCE DISTANCE MUST BE MAINTAINED BETWEEN THE BOOMS, ARMS OR OTHER PARTS THAT CAN BE RAISED ON THE EQUIPMENT FOR THE PETITIONER'S CONTRACTOR AND COMED'S EXISTING NORTHERLY 138,000 VOLT ELECTRIC TRANSMISSION CONDUCTORS. UNDER NO CIRCUMSTANCES, SHOULD TRUCK BEDS BE RAISED UNDERNEATH COMED TRANSMISSION LINES.



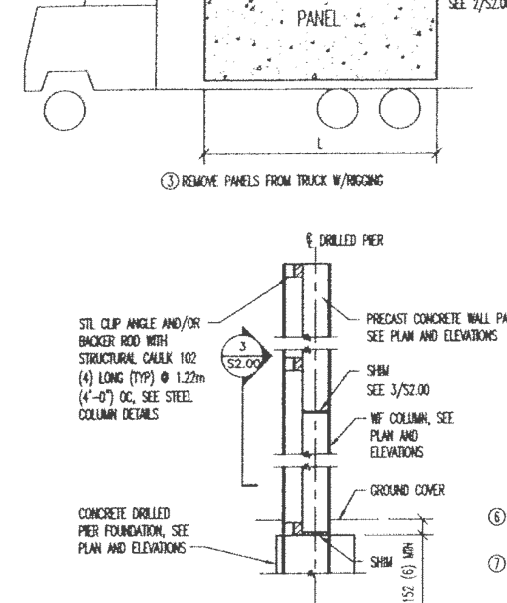
1 TYPICAL NOISE ABATEMENT WALL INSTALLATION SEQUENCE & PROCEDURES
 \$2.00 N.T.S.



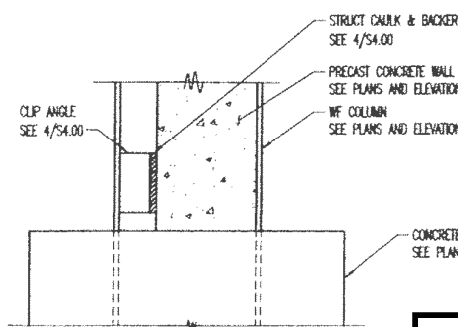
2 TYPICAL LIFTING INSERT DETAIL
 \$2.00 N.T.S.



3 HORIZONTAL JOINT DETAIL
 \$2.00 N.T.S.



4 PANEL CONNECTION TO COLUMN DETAIL
 \$2.00 N.T.S.

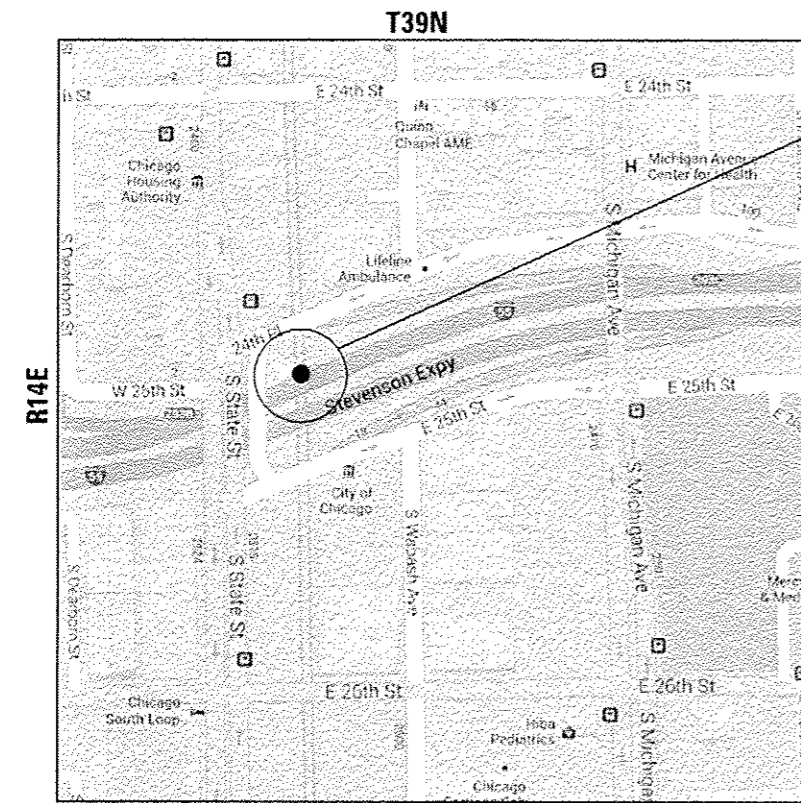


5 CLIP ANGLE DETAIL
 \$2.00 N.T.S.

NOTE:
 1) THESE DRAWINGS HAVE BEEN PREPARED FOR THE FABRICATION OF THE PRECAST NOISE ABATEMENT WALLS AND PROVIDE GENERAL INSTALLATION SEQUENCE AND PROCEDURES FOR THE CONTRACTOR. THE CONTRACTOR SHALL RETAIN SOLE RESPONSIBILITY FOR THE MEANS, METHODS, AND TECHNIQUES OF CONSTRUCTION OF THE NOISE ABATEMENT WALLS FOR COMPLIANCE WITH LAWS, REGULATIONS, AND CODES, AND FOR THE SAFETY OF CONSTRUCTION APPLICABLE TO THIS WORK.
 2) THIS IS A METRIC PROJECT.
 3) DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS NOTED OTHERWISE.

FOR INFORMATION ONLY

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CONCRETE NOISE ABATEMENT WALL REPAIR DETAILS AND NOTES (IL 394)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	DRAWN -	REVISED -					VAR.	2013-0411	VARIOUS	44	14
		CHECKED -	REVISED -					CONTRACT NO. 60W86				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

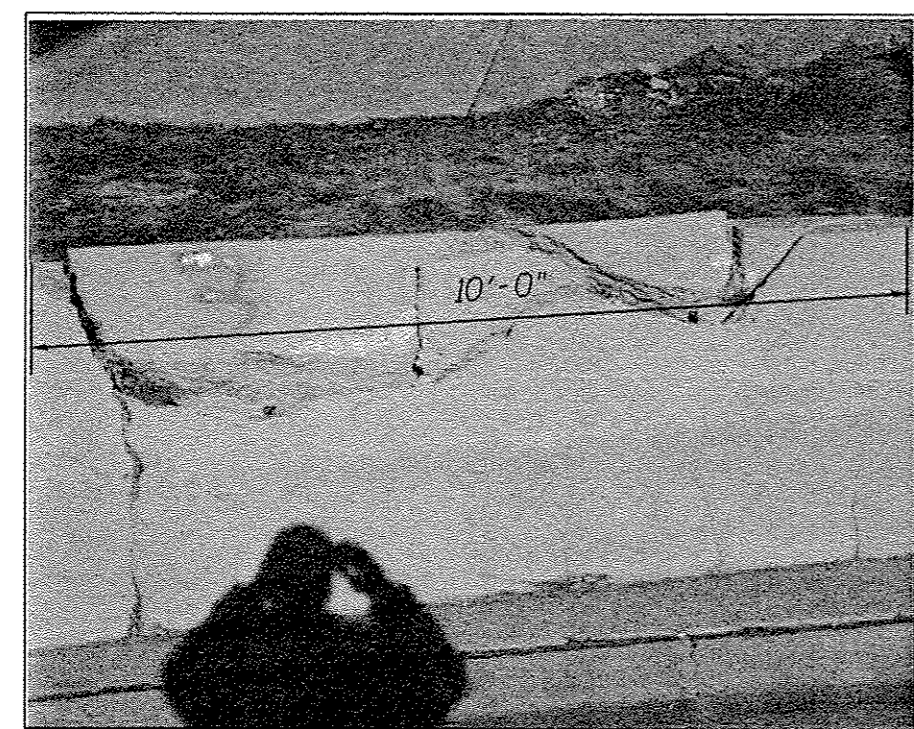
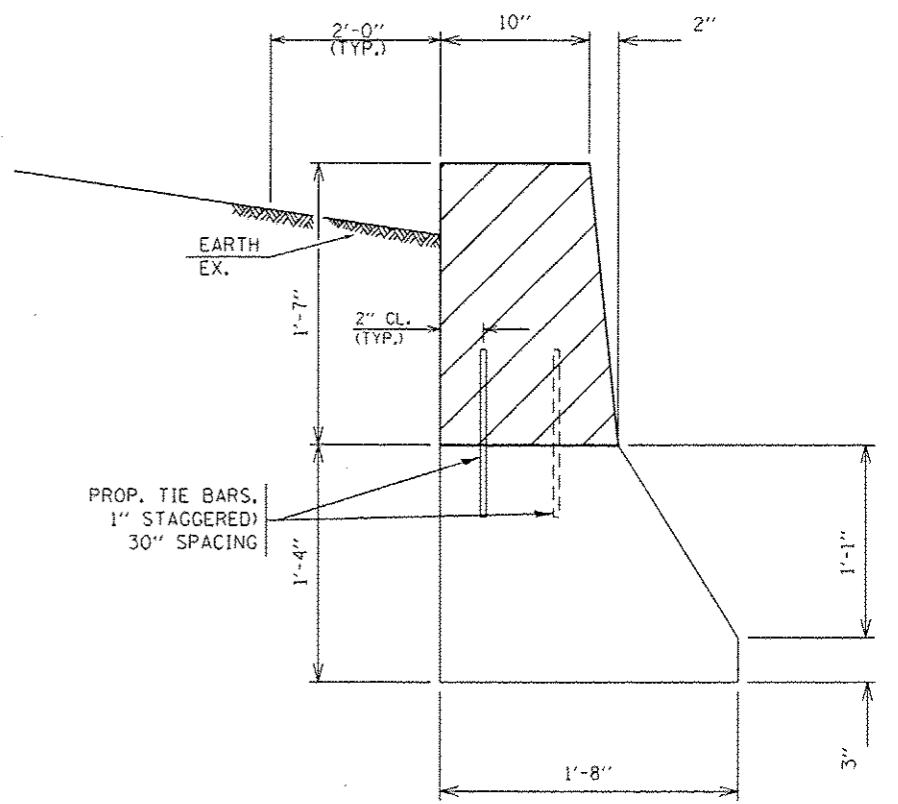


**CITY OF CHICAGO
SOUTH CHICAGO TOWNSHIP
COOK COUNTY**

**LOCATION 4: I-55 (STEVENSON EXPRESSWAY), SWB
187' EAST OF STATE ST. OVERPASS, RIGHT SIDE**

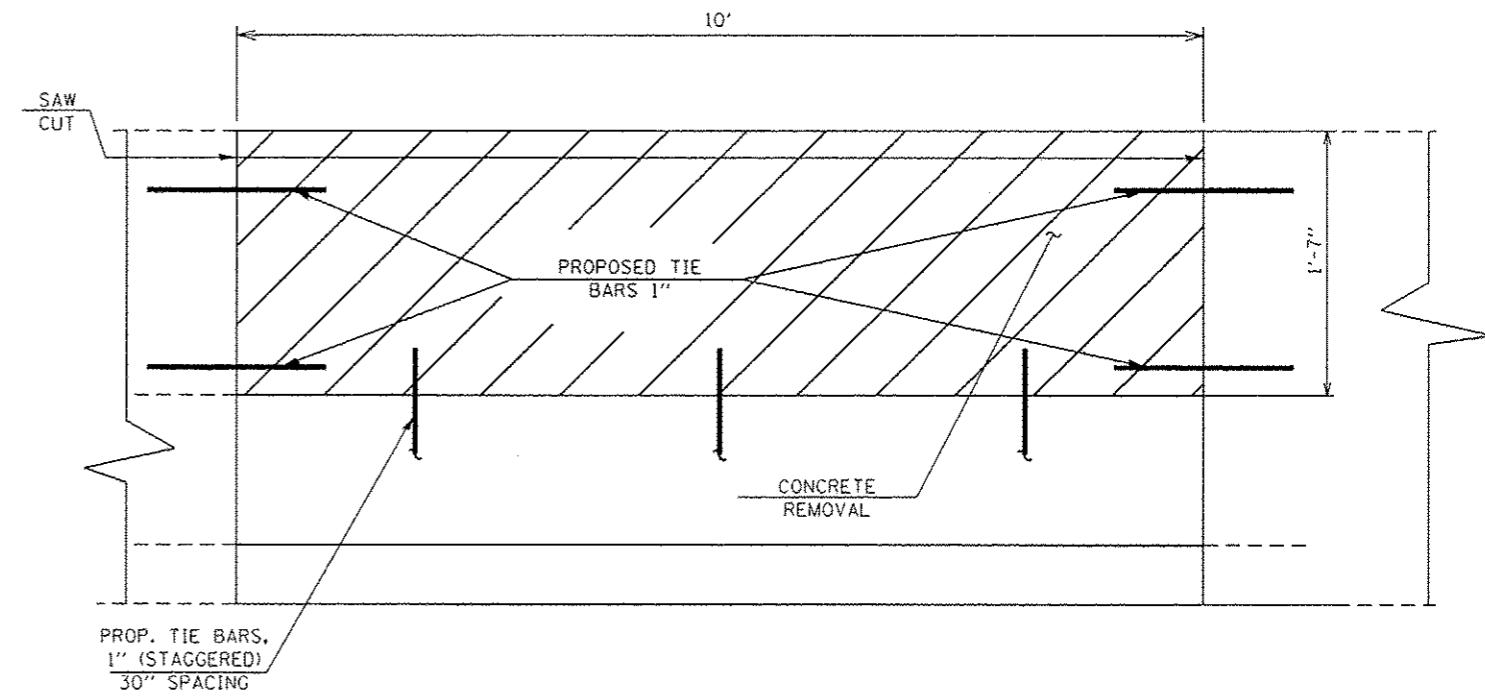
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	0.6
CONCRETE STRUCTURE	Cu. Yds.	0.6
TIE BARS	Each	8
SAW CUTS	Foot	13.17
EARTH EXCAVATION	Cu. Yds.	2.16
TEMPORARY EROSION CONTROL BLANKET	Sq. Yds.	2.22
TOPSOIL FURNISH AND PLACE, 4"	Sq. Yds.	2.22
SEEDING CLASS 2A	Acre	0.0005
PROTECTIVE COAT	Sq. Yds.	2.7



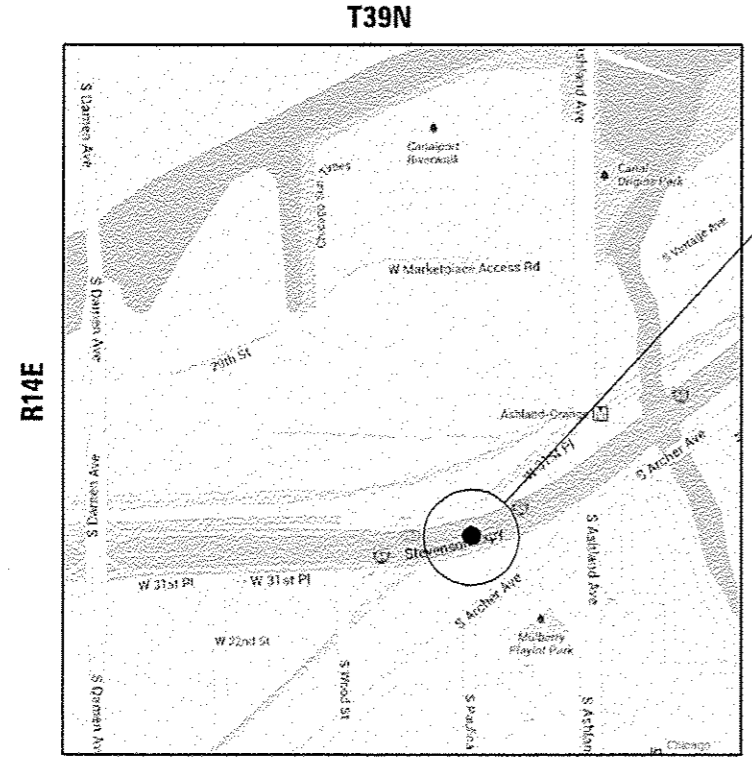
HALF DEPTH REPAIR
REPLACE 10' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #6

TRAFFIC DATA:
SPEED LIMIT: 50 MPH
2012 AOT: 102800



ELEVATION

FILE NAME: c:\pwwork\pvidot\seymorecp\08348982\01	USER NAME: SEYMORECP	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 4 I-55 (STEVENSON EXPRESSWAY)		F.A. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:	
Default	2013-0411	VAR.	2013-0411		VARIOUS	44	15					
PLOT SCALE: 1/8" = 1'-0"	CHECKED: -	REVISED: -	DATE: -		SCALE:	SHEET OF SHEETS:	STA. TO STA.	CONTRACT No. 60W86 ILLINOIS FED. AID PROJECT				
PLOT DATE: 7/23/2013	DATE: -	REVISED: -	DATE: -									



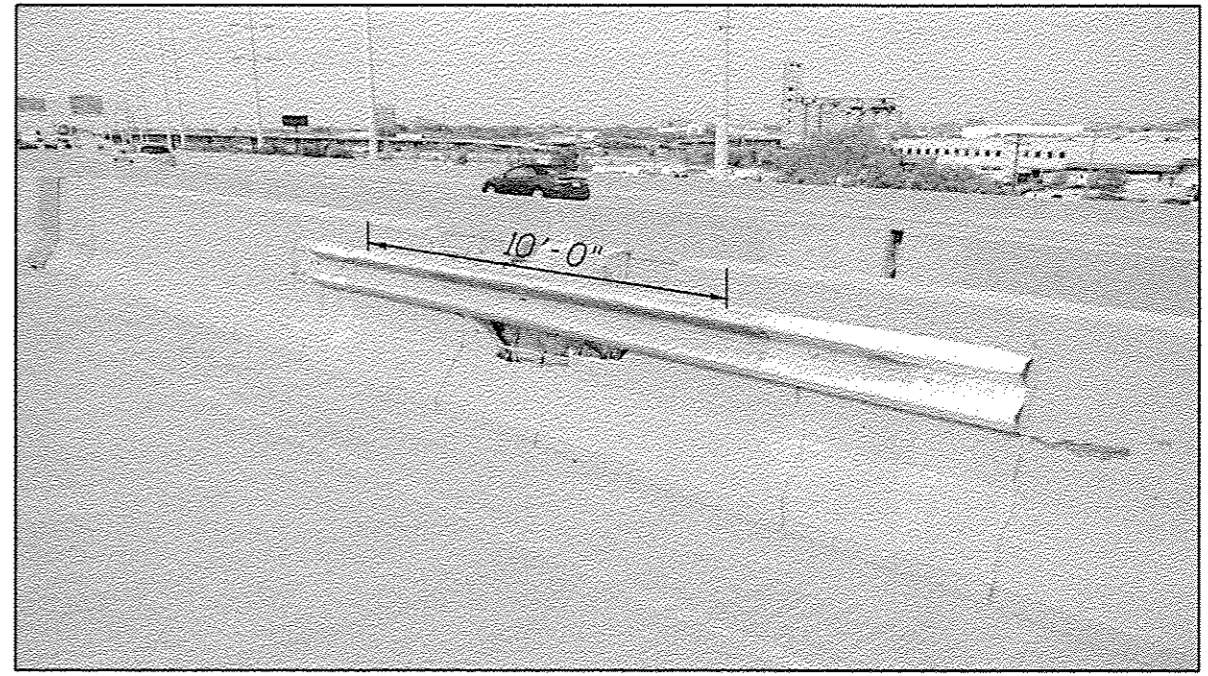
LOCATION 5: I-55 (STEVENSON EXPRESSWAY), EB
 836' WEST OF ASHLAND AVE. OVERPASS, LEFT SIDE
 (SN 016-1084)

CITY OF CHICAGO
SOUTH CHICAGO TOWNSHIP
COOK COUNTY

TRAFFIC DATA:
 SPEED LIMIT: 55 MPH
 2012 ADT: 132100



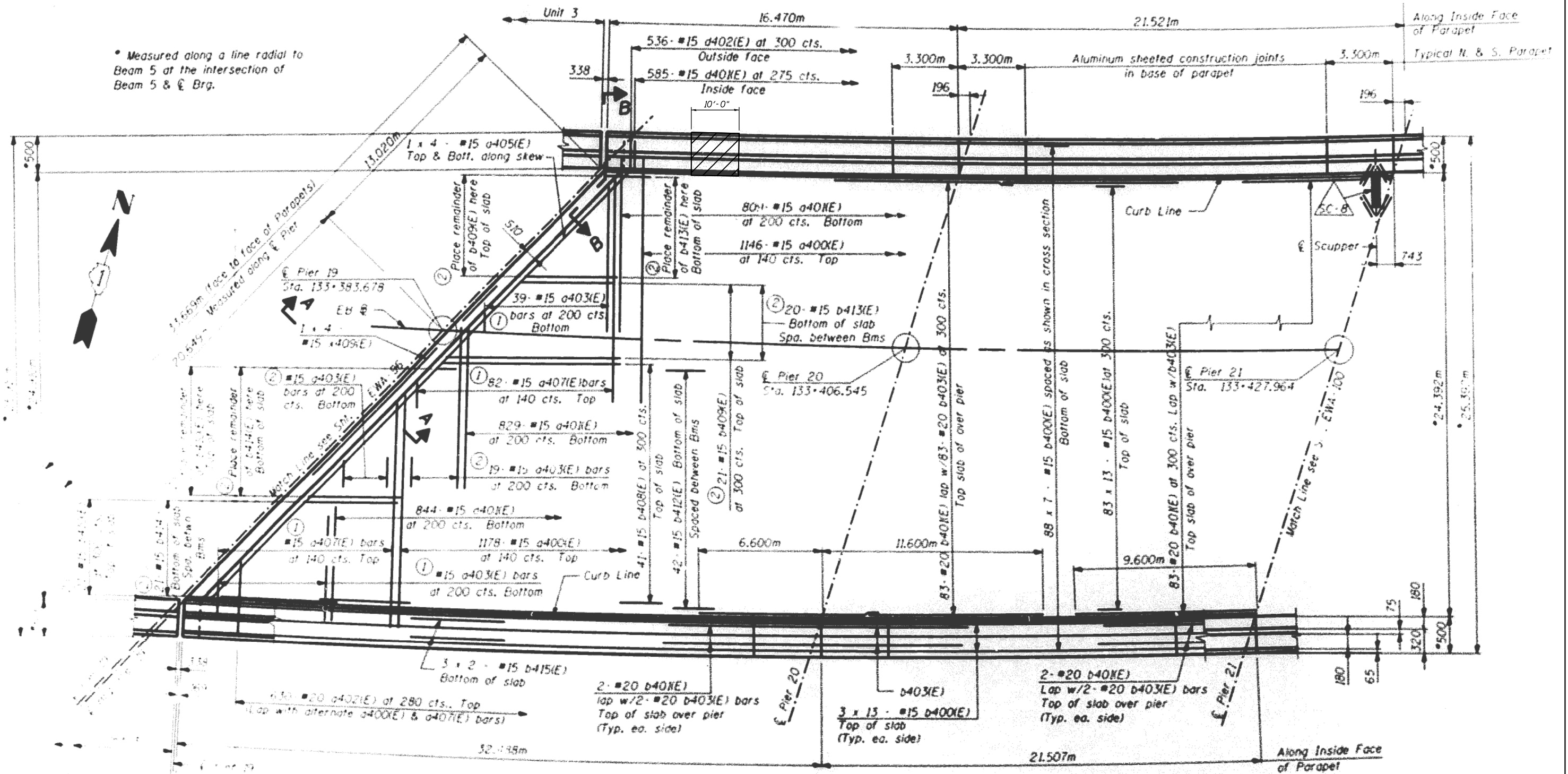
David Carl Puzey 8/7/13
 Expires 11/30/14



EB NORTH PARAPET REPAIR
 FOR MORE DETAIL SEE SHEET #7

FILE NAME *	USER NAME : SEYMORECP	DESIGNED - JAM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 5 (SN 016-1084), NORTH PARAPET I-55 (STEVENSON EXPRESSWAY)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pavidot\seymorecp\00348982\01	9813-ahh-plan.dgn	DRAWN -	REVISED -			VAR.	2013-0411	VARIOUS	44	16	
Default	PLT SCALE * 100.0000 / in.	CHECKED - VHO	REVISED -			CONTRACT NO. 60W86					
	PLT DATE * 7/11/2013	DATE -	REVISED -			SCALE:	SHEET 1 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

* Measured along a line radial to Beam 5 at the intersection of Beam 5 & E Brg.



PARTIAL PLAN

LEGEND

 HATCHED AREA INDICATES CONCRETE REMOVAL

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
et:\pw\work\p\dot\seymorecp\d0348982\0189013-shr-plan.dgn		DRAWN -	REVISED -
Default		CHECKED -	REVISED -
	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -
	PLOT DATE = 7/11/2013		


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

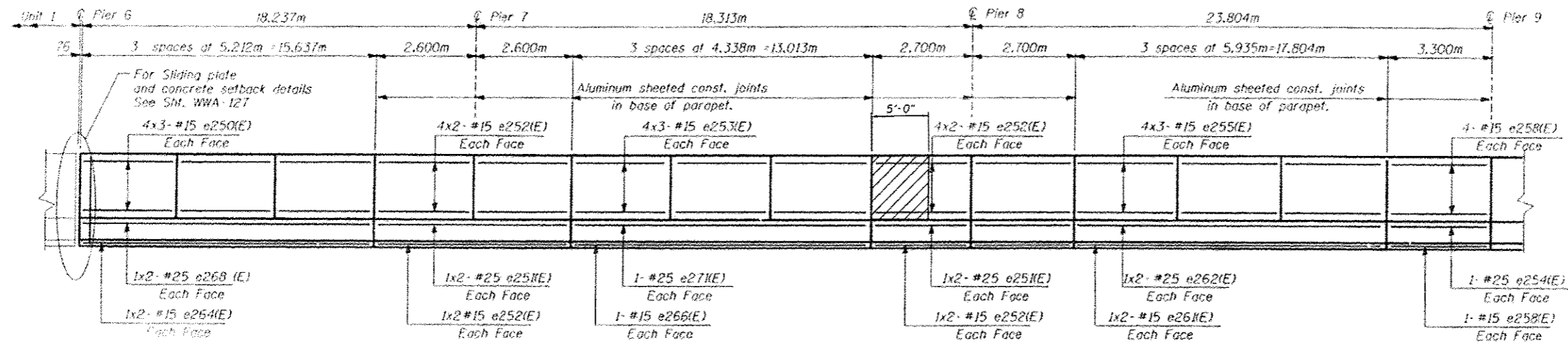
LOCATION 5 (SN 016-1084), EB
I-55 (STEVENS ON EXPRESSWAY)

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	17
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

LEGEND

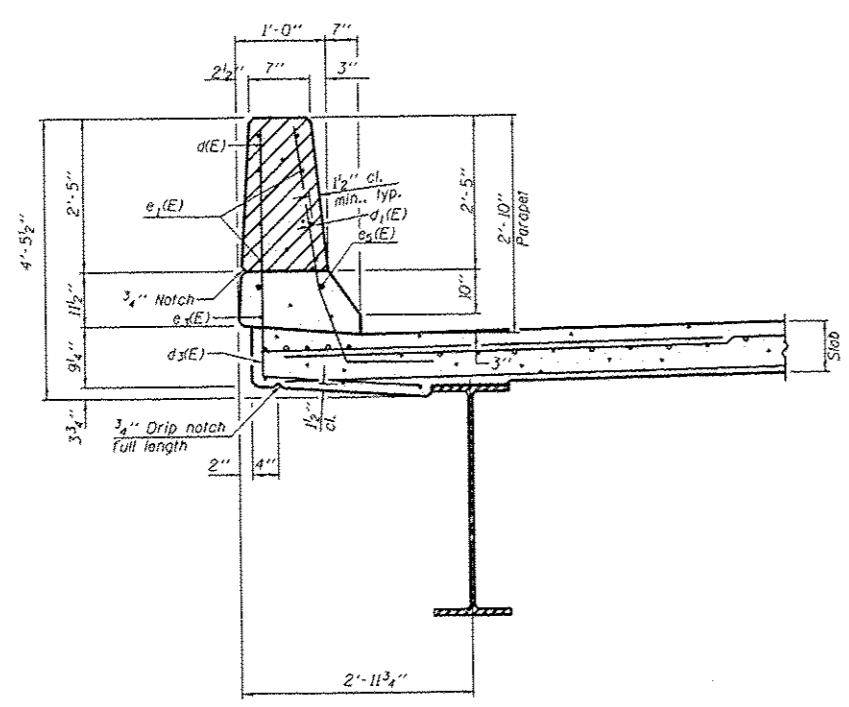
 HATCHED AREA INDICATES CONCRETE REMOVAL



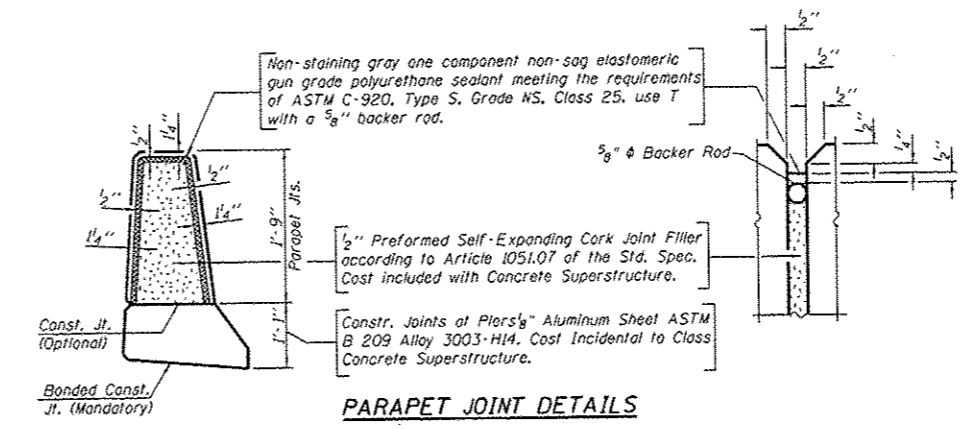
ELEVATION

INSIDE ELEVATION OF PARAPET

MINIMUM BAR LAP
(Parapet)
#4 bar = 2'-0"



EXISTING SECTION THRU PARAPET



PARAPET JOINT DETAILS

SUPERSTRUCTURE BILL OF MATERIAL

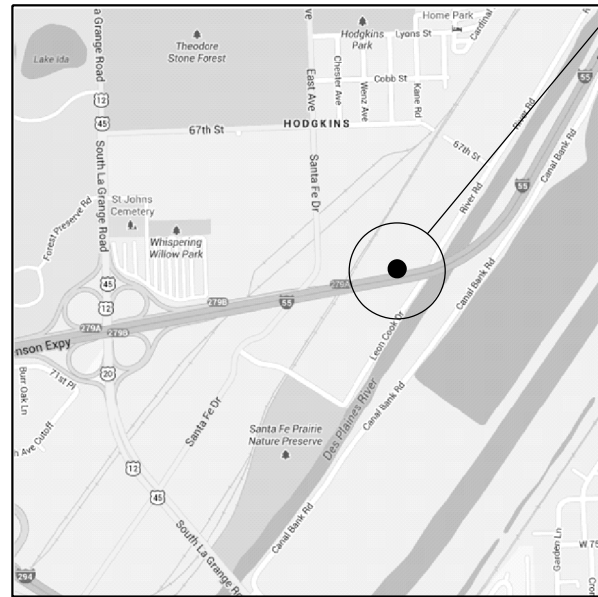
Bar No.	Size	Length	Shape
e ₁ (E)	#4	4'-10"	
REINFORCEMENT BARS, EPOXY COATED			
			Pound 26
CONCRETE SUPERSTRUCTURE			
			Cu. Yds. 0.7
CONCRETE REMOVAL			
			Cu. Yds. 0.4
SAW CUTS			
			Foot 10
PROTECTIVE COAT			
			Sq. Yds. 2.4

NOTE:
All edges shall have 3/4" chamfer.

PARAPET DETAILS
STRUCTURE NO. 016-1083



R12E

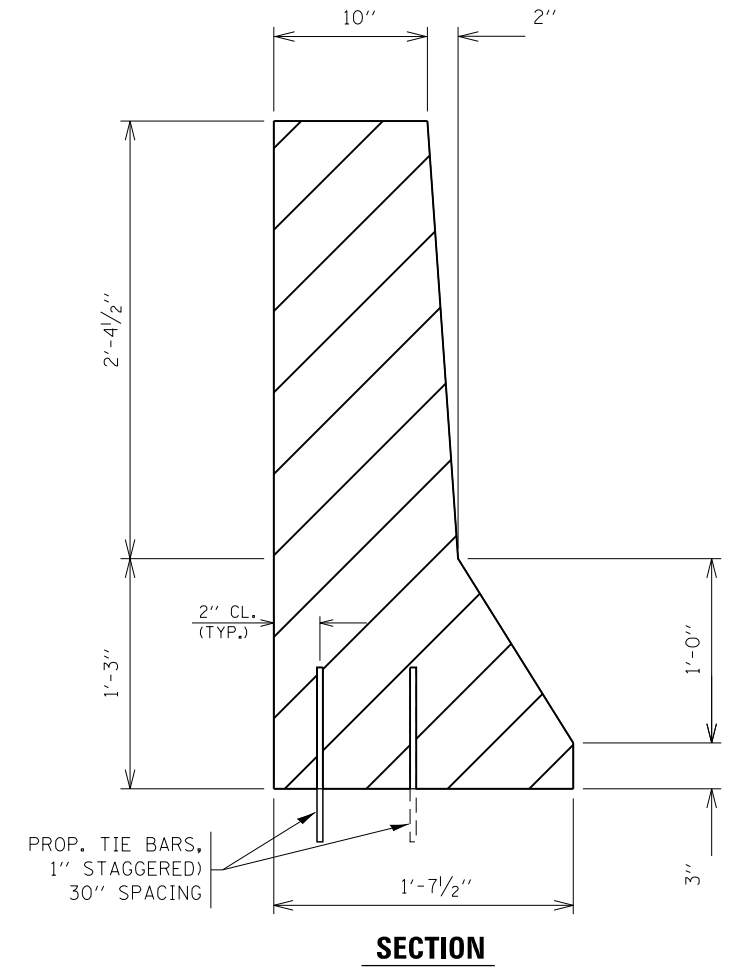


LOCATION 8: I-55 (STEVENSON EXPRESSWAY), WB
 0.69 MILES EAST OF LAGRANGE RD. OVERPASS, RIGHT SIDE

**VILLAGE OF HODGKINS
 LYONS TOWNSHIP
 COOK COUNTY**

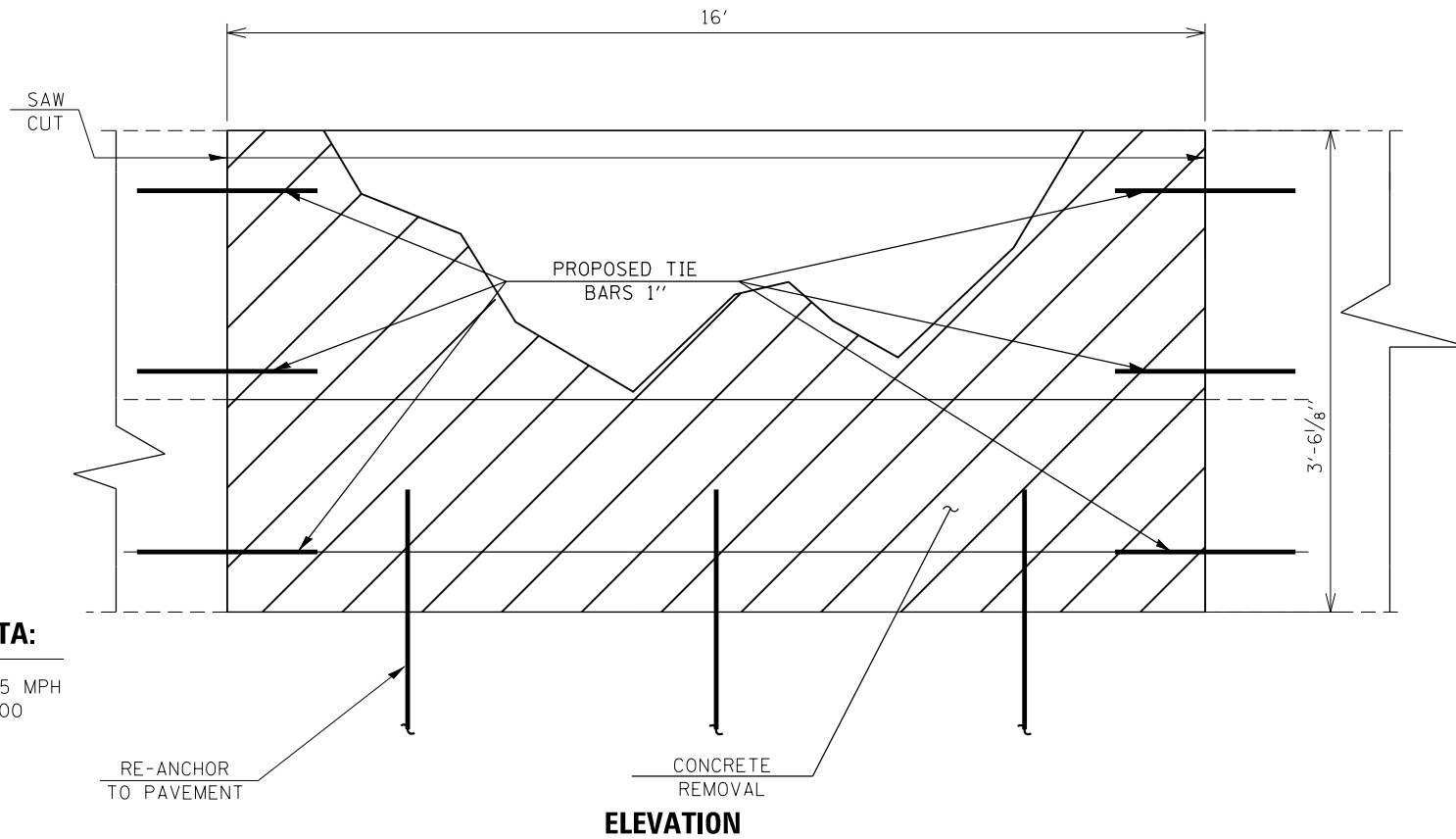
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	1.8
CONCRETE STRUCTURE	Cu. Yds.	2.4
TIE BARS	Each	13
SAW CUTS	Foot	7
PROTECTIVE COAT	Sq. Yds.	8.3



FULL DEPTH REPAIR
 REPLACE 16' OF BARRIER WALL
 FOR MORE DETAILS, SEE SHEET #5

TRAFFIC DATA:
 SPEED LIMIT: 55 MPH
 2012 ADT: 146900



FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
et:\pw\work\p\idot\seymorecp\d0348982\0189013-sh1-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

LOCATION 8			
I-55 (STEVENSON EXPRESSWAY)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	22
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

T36N

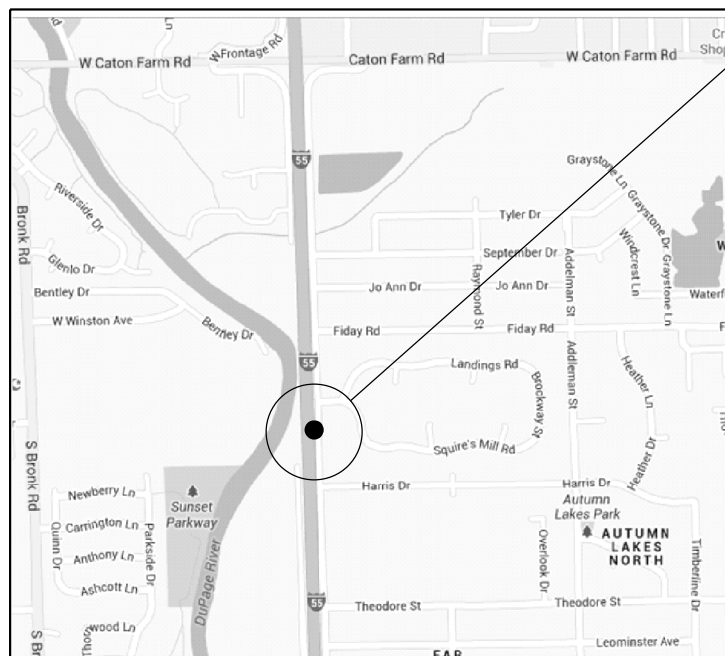
LOCATION 9: I-55 (STEVENS ON EXPRESSWAY), NB

0.37 MILES SOUTH OF CANTON FARM RD. OVERPASS, RIGHT SIDE

PANEL SCHEDULE (TOP)					
MARK	PANEL HEIGHT	PANEL WIDTH	QUANTITY	AREA EACH(S.F)	AREA TOTAL(S.F)
3T-1	10'-0"	20'-4"	2	203.3	406.6
PANEL SCHEDULE (BOTTOM)					
3B-10	6'-0"	20'-4"	1	122.0	122.0



R9E

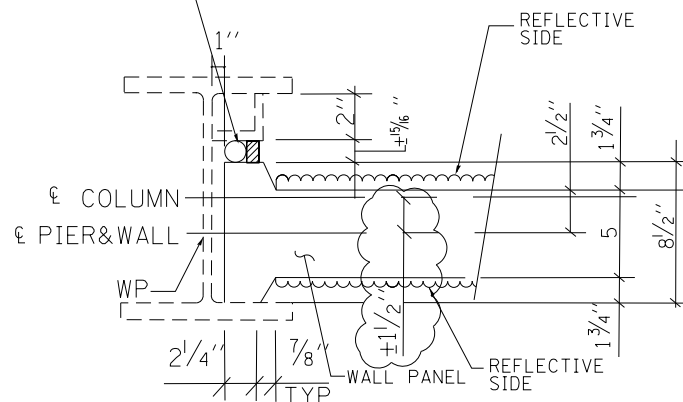


**CITY OF JOLIET
PLAINFIELD TOWNSHIP
WILL COUNTY**

TRAFFIC DATA:

SPEED LIMIT: 65 MPH
2011 ADT: 75600

BACKER ROD W/STR CAULK
(4" LONG @ 4'-0" OC, SEE
GEN NOTES SHT.



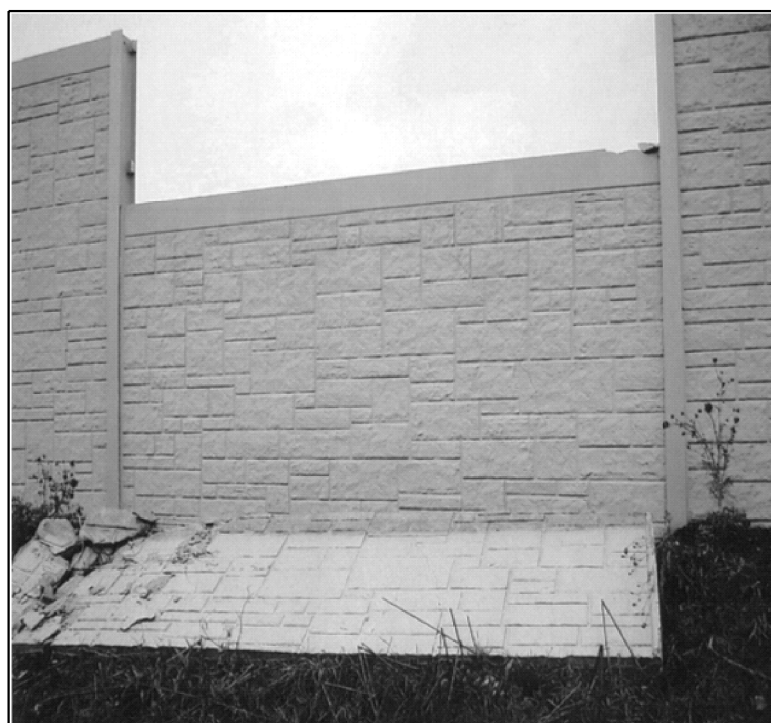
(W12X35)

PANEL TO COLUMN CONN @ W12

BILL OF MATERIAL

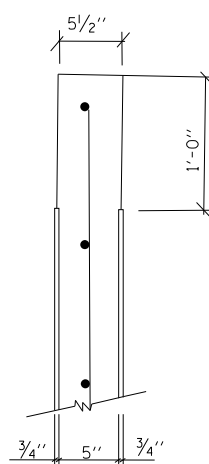
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	5.6
NOISE ABATEMENT WALL REPAIRS	Lump Sum	1

AREA OF REPAIR= 326 SQ FT

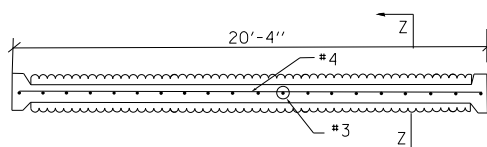


NOISE ABATEMENT WALL REPAIR
REPLACE PANELS
FOR MORE DETAILS, SEE SHEET #26

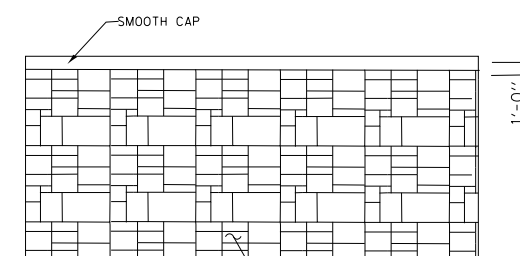
PANEL SECTION W/CAP & FINISH



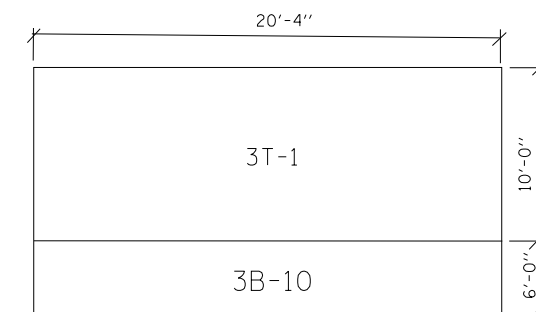
TYP CUT SECTION
Z-Z



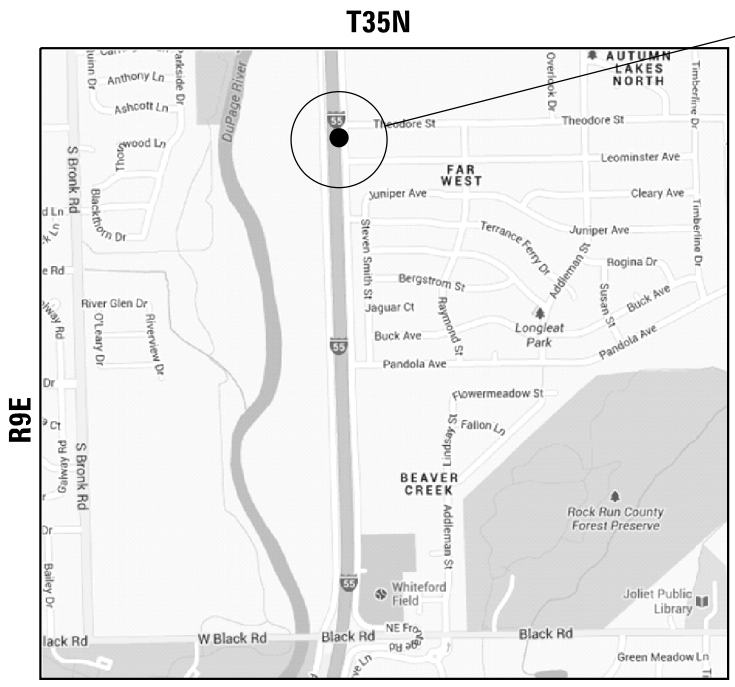
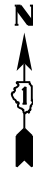
TYPICAL WALL PLAN



TYPICAL PANEL DETAIL W/ FINISH



TYPICAL PANEL DETAIL

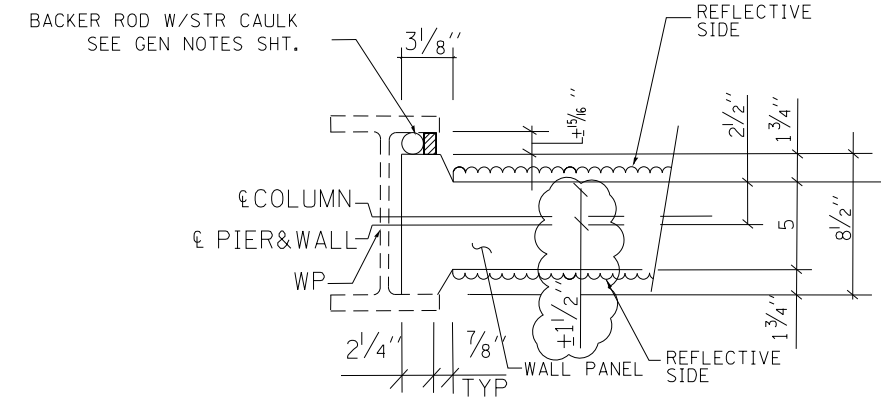


LOCATION 10: I-55 (STEVENS ON EXPRESSWAY), NB
 1 MILE NORTH OF BLACK RD. OVERPASS, RIGHT SIDE

PANEL SCHEDULE (TOP)					
MARK	PANEL HEIGHT	PANEL WIDTH	QUANTITY	AREA EACH(S.F)	AREA TOTAL(S.F)
3T-1	10'-0"	20'-4"	2	203.3	406.6
PANEL SCHEDULE (BOTTOM)					
3B-6	2'-6"	20'-4"	1	50.8	50.8

**CITY OF JOLIET
 TROY TOWNSHIP
 WILL COUNTY**

TRAFFIC DATA:
 SPEED LIMIT: 65 MPH
 2012 ADT: 75600



(W10X22)
 PANEL TO COLUMN CONN @ W10 WALL 3

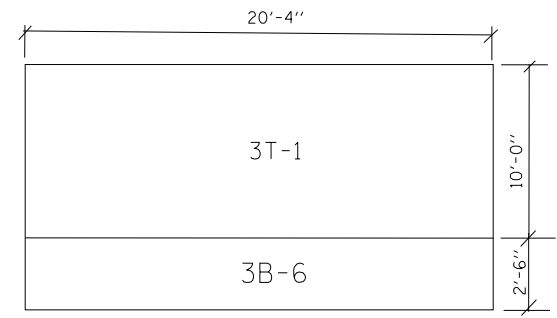
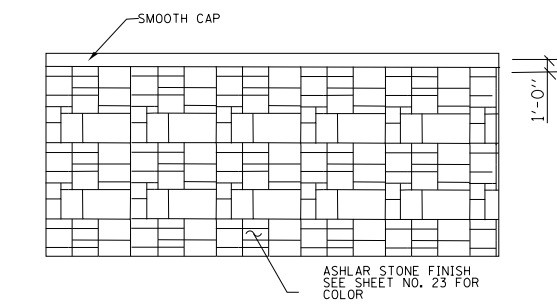
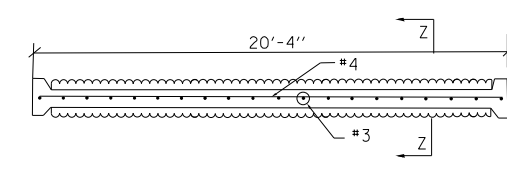
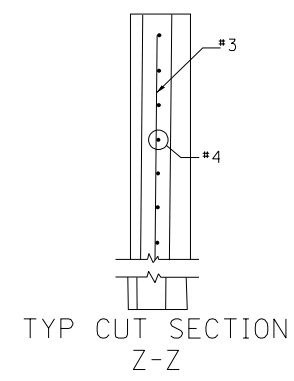
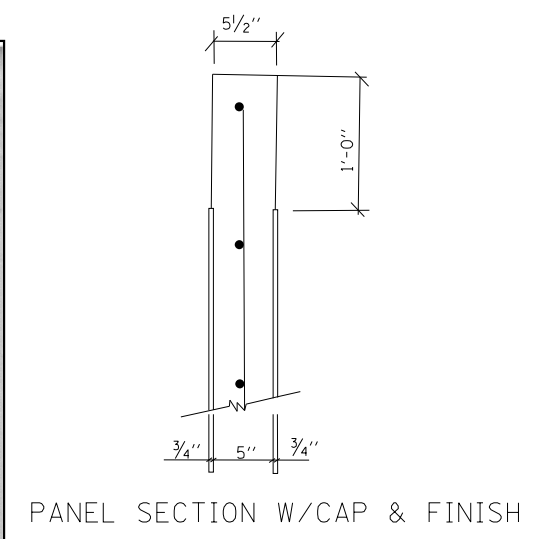
BILL OF MATERIAL

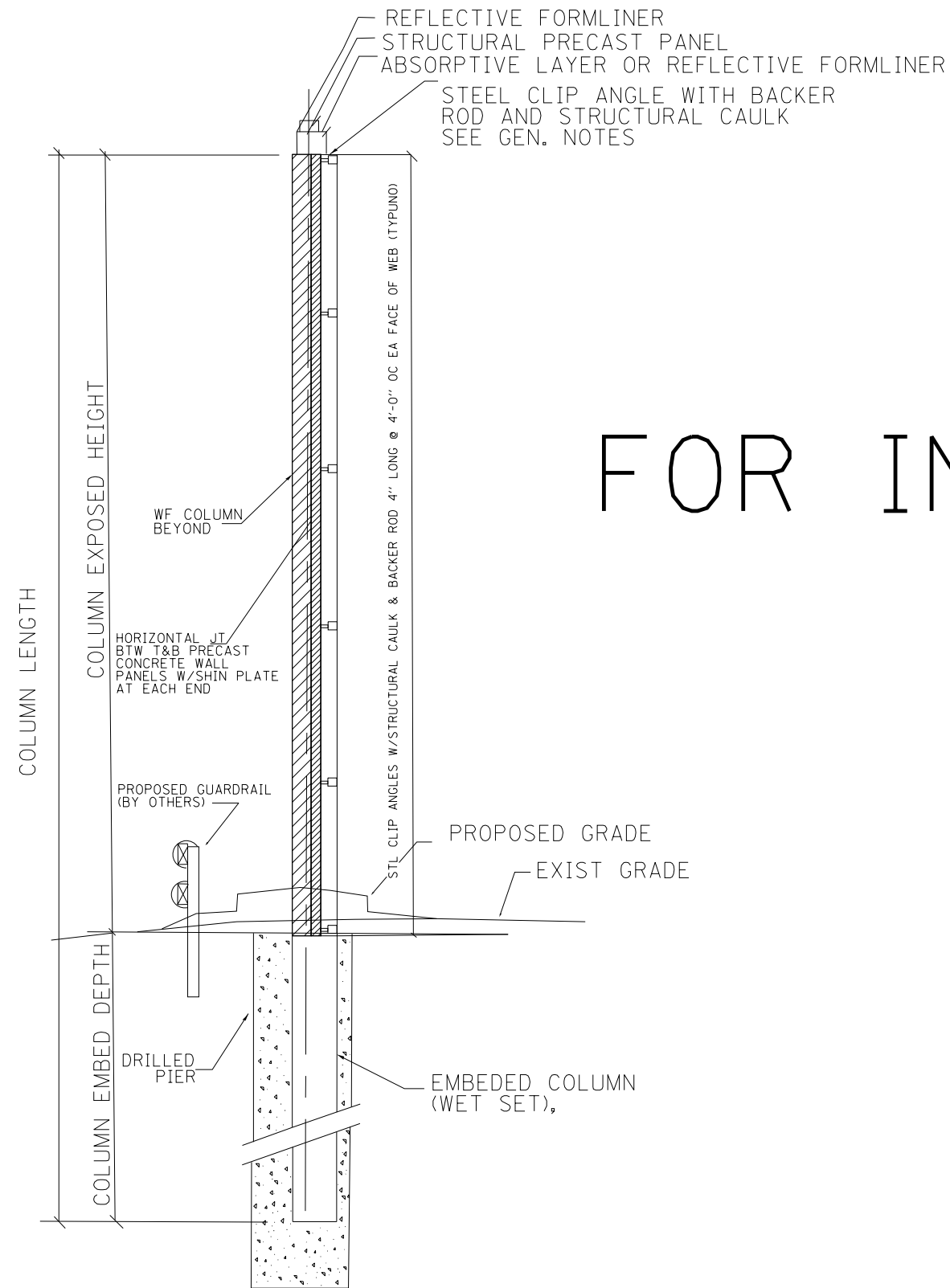
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	4.4
NOISE ABATEMENT WALL REPAIRS	Lump Sum	1

AREA OF REPAIR= 255 SQ FT



NOISE ABATEMENT WALL REPAIR
 REPLACE PANELS
 FOR MORE DETAILS, SEE SHEET #26





FOR INFORMATION ONLY

TYPICAL CROSS SECTION (GROUND MOUNTED)

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
ct:\pw\work\p\dot\seymorecp\d0348982\0189013-sh1-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONCRETE NOISE ABATEMENT WALL
TYPICAL SECTION

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	25
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY

GENERAL NOTES

- FIELD TOLERANCE IS IN ACCORDANCE WITH THE ERECTOR'S MANUAL - STANDARDS & GUIDELINES FOR THE ERECTION OF PRECAST CONCRETE PRODUCTS PER SECTION 8.2 OF THE PCI MANUAL (CURRENT EDITION).
- ERECTION TOLERANCES IN ACCORDANCE WITH PCI RECOMMENDED PRACTICE FOR ERECTION OF PRECAST CONCRETE & SECTION 8.3 OF THE PCI MANUAL (CURRENT EDITION).
- CONTRACTOR SHALL COORDINATE WITH THE PRECAST SUPPLIER SEGMENTS THAT ALLOW FOR THE LEAST AMOUNT OF THE TO UNLOAD FOR THE PRODUCTS SHIPPED. IN THE EVENT OF AN EXCESSIVE AMOUNT OF WAIT-TIME, THE CONTRACTOR AND PRECAST SUPPLIER SHALL COORDINATE THAT THE LOAD EITHER BE OFF-LOADED AND TEMPORARILY SUPPORTED OR RETURNED TO PRECAST SUPPLIER.
- CONTRACTOR SHALL PROVIDE AMPLE ROOM IN AN AREA OF FLAT, LEVEL GROUND ON SOILS SUITABLE TO SUPPORT WHEEL & OUTRIGGER LOADS FOR DELIVERY AND TEMPORARY STORAGE OF PRECAST PRODUCT (IF REQUIRED) ON ITS EDGE. CONTRACTOR SHALL COORDINATE WITH PRECAST SUPPLIER TEMPORARY STORAGE IF REQUIRED.
- ITEMS PROVIDED BY THE INSTALLATION CONTRACTOR:
 - ALL SHIMS UNDER PANELS.
 - ALL GROUT UNDER BASE PLATES AND IN ERECTION ANCHOR POCKETS, CHALKING AND BACKER RODS AT HORIZONTAL AND VERTICAL JOINTS.
 - ANCHOR BOLTS (PER DETAIL H ON 57.00)
 - ANCHOR BOLT WASHERS (PER DETAIL H ON 57.00)
 - ANCHOR BOLT WASHERS (PER DETAIL H ON 57.00)
- ITEMS PROVIDED BY PRECAST SUPPLIER:
 - STEEL POSTS WITH STEEL BASE PLATES (WHERE NECESSARY)
- STAIN SHALL BE PENETRATING CONCRETE STAIN (OR COMPARABLE) APPLIED IN SHOP PER MANUFACTURER'S RECOMMENDATIONS BY SUPPLIER AND PROJECT SPECIFICATIONS, COLOR: FEDERAL STANDARD 595-B. TOUCH UP IN FIELD WITH SAME MATERIAL AS NECESSARY.
- REPAIR OF PANELS IS PER MANUFACTURER'S SPECIFICATIONS.
- PANEL CAP SHALL BE SMOOTH FINISH. PANEL WILL BE ASHLAR STONE FORM LINER FINISH PER THE SPECIFICATIONS. (SEE PANEL DETAILS)
- THE MINIMUM REINFORCEMENT BAR COVER SHALL BE 1 1/2" RL.
- ALL REINFORCEMENT SHALL BE EPOXY COATED.
- PANEL DIMENSIONS SHALL BE WITHIN 1/8" RL.
- ANGULAR DISTORTION WITH REGARD TO PANEL SQUARENESS, DEFINED AS THE DIFFERENCE BETWEEN THE TWO DIAGONALS, SHALL NOT EXCEED 1/2" RL.
- SURFACE DEFECTS ON FORMED SURFACES MEASURED ON A LENGTH OF 5 FT SHALL NOT BE MORE THAN 0.10 IN PRIOR TO LOADING PANELS ON TRUCK FOR DELIVERY.
- POSTS SHALL BE INSTALLED PLUMB TO WITHIN 1/20 IN VERTICAL FOR EVERY 15 FT OF HEIGHT AND TO WITHIN 1/20 IN OF THE STATION AND OFFSET INDICATED.
- DRILLED SHAFT FOUNDATIONS SHALL BE PLACED WITHIN 2 IN OF THE STATION AND OFFSET INDICATED.
- ALL LIFTING INSERTS CAST INTO THE PANELS SHALL BE HOT DIPPED GALVANIZED & SHALL BE INSTALLED INTO THE TOP EDGE OF EACH PANEL AT QUARTER POINTS AS SHOWN IN STEP (3) OF DETAIL 1/SZ.00.

MATERIAL SPECIFICATIONS (PER DOT SPECIFICATIONS)

- PRECAST CONCRETE (NOISE ABATEMENT WALL, GROUND MOUNTED) CLASS PC
 - $f_c = 6000$ PSI @ 28 DAYS
 - $f_c = 2000$ PSI @ STRIPPING
 - $f_c = 4000$ PSI MIN. @ SHIPPING
 - DENSITY = 150 PCF
 - CONCRETE AND STEEL COLORS PER FEDERAL STANDARD 595-B COLOR COLOR OF NOISE ABATEMENT WALL SYSTEM = COLOR #30340 (LIGHT BROWN EARTH TONE)
 - CAISSON CONCRETE: CLASS CS, $f_c = 4000$ PSI @ 14 DAYS
 - REINFORCING STEEL: ASTM A706, FY = 60,000 PSI, EPOXY COATED
 - WELDED WIRE FABRIC: ASTM A197 (AKSHITO MESH), FY = 70,000 PSI MIN, EPOXY COATED
 - BASE PLATES: AASHTO M227, ASTM F1554 GRADE 55
 - ANCHOR BOLTS: AASHTO M227, ASTM F1554 GRADE 55
 - HEAVY HEX NUTS: ASTM A33
 - HARDENED STEEL WASHERS: ASTM F436
 - STEEL POSTS: AASHTO M270 GRADE 345 (GRADE 50), FY = 50,000 PSI*
 - LIFTING INSERTS: UNIVERSAL BUILDING PRODUCTS DFFAS8712C 57x7 1/2" DROP FORGED FOOT ANCHOR (LONG LIFT SYSTEM) OR EQUAL.
 - SHIMS: VERSA-A-SHIM HIGH IMPACT PLASTIC SHIMS PER ASTM D792 & ASTM D695
 - STRUCTURAL CHALK: SKOWAR 51 NS FLEXIBLE EPOXY CONTROL-JOINT SEALER/ADHESIVE OR EQUAL
 - BACKER ROD: MILE 180N FLOW PRODUCT SIZED PER BACKER ROD MANUFACTURING, INC. OR EQUAL
 - NON-STRUCTURAL CHALK SEALANT: AC-20 FIR PER PCORCA CORPORATION SPECIFICATIONS AND MANUFACTURING STANDARDS OR EQUAL.
 - EMERGENCY ACCESS DOOR HARDWARE PACKAGE:
 - MASTERTEC-CARR PRODUCTS:
 - a) HEAVY DOOR HINGE: NARROW-PROFILE WELD-ON LIFT OFF HINGES, STYLE 6 - CATALOG 115 ITEM 1315A55
 - b) SLIDE BOLT AT BASE OF DOOR: ADJUSTABLE-GUIDE EXTRA-LONG SLIDE BOLTS CATALOG 115 ITEM 1236A13
 - c) PADLOCK: HASS-STYLE 4 - CATALOG 114 ITEM 1546B
 - d) LOCK: KONG PADLOCK ITEM NO. 3770 FOR FIRE DEPARTMENT ACCESS
 - e) KONG BOX 3700 SERIES SURFACE MOUNT FOR HINGED DOOR
- *IF USED IN PLACE OF #3 TEMP/SURVIVAGE EPOXY COATED REINFORCEMENT
- **BASE PLATES AND STEEL POSTS SHALL BE GALVANIZED ACCORDING TO ASTM A335 AND AASHTO M111

INSTALLATION (GRADED CAISSON SHAFT INSTALLATION)

- AUGER HOLE TO REQUIRED DEPTH. SEE PRECAST SHOP DRAWINGS FOR PER LOCATIONS, DEPTH AS NECESSARY.
- PLACE REBAR PER DRILLED PIER/CAISSON DETAILS
- PLACE $f_c = 4000$ PSI CONCRETE IN AUGERED HOLE TO THE DEPTH AND ELEVATIONS SPECIFIED ON PLAN AND ELEVATION SHEETS AND FOOTING SCHEDULE OF THESE PRECAST SHOP DRAWINGS.
- NET SET STEEL COLUMNS IMMEDIATELY UPON PLACEMENT OF CONCRETE IN AUGERED HOLE TO THE DEPTH AND ELEVATIONS SPECIFIED ON THE PLAN, ELEVATION SHEETS, AND COLUMN SCHEDULE.
- DRILLED CAISSON FOUNDATIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ARTICLE 516.02 OF THE DOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- IF SOIL IS UNSUITABLE, IN ORDER TO PREVENT CAVING THE USE OF A 3/4" PERMANENT STEEL LINER MAY BE REQUIRED. INSTALLATION OF THE 3/4" PERMANENT LINER SHALL BE IN ACCORDANCE WITH DOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 516, ARTICLE 516.02(a). THIS APPLICATION MAY BE UTILIZED UNDER DRY OR WET CONDITIONS FELDING THE CHARACTERISTICS OF THE SOILS ENCOUNTERED.

(GROUT BY OTHERS)

- GROUT STRENGTH: 6000 PSI IN 28 DAYS
4000 PSI IN 3 DAYS
- (DO NOT ALLOW GROUT TO FREEZE PRIOR TO REACHING 5000 PSI)

(PANEL INSTALLATION - NOISE ABATEMENT WALL, GROUND MOUNTED)

- PRESET STEEL POSTS TO THE NECESSARY ELEVATION TO OBTAIN TOP OF ELEVATIONS SPECIFIED ON THE PLAN AND ELEVATION DRAWINGS. SEE 3&4/SZ.00 FOR SHIM DETAILS.
- SET PRECAST PANEL ON THE SHIMS AND INTO SLOT PROVIDED IN STEEL POST BETWEEN PANELS AND CLIP ANGLES. SEE COLUMN DETAILS ON SHEET 57.00.
- RELEASE PANEL FROM THE CRANE.
- FULLY CHALK JOINTS BETWEEN PANELS WITHIN 7 DAYS OF ERECTION OF PANEL.

(PANEL INSTALLATION - NOISE ABATEMENT WALL, STRUCTURE MOUNTED)

- PRESET BOTTOM NUTS AND WASHERS TO THE NECESSARY ELEVATION, AND SHIMS TO THE NECESSARY THICKNESS TO OBTAIN TOP OF PRECAST ELEVATIONS SPECIFIED ON THE PLAN AND ELEVATION SHEETS AND STRUCTURE MOUNTED DETAILS. (DETAILS 4 & 5 ON SHEET 57.00)
- ERECT PRECAST PANEL ON THE SHIMS AND INTO SLOT PROVIDED IN STEEL POSTS BETWEEN PANELS.
- INSTALL TOP NUTS AND WASHERS SHIM TIGHT.
- USING TOP AND BOTTOM NUTS AND WASHERS, PLUMB THE PRECAST PANEL.
- INSTALL BACKER ROD WITH STRUCTURAL CHALK TO STABILIZE PANEL.
- LOCK OFF DOUBLE NUTS STATUS, PLUS 1/4" TIGHT.
- INSTALL STABILIZATION PADS
- FULLY CHALK JOINTS BETWEEN PANELS ALONG CENTERLINE OF PANEL WIDTH SO THAT JOINT FILLER IS NOT VISIBLE TO HINDER THE AESTHETIC APPEARANCE OF THE WALL. CLEAN OFF ANY JOINT FILLER THAT PROTRUDES BEYOND STRUCTURAL WIDTH OF PANEL.
- RELEASE PANEL FROM THE CRANE.

DRILLED CAISSONS

- THIS ITEM SHALL CONSIST OF CONSTRUCTING CONCRETE SHAFT FOUNDATIONS CAST IN DRILLED HOLES IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS, DETAILS SHOWN ON THE PLANS AND FURTHER DESCRIBED HEREIN.
- CAISSON DESIGN IS BASED UPON SOIL TYPES AND PROPERTIES PROVIDED ON SHEETS 42 THRU 54 AND 97 THRU 100 OF THE DESIGN DRAWINGS. IF SOIL CONDITIONS IN FIELD DEVIATE FROM THOSE INDICATED IN THE DESIGN DRAWINGS, THE RESIDENT ENGINEER SHALL CONTACT LARSON ENGINEERING, INC. PRIOR TO CONTRACTOR COMPLETING THE CAISSON.
- THE DRILLING OF THE HOLES SHALL BE ACCOMPLISHED BY MEANS OF EITHER TRUCK MOUNTED OR CRANE MOUNTED EARTH AUGERS. THE DRILL UNIT USED SHALL BE SUCH THAT THE SHAFTS CAN BE EXCAVATED TO THE DIMENSIONS AND ALIGNMENT REQUIRED BY THE PLANS. THE DRILL UNIT SHALL HAVE ADEQUATE CAPACITY TO CREATE A SHAFT EXCAVATION TO A DEPTH OF 20 PERCENT BEYOND THE DEPTHS INDICATED ON THE PLANS.
- IF BOULDERS OR MASONRY FOUNDATIONS SHOULD BE ENCOUNTERED BELOW THE NATURAL GROUND DURING DRILLING, THE HOLES SHOULD CONTINUE THROUGH THESE OBSTRUCTIONS. ALL LOOSE MATERIAL EXISTING AT THE BOTTOM OF THE HOLE AFTER DRILLING OPERATIONS AND EXCAVATION SHALL BE REMOVED BEFORE PLACING CONCRETE IN THE HOLES.
- ALL EXCAVATIONS SHALL BE INSPECTED PRIOR TO CONCRETE PLACEMENT BY AN OWNER'S REPRESENTATIVE TO VERIFY SUITABLE BEARING MATERIAL OF CAPACITY AS SPECIFIED IN THE DESIGN DRAWINGS. THE CONTRACTOR SHALL HAVE AVAILABLE A SUITABLE LIGHT FOR THE INSPECTION OF THE DRILLED HOLES FOR ITS ENTIRE DEPTH. ALL HOLES SHALL BE EXAMINED FOR STRAIGHTNESS. THE ALIGNMENT OF THE SHAFT SHALL NOT VARY FROM VERTICAL BY MORE THAN 1.5%.
- NOTIFY THE OWNER'S REPRESENTATIVE AND LARSON ENGINEERING, INC. WHEN ADDITIONAL EXCAVATION IS REQUIRED TO MEET SUITABLE BEARING MATERIAL.
- SURFACE WATER SHALL NOT BE PERMITTED TO ENTER THE HOLE AND ALL WATER WHICH MAY HAVE INFILTRATED INTO THE HOLE SHALL BE REMOVED PRIOR TO PLACING CONCRETE THEREIN. A DRAINAGE SYSTEM OF SUFFICIENT CAPACITY SHALL BE INSTALLED AND OPERATED TO MAINTAIN THE CONSTRUCTION AREA FREE OF WATER AT ALL TIMES. IF DRAINING OF THE HOLE CANNOT BE READILY ACCOMPLISHED WITHOUT LOSS OF GROUND OR CREATING QUICK CONDITIONS, THEN THE HOLE SHALL BE LINED AND DRAINAGE CONCRETE SHALL BE USED.
- IF UNSUITABLE SOILS ARE FOUND, IN ORDER TO PREVENT CAVING OF THE HOLE BEFORE CONCRETE IS PLACED THEREIN, A PERMANENT LINER MAY BE INSTALLED AT THE OPTION OF THE CONTRACTOR PER DOT

(DRILLED CAISSONS CONT'D)

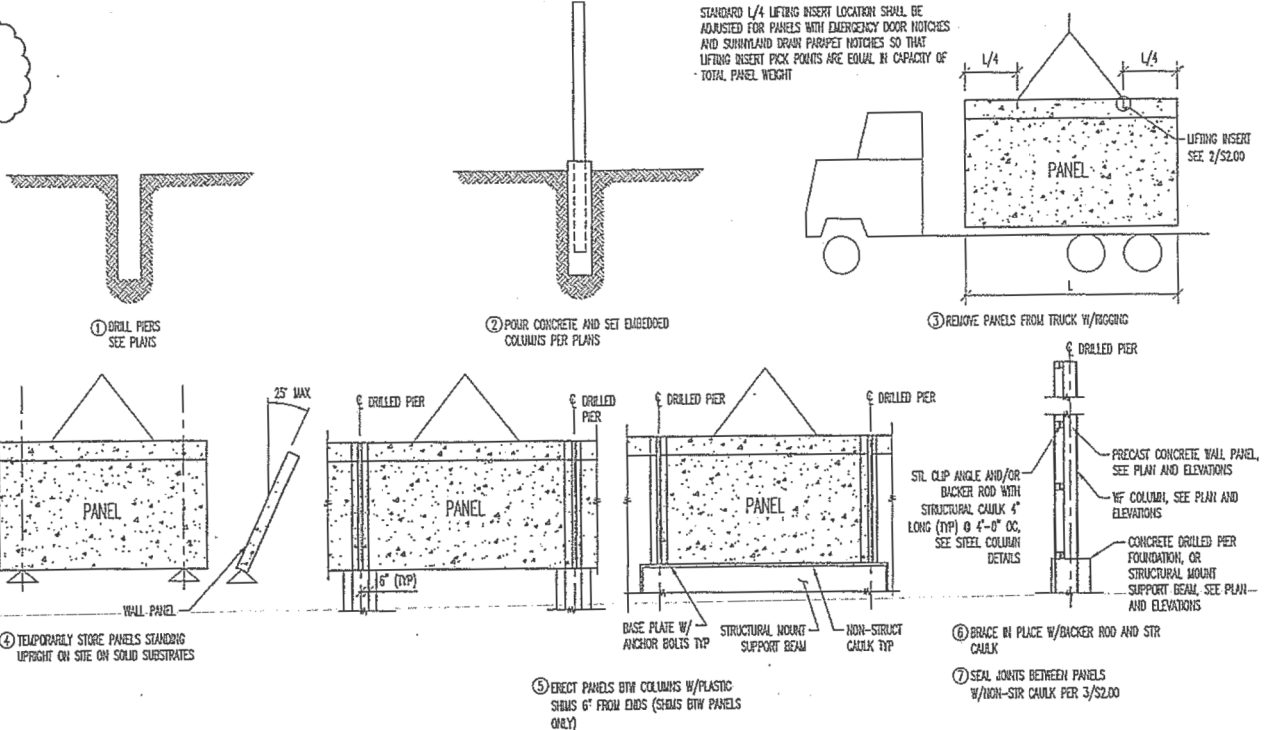
- STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 516, ARTICLE 516.02(a). PERMANENT LINERS SHALL BE INSTALLED AN EFFECTIVE HEIGHT ABOVE THE UNSUITABLE SOIL ELEVATIONS (APPROXIMATELY 1 FT TO 2 FT ABOVE) AND TO A DEPTH EQUAL TO OR GREATER THAN THE DESIGN DEPTH INDICATED IN THE SHOP DRAWINGS TO PREVENT CAVING.
- EXCAVATION MUST BE DONE TO OVER-EXCAVATE AREAS OF PROTECTED UNSUITABLE SOILS. OVER-EXCAVATION REQUIRES EXCAVATING PER OSHA REGULATIONS AND STANDARDS AND PROVIDING TEMPORARY PROTECTION AND SUPPORT TO UTILITIES NEAR AND WITHIN THE AREA OF EXCAVATION. TEMPORARY SHORING MAY BE REQUIRED IN ORDER TO MAINTAIN EXCAVATION WITHIN THE RIGHT-OF-WAY LIMITS OF THE PROJECT. THE CONCRETE DRILLED PIER SHALL BE SET TO ITS PROPER DEPTH, SIZE, AND ELEVATION PRIOR TO BACKFILLING. CONCRETE SHALL BE ALLOWED TO CURE TO 75% OF ITS DESIGN CAPACITY PRIOR TO BACKFILLING. THE AREA OF OVER-EXCAVATION AROUND THE CONCRETE DRILLED PIER SHALL BE BACKFILLED IN COMPACTED LIFTS OF NOT MORE THAN 8" AND SHALL BE WITHIN 5% OF THE STANDARD PROCTOR. THE CONTRACTOR CAN USE SOIL CLASSIFIED AS SUITABLE SOILS PER THE ENGINEER AS BACKFILL MATERIAL.
- THE METHOD OF PLACING CONCRETE IN THE SHAFTS SHALL BE PRE-APPROVED BY THE RESIDENT ENGINEER. THE METHOD USED SHALL BE ONE THAT WILL PROVIDE A CONTINUOUS FLOW WITH NO SEGREGATION OF THE CONCRETE MATERIALS.

SHALLOW SPREAD FOOTINGS

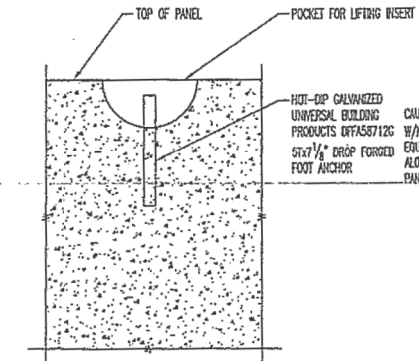
- THIS ITEM SHALL CONSIST OF CONSTRUCTING ISOLATED SHALLOW CONCRETE FOUNDATIONS CAST IN FORMED OUT EXCAVATED HOLES AS SHOWN ON THE PLANS AND DETAILS HEREIN.
- SPREAD FOOTING DESIGN IS BASED ON SOIL TYPES AND PROPERTIES PROVIDED ON SHEETS 42 THRU 54 OF THE DESIGN DRAWINGS. IF SOIL CONDITIONS IN FIELD DEVIATE FROM THOSE INDICATED IN THE DESIGN DRAWINGS, THE RESIDENT ENGINEER SHALL CONTACT LARSON ENGINEERING, INC. PRIOR TO PLACEMENT OF THE CONCRETE.
- SOIL SHALL BE COMPACTED TO 95% OF ITS STANDARD ... PRIOR TO PLACING CONCRETE.
- CONCRETE SHALL BE PROTECTED FROM FREEZING PRIOR TO BEING COVERED WITH BACKFILL.
- FOUNDATION SHALL BE BACKFILLED AS SOON AS POSSIBLE SO AS TO LIMIT THE EFFECTS OF WATER INFILTRATION AND FROST HEAVE.

ELECTRICAL

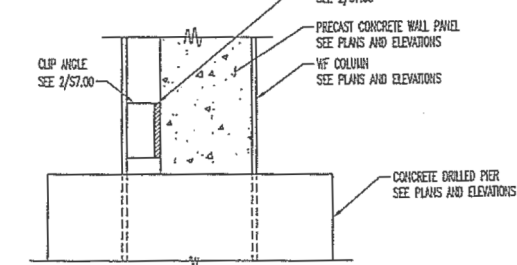
- WHEN WORKING IN THE VICINITY OF COMED'S ELECTRIC TRANSMISSION LINES DURING THE INSTALLATION, OSHA REQUIRES THAT A MIN. 20' WORKING CLEARANCE DISTANCE MUST BE MAINTAINED BETWEEN THE BOOMS, ARMS OR OTHER PARTS THAT CAN BE RAISED ON THE EQUIPMENT FOR THE PETITIONER'S CONTRACTOR AND COMED'S EXISTING SOUTHERLY 345,000 VOLT ELECTRIC TRANSMISSION CONDUCTORS. OSHA REQUIRES A MINIMUM OF 14' WORKING CLEARANCE DISTANCE MUST BE MAINTAINED BETWEEN THE BOOMS, ARMS OR OTHER PARTS THAT CAN BE RAISED ON THE EQUIPMENT FOR THE PETITIONER'S CONTRACTOR AND COMED'S EXISTING NORTHERLY 138,000 VOLT ELECTRIC TRANSMISSION CONDUCTORS. UNDER NO CIRCUMSTANCES, SHOULD TRUCK BEDS BE RAISED UNDERNEATH COMED TRANSMISSION LINES.



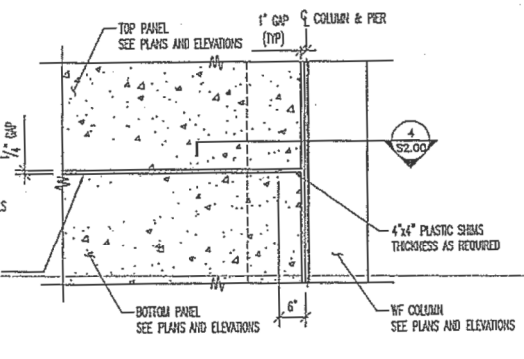
1 TYPICAL NOISE ABATEMENT WALL INSTALLATION SEQUENCE & PROCEDURES
\$2.00 N.T.S.



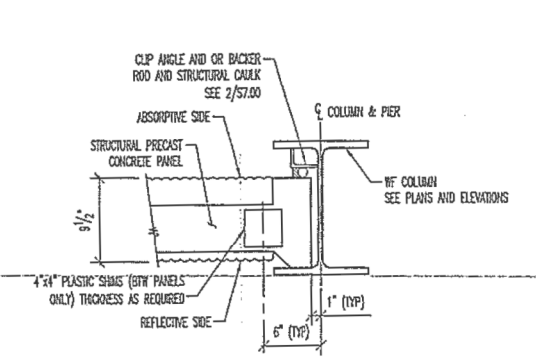
2 TYPICAL LIFTING INSERT DETAIL
\$2.00 N.T.S.



5 CLIP ANGLE DETAIL
\$2.00 N.T.S.



3 HORIZONTAL JOINT DETAIL
\$2.00 N.T.S.



4 PANEL CONNECTION TO COLUMN DETAIL
\$2.00 N.T.S.

NOTE: THESE DRAWINGS HAVE BEEN PREPARED FOR THE FABRICATION OF THE PRECAST NOISE ABATEMENT WALLS AND PROVIDE GENERAL INSTALLATION SEQUENCE AND PROCEDURES FOR THE CONTRACTOR. THE CONTRACTOR SHALL RETAIN SOLE RESPONSIBILITY FOR THE MEANS, METHODS, AND TECHNIQUES OF CONSTRUCTION OF THE NOISE ABATEMENT WALLS FOR COMPLIANCE WITH LAWS, REGULATIONS, AND CODES, AND FOR THE SAFETY OF CONSTRUCTION APPLICABLE TO THIS WORK.

APPROVED FOR MAIN CONTRACTOR'S RECORD DRAWINGS ONLY

DATE: 7/11/2013

Signature: [Signature]

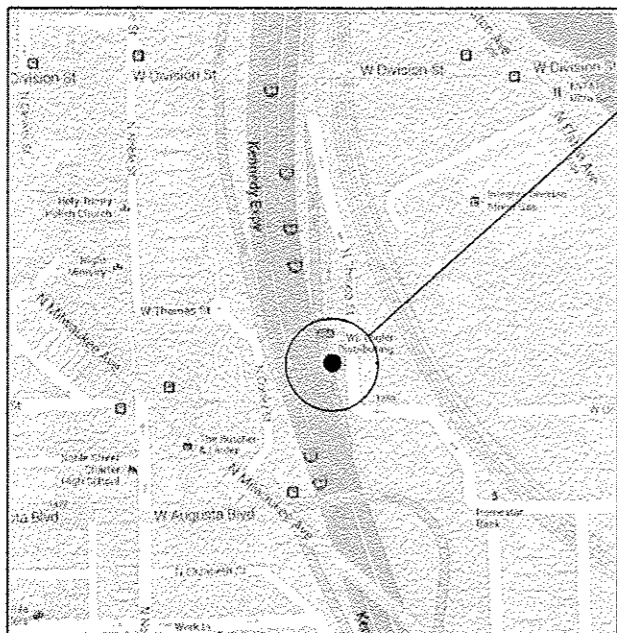
Engineer of Design and Traffic Structures

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TYPICAL CONCRETE NOISE ABATEMENT WALL REPAIR DETAILS AND NOTES (I-55)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	DRAWN -	REVISED -					VAR.	2013-0411	VARIOUS	44	26
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -									CONTRACT NO. 60W86
	PLOT DATE = 7/11/2013	DATE -	REVISED -									ILLINOIS FED. AID PROJECT



R14E

T39N



LOCATION 11: I-90/I-94 (KENNEDY EXPRESSWAY), NB
0.2 MILES SOUTH OF DIVISION ST. OVERPASS, RIGHT SIDE

**CITY OF CHICAGO
WEST CHICAGO TOWNSHIP
COOK COUNTY**



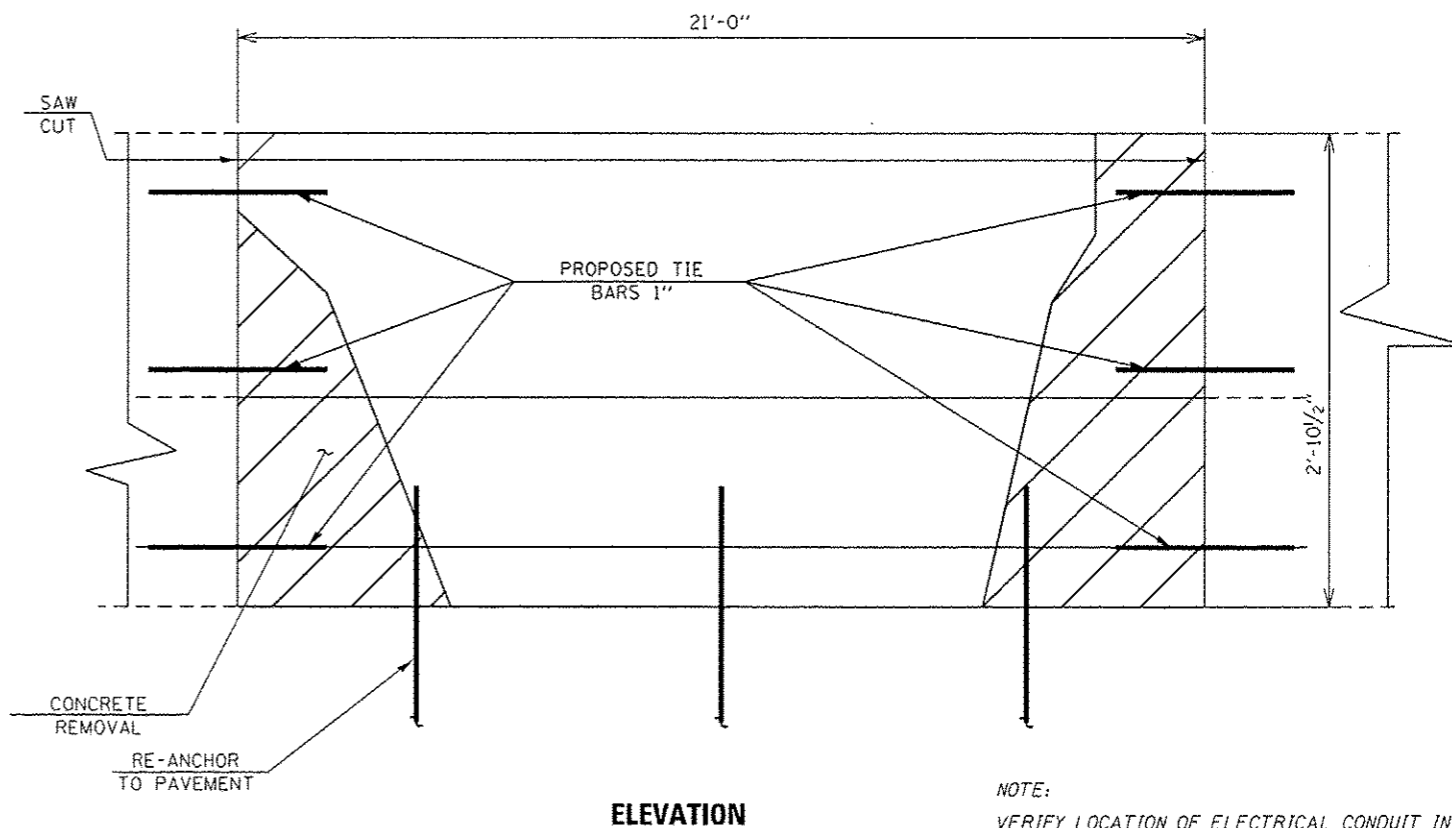
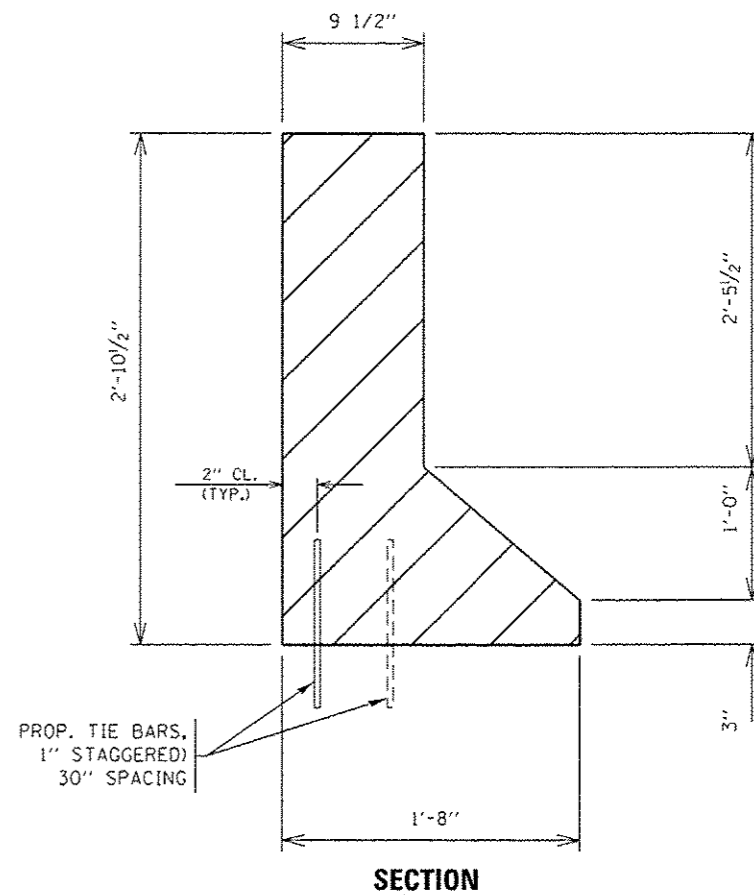
FULL DEPTH REPAIR
REPLACE 21' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #5

TRAFFIC DATA:

SPEED LIMIT: 55 MPH
2012 ADT: 292500

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	0.3
CONCRETE STRUCTURE	Cu. Yds.	2.2
TIE BARS	Each	15
SAW CUTS	Foot	5
PROTECTIVE COAT	Sq. Yds.	9.5



NOTE:
VERIFY LOCATION OF ELECTRICAL CONDUIT IN BARRIER WALL IN FIELD.

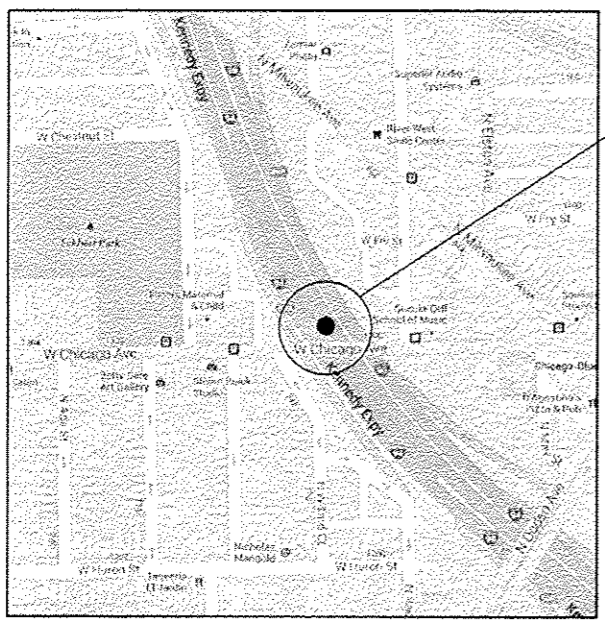
ELEVATION

FILE NAME =	USER NAME = SEYMDRECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 11 I-90/I-94 (KENNEDY EXPRESSWAY)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	VAR.	2013-0411	VARIOUS	44	27
		CHECKED -	REVISED -							CONTRACT	140	60W86
		DATE -	REVISED -							ILLINOIS FED. AID PROJECT		



R14E

T39N



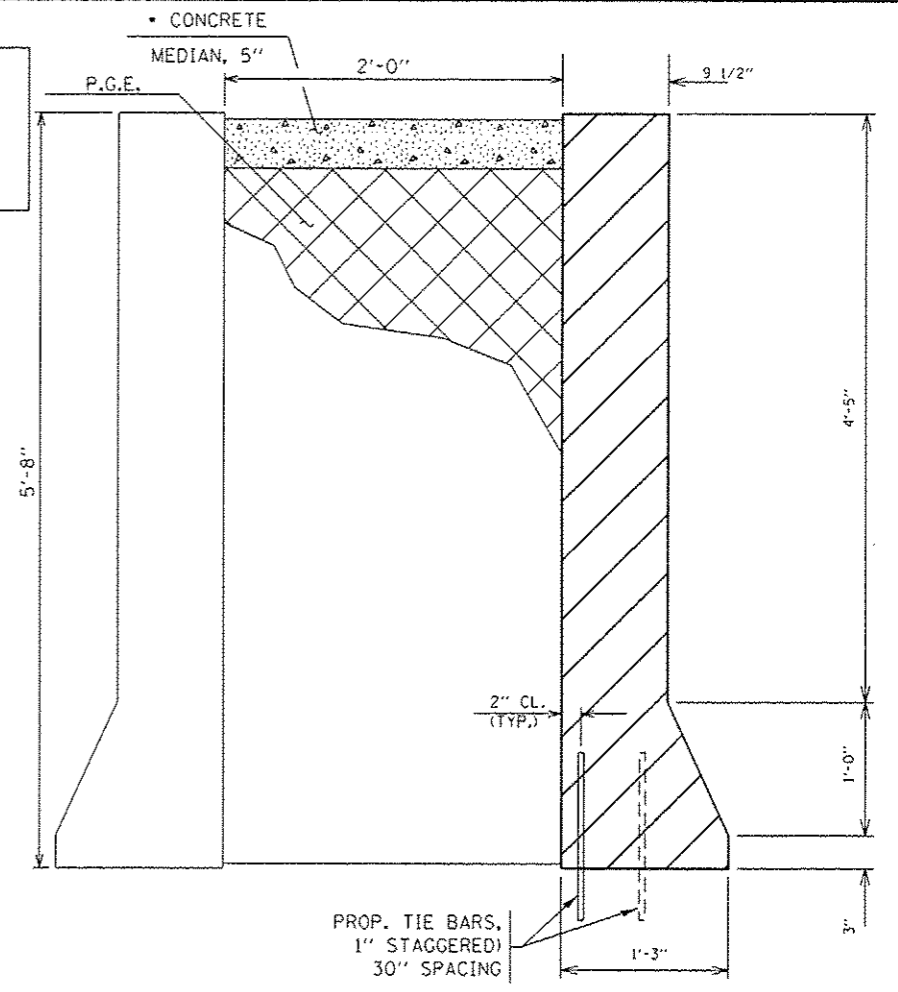
LOCATION 12: I-90/I-94 (KENNEDY EXPRESSWAY), SB
122' NORTH OF CHICAGO AVE. OVERPASS, LEFT SIDE

CITY OF CHICAGO
WEST CHICAGO TOWNSHIP
COOK COUNTY

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	2.0
CONCRETE STRUCTURE	Cu. Yds.	2.0
TIE BARS	Each	8
DOWEL BARS	Each	3
SAW CUTS	Foot	11
PROTECTIVE COAT	Sq. Yds.	10.5
POROUS GRANULAR EMBANKMENT	Cu. Yds.	2.0

* Cost of "Concrete Median, 5'" is included in "Concrete Structure"



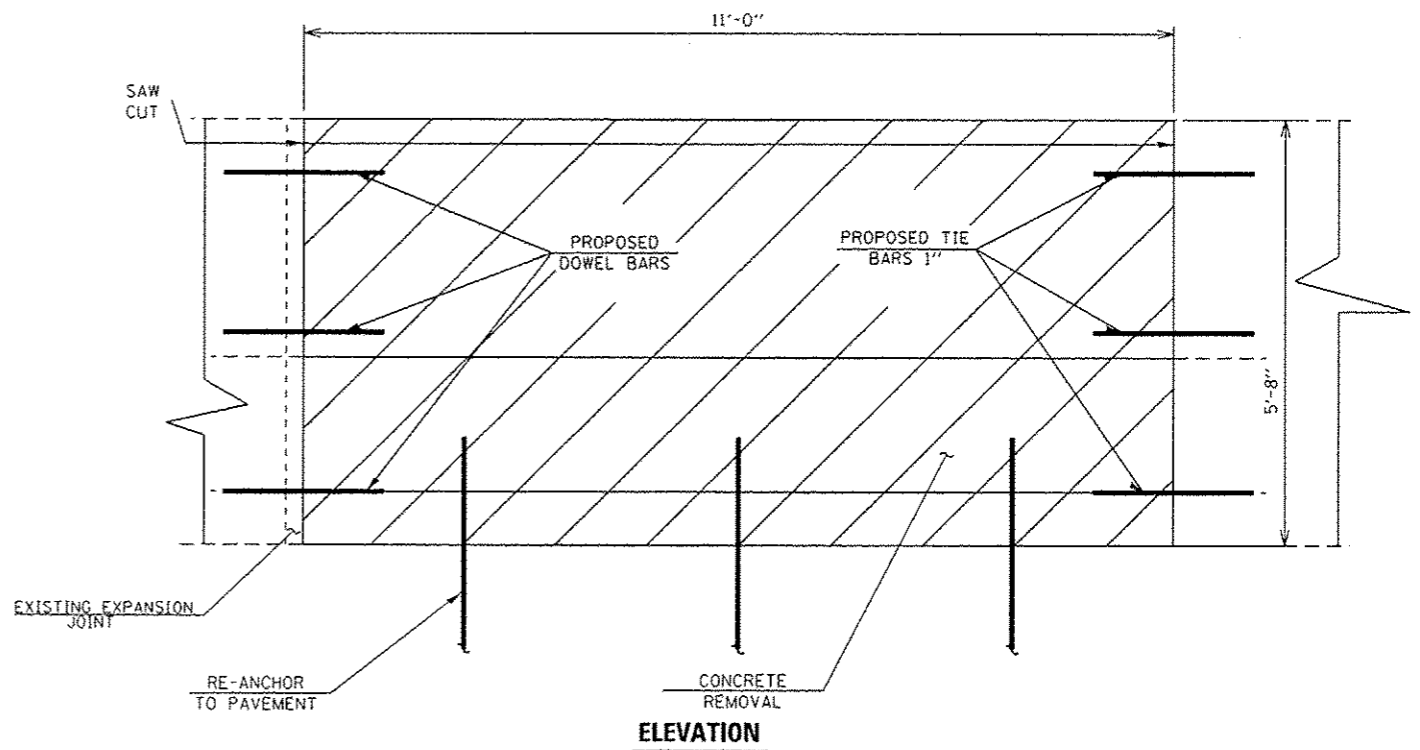
SECTION



FULL DEPTH REPAIR
REPLACE 11' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #5

TRAFFIC DATA:

SPEED LIMIT: 55 MPH
2012 ADT: 287400



ELEVATION

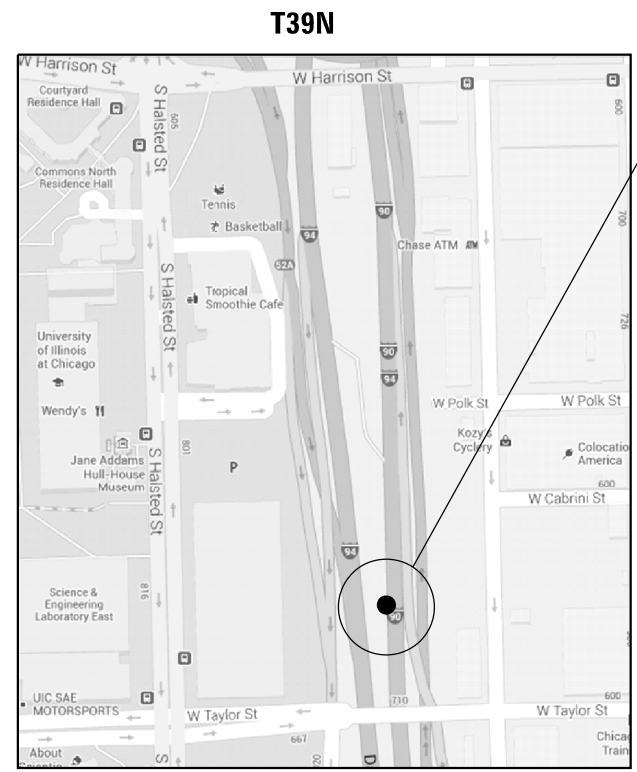
FILE NAME :	USER NAME : SEYHORECP	DESIGNED -	REVISED -
c:\pw_work\pavdot\seymorecp\d0348982\0158013-akt-pln.dgn		DRAWN -	REVISED -
Default	PLT SCALE * 1/80.0000' / in.	CHECKED -	REVISED -
	PLT DATE * 7/23/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LOCATION 12		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-90/I-94 (KENNEDY EXPRESSWAY)		VAR.	2013-0411	VARIOUS	44	28
SCALE:	SHEET OF SHEETS	STA.	TO STA.	CONTRACT NO. 60WB6		
[ILLINOIS] FED. AID PROJECT						



R14E

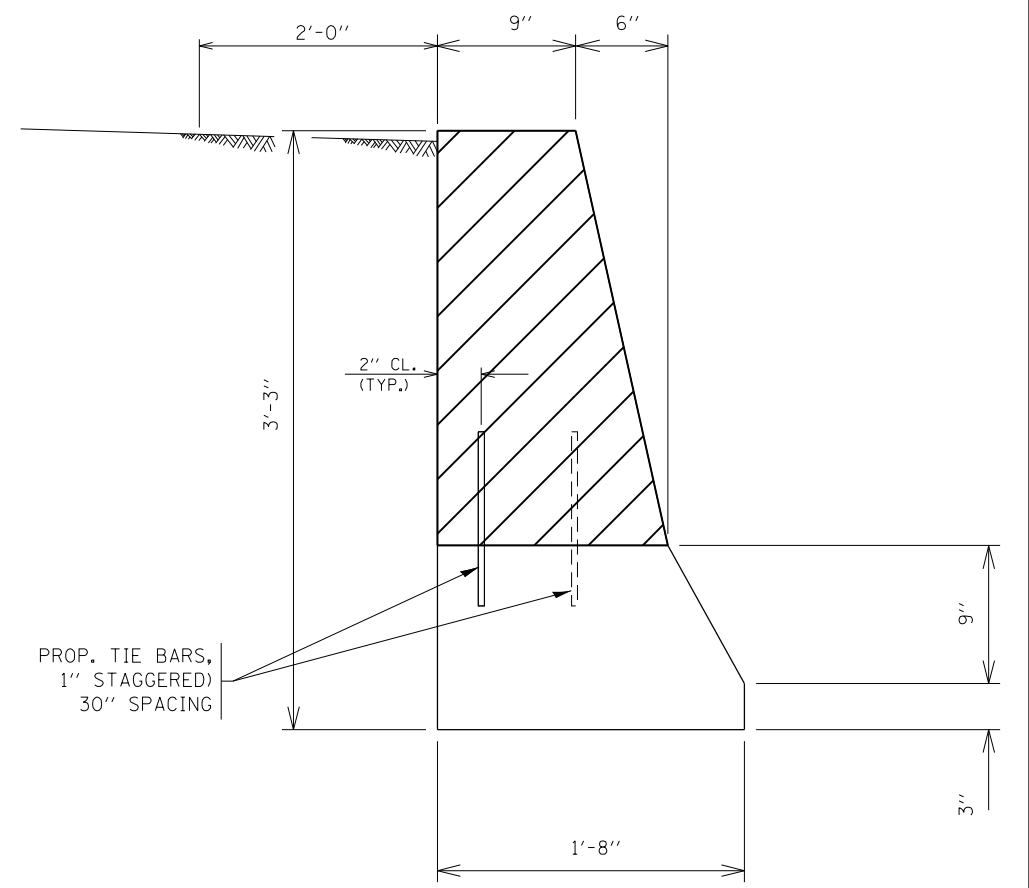


CITY OF CHICAGO
WEST CHICAGO TOWNSHIP
COOK COUNTY

LOCATION 13: I-90/I-94 (DAN RYAN EXPRESSWAY), NB
398' NORTH OF TAYLOR ST. OVERPASS, LEFT SIDE

BILL OF MATERIAL

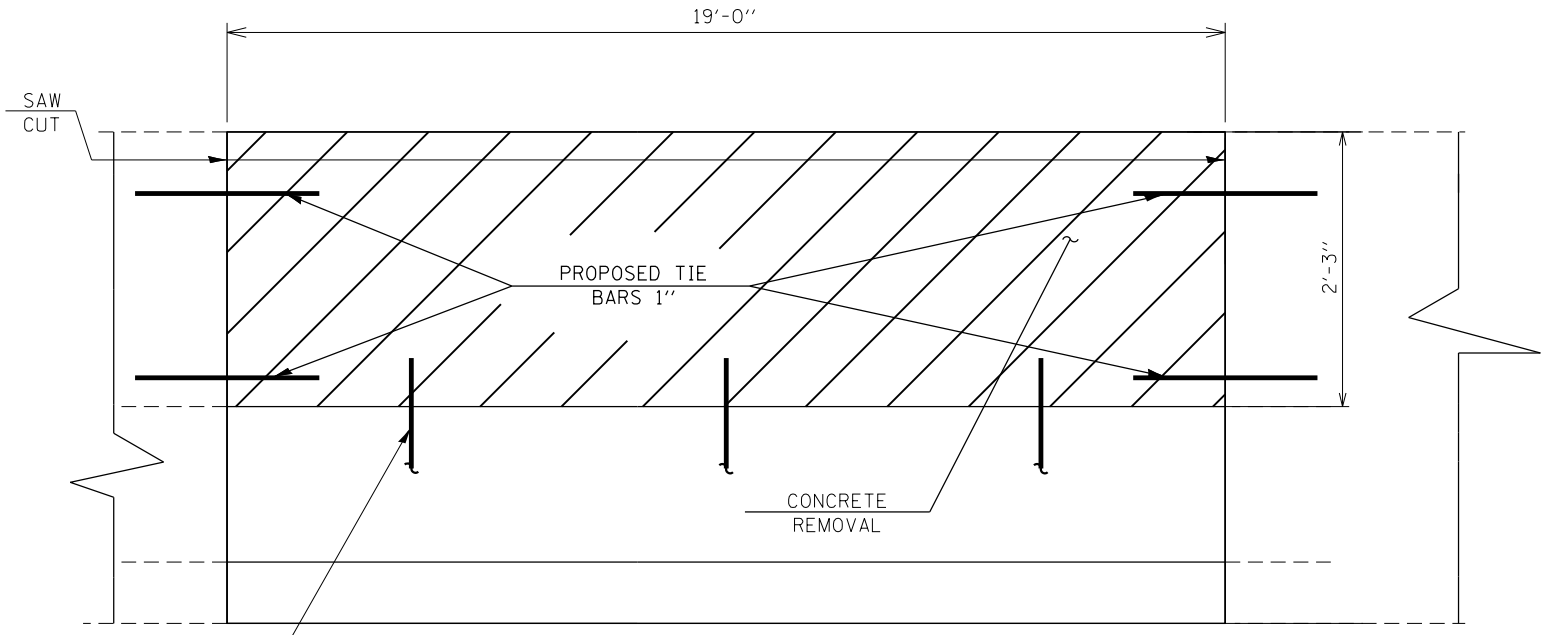
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	1.6
CONCRETE STRUCTURE	Cu. Yds.	1.6
TIE BARS	Each	12
SAW CUTS	Foot	24
EARTH EXCAVATION	Cu. Yds.	4.64
TEMPORARY EROSION CONTROL BLANKET	Sq. Yds.	4.2
TOPSOIL FURNISH AND PLACE, 4"	Sq. Yds.	4.2
SEEDING CLASS 2A	Acre	0.0009
PROTECTIVE COAT	Sq. Yds.	6.5



SECTION



HALF DEPTH REPAIR
REPLACE 19' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #6



ELEVATION

TRAFFIC DATA:
SPEED LIMIT: 45 MPH
2012 ADT: 211700

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
Default	Default	DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LOCATION 13
I-90/I-94 (DAN RYAN EXPRESSWAY)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	29
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

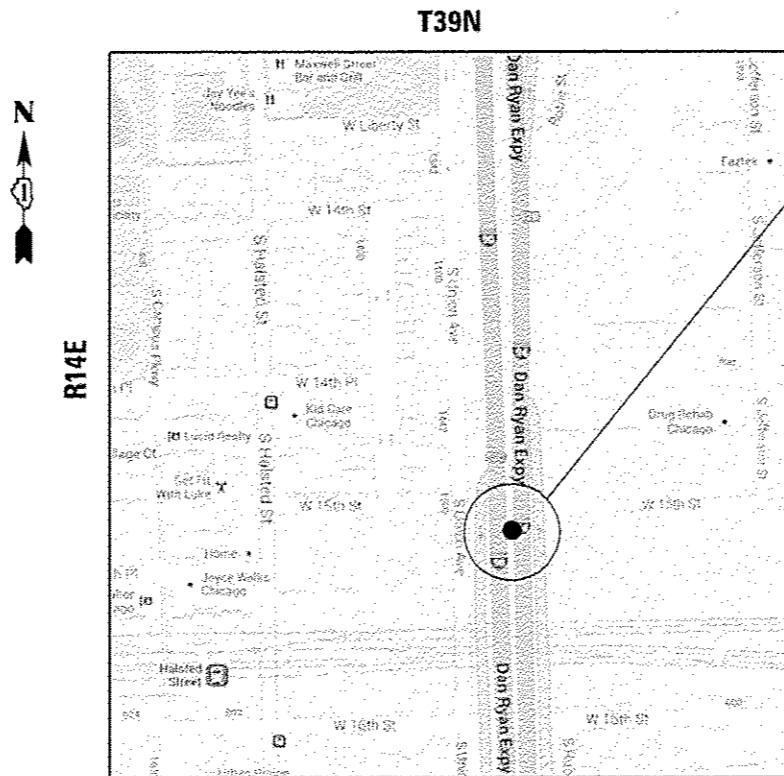
SCALE: SHEET OF SHEETS STA. TO STA.



CENTER PARAPET REPAIR FACING
NORTHBOUND PARAPET FACE

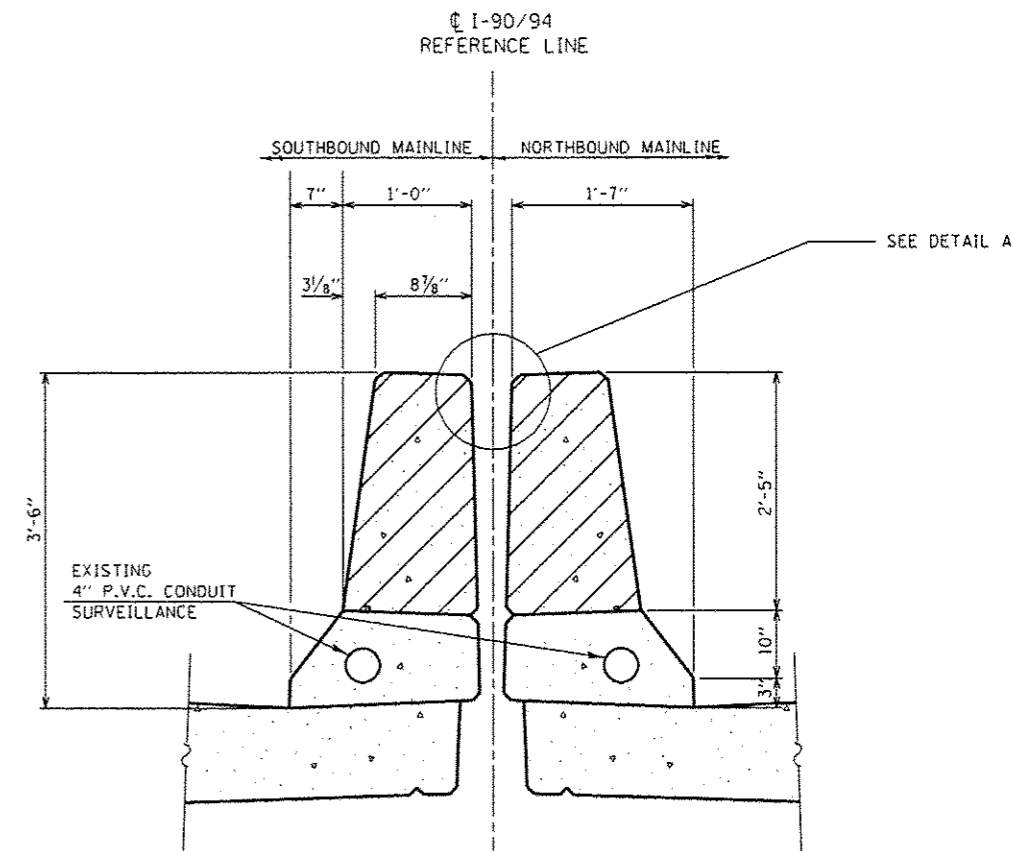


CENTER PARAPET REPAIR LOOKING SOUTH
FACING SOUTHBOUND PARAPET FACE
FOR MORE DETAILS, SEE SHEET #7



CITY OF CHICAGO
WEST CHICAGO TOWNSHIP
COOK COUNTY

LOCATION 14: I-90/I-94 (DAN RYAN EXPRESSWAY), NB AND SB
0.41 MILES SOUTH OF ROOSEVELT RD. OVERPASS, CENTER
PARAPET BOTH SIDES (SN 016-1110)

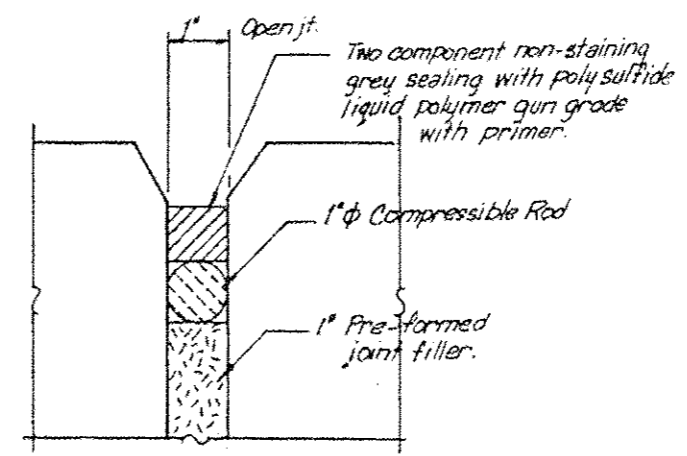


TRAFFIC DATA:

SPEED LIMIT: 55 MPH
2012 ADT: 253900



David Carl Puzey 8/7/13
Expires 11/30/14



MATERIALS SHOWN ARE TO BE INSTALLED
BETWEEN THE SOUTHBOUND AND NORTHBOUND
MEDIAN PARAPETS AND COST INCIDENTAL TO
CONCRETE SUPERSTRUCTURE.

DETAIL A
(EXISTING JOINT OPENING)

Notes:

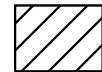
SEE SHEET NO. 27 FOR PARAPET DETAILS.
VERIFY 4\"/>

LEGEND

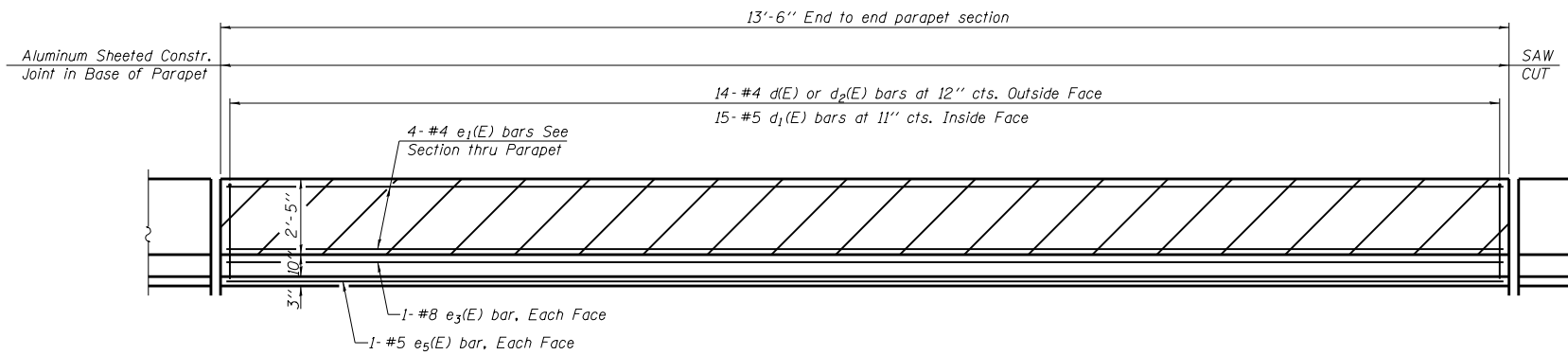
HATCHED AREA INDICATES
CONCRETE REMOVAL

FILE NAME:	USER NAME: SEYMORECP	DESIGNED: JAA	REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 14 (SN 016-1110) I-90/I-94 (DAN RYAN EXPRESSWAY)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\psidot\seymorecp\0348982\016013-shr-plan.dgn		DRAWN:	REVISED:	SCALE:	SHEET 1 OF 2 SHEETS	VAR.	2013-041J	VARIOUS	44	30
Default	PLOT SCALE: 100.0000 1/16"	CHECKED: VVV	REVISED:	DATE:	STA. TO STA.	CONTRACT NO. 60W86				
	PLOT DATE: 7/11/2013	DATE:	REVISED:			ILLINOIS FED. AID PROJECT				

LEGEND



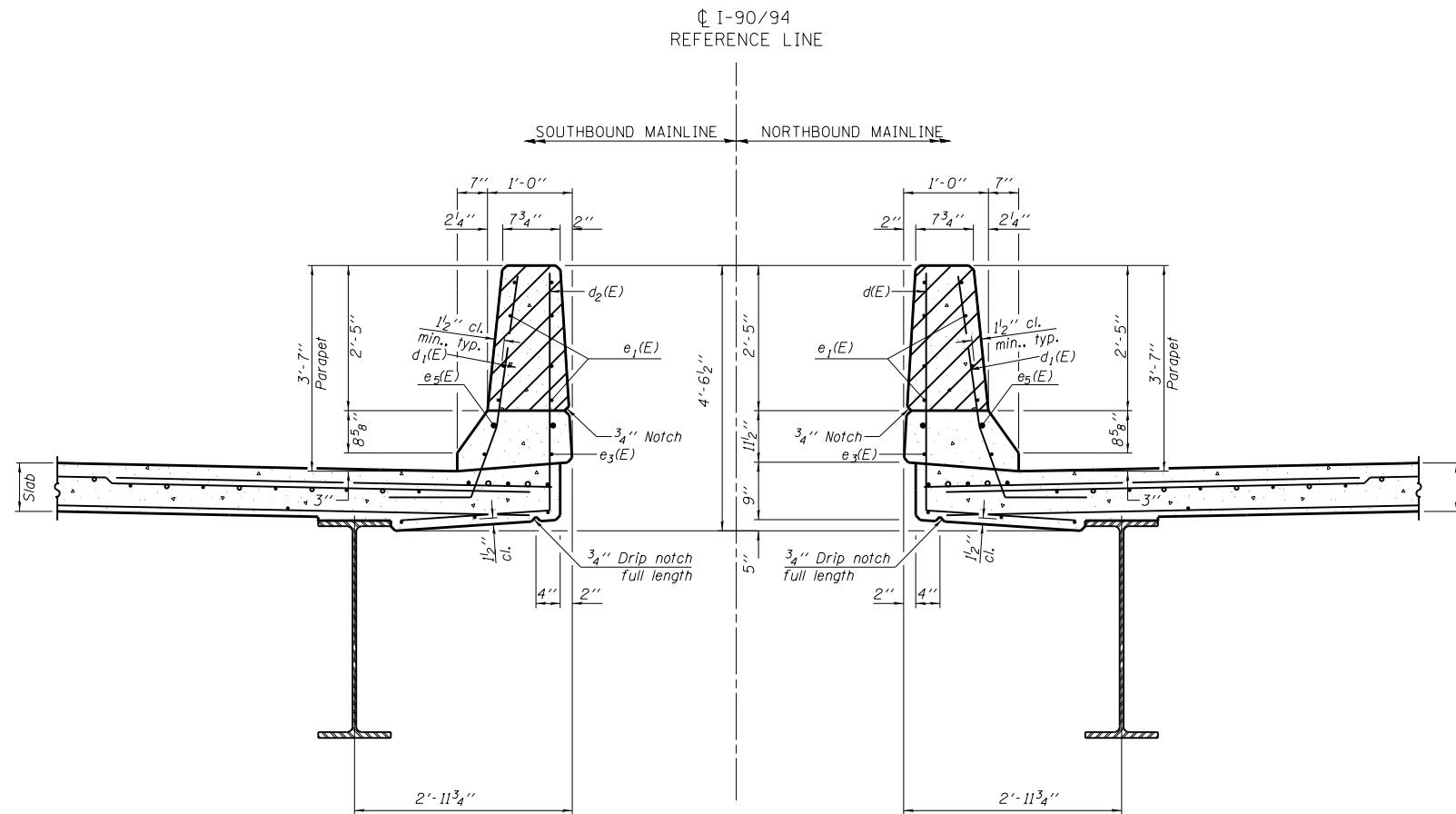
HATCHED AREA INDICATES CONCRETE REMOVAL



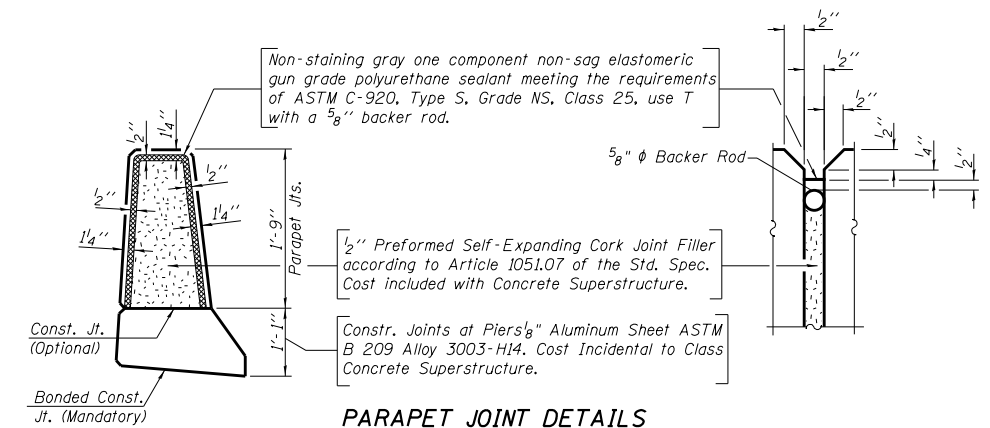
ELEVATION

**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
e ₁ (E)	16	#4	13'-4"	
REINFORCEMENT BARS, EPOXY COATED			Pound	143
CONCRETE SUPERSTRUCTURE			Cu. Yds.	3.31
CONCRETE REMOVAL			Cu. Yds.	3.31
SAW CUTS			Foot	37
PROTECTIVE COAT			Sq. Yds.	12.9



EXISTING SECTION THRU PARAPETS



PARAPET JOINT DETAILS

**PARAPET DETAILS
STRUCTURE NO. 016-1110**

NOTE:
All edges shall have 3/4" chamfer.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOCATION 14 (SN 016-1110) I-90/I-94 (DAN RYAN EXPRESSWAY)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	CHECKED -	REVISED -			VAR.	2013-0411	VARIOUS	44	31	
	Plot Date = 7/11/2013	DATE -	REVISED -			CONTRACT NO. 60W86					
						ILLINOIS FED. AID PROJECT					

T38N

R14E



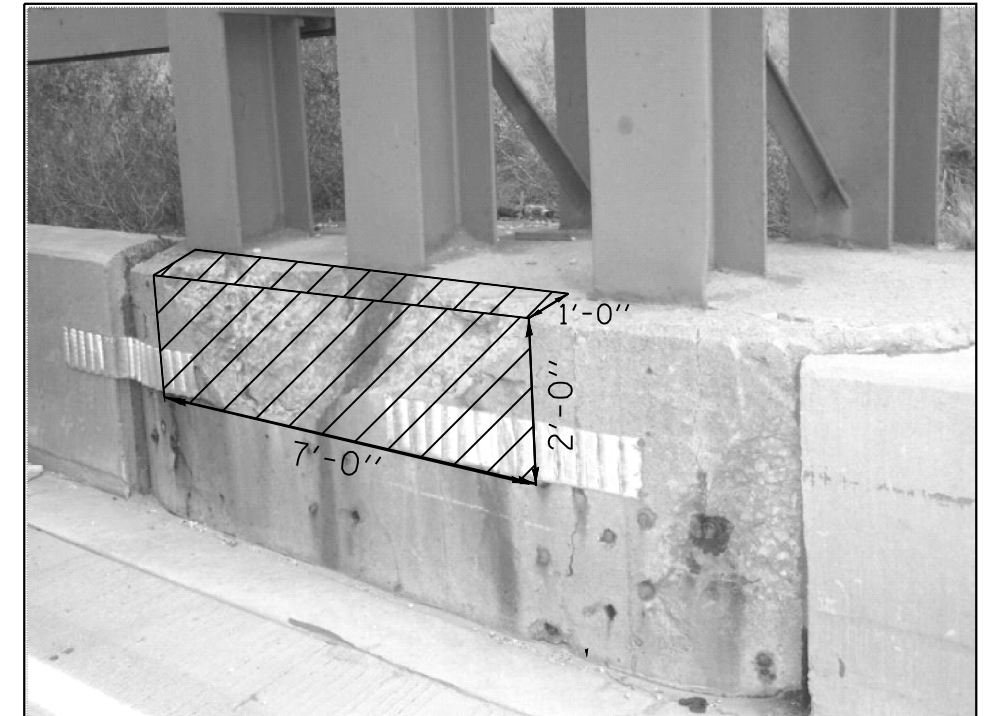
LOCATION 15: I-90/I-94 (DAN RYAN EXPRESSWAY), SB
 204' SOUTH OF 59TH ST. OVERPASS IN THE LOCALS,
 RIGHT SIDE



TRAFFIC DATA:

SPEED LIMIT: 55 MPH
 2012 ADT: 287700

**CITY OF CHICAGO
 LAKE TOWNSHIP
 COOK COUNTY**

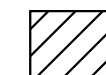


STRUCTURAL REPAIR OF CONCRETE > 5"

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
STRUCTURAL REPAIR OF CONCRETE >5"	Sq. Ft.	14
PROTECTIVE COAT	Sq. Yds.	2.33

LEGEND



HATCHED AREA INDICATES
 CONCRETE REMOVAL

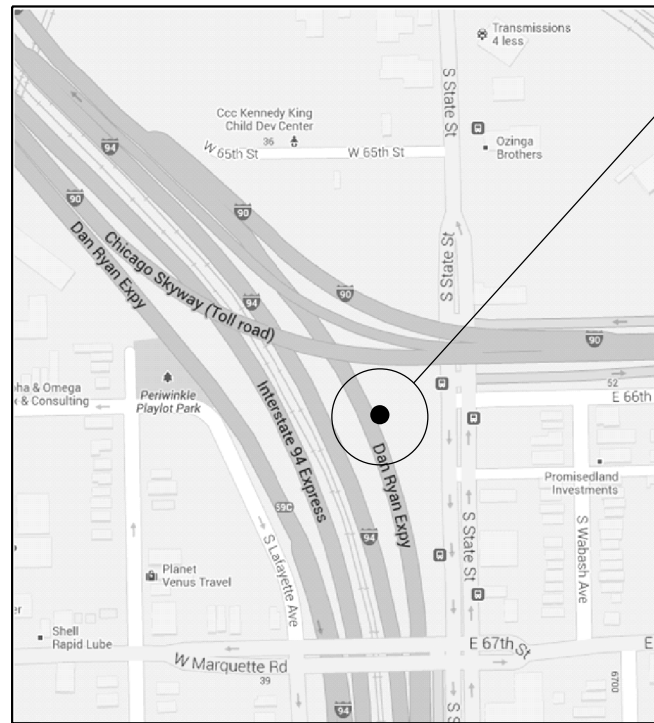
FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

LOCATION 15			
I-90/I-94 (DAN RYAN EXPRESSWAY)			
SCALE:	SHEET	OF	SHEETS
		STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	32
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

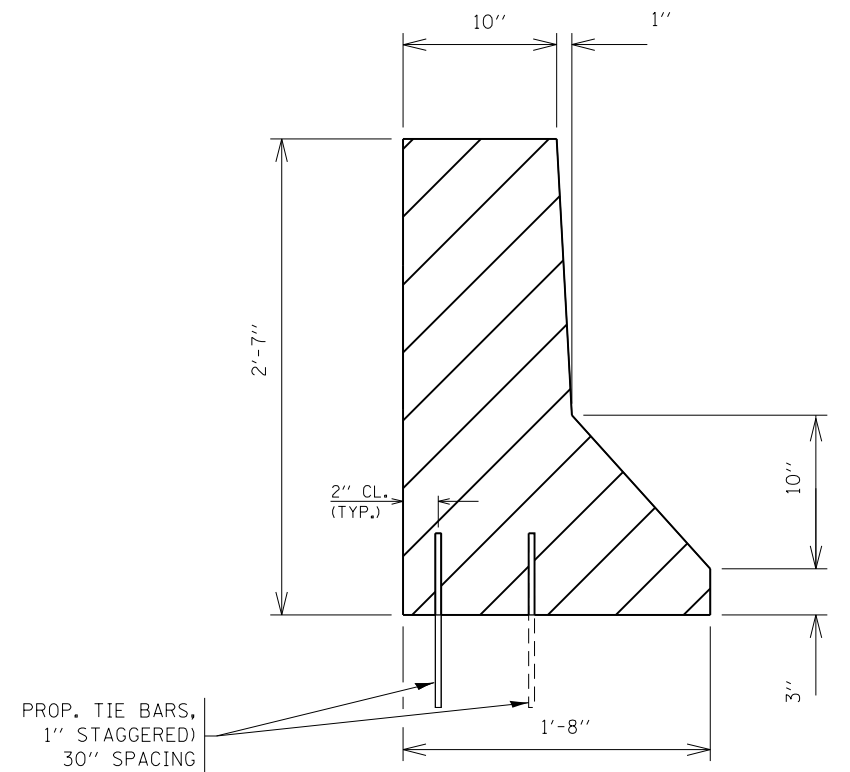
T38N



LOCATION 16: I-90/I-94 (DAN RYAN EXPRESSWAY), NB
 457' NORTH OF 67TH ST. OVERPASS IN THE LOCALS,
 RIGHT SIDE

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	1.6
CONCRETE STRUCTURE	Cu. Yds.	1.7
TIE BARS	Each	13
SAW CUTS	Foot	19
PROTECTIVE COAT	Sq. Yds.	4.2



SECTION

**CITY OF CHICAGO
 LAKE TOWNSHIP
 COOK COUNTY**



FULL DEPTH REPAIR
 REPLACE 16' OF BARRIER WALL
 FOR MORE DETAILS, SEE SHEET #5

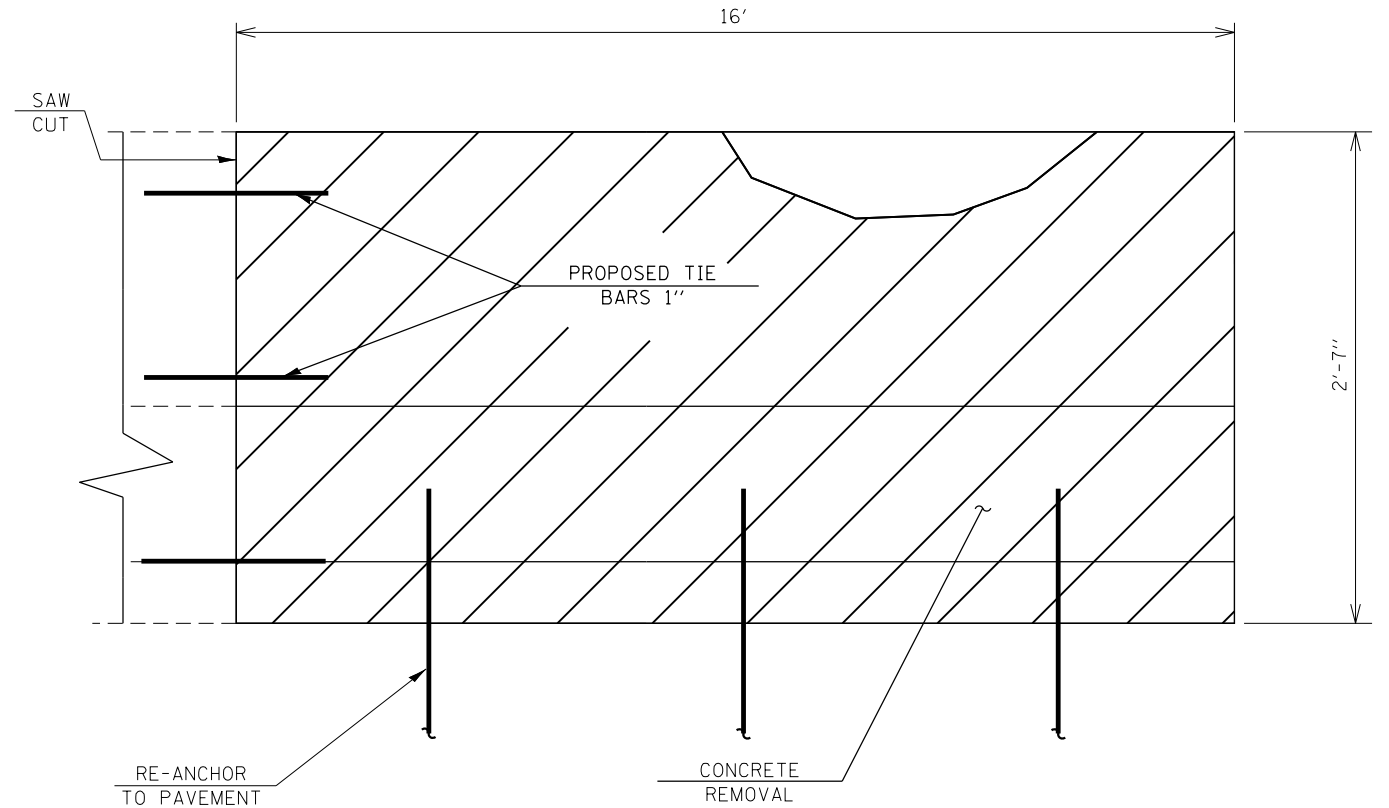
TRAFFIC DATA:

SPEED LIMIT: 55 MPH
 2012 ADT: 241700

Notes:

DISCONNECT AND RECONNECT EXISTING
 GUARDRAIL.

COST INCLUDED IN THE COST OF
 "CONCRETE STRUCTURE".



ELEVATION

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
et:\pw\work\p\dot\seymorecp\d0348982\0189013-sh-t-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

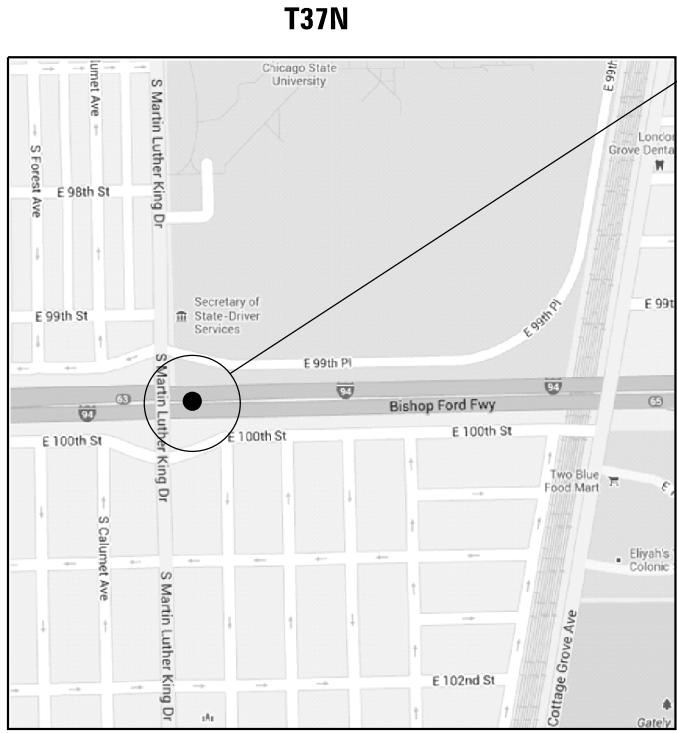
**LOCATION 16
 I-90/I-94 (DAN RYAN EXPRESSWAY)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	33
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				



R14E



LOCATION 17: I-90/I-94 (DAN RYAN EXPRESSWAY), WB
171' EAST OF KING DR. OVERPASS, LEFT SIDE

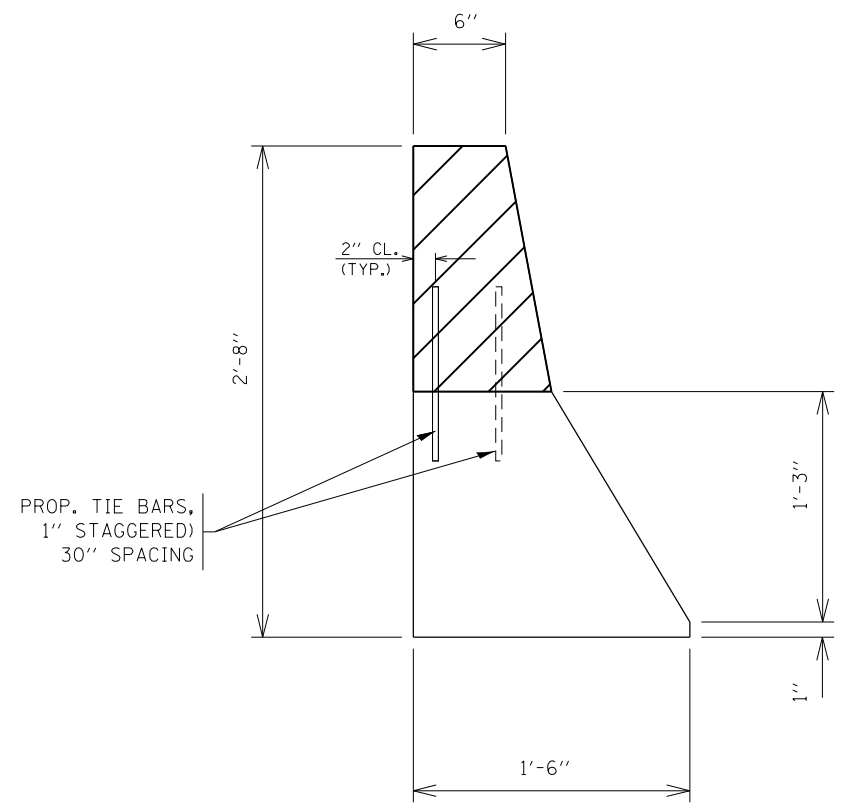
**CITY OF CHICAGO
HYDE PARK TOWNSHIP
COOK COUNTY**



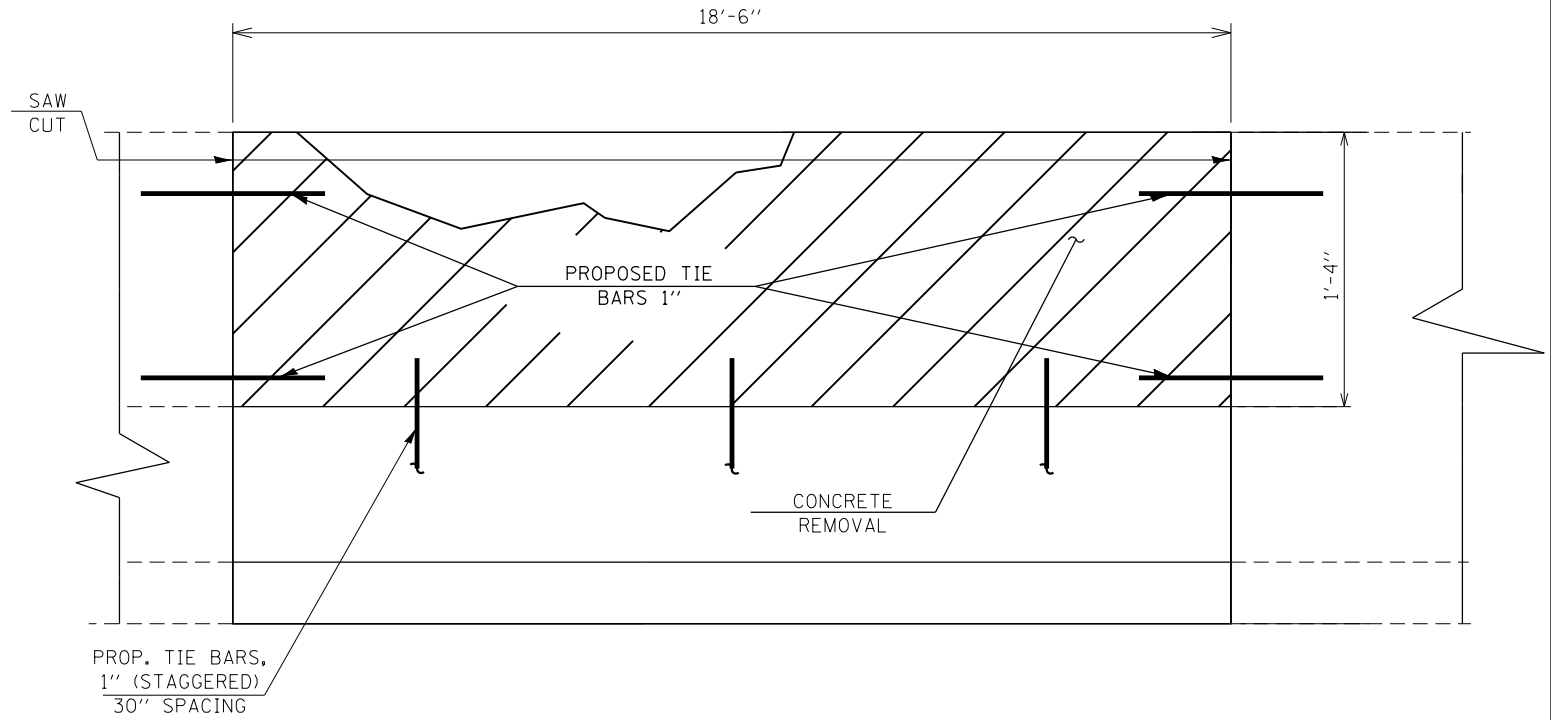
HALF DEPTH REPAIR
REPLACE 18 1/2' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #6

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	0.3
CONCRETE STRUCTURE	Cu. Yds.	0.6
TIE BARS	Each	12
SAW CUTS	Foot	21
PROTECTIVE COAT	Sq. Yds.	3.9



SECTION



ELEVATION

TRAFFIC DATA:
SPEED LIMIT: 55 MPH
2012 ADT: 153900

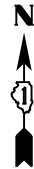
FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LOCATION 17
I-90/I-94 (DAN RYAN EXPRESSWAY)**

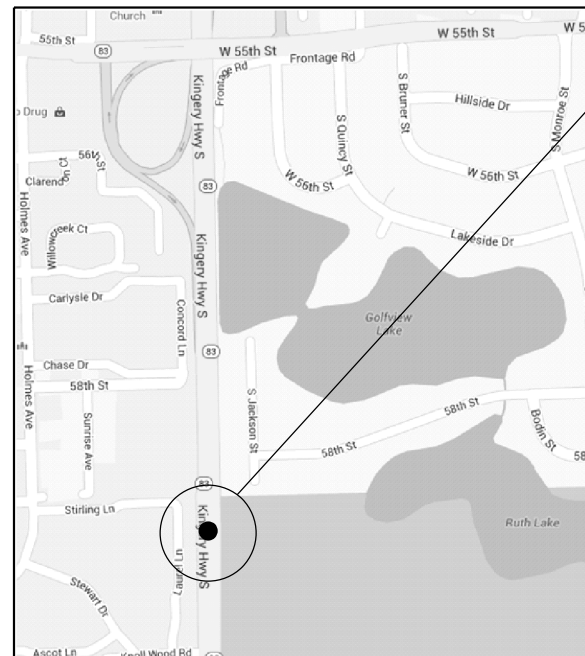
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	34
CONTRACT NO. 60W86			ILLINOIS FED. AID PROJECT	



R11E

T38N



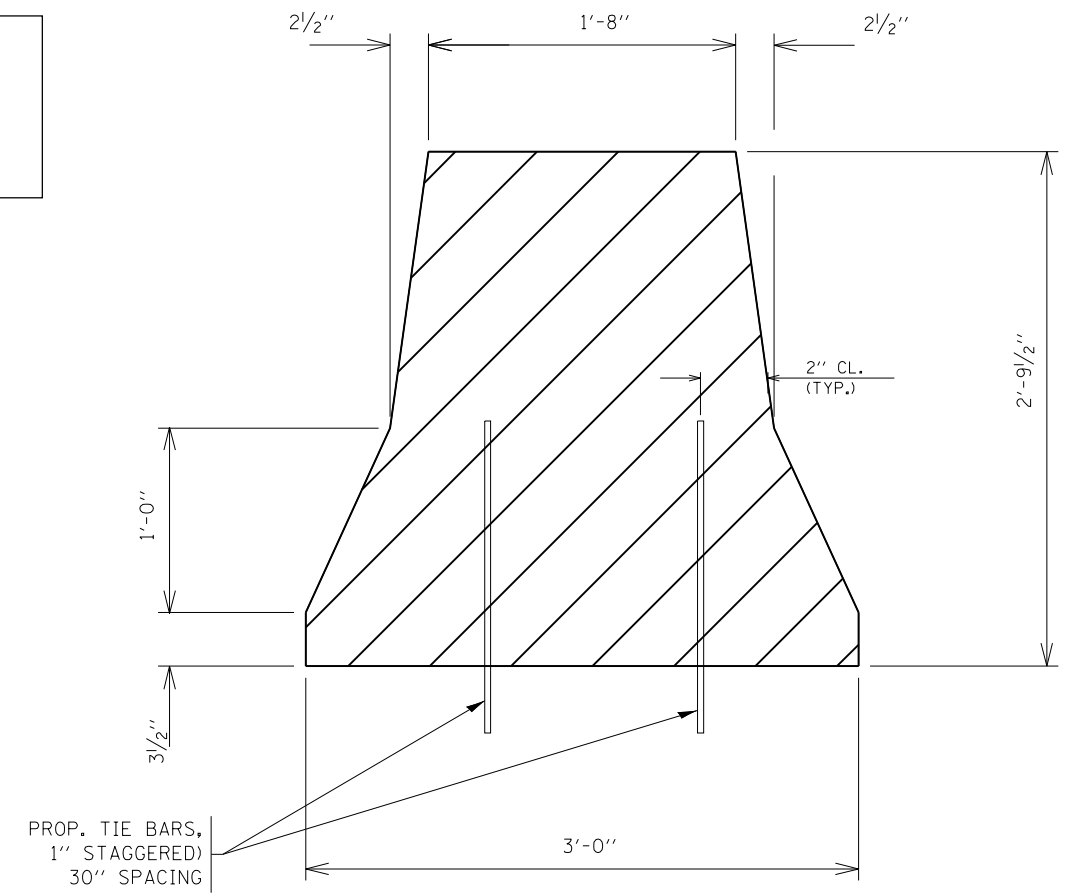
LOCATION 18: IL 83 (KINGERY HIGHWAY), SB

0.56 MILES SOUTH OF 55TH ST. OVERPASS, LEFT SIDE

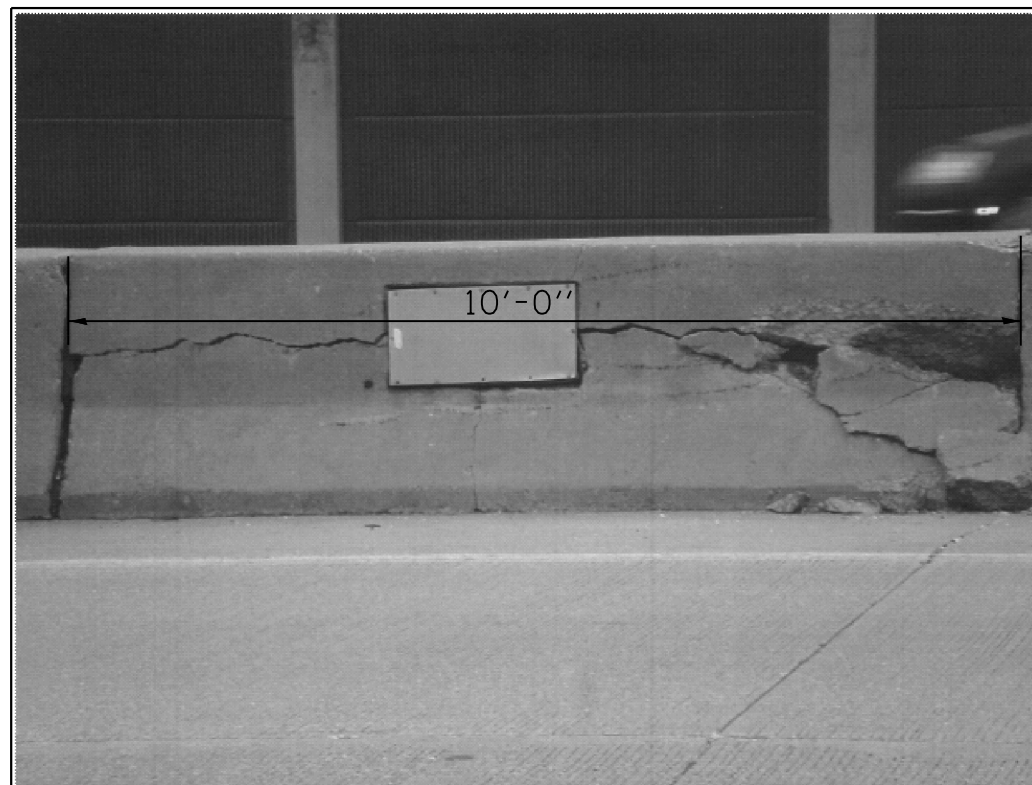
**VILLAGE OF HINSDALE
DOWNER'S GROVE TOWNSHIP
DUPAGE COUNTY**

BILL OF MATERIAL

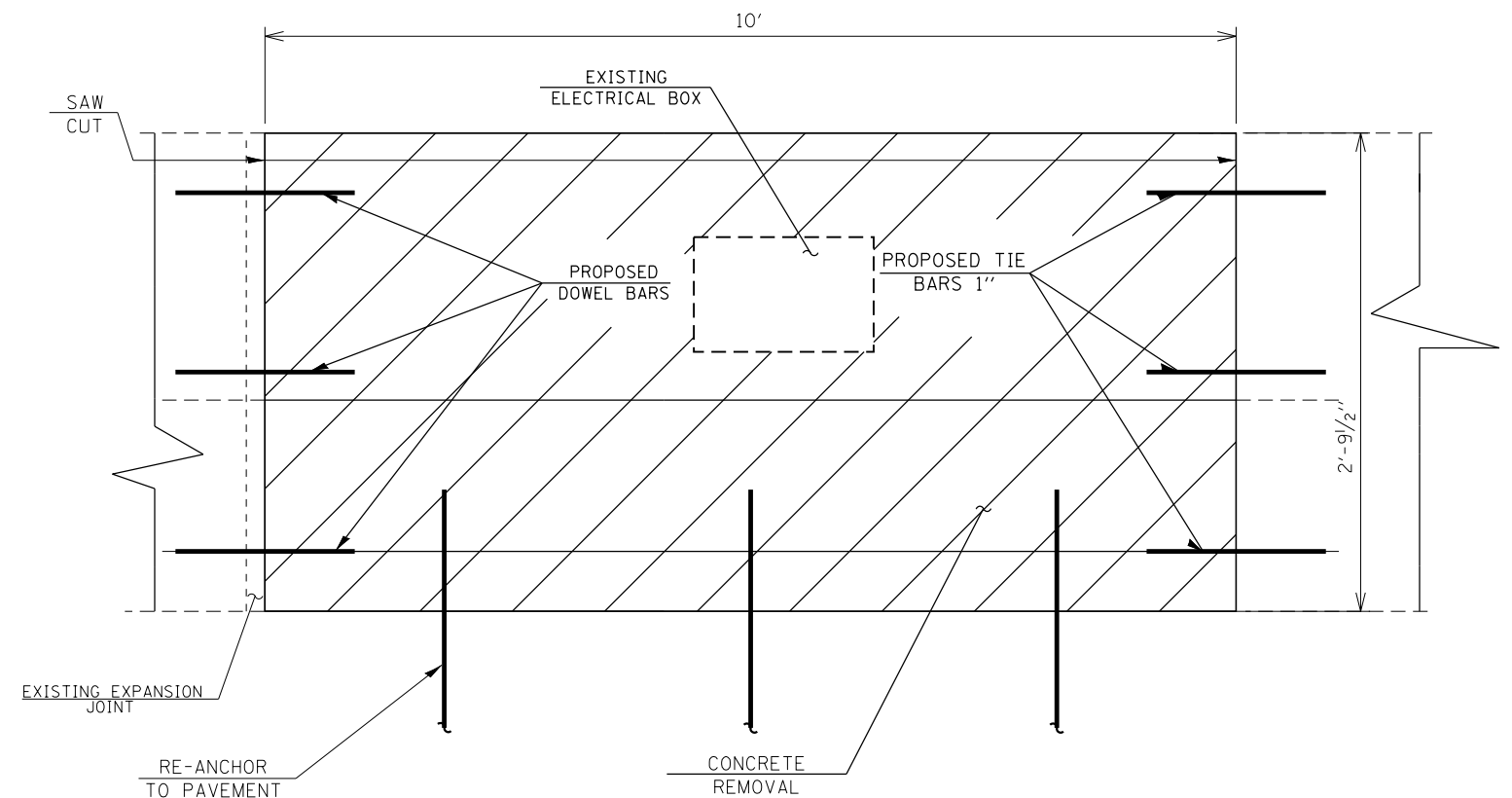
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	2.3
CONCRETE STRUCTURE	Cu. Yds.	2.3
TIE BARS	Each	7
DOWEL BARS	Each	3
SAW CUTS	Foot	6
PROTECTIVE COAT	Sq. Yds.	8.4



SECTION



FULL DEPTH REPAIR
REPLACE 10' OF BARRIER WALL
FOR MORE DETAILS, SEE SHEET #5



ELEVATION

TRAFFIC DATA:

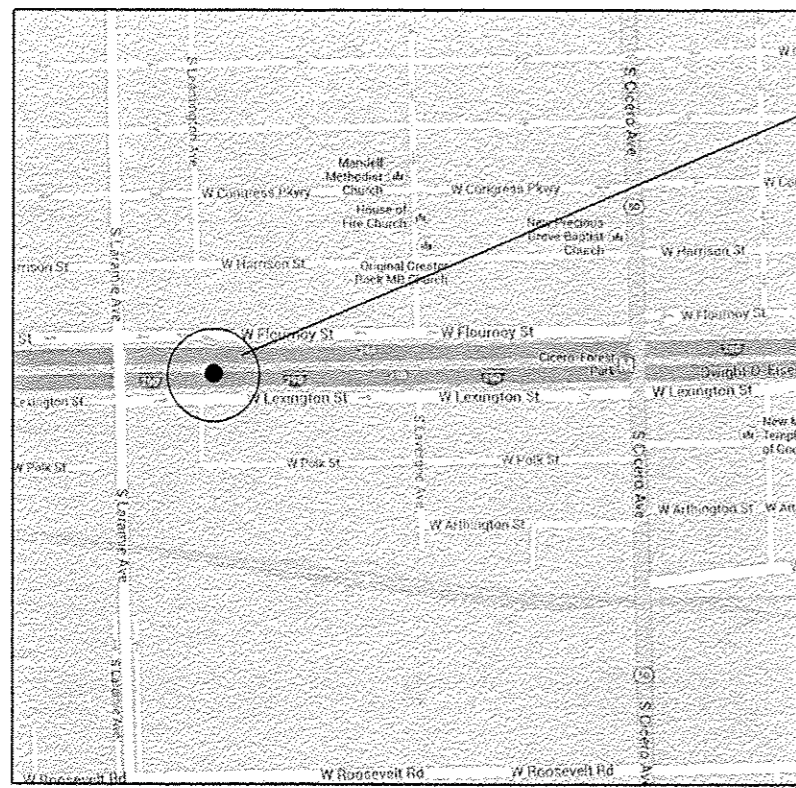
SPEED LIMIT: 55 MPH
2012 ADT: 58900

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED -
ct:\pw\work\p\d0348982\0189013-sh-t-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LOCATION 18 IL 83 (KINGERY HIGHWAY)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-0411	VARIOUS	44	35
			CONTRACT NO. 60W86	
ILLINOIS FED. AID PROJECT				

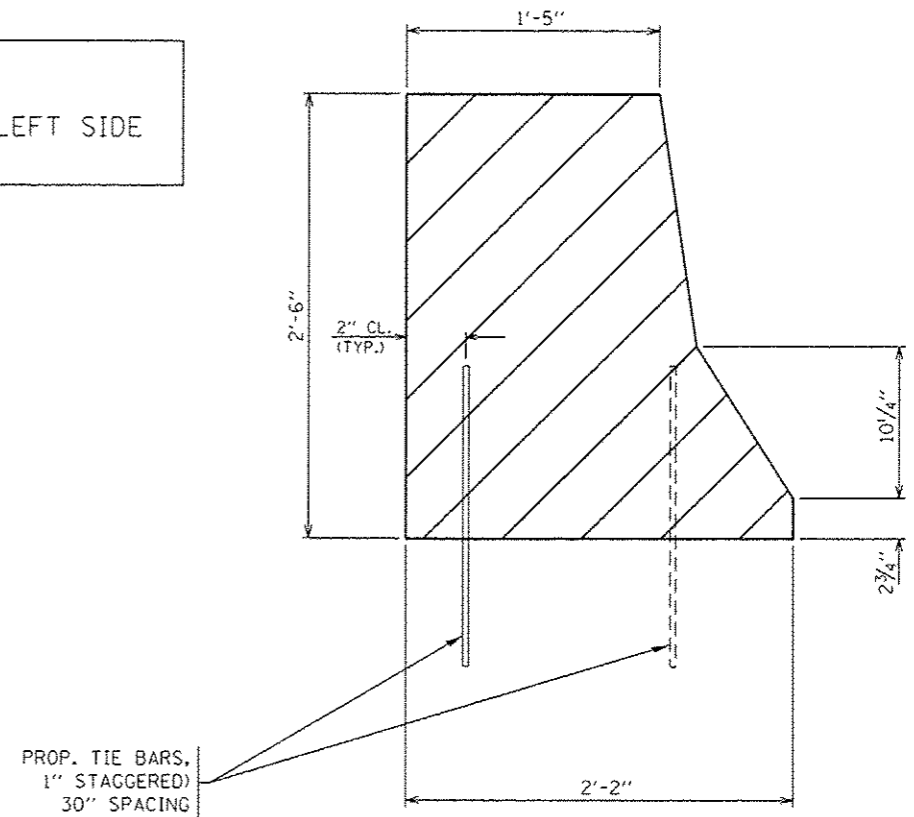


LOCATION 19: I-290 (EISENHOWER EXPRESSWAY), EB
 0.4 MILES WEST OF CICERO AVENUE OVERPASS, LEFT SIDE

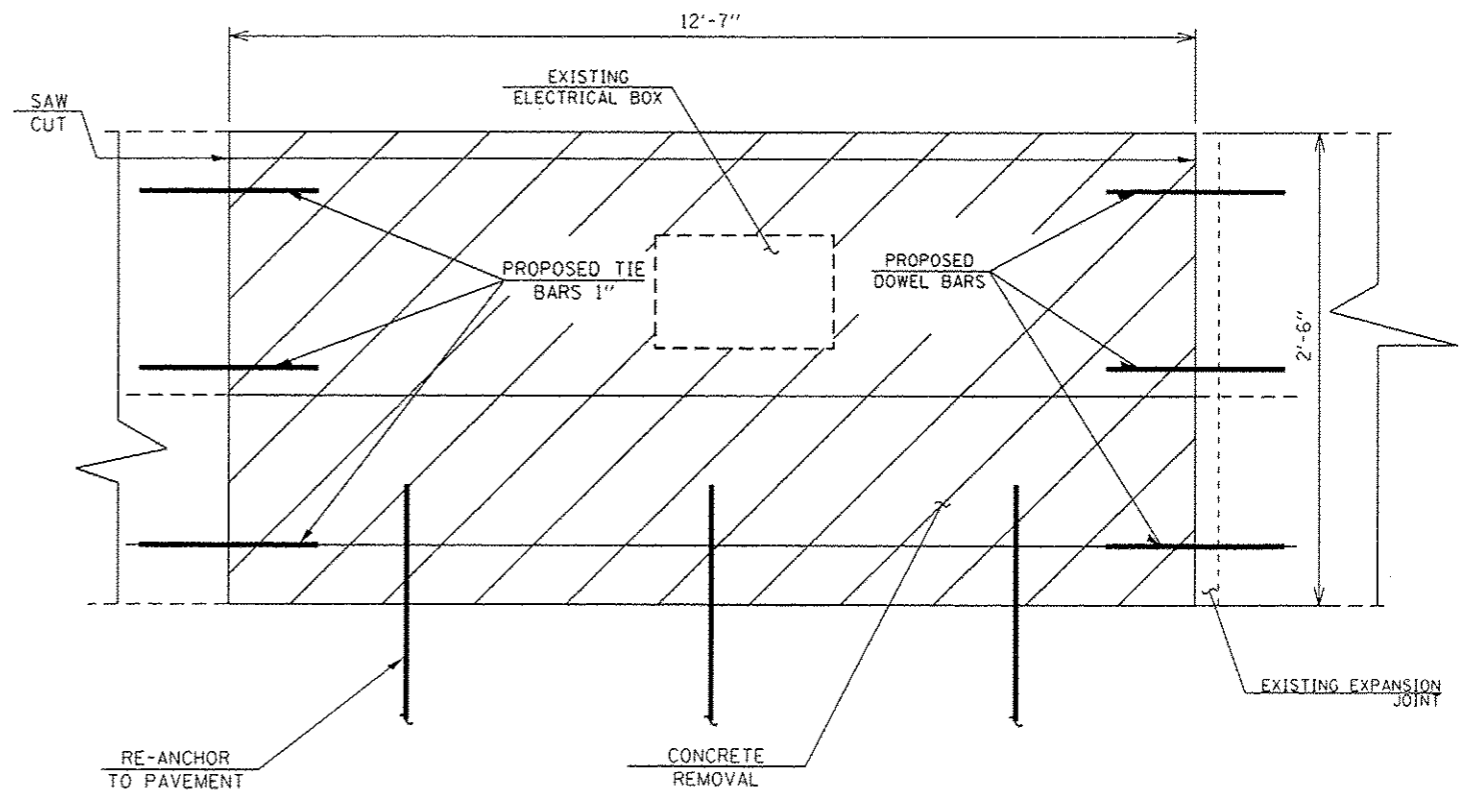
**CITY OF CHICAGO
 WEST CHICAGO TOWNSHIP
 COOK COUNTY**

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	Cu. Yds.	2.0
CONCRETE STRUCTURE	Cu. Yds.	2.0
TIE BARS	Each	9
DOWEL BARS	Each	3
SAW CUTS	Foot	5
PROTECTIVE COAT	Sq. Yds.	5.7



FULL DEPTH REPAIR
 REPLACE 12'-7" OF BARRIER WALL
 FOR MORE DETAILS, SEE SHEET #5



TRAFFIC DATA:

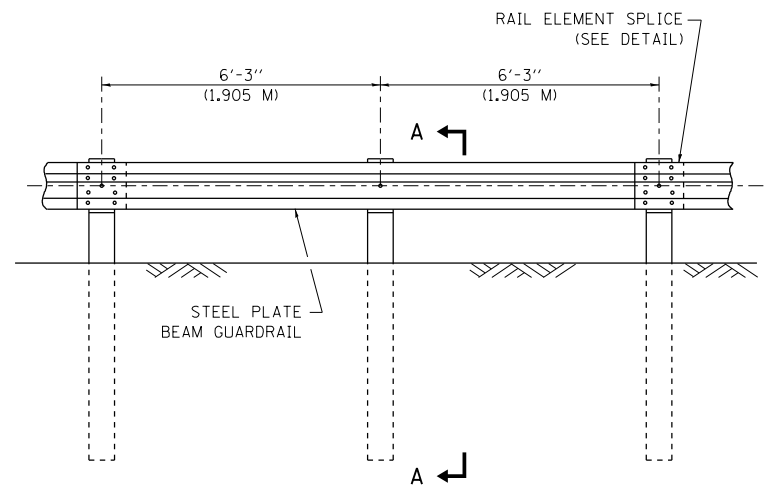
SPEED LIMIT: 55 MPH
 2012 ADT: 196700

ELEVATION

FILE NAME *	USER NAME = SEYMORECP	DESIGNED -	REVISED -
c:\p\work\avidot\seymorecp\08240982101	0813-abt-plandgn	DRAWN -	REVISED -
Default	PLOT SCALE = 1/8" = 1' / in.	CHECKED -	REVISED -
	PLOT DATE = 7/19/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

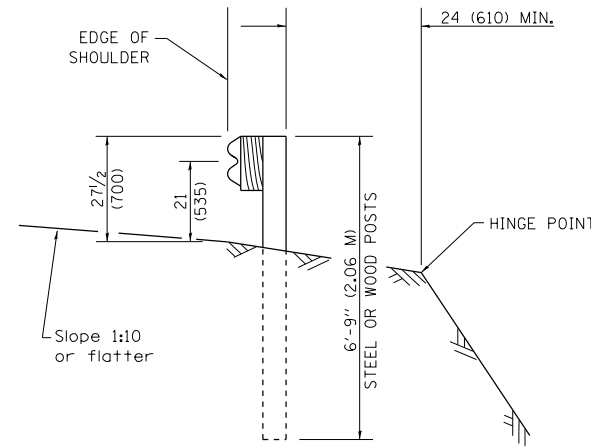
LOCATION 19		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-290 (EISENHOWER EXPRESSWAY)		VAR.	2013-0411	VARIOUS	44	35A
SCALE: 1" = 10'		SHEET OF SHEETS		STA. TO STA.		CONTRACT NO. 60W86
ILLINOIS FED. AID PROJECT						



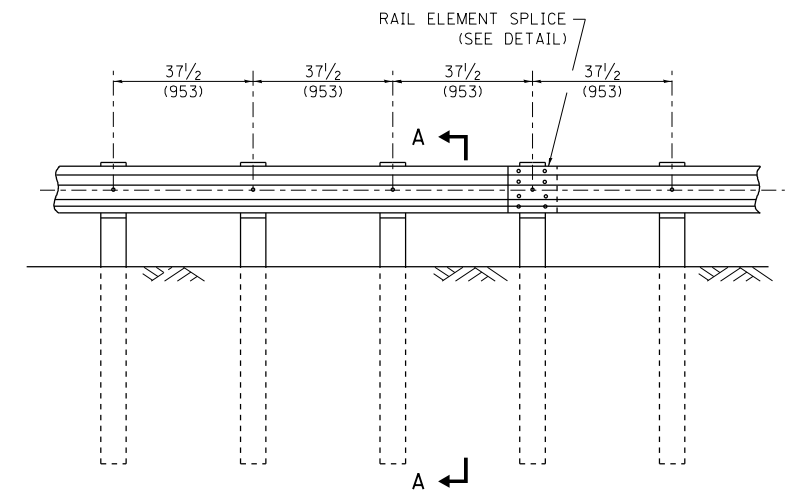
ELEVATION

TYPE A

6'-3" (1.905 M) TYPICAL POST SPACING



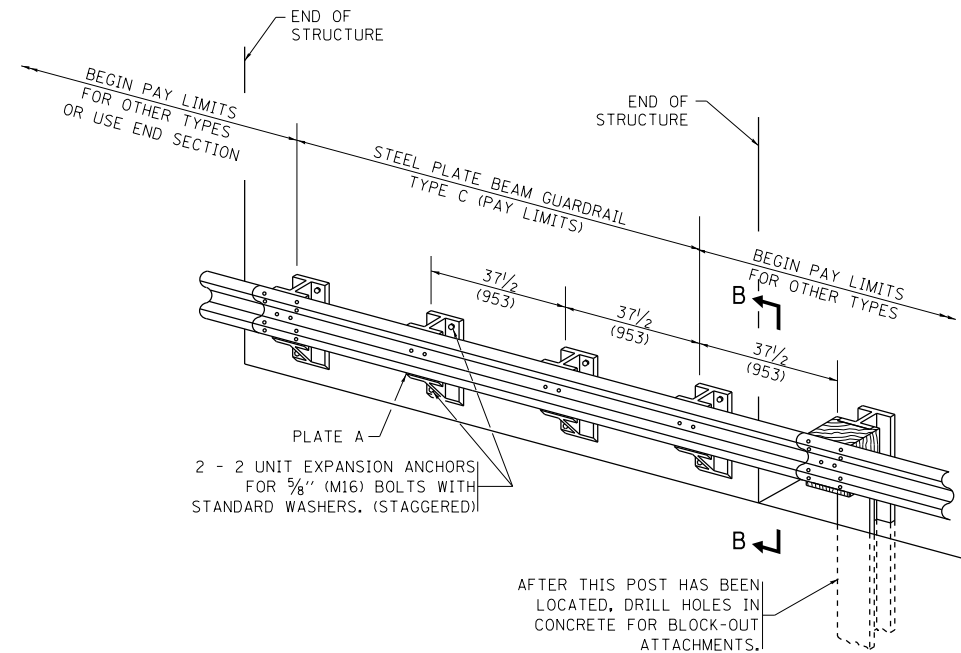
SECTION A-A



ELEVATION

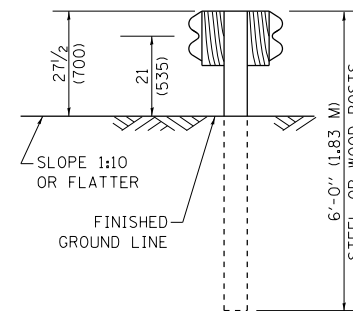
TYPE A

37 1/2 (953) CLOSED POST SPACING

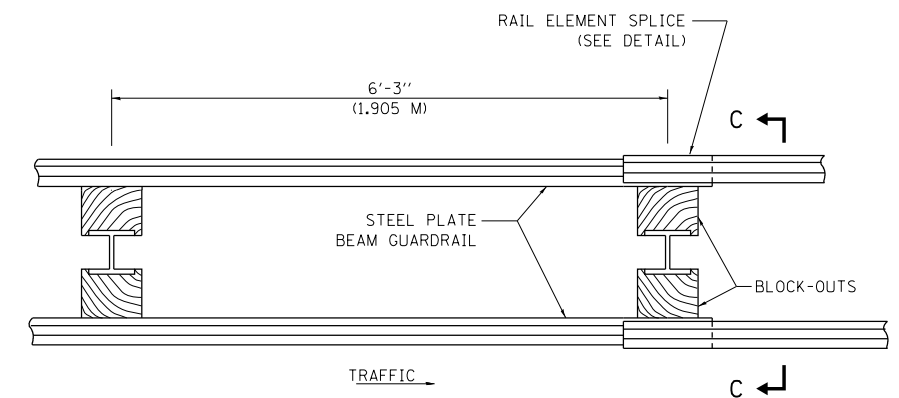


TYPE C

37 1/2 (953) BLOCK-OUT SPACING



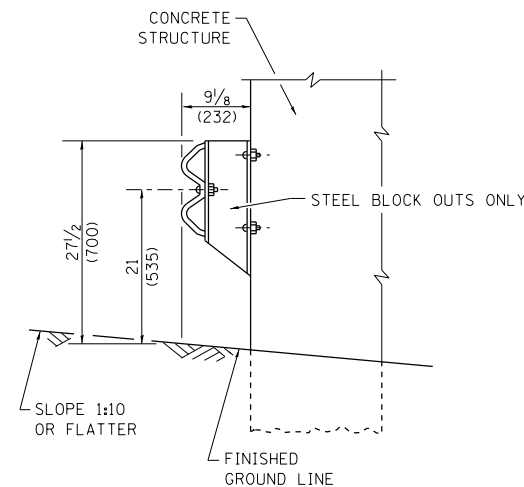
SECTION C-C



PLAN

TYPE D

DOUBLE STEEL PLATE BEAM GUARDRAIL
6'-3" (1.905 M) TYPICAL POST SPACING



SECTION B-B

GENERAL NOTES

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.

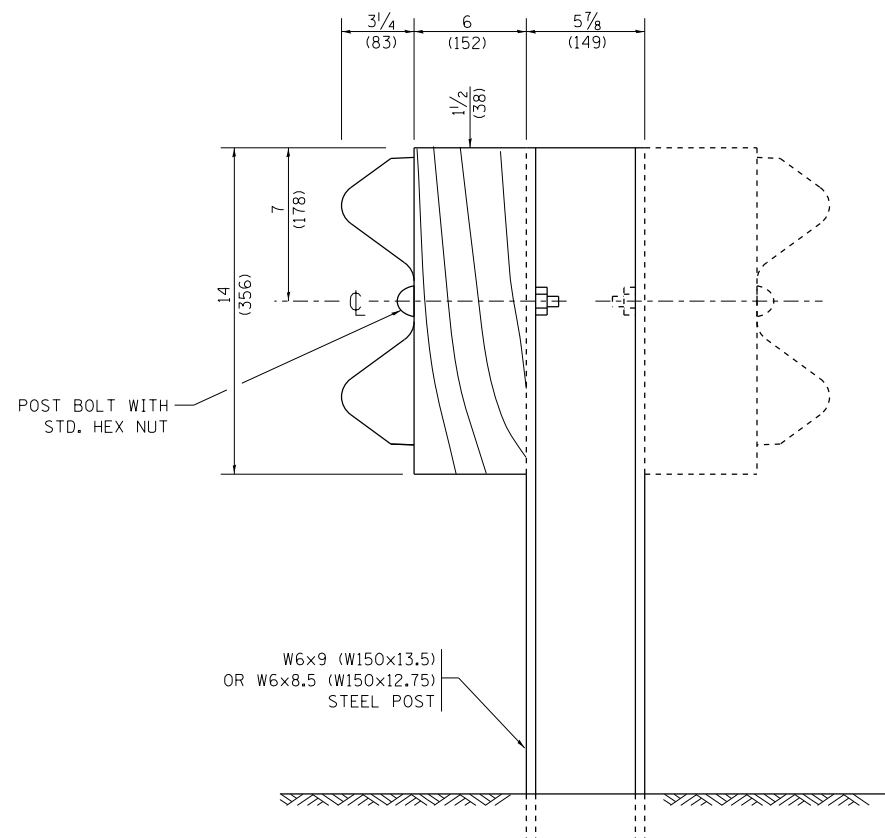
THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - 10-31-06
et:\pw\work\p\id\seymorecp\d0348982\Std.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

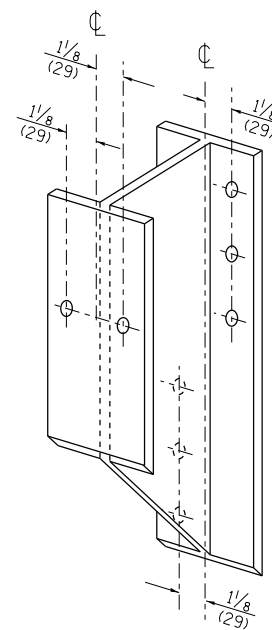
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			
SCALE: NONE	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.

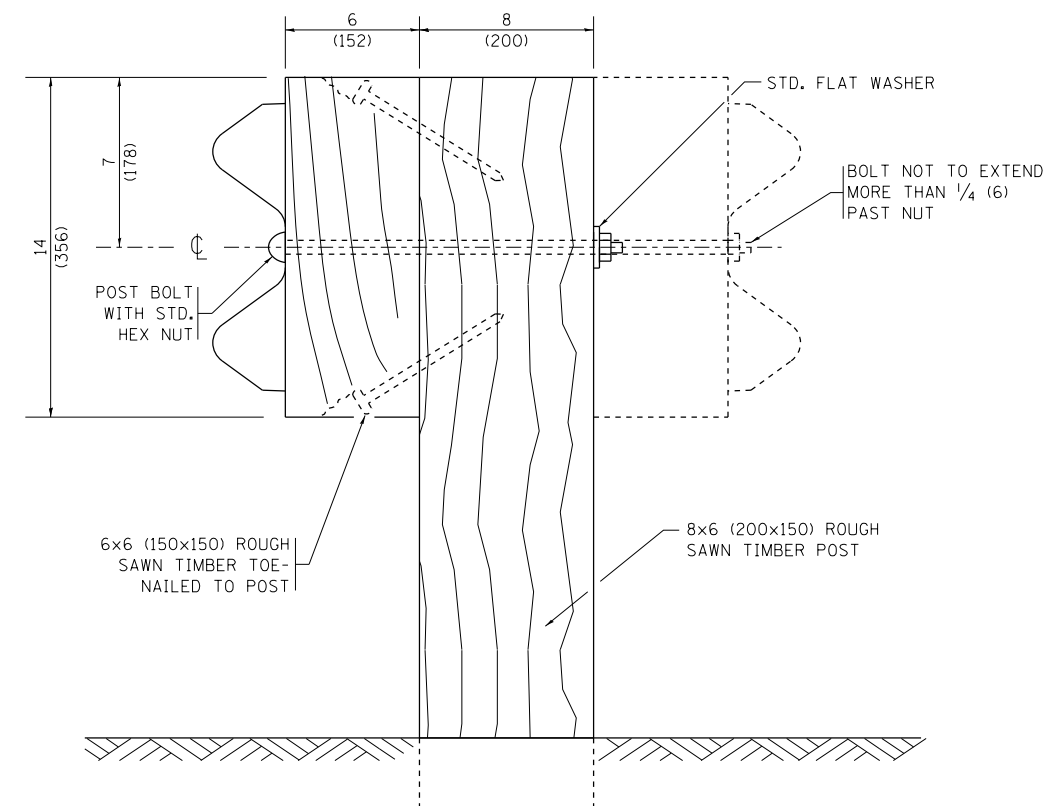
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	36
	BM-21	CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



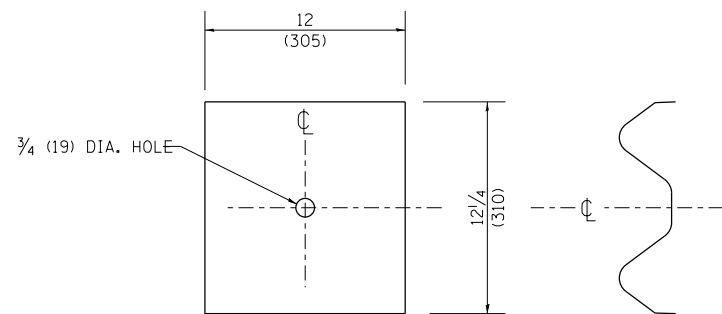
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



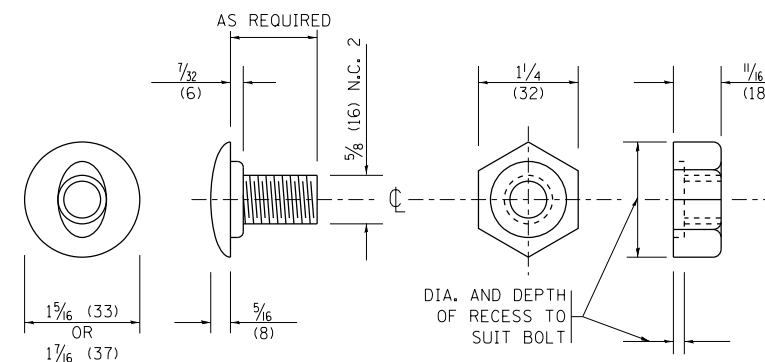
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



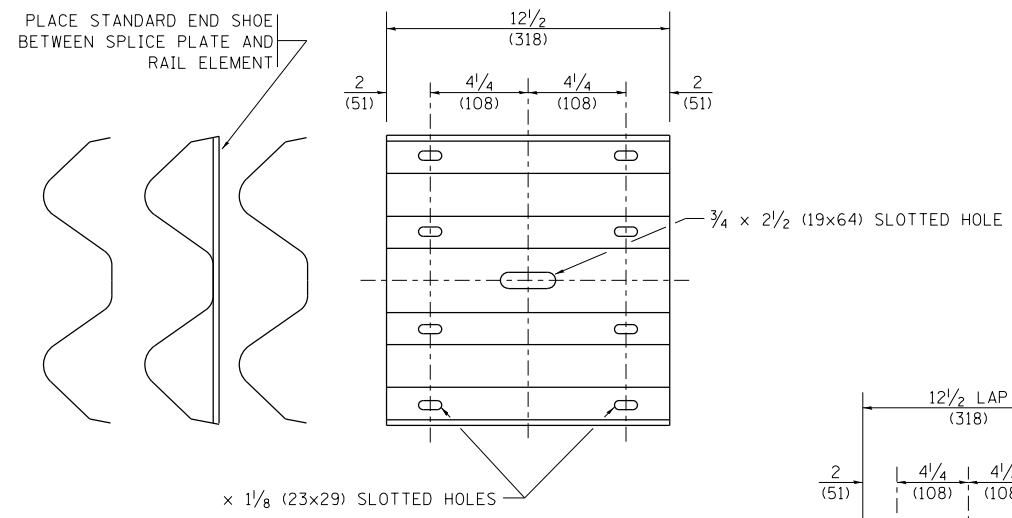
POST OR SPLICE BOLT & NUT

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - 10-31-06
et:\pw\work\p\dot\seymorecp\d0348982\dot\Std.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

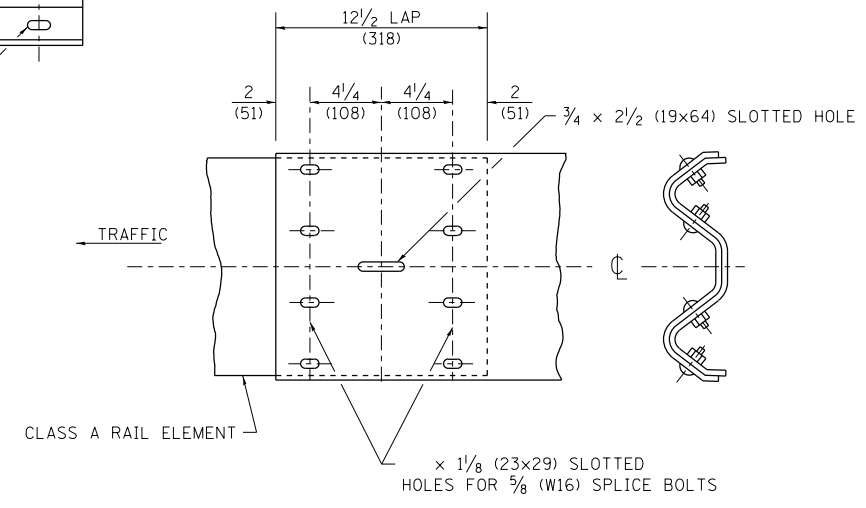
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			
SCALE: NONE	SHEET NO. 2	OF 4 SHEETS	STA. TO STA.

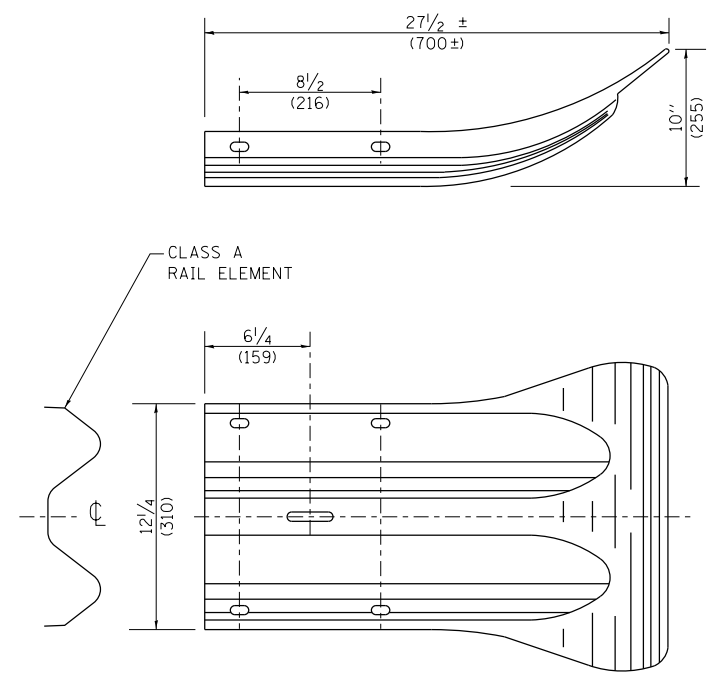
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	37
BM-21		CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



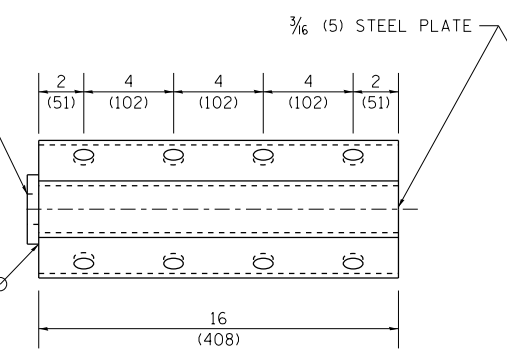
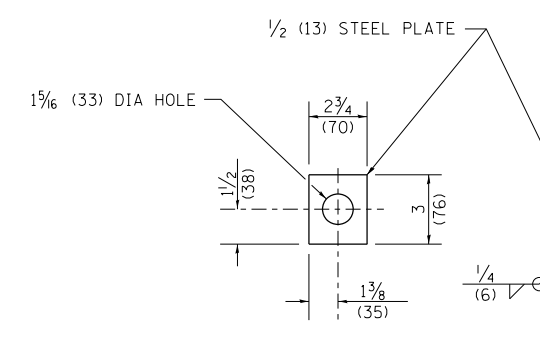
SPLICE PLATE



RAIL ELEMENT SPLICE



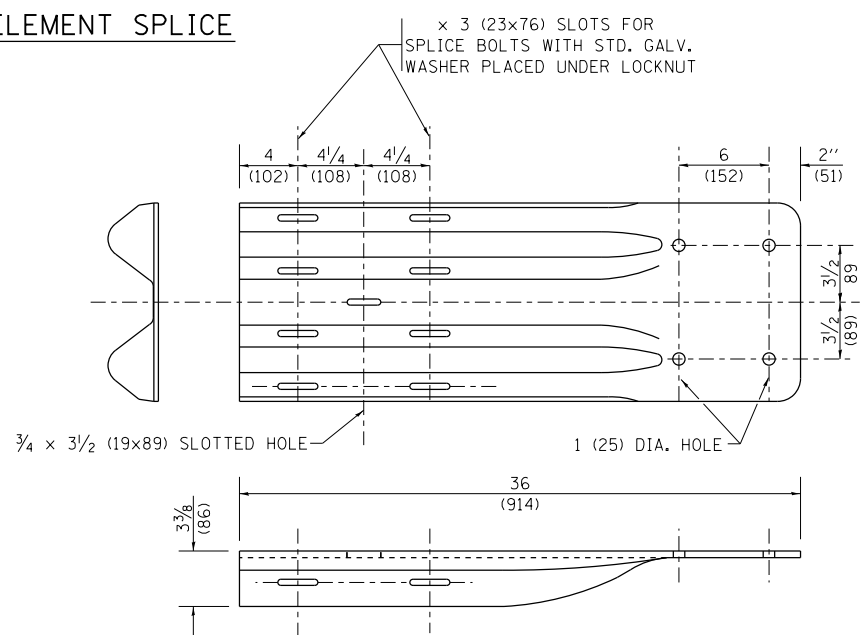
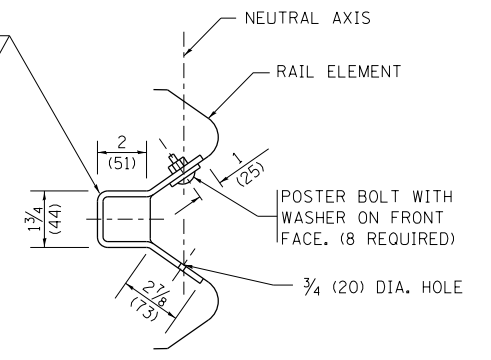
END SECTION



NOTE:

ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

ANCHORE PLATE T DETAILS



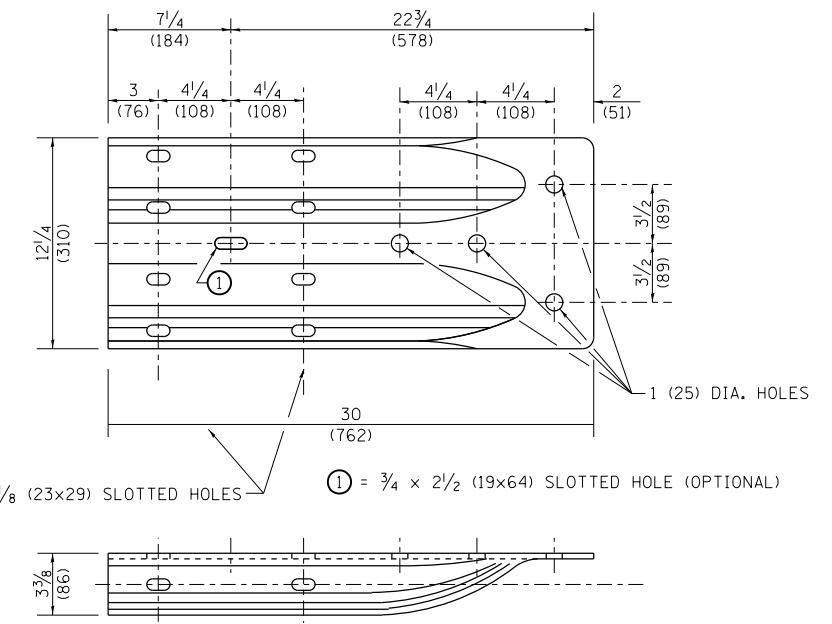
END SHOE

NOTE:

WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.



ALTERNATE END SHOE

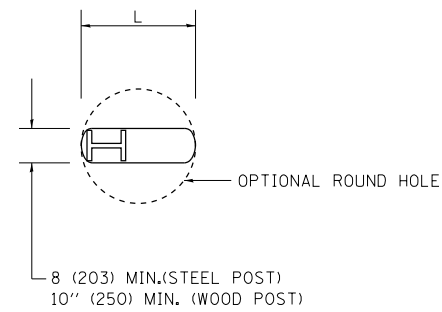
FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - 10-31-06
et:\pw\work\p\idot\seymorecp\d0348982\Std.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

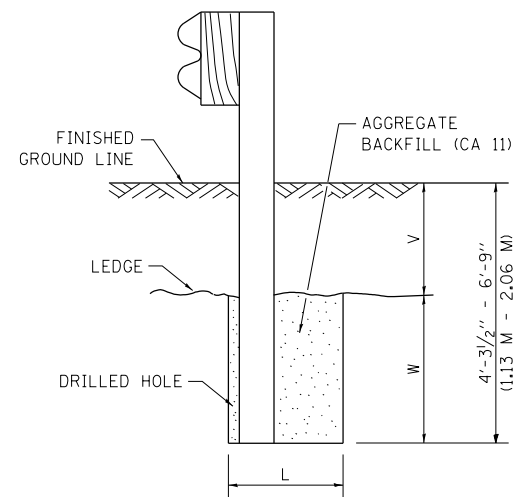
**REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	38
	BM-21		CONTRACT NO. 60W86	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN

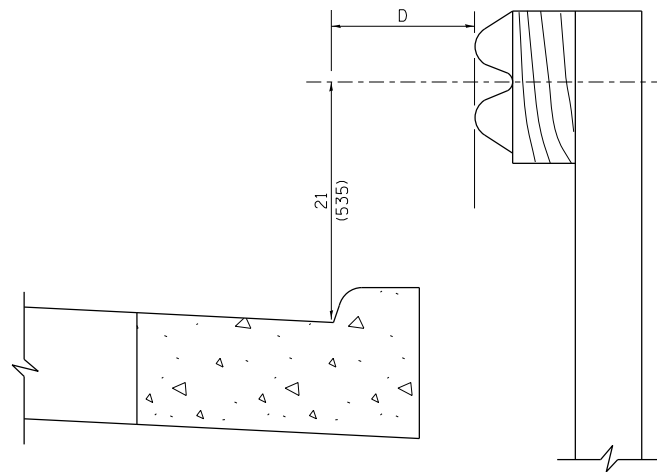


NOTE:

LEDGE LINE IS TOP OF ROCK LEDGE OR HARD SLAG FILL.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED



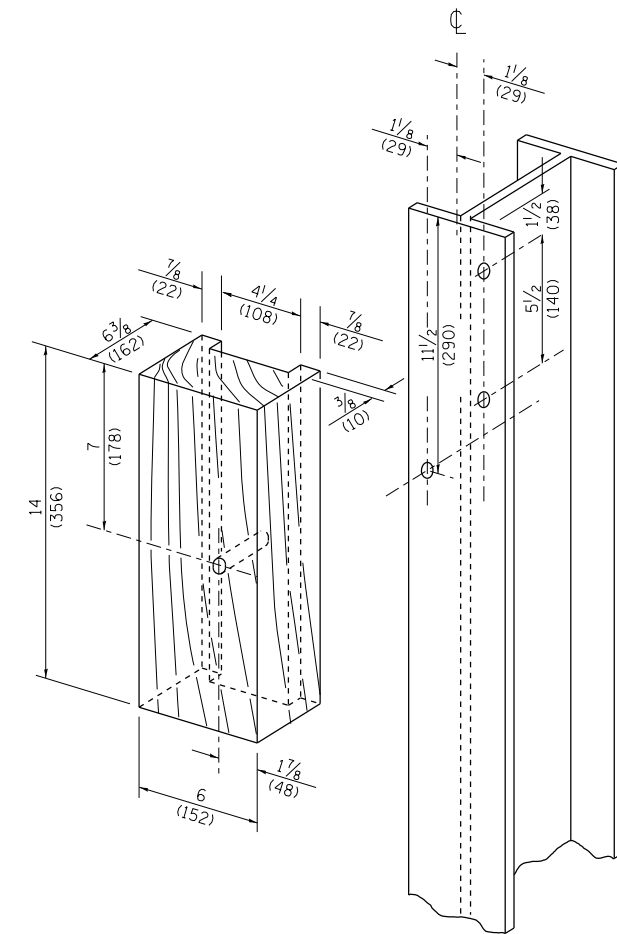
NOTE:

IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0" (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

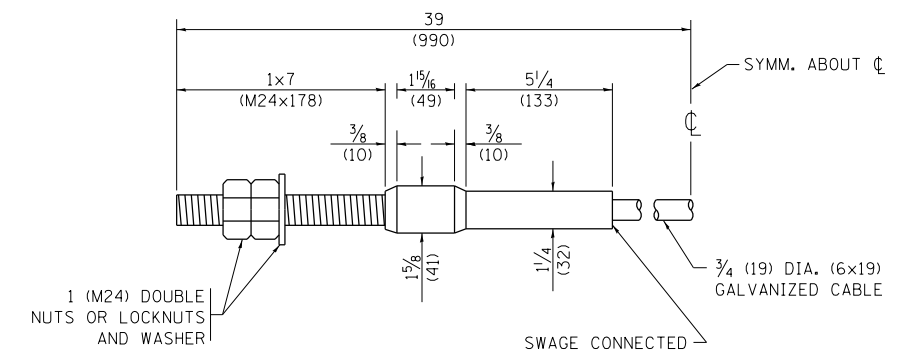
GUARDRAIL PLACED BEHIND CURB

(D = 0 DESIRABLE TO 12 (300) MAXIMUM)

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



WOOD BLOCK-OUT AND STEEL POST DETAILS



CABLE ASSEMBLY

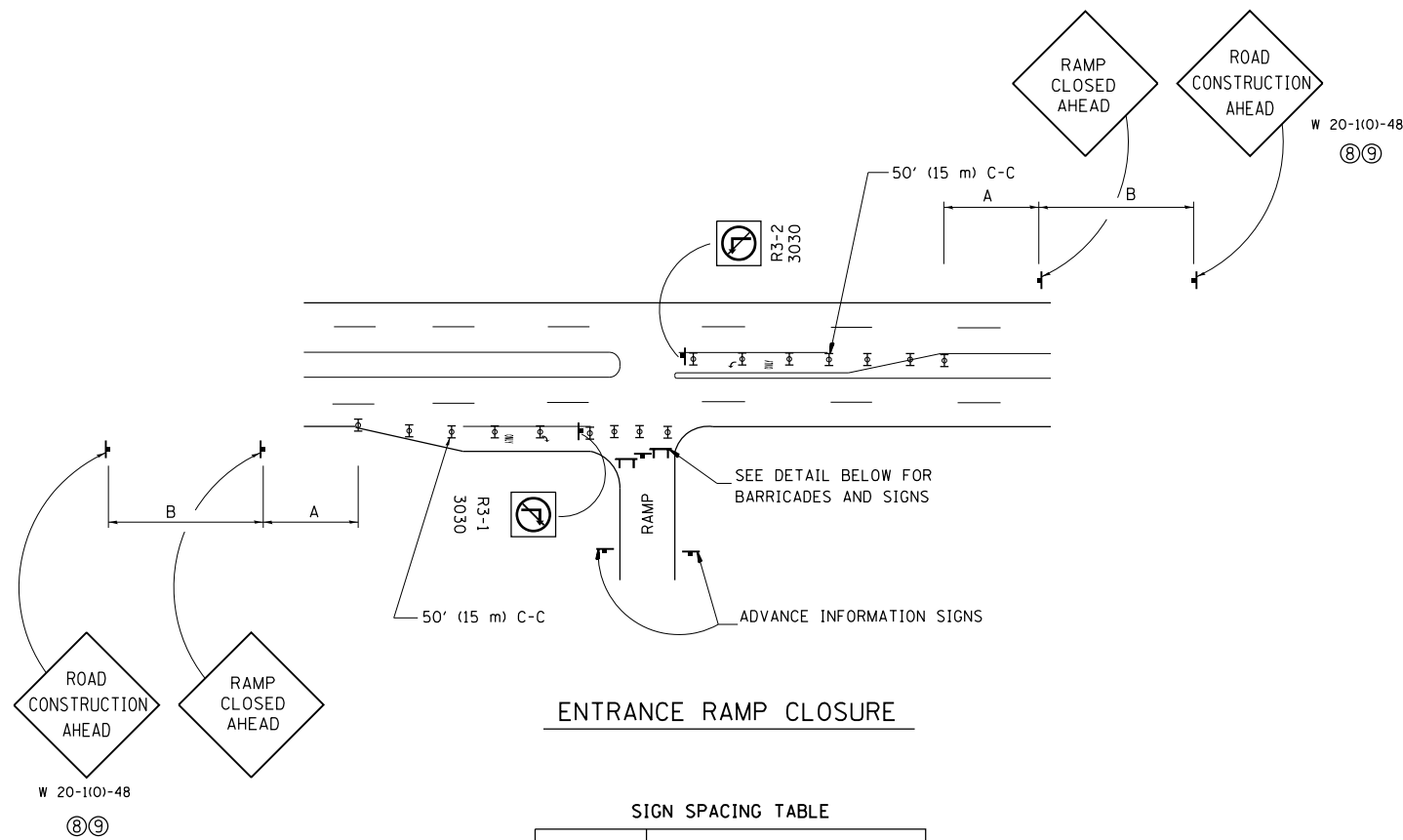
(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)
TIGHTEN TO TAUT TENSION

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - 10-31-06
et:\pwork\pwork\seymorecp\d0348982\1ststd.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 7/11/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			
SCALE: NONE	SHEET NO. 4	OF 4 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	39
BM-21		CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

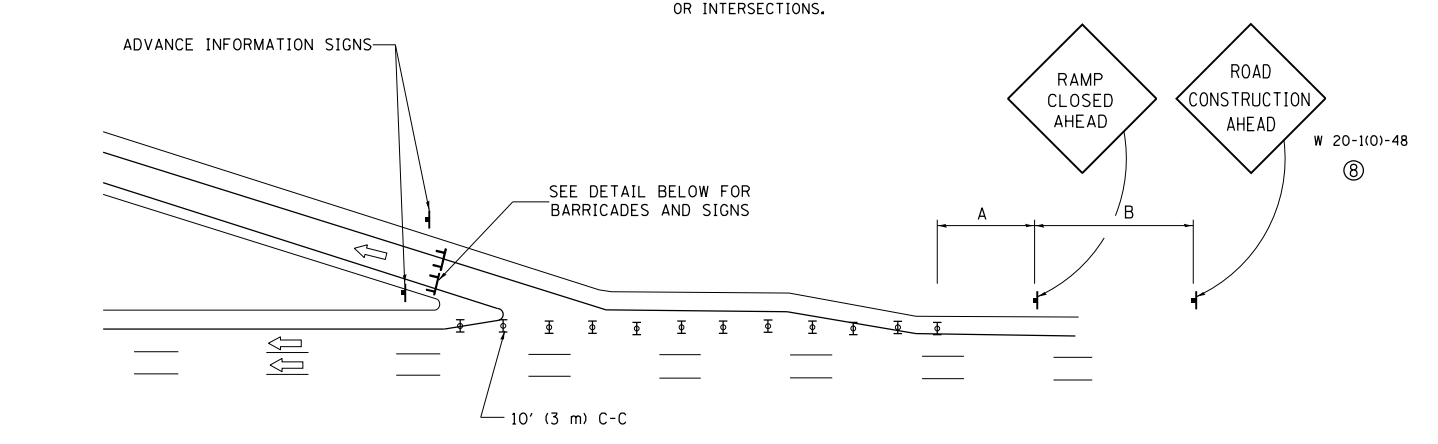


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

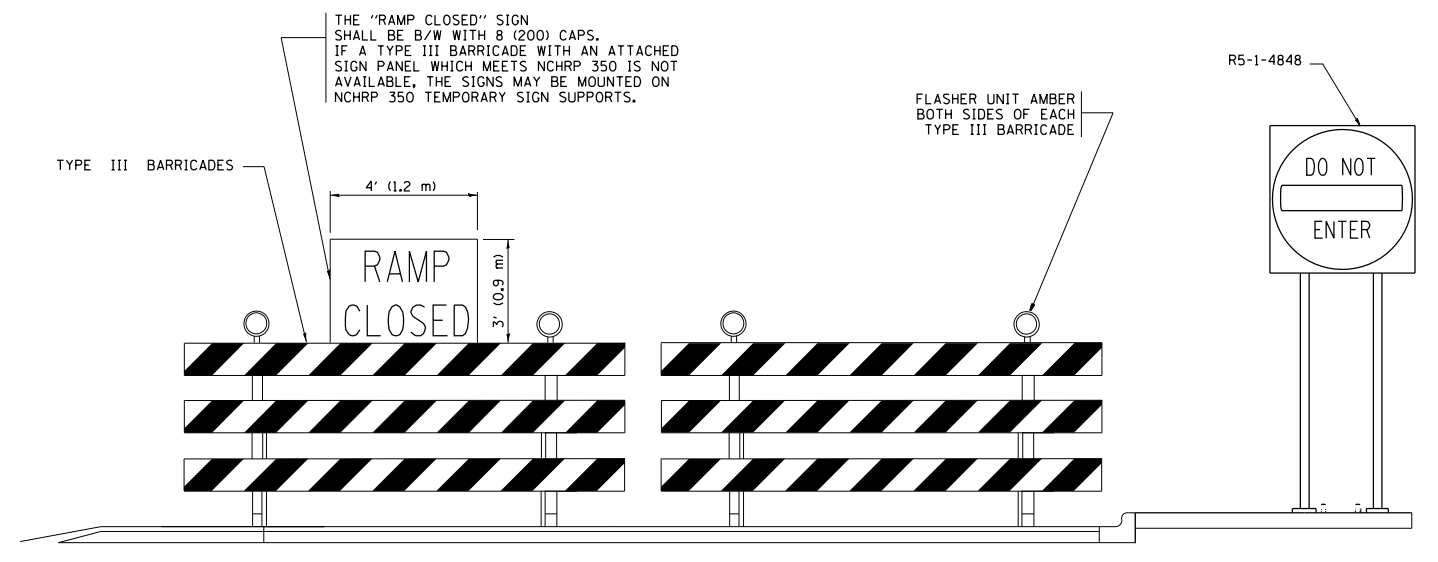
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

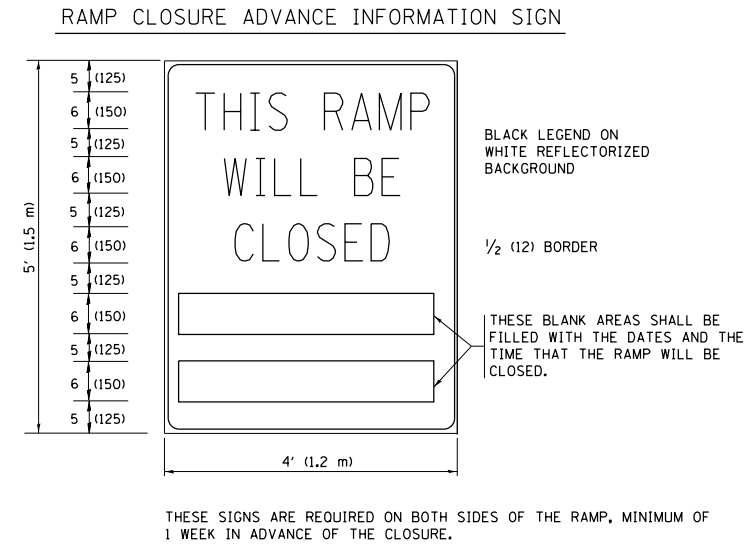
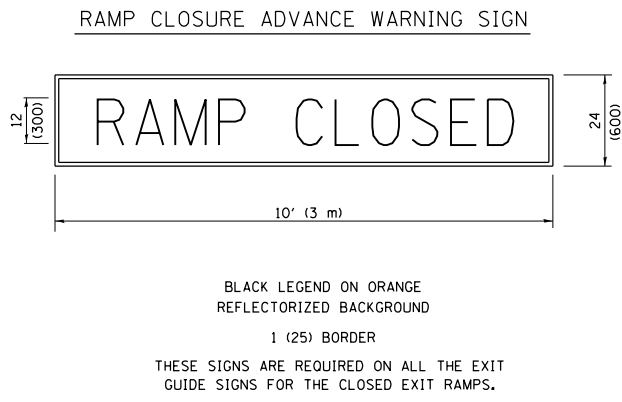


EXIT RAMP CLOSURE

- SYMBOLS**
- ⊥ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊓ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

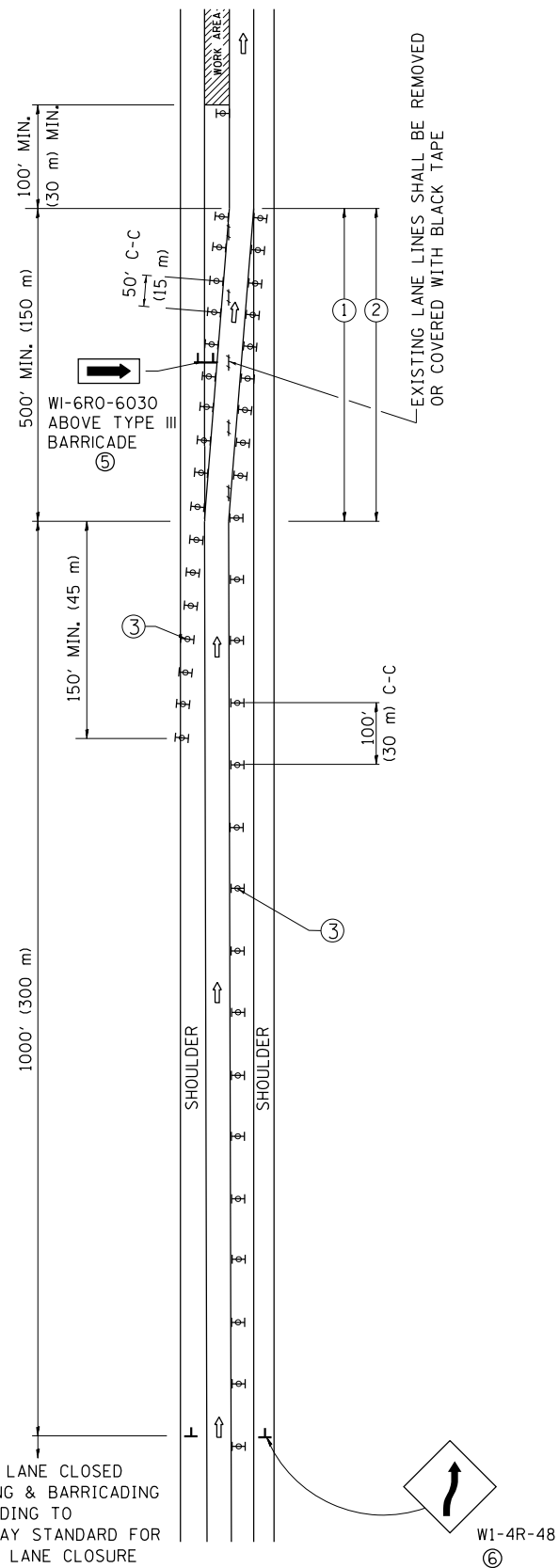
FILE NAME =	USER NAME = SEYMORECP	DESIGNED - DWS	REVISED - DWS/JAF 12-02
et:\pwork\pwork\seymorecp\d0348982\DrawStd.dgn		DRAWN -	REVISED - JAF 02-06
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - SPB 01-07
	PLOT DATE = 7/11/2013	DATE - 02-83	REVISED - SPB 12-09

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

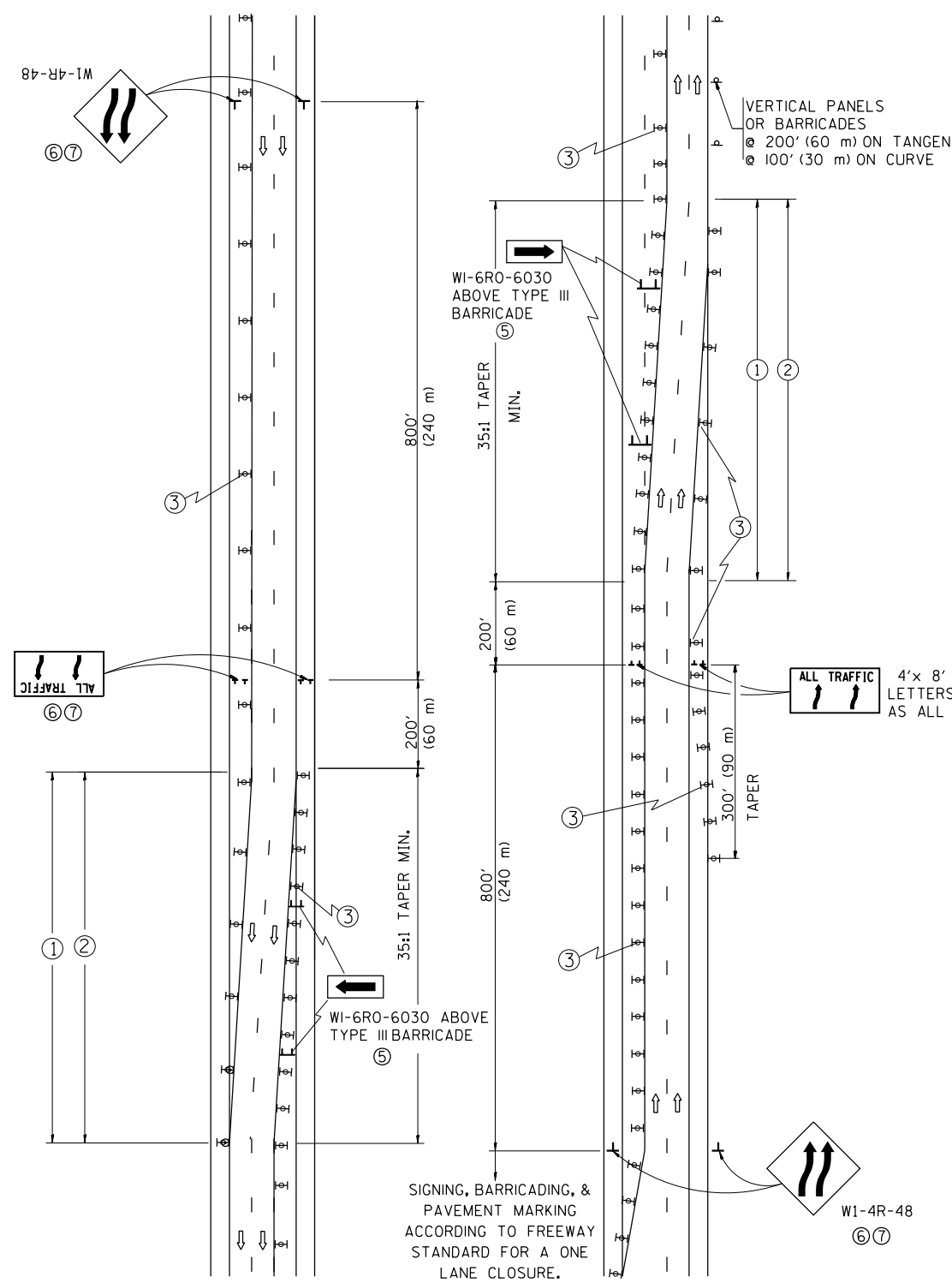
FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIABLES	2013-0411	VARIABLES	44	40
TC-08		CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



- ### GENERAL NOTES
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
 - CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
 - PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
 - ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
 - IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
 - WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
 - THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

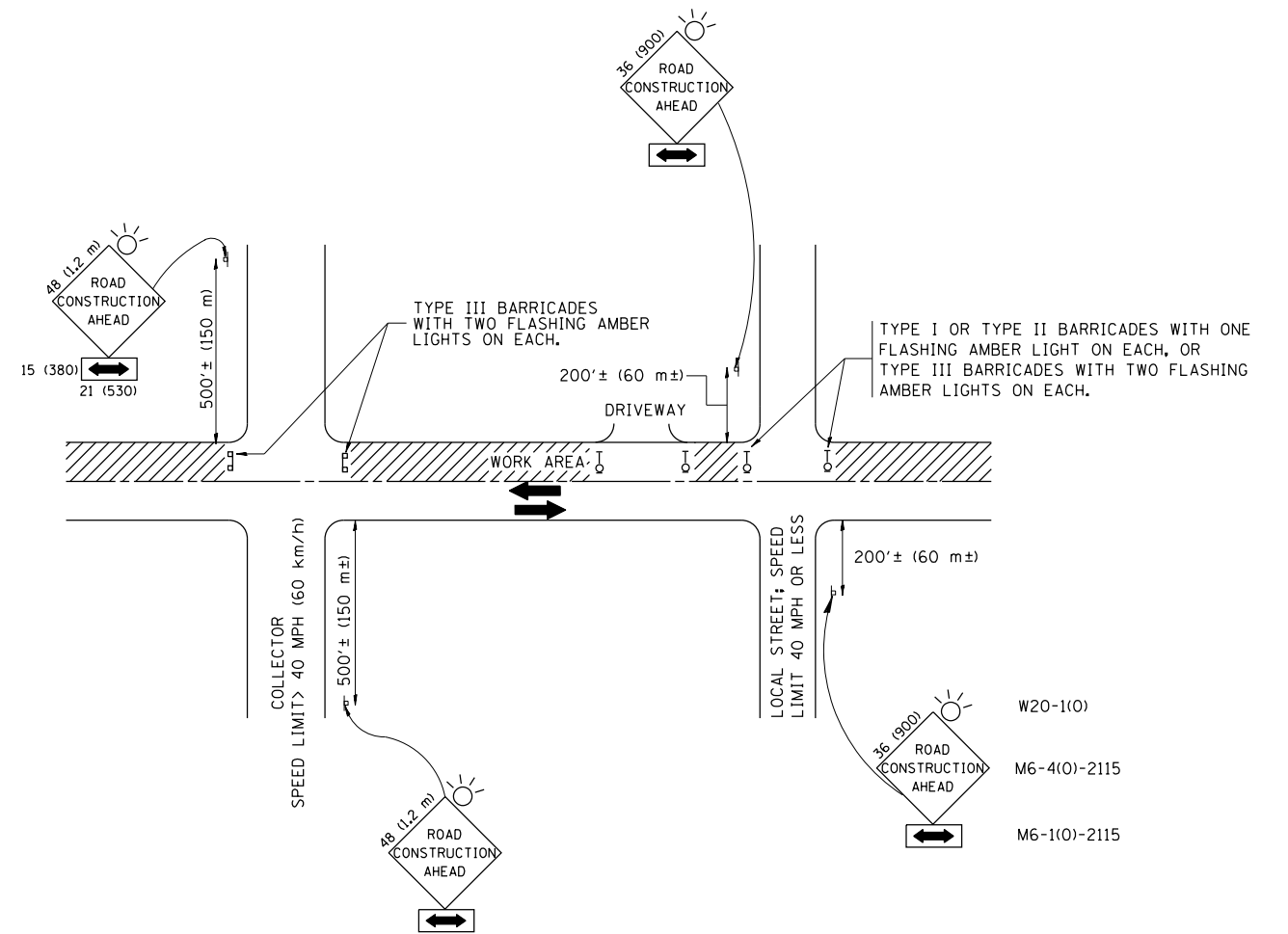
W24-1-48
⑦

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = SEYMORECP	DESIGNED - DWS	REVISED - JAF 01-03
Default	et:\pw\work\p\dot\seymorecp\d0348982\01std.dgn	DRAWN -	REVISED - JAF 02-06
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - SPB 01-07
	PLOT DATE = 7/11/2013	DATE - 02-87	REVISED - SPB 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FREWAY SINGLE & MULTI-LANE WEAVE		VARIES	2013-0411	VARIES	44	41
SCALE: NONE		TC-09		CONTRACT NO. 60W86		
SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
et:\pwork\pwork\seymorecp\d0348982\01std.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 7/11/2013	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

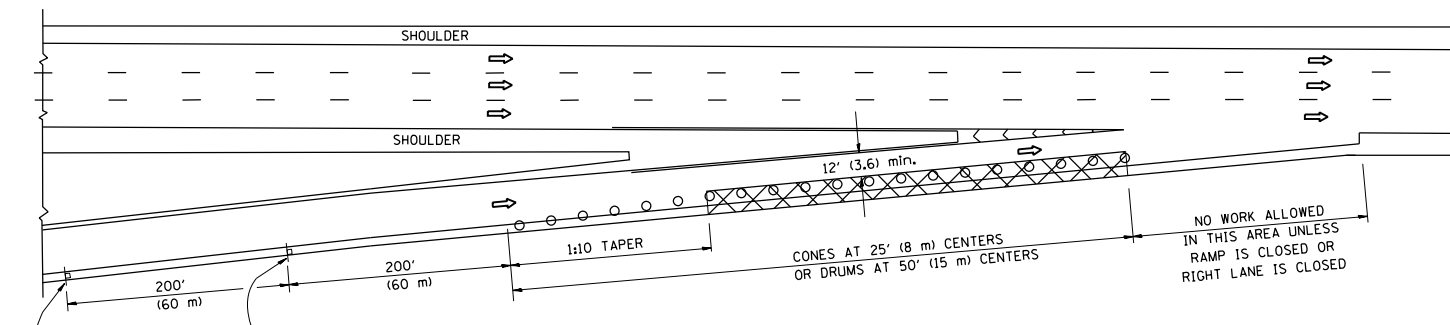
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

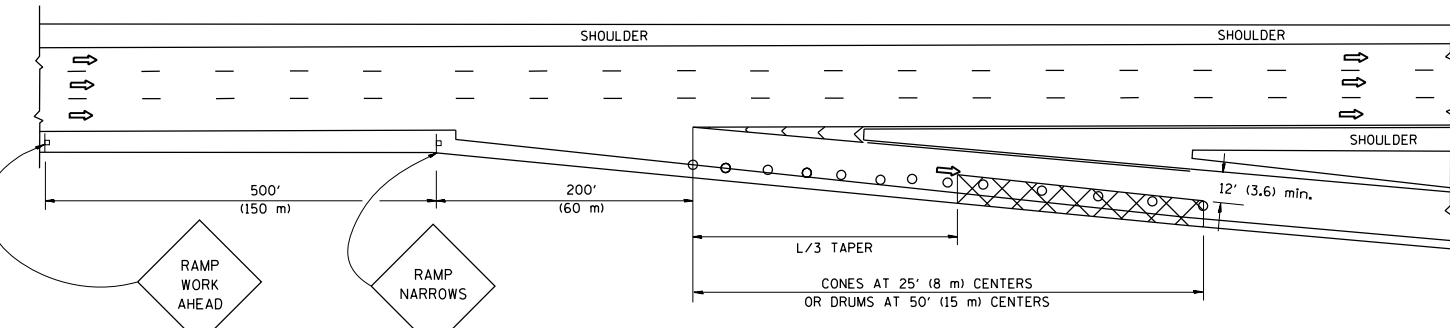
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	42
TC-10			CONTRACT NO. 60W86	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

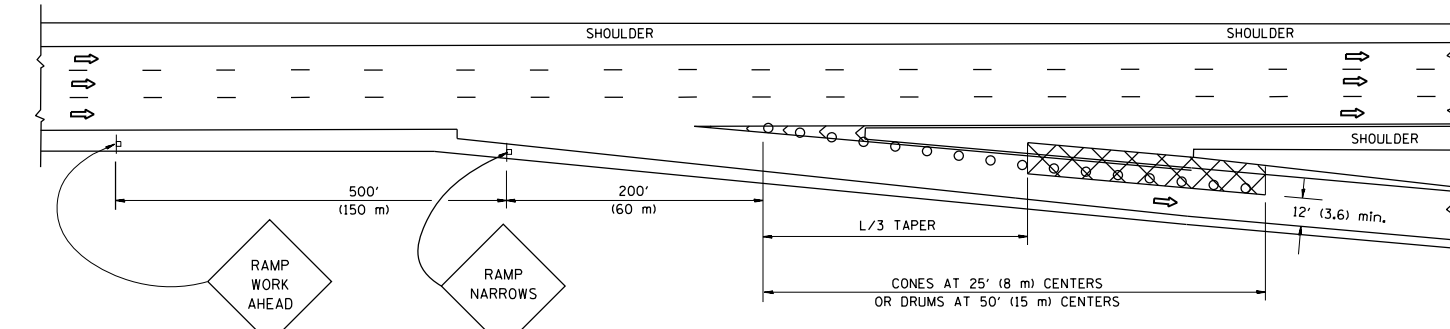
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

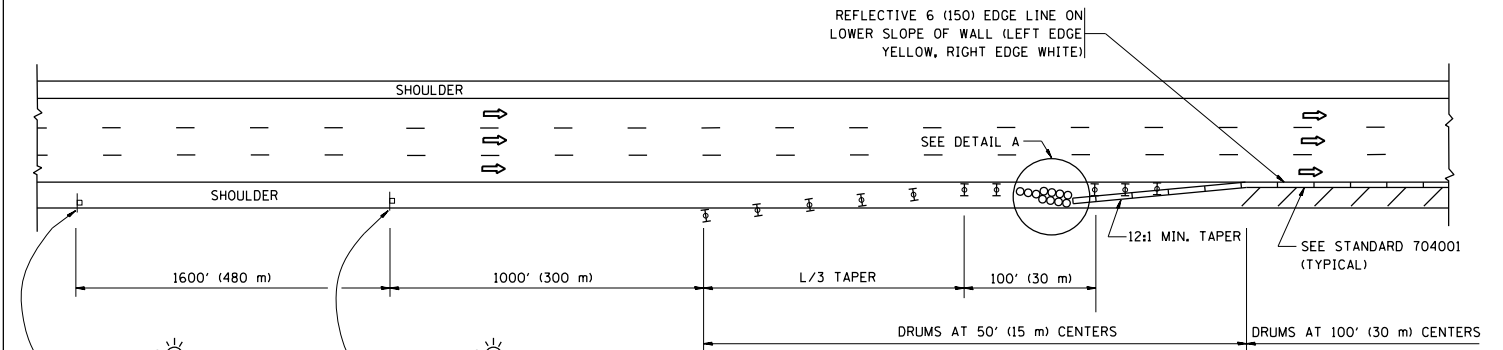
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

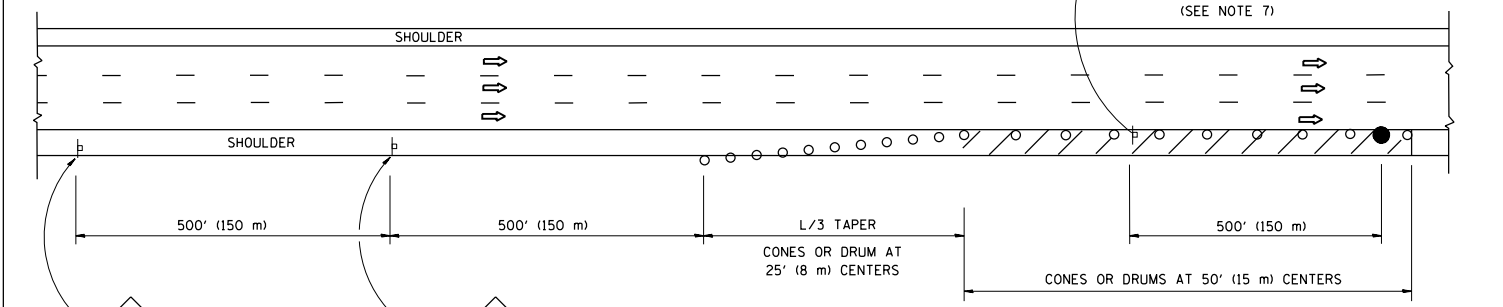
SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L=0.65(W)(S)$ $L=(W)(S)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

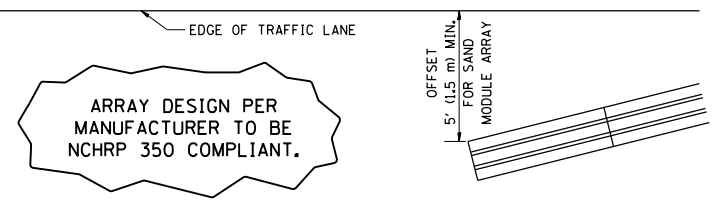


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.

THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

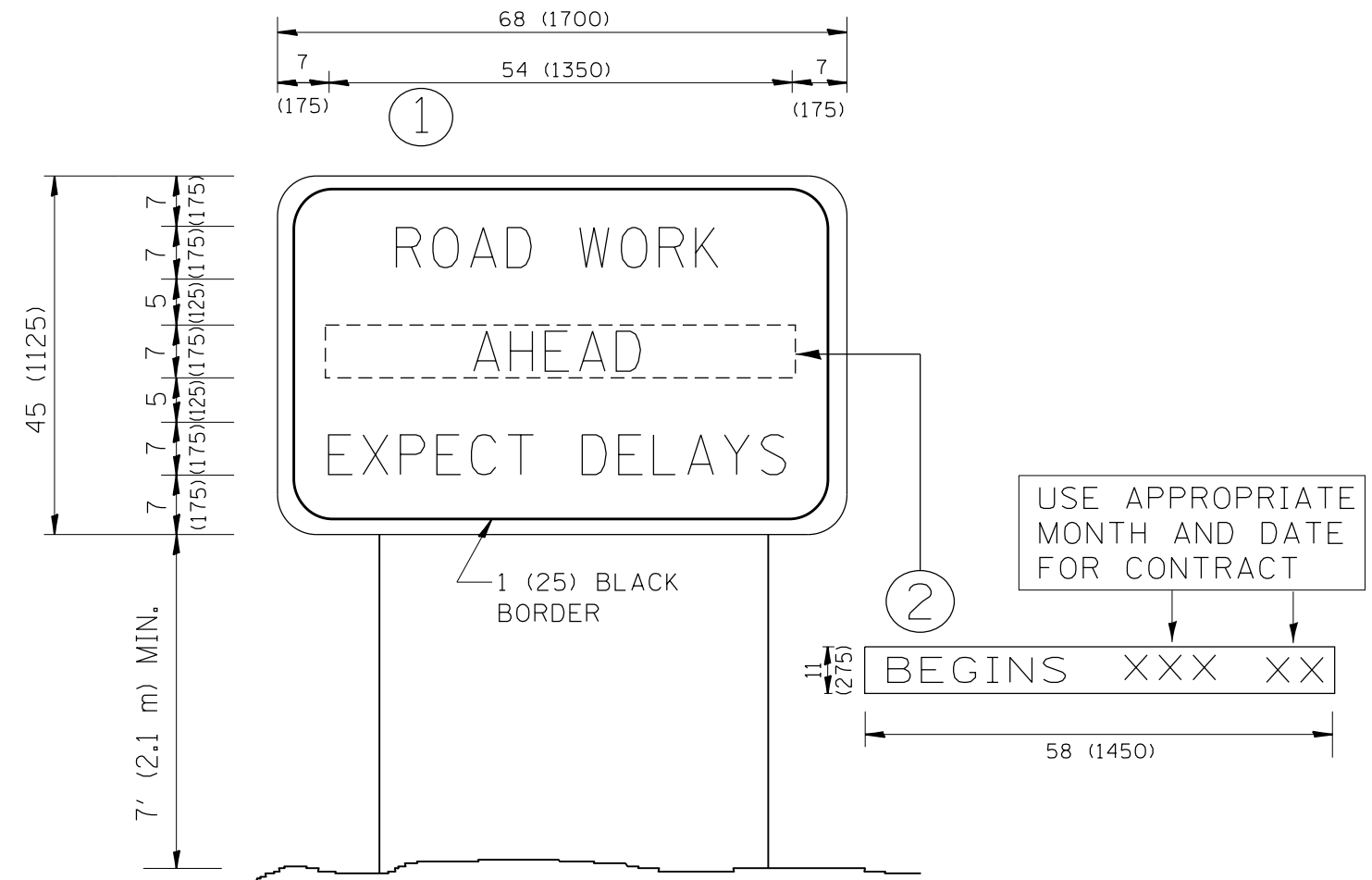
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - 04-03
et:\pwork\pwork\seymorecp\d0348982\DrawStd.dgn		DRAWN - D.W.S.	REVISED - J.A.F. 12-06
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 7/11/2013	DATE - 11-96	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY			
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	43
TC-17		CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = SEYMORECP	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pwork\pwork\seymorecp\d0348982\01std.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 7/11/2013	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VARIES	2013-0411	VARIES	44	44
TC-22		CONTRACT NO. 60W86		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				