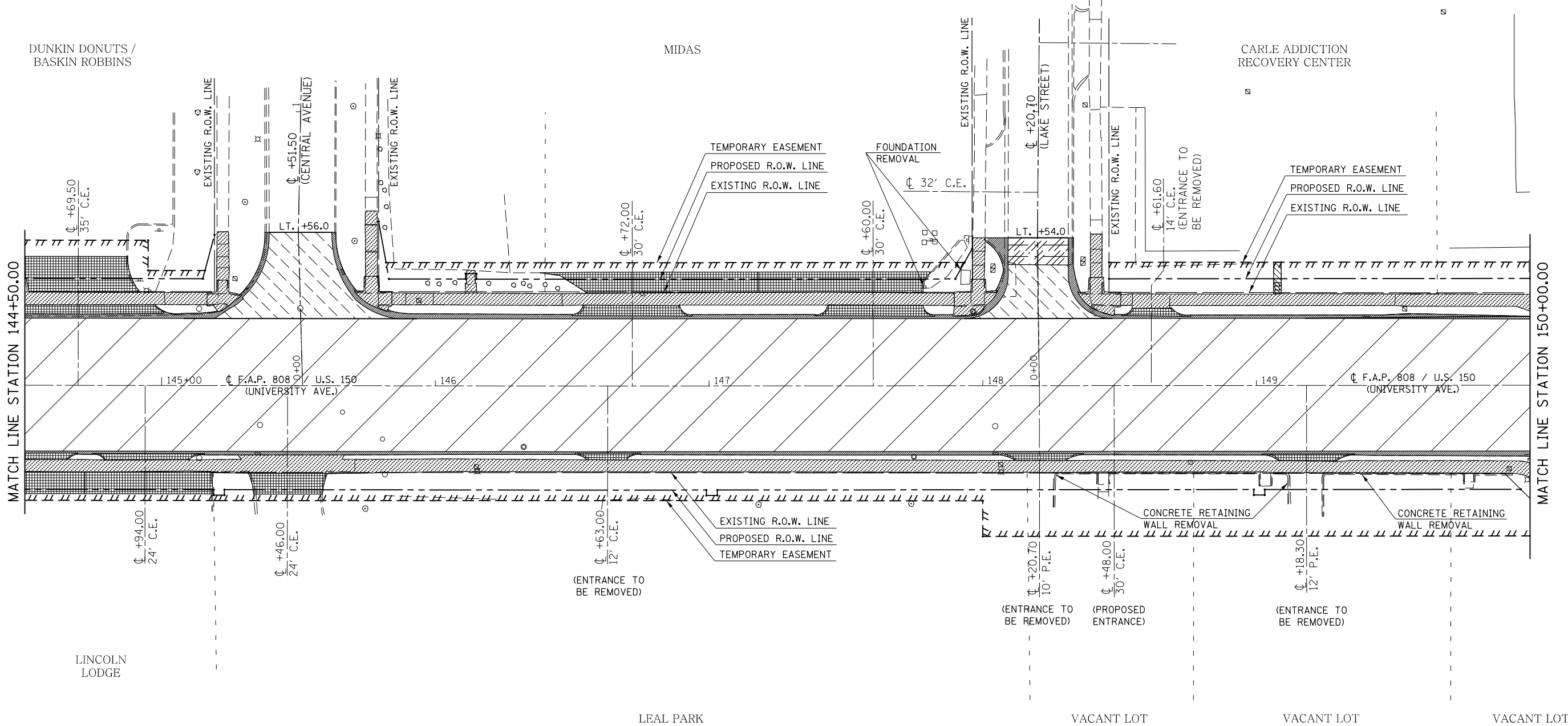


SEC. 8, T. 19N., R. 9E., 3RD P.M.



NOTE:
FOR REMOVAL LIMITS, SEE CURB & GUTTER
PLAN SHEETS AND ADA RAMP DETAILS FOR
STATIONS / OFFSETS.

NOTE:
SEE TRAFFIC SIGNAL PLAN SHEETS FOR
REMOVAL ITEMS ASSOCIATED WITH
TRAFFIC SIGNAL MODERNIZATION.



- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
- REMOVE EXISTING BRICK PAVERS
- PCC SURFACE REMOVAL, 1 1/2"
- REMOVE AND REINSTALL BRICK PAVER

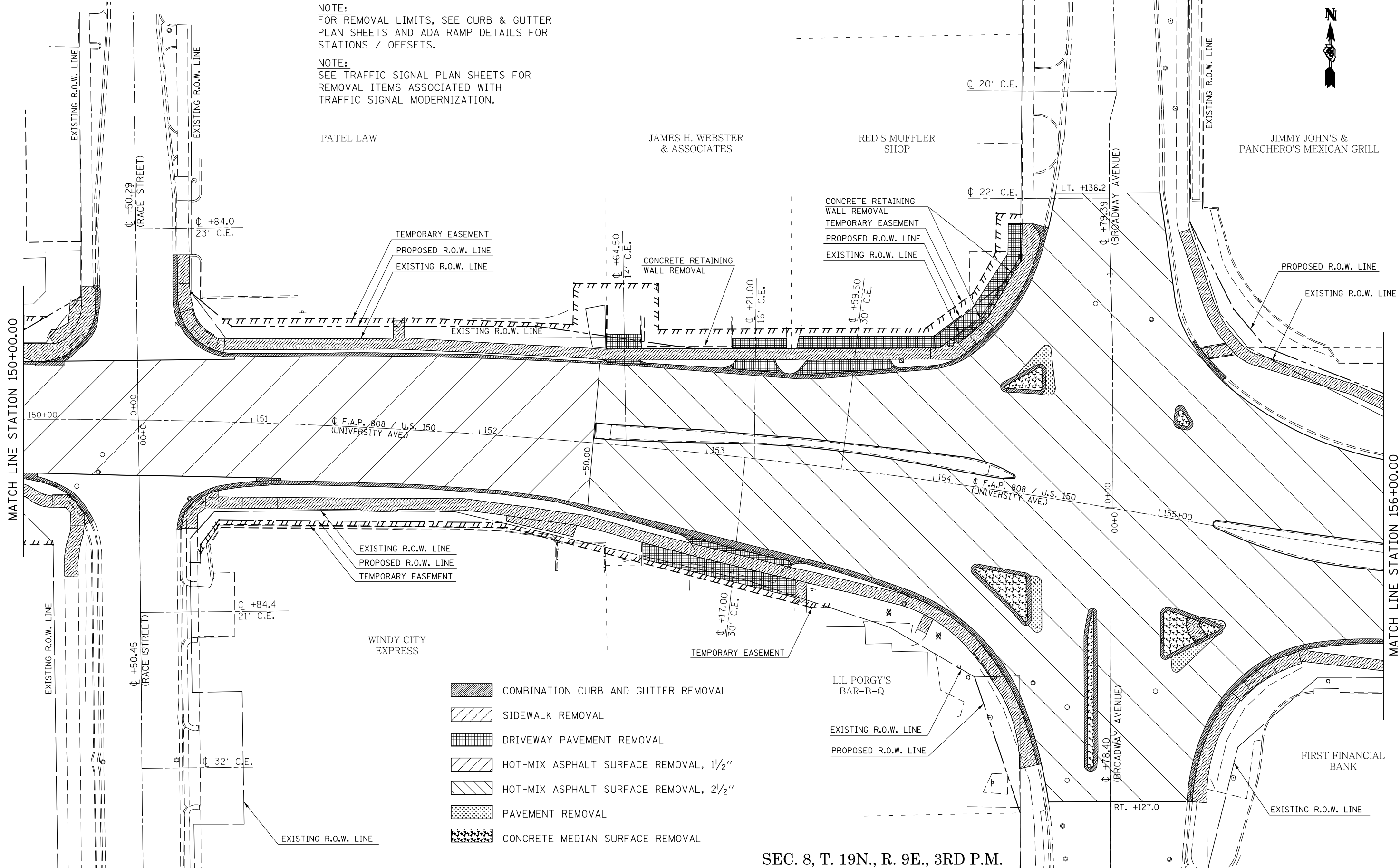
SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\11\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570853-shr-rem.dgn						DRAWN	REVISED	808	D5 HSIP 2016-1	CHAMPAIGN	499	101
PLOT SCALE = 40.0000' / in.						CHECKED -	REVISED -	CONTRACT NO. 70B53				
MODELNAME						DATE -	REVISED -	ILLINOIS FED. AID PROJECT				
SCALE:						SHEET 10 OF 14 SHEETS STA. 144+50.00 TO STA. 150+00.00						

SEC. 8, T. 19N., R. 9E., 3RD P.M.

NOTE:
FOR REMOVAL LIMITS, SEE CURB & GUTTER
PLAN SHEETS AND ADA RAMP DETAILS FOR
STATIONS / OFFSETS.

NOTE:
SEE TRAFFIC SIGNAL PLAN SHEETS FOR
REMOVAL ITEMS ASSOCIATED WITH
TRAFFIC SIGNAL MODERNIZATION.

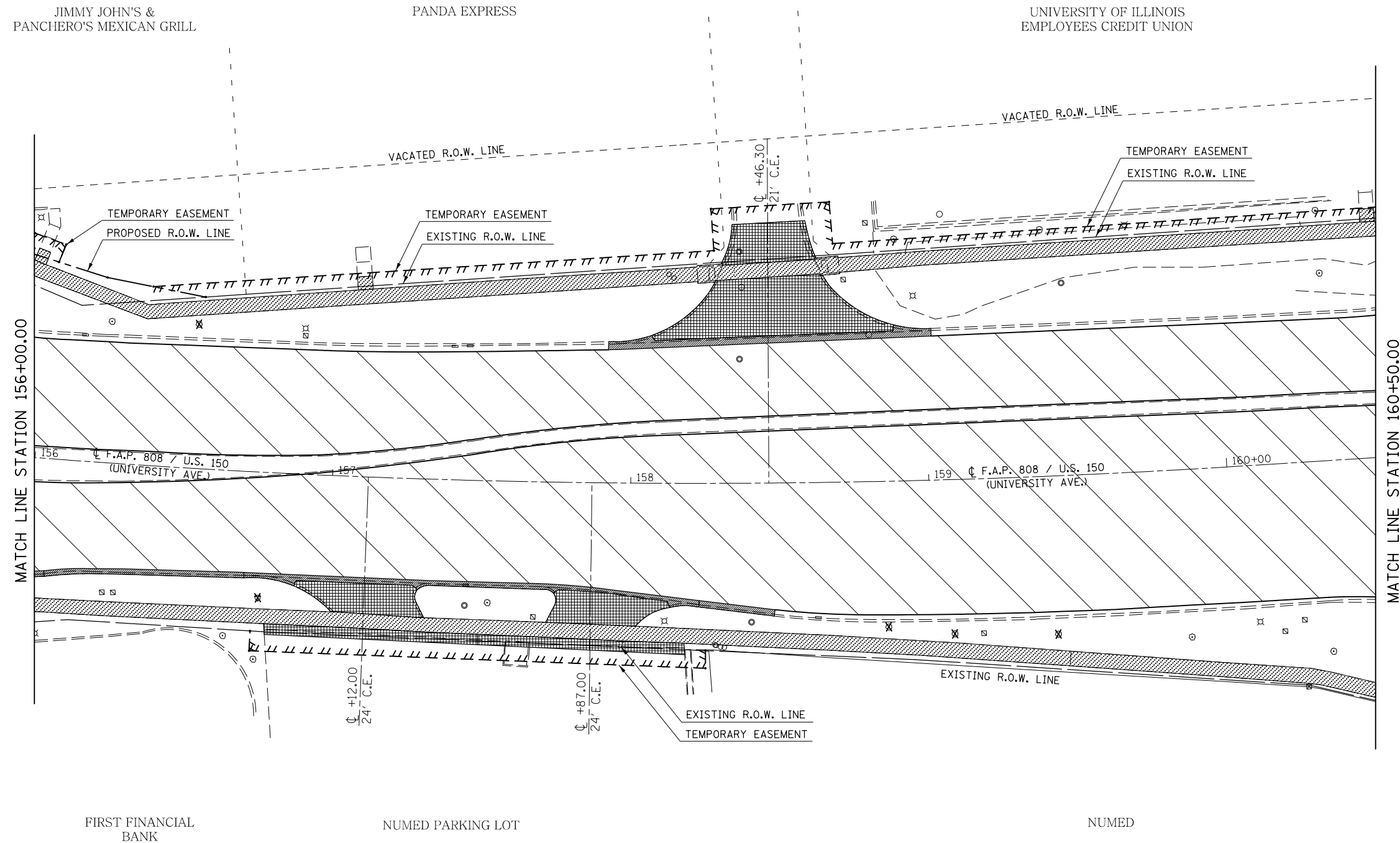


- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- PAVEMENT REMOVAL
- CONCRETE MEDIAN SURFACE REMOVAL

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\1\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-rem.dgn						808	D5 HSIP 2016-1	CHAMPAIGN	499	102		
PLOT SCALE = 40.0000' / in.						CONTRACT NO. 70B53						
PLOT DATE = 1/28/2019						ILLINOIS FED. AID PROJECT						
					SCALE:		SHEET 11 OF 14 SHEETS STA. 150+00.00 TO STA. 156+00.00					

SEC. 8, T. 19N., R. 9E., 3RD P.M.



- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"

NOTE:
FOR REMOVAL LIMITS, SEE CURB & GUTTER
PLAN SHEETS AND ADA RAMP DETAILS FOR
STATIONS / OFFSETS.

NOTE:
SEE TRAFFIC SIGNAL PLAN SHEETS FOR
REMOVAL ITEMS ASSOCIATED WITH
TRAFFIC SIGNAL MODERNIZATION.

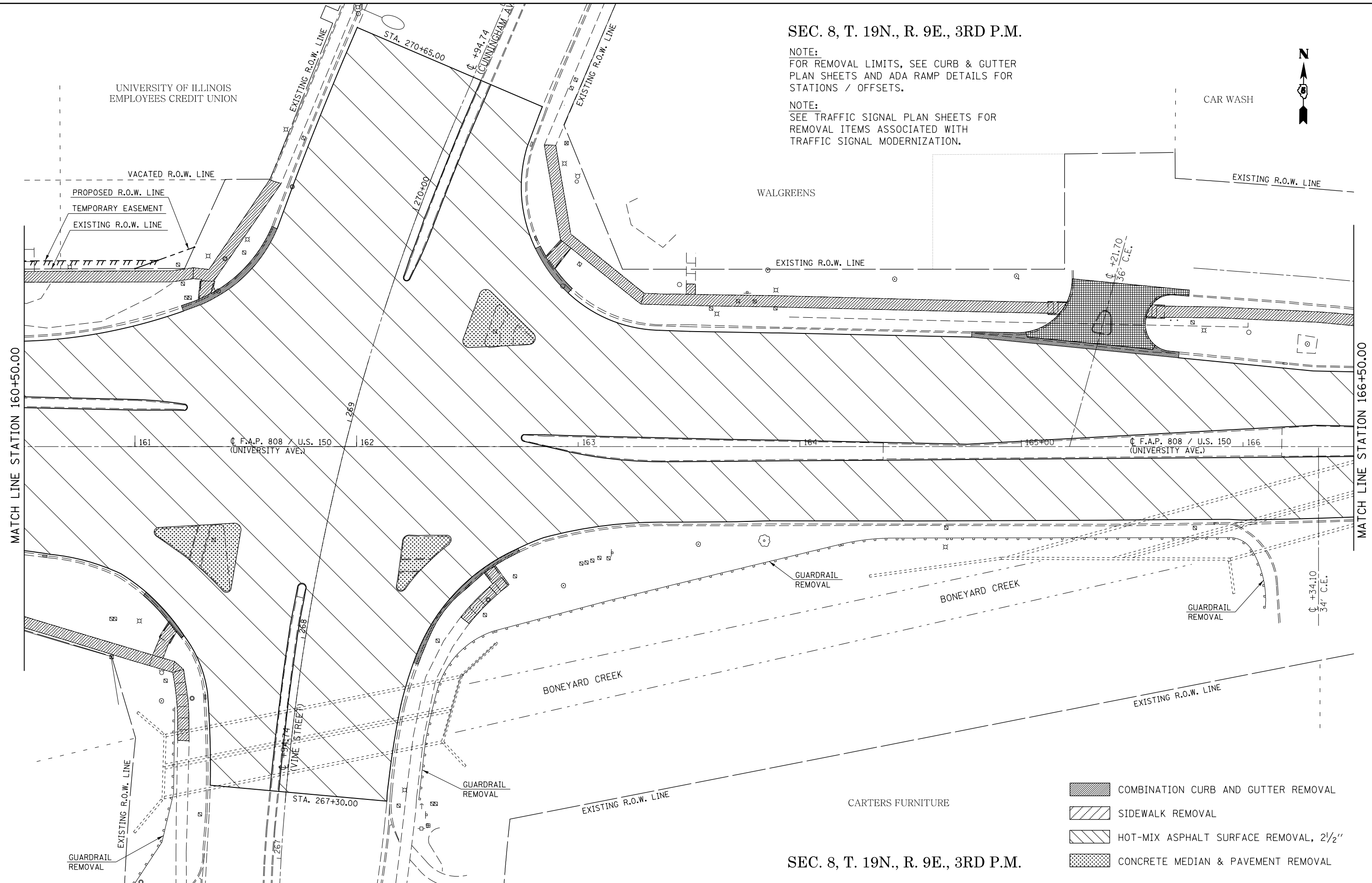
SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\0570853-shr-rem.dgn						DRAWN	808	D5 HSIP 2016-1	CHAMPAIGN	499	103
PLOT SCALE = 40.0000' / in.						CHECKED -	CONTRACT NO. 70B53				
PLOT DATE = 1/28/2019						DATE -	ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET 12 OF 14 SHEETS		STA. 156+00.00 TO STA. 160+50.00			

SEC. 8, T. 19N., R. 9E., 3RD P.M.

NOTE:
FOR REMOVAL LIMITS, SEE CURB & GUTTER
PLAN SHEETS AND ADA RAMP DETAILS FOR
STATIONS / OFFSETS.

NOTE:
SEE TRAFFIC SIGNAL PLAN SHEETS FOR
REMOVAL ITEMS ASSOCIATED WITH
TRAFFIC SIGNAL MODERNIZATION.

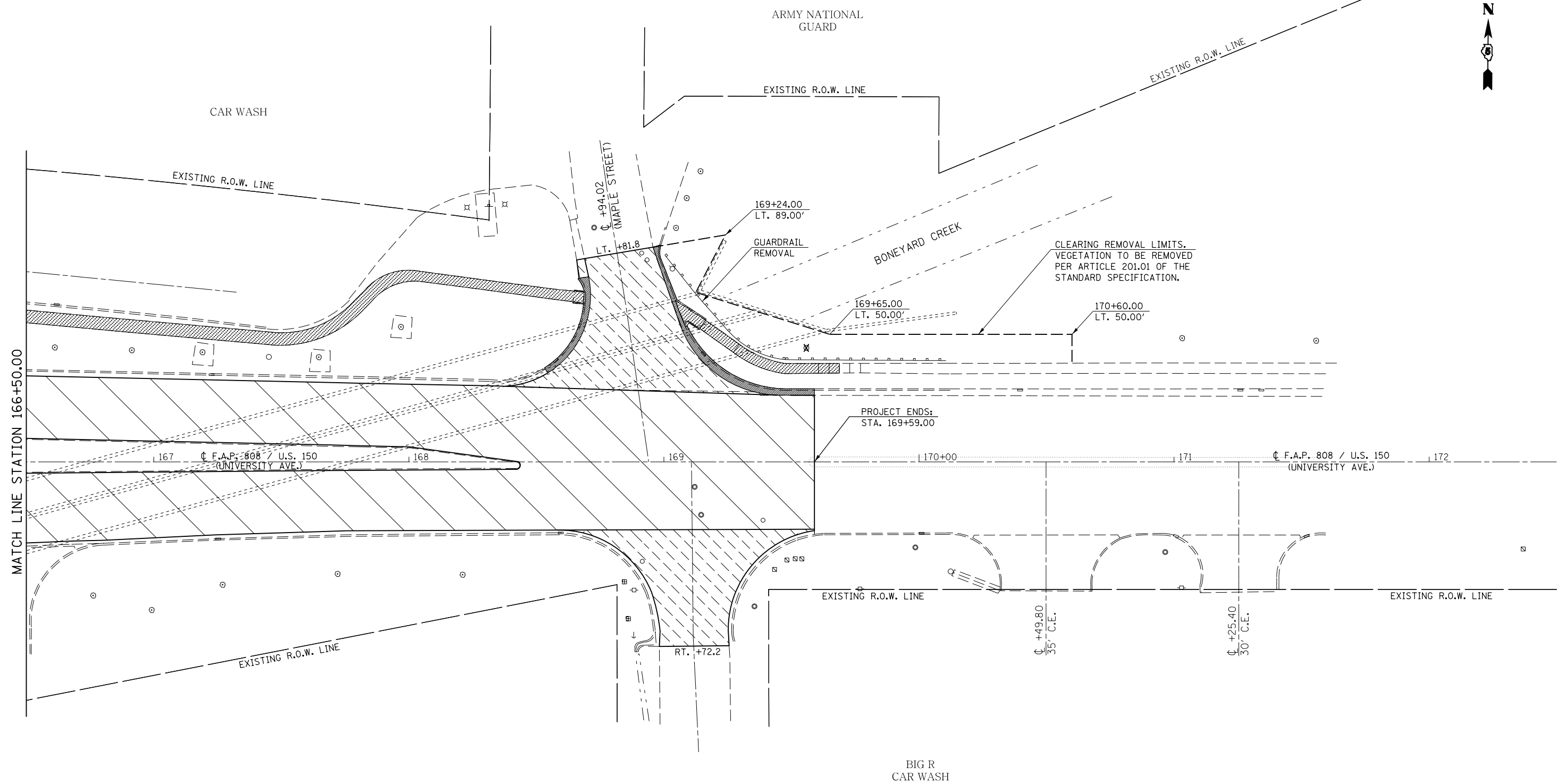


- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- CONCRETE MEDIAN & PAVEMENT REMOVAL

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0572\BROWNS\Design\0570853-shr-rem.dgn					REVISIONS	SCALE:	SHEET 13	OF 14	SHEETS	STA. 160+50.00	TO STA. 166+50.00	808	D5 HSIP 2016-1	CHAMPAIGN	499	104	
PLOT SCALE = 40.0000' / in.					CHECKED -												
PLOT DATE = 1/28/2019					DATE -												
												CONTRACT NO. 70B53		ILLINOIS FED. AID PROJECT			

SEC. 8, T. 19N., R. 9E., 3RD P.M.



- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL

NOTE:
FOR REMOVAL LIMITS, SEE CURB & GUTTER PLAN SHEETS AND ADA RAMP DETAILS FOR STATIONS / OFFSETS.

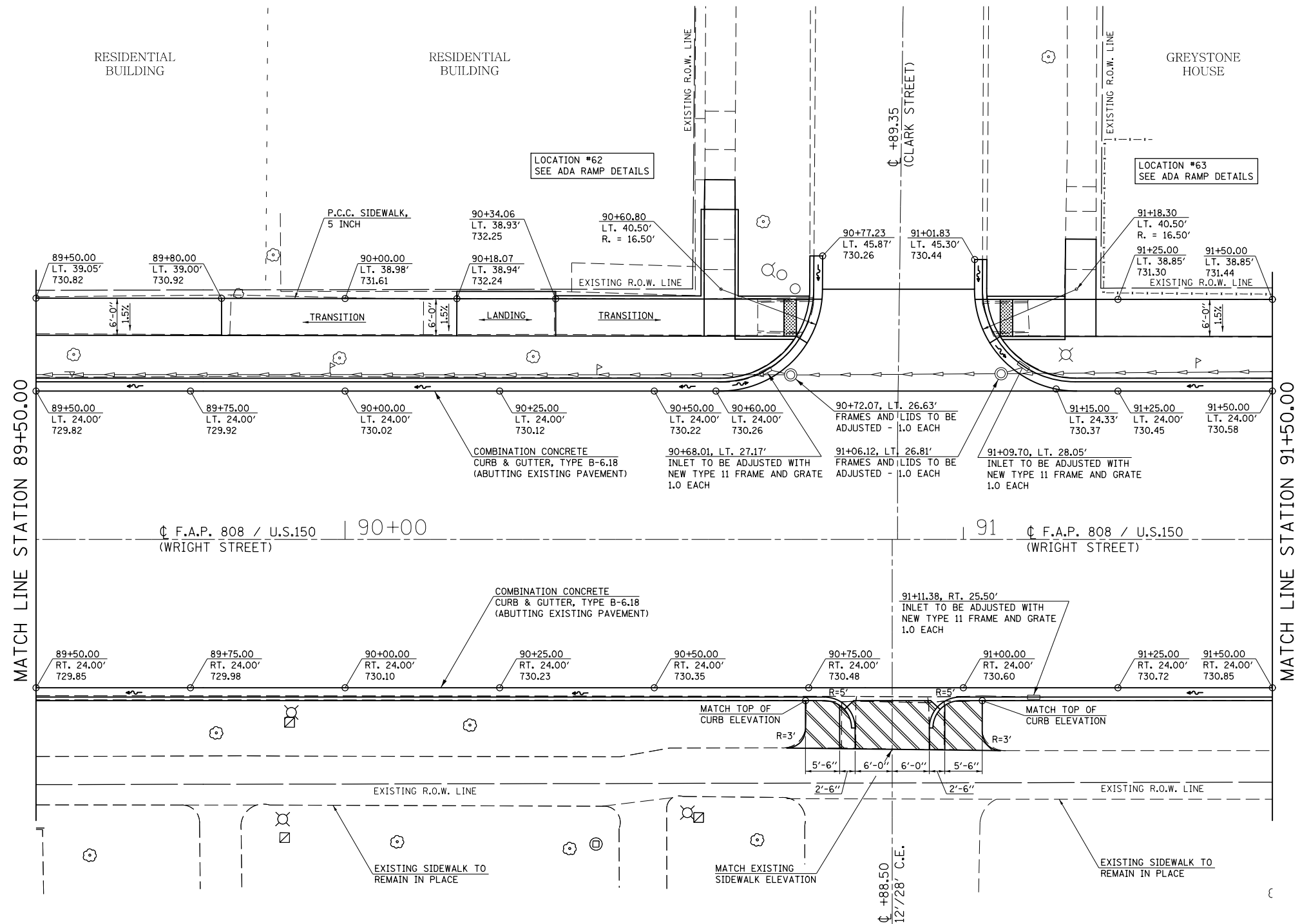
NOTE:
SEE TRAFFIC SIGNAL PLAN SHEETS FOR REMOVAL ITEMS ASSOCIATED WITH TRAFFIC SIGNAL MODERNIZATION.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\BROWND\Design\0579853-shr-rem.dgn						DRAWN	808	D5 HSIP 2016-1	CHAMPAIGN	499	105
PLOT SCALE = 40.0000' / in.						CHECKED -	CONTRACT NO. 70B53				
PLOT DATE = 1/28/2019						DATE -	ILLINOIS FED. AID PROJECT				

SCALE: SHEET 14 OF 14 SHEETS STA. 166+50.00 TO STA. 172+00.00

SEC. 7, T. 19N., R. 9E., 3RD P.M.



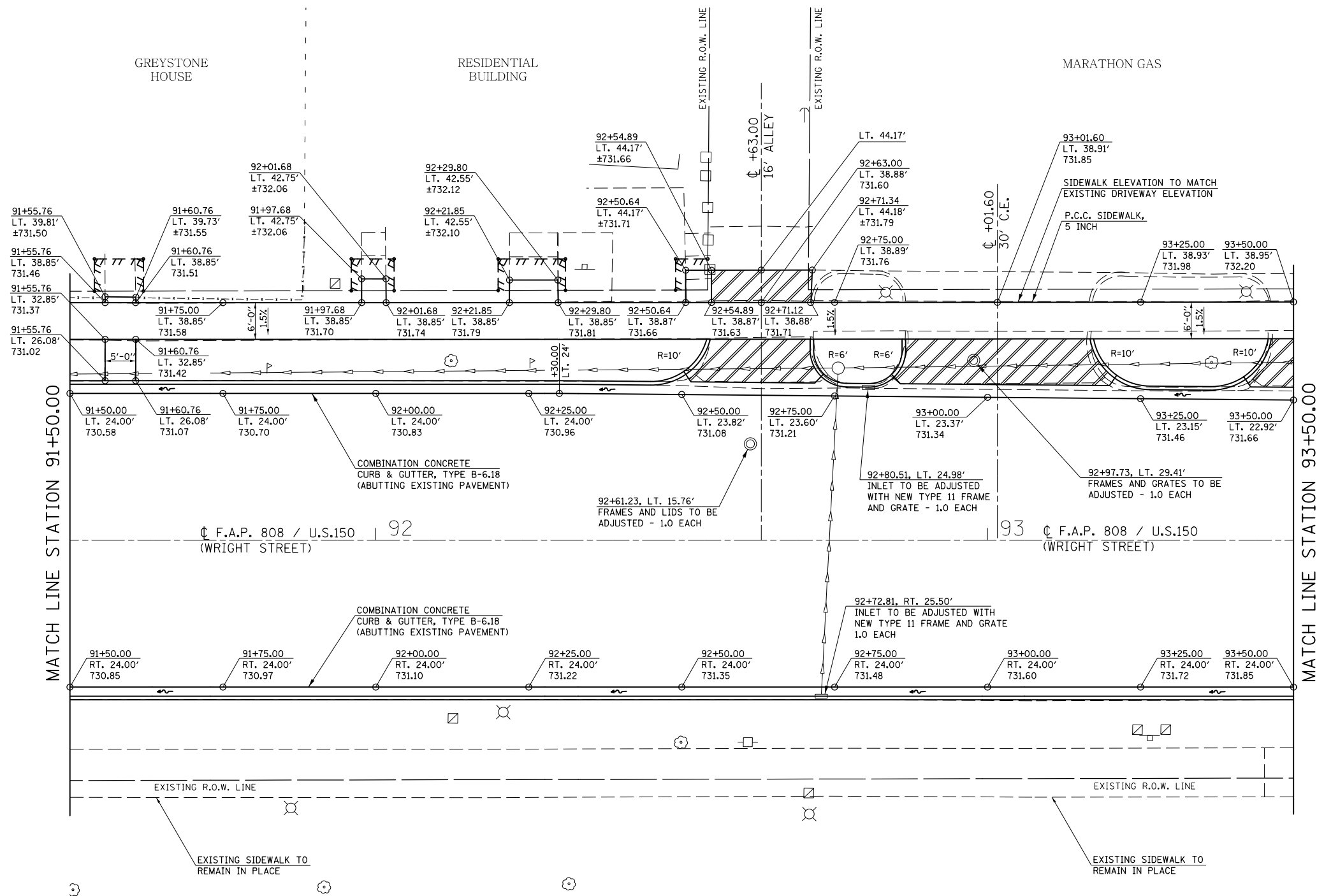
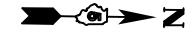
- P.C.C. DRIVEWAY PAVEMENT, 6 INCH
- P.C.C. DRIVEWAY PAVEMENT, 8 INCH

SEC. 7, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS (WRIGHT STREET)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\work\11084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0577\Drawings\Design\0570853-shr-curb.dgn						DRAWN	REVISED	808	D5 HSIP 2016-1	CHAMPAIGN	499	107
PLOT SCALE = 20.0000' / in.						CHECKED -	REVISED -	CONTRACT NO. 70B53				
PLOT DATE = 1/28/2019						DATE -	REVISED -	ILLINOIS FED. AID PROJECT				

SCALE: 1" = 10' SHEET 2 OF 3 SHEETS STA. 89+50.00 TO STA. 91+50.00

SEC. 7, T. 19N., R. 9E., 3RD P.M.



P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 7, T. 19N., R. 9E., 3RD P.M.

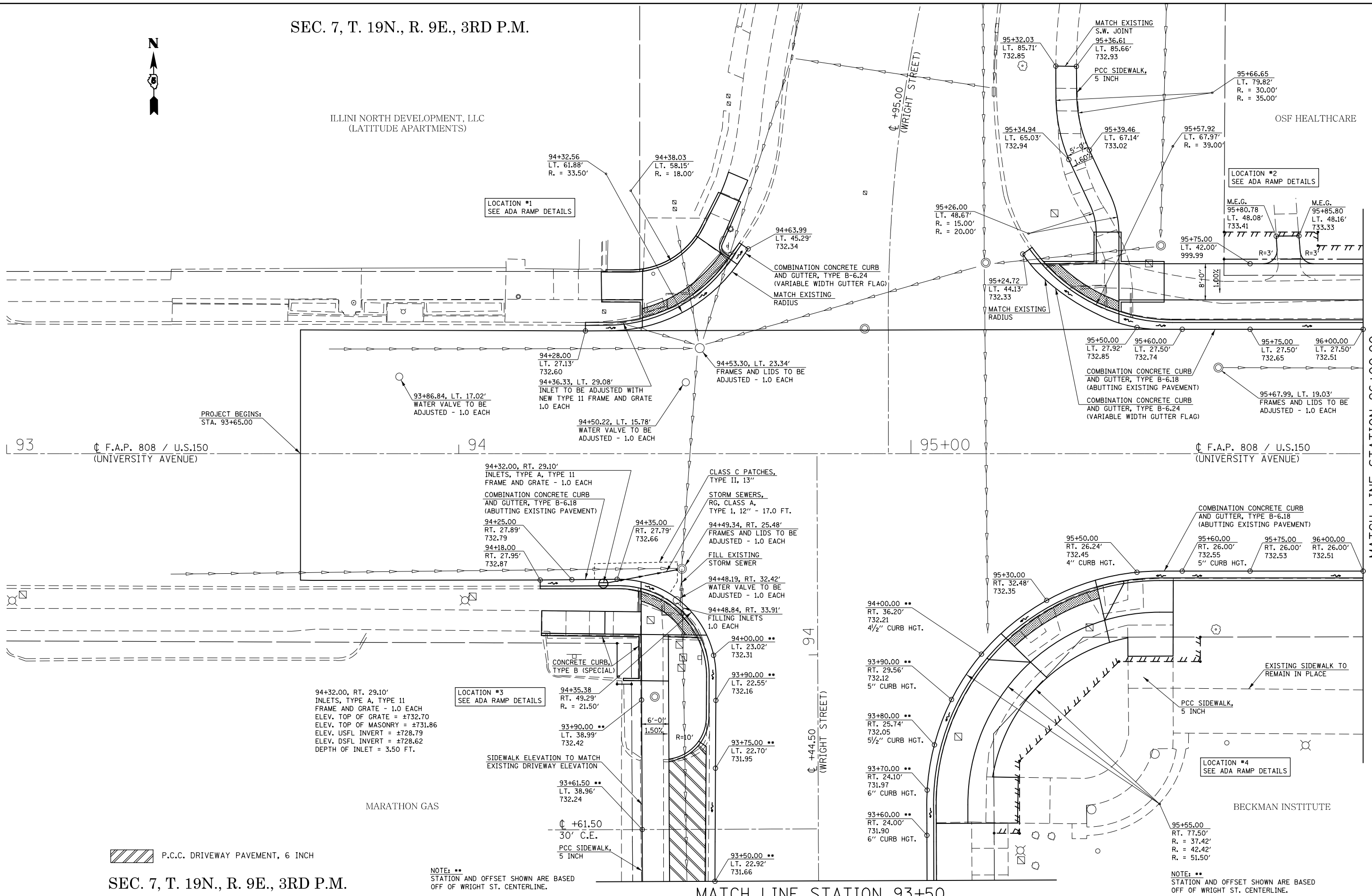
FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS (WRIGHT STREET)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\Drawings\Design\0579B53-shr-curb.dgn	PLotted SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	108
MODELNAME	PLotted DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10' SHEET 3 OF 3 SHEETS STA. 91+50.00 TO STA. 93+50.00			CONTRACT NO. 70B53				
ILLINOIS FED. AID PROJECT												

SEC. 7, T. 19N., R. 9E., 3RD P.M.



ILLINI NORTH DEVELOPMENT, LLC
(LATITUDE APARTMENTS)

OSF HEALTHCARE



PROJECT BEGINS:
STA. 93+65.00

93 F.A.P. 808 / U.S.150
(UNIVERSITY AVENUE)

94

95+00

F.A.P. 808 / U.S.150
(UNIVERSITY AVENUE)

MATCH LINE STATION 96+00.00

SEC. 7, T. 19N., R. 9E., 3RD P.M.

NOTE: **
STATION AND OFFSET SHOWN ARE BASED
OFF OF WRIGHT ST. CENTERLINE.

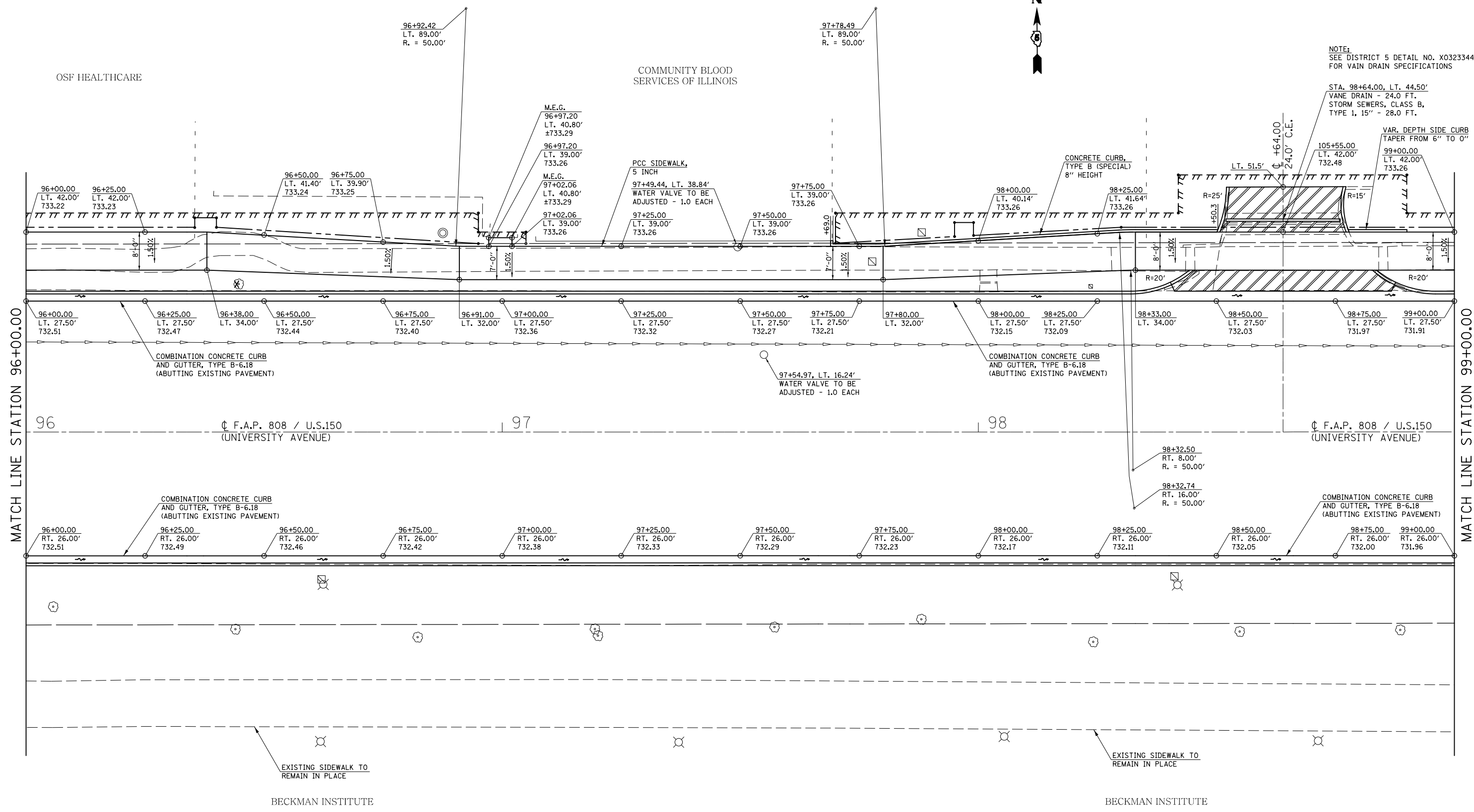
MATCH LINE STATION 93+50

NOTE: **
STATION AND OFFSET SHOWN ARE BASED
OFF OF WRIGHT ST. CENTERLINE.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-sh-curb.dgn		DRAWN -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	109
PLOT SCALE = 20.0000' / in.		CHECKED -	REVISED -					CONTRACT NO. 70B53				
\$MODELNAME\$		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

SCALE: 1" = 10' SHEET 1 OF 33 SHEETS STA. 93+00.00 TO STA. 96+00.00

SEC. 7, T. 19N., R. 9E., 3RD P.M.



NOTE:
SEE DISTRICT 5 DETAIL NO. X0323344
FOR VAIN DRAIN SPECIFICATIONS

STA. 98+64.00, LT. 44.50'
VANE DRAIN - 24.0 FT.
STORM SEWERS, CLASS B,
TYPE 1, 15" - 28.0 FT.

MATCH LINE STATION 96+00.00

MATCH LINE STATION 99+00.00

96 F.A.P. 808 / U.S.150 (UNIVERSITY AVENUE) 97 98 F.A.P. 808 / U.S.150 (UNIVERSITY AVENUE)

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (ABUTTING EXISTING PAVEMENT)

BECKMAN INSTITUTE

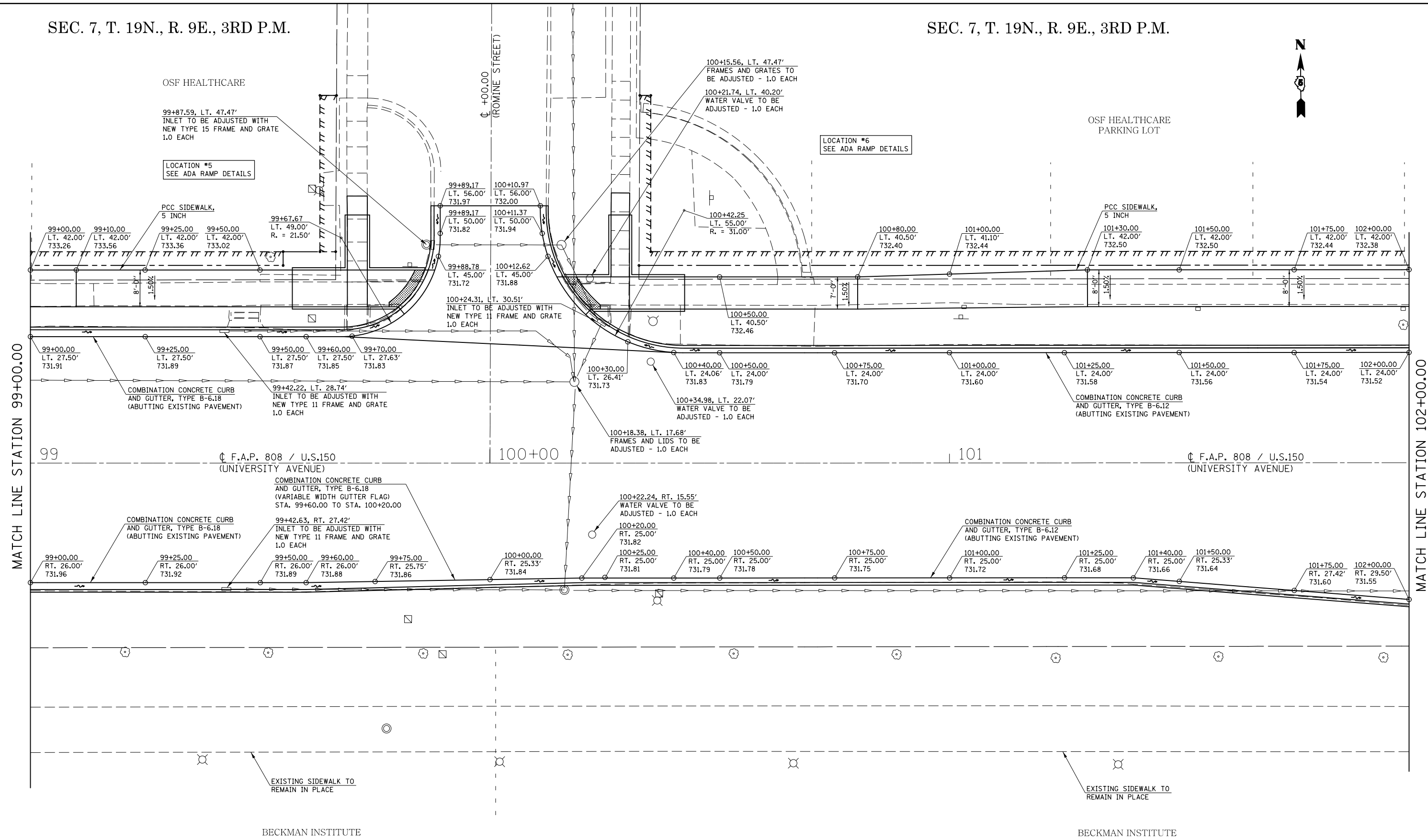
P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 7, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
p:\11084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn								808	D5 HSIP 2016-1	CHAMPAIGN	499	110			
PLOT SCALE = 20.0000' / in.								CHECKED -	REVISED -	CONTRACT NO. 70B53					
PLOT DATE = 1/28/2019								DATE -	REVISED -	ILLINOIS FED. AID PROJECT					
				SCALE: 1" = 10'			SHEET 2 OF 33 SHEETS			STA. 96+00.00 TO STA. 99+00.00					

SEC. 7, T. 19N., R. 9E., 3RD P.M.

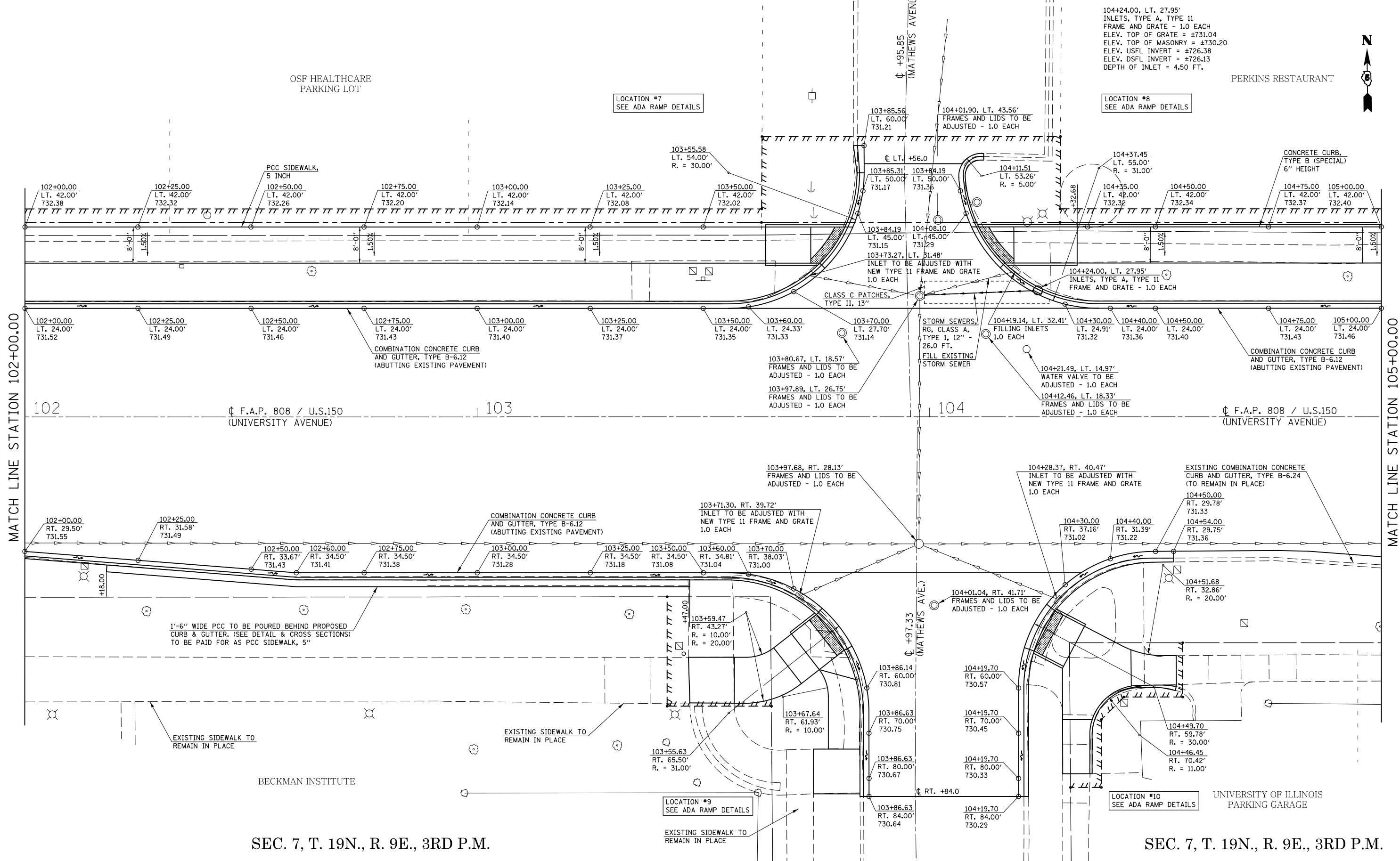
SEC. 7, T. 19N., R. 9E., 3RD P.M.



SEC. 7, T. 19N., R. 9E., 3RD P.M.

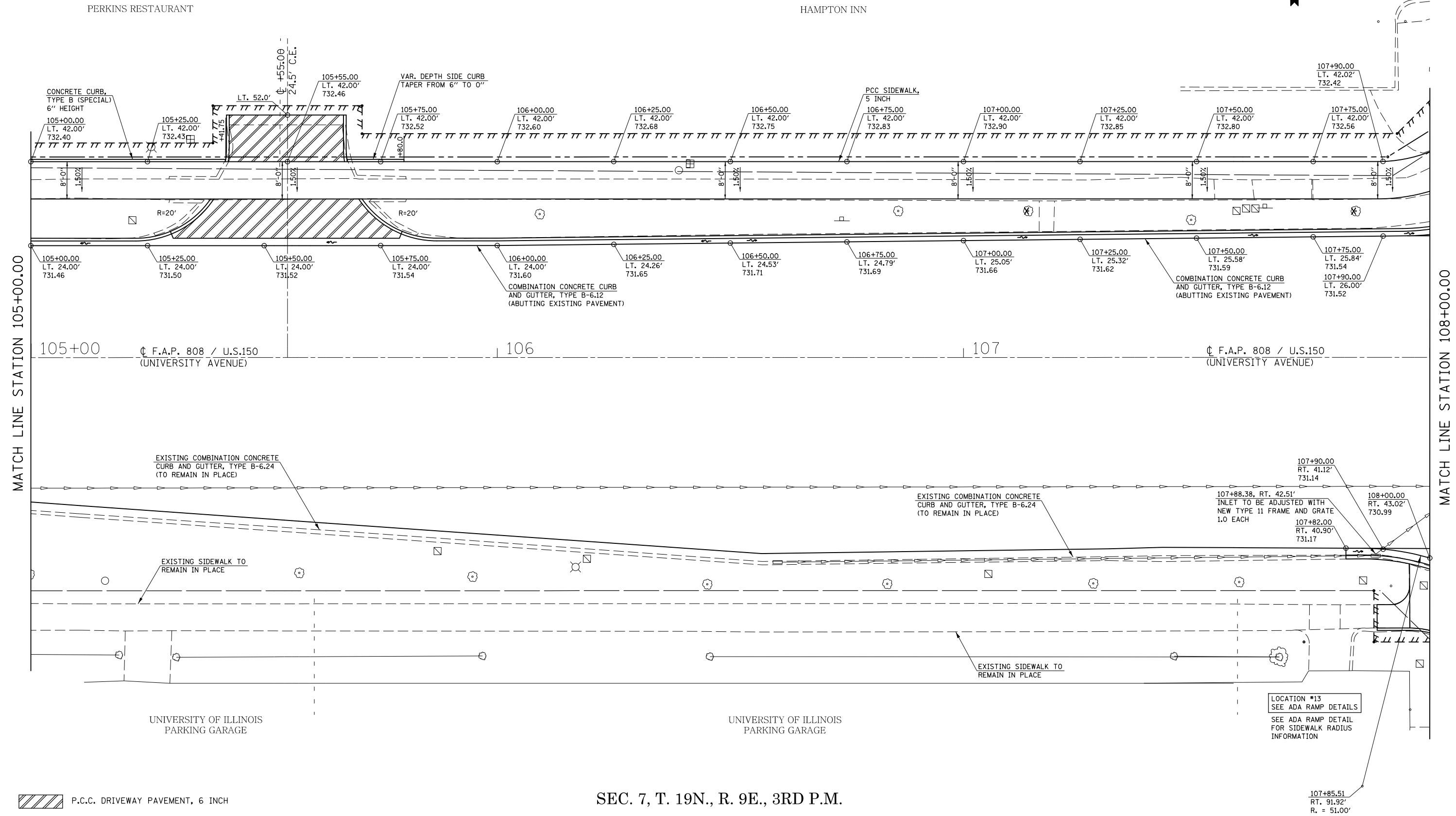
FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\057\DRAWING\0570853-shr-curb.dgn		CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	111	
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -			CONTRACT NO. 70B53					
	PLOT DATE = 1/28/2019					ILLINOIS FED. AID PROJECT					

SCALE: 1" = 10' SHEET 3 OF 33 SHEETS STA. 99+00.00 TO STA. 102+00.00



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn		CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	112	
\$MODELNAME\$		DATE -	REVISED -			CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT		

SEC. 7, T. 19N., R. 9E., 3RD P.M.

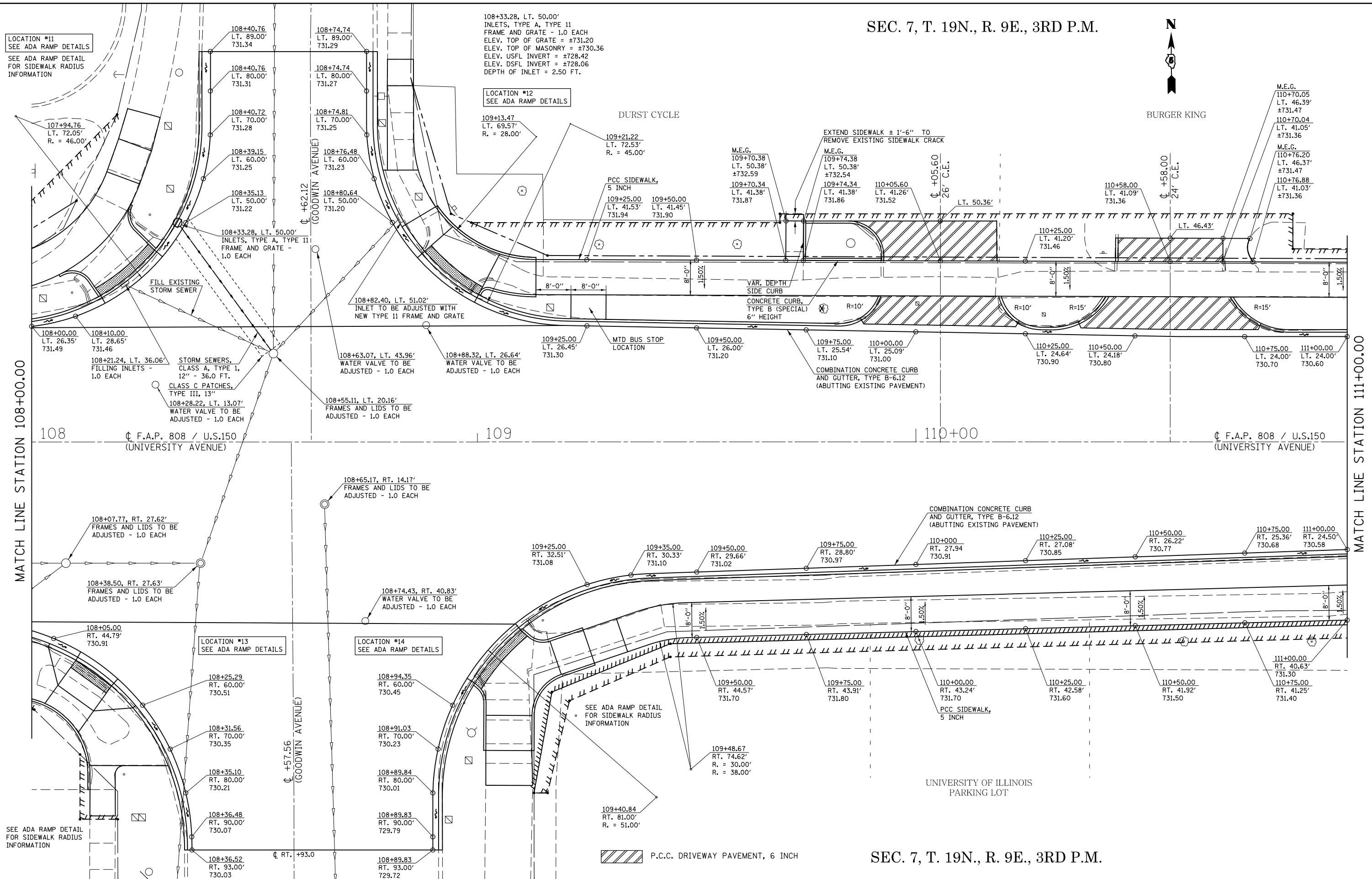


MATCH LINE STATION 105+00.00

MATCH LINE STATION 108+00.00

SEC. 7, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\11\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn								SCALE: 1" = 10'	SHEET 5	OF 33 SHEETS	STA. 105+00.00	TO STA. 108+00.00	808	D5 HSIP 2016-1
#MODELNAME#	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT			
	PLOT DATE = 1/28/2019	DATE -	REVISED -											



LOCATION #11
SEE ADA RAMP DETAILS
SEE ADA RAMP DETAIL
FOR SIDEWALK RADIUS
INFORMATION

LOCATION #12
SEE ADA RAMP DETAILS

LOCATION #13
SEE ADA RAMP DETAILS

LOCATION #14
SEE ADA RAMP DETAILS

MATCH LINE STATION 108+00.00

MATCH LINE STATION 111+00.00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

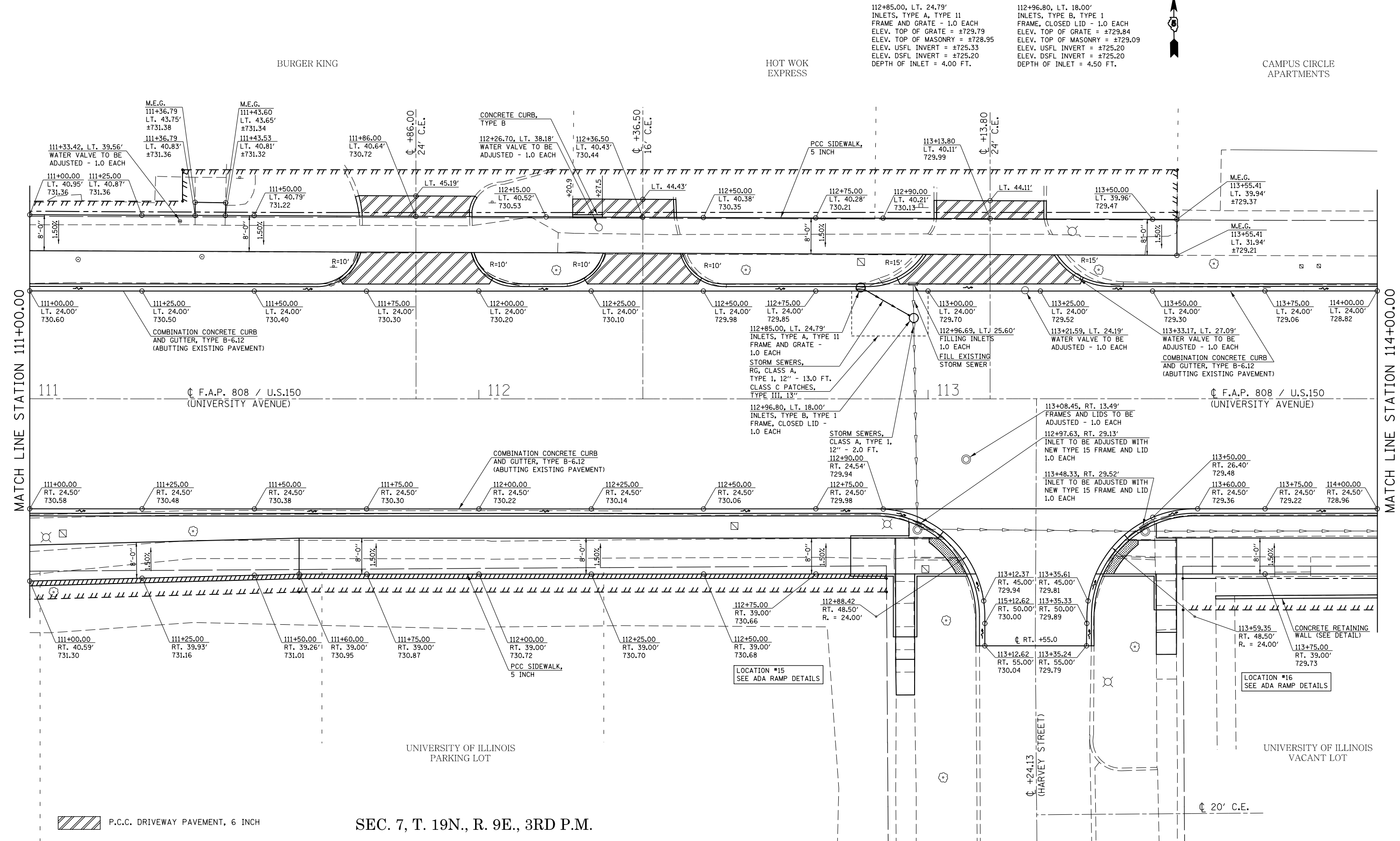
CURB & GUTTER PLAN SHEETS

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
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PLOT SCALE = 20.0000' / in.		CHECKED -	REVISED -
MODELNAME =		DATE -	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	114
			CONTRACT NO. 70B53	
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 10' SHEET 6 OF 33 SHEETS STA. 108+00.00 TO STA. 111+00.00

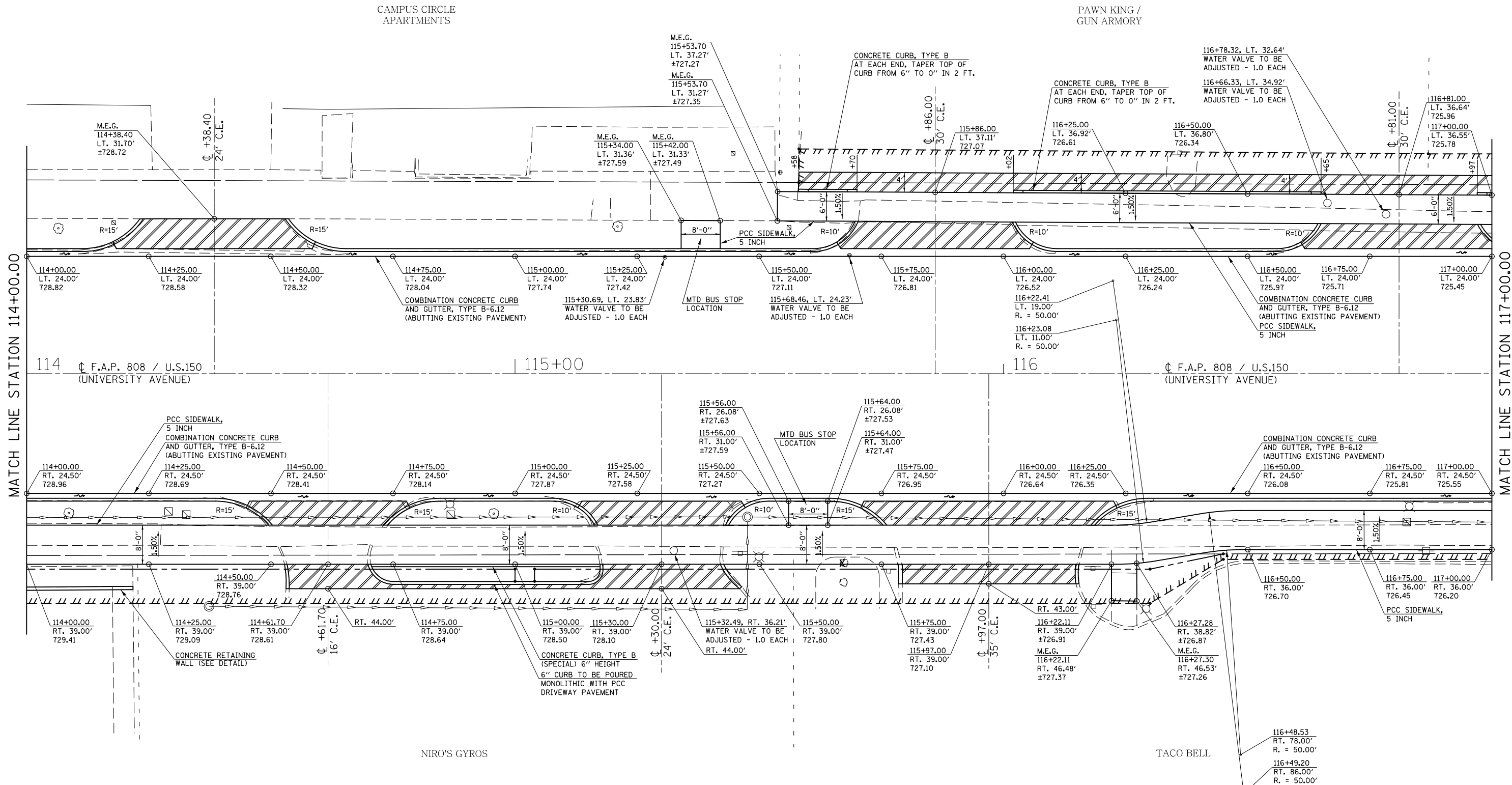
SEC. 7, T. 19N., R. 9E., 3RD P.M.



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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#MODELNAME#	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -					CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	
	PLOT DATE 1/28/2019							SCALE: 1" = 10'	SHEET 7 OF 33 SHEETS	STA. 111+00.00 TO STA. 114+00.00		

SEC. 7, T. 19N., R. 9E., 3RD P.M.

NOTE:
 PROVIDE DRAINAGE OPENINGS IN CONCRETE CURB, TYPE B AS NECESSARY PER DISTRICT DETAIL 60600605

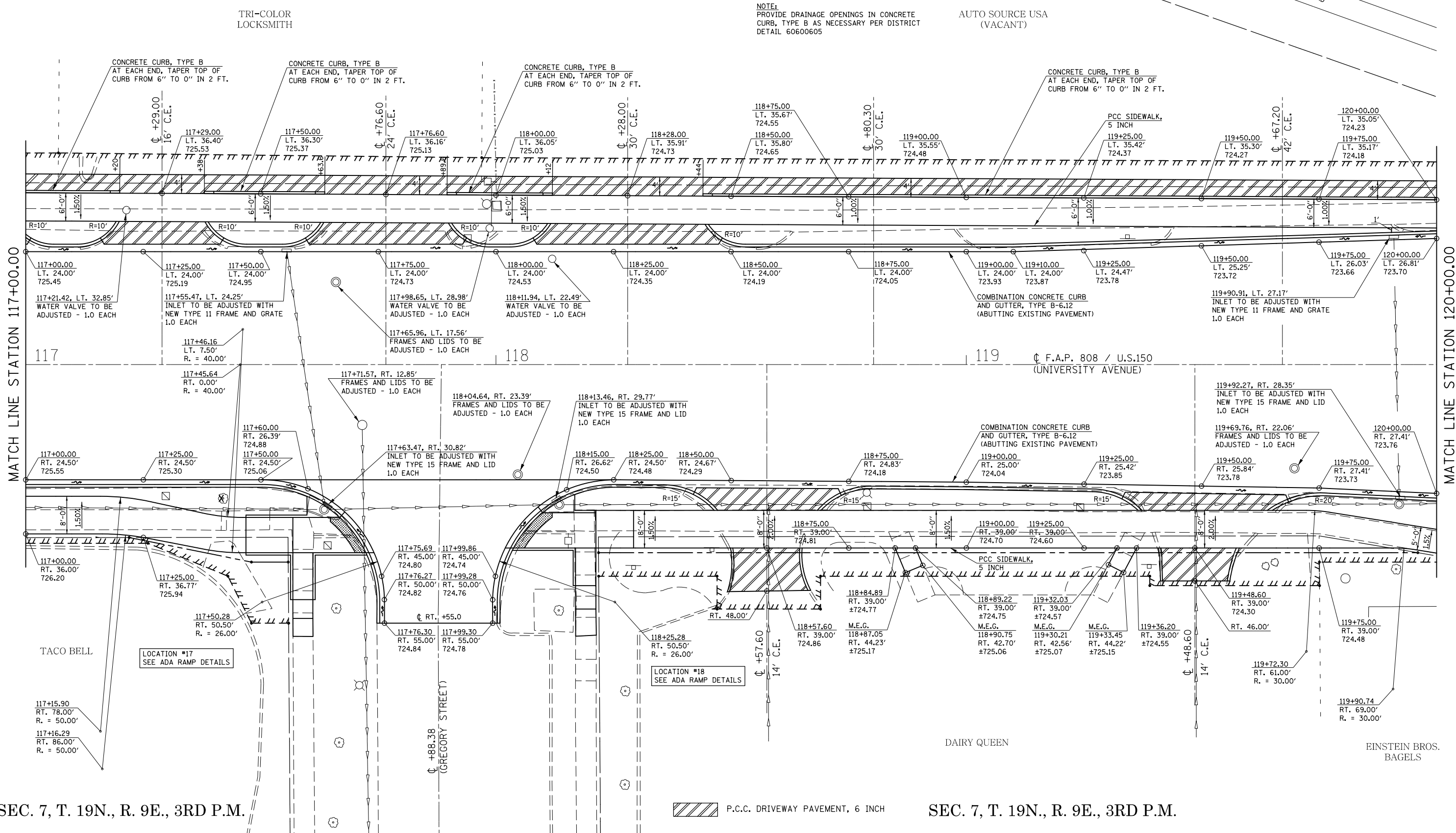


P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 7, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	116
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10' SHEET 8 OF 33 SHEETS STA. 114+00.00 TO STA. 117+00.00			CONTRACT NO. 70B53				
							ILLINOIS FED. AID PROJECT					

SEC. 7, T. 19N., R. 9E., 3RD P.M.



NOTE:
 PROVIDE DRAINAGE OPENINGS IN CONCRETE CURB, TYPE B AS NECESSARY PER DISTRICT DETAIL 60600605

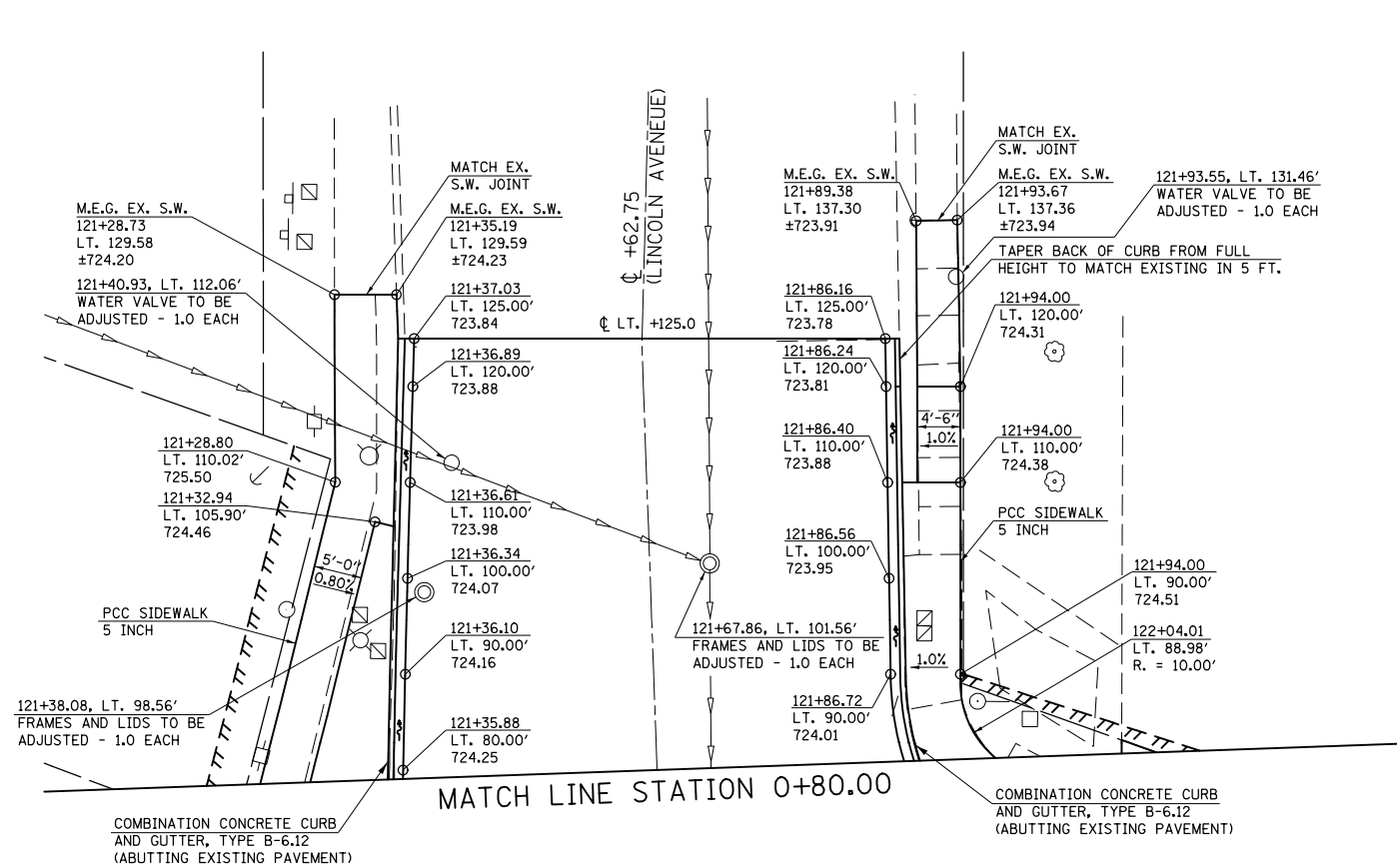
AUTO SOURCE USA
 (VACANT)

SEC. 7, T. 19N., R. 9E., 3RD P.M.

SEC. 7, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\1\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 5\Projects\057\Drawings\Design\0570853-sh-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	117	
MODELNAME =	PLOT DATE = 1/28/2019	DATE -	REVISED -			SCALE: 1" = 10'		SHEET 9 OF 33 SHEETS		STA. 117+00.00 TO STA. 120+00.00	ILLINOIS FED. AID PROJECT
						CONTRACT NO. 70B53					

NORTH LEG LINCOLN AVE.



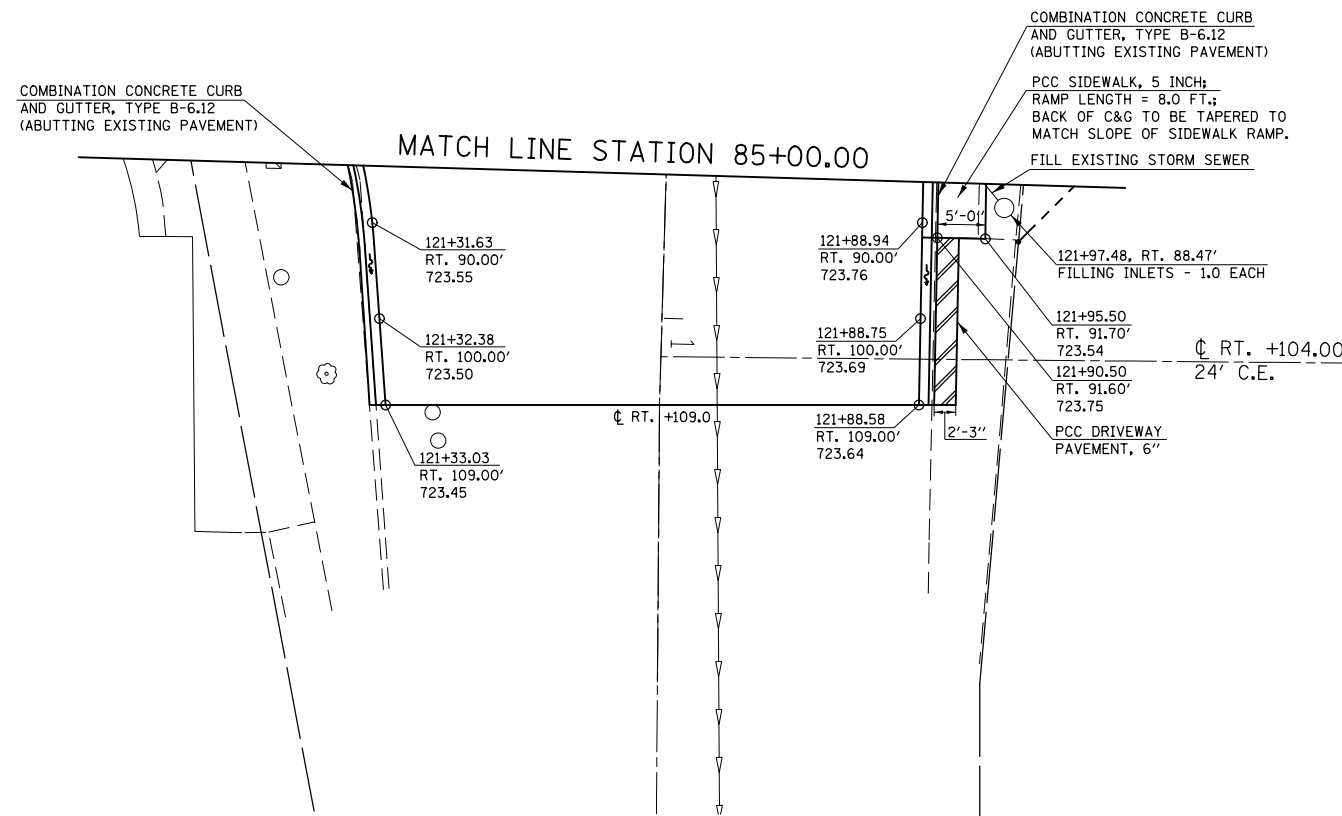
122+13.87, LT. 60.00'
 INLETS, TYPE A, TYPE 11
 FRAME AND GRATE - 1.0 EACH
 ELEV. TOP OF GRATE = ±723.78
 ELEV. TOP OF MASONRY = ±722.95
 ELEV. USFL INVERT = ±721.12
 ELEV. DSFL INVERT = ±720.69
 DEPTH OF INLET = 2.00 FT.

122+56.00, LT. 63.80'
 INLETS, TYPE A,
 TYPE 8 GRATE - 1.0 EACH
 ELEV. TOP OF GRATE = ±724.16
 ELEV. TOP OF MASONRY = ±723.91
 ELEV. USFL INVERT = ±721.08
 ELEV. DSFL INVERT = ±720.98
 DEPTH OF INLET = 3.00 FT.

122+57.50, LT. 53.00'
 INLETS, TYPE B, TYPE 1
 FRAME, CLOSED LID - 1.0 EACH
 ELEV. TOP OF GRATE = ±724.50
 ELEV. TOP OF MASONRY = ±723.75
 ELEV. USFL INVERT = ±718.14
 ELEV. DSFL INVERT = ±718.14
 DEPTH OF INLET = 6.00 FT.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\Drawings\Design\0579B53-shr-curb.dgn	DESIGNED -	REVISED -	808					D5 HSIP 2016-1	CHAMPAIGN	499	119	
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70B53							
	PLOT DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10'	SHEET 11 OF 33 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				

SOUTH LEG LINCOLN AVE.



122+04.00, RT. 78.00'
 INLETS, TYPE A,
 TYPE 8 GRATE - 1.0 EACH
 ELEV. TOP OF GRATE = ±723.05
 ELEV. TOP OF MASONRY = ±722.80
 ELEV. USFL INVERT = ±720.36
 ELEV. DSFL INVERT = ±720.22
 DEPTH OF INLET = 3.00 FT.

122+30.00, RT. 52.00'
 INLETS, TYPE A,
 TYPE 8 GRATE - 1.0 EACH
 ELEV. TOP OF GRATE = ±723.25
 ELEV. TOP OF MASONRY = ±723.00
 ELEV. USFL INVERT = ±720.80
 ELEV. DSFL INVERT = ±720.44
 DEPTH OF INLET = 2.50 FT.

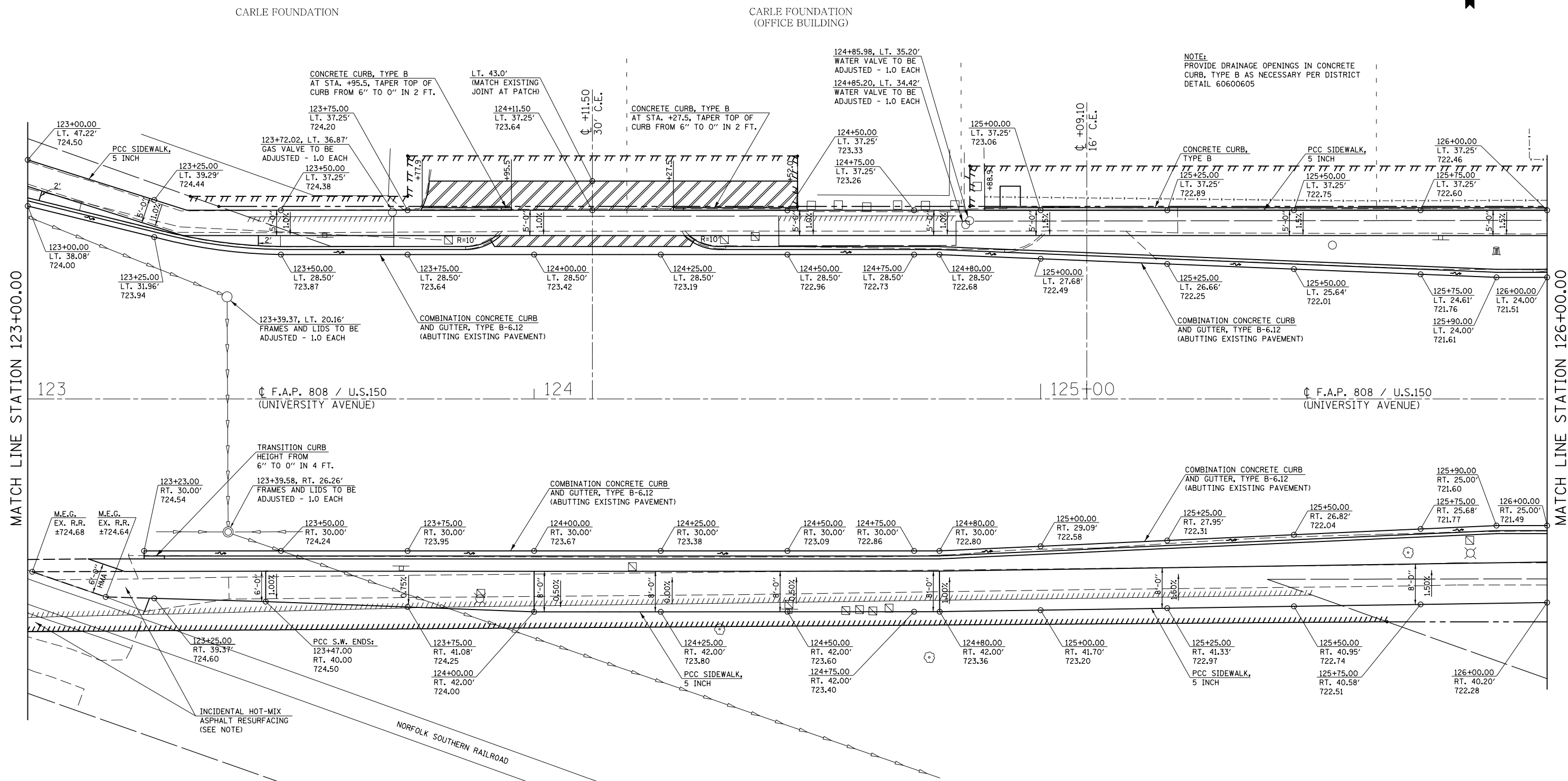
SEC. 7, T. 19N., R. 9E., 3RD P.M.

P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\0570853-shr-curb.dgn								SCALE: 1" = 10'			808	D5 HSIP 2016-1
PLOT SCALE = 20.0000' / in.					SHEET 12 OF 33 SHEETS			CONTRACT NO. 70B53				
PLOT DATE = 1/28/2019					STA. TO STA.			ILLINOIS FED. AID PROJECT				

SEC. 8, T. 19N., R. 9E., 3RD P.M.



NOTE:
 THE PROPOSED PCC C&G AND PCC SIDEWALK SHALL TERMINATE A MINIMUM OF 10 FT. FROM THE NEAREST RAIL AS SHOWN IN THE PLANS. THE SPACE BETWEEN THESE ITEMS AND THE RAILROAD TRACKS WILL BE RESURFACED WITH INCIDENTAL HOT-MIX ASPHALT RESURFACING.

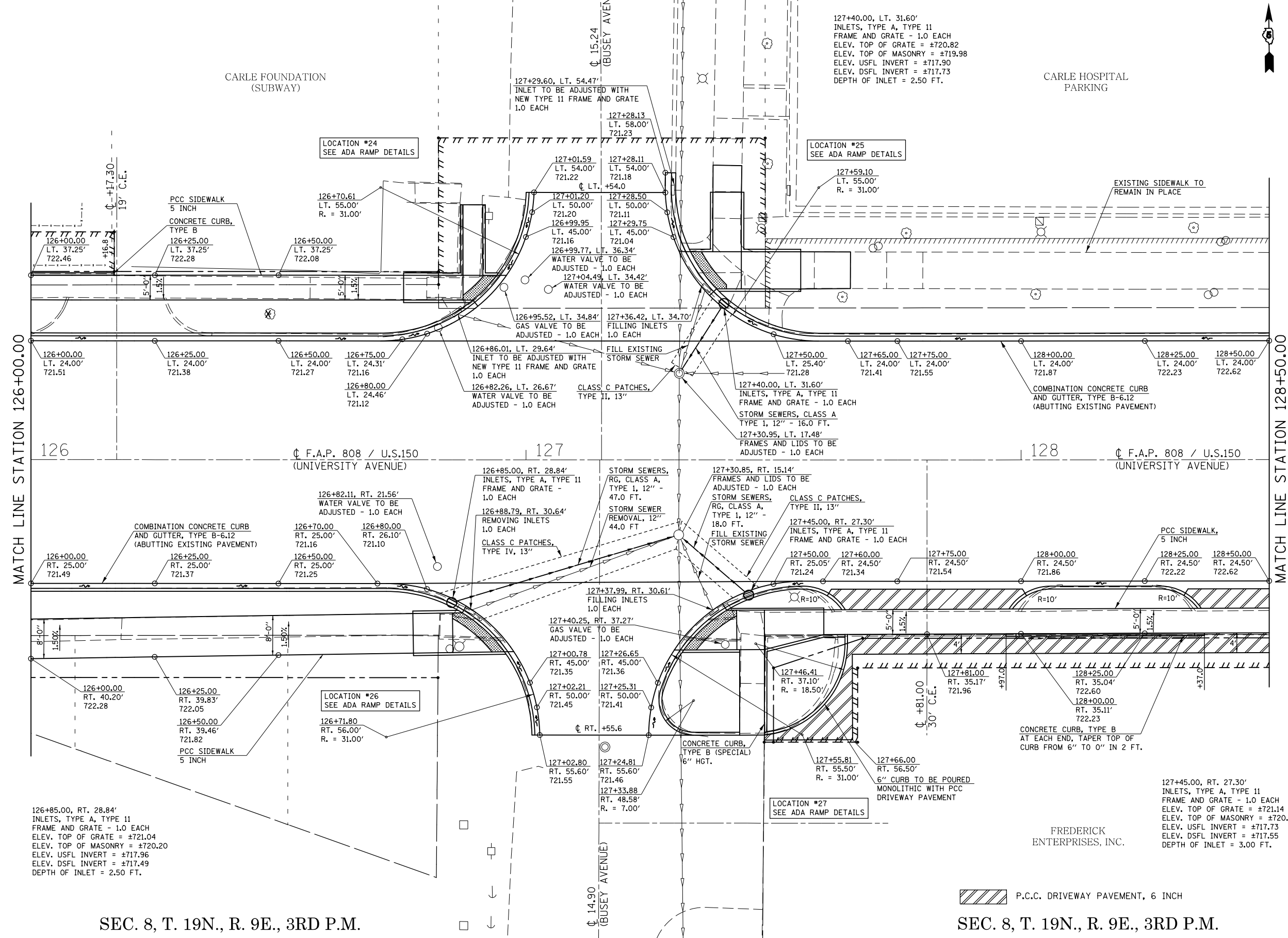
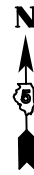
P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\BID\Drawings\Design\0570853-sh-curb.dgn		CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	121
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -					CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	
	PLOT DATE = 1/28/2019							SCALE: 1" = 10'	SHEET 13 OF 33 SHEETS	STA. 123+00.00 TO STA. 126+00.00		

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

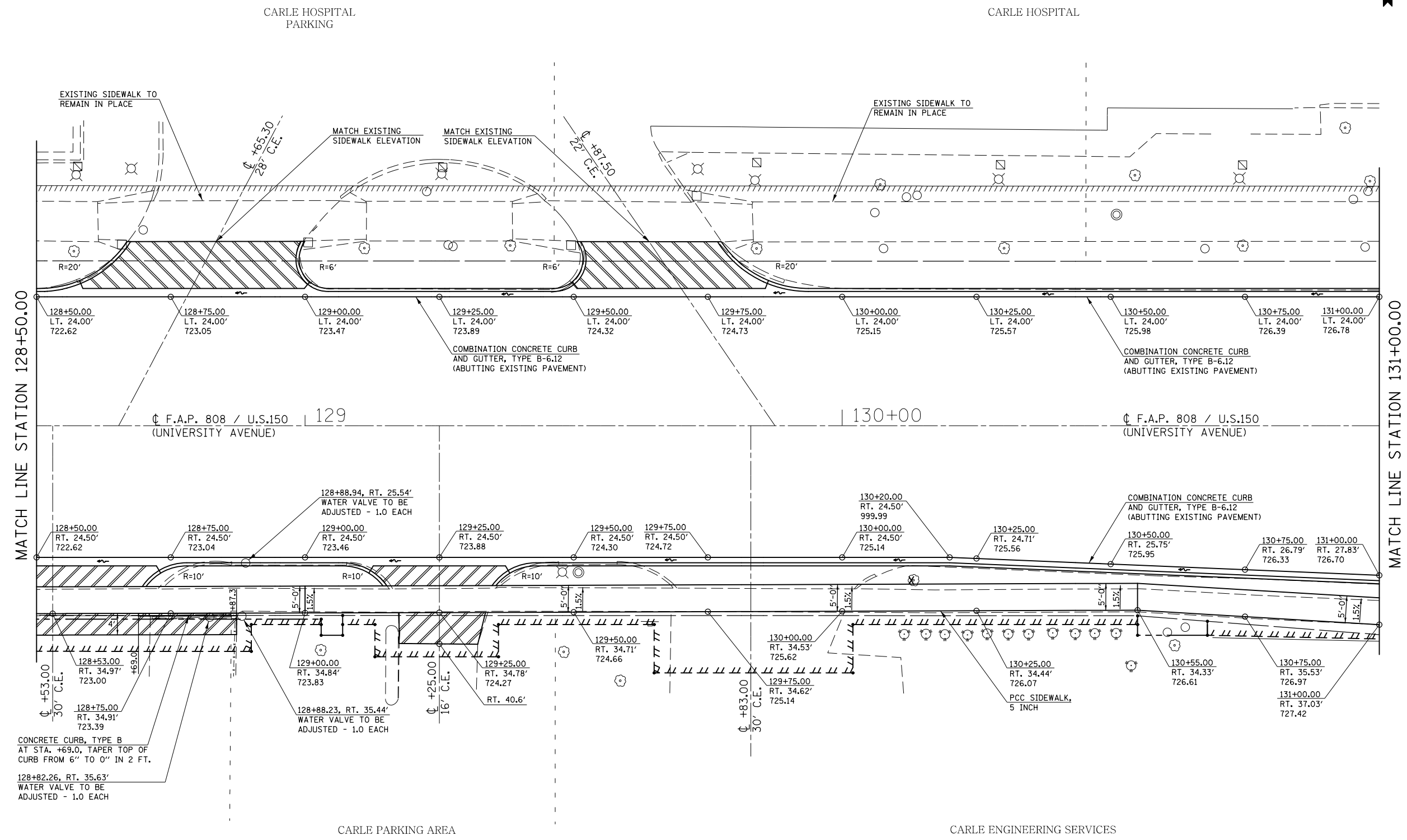


SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579B53-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	122	
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -			SCALE: 1" = 10' SHEET 14 OF 33 SHEETS STA. 126+00.00 TO STA. 126+50.00.		ILLINOIS FED. AID PROJECT CONTRACT NO. 70B53			

SEC. 8, T. 19N., R. 9E., 3RD P.M.



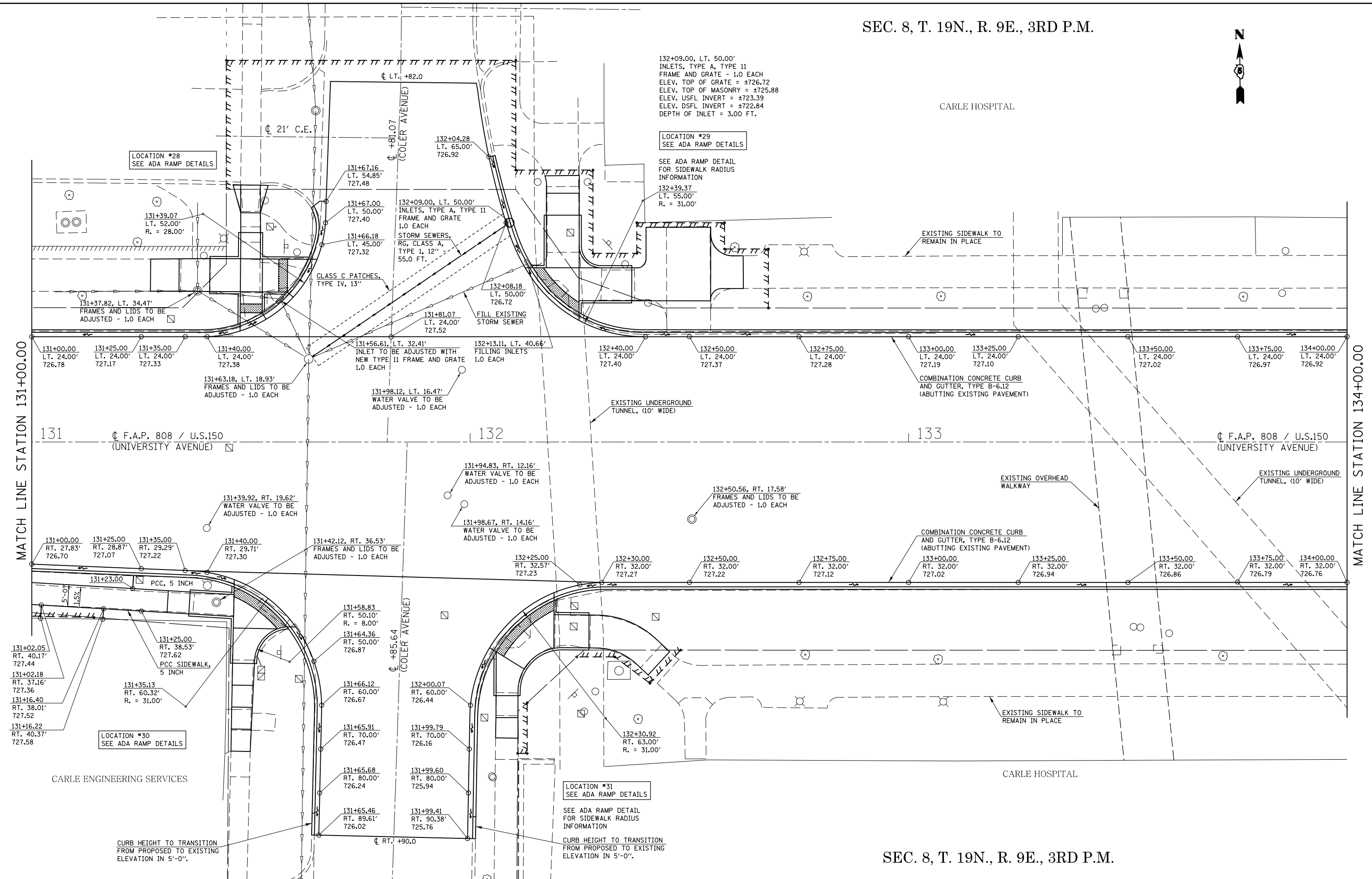
- P.C.C. DRIVEWAY PAVEMENT, 6 INCH
- P.C.C. DRIVEWAY PAVEMENT, 8 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\0579\Design\0579B53-shr-curb.dgn						808	D5 HSIP 2016-1	CHAMPAIGN	499	123	
PLOT SCALE = 20.0000' / in.						CONTRACT NO. 70B53					
PLOT DATE = 1/28/2019						ILLINOIS FED. AID PROJECT					
SCALE: 1" = 10'						SHEET 15 OF 33 SHEETS STA. 128+50.00 TO STA. 131+00.00					

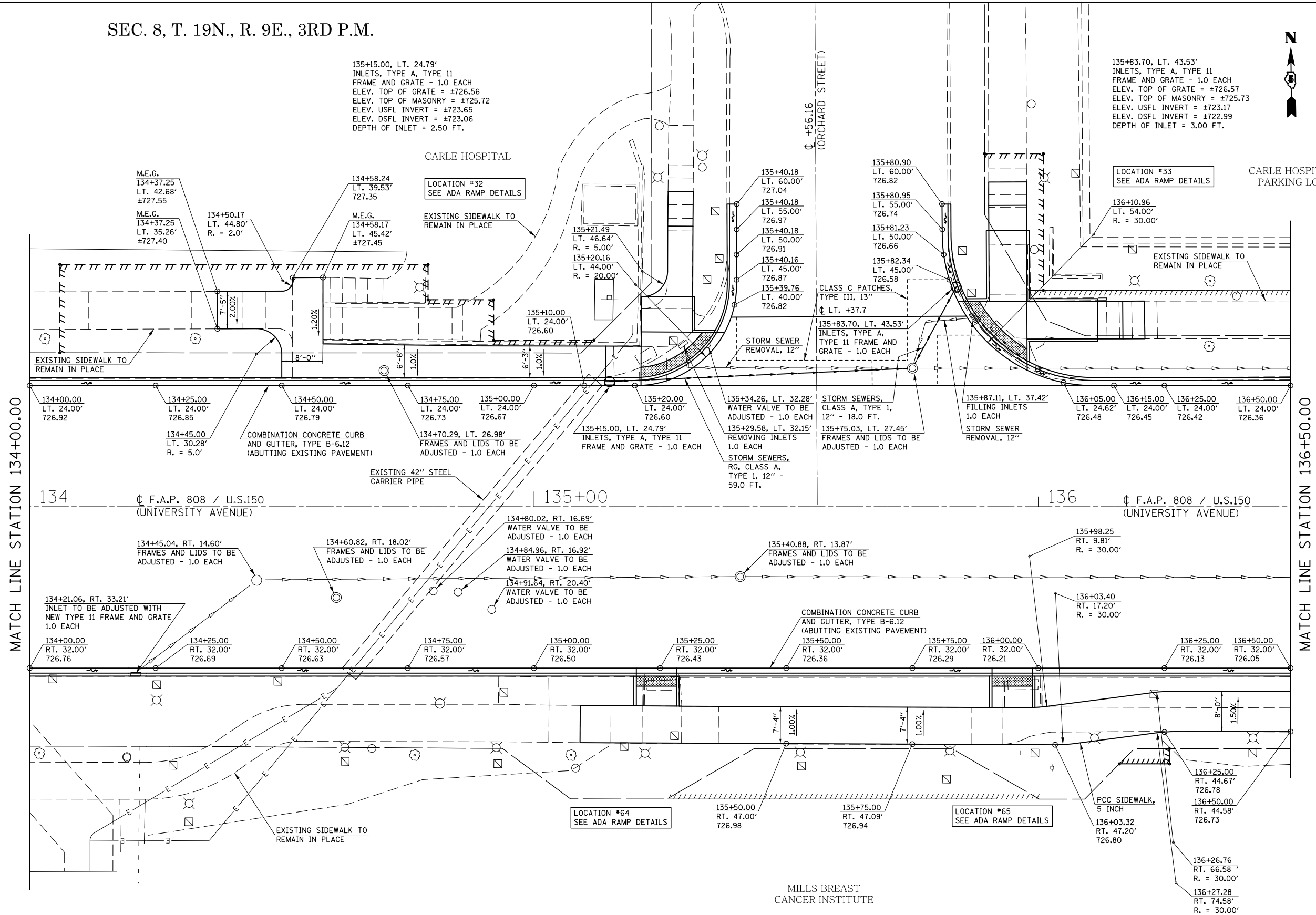


CARLE HOSPITAL



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579B53-shr-curb.dgn							REVISED -	808	D5 HSIP 2016-1	CHAMPAIGN	499	124	
PLOT SCALE = 20.0000' / in.							REVISED -	CONTRACT NO. 70B53					
PLOT DATE = 1/28/2019							REVISED -						
				SCALE: 1" = 10'		SHEET 16 OF 33 SHEETS		STA. 131+00.00 TO STA. 134+00.00		ILLINOIS FED. AID PROJECT			

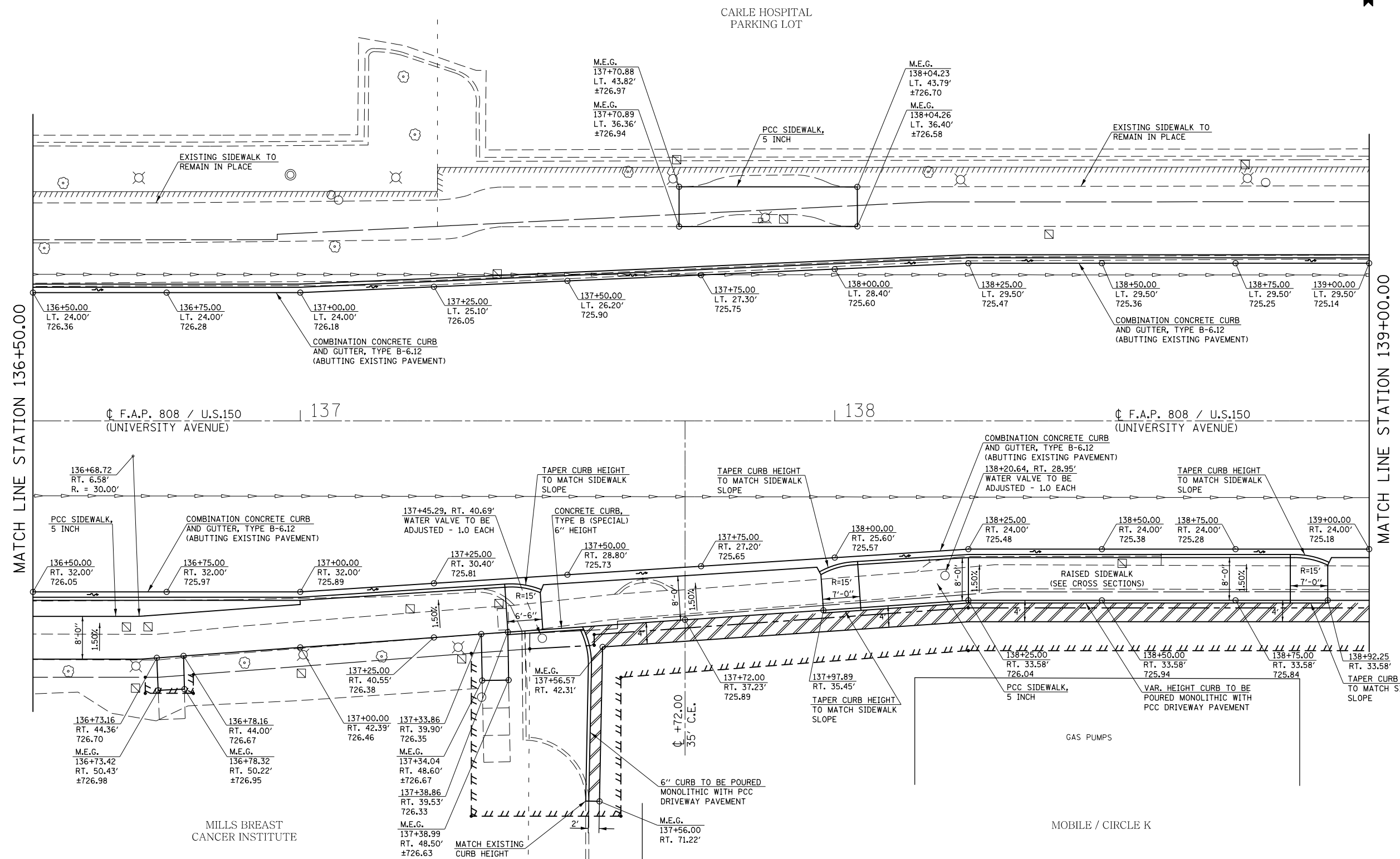
SEC. 8, T. 19N., R. 9E., 3RD P.M.



SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\1\084EBIDINTEG.illinois.gov\PIWIDOTDocuments\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn		CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	125	
PLOT SCALE = 20.0000' / in.		DATE -	REVISED -					SCALE: 1" = 10'			SHEET 17 OF 33 SHEETS STA. 134+00.00 TO STA. 136+50.00		
#MODELNAME#								ILLINOIS FED. AID PROJECT			CONTRACT NO. 70B53		

SEC. 8, T. 19N., R. 9E., 3RD P.M.



MATCH LINE STATION 136+50.00

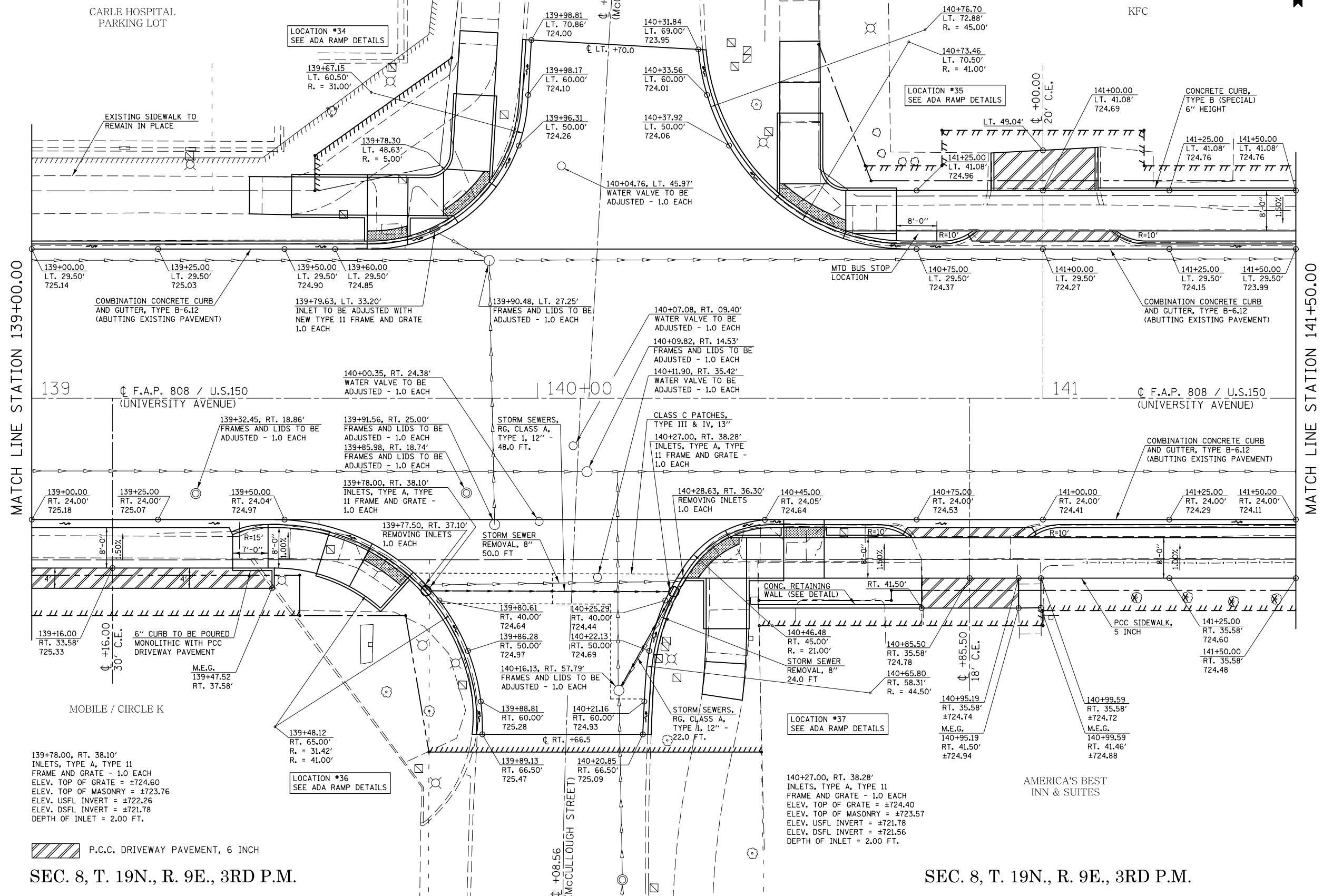
MATCH LINE STATION 139+00.00

P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\1\084EBID\INTEG.illinois.gov\PIWID\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-sh-curb.dgn								REVISED -	808	D5 HSIP 2016-1	CHAMPAIGN	499	126
PLOT SCALE = 20.0000' / in.								REVISED -	CONTRACT NO. 70B53				
PLOT DATE = 1/28/2019								REVISED -	ILLINOIS FED. AID PROJECT				
				SCALE: 1" = 10'			SHEET 18 OF 33 SHEETS STA. 136+50.00 TO STA. 139+00.00						



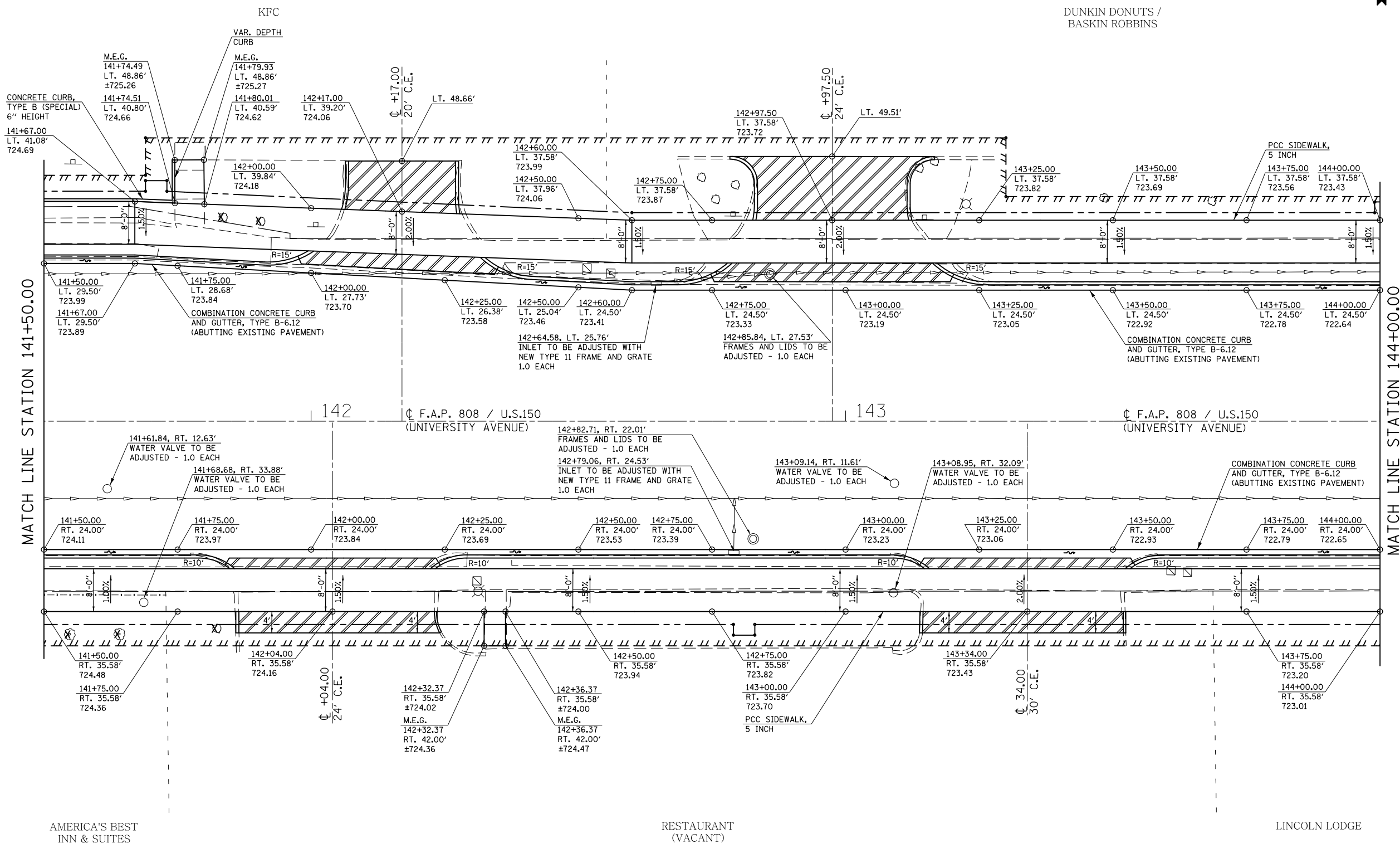
MOBILE / CIRCLE K

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-sh-curb.dgn		CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	127	
MODELNAME =	PLOT DATE = 1/28/2019	DATE -	REVISED -			CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT		
						SCALE: 1" = 10' SHEET 19 OF 33 SHEETS STA. 139+00.00 TO STA. 141+50.00					

SEC. 8, T. 19N., R. 9E., 3RD P.M.



P.C.C. DRIVEWAY PAVEMENT, 6 INCH

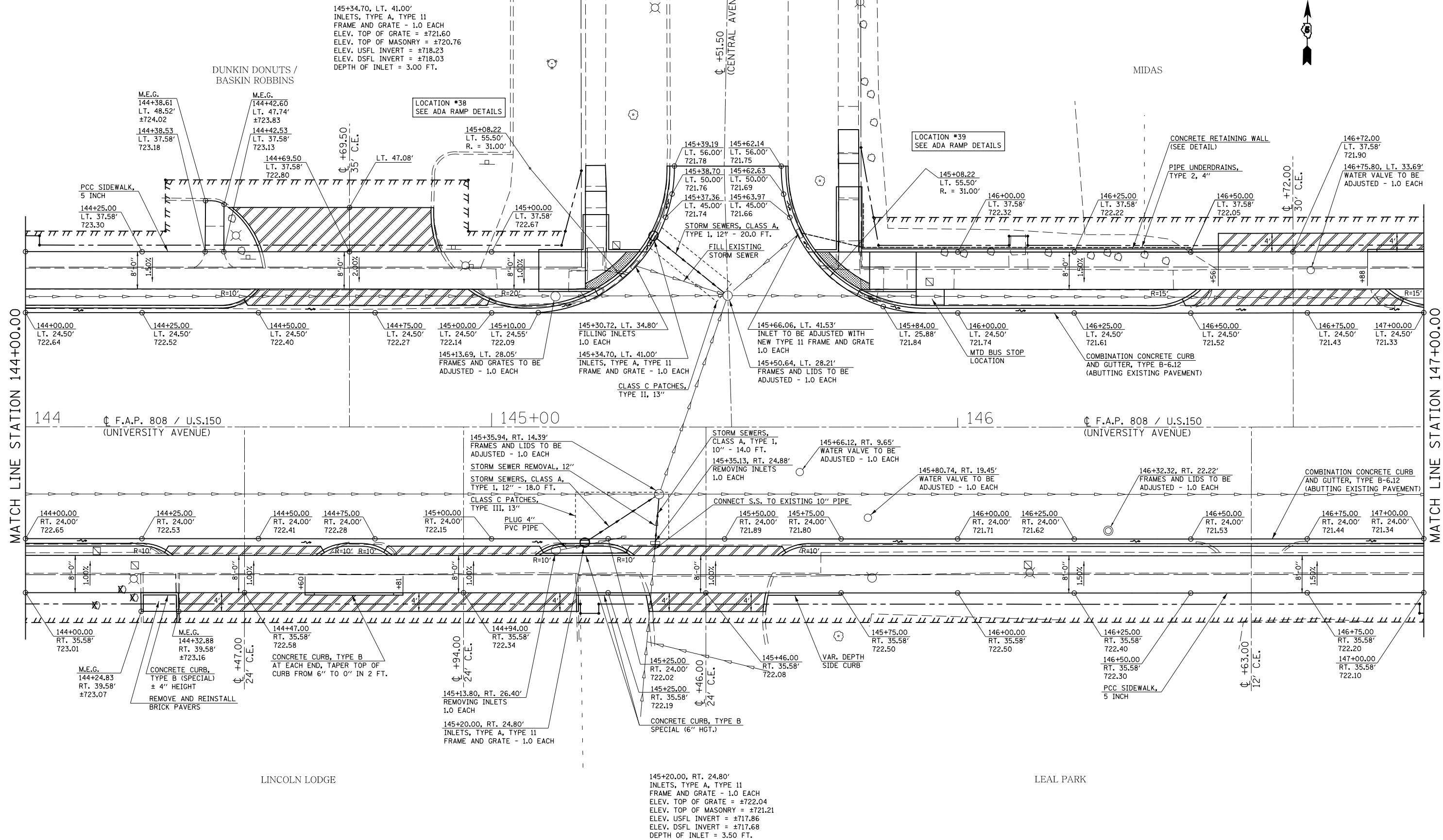
SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG.illinois.gov\PIWIDT\Documents\DOT Offices\District 5\Projects\057\DRAWING\057\0853-shr-curb.dgn						808	D5 HSIP 2016-1	CHAMPAIGN	499	128	
PLOT SCALE = 20.0000' / in.						CONTRACT NO. 70B53					
PLOT DATE = 1/28/2019						ILLINOIS FED. AID PROJECT					

SCALE: 1" = 10' SHEET 20 OF 33 SHEETS STA. 141+50.00 TO STA. 144+00.00

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.



MATCH LINE STATION 144+00.00

MATCH LINE STATION 147+00.00

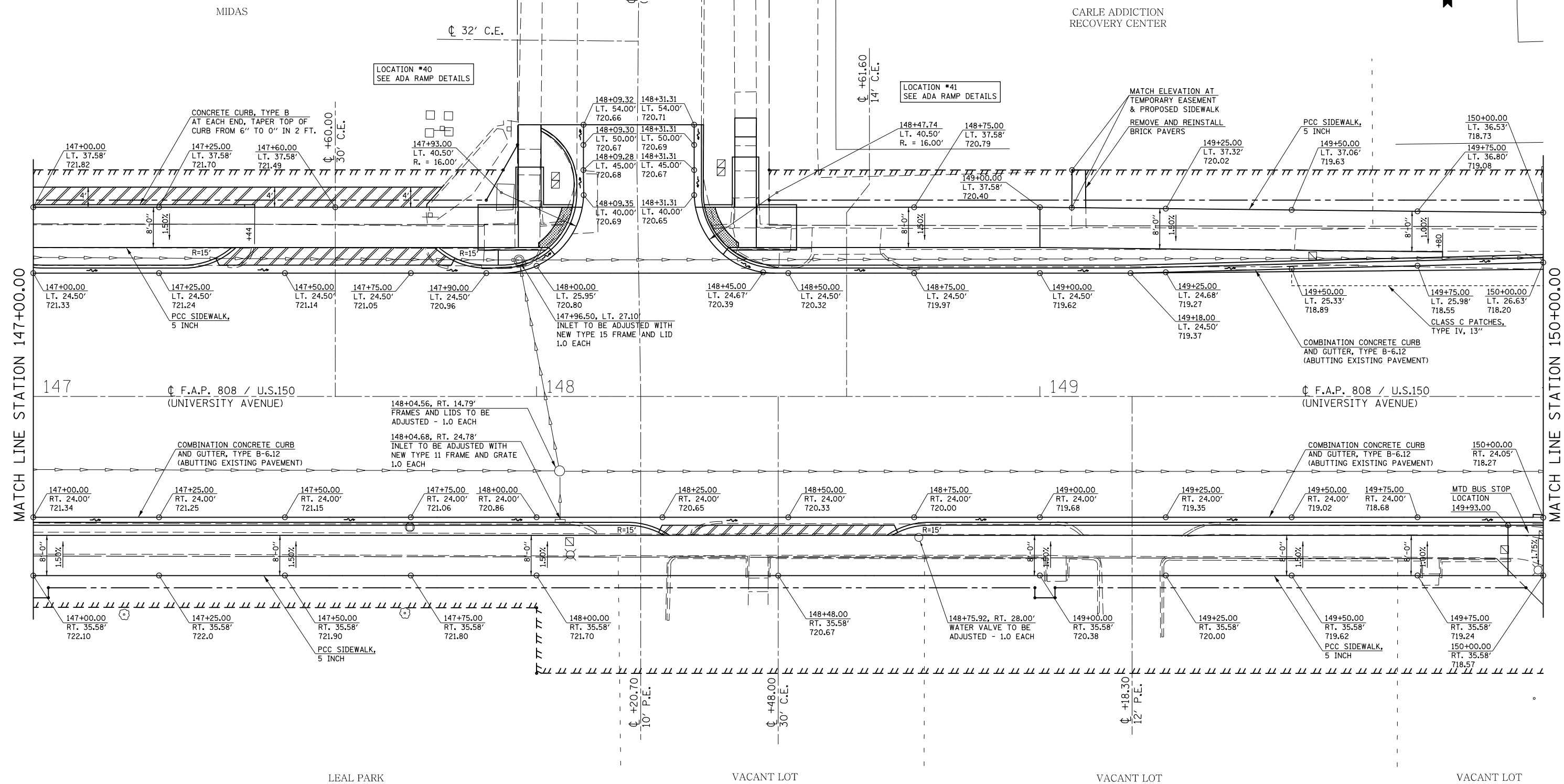
P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-sh-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			808	D5 HSIP 2016-1	CHAMPAIGN	499	129	
MODELNAME	PLOT DATE = 1/28/2019	DATE -	REVISED -			SCALE: 1" = 10'		SHEET 21 OF 33 SHEETS		STA. 144+00.00 TO STA. 147+00.00	ILLINOIS FED. AID PROJECT
						CONTRACT NO. 70B53					

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.



MATCH LINE STATION 147+00.00

MATCH LINE STATION 150+00.00

P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
pw:\IL084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn		CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB & GUTTER PLAN SHEETS

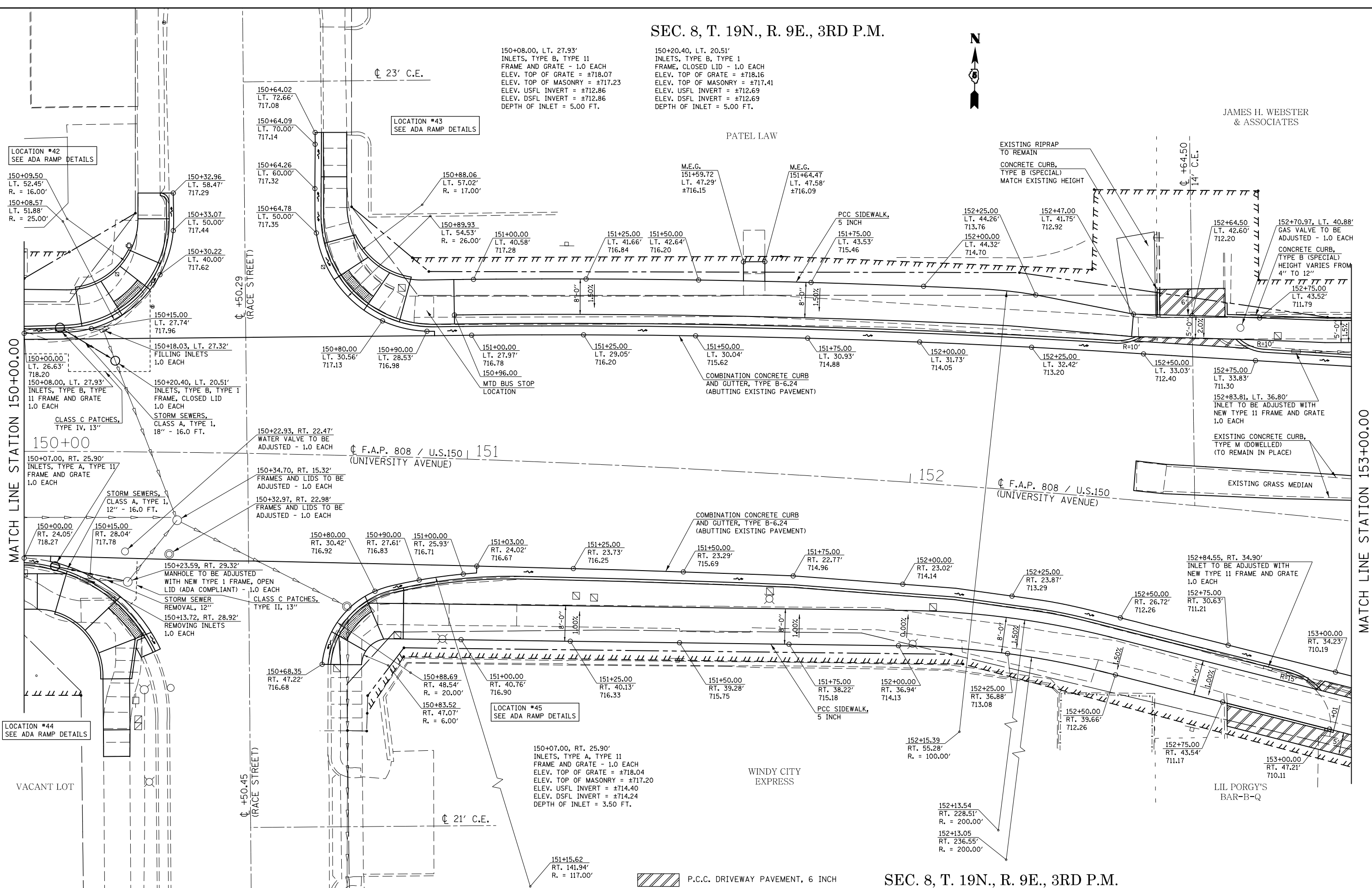
SCALE: 1" = 10' SHEET 22 OF 33 SHEETS STA. 147+00.00 TO STA. 150+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	130
ILLINOIS FED. AID PROJECT			CONTRACT NO. 70B53	

SEC. 8, T. 19N., R. 9E., 3RD P.M.



JAMES H. WEBSTER & ASSOCIATES



FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\1\084EBIDINTEG\illinois.gov\PIDOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn		CHECKED -	REVISED -
#MODELNAME#	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB & GUTTER PLAN SHEETS
SCALE: 1" = 10'
SHEET 23 OF 33 SHEETS
STA. 150+00.00 TO STA. 153+00.00

F.A.P. RTE. 808	SECTION D5 HSIP 2016-1	COUNTY CHAMPAIGN	TOTAL SHEETS 499	SHEET NO. 131
				CONTRACT NO. 70B53
ILLINOIS FED. AID PROJECT				

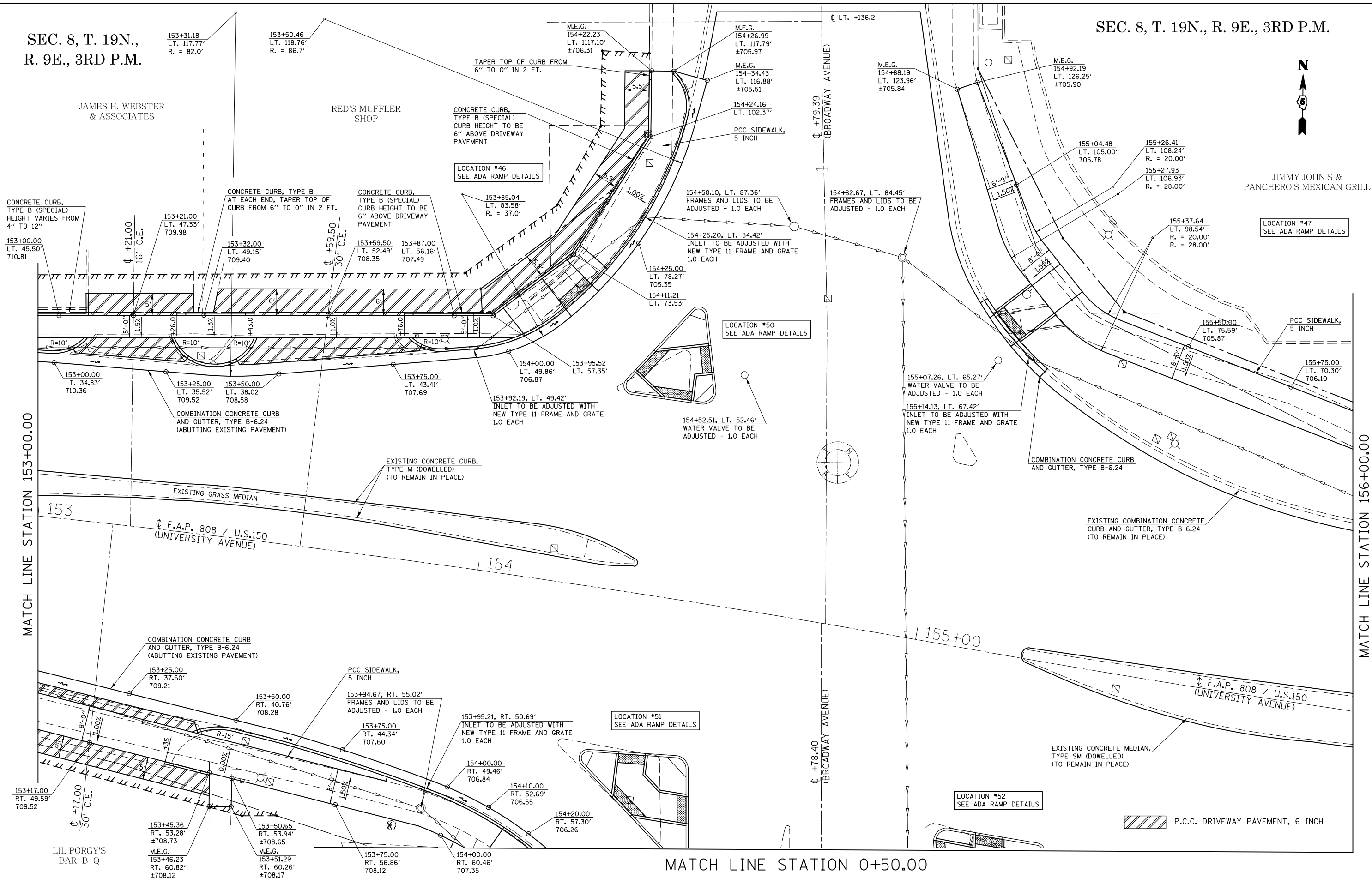
SEC. 8, T. 19N.,
R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

JAMES H. WEBSTER
& ASSOCIATES

RED'S MUFFLER SHOP

JIMMY JOHN'S &
PANCHERO'S MEXICAN GRILL



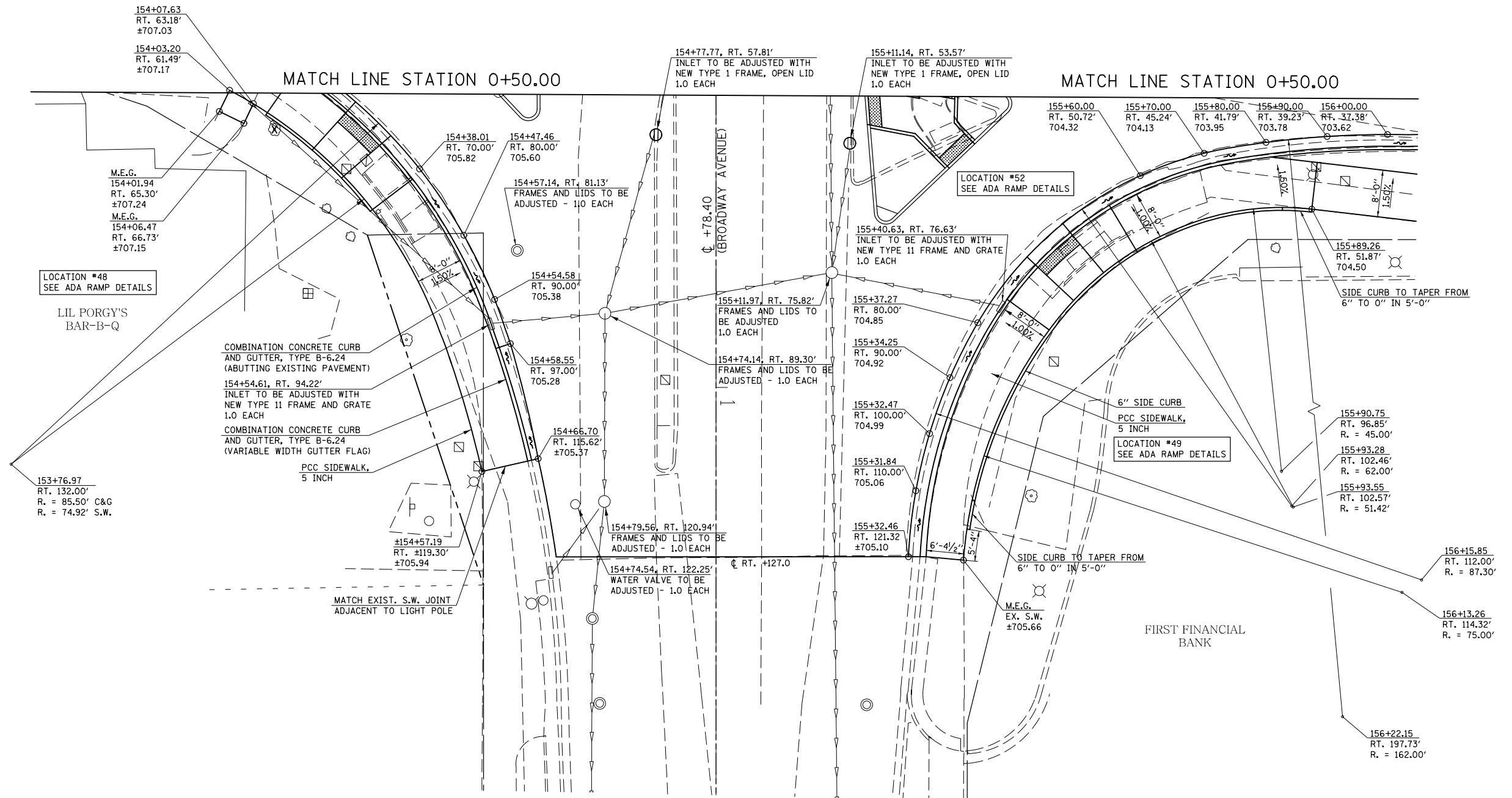
MATCH LINE STATION 153+00.00

MATCH LINE STATION 156+00.00

MATCH LINE STATION 0+50.00

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	132
MODELNAME	PLOT DATE = 1/28/2019	DATE -	REVISED -					SCALE: 1" = 10' SHEET 24 OF 33 SHEETS STA. 153+00.00 TO STA. 156+00.00			CONTRACT NO. 70B53	
								ILLINOIS FED. AID PROJECT				

SOUTH LEG BROADWAY AVENUE

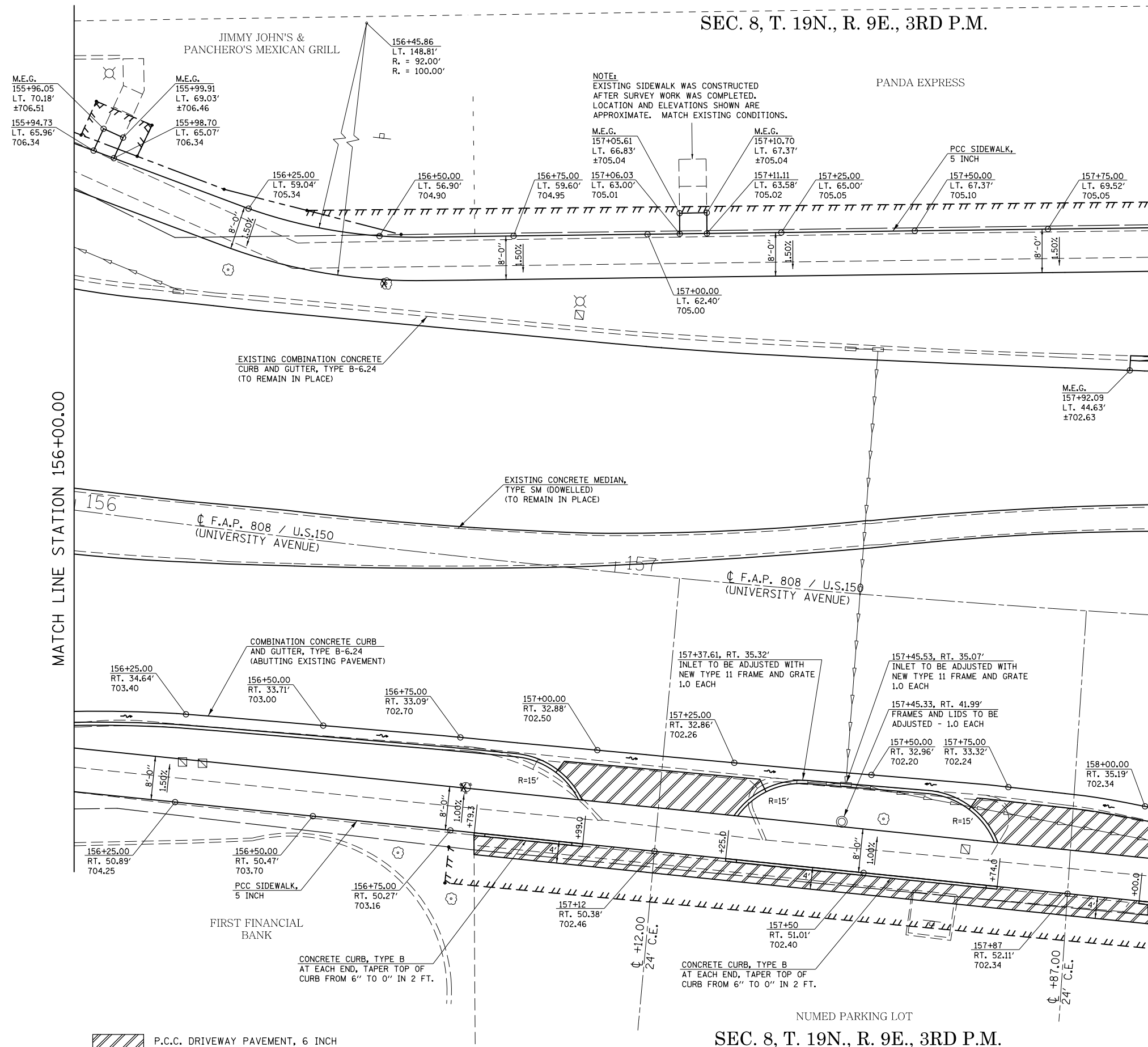


SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570853-shr-curb.dgn						REVISED -	808	D5 HSIP 2016-1	CHAMPAIGN	499	133
PLOT SCALE = 20.0000' / in.						REVISED -	CONTRACT NO. 70B53				
MODELNAME						REVISED -	ILLINOIS FED. AID PROJECT				
PLOT DATE = 1/28/2019		DATE -		SCALE: 1" = 10'		SHEET 25 OF 33 SHEETS		STA. TO STA.			

SEC. 8, T. 19N., R. 9E., 3RD P.M.



MATCH LINE STATION 156+00.00

MATCH LINE STATION 158+00.00

P.C.C. DRIVEWAY PAVEMENT, 6 INCH

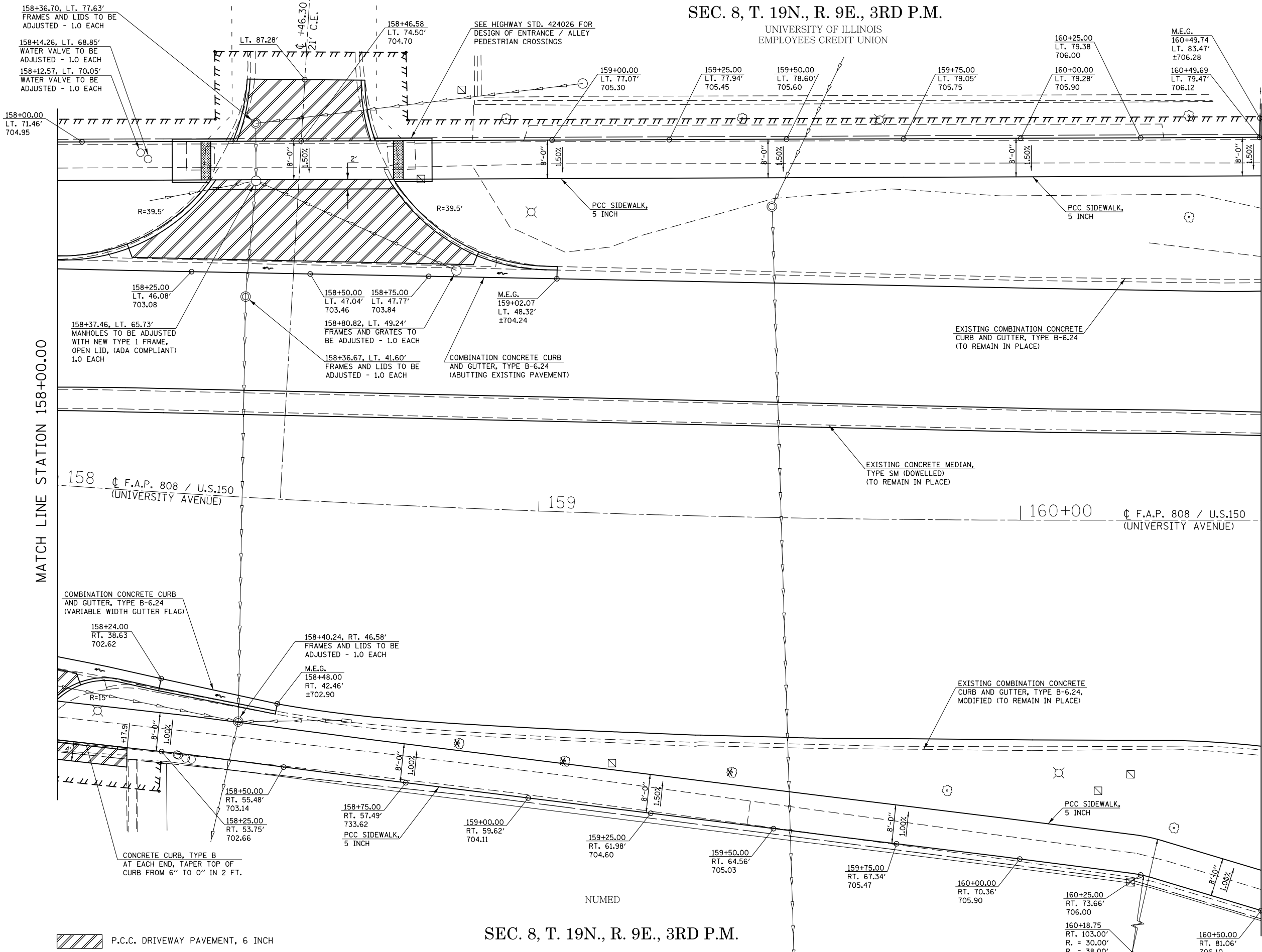
SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0577\Drawings\Design\05770853-sh-curb.dgn					DRAWN				808	D5 HSIP 2016-1	CHAMPAIGN	499	134
PLOT SCALE = 20.0000' / in.					CHECKED -				CONTRACT NO. 70B53				
\$MODELNAME\$					DATE -				ILLINOIS FED. AID PROJECT				

SCALE: 1" = 10' SHEET 26 OF 33 SHEETS STA. TO STA.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

UNIVERSITY OF ILLINOIS
EMPLOYEES CREDIT UNION

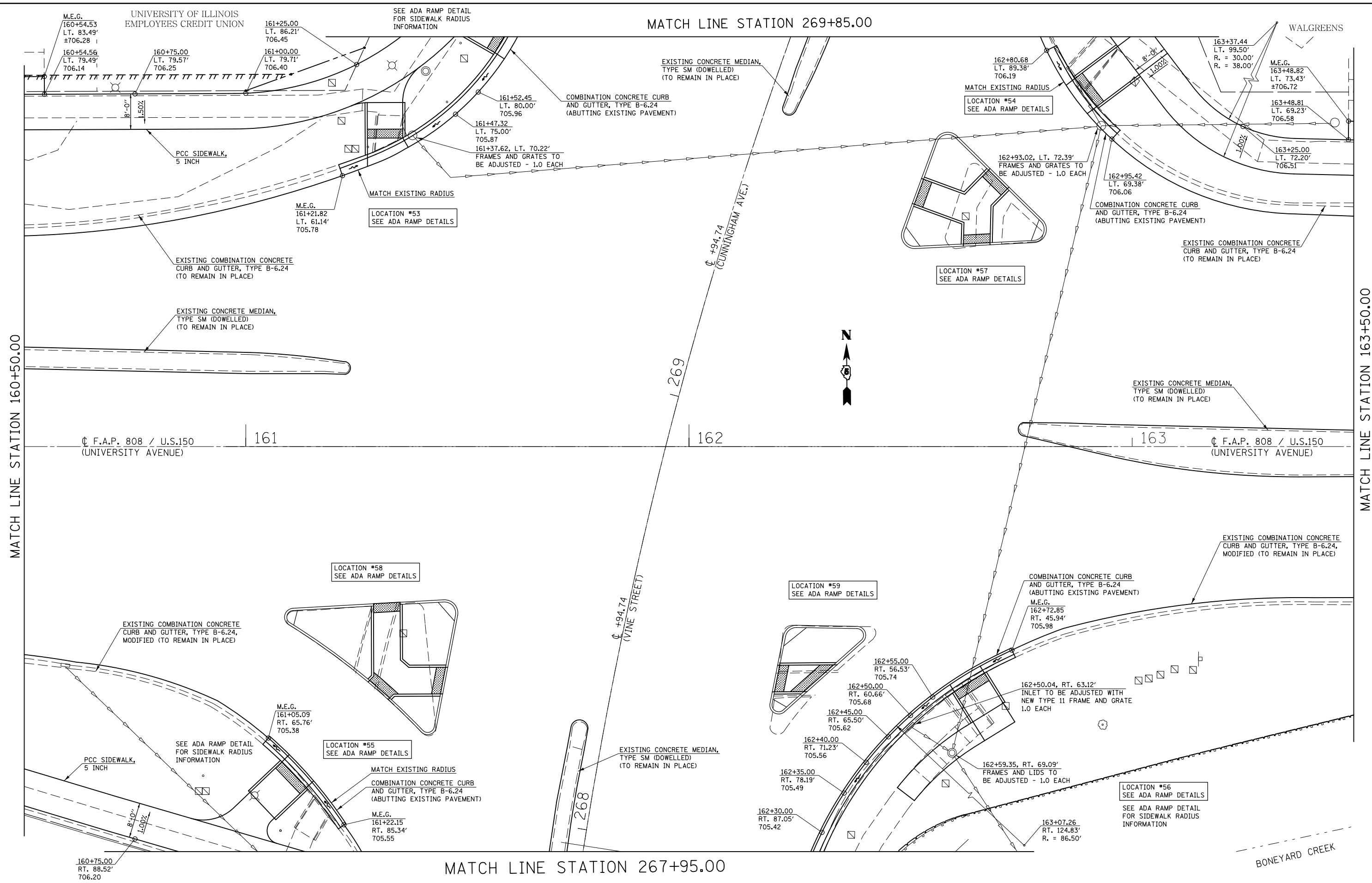


SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\BROWNDATA\Design\0579B53-sh-curb.dgn	DESIGNED BY	REVISED BY	808					D5 HSIP 2016-1	CHAMPAIGN	499	135	
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 70B53									
MODELNAME	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									

SCALE: 1" = 10' SHEET 27 OF 33 SHEETS STA. 158+00.00 TO STA. 160+50.00

MATCH LINE STATION 269+85.00

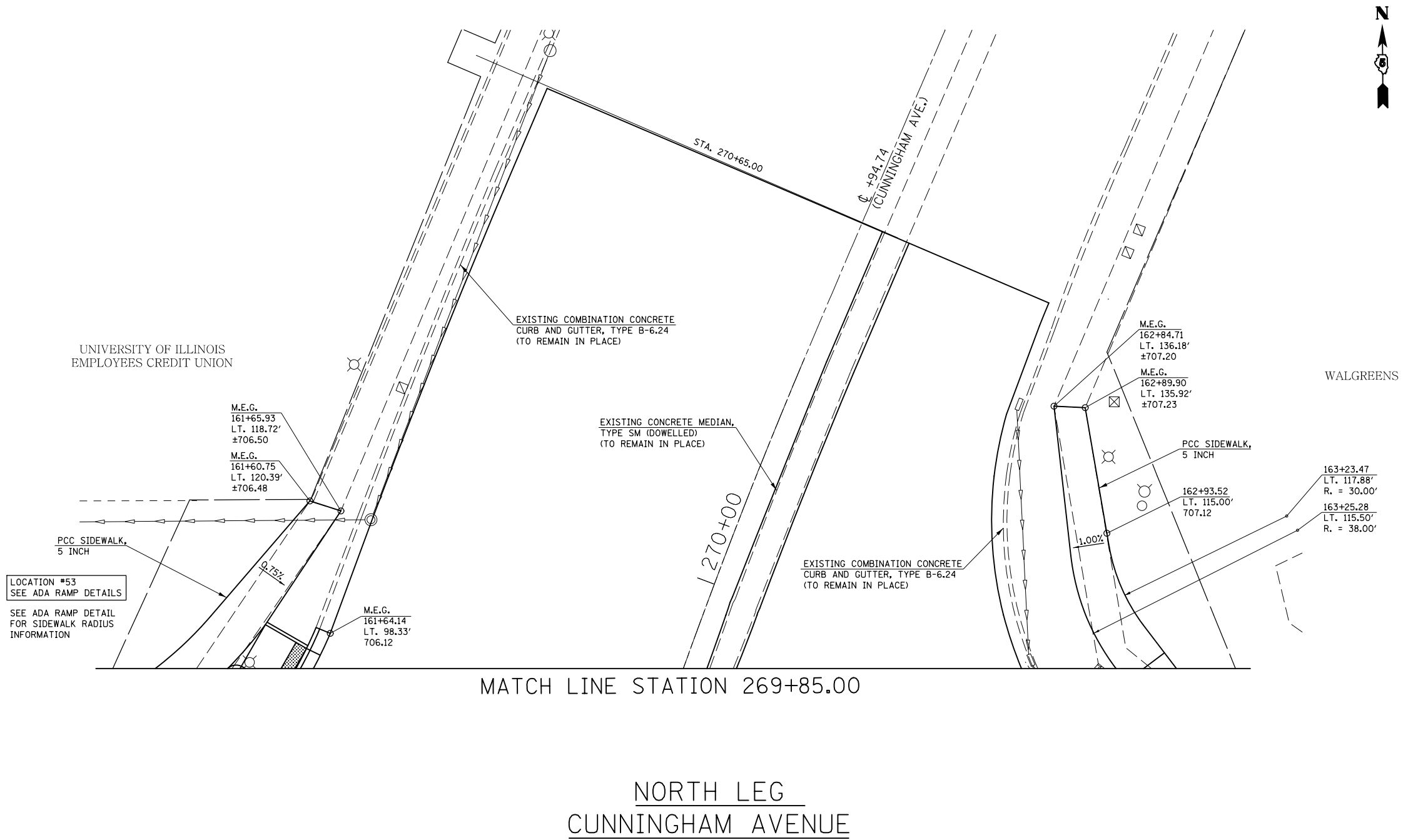


MATCH LINE STATION 160+50.00

MATCH LINE STATION 163+50.00

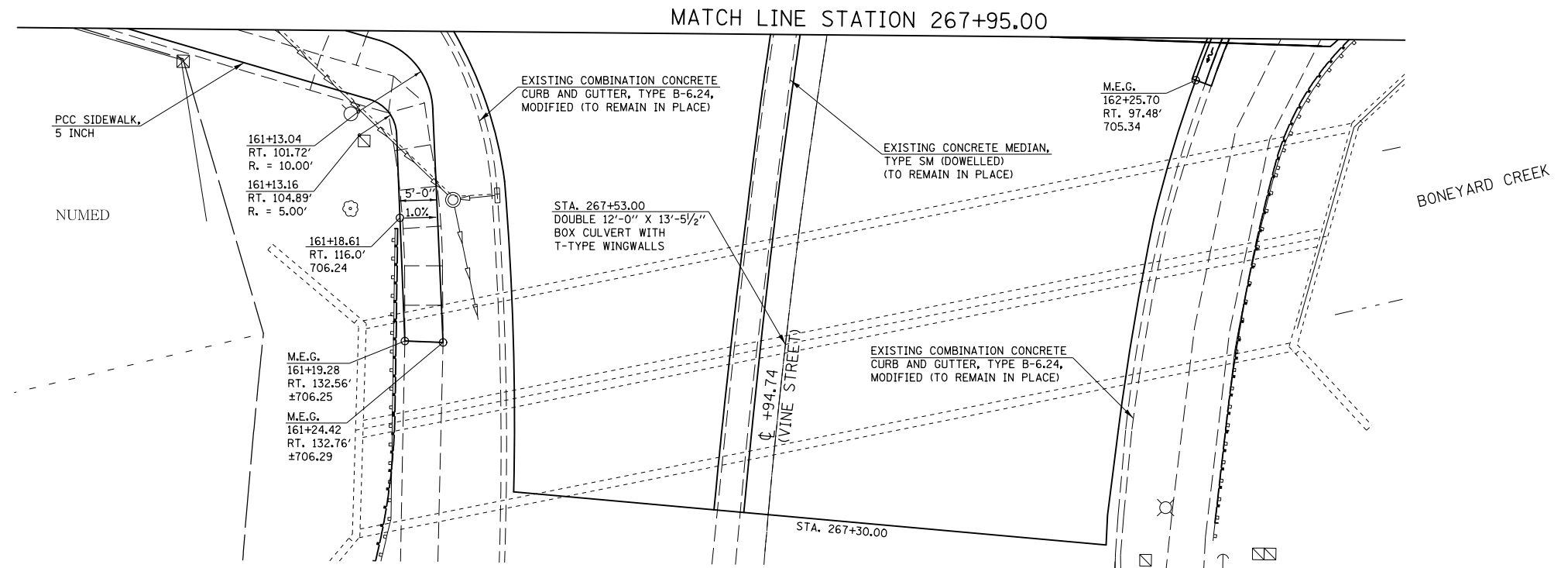
MATCH LINE STATION 267+95.00

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\BROWN\057\Design\0570853-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	136
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10' SHEET 28 OF 33 SHEETS STA. 160+50.00 TO STA. 163+50.00			CONTRACT NO. 70B53				
ILLINOIS FED. AID PROJECT												



NORTH LEG
CUNNINGHAM AVENUE

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\057\Drawings\Design\0570853-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	137
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10' SHEET 29 OF 33 SHEETS STA. TO STA.			CONTRACT NO. 70B53				
ILLINOIS FED. AID PROJECT												



SOUTH LEG
CUNNINGHAM AVENUE

SEC. 8, T. 19N., R. 9E., 3RD P.M.

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570853-shr-curb.dgn								SCALE: 1" = 10'			808	D5 HSIP 2016-1
PLOT SCALE = 20.0000' / in.					SHEET 30 OF 33 SHEETS			CONTRACT NO. 70B53		ILLINOIS FED. AID PROJECT		
MODELNAME					STA. TO STA.							

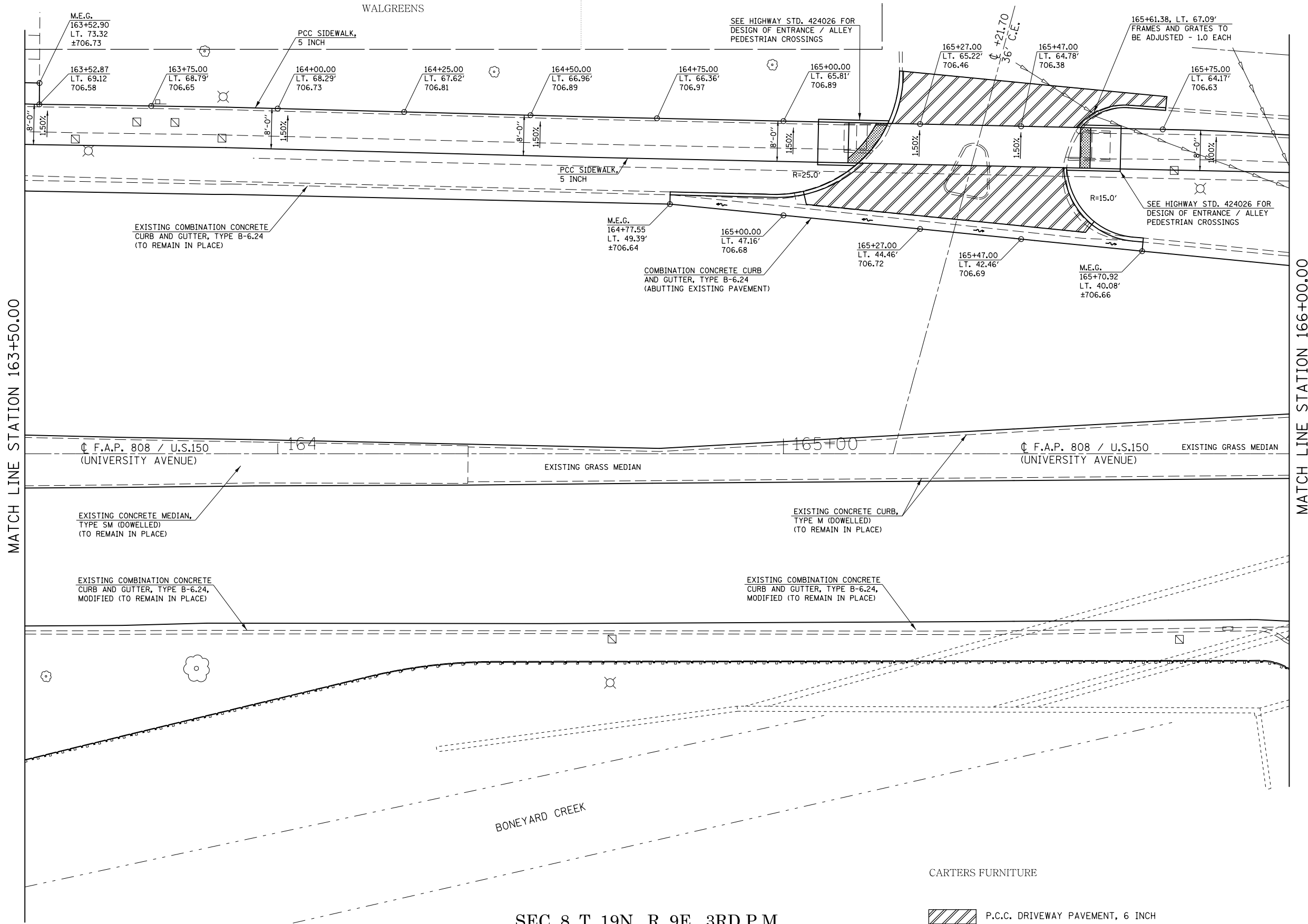
SEC. 8, T. 19N., R. 9E., 3RD P.M.

CAR WASH

WALGREENS

SEE HIGHWAY STD. 424026 FOR DESIGN OF ENTRANCE / ALLEY PEDESTRIAN CROSSINGS

165+61.38, LT. 67.09' FRAMES AND GRATES TO BE ADJUSTED - 1.0 EACH



MATCH LINE STATION 163+50.00

MATCH LINE STATION 166+00.00

CL F.A.P. 808 / U.S.150 (UNIVERSITY AVENUE)

EXISTING GRASS MEDIAN

CL F.A.P. 808 / U.S.150 (UNIVERSITY AVENUE)

EXISTING GRASS MEDIAN

EXISTING CONCRETE MEDIAN, TYPE SM (DOWELLED) (TO REMAIN IN PLACE)

EXISTING CONCRETE CURB, TYPE M (DOWELLED) (TO REMAIN IN PLACE)

EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24, MODIFIED (TO REMAIN IN PLACE)

EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24, MODIFIED (TO REMAIN IN PLACE)

BONEYARD CREEK

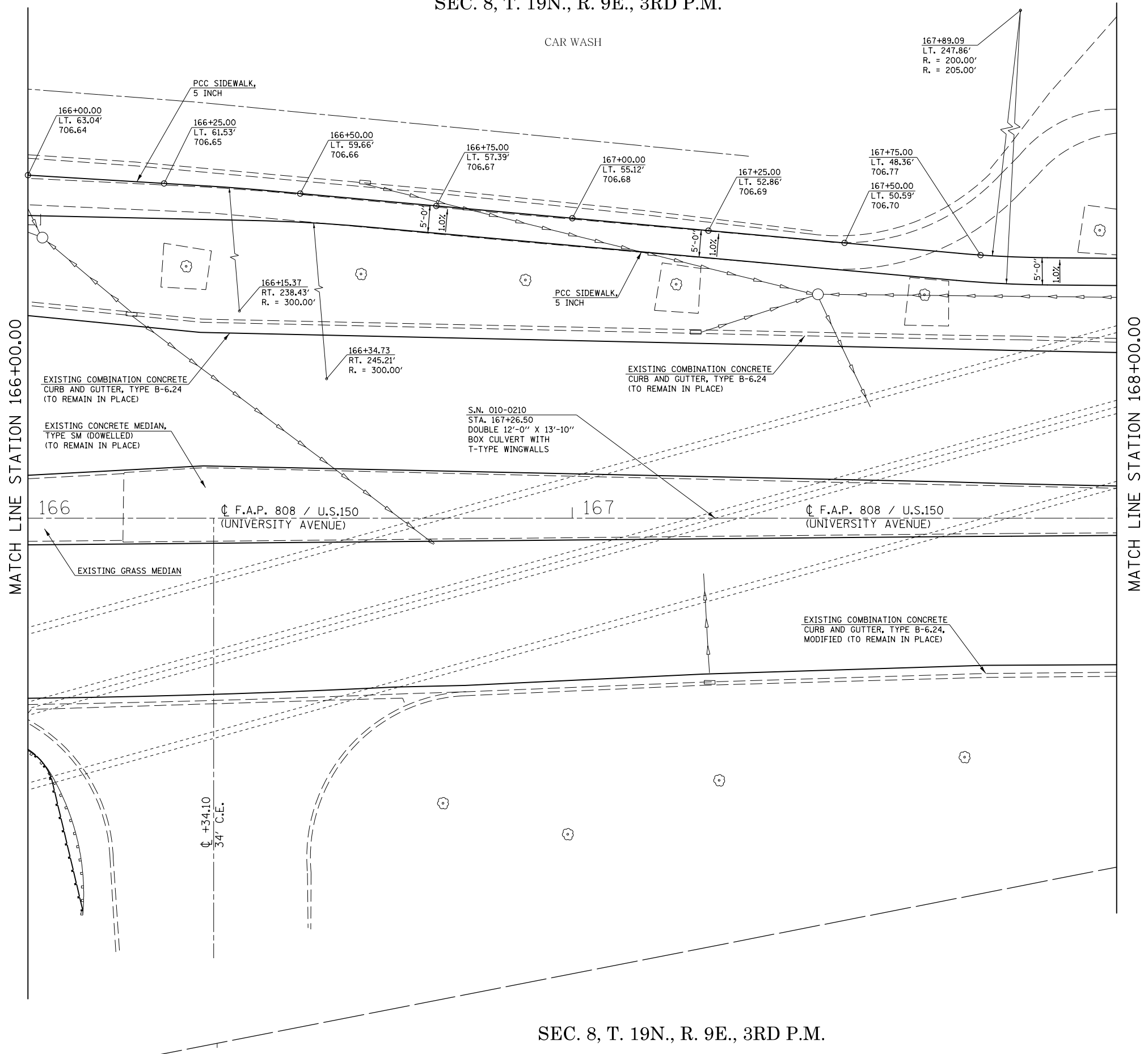
CARTERS FURNITURE

P.C.C. DRIVEWAY PAVEMENT, 6 INCH

SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\057\0853-shr-curb.dgn		CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	139
MODELNAME#	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -		SCALE: 1" = 10'			SHEET 31 OF 33 SHEETS STA. 163+50.00 TO STA. 166+00.00			CONTRACT NO. 70B53	
	PLOT DATE = 1/28/2019							ILLINOIS FED. AID PROJECT				

SEC. 8, T. 19N., R. 9E., 3RD P.M.



SEC. 8, T. 19N., R. 9E., 3RD P.M.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB & GUTTER PLAN SHEETS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 5\Projects\0577\Drawings\Design\05770853-shr-curb.dgn	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -					808	D5 HSIP 2016-1	CHAMPAIGN	499	140
MODELNAME	PLOT DATE = 1/28/2019	DATE -	REVISED -		SCALE: 1" = 10' SHEET 32 OF 33 SHEETS STA.166+00.00 TO STA.168+00.00			CONTRACT NO. 70B53				
							ILLINOIS FED. AID PROJECT					

LOCATION #1

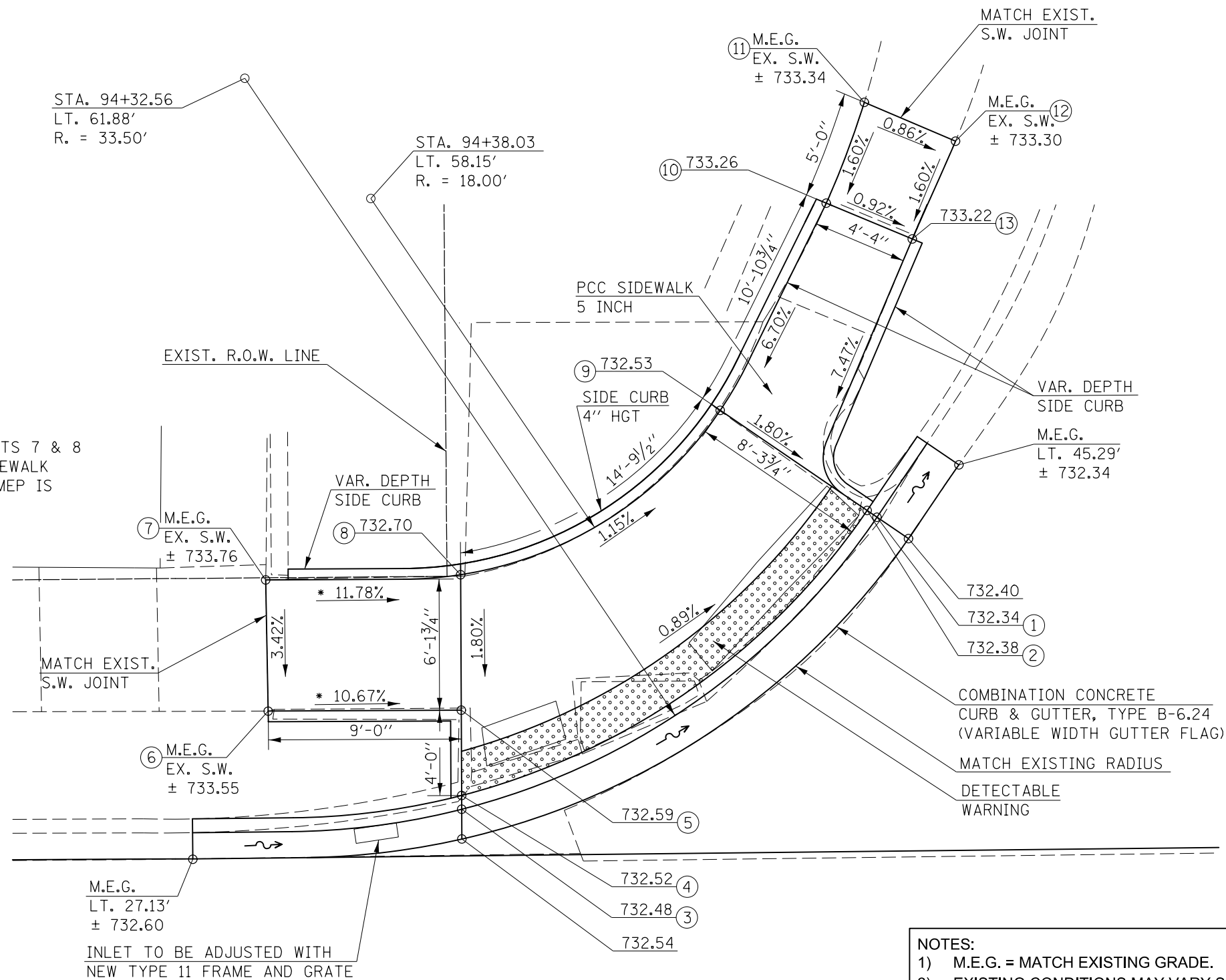
N.W. QUADRANT
U.S. 150 & WRIGHT ST.



LOCATION #1 - N.W. QUADRANT
U.S. 150 & WRIGHT ST.

LOCATION	STATION	OFFSET
1	94+60.14	LT. 42.87
2	94+59.68	LT. 43.19
3	94+40.60	LT. 29.36
4	94+40.60	LT. 30.01
5	94+40.60	LT. 34.00
6	94+31.58	LT. 34.03
7	94+31.52	LT. 40.17
8	94+40.60	LT. 40.34
9	94+52.85	LT. 47.93
10	94+57.89	LT. 57.58
11	94+59.65	LT. 62.27
12	94+63.92	LT. 60.45
13	94+61.90	LT. 55.87

* NOTE:
THE SIDEWALK BETWEEN POINTS 7 & 8
AND POINTS 5 & 6 IS A SIDEWALK
TRANSITION, THEREFORE AN MEP IS
NOT REQUIRED.



WRIGHT STREET

U.S. RTE. 150
(UNIVERSITY AVE.)

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMP.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 ALONG WRIGHT STREET TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 / VARIABLE WIDTH GUTTER FLAG)

INLET TO BE ADJUSTED WITH
NEW TYPE 11 FRAME AND GRATE

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WRIGHT STREET ADA RAMP DETAILS			F.A.P. RTE. 808	SECTION D5 HSIP 2016-1	COUNTY CHAMPAIGN	TOTAL SHEETS 499	SHEET NO. 142
CONTRACT NO. 70B53	SCALE: 1" = 3'	SHEET 1	OF 4 SHEETS		STA.	TO STA.	ILLINOIS FED. AID PROJECT					
CONTRACT NO. 70B53												

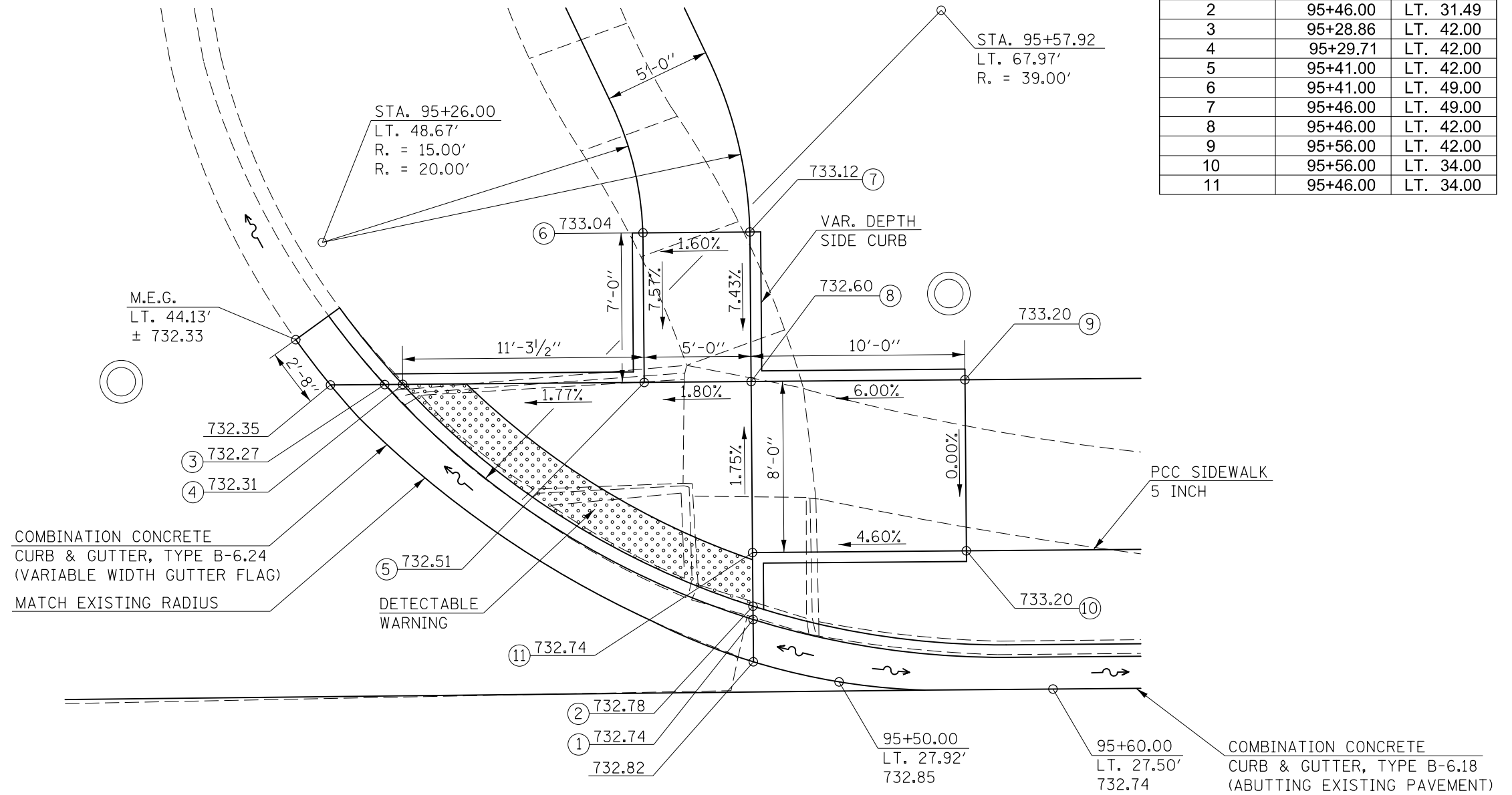
LOCATION #2
N.E. QUADRANT
U.S. 150 & WRIGHT ST.

LOCATION #2 - N.E. QUADRANT
 U.S. 150 & WRIGHT ST.

LOCATION	STATION	OFFSET
1	95+46.00	LT. 30.87
2	95+46.00	LT. 31.49
3	95+28.86	LT. 42.00
4	95+29.71	LT. 42.00
5	95+41.00	LT. 42.00
6	95+41.00	LT. 49.00
7	95+46.00	LT. 49.00
8	95+46.00	LT. 42.00
9	95+56.00	LT. 42.00
10	95+56.00	LT. 34.00
11	95+46.00	LT. 34.00



WRIGHT STREET



U.S. RTE. 150
 (UNIVERSITY AVE.)

NOTE:
 CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 ALONG WRIGHT STREET TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 ON UNIVERSITY AVENUE.
 (TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 / VARIABLE WIDTH GUTTER FLAG)

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.



LOCATION #3

S.W. QUADRANT

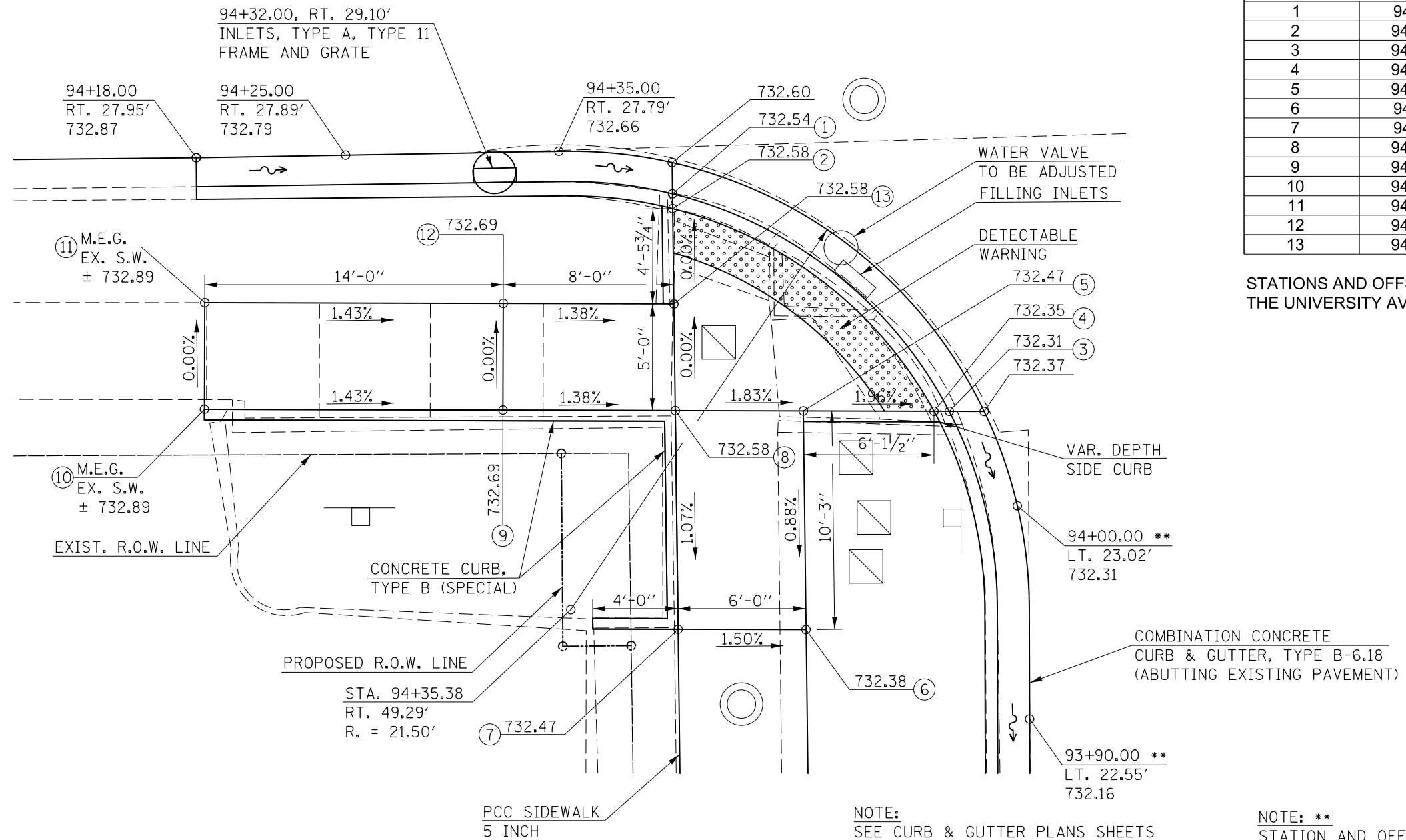
U.S. 150 & WRIGHT ST.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #3 - S.W. QUADRANT
U.S. 150 & WRIGHT ST.

LOCATION	STATION	OFFSET
1	94+40.31	RT. 29.83
2	94+40.32	RT. 30.52
3	94+53.20	RT. 40.14
4	94+52.50	RT. 40.13
5	94+46.36	RT. 40.06
6	94+46.41	RT. 50.31
7	94+40.41	RT. 50.25
8	94+40.36	RT. 40.00
9	94+32.28	RT. 39.91
10	94+18.28	RT. 39.75
11	94+18.34	RT. 34.75
12	94+32.34	RT. 34.91
13	94+40.34	RT. 35.00

STATIONS AND OFFSETS ARE BASED OFF OF THE UNIVERSITY AVENUE CENTERLINE.



NOTE:
SEE CURB & GUTTER PLANS SHEETS FOR STORM SEWER IMPROVEMENTS.

NOTE: **
STATION AND OFFSET SHOWN ARE BASED OFF OF WRIGHT ST. CENTERLINE

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #4

S.E. QUADRANT U.S. 150 & WRIGHT ST.



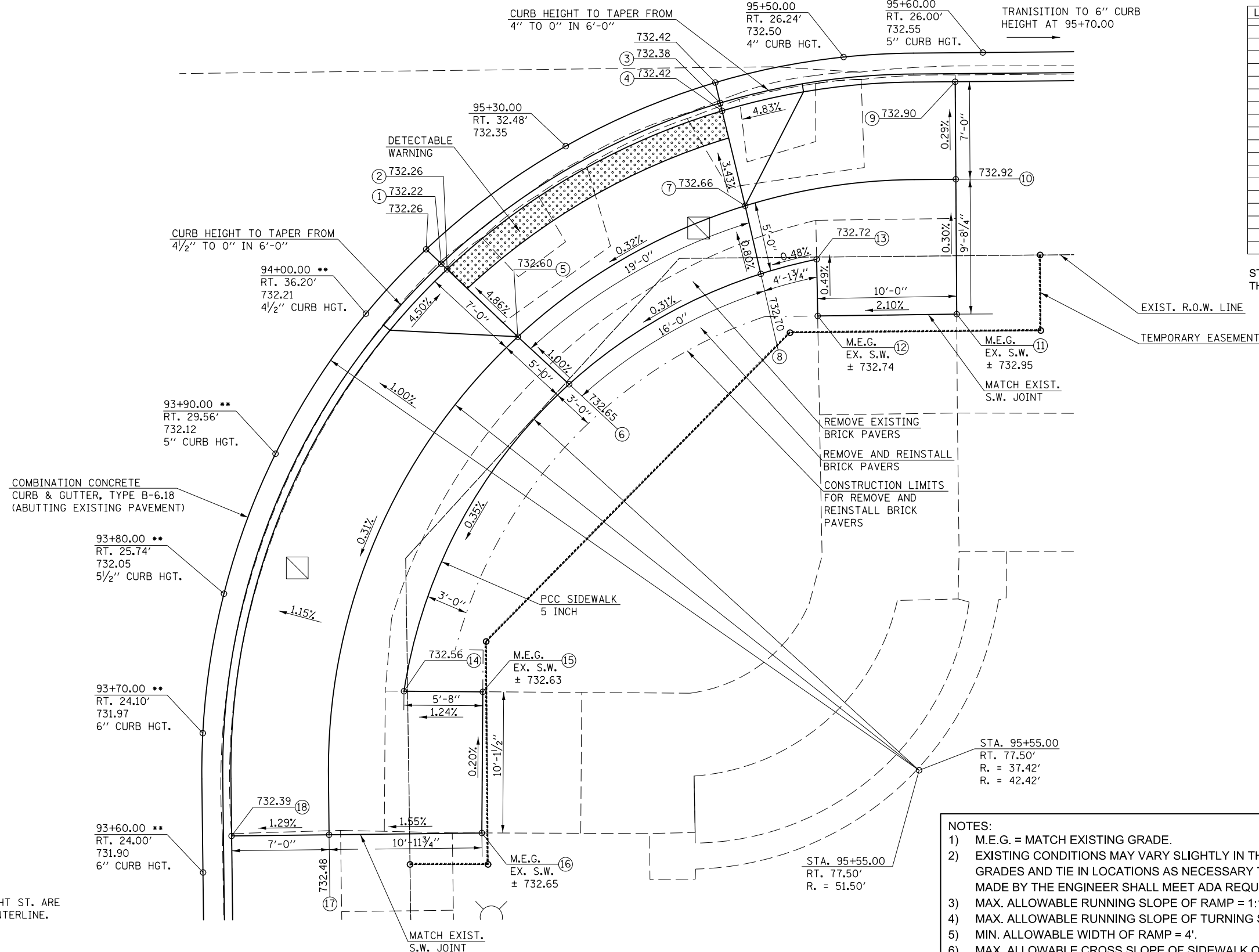
U.S. RTE. 150
(UNIVERSITY AVE.)

WRIGHT STREET

LOCATION #4 - S.E. QUADRANT
U.S. 150 & WRIGHT ST.

LOCATION	STATION	OFFSET
1	95+20.99	RT. 40.85
2	95+21.41	RT. 41.26
3	95+41.12	RT. 29.47
4	95+41.25	RT. 30.03
5	95+26.45	RT. 46.13
6	95+30.10	RT. 49.57
7	95+42.85	RT. 36.86
8	95+43.93	RT. 41.76
9	95+58.01	RT. 28.08
10	95+57.99	RT. 35.08
11	95+57.98	RT. 44.77
12	95+47.99	RT. 44.82
13	95+47.95	RT. 40.75
14	95+18.05	RT. 71.63
15	95+23.73	RT. 71.60
16	95+23.58	RT. 81.74
17	95+12.60	RT. 81.78
18	95+05.60	RT. 81.81

STATIONS AND OFFSETS ARE BASED OFF OF
THE UNIVERSITY AVENUE CENTERLINE.



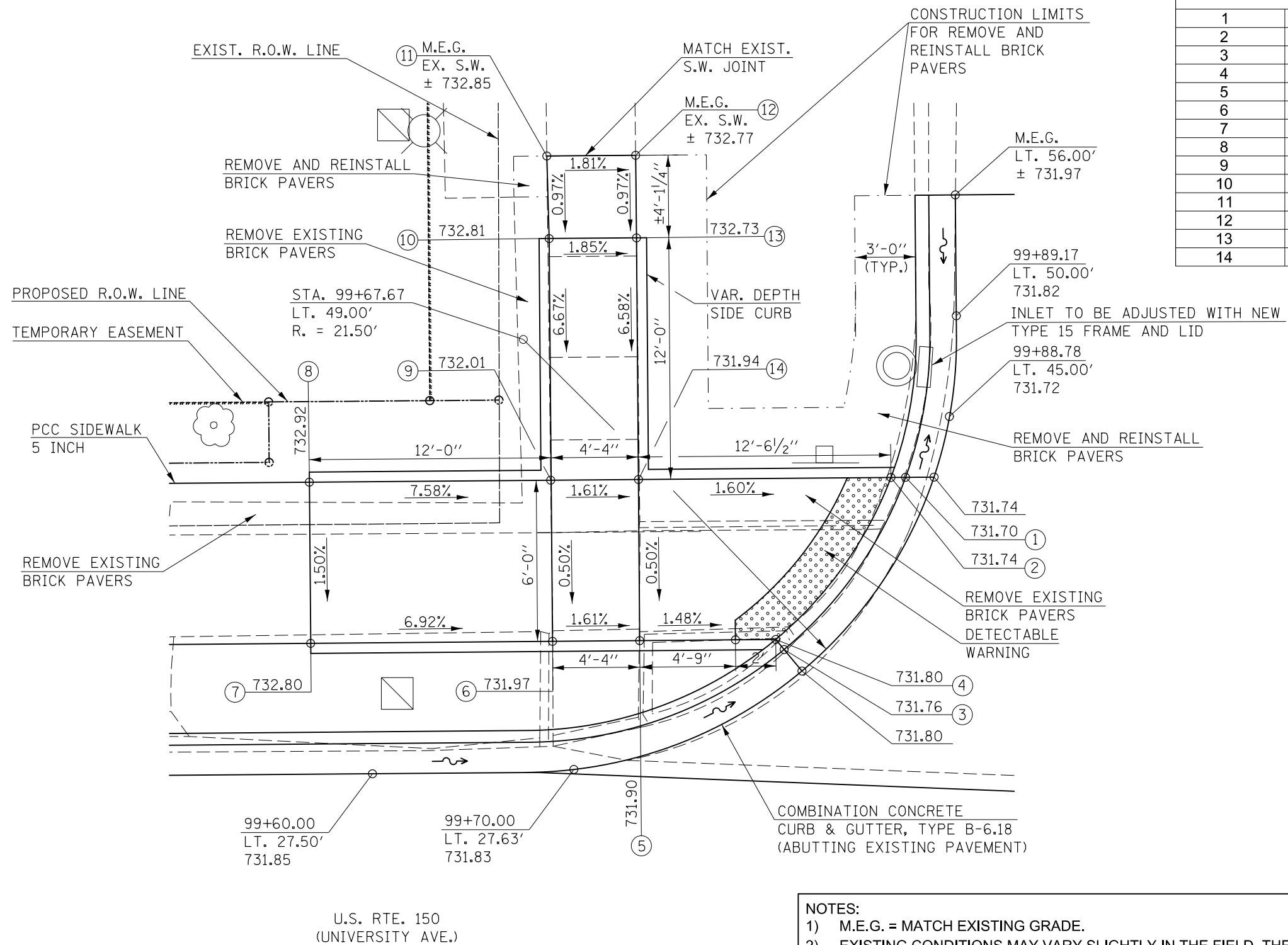
- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #5

N.W. QUADRANT
U.S. 150 & ROMINE ST.

LOCATION #5 - N.W. QUADRANT
U.S. 150 & ROMINE ST.

LOCATION	STATION	OFFSET
1	99+86.58	LT. 42.00
2	99+85.85	LT. 42.00
3	99+80.48	LT. 33.50
4	99+80.07	LT. 34.00
5	99+73.32	LT. 34.00
6	99+68.99	LT. 34.00
7	99+56.99	LT. 34.00
8	99+56.99	LT. 42.00
9	99+68.99	LT. 42.00
10	99+68.99	LT. 54.00
11	99+68.91	LT. 58.10
12	99+73.31	LT. 58.10
13	99+73.32	LT. 54.00
14	99+73.32	LT. 42.00



- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #6

N.E. QUADRANT

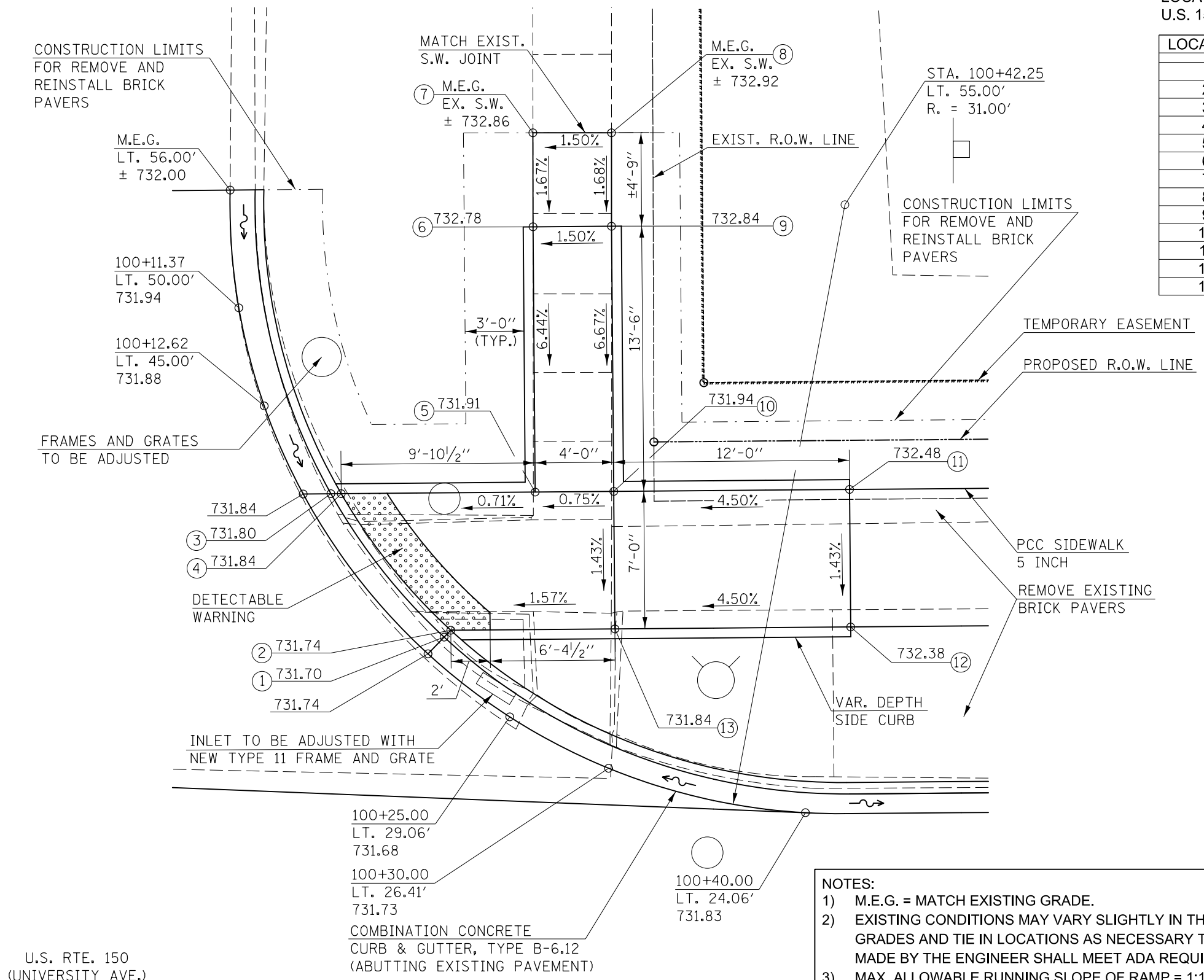
U.S. 150 & ROMINE ST.

LOCATION #6 - N.E. QUADRANT
U.S. 150 & ROMINE ST.

LOCATION	STATION	OFFSET
1	100+21.68	LT. 33.15
2	100+22.01	LT. 33.50
3	100+15.97	LT. 40.50
4	100+16.50	LT. 40.50
5	100+26.37	LT. 40.50
6	100+26.37	LT. 54.00
7	100+26.40	LT. 58.79
8	100+30.40	LT. 58.76
9	100+30.37	LT. 54.00
10	100+30.37	LT. 40.50
11	100+42.37	LT. 40.50
12	100+42.37	LT. 33.50
13	100+30.37	LT. 33.50



ROMINE STREET

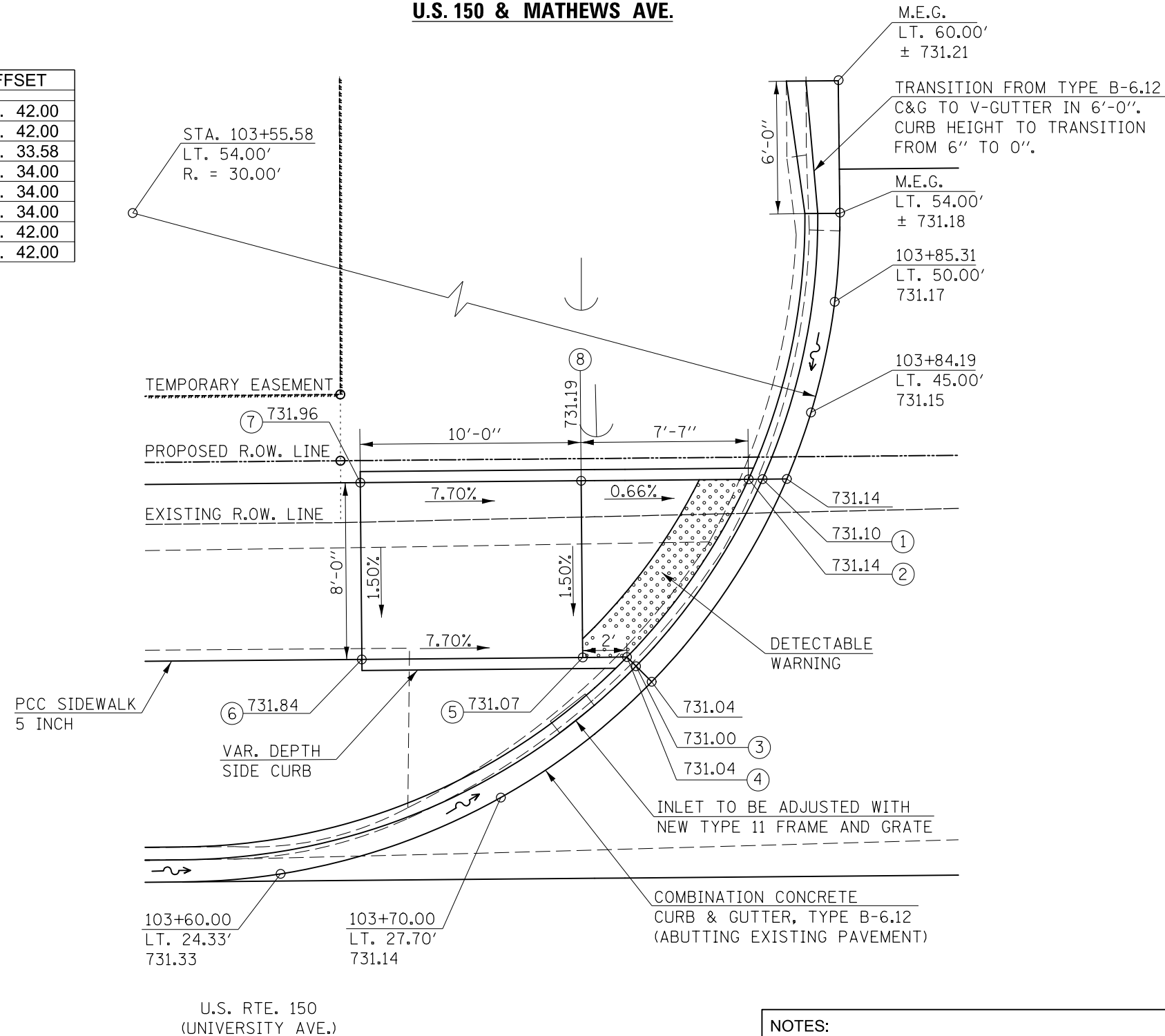


- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #7
N.W. QUADRANT
U.S. 150 & MATHEWS AVE.

LOCATION #7 - N.W. QUADRANT
 U.S. 150 & MATHEWS AVE.

LOCATION	STATION	OFFSET
1	103+81.98	LT. 42.00
2	103+81.33	LT. 42.00
3	103+76.17	LT. 33.58
4	103+75.76	LT. 34.00
5	103+73.76	LT. 34.00
6	103+63.76	LT. 34.00
7	103+63.76	LT. 42.00
8	103+73.76	LT. 42.00



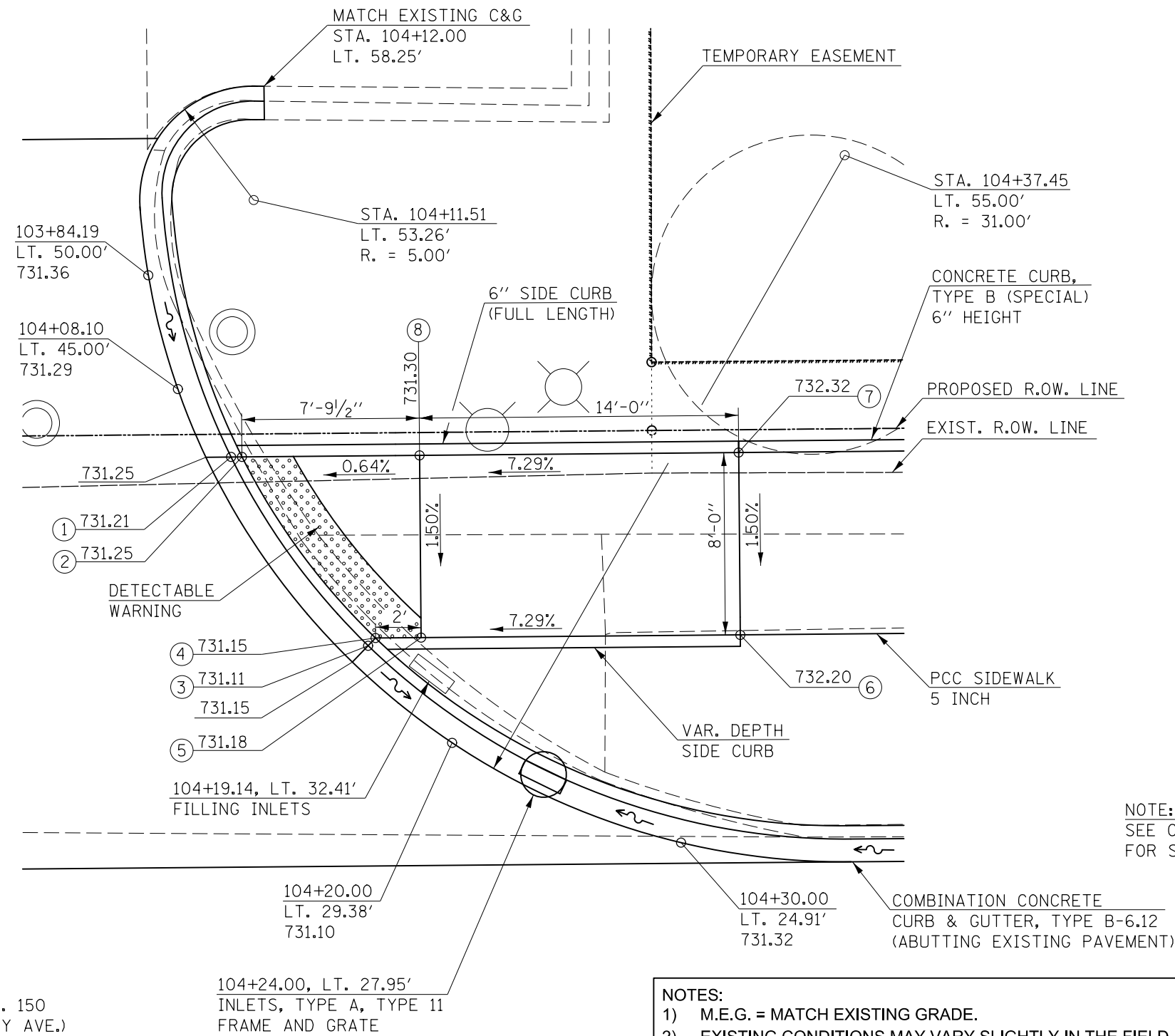
- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #8
N.E. QUADRANT
U.S. 150 & MATHEWS AVE.



LOCATION #8 - N.E. QUADRANT
 U.S. 150 & MATHEWS AVE.

LOCATION	STATION	OFFSET
1	104+10.41	LT. 42.00
2	104+10.89	LT. 42.00
3	104+16.35	LT. 33.67
4	104+16.68	LT. 34.00
5	104+18.68	LT. 34.00
6	104+32.68	LT. 34.00
7	104+32.68	LT. 42.00
8	104+18.68	LT. 42.00



NOTE:
 SEE CURB & GUTTER PLANS SHEETS
 FOR STORM SEWER IMPROVEMENTS.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MATHEWS AVENUE ADA RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\05790853-shr-ramps.dgn	DESIGNED -	REVISED -	808			D5 HSIP 2016-1	CHAMPAIGN	499	149	
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70B53		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 1/28/2019	DATE -	REVISED -			SCALE: 1" = 3'	SHEET 2 OF 4 SHEETS	STA.	TO STA.	

LOCATION #9

S.W. QUADRANT

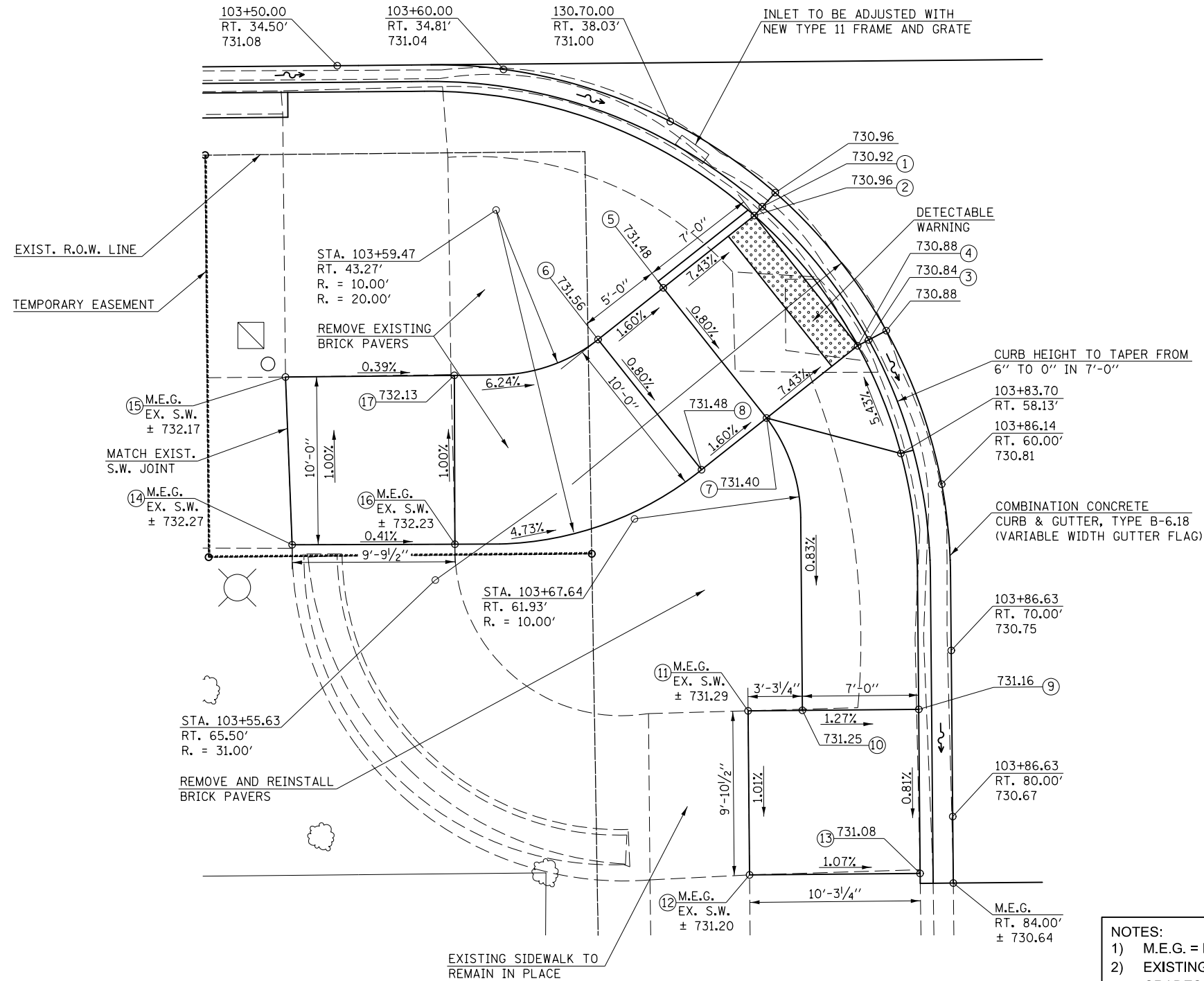
U.S. 150 & MATHEWS AVE.



U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #9 - S.W. QUADRANT
U.S. 150 & MATHEWS AVE.

LOCATION	STATION	OFFSET
1	103+75.50	RT. 43.21
2	103+75.01	RT. 43.74
3	103+81.83	RT. 51.28
4	103+81.16	RT. 51.62
5	103+69.49	RT. 48.04
6	103+65.55	RT. 51.11
7	103+75.64	RT. 55.93
8	103+71.70	RT. 59.00
9	103+84.64	RT. 73.53
10	103+77.64	RT. 73.53
11	103+74.37	RT. 73.53
12	103+74.37	RT. 83.40
13	103+84.64	RT. 83.40
14	103+47.03	RT. 63.30
15	103+46.72	RT. 53.20
16	103+56.82	RT. 63.35
17	103+56.87	RT. 53.19



NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 ALONG MATHEWS AVENUE TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 / VARIABLE WIDTH GUTTER FLAG)

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #10

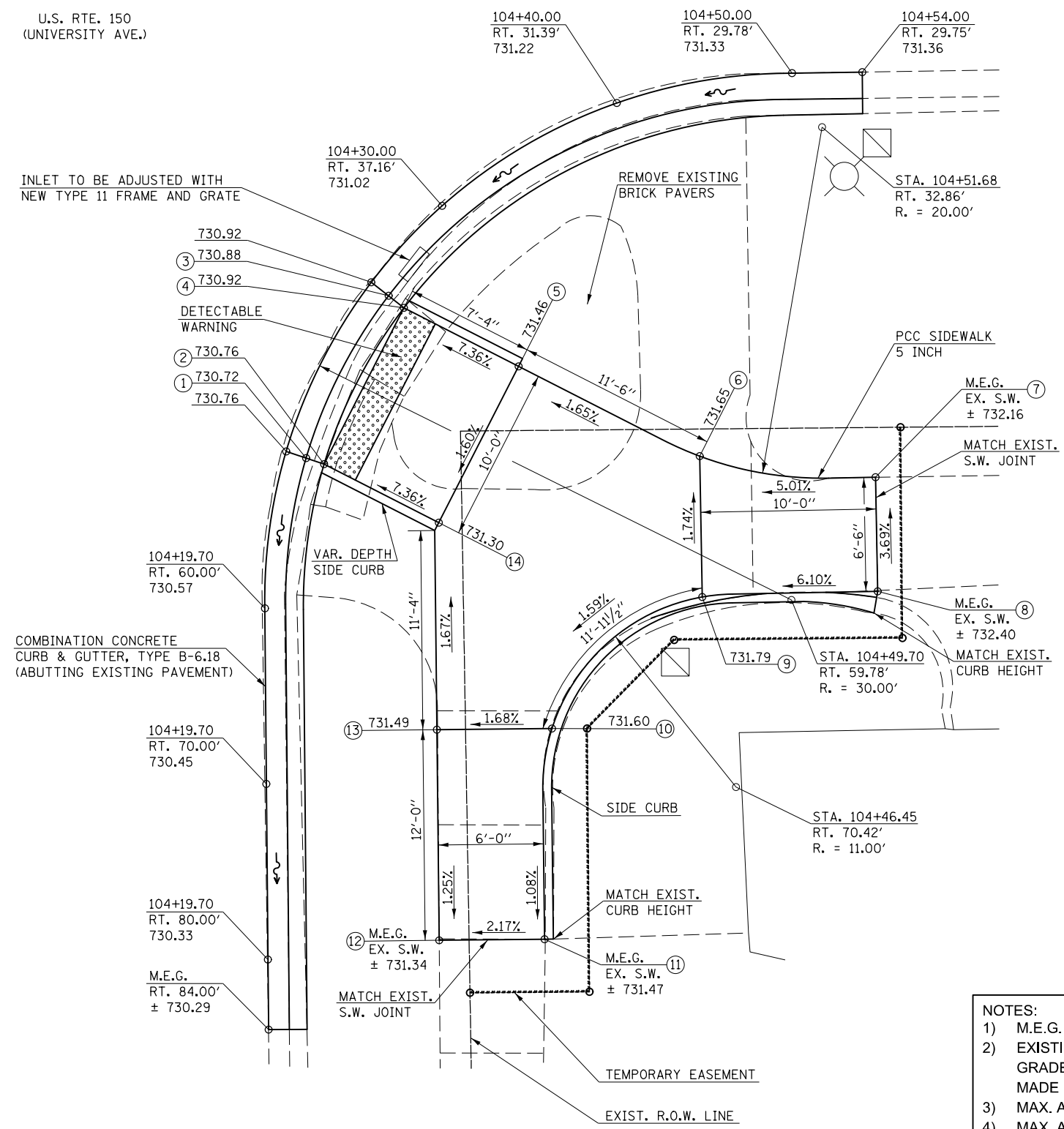
S.E. QUADRANT

U.S. 150 & MATHEWS AVE.



LOCATION #10- S.E. QUADRANT
U.S. 150 & MATHEWS AVE.

LOCATION	STATION	OFFSET
1	104+22.13	RT. 51.46
2	104+23.15	RT. 51.82
3	104+26.91	RT. 42.26
4	104+27.78	RT. 42.95
5	104+34.28	RT. 46.34
6	104+44.54	RT. 51.55
7	104+54.56	RT. 52.84
8	104+54.62	RT. 59.34
9	104+44.62	RT. 59.57
10	104+36.00	RT. 66.98
11	104+35.47	RT. 78.98
12	104+29.47	RT. 79.00
13	104+29.44	RT. 67.00
14	104+29.65	RT. 55.21

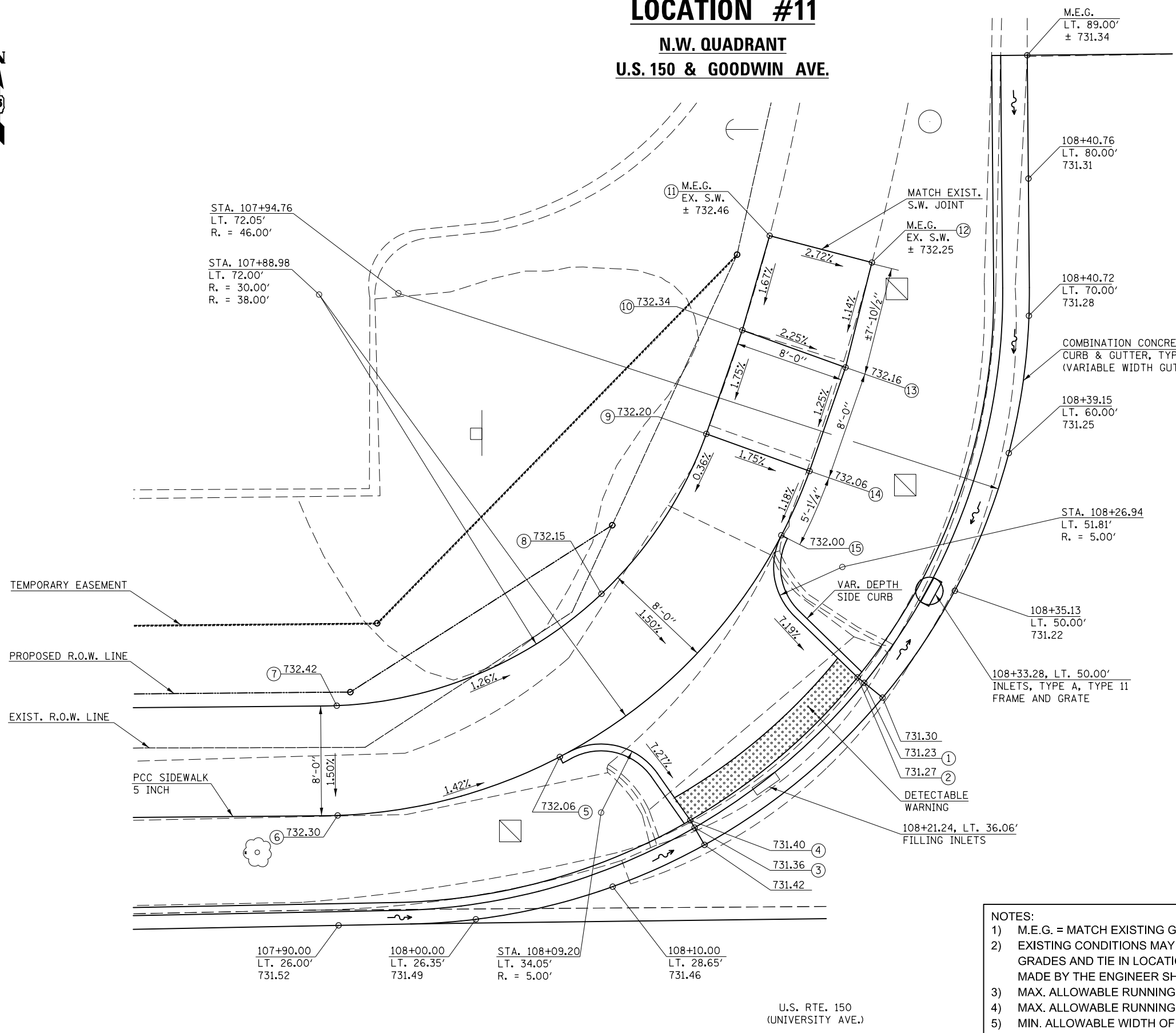


- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #11

N.W. QUADRANT

U.S. 150 & GOODWIN AVE.



LOCATION #11 - N.W. QUADRANT
U.S. 150 & GOODWIN AVE.

LOCATION	STATION	OFFSET
1	108+28.48	LT. 43.34
2	108+27.99	LT. 43.75
3	108+16.01	LT. 32.90
4	108+15.71	LT. 33.43
5	108+06.23	LT. 38.14
6	107+90.00	LT. 34.01
7	107+90.00	LT. 42.01
8	108+09.37	LT. 50.00
9	108+17.11	LT. 61.59
10	108+19.81	LT. 69.13
11	108+21.86	LT. 76.00
12	108+29.30	LT. 73.98
13	108+27.31	LT. 66.35
14	108+24.62	LT. 58.82
15	108+22.53	LT. 54.15

GOODWIN AVENUE

NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 ALONG GOODWIN AVENUE TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 / VARIABLE WIDTH GUTTER FLAG)

NOTE:
SEE CURB & GUTTER PLANS SHEETS FOR STORM SEWER IMPROVEMENTS.

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

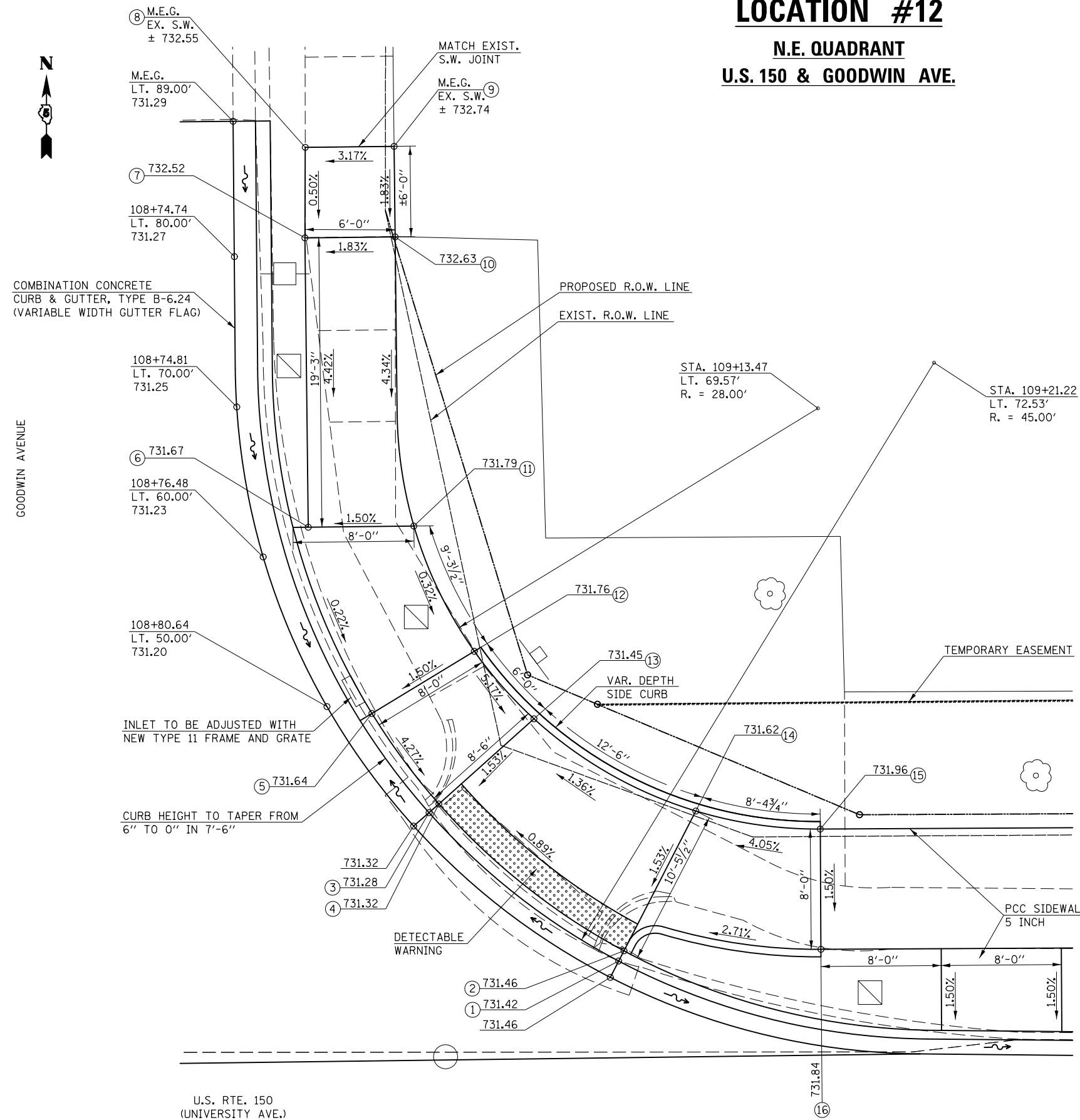
LOCATION #12

N.E. QUADRANT

U.S. 150 & GOODWIN AVE.

LOCATION #12 - N.E. QUADRANT
U.S. 150 & GOODWIN AVE.

LOCATION	STATION	OFFSET
1	108+99.90	LT. 32.90
2	109+00.25	LT. 33.58
3	108+87.36	LT. 42.89
4	108+88.02	LT. 43.46
5	108+83.62	LT. 49.52
6	108+79.49	LT. 61.95
7	108+79.45	LT. 81.20
8	108+79.52	LT. 87.23
9	108+85.43	LT. 87.24
10	108+85.45	LT. 81.22
11	108+86.53	LT. 61.97
12	108+90.47	LT. 53.60
13	108+94.39	LT. 49.07
14	109+05.11	LT. 42.85
15	109+13.38	LT. 41.57
16	109+13.36	LT. 33.57



NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 ALONG GOODWIN AVENUE TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 / VARIABLE WIDTH GUTTER FLAG)

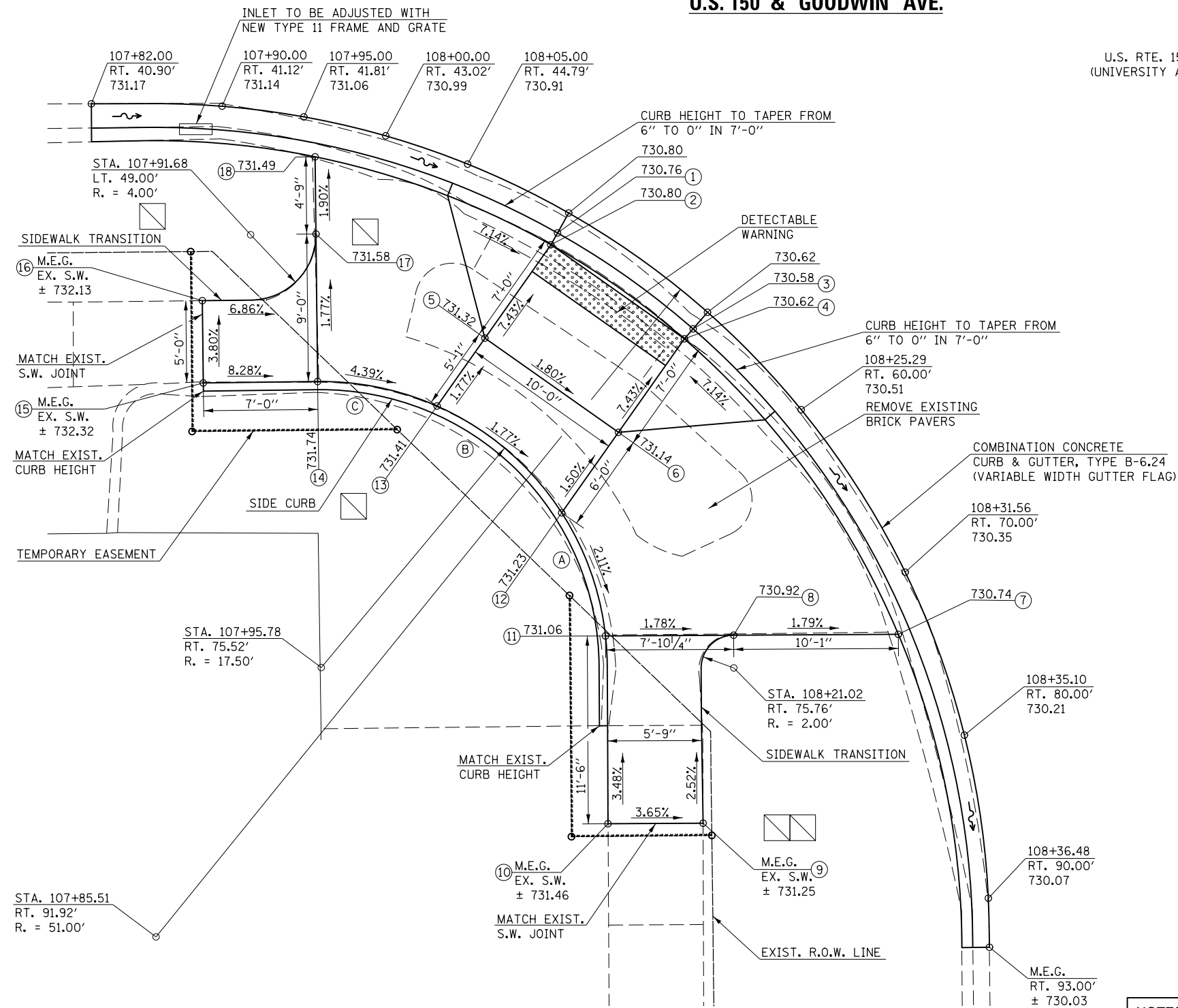
- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #13

S.W. QUADRANT U.S. 150 & GOODWIN AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #13 - S.W. QUADRANT
U.S. 150 & GOODWIN AVE.



LOCATION	STATION	OFFSET
1	108+10.47	RT. 49.05
2	108+10.05	RT. 49.77
3	108+18.71	RT. 55.00
4	108+18.17	RT. 55.61
5	108+05.97	RT. 55.45
6	108+14.09	RT. 61.29
7	108+31.11	RT. 73.80
8	108+21.03	RT. 73.76
9	108+19.06	RT. 85.26
10	108+13.24	RT. 85.24
11	108+13.18	RT. 73.74
12	108+10.57	RT. 66.18
13	108+03.00	RT. 59.58
14	107+95.70	RT. 58.02
15	107+88.70	RT. 58.02
16	107+88.70	RT. 53.02
17	107+95.68	RT. 48.99
18	107+95.66	RT. 44.26

LOCATION	POINT TO POINT	LENGTH
A	PT. 11 TO PT. 12	8' - 0 3/4"
B	PT. 12 TO PT. 13	10' - 2 1/4"
C	PT. 13 TO PT. 14	7' - 6 1/4"

NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 ALONG GOODWIN AVENUE TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 / VARIABLE WIDTH GUTTER FLAG)

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

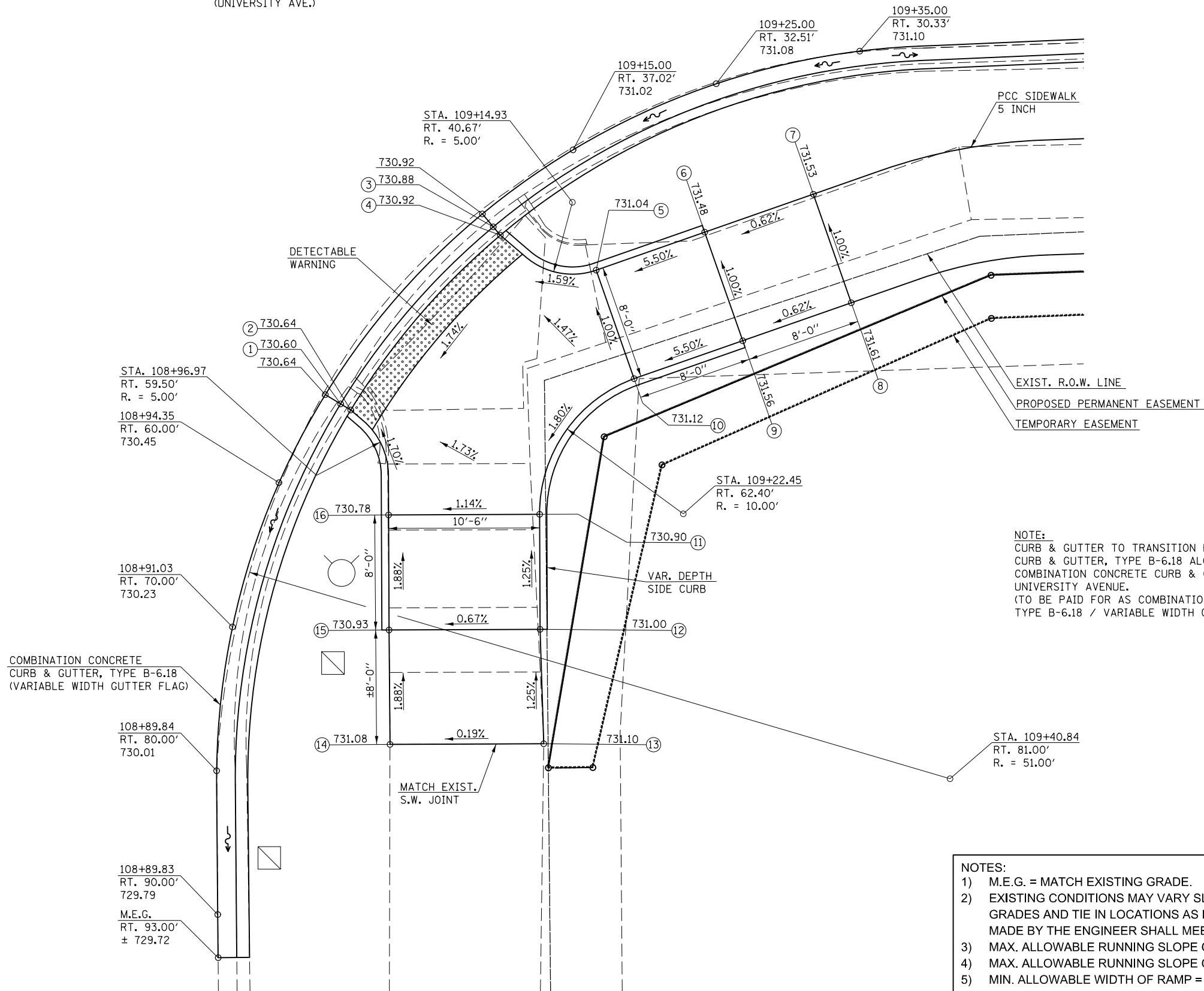
LOCATION #14

S.E. QUADRANT U.S. 150 & GOODWIN AVE.



U.S. RTE. 150
(UNIVERSITY AVE.)

GOODWIN AVENUE



LOCATION #14 - S.E. QUADRANT
U.S. 150 & GOODWIN AVE.

LOCATION	STATION	OFFSET
1	108+98.69	RT. 54.55
2	108+99.39	RT. 54.98
3	109+09.41	RT. 42.33
4	109+09.90	RT. 42.92
5	109+16.54	RT. 45.40
6	109+24.11	RT. 42.83
7	109+31.69	RT. 40.26
8	109+34.26	RT. 47.83
9	109+26.69	RT. 50.40
10	109+19.11	RT. 52.98
11	109+12.45	RT. 62.36
12	109+12.60	RT. 70.36
13	109+12.60	RT. 78.32
14	109+01.91	RT. 78.27
15	109+01.92	RT. 70.31
16	109+01.95	RT. 62.31

NOTE:
CURB & GUTTER TO TRANSITION FROM COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 ALONG GOODWIN AVENUE TO COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 ON UNIVERSITY AVENUE.
(TO BE PAID FOR AS COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18 / VARIABLE WIDTH GUTTER FLAG)

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.



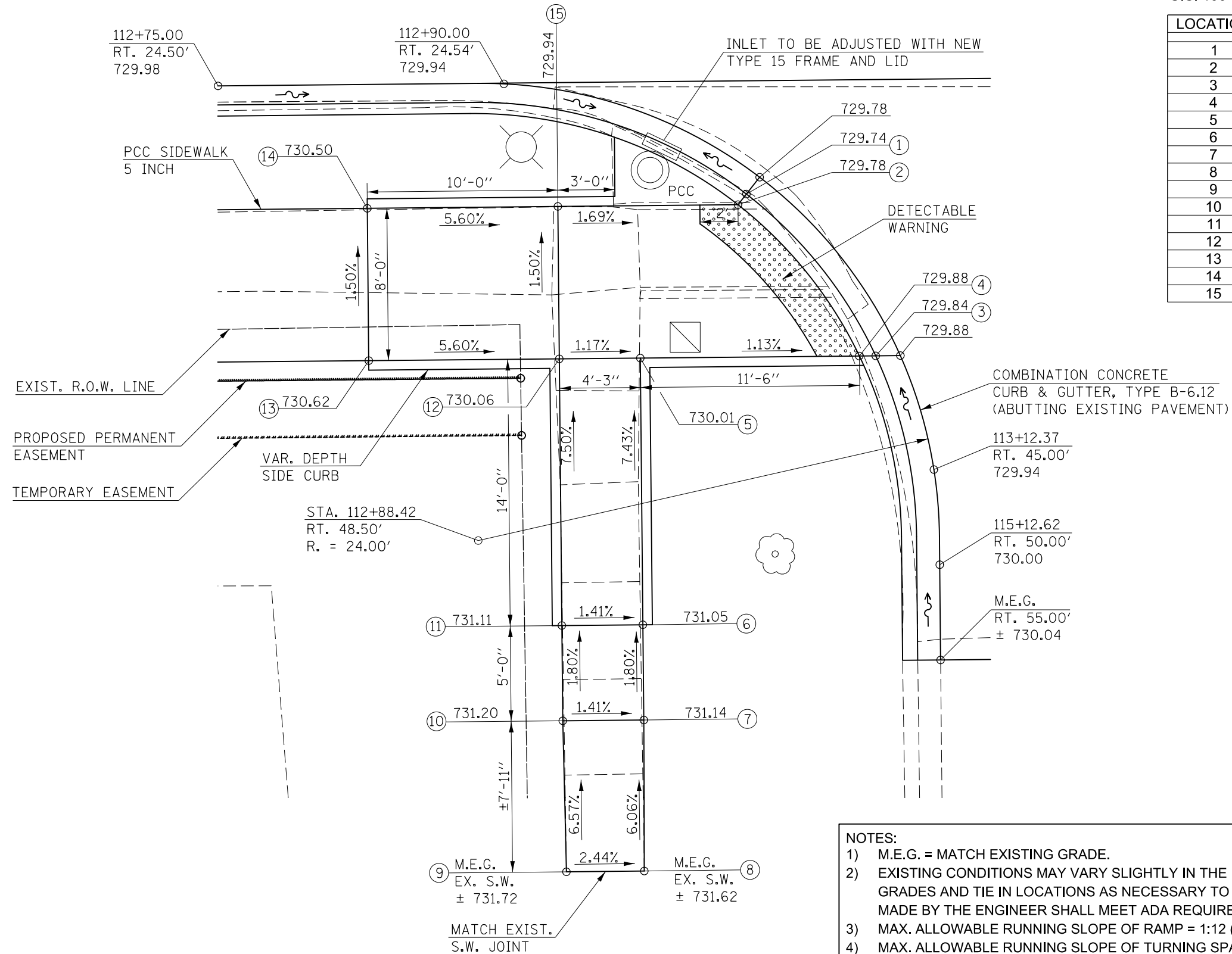
LOCATION #15

S.W. QUADRANT

U.S. 150 & HARVEY ST.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #15 - S.W. QUADRANT
U.S. 150 & HARVEY ST.



LOCATION	STATION	OFFSET
1	113+02.67	RT. 30.44
2	113+02.22	RT. 31.00
3	113+09.37	RT. 39.00
4	113+08.52	RT. 39.00
5	112+97.02	RT. 39.00
6	112+97.02	RT. 53.00
7	112+97.02	RT. 58.00
8	112+96.97	RT. 65.95
9	112+92.88	RT. 65.95
10	112+92.77	RT. 58.00
11	112+92.77	RT. 53.00
12	112+92.77	RT. 39.00
13	112+82.77	RT. 39.00
14	112+82.77	RT. 31.00
15	112+92.77	RT. 31.00

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #16

S.E. QUADRANT
U.S. 150 & HARVEY ST.

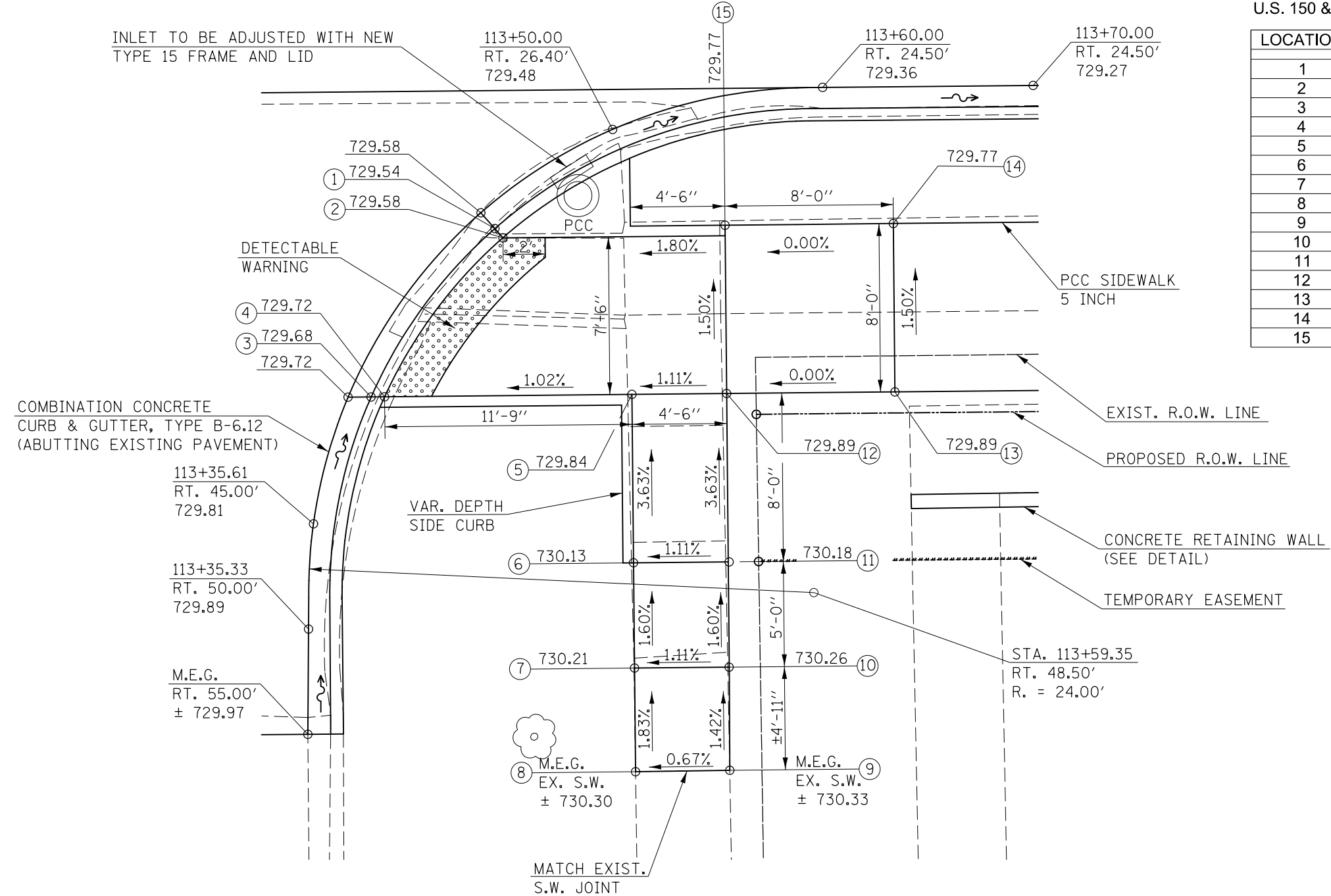
LOCATION #16 - S.E. QUADRANT
U.S. 150 & HARVEY ST.

LOCATION	STATION	OFFSET
1	113+44.36	RT. 31.06
2	113+44.74	RT. 31.50
3	113+38.41	RT. 39.00
4	113+39.05	RT. 39.00
5	113+50.80	RT. 39.00
6	113+50.80	RT. 47.00
7	113+50.80	RT. 52.00
8	113+50.80	RT. 56.93
9	113+55.27	RT. 56.91
10	113+55.30	RT. 52.00
11	113+55.30	RT. 47.00
12	113+55.30	RT. 39.00
13	113+63.30	RT. 39.00
14	113+63.30	RT. 31.00
15	113+55.30	RT. 31.00



U.S. RTE. 150
(UNIVERSITY AVE.)

HARVEY STREET

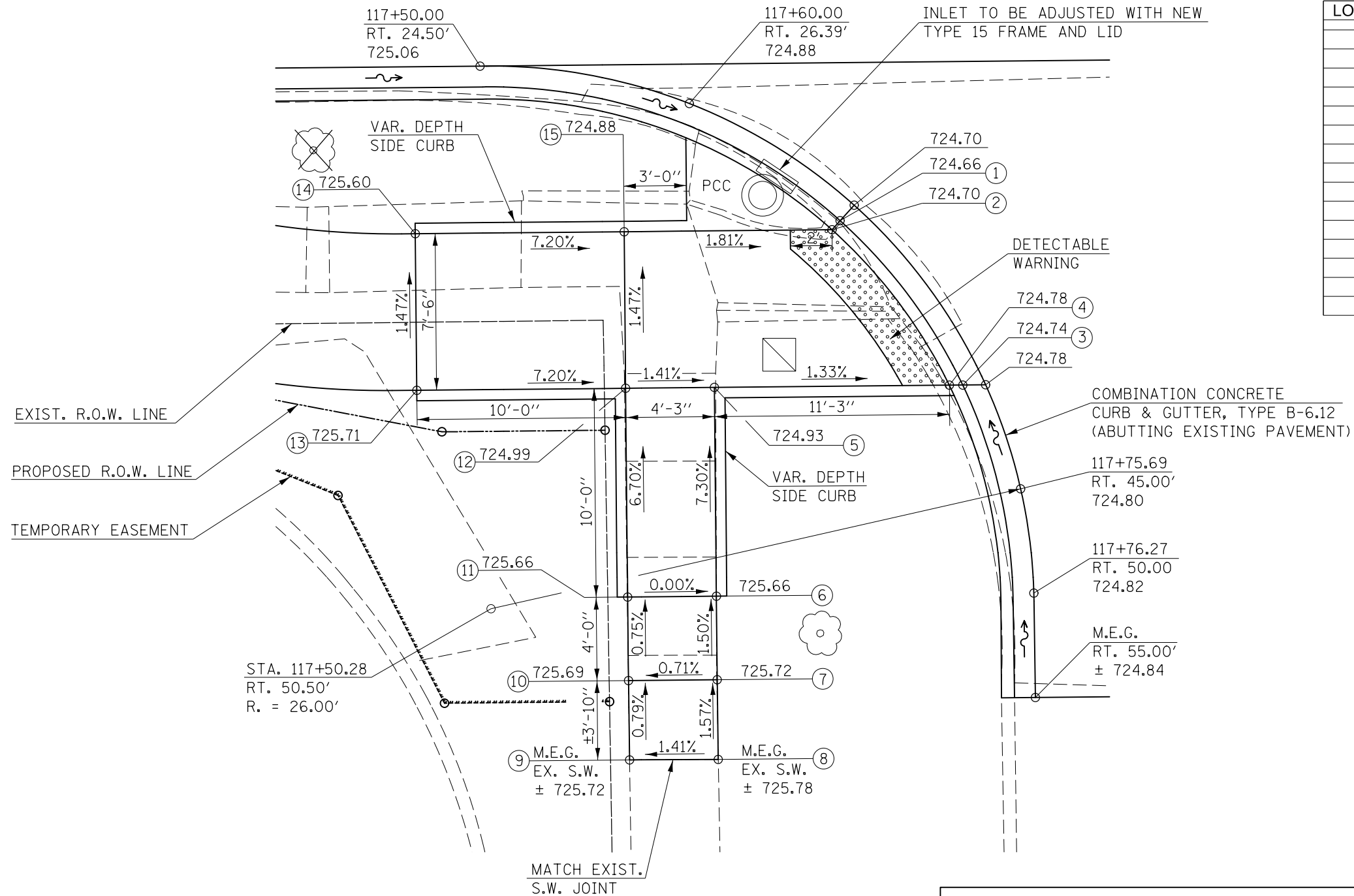


- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMP.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #17

S.W. QUADRANT
U.S. 150 & GREGORY ST.

LOCATION #17 - S.W. QUADRANT
U.S. 150 & GREGORY ST.



LOCATION	STATION	OFFSET
1	117+67.17	RT. 32.07
2	117+66.77	RT. 32.50
3	117+72.96	RT. 40.00
4	117+72.32	RT. 40.00
5	117+61.07	RT. 40.00
6	117+61.07	RT. 50.00
7	117+61.07	RT. 54.00
8	117+61.08	RT. 57.82
9	117+56.84	RT. 57.80
10	117+56.82	RT. 54.00
11	117+56.82	RT. 50.00
12	117+56.82	RT. 40.00
13	117+46.82	RT. 40.00
14	117+46.82	RT. 32.50
15	117+56.82	RT. 32.50

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.



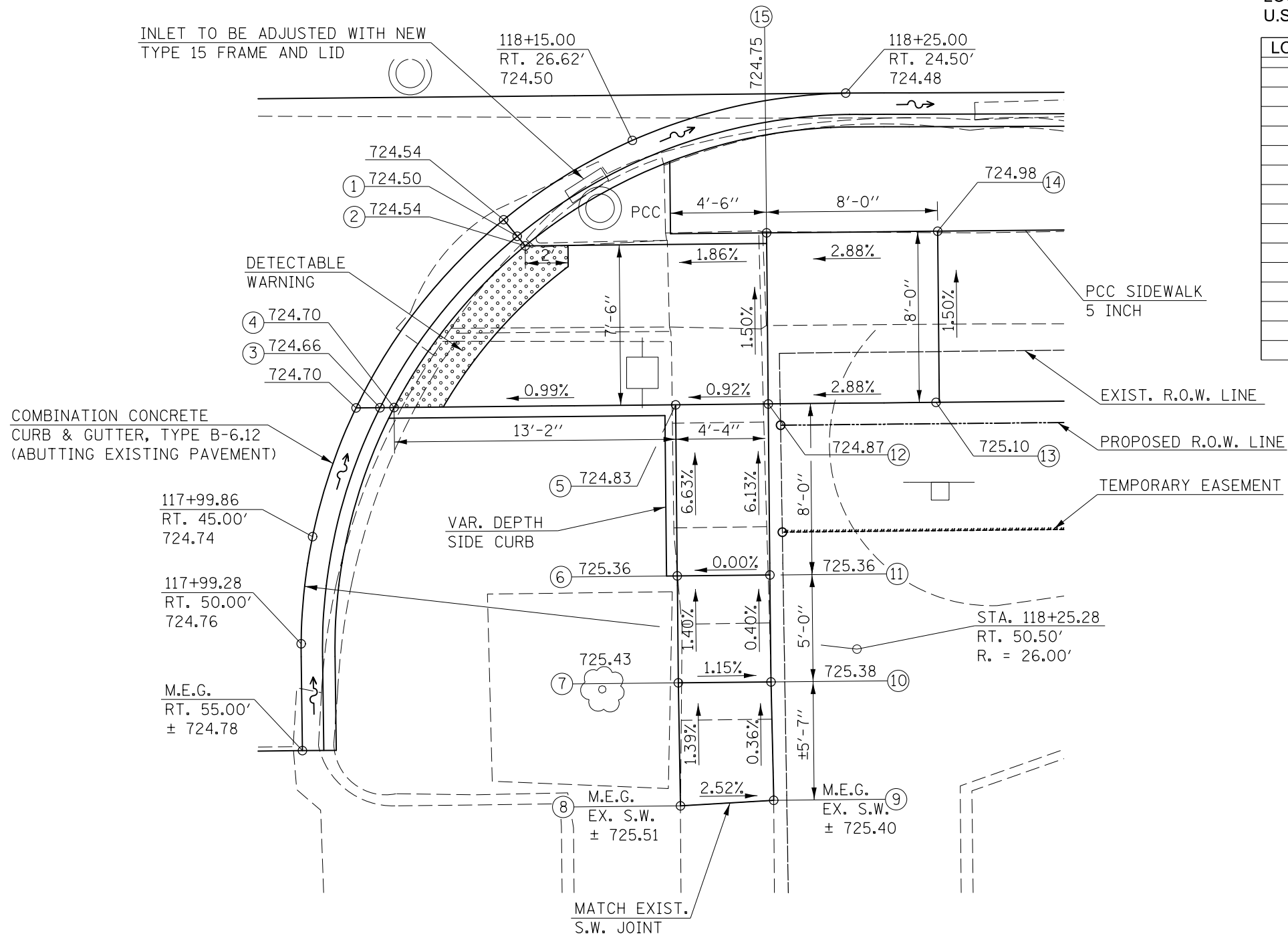
U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #18

S.E. QUADRANT

U.S. 150 & GREGORY ST.

LOCATION #18 - S.E. QUADRANT
U.S. 150 & GREGORY ST.



LOCATION	STATION	OFFSET
1	118+09.57	RT. 31.05
2	118+09.94	RT. 31.50
3	118+03.08	RT. 39.00
4	118+03.74	RT. 39.00
5	118+16.90	RT. 39.00
6	118+16.90	RT. 47.00
7	118+16.90	RT. 52.00
8	118+16.95	RT. 57.75
9	118+21.32	RT. 57.53
10	118+21.24	RT. 52.00
11	118+21.24	RT. 47.00
12	118+21.24	RT. 39.00
13	118+29.24	RT. 39.00
14	118+29.24	RT. 31.00
15	118+21.24	RT. 31.00

NOTES:

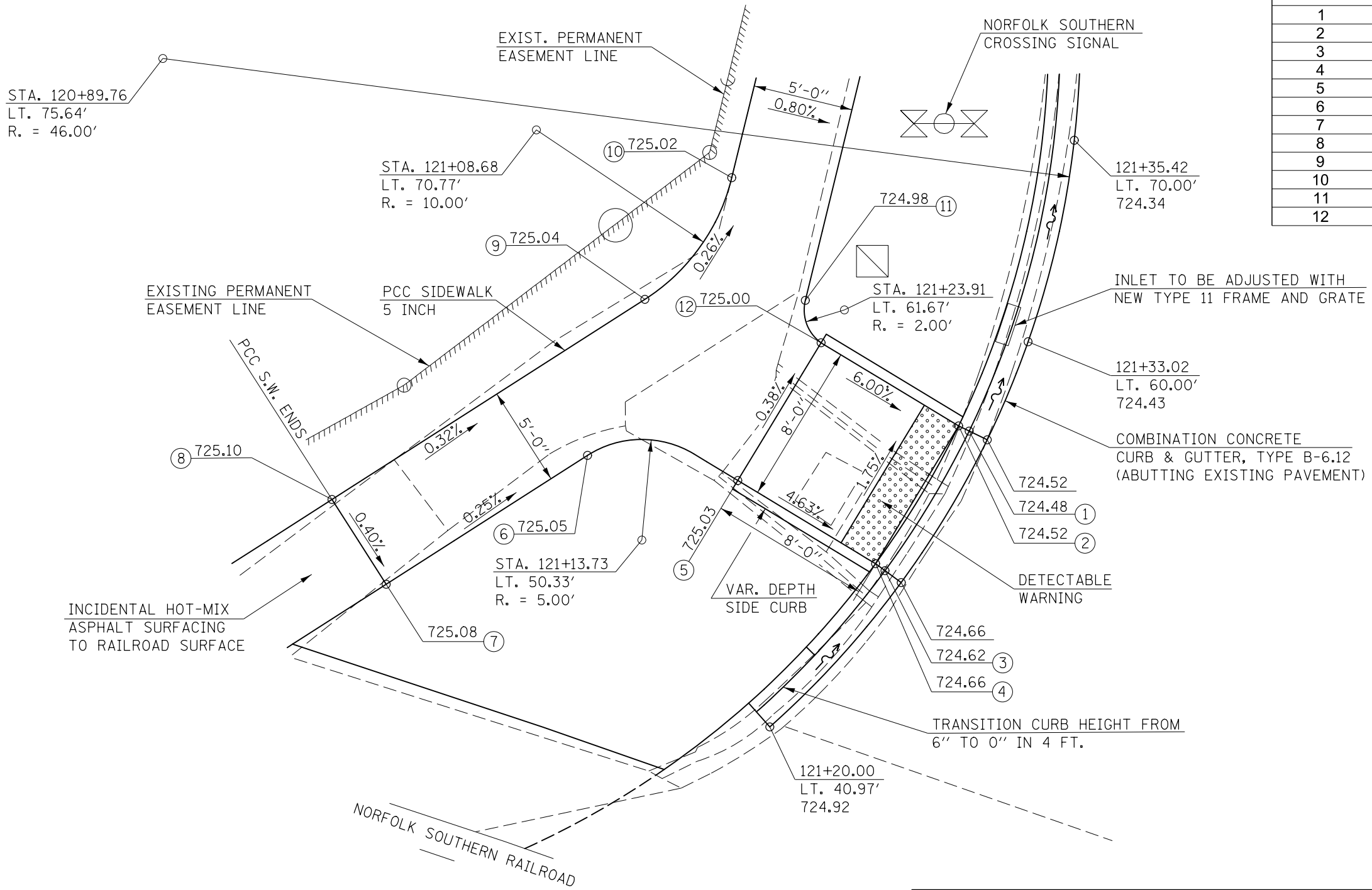
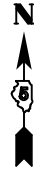
- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #19

N.W. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION #19 - N.W. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION	STATION	OFFSET
1	121+30.05	LT. 55.59
2	121+29.53	LT. 55.85
3	121+25.80	LT. 48.69
4	121+25.33	LT. 49.04
5	121+18.53	LT. 53.24
6	121+11.07	LT. 54.57
7	121+01.00	LT. 48.25
8	120+98.34	LT. 52.49
9	121+13.99	LT. 62.30
10	121+18.37	LT. 68.30
11	121+21.97	LT. 62.17
12	121+22.73	LT. 60.05



- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
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	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LINCOLN AVENUE
ADA RAMP DETAILS**

SCALE: 1" = 3' SHEET 1 OF 5 SHEETS STA. TO STA.

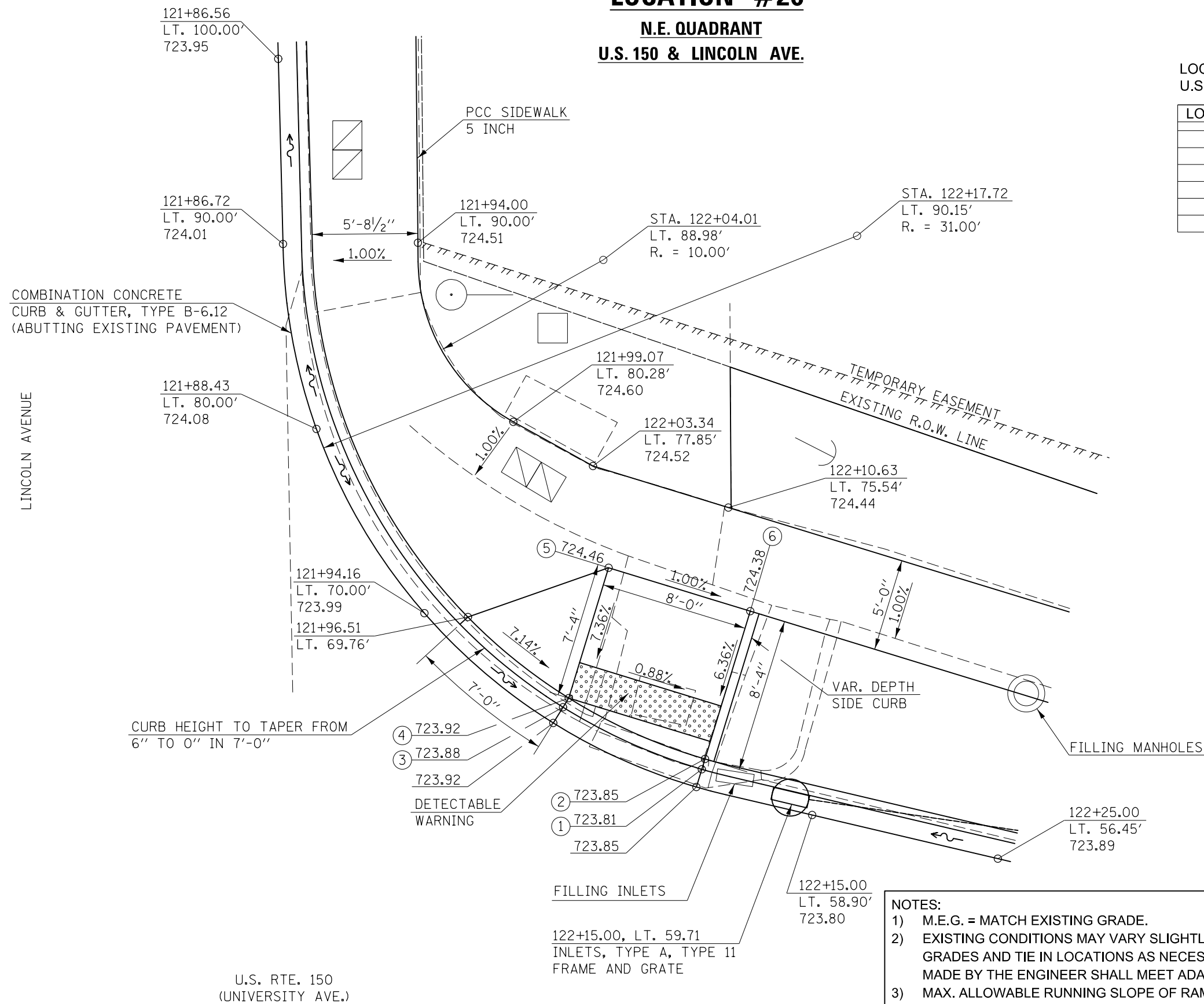
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	160
CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	

LOCATION #20

N.E. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION #20 - N.E. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION	STATION	OFFSET
1	122+09.06	LT. 61.42
2	122+09.24	LT. 61.98
3	122+01.60	LT. 64.85
4	122+01.91	LT. 65.34
5	122+04.14	LT. 72.35
6	122+11.76	LT. 69.94



NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
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	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LINCOLN AVENUE
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	161
CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	

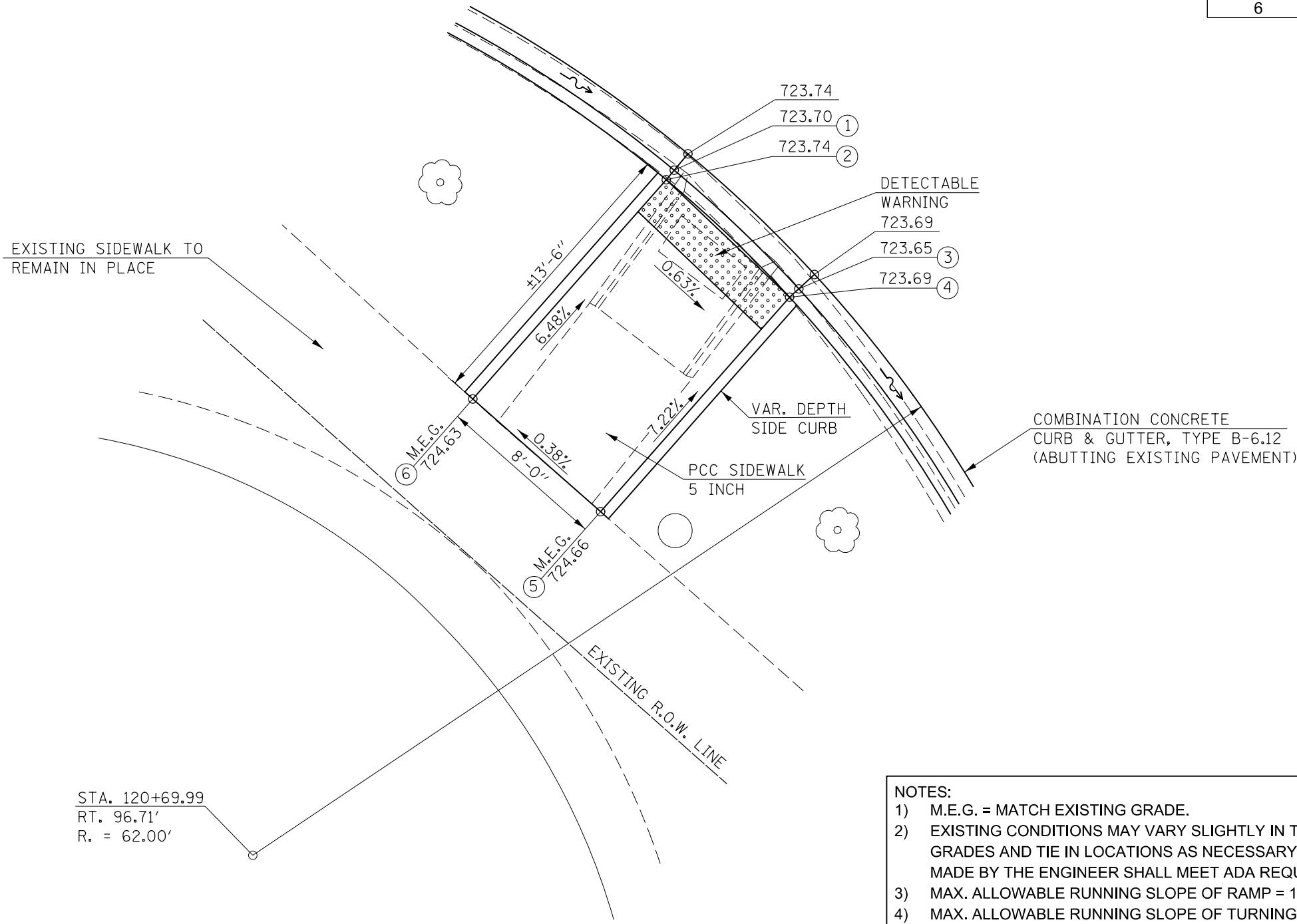
LOCATION #21

S.W. QUADRANT
U.S. 150 & LINCOLN AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #21 - S.W. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION	STATION	OFFSET
1	121+09.49	RT. 50.22
2	121+09.11	RT. 50.67
3	121+15.29	RT. 55.85
4	121+14.86	RT. 56.24
5	121+05.88	RT. 66.23
6	120+99.93	RT. 60.89



- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #22

S.E. QUADRANT
U.S. 150 & LINCOLN AVE.

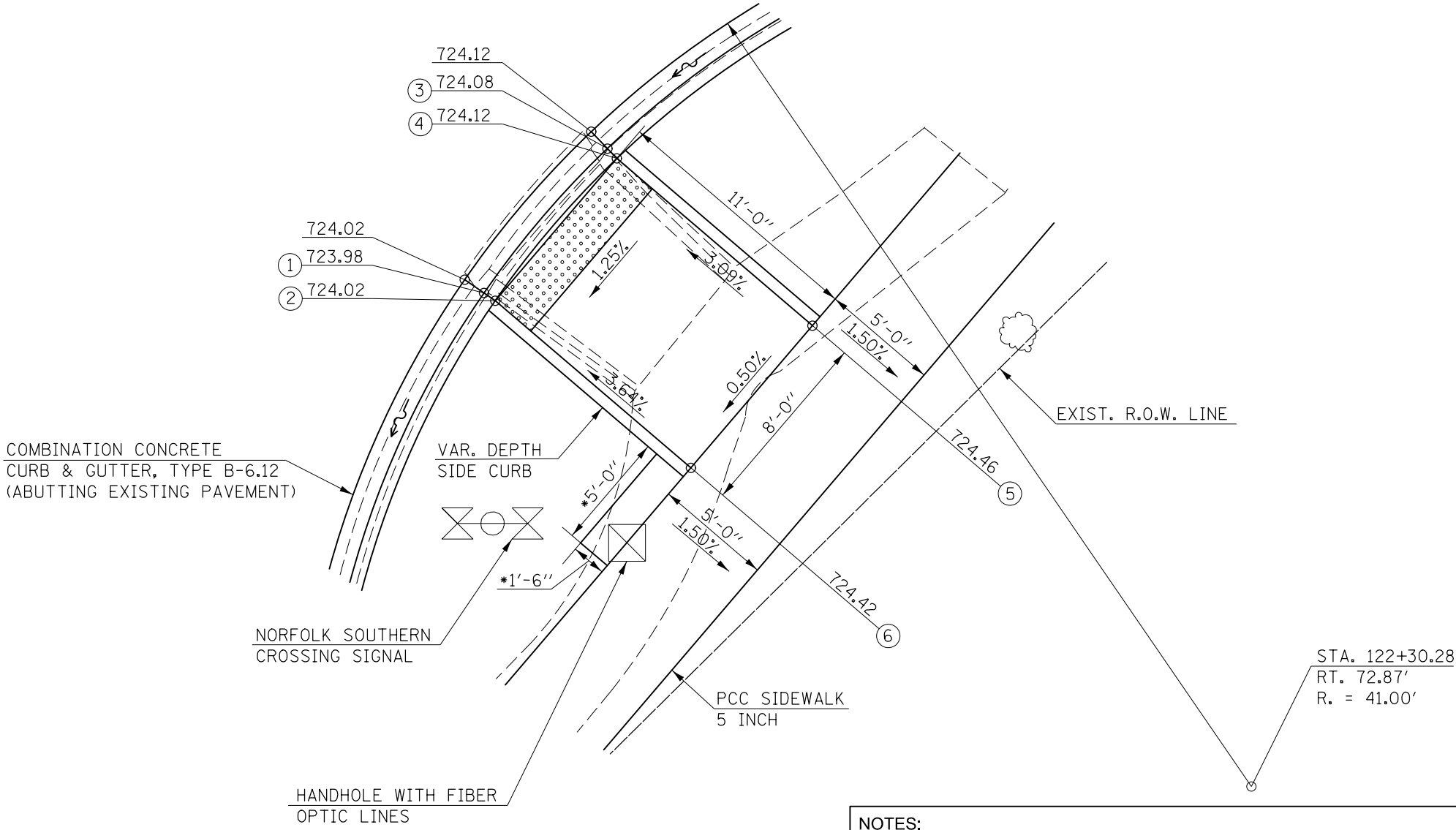
LOCATION #22 - S.E. QUADRANT
U.S. 150 & LINCOLN AVE.

LOCATION	STATION	OFFSET
1	121+97.61	RT. 49.79
2	121+98.09	RT. 50.12
3	122+02.94	RT. 43.67
4	122+03.34	RT. 44.09
5	122+11.64	RT. 51.32
6	122+06.38	RT. 57.35



U.S. RTE. 150
(UNIVERSITY AVE.)

LINCOLN AVENUE



*** NOTE:**
IF THE HANDHOLE WITH FIBER OPTIC LINES CAN NOT BE MOVED OUT OF THE PROPOSED SIDEWALK, CONSTRUCT A 1'-6" X 5'-0" PCC SIDEWALK AROUND THE HANDHOLE.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LINCOLN AVENUE ADA RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0570853-shr-ramps.dgn	DESIGNED -	REVISED -	808			D5 HSIP 2016-1	CHAMPAIGN	499	163	
MODELNAME	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70B53		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 1/28/2019	DATE -	REVISED -			SCALE: 1" = 3'	SHEET 4 OF 5 SHEETS	STA.	TO STA.	

LOCATION #23

N.E. QUADRANT (ISLAND)

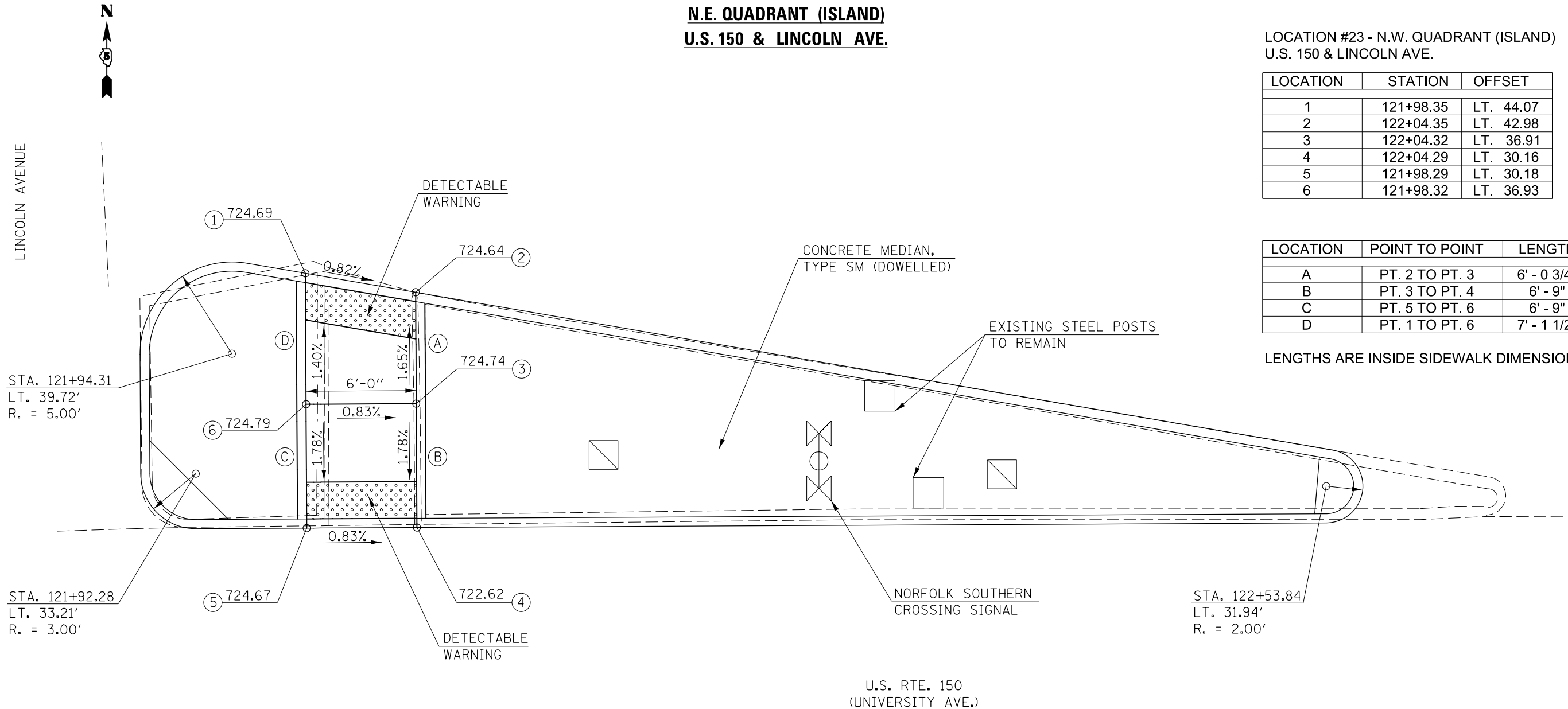
U.S. 150 & LINCOLN AVE.

LOCATION #23 - N.W. QUADRANT (ISLAND)
U.S. 150 & LINCOLN AVE.

LOCATION	STATION	OFFSET
1	121+98.35	LT. 44.07
2	122+04.35	LT. 42.98
3	122+04.32	LT. 36.91
4	122+04.29	LT. 30.16
5	121+98.29	LT. 30.18
6	121+98.32	LT. 36.93

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 3	6' - 0 3/4"
B	PT. 3 TO PT. 4	6' - 9"
C	PT. 5 TO PT. 6	6' - 9"
D	PT. 1 TO PT. 6	7' - 1 1/2"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS



CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CURB & GUTTER SHALL BE REMOVED AND WILL BE PAID FOR AS COMBINATION CURB & GUTTER REMOVAL.
- 3) THE CONCRETE MEDIAN SURFACE 4" SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN SURFACE REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 4) THE EXISTING SIDEWALK SHALL BE REMOVED AND WILL BE PAID FOR AS SIDEWALK REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 5) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 13". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT THE SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 6) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

NOTE:

SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE PROPOSED ISLANDS.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglino	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570853-shr-ramps.dgn		CHECKED -	REVISED -
		DATE -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LINCOLN AVENUE
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	164
CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	

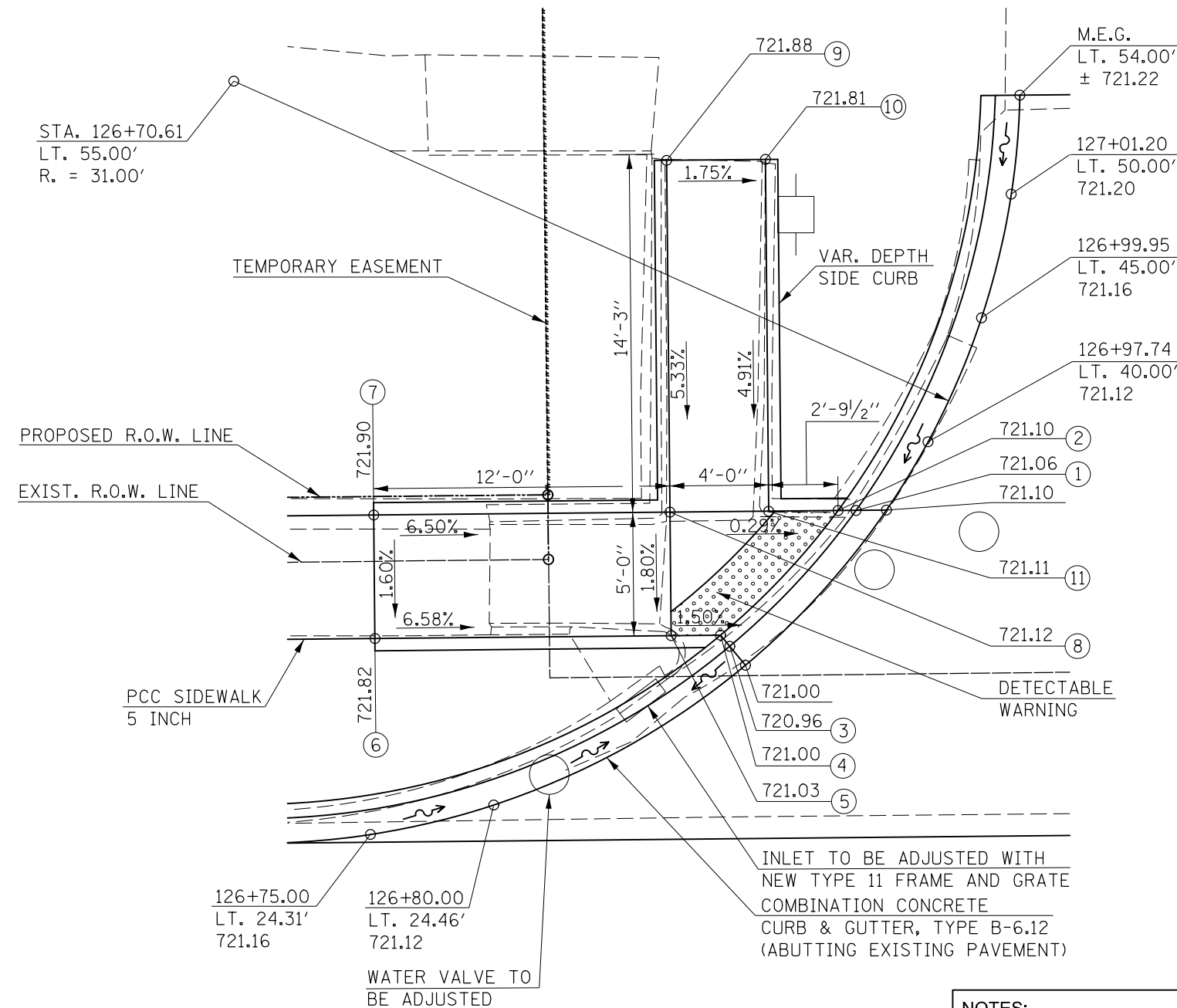


LOCATION #24

N.W. QUADRANT
U.S. 150 & BUSEY AVE.

LOCATION #24 - N.W. QUADRANT
U.S. 150 & BUSEY AVE.

LOCATION	STATION	OFFSET
1	126+94.79	LT. 37.25
2	126+94.06	LT. 37.25
3	126+89.62	LT. 31.80
4	126+89.26	LT. 32.25
5	126+87.26	LT. 32.25
6	126+75.26	LT. 32.25
7	126+75.26	LT. 37.25
8	126+87.26	LT. 37.25
9	126+87.26	LT. 51.50
10	126+91.26	LT. 51.50
11	126+91.26	LT. 37.25



BUSEY AVENUE

U.S. RTE. 150
(UNIVERSITY AVE.)

NOTES:

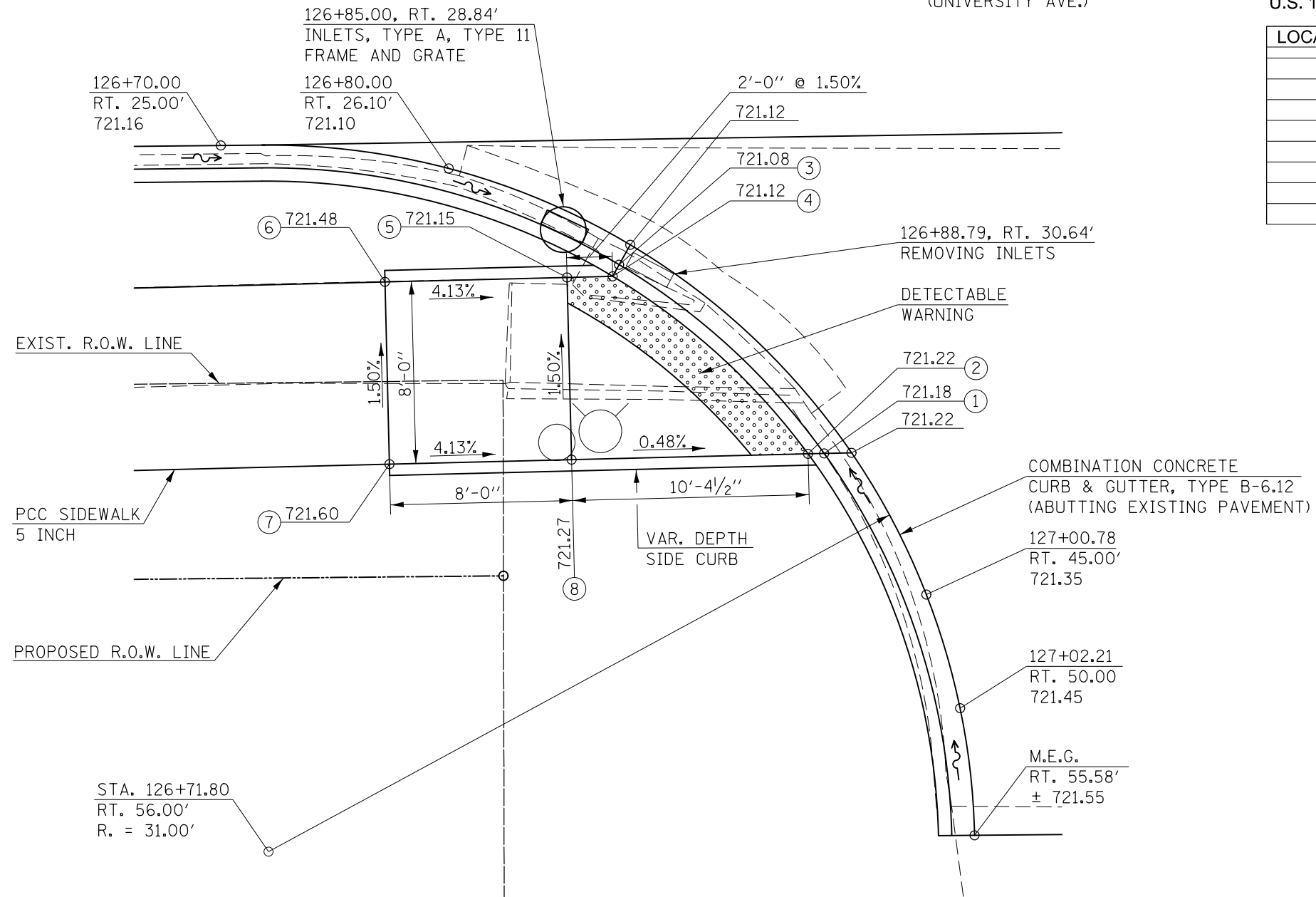
- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #26

S.W. QUADRANT
U.S. 150 & BUSEY AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #26 - S.W. QUADRANT
U.S. 150 & BUSEY AVE.



LOCATION	STATION	OFFSET
1	126+96.36	RT. 38.77
2	126+95.65	RT. 38.77
3	126+87.44	RT. 30.40
4	126+87.15	RT. 30.90
5	126+85.15	RT. 30.93
6	126+77.15	RT. 31.05
7	126+77.27	RT. 39.05
8	126+85.27	RT. 38.93

BUSEY AVENUE

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
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- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUSEY AVENUE ADA RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\BROWND\Design\0570853-sh-ramps.dgn	DESIGNED -	REVISED -	808			D5 HSIP 2016-1	CHAMPAIGN	499	167	
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70B53				
	PLOT DATE = 1/28/2019	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

SCALE: 1" = 3' SHEET 3 OF 4 SHEETS STA. TO STA.



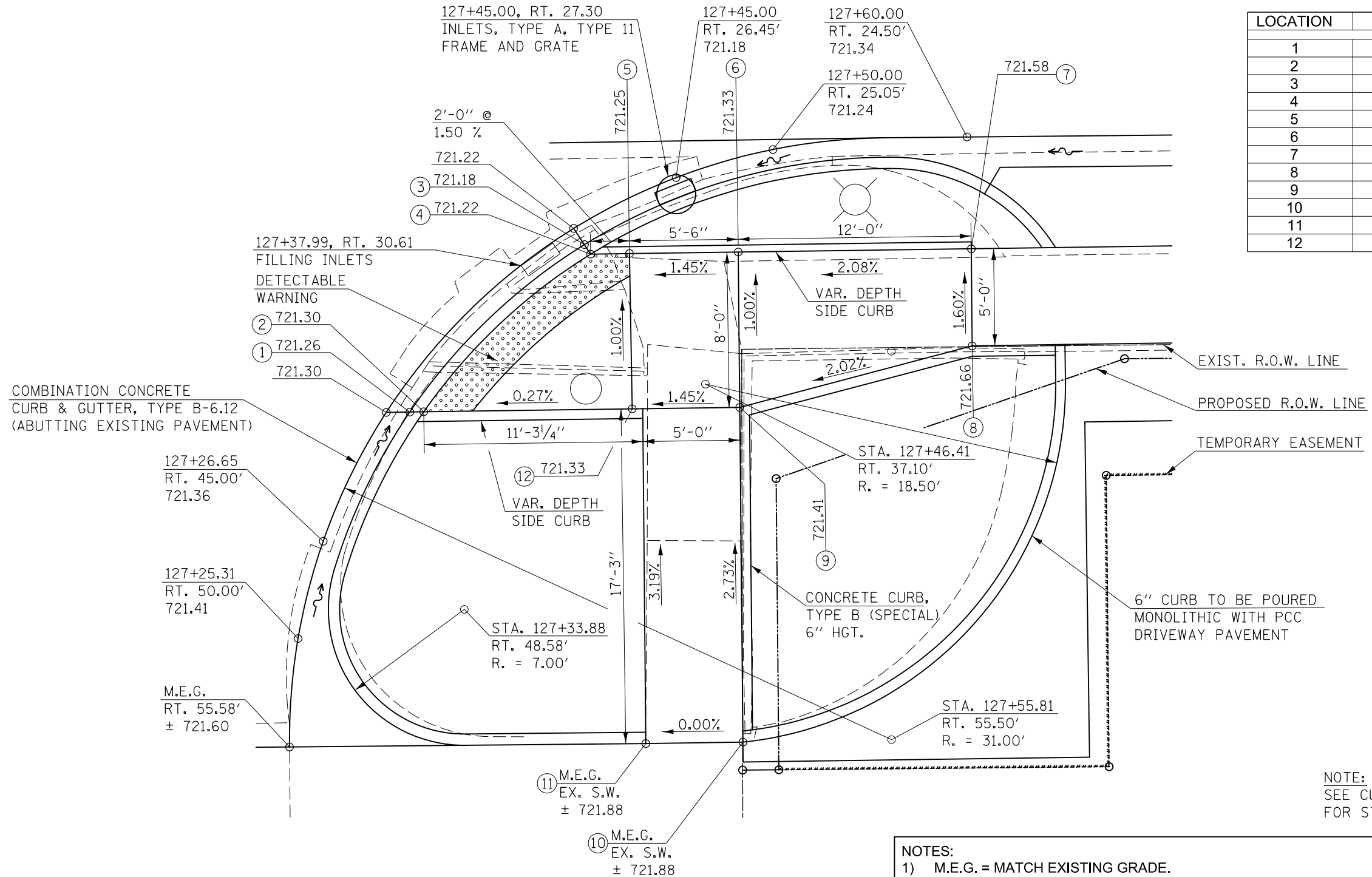
U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #27

S.E. QUADRANT
U.S. 150 & BUSEY AVE.

LOCATION #27 - S.E. QUADRANT
U.S. 150 & BUSEY AVE.

LOCATION	STATION	OFFSET
1	127+31.17	RT. 38.39
2	127+31.89	RT. 38.39
3	127+40.24	RT. 29.86
4	127+40.56	RT. 30.35
5	127+42.56	RT. 30.34
6	127+48.16	RT. 30.31
7	127+60.16	RT. 30.26
8	127+60.16	RT. 35.26
9	127+48.16	RT. 38.31
10	127+48.16	RT. 55.56
11	127+43.16	RT. 55.58
12	127+42.62	RT. 38.34



NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

NOTES:

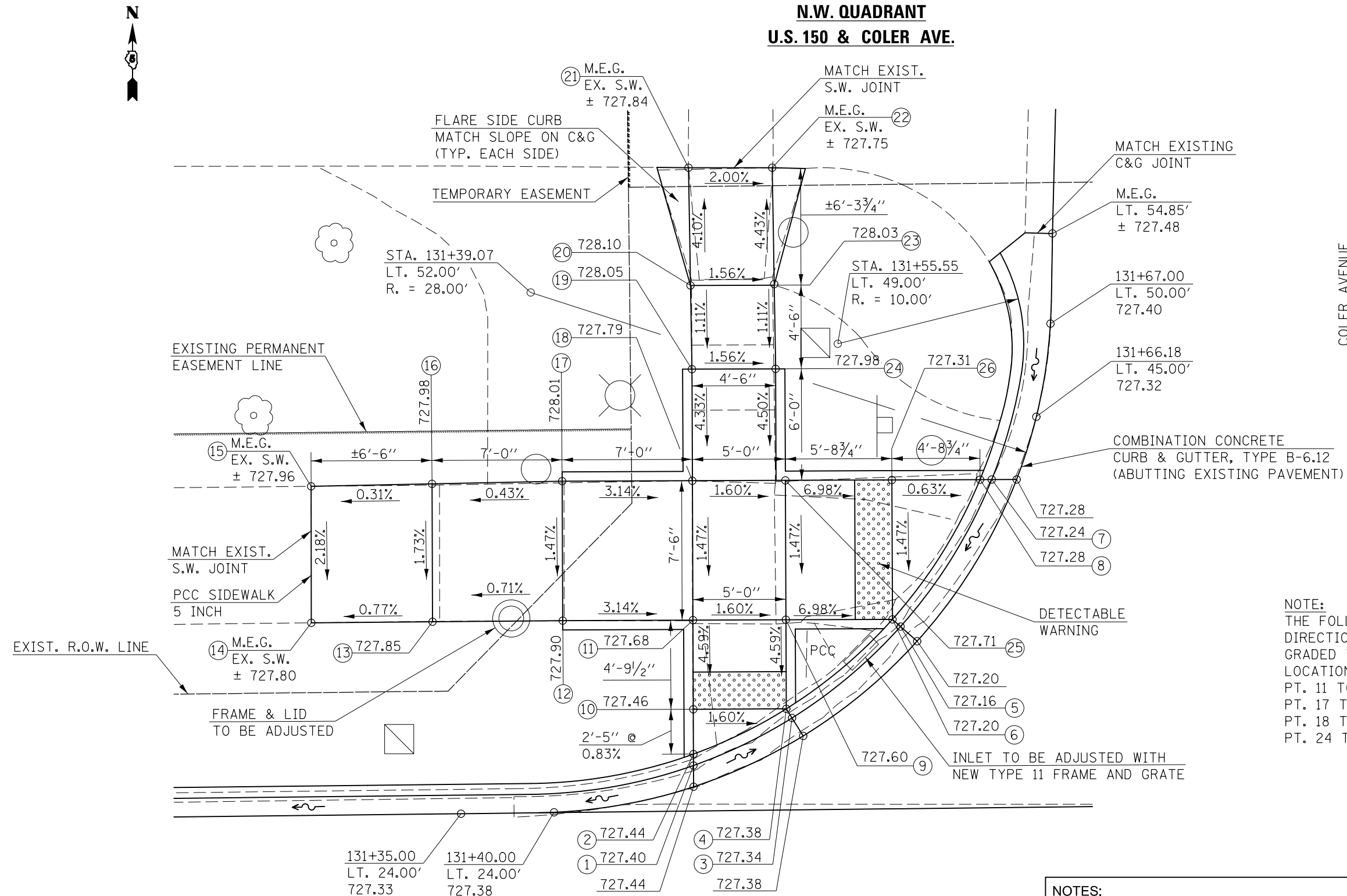
- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUSEY AVENUE ADA RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0577\Drawings\Design\0570853-shr-ramps.dgn	DRAWN	REVISION	REVISION			808	D5 HSIP 2016-1	CHAMPAIGN	499	168	
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISION			CONTRACT NO. 70B53					
	PLOT DATE = 1/28/2019	DATE -	REVISION			ILLINOIS FED. AID PROJECT					

LOCATION #28

N.W. QUADRANT
U.S. 150 & COLER AVE.

LOCATION #28 - N.W. QUADRANT
U.S. 150 & COLER AVE.



LOCATION	STATION	OFFSET
1	131+47.53	LT. 26.45
2	131+47.54	LT. 27.07
3	131+52.86	LT. 28.94
4	131+52.55	LT. 29.44
5	131+58.75	LT. 33.80
6	131+58.32	LT. 34.19
7	131+63.74	LT. 41.65
8	131+63.11	LT. 41.66
9	131+52.59	LT. 34.23
10	131+47.55	LT. 29.48
11	131+47.59	LT. 34.27
12	131+40.59	LT. 34.32
13	131+33.59	LT. 34.34
14	131+27.07	LT. 34.36
15	131+27.14	LT. 41.70
16	131+33.65	LT. 41.76
17	131+40.65	LT. 41.82
18	131+47.65	LT. 41.77
19	131+47.69	LT. 47.77
20	131+47.67	LT. 52.27
21	131+47.63	LT. 58.61
22	131+52.13	LT. 58.56
23	131+52.17	LT. 52.24
24	131+52.19	LT. 47.74
25	131+52.65	LT. 41.73
26	131+58.37	LT. 41.69

NOTE:
THE FOLLOWING SIDE CURBS MAY BE OMITTED PER THE DIRECTION OF THE ENGINEER IF EXISING EARTH CAN BE GRADED TO MATCH THE PROPOSED SIDEWALK ELEVATIONS.
LOCATIONS:
PT. 11 TO PT. 12
PT. 17 TO PT. 18
PT. 18 TO PT. 19
PT. 24 TO PT. 25

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

U.S. RTE. 150
(UNIVERSITY AVE.)

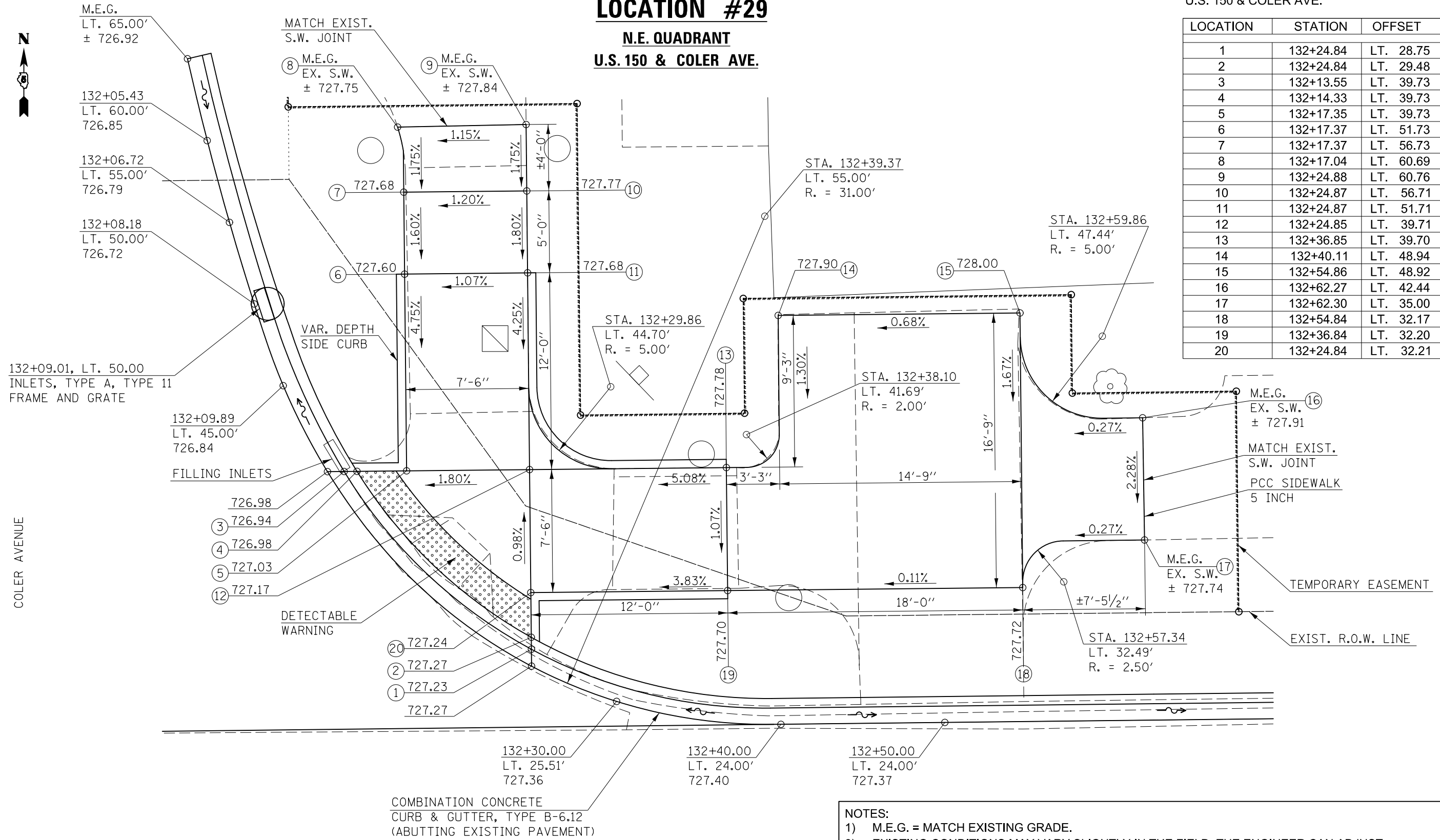
LOCATION #29

N.E. QUADRANT

U.S. 150 & COLER AVE.

LOCATION #29 - N.E. QUADRANT
U.S. 150 & COLER AVE.

LOCATION	STATION	OFFSET
1	132+24.84	LT. 28.75
2	132+24.84	LT. 29.48
3	132+13.55	LT. 39.73
4	132+14.33	LT. 39.73
5	132+17.35	LT. 39.73
6	132+17.37	LT. 51.73
7	132+17.37	LT. 56.73
8	132+17.04	LT. 60.69
9	132+24.88	LT. 60.76
10	132+24.87	LT. 56.71
11	132+24.87	LT. 51.71
12	132+24.85	LT. 39.71
13	132+36.85	LT. 39.70
14	132+40.11	LT. 48.94
15	132+54.86	LT. 48.92
16	132+62.27	LT. 42.44
17	132+62.30	LT. 35.00
18	132+54.84	LT. 32.17
19	132+36.84	LT. 32.20
20	132+24.84	LT. 32.21



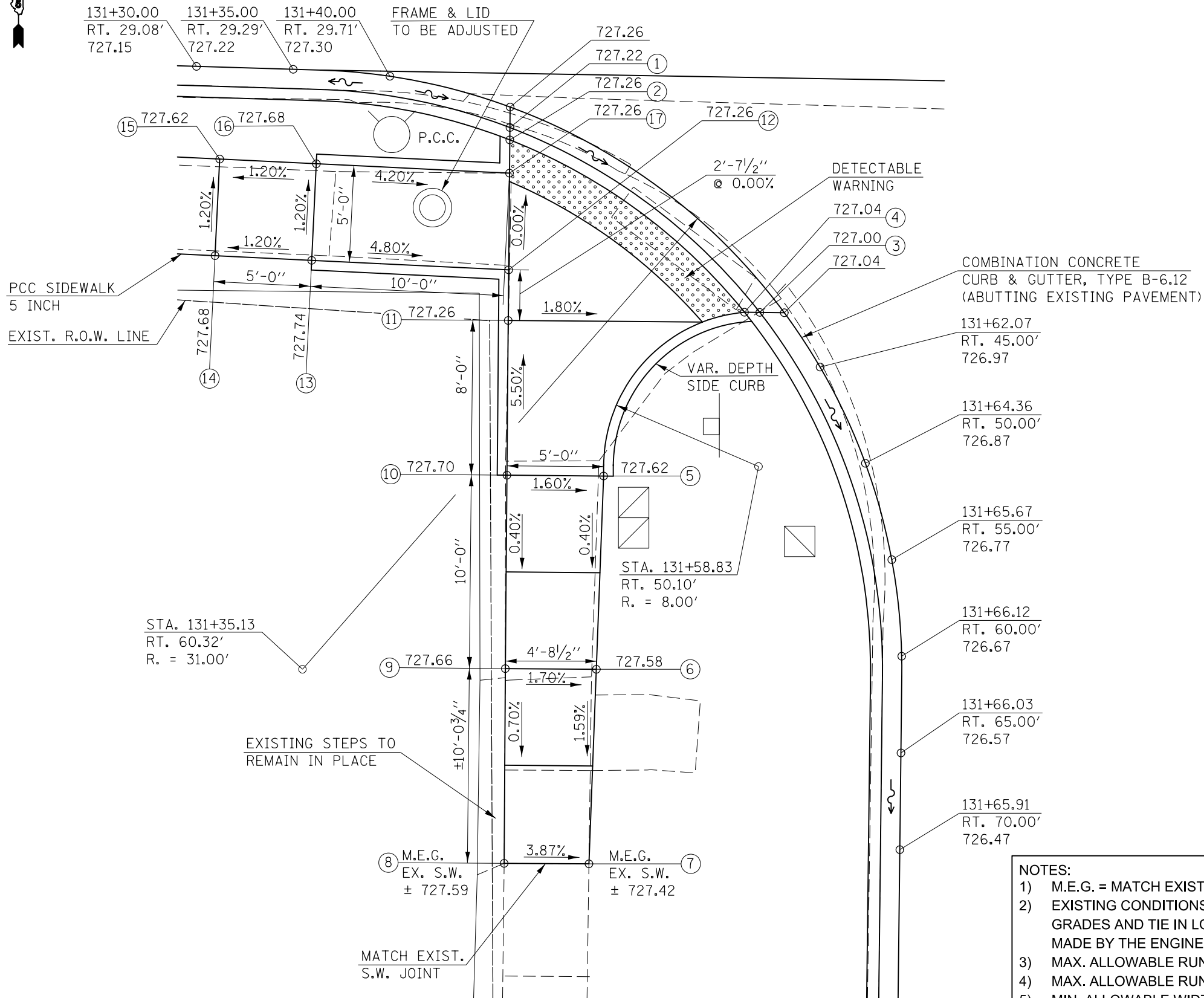
NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #30

S.W. QUADRANT
U.S. 150 & COLER AVE.



LOCATION #30 - S.W. QUADRANT
U.S. 150 & COLER AVE.

LOCATION	STATION	OFFSET
1	131+46.17	RT. 32.43
2	131+46.16	RT. 33.07
3	131+58.99	RT. 42.14
4	131+58.19	RT. 42.12
5	131+50.81	RT. 50.50
6	131+50.32	RT. 60.50
7	131+49.83	RT. 70.55
8	131+45.44	RT. 70.47
9	131+45.62	RT. 60.40
10	131+45.81	RT. 50.41
11	131+45.97	RT. 42.41
12	131+46.02	RT. 39.79
13	131+35.84	RT. 39.18
14	131+30.85	RT. 38.88
15	131+31.15	RT. 33.89
16	131+36.14	RT. 34.19
17	131+46.12	RT. 34.79

COLER AVENUE

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
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- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #31

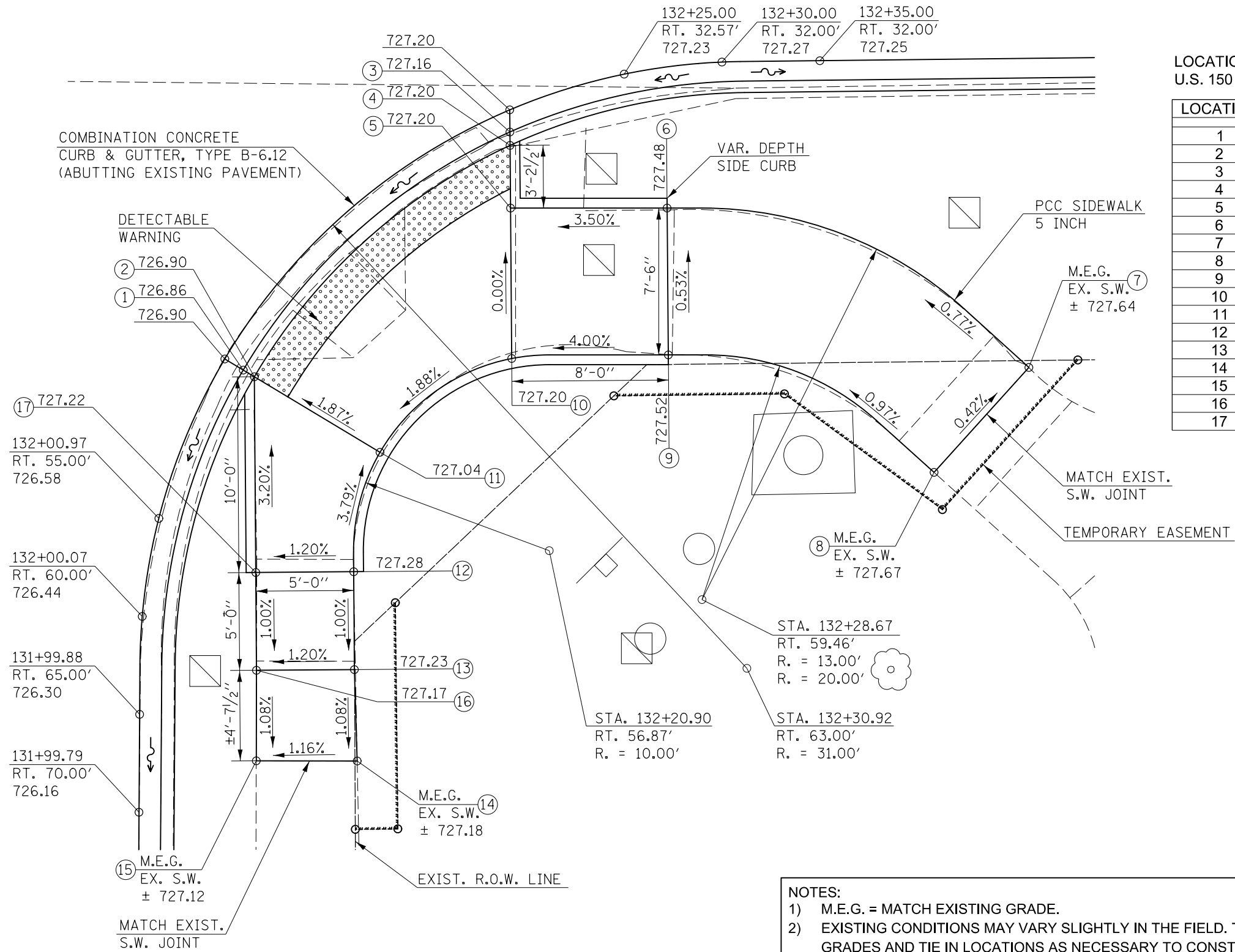
S.E. QUADRANT

U.S. 150 & COLER AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)



COLER AVENUE



LOCATION #31 - S.E. QUADRANT
U.S. 150 & COLER AVE.

LOCATION	STATION	OFFSET
1	132+05.37	RT. 47.47
2	132+05.94	RT. 47.82
3	132+19.13	RT. 35.47
4	132+19.13	RT. 36.14
5	132+19.11	RT. 39.35
6	132+27.11	RT. 39.44
7	132+45.49	RT. 47.79
8	132+40.59	RT. 53.10
9	132+27.09	RT. 46.94
10	132+19.09	RT. 47.04
11	132+12.31	RT. 51.75
12	132+10.89	RT. 57.83
13	132+10.87	RT. 62.83
14	132+10.96	RT. 67.51
15	132+05.81	RT. 67.44
16	132+05.87	RT. 62.82
17	132+05.89	RT. 57.82

NOTES:

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- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0577\Drawings\Design\05770853-shr-ramps.dwg		REVISIONS	REVISIONS
	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

COLER AVENUE
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 4 OF 4 SHEETS STA. TO STA.

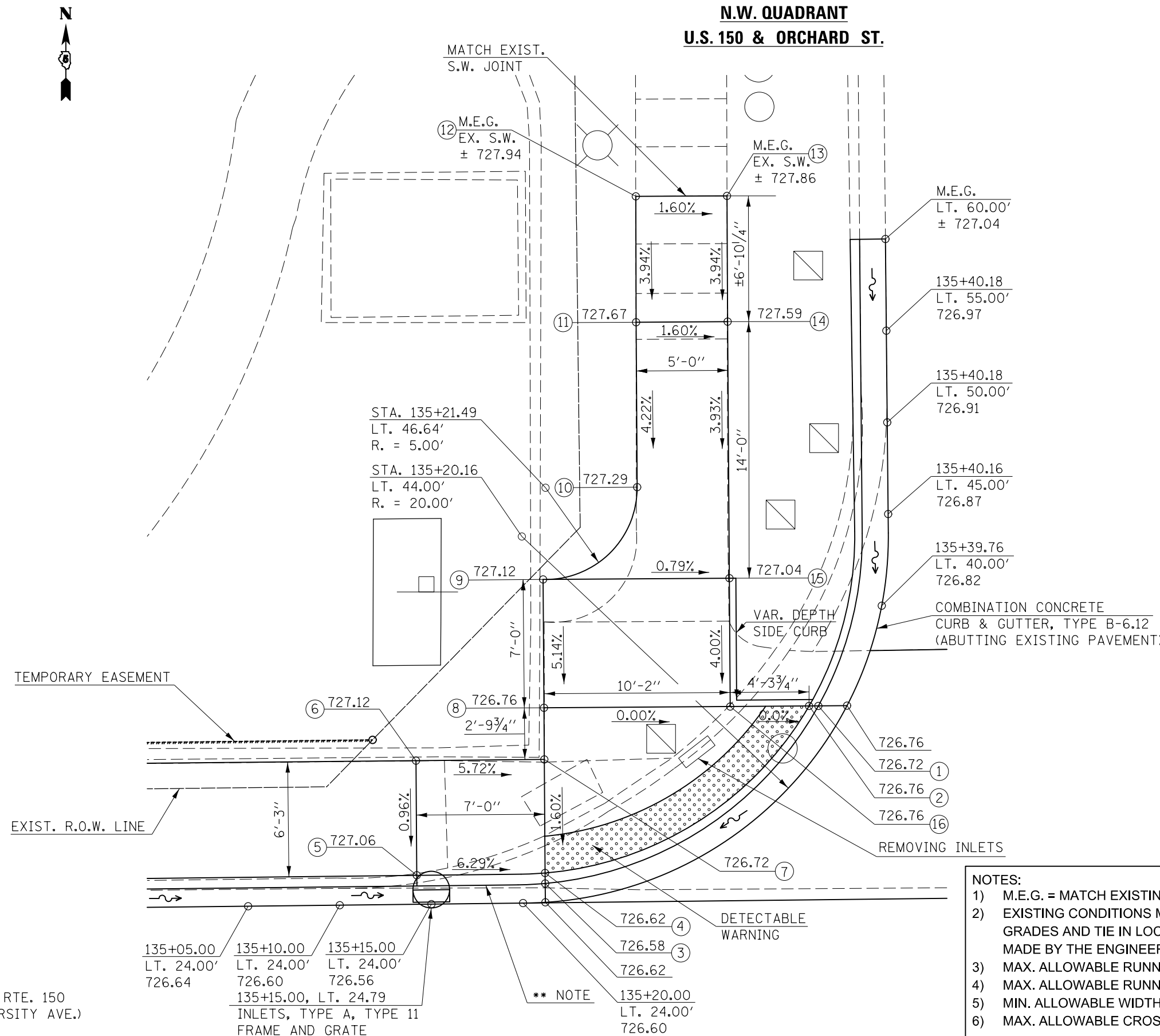
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	172
			CONTRACT NO. 70B53	
ILLINOIS FED. AID PROJECT				

LOCATION #32

N.W. QUADRANT
U.S. 150 & ORCHARD ST.

LOCATION #32 - N.W. QUADRANT
U.S. 150 & ORCHARD ST.

LOCATION	STATION	OFFSET
1	135+36.24	LT. 34.57
2	135+35.73	LT. 34.57
3	135+21.22	LT. 25.06
4	135+21.22	LT. 25.63
5	135+14.22	LT. 25.58
6	135+14.25	LT. 31.84
7	135+21.25	LT. 31.82
8	135+21.26	LT. 34.64
9	135+21.30	LT. 41.64
10	135+26.49	LT. 46.62
11	135+26.53	LT. 55.62
12	135+26.59	LT. 62.48
13	135+31.57	LT. 62.45
14	135+31.53	LT. 55.59
15	135+31.46	LT. 41.59
16	135+31.43	LT. 34.59



**** NOTE:**
CURB HEIGHT TO TAPER FROM
6" TO 0" IN 7'-0"

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

NOTES:

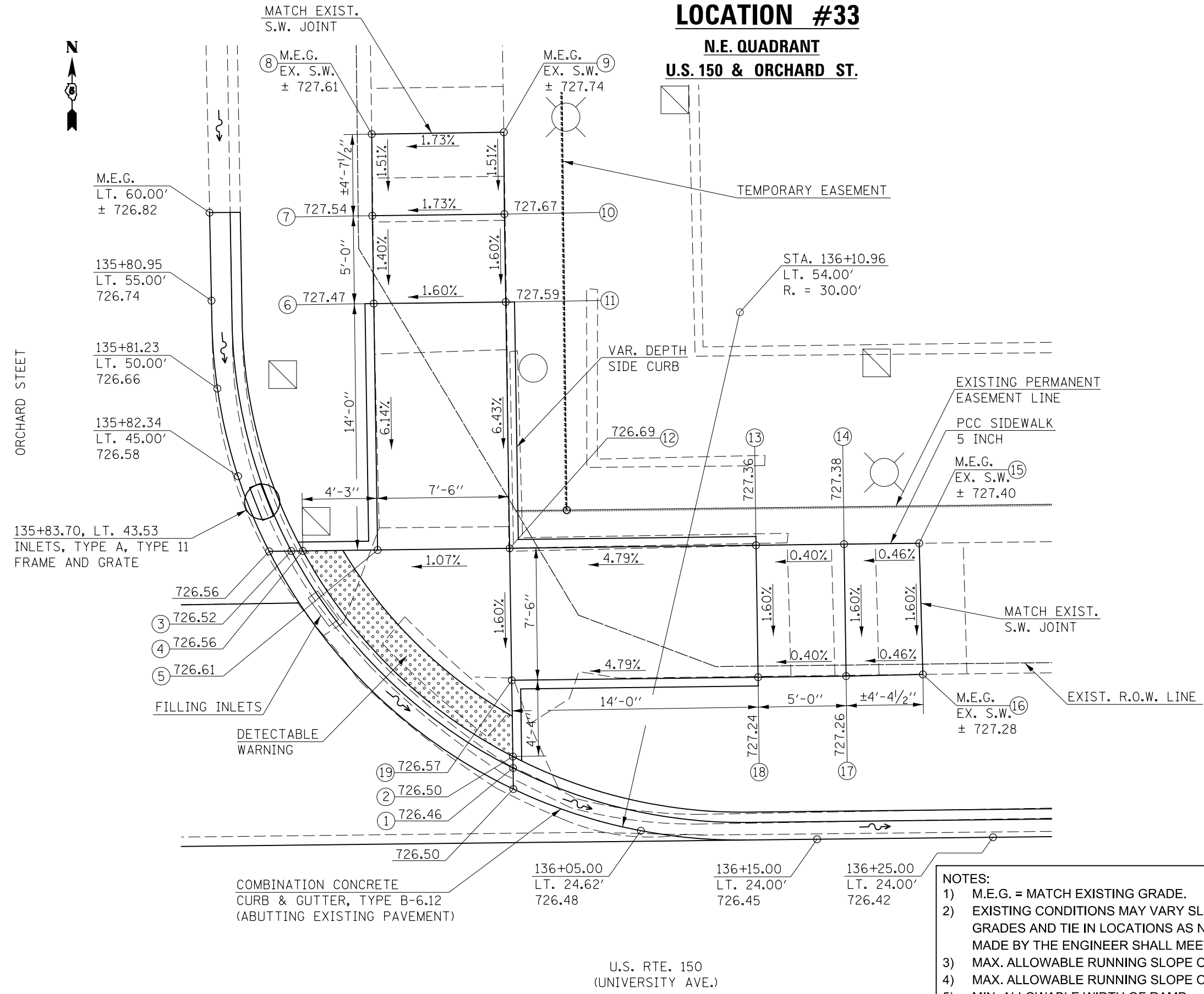
- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #33

N.E. QUADRANT U.S. 150 & ORCHARD ST.

LOCATION 33 - N.E. QUADRANT
U.S. 150 & ORCHARD ST.

LOCATION	STATION	OFFSET
1	135+97.78	LT. 28.24
2	135+97.78	LT. 28.90
3	135+85.33	LT. 40.72
4	135+85.98	LT. 40.72
5	135+90.23	LT. 40.72
6	135+90.17	LT. 54.72
7	135+90.15	LT. 59.72
8	135+90.16	LT. 64.36
9	135+97.66	LT. 64.37
10	135+97.65	LT. 59.73
11	135+97.67	LT. 54.73
12	135+97.73	LT. 40.73
13	136+11.73	LT. 40.74
14	136+16.73	LT. 40.75
15	136+21.00	LT. 40.75
16	136+21.12	LT. 33.30
17	136+16.76	LT. 33.25
18	136+11.76	LT. 33.24
19	135+97.76	LT. 33.23



NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

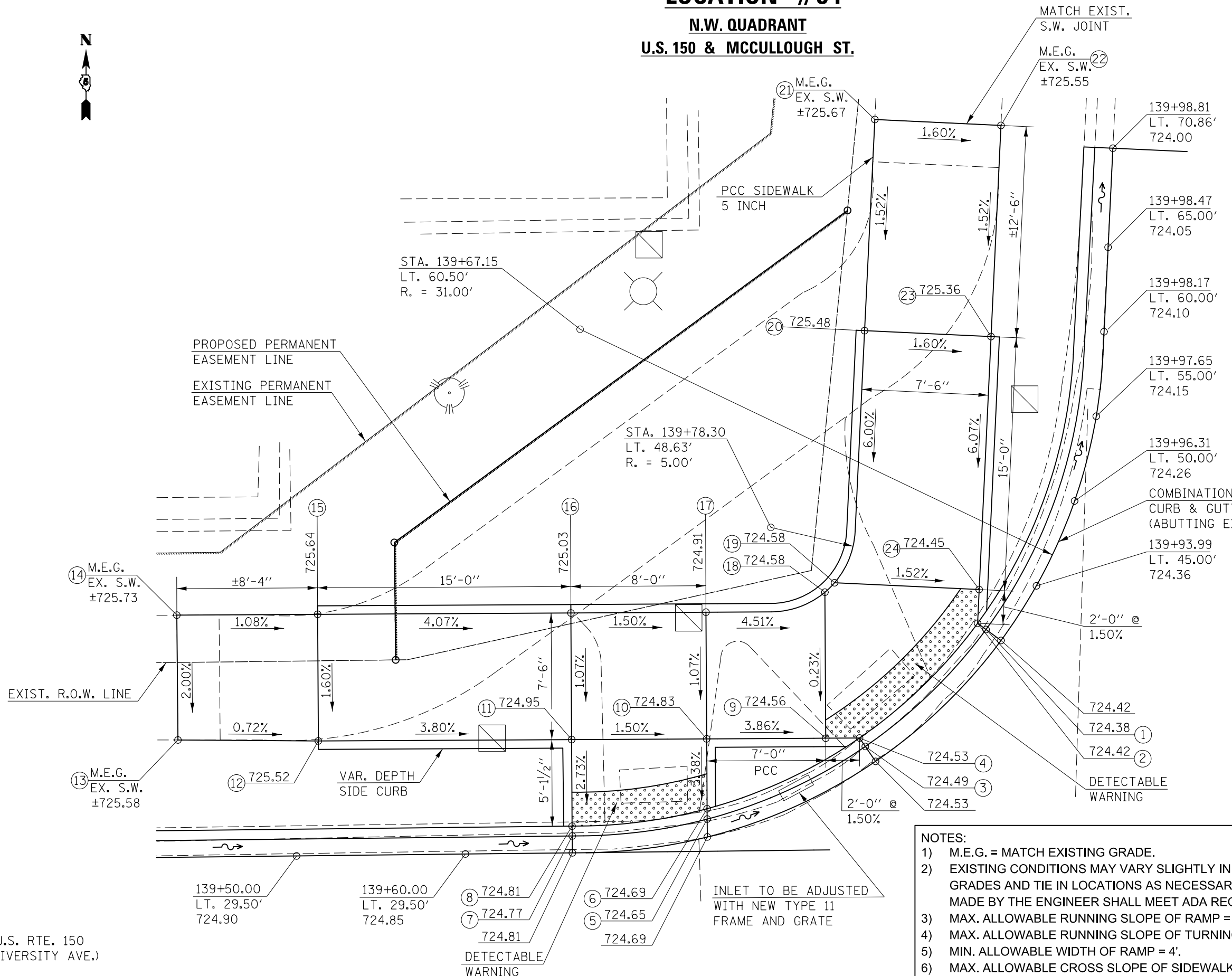
LOCATION #34

N.W. QUADRANT

U.S. 150 & MCCULLOUGH ST.

LOCATION #34 - N.W. QUADRANT
U.S. 150 & MCCULLOUGH ST.

LOCATION	STATION	OFFSET
1	139+90.98	LT. 42.45
2	139+90.49	LT. 42.82
3	139+83.75	LT. 35.60
4	139+83.41	LT. 36.10
5	139+74.34	LT. 31.41
6	139+74.34	LT. 32.02
7	139+66.34	LT. 30.50
8	139+66.34	LT. 31.08
9	139+81.41	LT. 36.12
10	139+74.37	LT. 36.16
11	139+66.37	LT. 36.21
12	139+51.37	LT. 36.30
13	139+43.05	LT. 36.45
14	139+43.07	LT. 43.84
15	139+51.41	LT. 43.79
16	139+66.41	LT. 43.71
17	139+74.41	LT. 43.66
18	139+81.47	LT. 44.76
19	139+82.04	LT. 45.31
20	139+83.99	LT. 60.23
21	139+84.74	LT. 72.72
22	139+92.20	LT. 72.28
23	139+91.47	LT. 59.79
24	139+90.60	LT. 44.81



- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

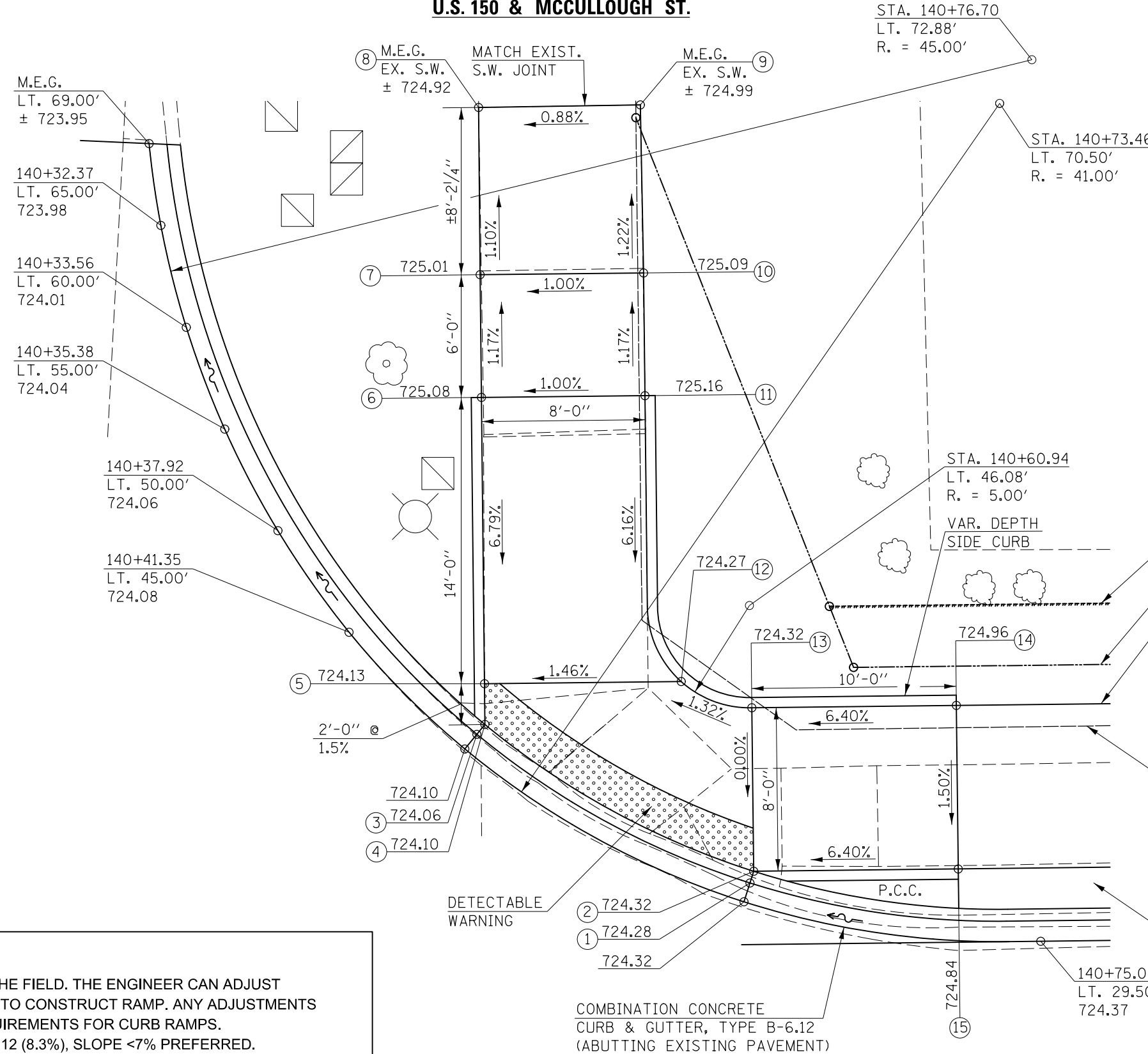
LOCATION #35

N.E. QUADRANT

U.S. 150 & MCCULLOUGH ST.



MCCULLOUGH STREET



LOCATION #35 - N.E. QUADRANT
U.S. 150 & MCCULLOUGH ST.

LOCATION	STATION	OFFSET
1	140+60.81	LT. 32.51
2	140+61.00	LT. 33.08
3	140+47.54	LT. 39.93
4	140+47.94	LT. 40.41
5	140+47.94	LT. 42.41
6	140+47.95	LT. 56.41
7	140+47.95	LT. 62.41
8	140+47.97	LT. 70.59
9	140+55.89	LT. 70.63
10	140+55.95	LT. 62.40
11	140+55.95	LT. 56.40
12	140+57.56	LT. 42.40
13	140+61.00	LT. 41.08
14	140+71.00	LT. 41.08
15	140+71.00	LT. 33.08

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMP.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

U.S. RTE. 150
(UNIVERSITY AVE.)

FILE NAME =	USER NAME = eaglinge	DESIGNED - P.	REVISED -
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579B53-shr-ramps.dgn			REVISED -
	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MCCULLOUGH STREET
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 2 OF 4 SHEETS STA. TO STA.

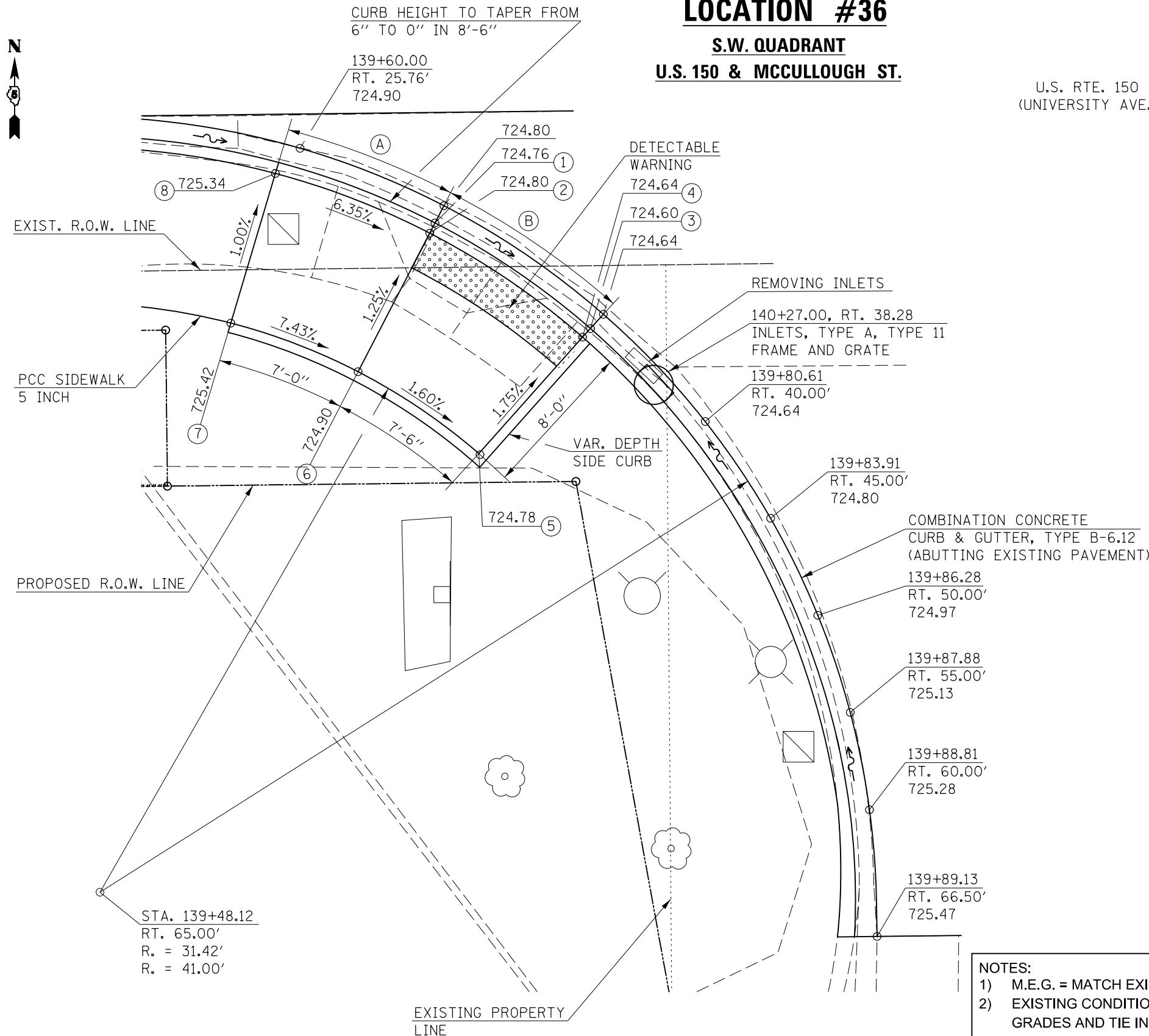
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	176
CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	

LOCATION #36

S.W. QUADRANT
U.S. 150 & MCCULLOUGH ST.

U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #36 - S.W. QUADRANT
U.S. 150 & MCCULLOUGH ST.



LOCATION	STATION	OFFSET
1	139+66.88	RT. 29.66
2	139+66.60	RT. 30.18
3	139+74.78	RT. 35.17
4	139+74.37	RT. 35.60
5	139+68.99	RT. 41.51
6	139+62.84	RT. 37.25
7	139+56.34	RT. 34.68
8	139+58.72	RT. 27.04

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 8	8' - 6"
B	PT. 2 TO PT. 4	9' - 6"

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #37

S.E. QUADRANT

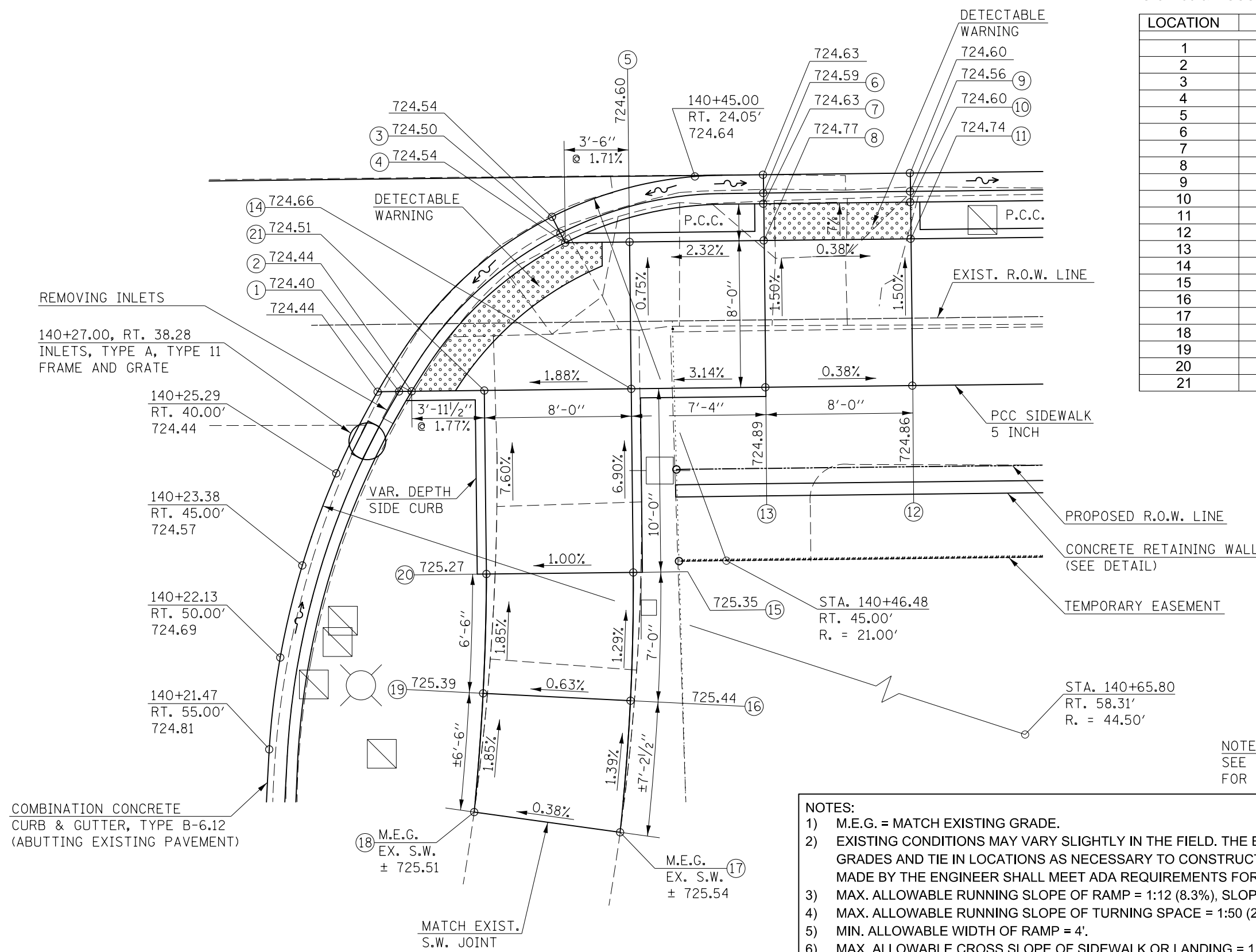
U.S. 150 & MCCULLOUGH ST.

LOCATION #37 - S.E. QUADRANT
U.S. 150 & MCCULLOUGH ST.



U.S. RTE. 150
(UNIVERSITY AVE.)

MCCULLOUGH STREET



LOCATION	STATION	OFFSET
1	140+28.76	RT. 35.58
2	140+29.44	RT. 35.58
3	140+37.64	RT. 27.06
4	140+37.90	RT. 27.58
5	140+41.40	RT. 27.58
6	140+48.73	RT. 25.00
7	140+48.73	RT. 25.58
8	140+48.73	RT. 27.58
9	140+56.73	RT. 25.00
10	140+56.73	RT. 25.58
11	140+56.73	RT. 27.58
12	140+56.73	RT. 35.58
13	140+48.73	RT. 35.58
14	140+41.40	RT. 35.58
15	140+41.40	RT. 45.58
16	140+41.17	RT. 52.57
17	140+40.52	RT. 59.73
18	140+32.59	RT. 58.54
19	140+33.15	RT. 52.08
20	140+33.40	RT. 45.58
21	140+33.40	RT. 35.58

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

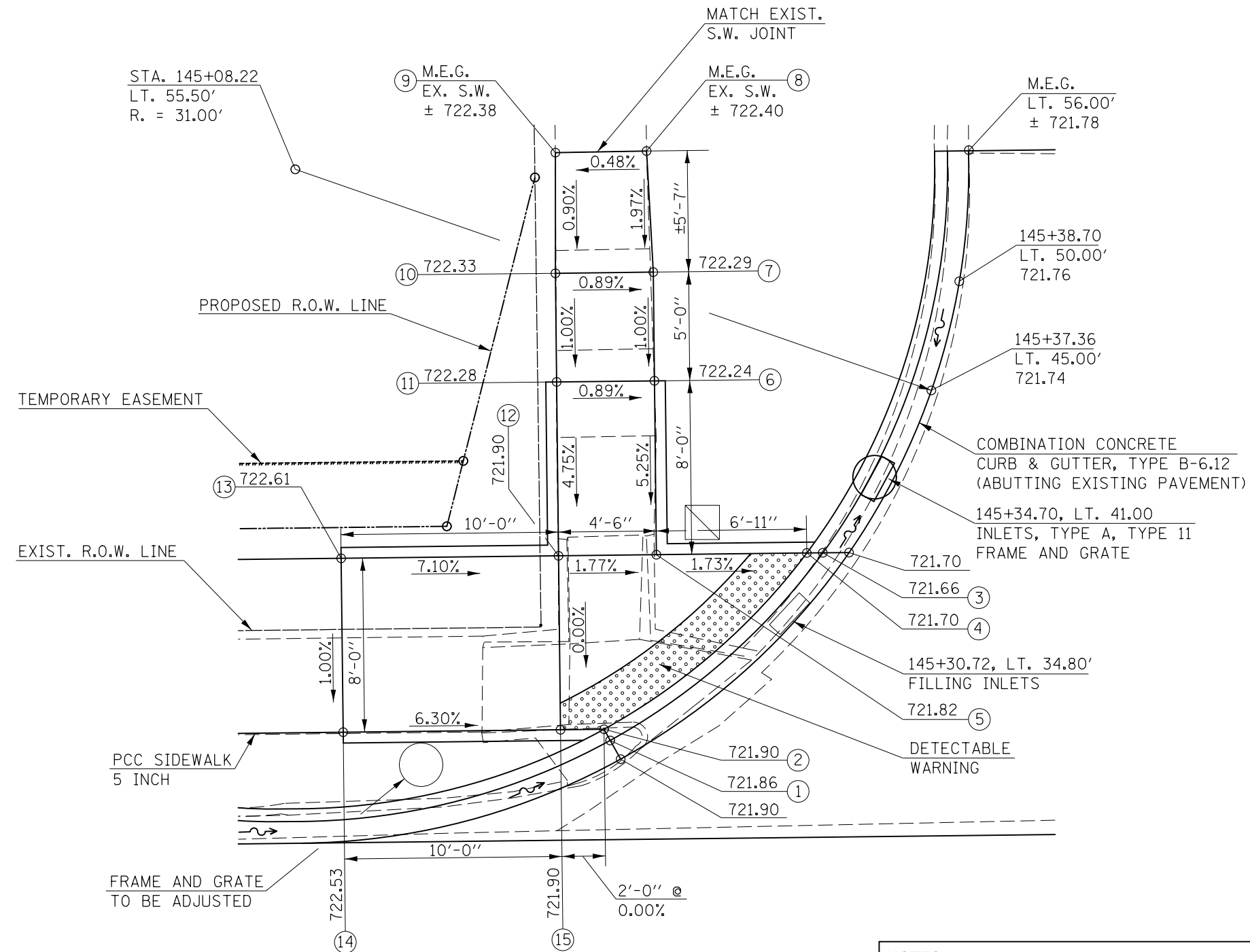
- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.



LOCATION #38

N.W. QUADRANT
U.S. 150 & CENTRAL AVE.

LOCATION #38 - N.W. QUADRANT
U.S. 150 & CENTRAL AVE.



LOCATION	STATION	OFFSET
1	145+22.40	LT. 29.07
2	145+22.12	LT. 29.58
3	145+32.28	LT. 37.58
4	145+31.54	LT. 37.58
5	145+24.62	LT. 37.58
6	145+24.62	LT. 45.58
7	145+24.62	LT. 50.58
8	145+24.38	LT. 56.16
9	145+20.19	LT. 56.16
10	145+20.12	LT. 50.58
11	145+20.12	LT. 45.58
12	145+20.12	LT. 37.58
13	145+10.12	LT. 37.58
14	145+10.12	LT. 29.58
15	145+20.12	LT. 29.58

CENTRAL AVENUE

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

- NOTES:
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 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

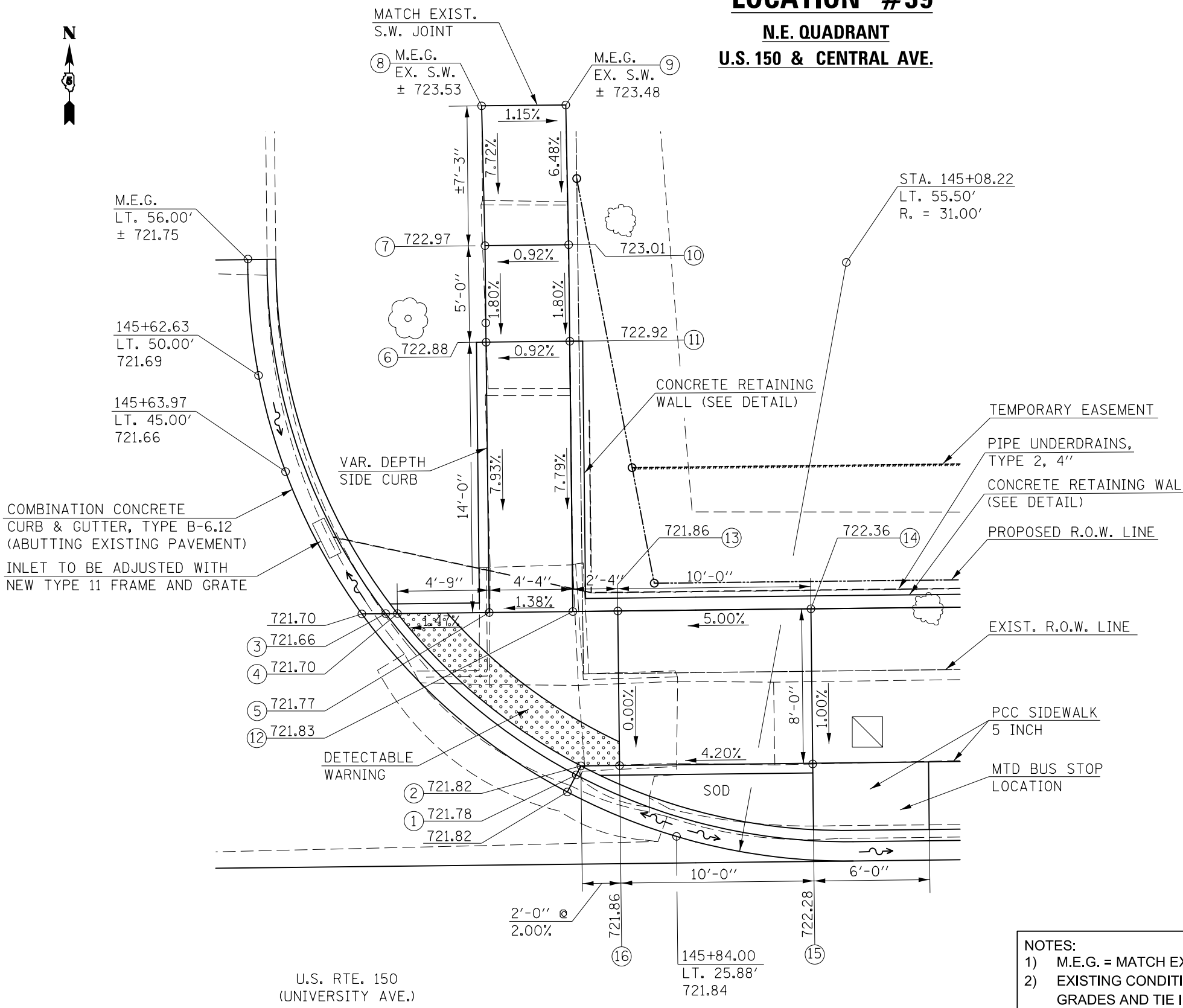
LOCATION #39

N.E. QUADRANT U.S. 150 & CENTRAL AVE.

LOCATION #39 - N.E. QUADRANT
U.S. 150 & CENTRAL AVE.

LOCATION	STATION	OFFSET
1	145+78.85	LT. 29.12
2	145+79.10	LT. 29.58
3	145+69.07	LT. 37.58
4	145+69.68	LT. 37.58
5	145+74.43	LT. 37.58
6	145+74.43	LT. 51.58
7	145+74.43	LT. 56.58
8	145+74.34	LT. 63.83
9	145+78.70	LT. 63.83
10	145+78.77	LT. 56.58
11	145+78.77	LT. 51.58
12	145+78.77	LT. 37.58
13	145+81.10	LT. 37.58
14	145+91.10	LT. 37.58
15	145+91.10	LT. 29.58
16	145+81.10	LT. 29.58

CENTRAL AVENUE



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

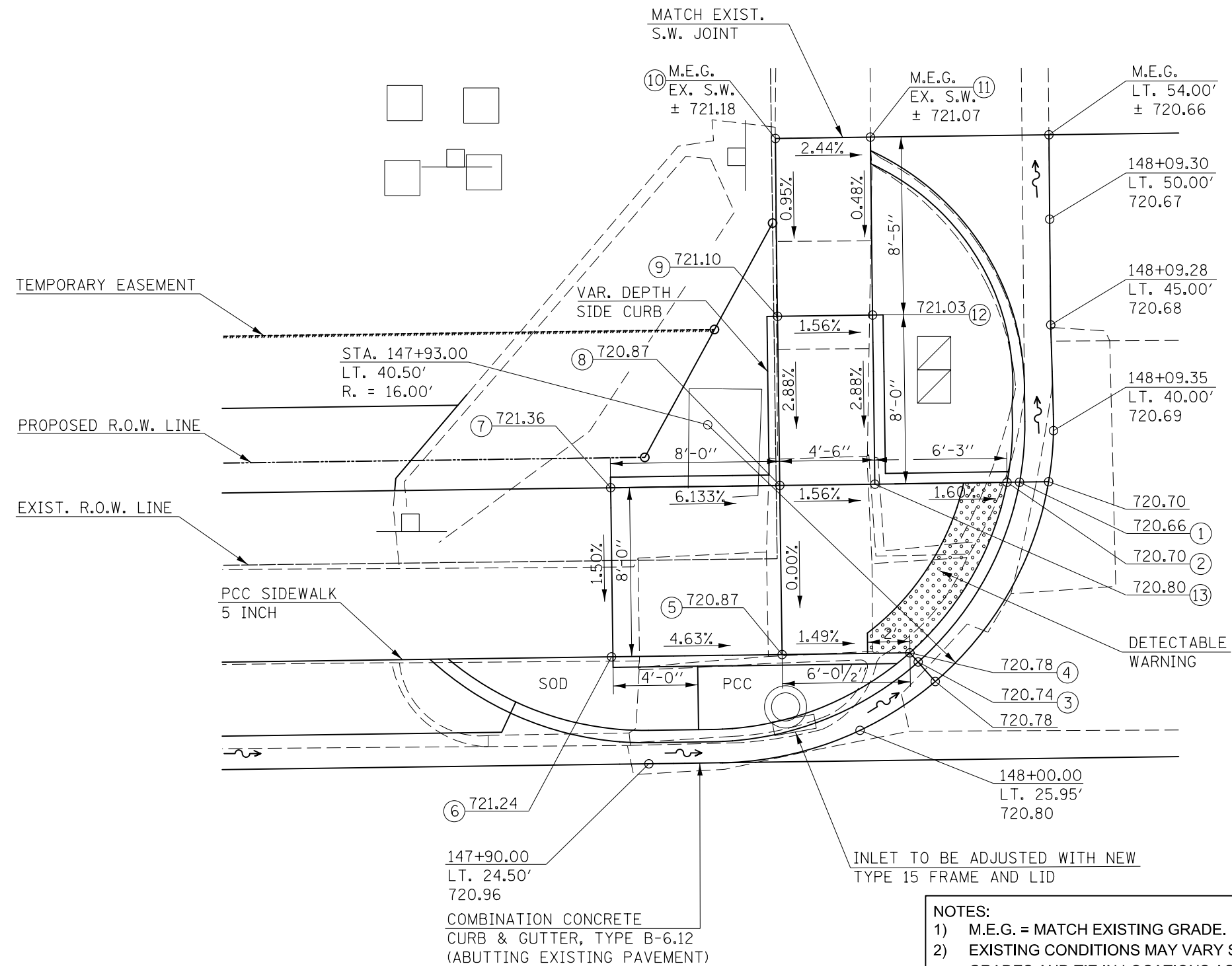
LOCATION #40

N.W. QUADRANT
U.S. 150 & LAKE ST.



LOCATION #40 - N.W. QUADRANT
U.S. 150 & LAKE ST.

LOCATION	STATION	OFFSET
1	148+07.71	LT. 37.58
2	148+07.12	LT. 37.58
3	148+02.79	LT. 29.14
4	148+02.41	LT. 29.58
5	147+96.37	LT. 29.58
6	147+88.37	LT. 29.58
7	147+88.37	LT. 37.58
8	147+96.37	LT. 37.58
9	147+96.37	LT. 45.58
10	147+96.37	LT. 54.00
11	148+00.87	LT. 54.00
12	148+00.87	LT. 45.58
13	148+00.87	LT. 37.58



U.S. RTE. 150
(UNIVERSITY AVE.)

LAKE STREET

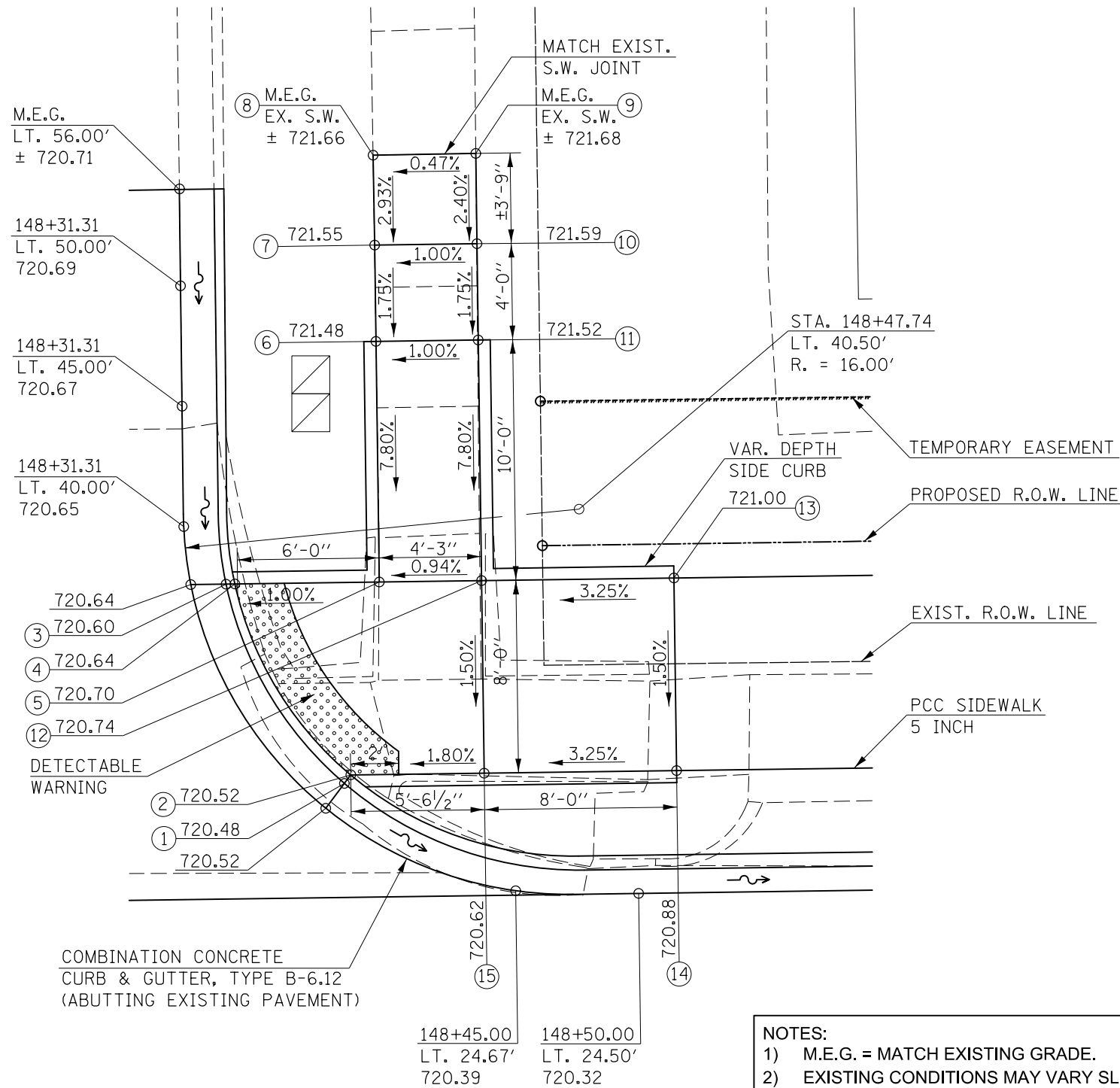
- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #41

N.E. QUADRANT
U.S. 150 & LAKE ST.



LAKE STREET



LOCATION #41 - N.W. QUADRANT
U.S. 150 & LAKE ST.

LOCATION	STATION	OFFSET
1	148+37.83	LT. 29.24
2	148+38.10	LT. 29.58
3	148+33.03	LT. 37.58
4	148+33.40	LT. 37.58
5	148+39.40	LT. 37.58
6	148+39.40	LT. 47.58
7	148+39.40	LT. 51.58
8	148+39.38	LT. 55.32
9	148+43.65	LT. 55.33
10	148+43.65	LT. 51.58
11	148+43.65	LT. 47.58
12	148+43.65	LT. 37.58
13	148+51.65	LT. 37.58
14	148+51.65	LT. 29.58
15	148+43.65	LT. 29.58

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\0579\0853-shr-ramps.dgn		REVISIONS -	REVISIONS -
	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISIONS -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

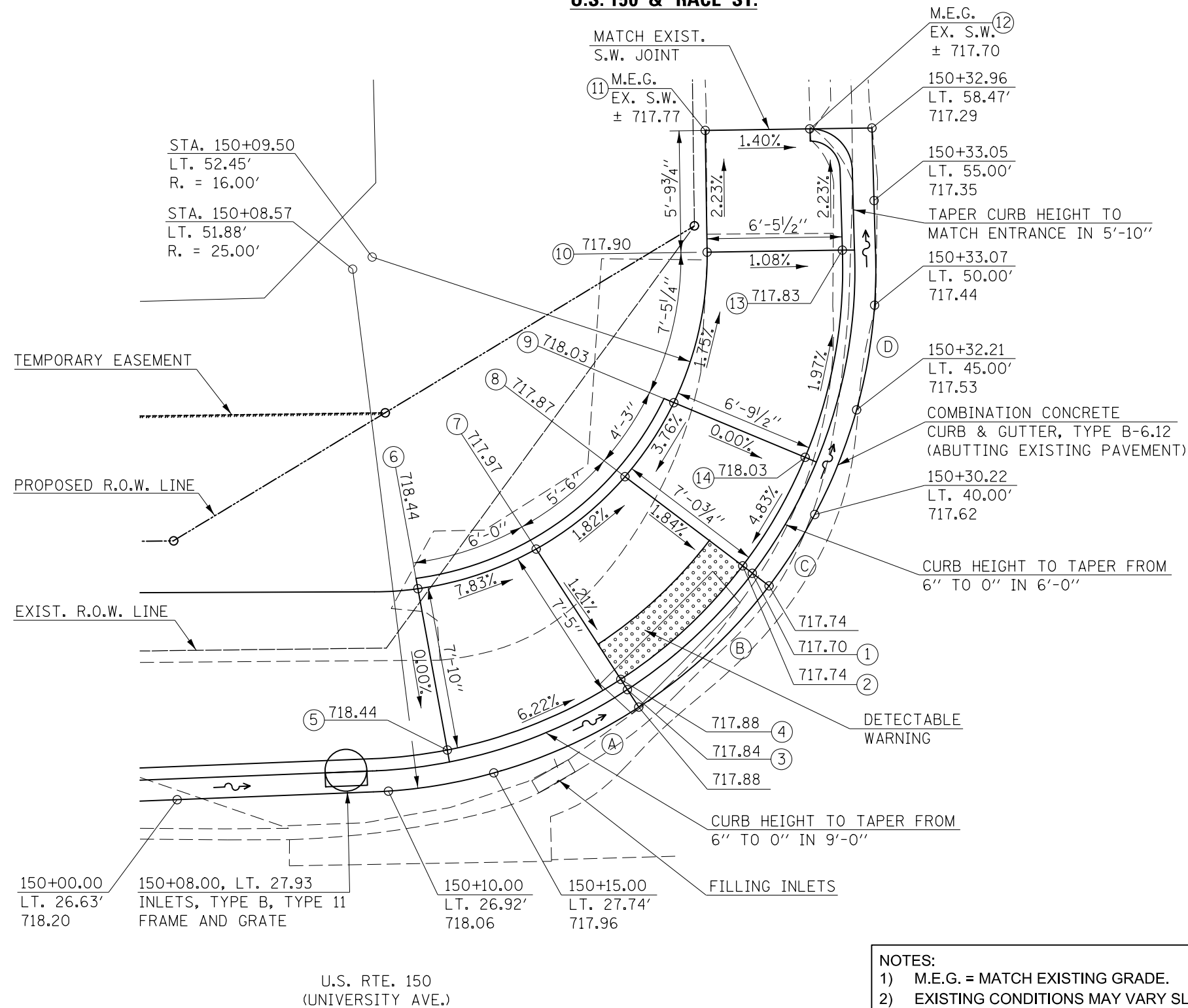
LAKE STREET
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	182
CONTRACT NO. 70B53			ILLINOIS FED. AID PROJECT	

LOCATION #42

N.W. QUADRANT
U.S. 150 & RACE ST.



LOCATION #42 - N.W. QUADRANT
U.S. 150 & RACE ST.

LOCATION	STATION	OFFSET
1	150+27.27	LT. 37.21
2	150+26.82	LT. 37.56
3	150+21.34	LT. 31.68
4	150+21.03	LT. 32.17
5	150+12.81	LT. 28.86
6	150+11.49	LT. 36.58
7	150+17.08	LT. 38.43
8	150+21.29	LT. 41.85
9	150+23.61	LT. 45.38
10	150+25.21	LT. 52.56
11	150+25.15	LT. 58.38
12	150+30.06	LT. 58.43
13	150+31.56	LT. 52.63
14	150+29.78	LT. 42.73

LOCATION	POINT TO POINT	LENGTH
A	PT. 4 TO PT. 5	9' - 0"
B	PT. 2 TO PT. 4	8' - 0"
C	PT. 2 TO PT. 14	6' - 0"
D	PT. 13 TO PT. 14	10' - 1 3/4"

NOTE:
SEE CURB & GUTTER PLANS SHEETS
FOR STORM SEWER IMPROVEMENTS.

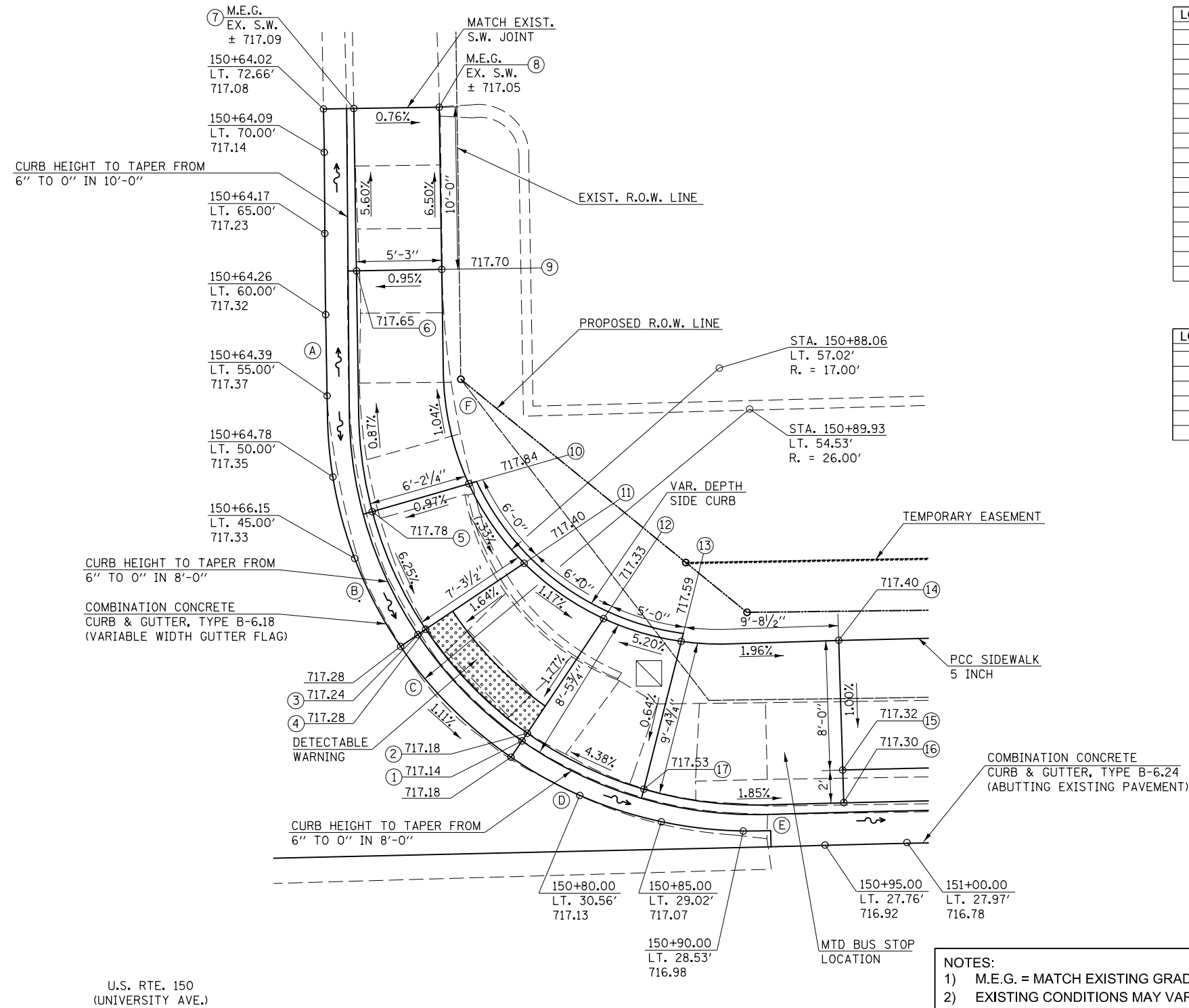
- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #43

N.E. QUADRANT U.S. 150 & RACE ST.



RACE STREET



LOCATION #43 - N.E. QUADRANT
U.S. 150 & RACE ST.

LOCATION	STATION	OFFSET
1	150+76.44	LT. 33.86
2	150+76.76	LT. 34.35
3	150+70.03	LT. 40.34
4	150+70.51	LT. 40.67
5	150+67.19	LT. 47.89
6	150+66.09	LT. 67.72
7	150+65.85	LT. 72.71
8	150+70.97	LT. 72.85
9	150+71.23	LT. 62.85
10	150+73.01	LT. 49.67
11	150+76.43	LT. 44.82
12	150+81.30	LT. 41.47
13	150+86.03	LT. 40.14
14	150+95.59	LT. 40.39
15	150+96.00	LT. 32.40
16	150+96.10	LT. 30.40
17	150+83.90	LT. 31.00

LOCATION	POINT TO POINT	LENGTH
A	PT. 5 TO PT. 6	14' - 11"
B	PT. 4 TO PT. 5	8' - 0"
C	PT. 2 TO PT. 4	9' - 0"
D	PT. 2 TO PT. 17	8' - 0"
E	PT. 16 TO PT. 17	12' - 5"
F	PT. 9 TO PT. 10	13' - 5 1/4"

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #44

S.W. QUADRANT U.S. 150 & RACE ST.

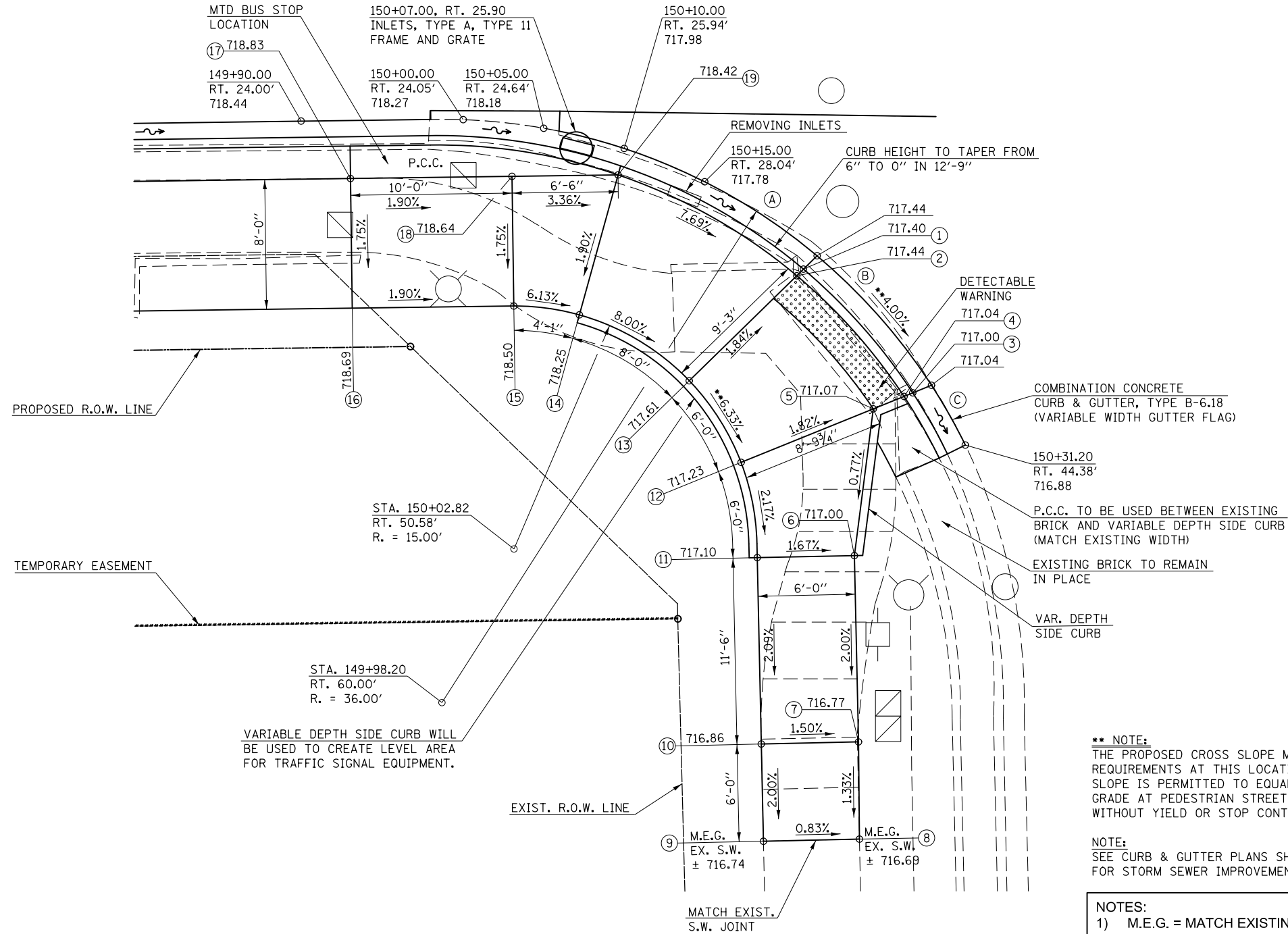


U.S. RTE. 150
(UNIVERSITY AVE.)

LOCATION #44 - S.W. QUADRANT
U.S. 150 & RACE ST.

LOCATION	STATION	OFFSET
1	150+21.12	RT. 33.48
2	150+20.70	RT. 33.88
3	150+27.91	RT. 41.15
4	150+27.37	RT. 41.36
5	150+25.44	RT. 42.12
6	150+24.21	RT. 51.17
7	150+24.40	RT. 62.68
8	150+24.43	RT. 68.68
9	150+18.36	RT. 68.77
10	150+18.27	RT. 62.77
11	150+18.10	RT. 51.27
12	150+17.13	RT. 45.38
13	150+13.93	RT. 40.31
14	150+07.09	RT. 36.18
15	150+03.01	RT. 35.58
16	149+93.00	RT. 35.58
17	149+93.00	RT. 27.58
18	150+03.00	RT. 27.58
19	150+09.61	RT. 27.57

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 19	12' - 9"
B	PT. 2 TO PT. 4	10' - 0"
C	PT. 4 TO C&G JOINT	4' - 4 1/2"



**** NOTE:**
THE PROPOSED CROSS SLOPE MEETS PROWAG REQUIREMENTS AT THIS LOCATION. THE CROSS SLOPE IS PERMITTED TO EQUAL THE STREET GRADE AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL.

NOTE:
SEE CURB & GUTTER PLANS SHEETS FOR STORM SEWER IMPROVEMENTS.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #45

S.E. QUADRANT
U.S. 150 & RACE ST.

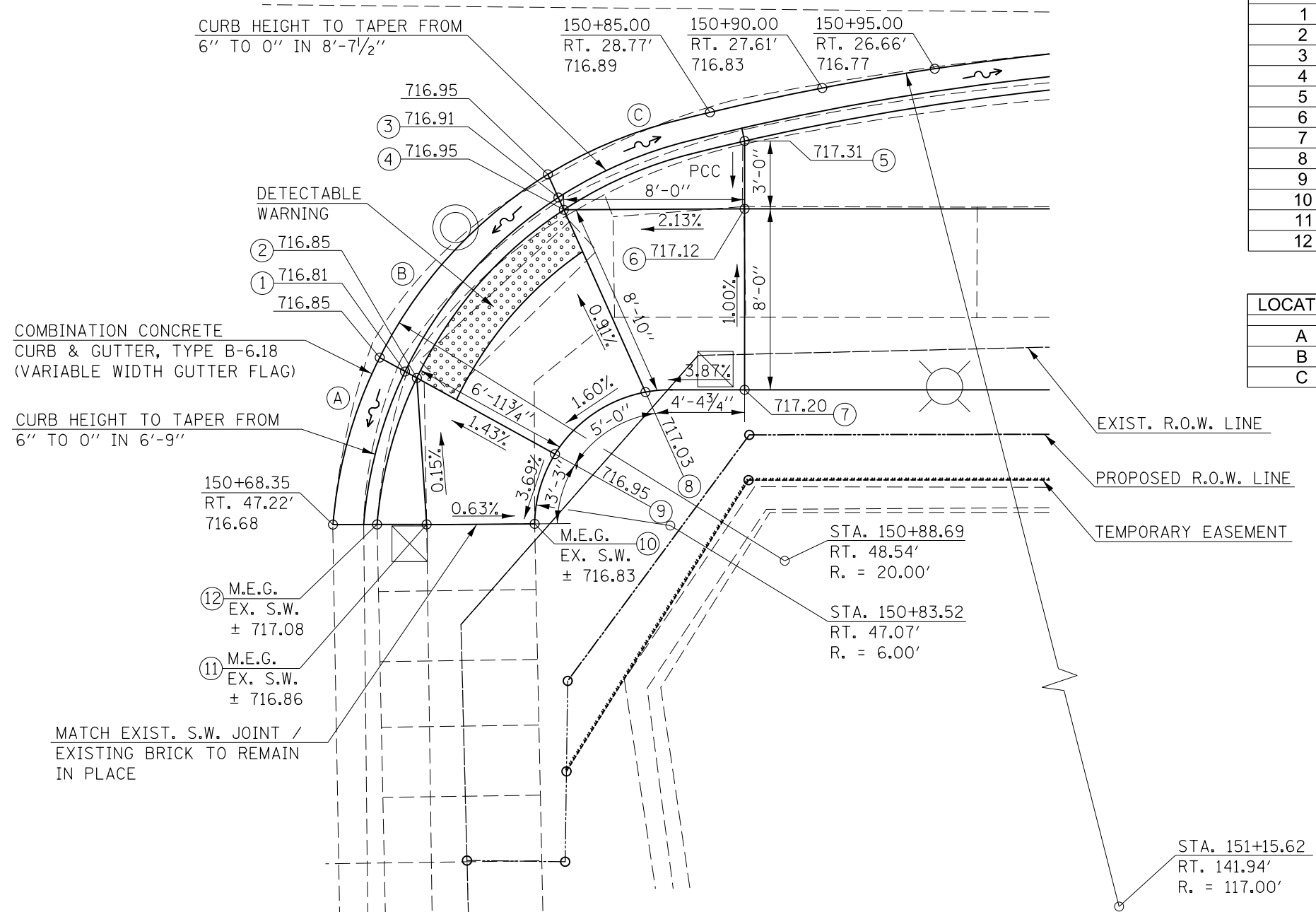
U.S. RTE. 150
(UNIVERSITY AVE.)



LOCATION #45 - S.E. QUADRANT
U.S. 150 & RACE ST.

LOCATION	STATION	OFFSET
1	150+71.51	RT. 40.42
2	150+72.04	RT. 40.70
3	150+78.28	RT. 32.64
4	150+78.53	RT. 33.18
5	150+86.57	RT. 30.01
6	150+86.62	RT. 33.01
7	150+86.76	RT. 41.01
8	150+82.31	RT. 41.18
9	150+78.28	RT. 43.98
10	150+77.42	RT. 47.08
11	150+72.56	RT. 47.16
12	150+70.33	RT. 47.19

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 12	6' - 9"
B	PT. 2 TO PT. 4	10' - 0"
C	PT. 4 TO PT. 5	8' - 7 1/2"



- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

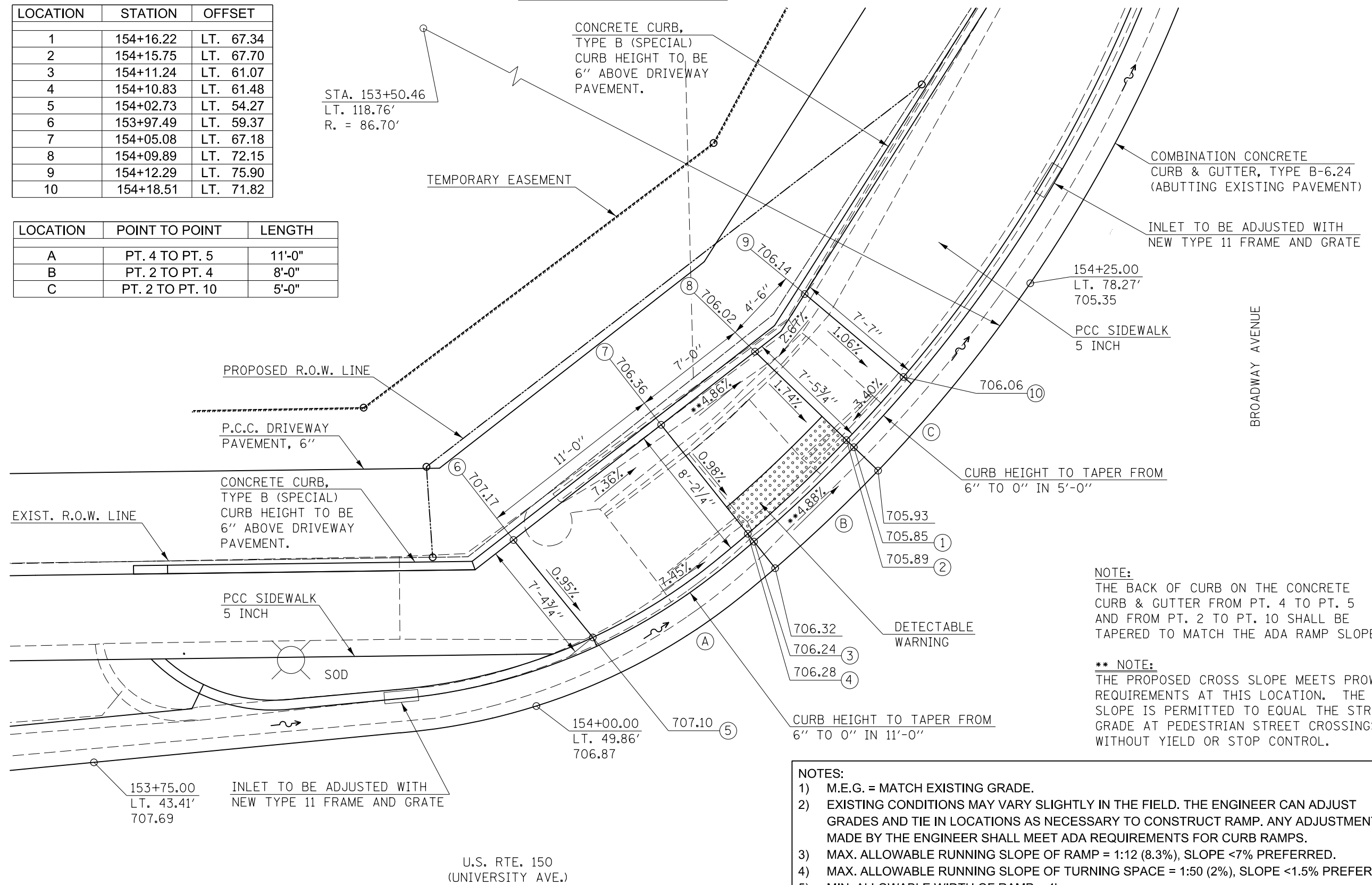
LOCATION #46

N.W. QUADRANT
U.S. 150 & BROADWAY AVE.

LOCATION #46 - N.W. QUADRANT
U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	154+16.22	LT. 67.34
2	154+15.75	LT. 67.70
3	154+11.24	LT. 61.07
4	154+10.83	LT. 61.48
5	154+02.73	LT. 54.27
6	153+97.49	LT. 59.37
7	154+05.08	LT. 67.18
8	154+09.89	LT. 72.15
9	154+12.29	LT. 75.90
10	154+18.51	LT. 71.82

LOCATION	POINT TO POINT	LENGTH
A	PT. 4 TO PT. 5	11'-0"
B	PT. 2 TO PT. 4	8'-0"
C	PT. 2 TO PT. 10	5'-0"



NOTE:
THE BACK OF CURB ON THE CONCRETE CURB & GUTTER FROM PT. 4 TO PT. 5 AND FROM PT. 2 TO PT. 10 SHALL BE TAPERED TO MATCH THE ADA RAMP SLOPE.

**** NOTE:**
THE PROPOSED CROSS SLOPE MEETS PROWAG REQUIREMENTS AT THIS LOCATION. THE CROSS SLOPE IS PERMITTED TO EQUAL THE STREET GRADE AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

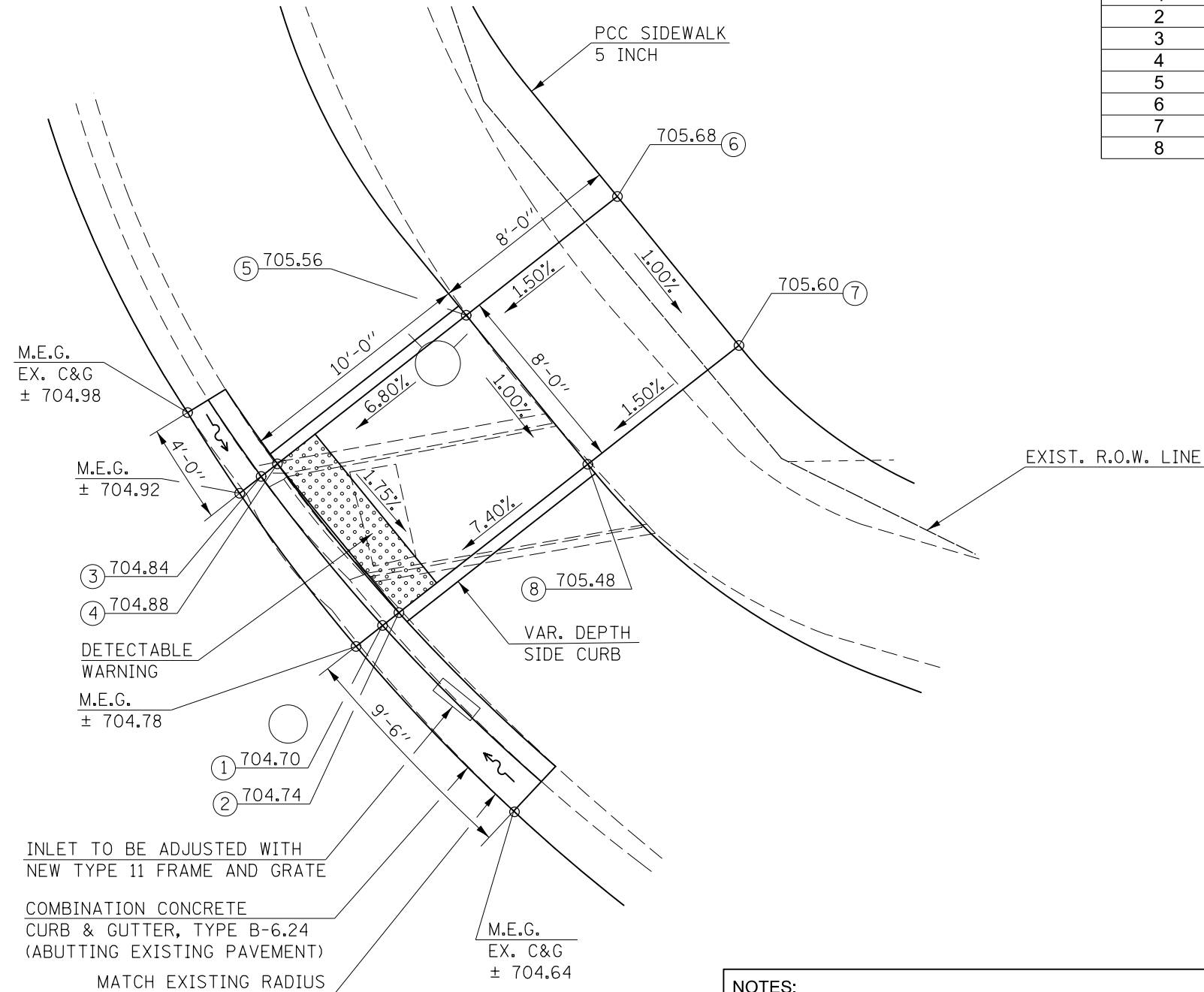
LOCATION #47
N.E. QUADRANT
U.S. 150 & BROADWAY AVE.



LOCATION #47 - N.E. QUADRANT
 U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	155+10.50	LT. 69.96
2	155+11.10	LT. 70.60
3	155+04.55	LT. 75.26
4	155+05.11	LT. 75.89
5	155+11.88	LT. 83.27
6	155+17.46	LT. 89.16
7	155+23.63	LT. 83.84
8	155+18.05	LT. 77.96

BROADWAY AVENUE



U.S. RTE. 150
 (UNIVERSITY AVE.)

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BROADWAY AVENUE ADA RAMP DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\057\DRAWING\0570853-shr-ramps.dgn	DESIGNED -	REVISED -	808			D5 HSIP 2016-1	CHAMPAIGN	499	188	
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70B53				
	PLOT DATE = 1/28/2019	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

SCALE: 1" = 3' SHEET 2 OF 7 SHEETS STA. TO STA.

LOCATION #48

S.W. QUADRANT

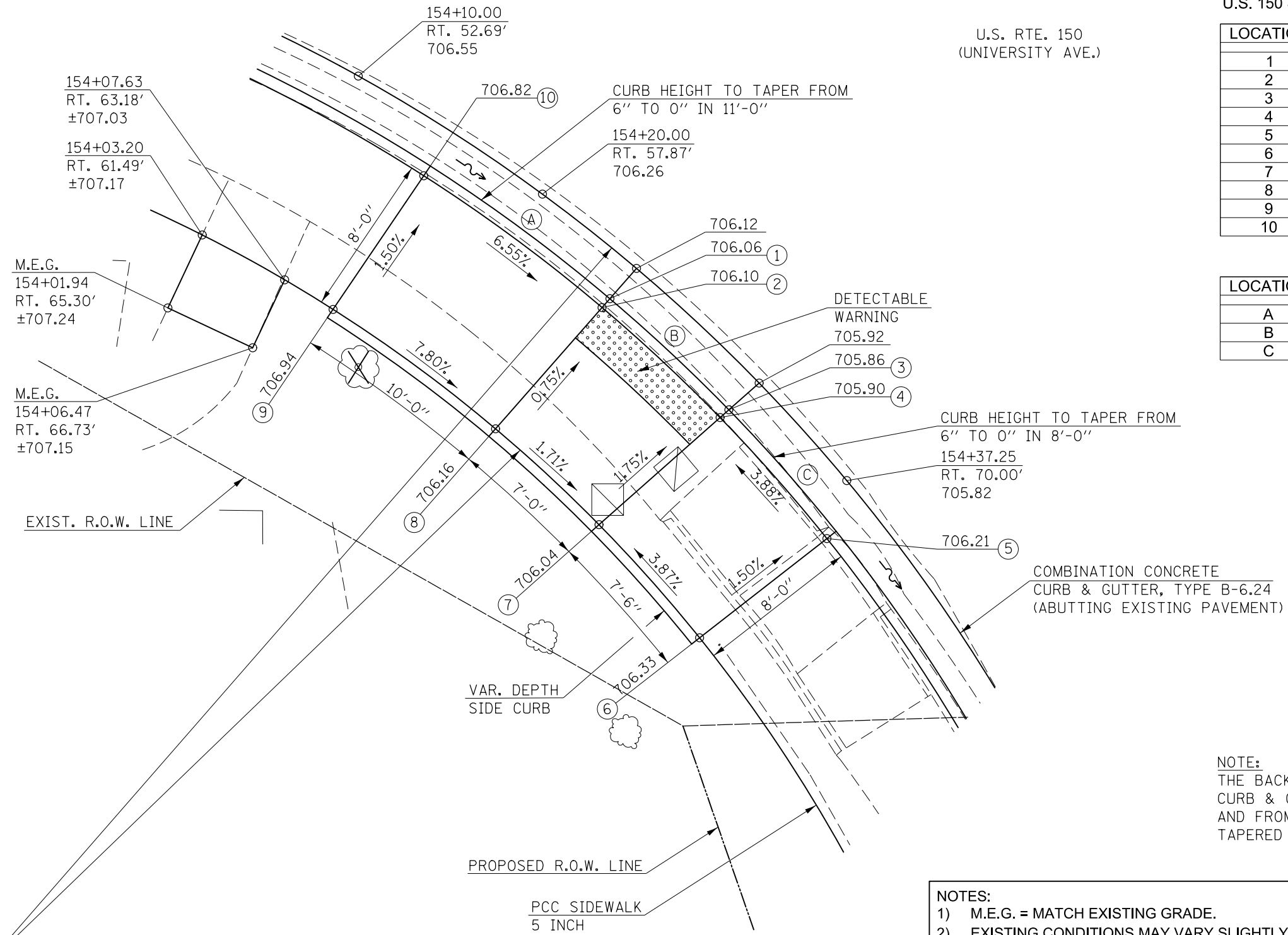
U.S. 150 & BROADWAY AVE.



LOCATION #48 - S.W. QUADRANT
U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	154+24.09	RT. 62.00
2	154+23.76	RT. 62.48
3	154+30.83	RT. 66.65
4	154+30.44	RT. 67.10
5	154+36.66	RT. 72.31
6	154+30.94	RT. 78.05
7	154+25.09	RT. 73.17
8	154+19.22	RT. 69.14
9	154+13.01	RT. 65.65
10	154+16.68	RT. 58.50

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 10	11'-0"
B	PT. 2 TO PT. 4	8'-0"
C	PT. 4 TO PT. 5	8'-0"



NOTE:
THE BACK OF CURB ON THE CONCRETE CURB & GUTTER FROM PT. 4 TO PT. 5 AND FROM PT. 2 TO PT. 10 SHALL BE TAPERED TO MATCH THE ADA RAMP SLOPE.

- NOTES:
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

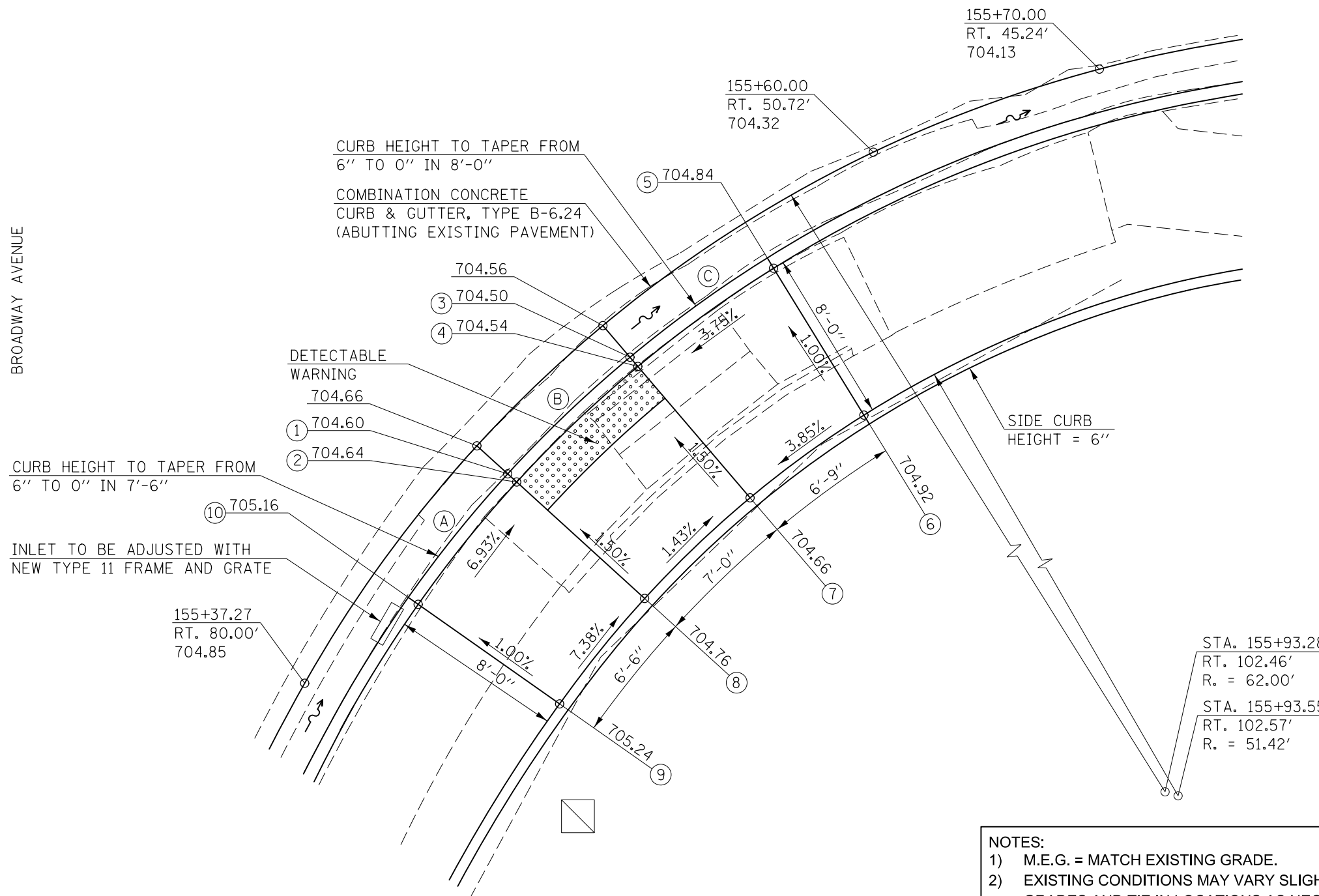
STA. 153+76.97
RT. 132.00'
R. = 85.50' C&G
R. = 74.92' S.W.

LOCATION #49
S.E. QUADRANT
U.S. 150 & BROADWAY AVE.



U.S. RTE. 150
 (UNIVERSITY AVE.)

BROADWAY AVENUE



LOCATION #49 - S.E. QUADRANT
 U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	155+45.15	RT. 68.64
2	155+45.62	RT. 68.97
3	155+50.00	RT. 62.24
4	155+50.43	RT. 62.62
5	155+56.06	RT. 57.00
6	155+61.26	RT. 63.34
7	155+56.55	RT. 68.05
8	155+52.36	RT. 73.58
9	155+49.16	RT. 79.18
10	155+41.93	RT. 75.48

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 10	7'-6"
B	PT. 2 TO PT. 4	8'-0"
C	PT. 4 TO PT. 5	8'-0"

NOTE:
 THE BACK OF CURB ON THE CONCRETE CURB & GUTTER FROM PT. 4 TO PT. 5 AND FROM PT. 2 TO PT. 10 SHALL BE TAPERED TO MATCH THE ADA RAMP SLOPE.

NOTE:
 SEE CURB & GUTTER PLAN SHEETS FOR CURB & GUTTER RADIUS ON EACH SIDE OF ADA PEDESTRIAN RAMP.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570853-shr-ramps.dgn		REVISIONS	REVISED -
	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 1/28/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BROADWAY AVENUE
ADA RAMP DETAILS

SCALE: 1" = 3' SHEET 4 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
808	D5 HSIP 2016-1	CHAMPAIGN	499	190
			CONTRACT NO. 70B53	
ILLINOIS FED. AID PROJECT				



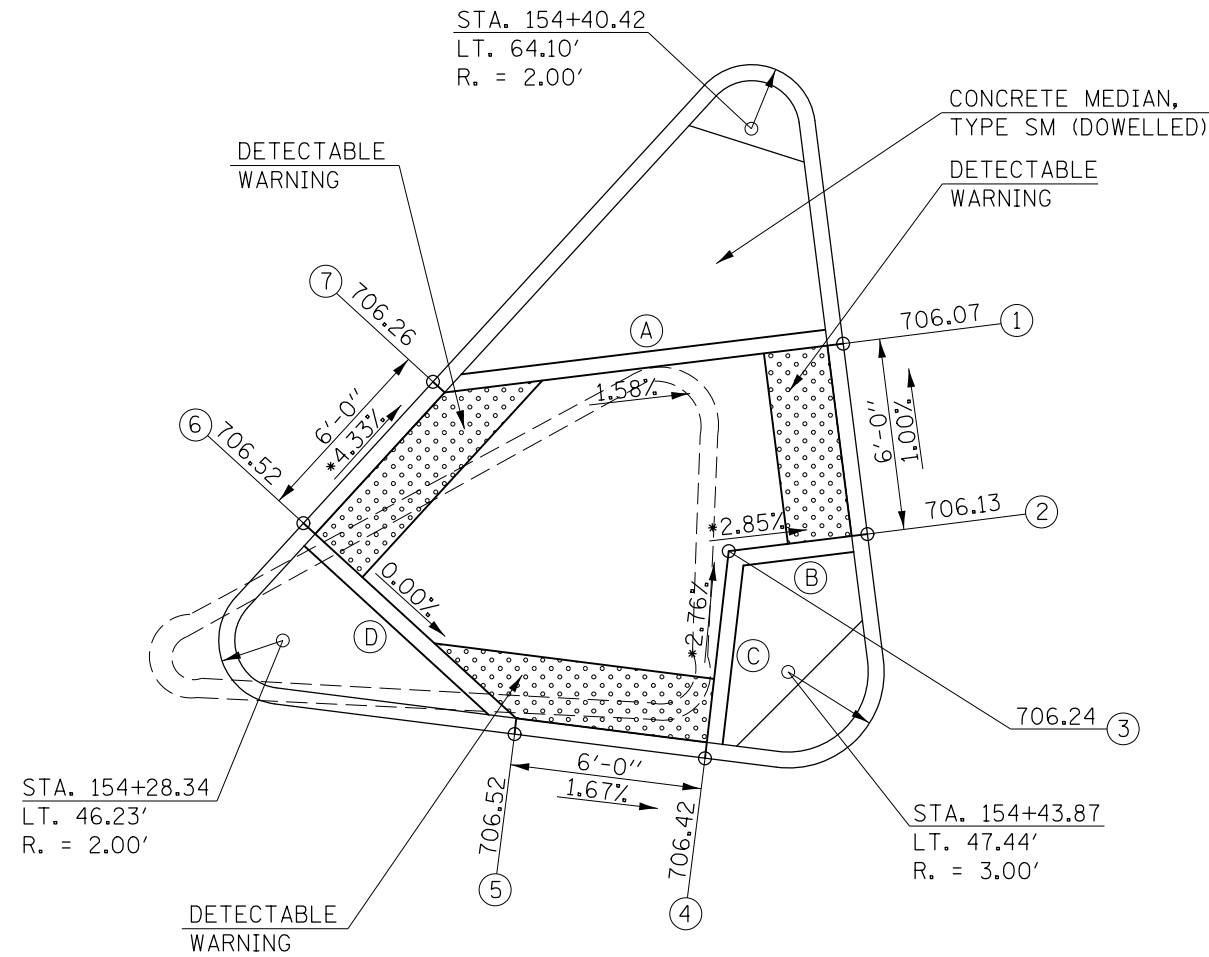
LOCATION #50
N.W. QUADRANT (ISLAND)
U.S. 150 & BROADWAY AVE.

LOCATION #50 - N.W. QUADRANT (ISLAND)
 U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	154+44.13	LT. 57.84
2	154+45.69	LT. 52.06
3	154+41.54	LT. 50.92
4	154+41.71	LT. 44.41
5	154+35.80	LT. 44.32
6	154+28.48	LT. 49.95
7	154+31.83	LT. 54.89

LOCATION	POINT TO POINT	LENGTH
A	PT. 1 TO PT. 7	12' - 0 1/2"
B	PT. 2 TO PT. 3	3' - 10 1/2"
C	PT. 3 TO PT. 4	6' - 6 1/4"
D	PT. 5 TO PT. 6	9' - 0 1/4"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS



*** NOTE:**
 THE CROSS SLOPE GREATER THAN 2% IS ACCEPTABLE AT THIS LOCATION PER (PROWAG) REQUIREMENTS. THE CROSS SLOPE CAN MATCH THE EXISTING PAVEMENT WHEN THERE IS NO STOP CONTROL AND THE EXISTING PAVEMENT SLOPE IS LESS THAN 5%.

NOTE:
 SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE PROPOSED ISLANDS.

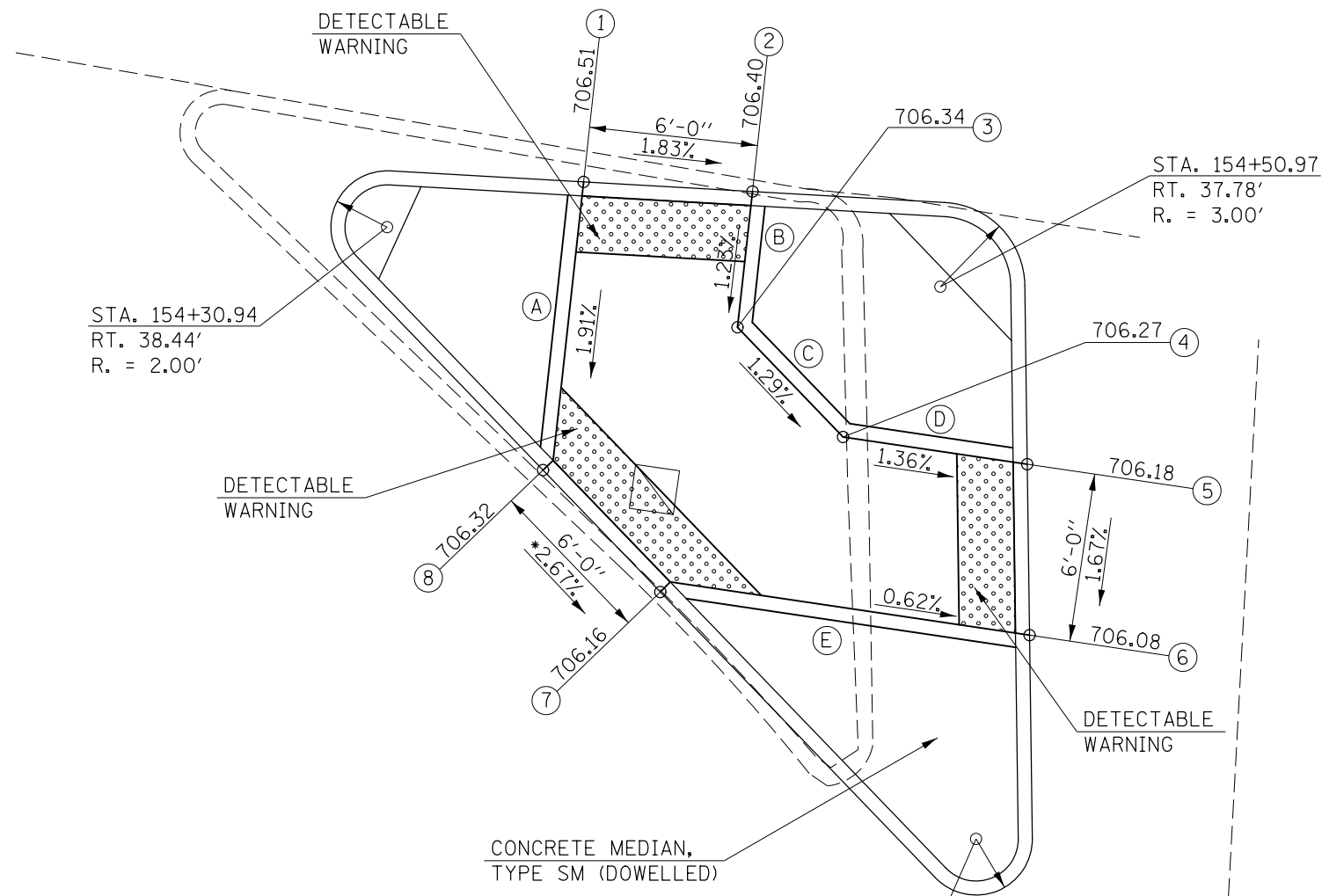
CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CURB & GUTTER SHALL BE REMOVED AND WILL BE PAID FOR AS COMBINATION CURB & GUTTER REMOVAL.
- 3) THE CONCRETE MEDIAN SURFACE 4" SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN SURFACE REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 4) THE EXISTING PAVEMENT UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 5) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT THE SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 6) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #51
S.W. QUADRANT (ISLAND)
U.S. 150 & BROADWAY AVE.



LOCATION #51 - S.W. QUADRANT (ISLAND)
 U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	154+37.72	RT. 35.89
2	154+43.79	RT. 35.38
3	154+43.95	RT. 40.24
4	154+48.27	RT. 43.56
5	154+54.99	RT. 43.58
6	154+55.97	RT. 49.58
7	154+42.53	RT. 49.94
8	154+37.71	RT. 46.23

LOCATION	POINT TO POINT	LENGTH
A	PT. 1 TO PT. 8	9' - 11 1/2"
B	PT. 2 TO PT. 3	4' - 10 1/4"
C	PT. 3 TO PT. 4	5' - 4 3/4"
D	PT. 4 TO PT. 5	6' - 7 1/2"
E	PT. 6 TO PT. 7	12' - 10 3/4"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS

*** NOTE:**
 THE CROSS SLOPE GREATER THAN 2% IS ACCEPTABLE AT THIS LOCATION PER (PROWAG) REQUIREMENTS. THE CROSS SLOPE CAN MATCH THE EXISTING PAVEMENT SLOPE WHEN THERE IS NO STOP CONTROL AND THE EXISTING PAVEMENT SLOPE IS LESS THAN 5%.

NOTE:
 SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE PROPOSED ISLANDS.

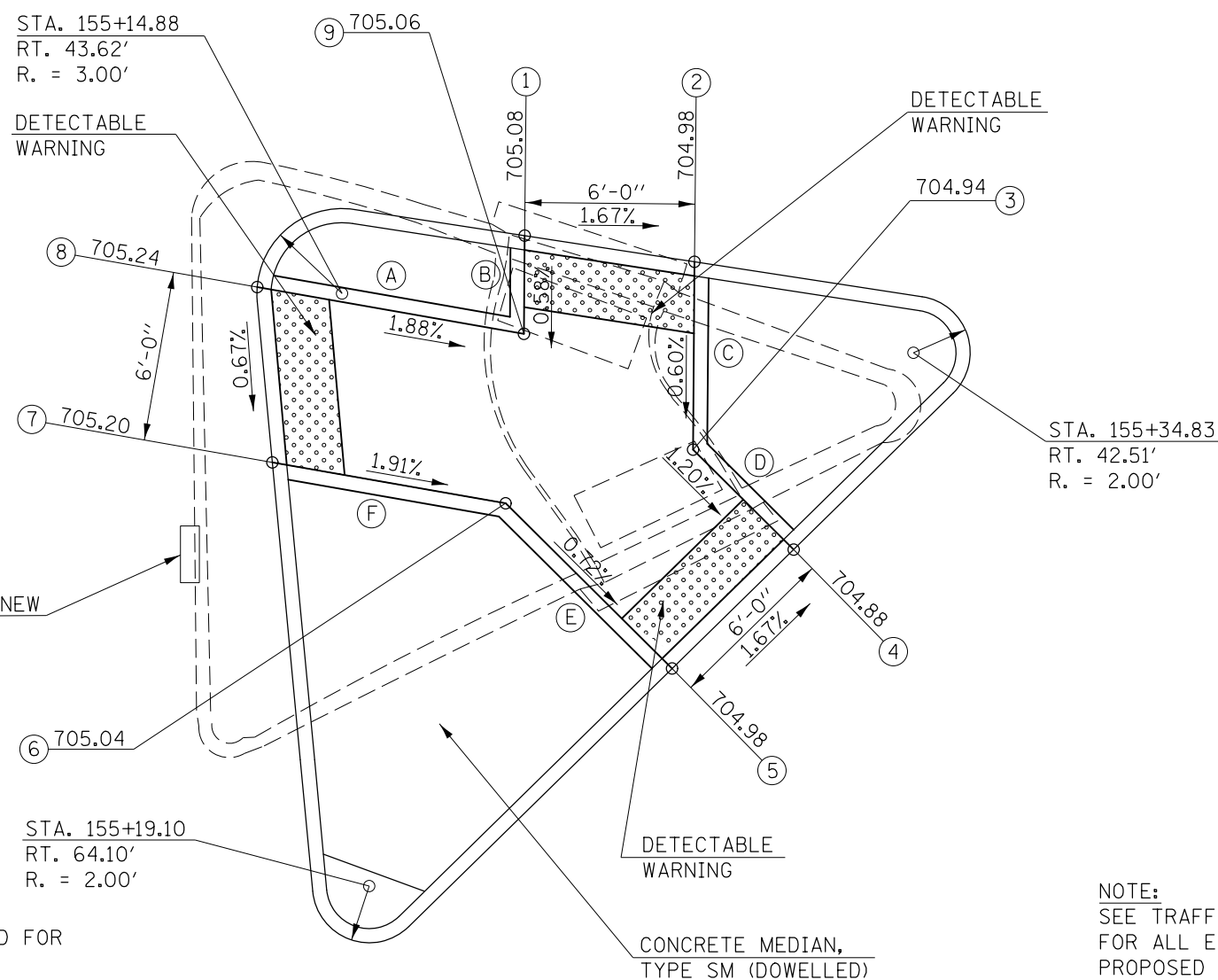
CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CURB & GUTTER SHALL BE REMOVED AND WILL BE PAID FOR AS COMBINATION CURB & GUTTER REMOVAL.
- 3) THE CONCRETE MEDIAN SURFACE 4" SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN SURFACE REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 4) THE EXISTING PAVEMENT UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 5) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT THE SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 6) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #52
S.E. QUADRANT (ISLAND)
U.S. 150 & BROADWAY AVE.



LOCATION #52 - S.E. QUADRANT (ISLAND)
 U.S. 150 & BROADWAY AVE.

LOCATION	STATION	OFFSET
1	155+20.84	RT. 40.57
2	155+26.81	RT. 40.54
3	155+27.79	RT. 47.10
4	155+31.77	RT. 50.02
5	155+28.26	RT. 54.84
6	155+21.66	RT. 50.00
7	155+13.45	RT. 49.90
8	155+11.94	RT. 43.88

LOCATION	POINT TO POINT	LENGTH
A	PT. 8 TO PT. 9	9' - 6 3/4"
B	PT. 1 TO PT. 9	3' - 5 1/2"
C	PT. 2 TO PT. 3	6' - 7 1/2"
D	PT. 3 TO PT. 4	5' - 0"
E	PT. 5 TO PT. 6	8' - 3 1/4"
F	PT. 6 TO PT. 7	8' - 4 1/4"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS

CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CURB & GUTTER SHALL BE REMOVED AND WILL BE PAID FOR AS COMBINATION CURB & GUTTER REMOVAL.
- 3) THE CONCRETE MEDIAN SURFACE 4" SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN SURFACE REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 4) THE EXISTING SIDEWALK SHALL BE REMOVED AND WILL BE PAID FOR AS SIDEWALK REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL THE MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 5) THE EXISTING PAVEMENT UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT.
- 6) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT THE SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 7) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

NOTE:

SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE PROPOSED ISLANDS.

NOTE:

SEE CURB & GUTTER PLANS SHEETS FOR STORM SEWER IMPROVEMENTS.

NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

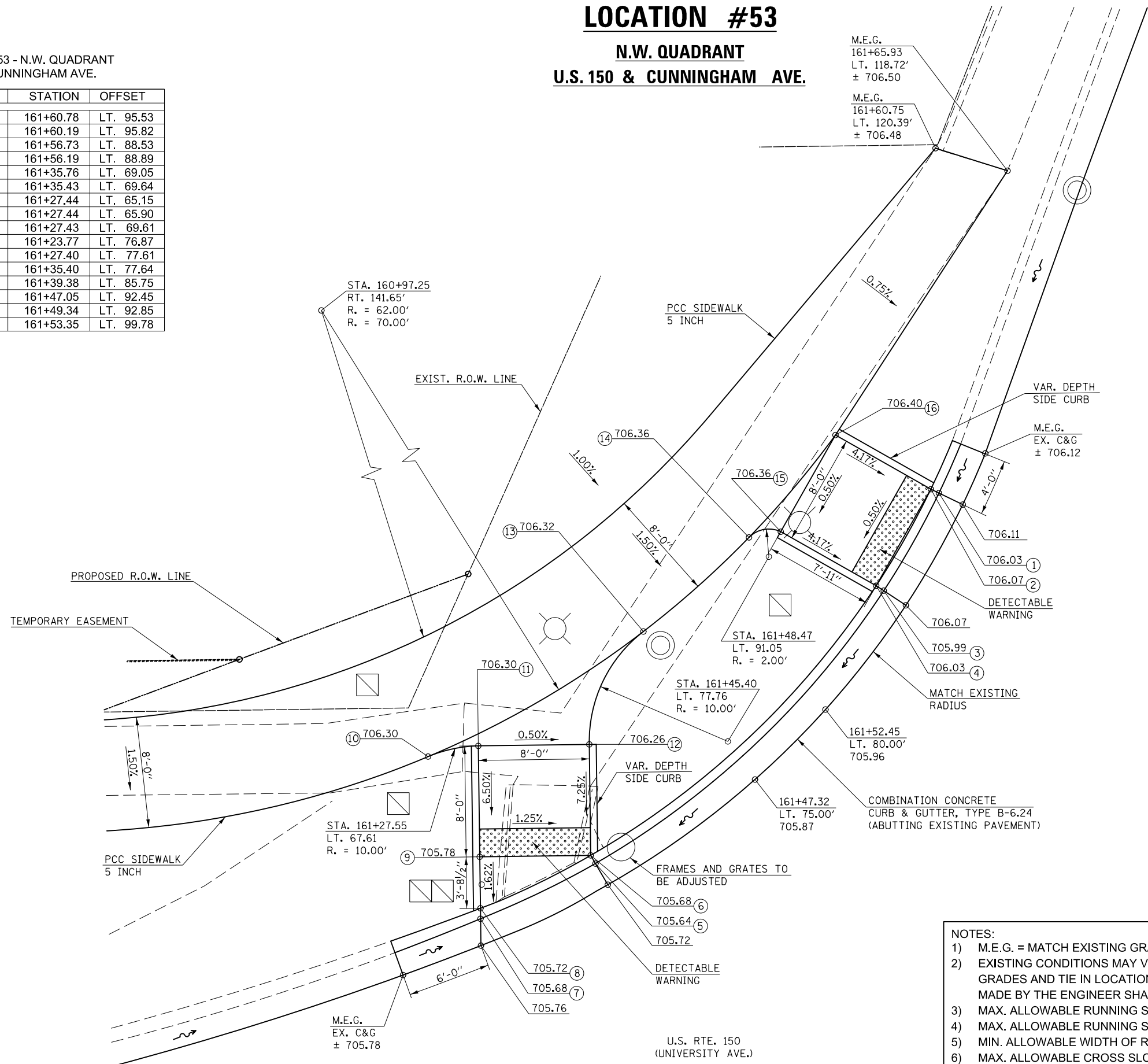
LOCATION #53

N.W. QUADRANT

U.S. 150 & CUNNINGHAM AVE.

LOCATION #53 - N.W. QUADRANT
U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	161+60.78	LT. 95.53
2	161+60.19	LT. 95.82
3	161+56.73	LT. 88.53
4	161+56.19	LT. 88.89
5	161+35.76	LT. 69.05
6	161+35.43	LT. 69.64
7	161+27.44	LT. 65.15
8	161+27.44	LT. 65.90
9	161+27.43	LT. 69.61
10	161+23.77	LT. 76.87
11	161+27.40	LT. 77.61
12	161+35.40	LT. 77.64
13	161+39.38	LT. 85.75
14	161+47.05	LT. 92.45
15	161+49.34	LT. 92.85
16	161+53.35	LT. 99.78



CUNNINGHAM AVENUE

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMP.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #54

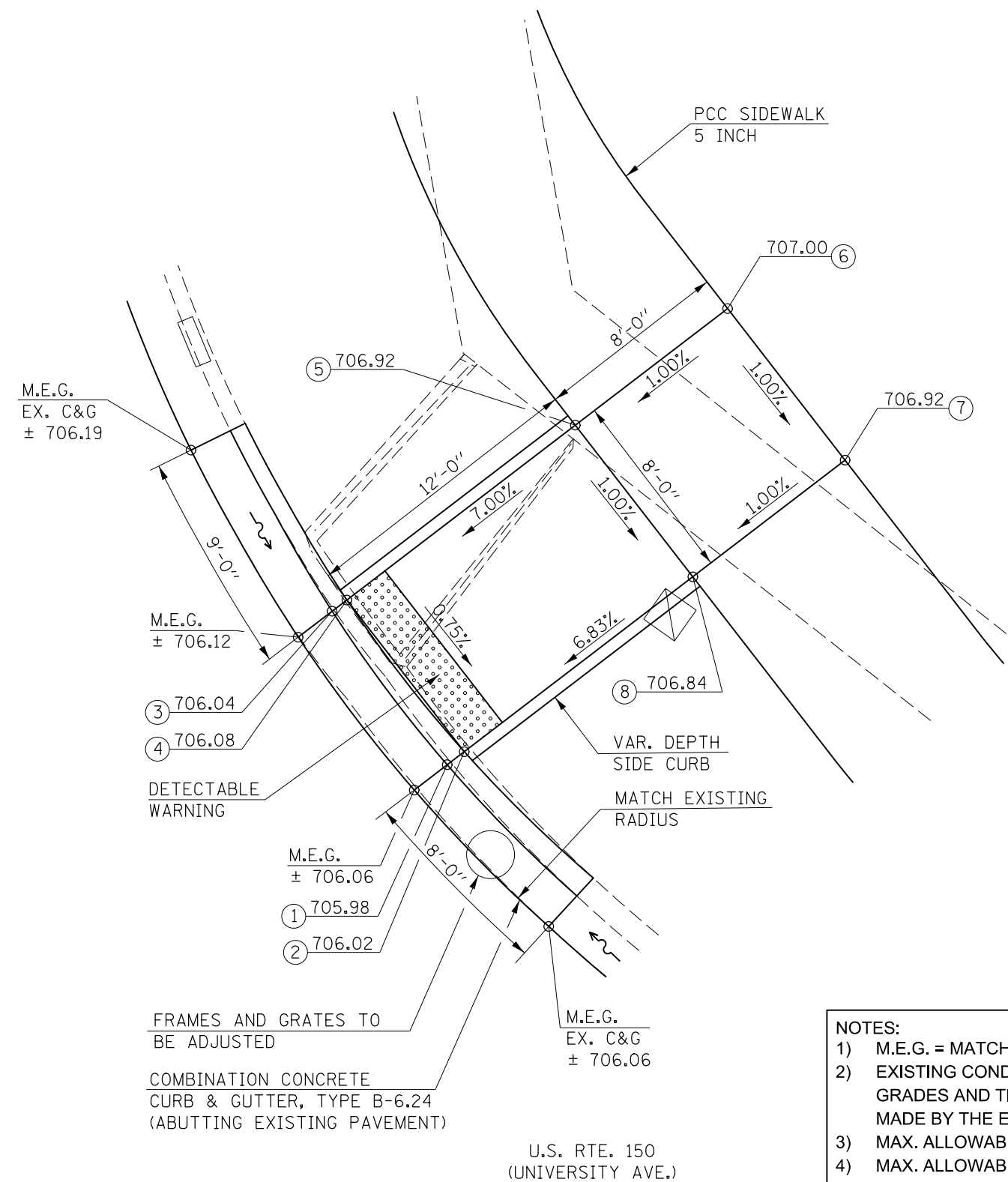
N.E. QUADRANT

U.S. 150 & CUNNINGHAM AVE.



LOCATION #54 - N.E. QUADRANT
U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	162+91.25	LT. 76.16
2	162+91.97	LT. 76.70
3	162+86.51	LT. 82.61
4	162+87.12	LT. 83.07
5	162+96.72	LT. 90.28
6	163+03.12	LT. 95.08
7	163+07.96	LT. 88.71
8	163+01.56	LT. 83.91



NOTES:

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

FILE NAME =	USER NAME = eaglinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CUNNINGHAM AVENUE ADA RAMP DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0577\Drawings\Design\0570853-shr-ramps.dgn		DRAWN	REVISED -		808	D5 HSIP 2016-1	CHAMPAIGN	499	195			
\$MODELNAME\$	PLOT SCALE = 6.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70B53							
	PLOT DATE = 1/28/2019	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

SCALE: 1" = 3' SHEET 2 OF 7 SHEETS STA. TO STA.

LOCATION #55

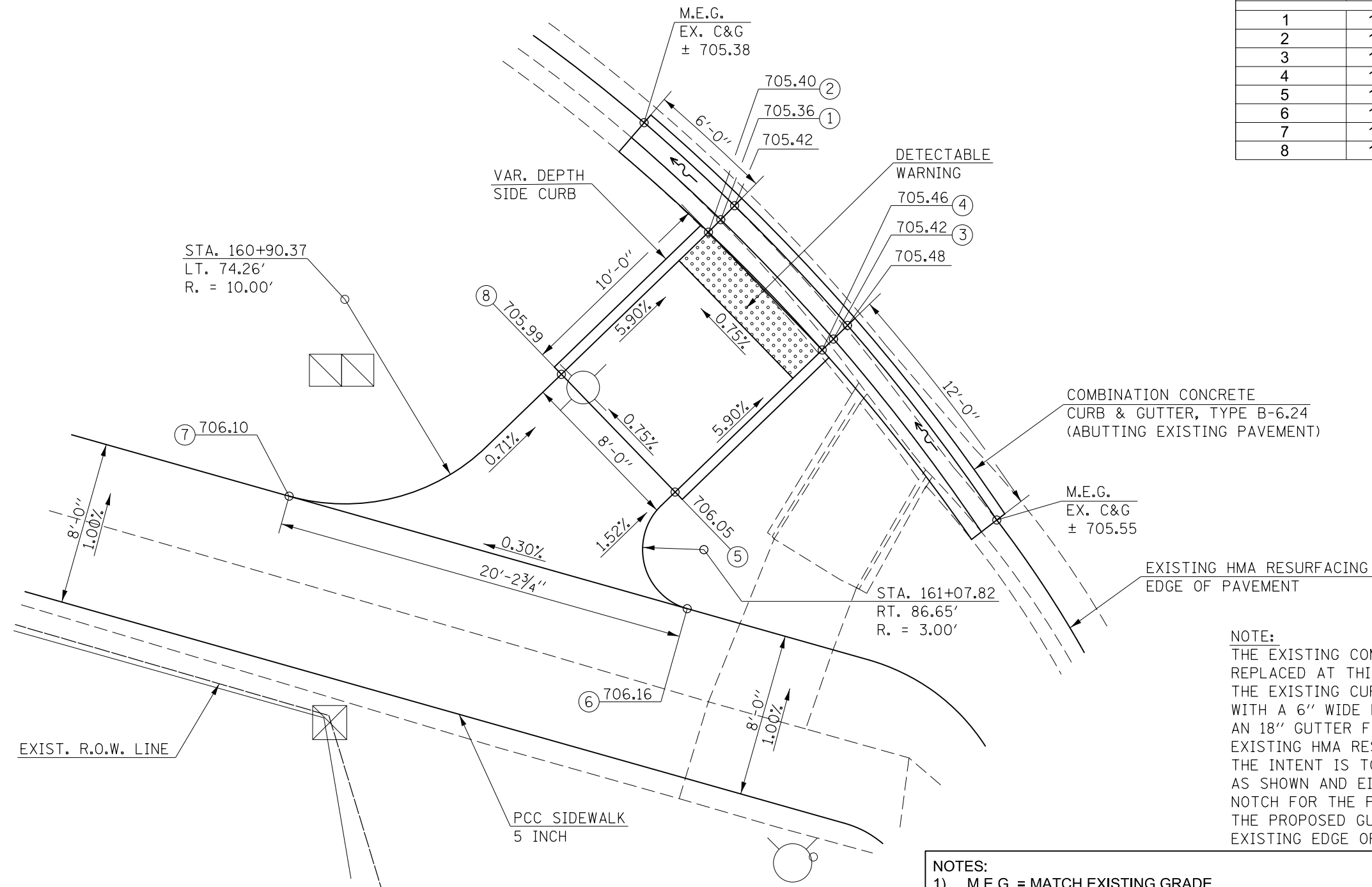
S.W. QUADRANT
U.S. 150 & CUNNINGHAM AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)



LOCATION #55 - S.W. QUADRANT
U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	161+08.79	RT. 70.53
2	161+08.18	RT. 71.14
3	161+14.24	RT. 76.42
4	161+13.69	RT. 76.95
5	161+06.44	RT. 83.83
6	161+06.98	RT. 89.53
7	160+87.56	RT. 83.86
8	161+00.93	RT. 78.03



NOTE:
THE EXISTING COMBINATON CURB & GUTTER TO BE REPLACED AT THIS LOCATION IS B-6.24. THE EXISTING CURB & GUTTER HAS BEEN MODIFIED WITH A 6" WIDE NOTCH BY 1 1/2" DEEP TO CREATE AN 18" GUTTER FLAG AND TO ALLOW FOR THE EXISTING HMA RESURFACING. THE INTENT IS TO REPLACE THE CURB & GUTTER AS SHOWN AND EITHER FORM A 6" WIDE X 1 1/2" NOTCH FOR THE PROPOSED RESURFACING OR MILL THE PROPOSED GUTTER FLAG TO MATCH THE EXISTING EDGE OF PAVEMENT RADIUS.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #56

S.E. QUADRANT

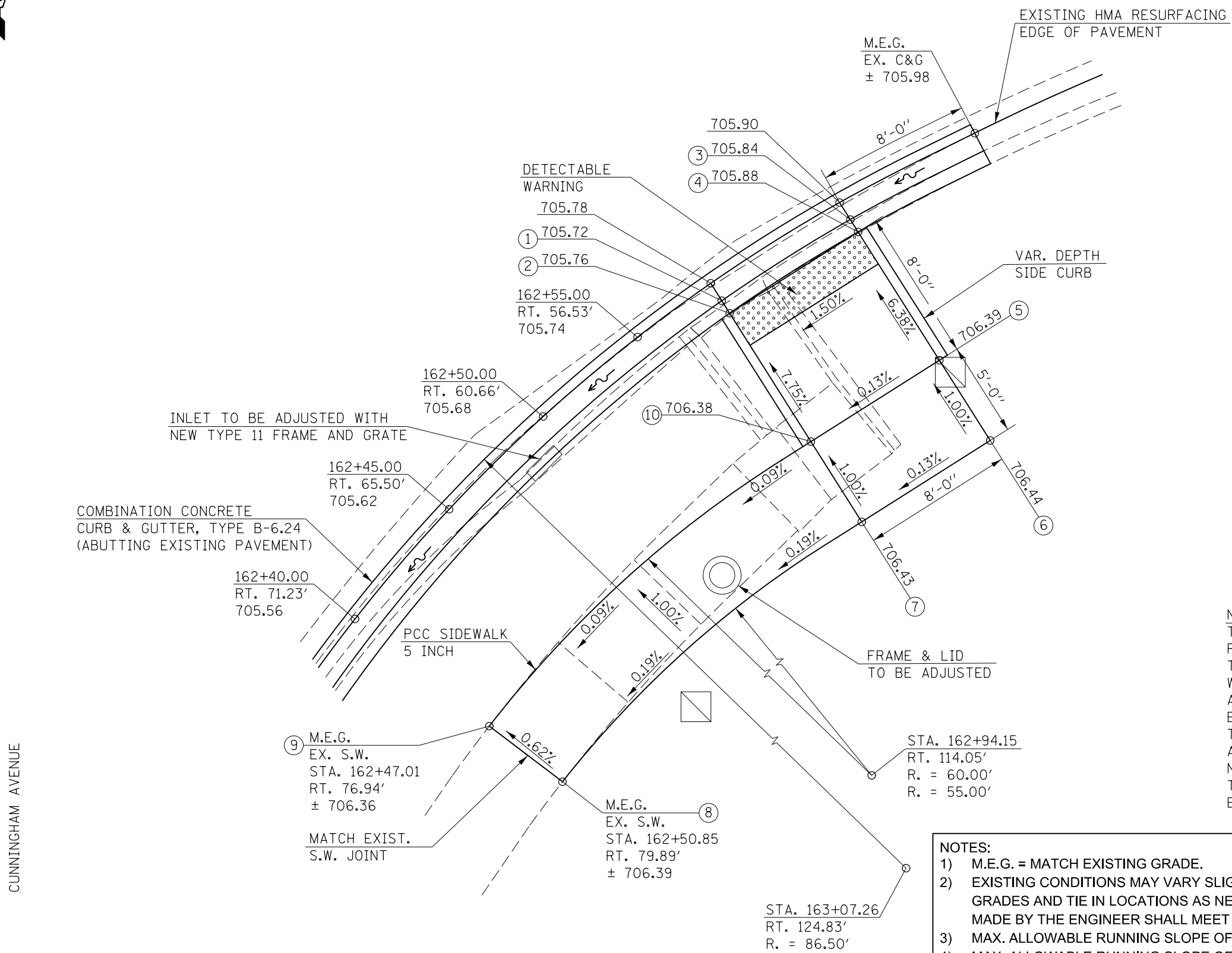
U.S. 150 & CUNNINGHAM AVE.

U.S. RTE. 150
(UNIVERSITY AVE.)



LOCATION #56 - S.E. QUADRANT
U.S. 150 & CUNNINGHAM AVE.

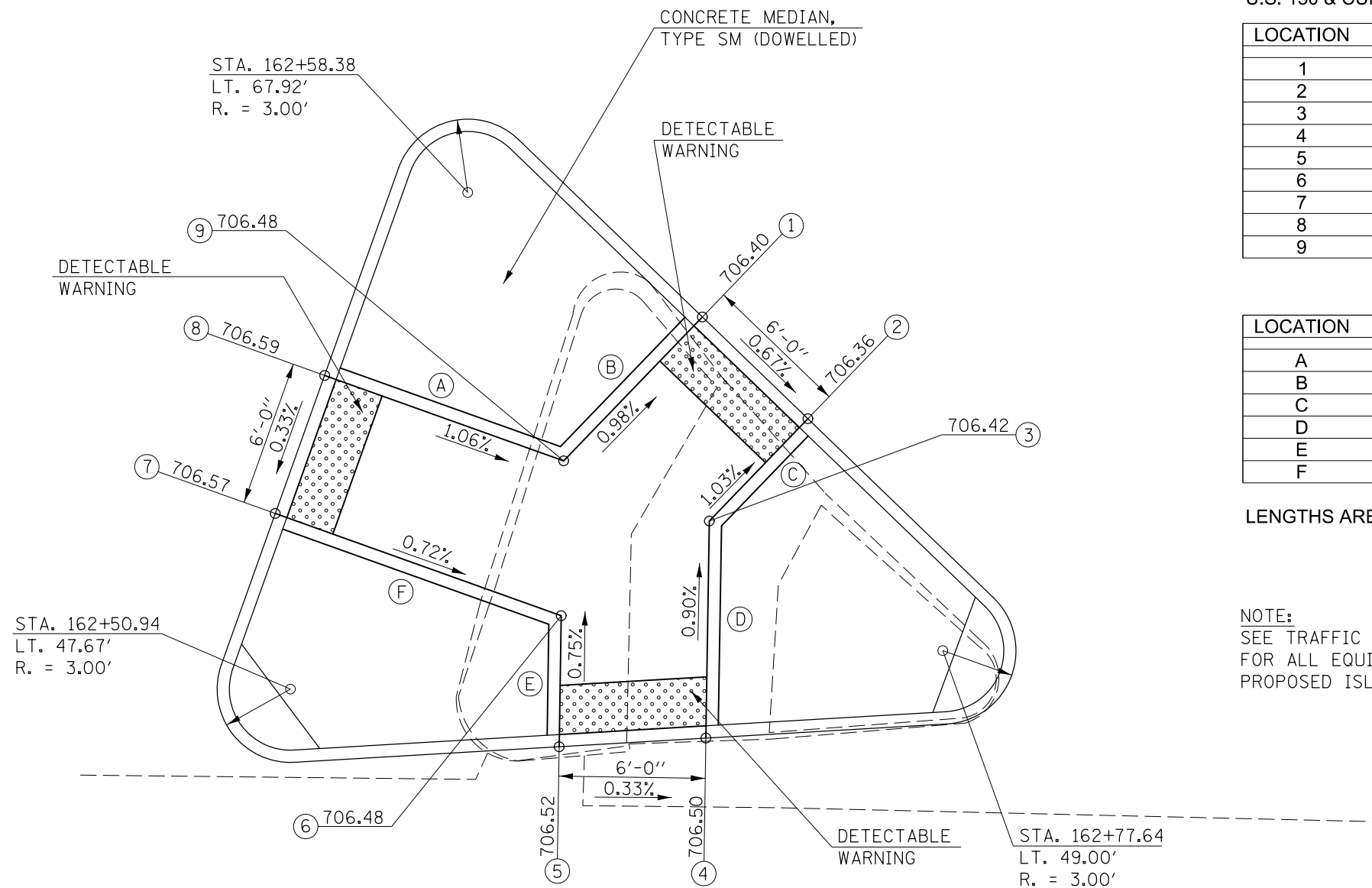
LOCATION	STATION	OFFSET
1	162+59.46	RT. 54.65
2	162+59.87	RT. 55.30
3	162+66.26	RT. 50.43
4	162+66.66	RT. 51.09
5	162+70.88	RT. 57.88
6	162+73.52	RT. 62.13
7	162+66.72	RT. 66.35
8	162+50.85	RT. 79.89
9	162+47.02	RT. 76.93
10	162+64.08	RT. 62.10



NOTE:
THE EXISTING COMBINATON CURB & GUTTER TO BE REPLACED AT THIS LOCATION IS B-6.24. THE EXISTING CURB & GUTTER HAS BEEN MODIFIED WITH A 6" WIDE NOTCH BY 1/2" DEEP TO CREATE AN 18" GUTTER FLAG AND TO ALLOW FOR THE EXISTING HMA RESURFACING. THE INTENT IS TO REPLACE THE CURB & GUTTER AS SHOWN AND EITHER FORM A 6" WIDE X 1/2" NOTCH FOR THE PROPOSED RESURFACING OR MILL THE PROPOSED GUTTER FLAG TO MATCH THE EXISTING EDGE OF PAVEMENT RADIUS.

- NOTES:**
- 1) M.E.G. = MATCH EXISTING GRADE.
 - 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
 - 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
 - 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
 - 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
 - 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #57
N.E. QUADRANT (ISLAND)
U.S. 150 & CUNNINGHAM AVE.



LOCATION #57 - N.E. QUADRANT (ISLAND)
 U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	162+67.93	LT. 62.74
2	162+72.21	LT. 58.54
3	162+68.14	LT. 54.39
4	162+67.92	LT. 45.51
5	162+61.91	LT. 45.21
6	162+62.04	LT. 50.57
7	162+50.38	LT. 54.86
8	162+52.45	LT. 60.49
9	162+62.20	LT. 56.91

LOCATION	POINT TO POINT	LENGTH
A	PT. 8 TO PT. 9	10' - 4 3/4"
B	PT. 1 TO PT. 9	8' - 2"
C	PT. 2 TO PT. 3	5' - 9 3/4"
D	PT. 3 TO PT. 4	8' - 10 1/2"
E	PT. 5 TO PT. 6	5' - 4 1/4"
F	PT. 6 TO PT. 7	12' - 5"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS

NOTE:
 SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS
 FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE
 PROPOSED ISLANDS.

CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CONCRETE MEDIAN, TYPE SM DOWELLED SURFACE SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE TOP OF THE EXISTING PAVEMENT.
- 3) THE EXISTING PAVEMENT UNDER THE CONCRETE MEDIAN, TYPE SM DOWELLED AND UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT. THE SIDEWALK AT THIS LOCATION WAS POURED FULL DEPTH AND IS INCLUDED IN PAVEMENT REMOVAL.
- 4) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 5) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
- 3) MAX. ALLOWABLE RUNNING SLOPE OF RAMP = 1:12 (8.3%), SLOPE <7% PREFERRED.
- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #58

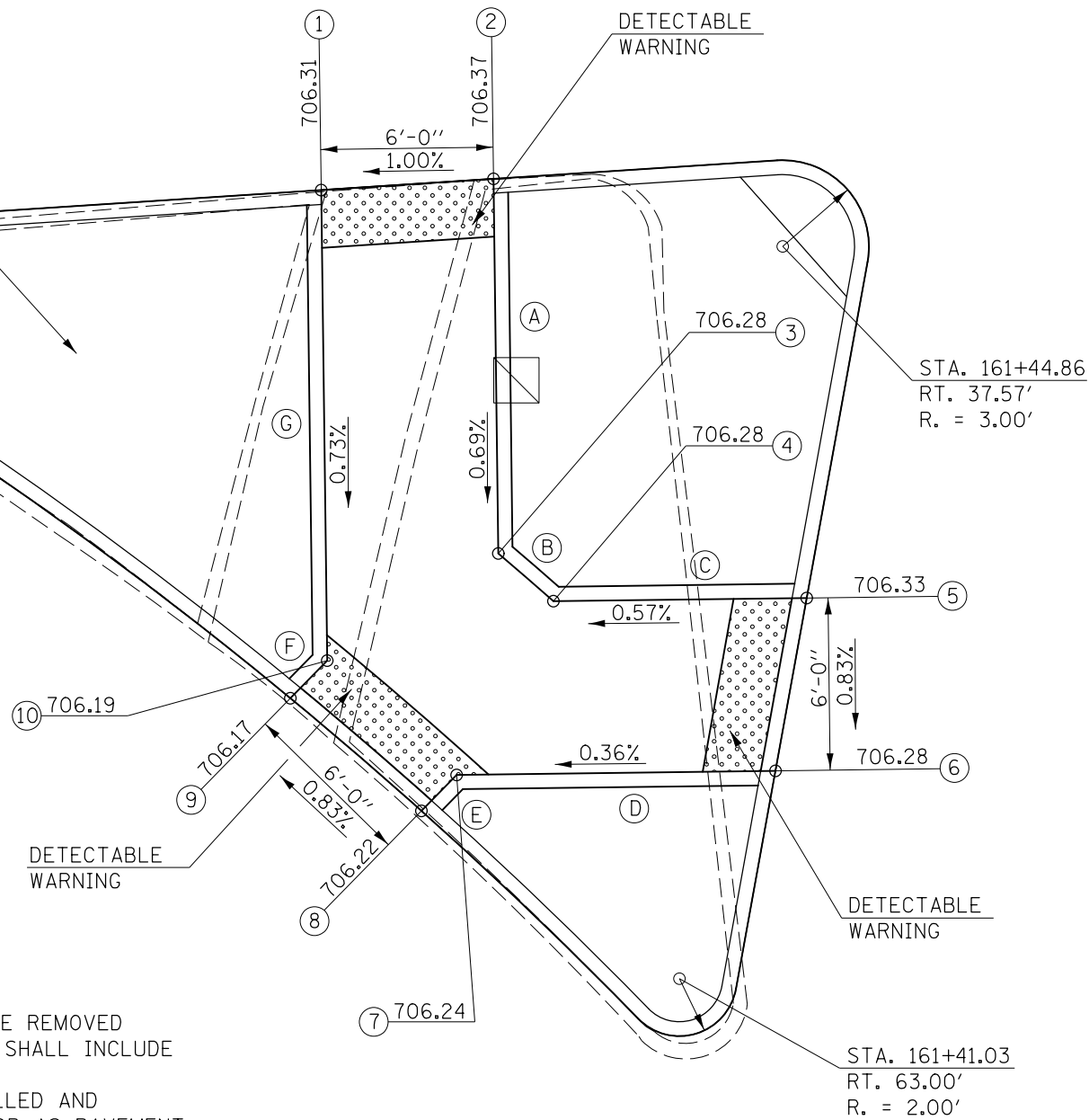
S.W. QUADRANT (ISLAND)

U.S. 150 & VINE STREET



CONCRETE MEDIAN,
TYPE SM (DOWELLED)

STA. 161+10.75
RT. 38.48'
R. = 2.00'



LOCATION #58 - S.W. QUADRANT (ISLAND)
U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	161+28.82	RT. 35.47
2	161+34.82	RT. 35.13
3	161+34.86	RT. 48.16
4	161+36.77	RT. 49.83
5	161+45.58	RT. 49.80
6	161+44.44	RT. 55.81
7	161+33.35	RT. 55.85
8	161+32.11	RT. 57.08
9	161+27.59	RT. 53.12
10	161+28.88	RT. 51.83

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 3	13' - 0 1/4"
B	PT. 3 TO PT. 4	2' - 6 1/2"
C	PT. 4 TO PT. 5	8' - 9 3/4"
D	PT. 6 TO PT. 7	11' - 1"
E	PT. 7 TO PT. 8	1' - 9"
F	PT. 9 TO PT. 10	1' - 10"
G	PT. 1 TO PT. 10	16' - 4 1/4"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS

NOTE:
SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS
FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE
PROPOSED ISLANDS.

CONSTRUCTION NOTES:

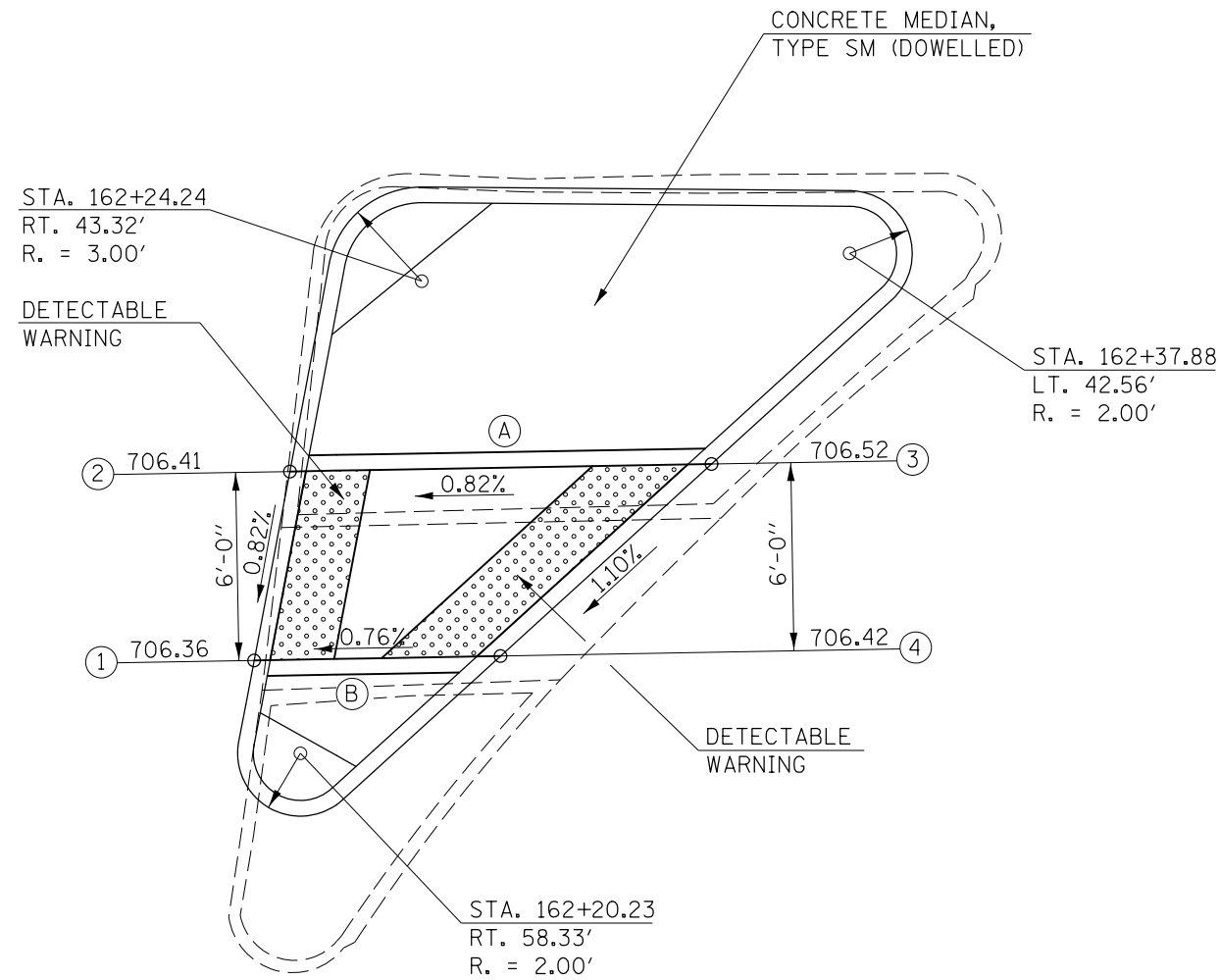
- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CONCRETE MEDIAN, TYPE SM DOWELLED SURFACE SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE TOP OF THE EXISTING PAVEMENT.
- 3) THE EXISTING PAVEMENT UNDER THE CONCRETE MEDIAN, TYPE SM DOWELLED AND UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT. THE SIDEWALK AT THIS LOCATION WAS POURED FULL DEPTH AND IS INCLUDED IN PAVEMENT REMOVAL.
- 4) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PCC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 5) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
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- 4) MAX. ALLOWABLE RUNNING SLOPE OF TURNING SPACE = 1:50 (2%), SLOPE <1.5% PREFERRED.
- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
- 6) MAX. ALLOWABLE CROSS SLOPE OF SIDEWALK OR LANDING = 1:50 (2%), SLOPE <1.5% PREFERRED.

LOCATION #59

S.E. QUADRANT (ISLAND)

U.S. 150 & VINE STREET



LOCATION #59 - S.E. QUADRANT (ISLAND)
U.S. 150 & CUNNINGHAM AVE.

LOCATION	STATION	OFFSET
1	162+18.79	RT. 55.36
2	162+19.98	RT. 49.35
3	162+33.42	RT. 49.23
4	162+26.64	RT. 55.29

LOCATION	POINT TO POINT	LENGTH
A	PT. 2 TO PT. 3	13' - 5 1/4"
B	PT. 1 TO PT. 4	7' - 10 1/4"

LENGTHS ARE INSIDE SIDEWALK DIMENSIONS

NOTE:
SEE TRAFFIC SIGNAL MODERNIZATION PLAN SHEETS FOR ALL EQUIPMENT TO BE CONSTRUCTED IN THE PROPOSED ISLANDS.

CONSTRUCTION NOTES:

- 1) THE EXISTING ISLAND SHALL BE REMOVED AND REPLACED.
- 2) THE EXISTING CONCRETE MEDIAN, TYPE SM DOWELLED SURFACE SHALL BE REMOVED AND WILL BE PAID FOR AS CONCRETE MEDIAN REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE TOP OF THE EXISTING PAVEMENT.
- 3) THE EXISTING PAVEMENT UNDER THE CONCRETE MEDIAN, TYPE SM DOWELLED AND UNDER THE PROPOSED MEDIAN SHALL BE REMOVED AND WILL BE PAID FOR AS PAVEMENT REMOVAL. THE REMOVAL SHALL INCLUDE REMOVING ALL MATERIAL TO THE BOTTOM OF THE EXISTING PAVEMENT. THE SIDEWALK AT THIS LOCATION WAS POURED FULL DEPTH AND IS INCLUDED IN PAVEMENT REMOVAL.
- 4) THE VOID LEFT BY THE REMOVAL ITEMS SHALL BE FILLED WITH PCC BASE COURSE, 10". THE TOP ELEVATION OF THE PPC BASE COURSE SHALL BE 2 1/2" BELOW THE PROPOSED HMA SURFACE ELEVATION EXCEPT AT SIDEWALK LOCATIONS. THE AREA THAT WILL BE EXPOSED FOR THE PROPOSED SIDEWALK SHALL BE FINISHED PER SECTION 424 OF THE STANDARD SPECIFICATIONS AND TO THE ELEVATIONS SHOWN ON THIS DETAIL.
- 5) UPON COMPLETION OF THE PCC BASE COURSE, A CONCRETE MEDIAN, TYPE SM (DOWELLED) SHALL BE CONSTRUCTED PER THE LEFT SIDE OF SECTION A-A ON DISTRICT 5 DETAIL NO. 60622354.

- 1) M.E.G. = MATCH EXISTING GRADE.
- 2) EXISTING CONDITIONS MAY VARY SLIGHTLY IN THE FIELD. THE ENGINEER CAN ADJUST GRADES AND TIE IN LOCATIONS AS NECESSARY TO CONSTRUCT RAMP. ANY ADJUSTMENTS MADE BY THE ENGINEER SHALL MEET ADA REQUIREMENTS FOR CURB RAMPS.
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- 5) MIN. ALLOWABLE WIDTH OF RAMP = 4'.
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