

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	1
		ILLINOIS	CONTRACT NO. 62P40	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN:

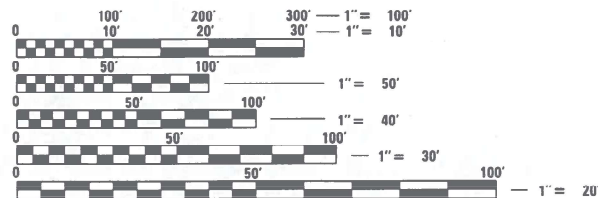
THE VILLAGE OF BARRINGTON
THE VILLAGE OF BEACH PARK
THE VILLAGE OF LAKE ZURICH
THE VILLAGE OF WINTHROP HARBOR
THE CITY OF NORTH CHICAGO
THE CITY OF PARK CITY
THE CITY OF WAUKEGAN

PROPOSED
HIGHWAY PLANS

VARIOUS ROUTES
SECTION 2021-123-SW
VARIOUS LOCATIONS
ADA IMPROVEMENTS
LAKE COUNTY

C-91-249-21

FOR GENERAL LOCATION MAP, SEE SHEET NO. 7



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 62P40

LIMITS	ADT (YEAR)	SPEED LIMIT	LOCATION
US RTE 12: AT AT IL RTE 22 (MAIN STREET)	37,900 (2021)	45 MPH	LAKE ZURICH
US RTE 12: AT ENTRANCE TO WALGREENS NO MILLER AVENUE	32,300 (2021)	50 MPH	LAKE ZURICH
IL RTE 59: NO MAIN STREET/WEST COUNTRY LINE ROAD TO WASHINGTON STREET	14,200 (2021)	25 MPH	BARRINGTON
IL RTE 59: AT LIONS DRIVE	14,700 (2021)	30 MPH	BARRINGTON
IL RTE 59: ROSLYN ROAD TO COVINGTON DRIVE	14,500 (2021)	35 MPH	BARRINGTON
WEST COUNTRY LINE ROAD: AT RAILROAD CROSSING (W/O LAGESHULTE STREET)	16,900 (2022)	30 MPH	BARRINGTON
EAST COUNTRY LINE ROAD: AT RAILROAD CROSSING/NORTH COOK STREET	16,000 (2022)	25 MPH	BARRINGTON
IL 137: MERIDIAN DRIVE TO RAY STREET	18,700 (2021)	30 MPH	NORTH CHICAGO
IL 120: AT KNIGHT AVENUE/RUTH WILCOX AVENUE	27,900 (2021)	45 MPH	PARK CITY
OLD PLANK ROAD: AT RUTH WILCOX AVENUE	-	25 MPH	PARK CITY
IL 131: GRANDVIEW AVENUE TO SOUTHVIEW AVENUE	29,700 (2021)	35 MPH	WAUKEGAN
IL 137: AT BEACH ROAD	17,300 (2021)	40 MPH	BEACH PARK
IL 137: 6TH ST TO PARK AVENUE	10,100 (2021)	35 MPH	WINTHROP HARBOR

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED July 8, 2024
Jane R. Rios REGIONAL ENGINEER

August 16, 2024
Scott A. Elk ENGINEER OF DESIGN AND ENVIRONMENT

August 16, 2024
James M. Ginn DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 13

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OF THE STATE OF ILLINOIS

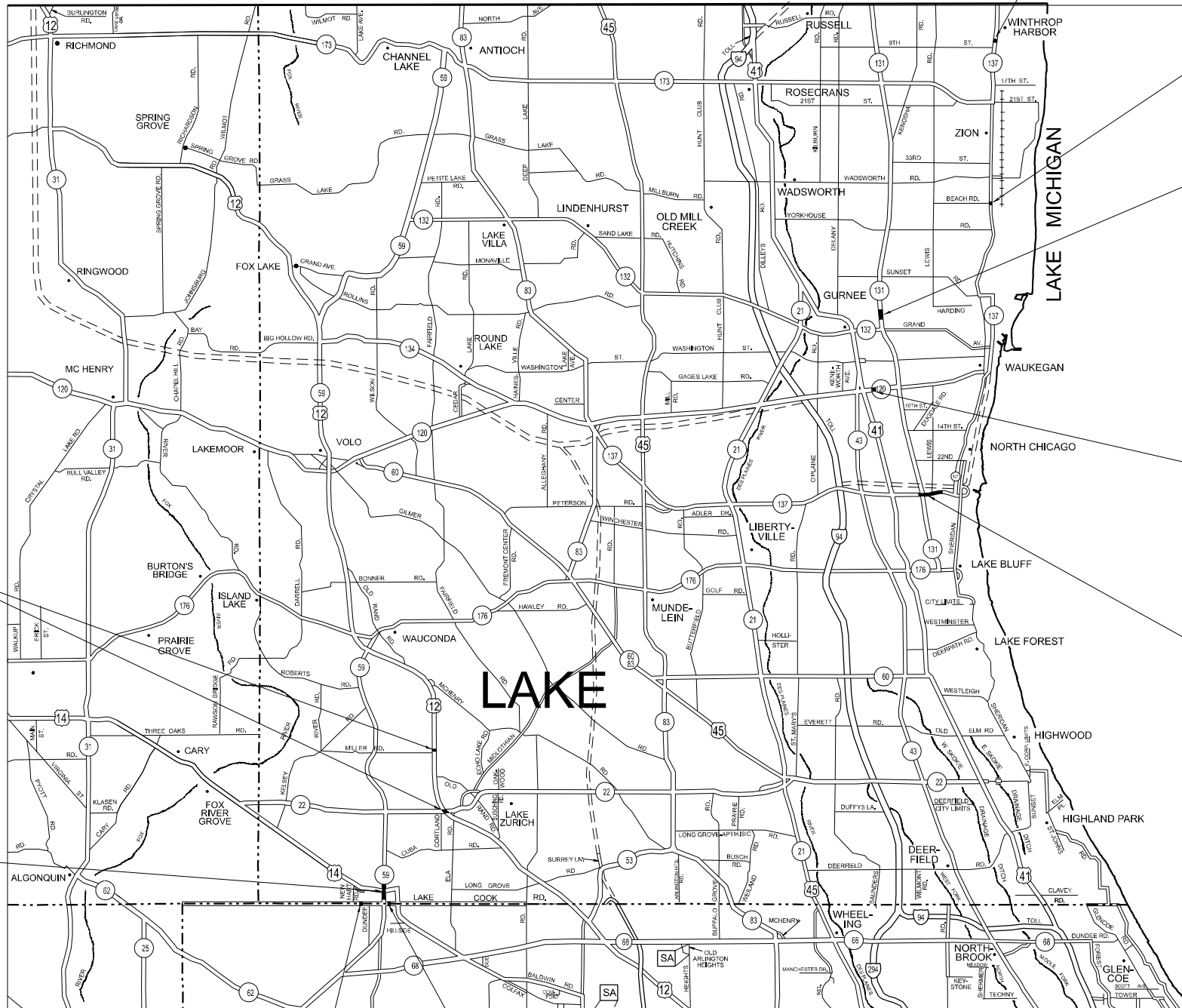
HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	HMA SURFACE COURSE, MIX "D", IL 9.5, N50; 2"	4% @ 50 GYR	QC/QA
MEDIAN SURFACE/ BIKEWAY	HMA SURFACE COURSE, MIX "D", IL 9.5, N50; MEDIAN SURFACE-4", BIKEWAY-2"	4% @ 50 GYR	QC/QA
DRIVEWAYS	HMA SURFACE COURSE, MIX "D", IL 9.5, N50; 2"	4% @ 50 GYR	QC/QA
	HMA BASE COURSE (HMA BINDER IL-19 mm); CE-8"	4% @ 50 GYR	QC/QA
CLASS D PATCHES (SPECIAL)	HMA SURFACE COURSE, MIX "D", IL 9.5, N50; 2"	4% @ 50 GYR	QC/QA
	HMA BINDER IL-19 mm; 8"	4% @ 50 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)			

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I				
20200100	EARTH EXCAVATION	CU YD	60.0	60.0						44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	38	38					
21101615	TOPSOIL FURNISH AND PLACE, 4'	SQ YD	316	316						44000300	CURB REMOVAL	FOOT	26	26					
25200110	SODDING, SALT TOLERANT	SQ YD	316	316						44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	43	43					
25200200	SUPPLEMENTAL WATERING	UNIT	3.2	3.2						44000600	SIDEWALK REMOVAL	SQ FT	4631	4631					
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	13.0	13.0						44200050	WELDED WIRE REINFORCEMENT	SQ YD	17.4	17.4					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	5	5						44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	17	17					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	128.3	128.3						44201299	DOWEL BARS 1 1/2"	EACH	8	8					
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5,	TON	31.5	31.5						44213200	SAW CUTS	FOOT	109	109					
	MIX "D", N50																		
										44213204	TIE BARS 3/4"	EACH	31	31					
42001300	PROTECTIVE COAT	SQ YD	974	974															
										60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY	SQ YD	19	19															
	PAVEMENT, 8 INCH									60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5	SQ FT	6235	6235						60300305	FRAMES AND LIDES TO BE ADJUSTED	EACH	7	7					
	INCH																		
										60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1					
42400800	DETECTABLE WARNINGS	SQ FT	245	245															
										60600605	CONCRETE CURB, TYPE B	FOOT	26	26					
44000100	PAVEMENT REMOVAL	SQ YD	5	5															
										60603800	COMBINATION CONCRETE CURB AND	FOOT	49	49					
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	241	241							GUTTER, TYPE B-6.12								
											* SPECIALTY ITEMS								
FILE NAME =		USER NAME = RanaKala		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			SUMMARY OF QUANTITIES VARIOUS ROUTES – VARIOUS LOCATIONS			F.A. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
pw://ltdot-pw.bentley.com/PWIDOT/Documents/IDOT		Offices/District 1/ORD Projects/DI21021/CADData/CADsheets/DRAWN		CHECKED -		REVISED -								VAR.	2021-123-SW		LAKE	69	3
		PLOT SCALE = 100,000 ' / 1 in.		DATE -		REVISED -											CONTRACT NO. 60P40		
		PLOT DATE = 7/11/2024												SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE								
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I							
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	60.0	60.0						* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5								
66900530	SOIL DISPOSAL ANALYSIS	EACH	15	15						* 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5								
66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION	L SUM	1	1						* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	24	24								
	PLAN										4"											
66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION	L SUM	1	1						* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	1270	1270								
	REPORT										6"											
66901006	REGULATED SUBSTANCES MONITORING	CAL DA	20	20						* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	343	343								
											12"											
67100100	MOBILIZATION	L SUM	1	1						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	276	276								
70102620	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1							24"											
	STANDARD 701501																					
										* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE	FOOT	169	169								
70102625	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1							6"											
	STANDARD 701606																					
										* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE	FOOT	37	37								
70102630	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1							24"											
	STANDARD 701601																					
										78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1183	1183								
70102632	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1																		
	STANDARD 701602									* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	263		263							
											2" DIA.											
70102635	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1																		
	STANDARD 701701									* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	345		345							
											4" DIA.											
70102640	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1																		
	STANDARD 701801										* SPECIALTY ITEMS											
FILE NAME =		USER NAME = Rana,Kala		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES VARIOUS ROUTES – VARIOUS LOCATIONS				F.A. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.	
pw://11dot-pw.bentley.com/PWIDOT/Documents/IDOT		Offices/District 1/ORD Projects/DI21021/CADData/CADsheets/DI21021/IN-500.dgn		CHECKED -		REVISED -										VAR.	2021-I23-SW		LAKE	69	4	
				PLOT DATE = 7/11/2024		DATE -														CONTRACT NO. 60P40		
																SCALE:		SHEET NO. OF		SHEETS	STA. TO STA.	

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE								
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE ROADWAY 002I	100% STATE SIGNALS 002I							
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	8		8					X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	1420	1420								
	INSTALLATION																					
										* X1400221	UPGRADE EXISTING CONTROLLER TO NTCIP	EACH	1		1							
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	4292		4292						SPECIAL											
	14 2C																					
										* X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	5		5							
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	3195		3195																	
	14 3C									* X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	9		9							
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT	FOOT	306		306					X4240430	PORTLAND CEMENT CONCRETE SIDEWALK	SQ FT	113	113								
	GROUNDING CONDUCTOR, NO. 6 1C										5 INCH (SPECIAL)											
87900200	DRILL EXISTING HANDHOLE	EACH	20		20					X4240800	DETECTABLE WARNING (SPECIAL)	SQ FT	357	357								
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE,	EACH	18		18					X4400501	COMBINATION CURB AND GUTTER REMOVAL	FOOT	47	47								
	BRACKET MOUNTED WITH COUNTDOWN TIMER										AND REPLACEMENT LESS THAN OR EQUAL											
											TO 10 FEET											
89500200	RELOCATE EXISTING PEDESTRIAN SIGNAL	EACH	2		2																	
	HEAD									X4400503	COMBINATION CURB AND GUTTER REMOVAL	FOOT	725	725								
											AND REPLACEMENT GREATER THAN 10 FEET											
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	6		6																	
										X4403800	MEDIAN SURFACE REMOVAL	SQ FT	275	275								
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	554		554																	
										X4421763	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	17	17								
89502375	REMOVE EXISTING TRAFFIC SIGNAL	EACH	4		4						(SPECIAL)											
	EQUIPMENT																					
										X6061311	CONCRETE MEDIAN SURFACE, 5 INCH	SQ FT	181	181								
89502376	REBUILD EXISTING HANDHOLE	EACH	5		5																	
										X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12								
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1																		
											* SPECIALTY ITEMS											
FILE NAME =		USER NAME = RanaKala		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES VARIOUS ROUTES – VARIOUS LOCATIONS				F.A. RTE.		SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
pw://11dat-pw.bentley.com/PWIDOT/Documents/IDOT		Offices/District 1/ORD Projects/D12021/CADData/CADsheets/DRAWN-500.dgn		CHECKED -		REVISED -										VAR.		2021-123-SW		LAKE	69	5
		PLOT SCALE = 100.000 ' / 1in.		DATE -		REVISED -														CONTRACT NO. 60P40		
		PLOT DATE = 7/11/2024				REVISED -										SCALE:		SHEET NO. OF SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

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**US 12
(VILLAGE OF LAKE ZURICH)**

- IL 22
- ENTRANCE TO WALGREEN
(NORTH OF MILLER AVENUE)

**IL 59
(VILLAGE OF BARRINGTON)**

- RAIL CROSSING (SOUTH
OF FRANKLIN STREET)
- FRANKLIN STREET
- WASHINGTON STREET
- LIONS DRIVE
- ROSYLN ROAD
- COVINGTON DRIVE

**WEST COUNTY LINE ROAD
(VILLAGE OF BARRINGTON)**

- RAILROAD CROSSING
(W/O LAGESHULTE STEET)

EAST COUNTY LINE ROAD (VILLAGE OF BARRINGTON)

- NORTH COOK STREET/RAILROAD CROSSING

IL 137 (VILLAGE OF WINTHROP HARBOR)

- 6TH STREET
- PARK AVENUE

IL 137 (VILLAGE OF BEACH PARK)

- BEACH ROAD

IL 131 (CITY OF WAUKEGAN)

- GRANDVIEW AVENUE
- SOUTHVIEW AVENUE

IL 120 (CITY OF PARK CITY)

- KNIGHT AVENUE/RUTH WILCOX AVENUE
- OLD PLANK ROAD (CITY OF PARK CITY)**
- RUTH WILCOX AVENUE

IL 137 (CITY OF NORTH CHICAGO)

- MERIDIAN DRIVE
- LEWIS AVENUE
- RAY STREET

MODEL: D:\data\11000\11000.dwg
FILE NAME: 11000.dwg
PROJECT: 11000
DATE: 7/11/2024

USER NAME	= Rana,Kabo	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
PLOT SCALE	= 100,000' / in.	CHECKED	-	REVISED	-
PLOT DATE	= 7/11/2024	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL LOCATION MAP
VARIOUS ROUTES - VARIOUS LOCATIONS

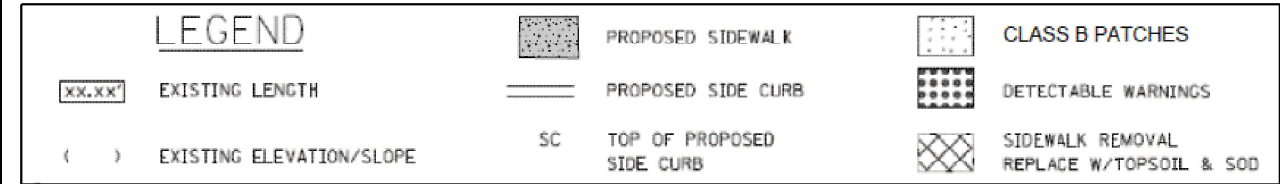
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	7
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

Model: Default
File Name: \\lbt-cw-bentley.com\\P\\W\\DOT\\Documents\\DOT 0 cas\\District 1\\ORD Projects\\D12 102 1\\CAD\\Data\\CAD\\sheet\\D12 102 1-qty-schedule.dgn

SIDEWALK SCHEDULE OF QUANTITIES																																																			
INTERSECTION	EARTH EXCAVATION	TOPSOIL FURNISH AND PLACE, 4"	SODDING, SALT TOLERANT	SUPPLEMENTAL WATERING	AGGREGATE BASE COURSE, TYPE B 6"	HOT-MIX ASPHALT BASE COURSE, 8"	BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", NSD	PROTECTIVE COAT	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	DETECTABLE WARNINGS	PAVEMENT REMOVAL	HOT-MIX ASPHALT SURFACE REMOVAL 2"	DRIVEWAY PAVEMENT REMOVAL	CURB REMOVAL	COMBINATION CURB AND GUTTER REMOVAL	SIDEWALK REMOVAL	WELDED WIRE REINFORCEMENT	CLASS B PATCHES, TYPE III, 10 INCH	DOWNEL BARS 1 1/2"	SAW CUTS	TIE BARS 3/4"	CATCH BASINS TO BE ADJUSTED	FRAMES AND GRATES TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, OPEN LID	CONCRETE CURB, TYPE B	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-5.12	REMOVE SIGN PANEL ASSEMBLY - TYPE A	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	PAVEMENT MARKING REMOVAL - WATER BLASTING	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	REBUILD EXISTING HANDHOLE	REMOVE AND REINSTALL BRICK PAVER	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)	DETECTABLE WARNINGS (SPECIAL)	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	MEDIAN SURFACE REMOVAL	CLASS D PATCHES, TYPE II, 10 INCH (SPECIAL)	CONCRETE MEDIAN SURFACE, 5 INCH			
	CU YD	SQ YD	SQ YD	UNIT	SQ YD	SQ YD	POUND	TON	SQ YD	SQ YD	SQ FT	SQ FT	SQ YD	SQ YD	SQ YD	FOOT	FOOT	SQ FT	SQ YD	SQ YD	EACH	FOOT	EACH	EACH	EACH	EACH	FOOT	FOOT	EACH	EACH	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	EACH	EACH	SQ FT	SQ FT	SQ FT	FOOT	FOOT	SQ FT	SQ YD	SQ FT				
	20.000100	21101615	25200110	25200200	35101800	35501316	40600290	40604060	42001300	42300400	42400200	42400800	44000100	44000157	44000200	44000300	44000500	44000600	44200050	44200974	44201299	44213200	44213204	60250200	60300105	60300305	60406000	60600605	60603800	72400100	72400500	78000200	78000400	78000600	78000650	78009006	78009024	78300202	85000200	89502376	X0327611	X4240430	X4240800	X4400501	X4400503	X4403800	X4421763	X6061311			
US Route 12 and Illinois Route 22 (Main St)	5.9	17	17	0.17	0	0	0.0	0	110	0	665	56	0	0	0	0	0	655	17.4	17	8	109	31	0	1	1	0	0	0	0	0	0	0	396	0	92	0	0	382	1	1	0	22	0	0	106	0	0	0	0	
US Route 12 and North of Miller Avenue (entrance to walgreens)	1.6	27	27	0.27	0	0	16.7	2	34	0	206	20	0	19	0	0	23	218	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	94	0	0	0	47	0	0	0	0	0	0	31	0	12	0	0		
Illinois Route 59 and Rail Crossing (South of Franklin St)	1.8	10	10	0.10	0	0	0.0	0	28	0	247	0	0	0	0	0	0	183	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	66	0	49	0	0	0	0	0	0		
Illinois Route 59 and Franklin Street	5.3	13	13	0.13	0	0	17.5	4	95	0	598	0	0	39	0	0	0	152	0	0	0	0	0	1	1	3	1	0	0	1	1	0	0	0	0	0	0	79	1	2	451	19	28	0	85	0	0	0	0		
Illinois Route 59 and Washington Street	5.3	6	6	0.06	0	0	15.2	4	93	0	593	0	0	34	0	0	0	210	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	83	0	16	0	0	73.5	1	2	447	26	24	0	79	0	0	0		
Illinois Route 59 and Lions Drive	3.4	8	8	0.08	0	0	8.2	2	67	0	412	0	0	18	0	0	0	283	0	0	0	0	0	0	2	0	0	0	0	0	1	1	0	90	0	17	0	0	79	0	0	138	29	21	0	63	0	0	0		
Illinois Route 59 and Roslyn Road	4.4	39	39	0.39	0	0	19.7	5	51	0	451	0	0	44	0	0	0	509	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	58	65	13	0	0	120	0	0	0	0	43	0	0	0	0			
Illinois Route 59 and Covington Drive	3.0	27	27	0.27	0	0	11.2	3	48	0	369	0	5	25	0	0	21	306	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	0	21	0	5	0		
West County Line Road and Railroad crossing (East of Raymond Ave)	3.5	37	37	0.37	0	0	0.0	0	47	0	421	0	0	0	0	0	0	421	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	40	0	0	0	0	0	
East County Line Road and North Cook St (railroad crossing)	4.9	9	9	0.09	0	0	4.8	1	80	19	560	0	0	11	19	0	0	246	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	278	41	0	0	213	0	0	318	0	76	6	51	0	0	0		
IL 137 (Buckley Road) and Meridian Drive	1.5	16	16	0.16	0	0	0.0	0	5	0	25	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	
IL 137 (Buckley Road) and Lewis Avenue	2.0	17	17	0.17	0	0	4.7	2.4	69	0	154	76	0	0	0	26	0	104	0	0	0	0	0	0	0	0	0	0	26	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35	62	275	0	181	
IL 137 (Buckley Road) and Ray Street	1.5	5	5	0.05	0	0	0.0	0	8	0	69	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
IL 120 (Belvidere Road) and Knight Avenue/Ruth Wilcox Avenue	0.7	4	4	0.04	0	0	0.0	0.0	13	0	60	11	0	0	0	0	0	52	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	139	0	38	98	24	146	0	0	0	0	0	18	0	0	0	0		
Old Plank Road and Ruth Wilcox Avenue	0.7	9	9	0.09	0	0	0.0	0.0	9	0	64	10	0	0	0	0	0	62	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	0	0	0	0	0	
IL 131 (Green Bay Road) and Grandview Avenue	2.3	15	15	0.15	0	0	9.9	2.5	41	0	229	0	0	22	0	0	0	239	0	0	0	0	0	0	0	2	0	0	0	1	1	0	88	0	14	0	0	22	0	0	0	0	36	0	46	0	0	0	0		
IL 131 (Green Bay Road) and Southview Avenue	3.0	21	21	0.21	0	0	0.0	0	39	0	238	0	0	0	0	0	0	251	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	0	37	0	0	0	0		
IL 137 (Sheridan Road) and Beach Road	3.1	11	11	0.11	13	0	5.9	1.5	28	0	245	24	0	0	0	0	0	362	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
IL 137 (Sheridan Road) and 6th Street	3.3	13	13	0.13	0	5	7.9	2.3	61	0	361	23	0	15	19	0	0	212	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	82	0	14	0	0	22	0	0	0	6	0	0	61	0	0	0	0	
IL 137 (Sheridan Road) and Park Avenue	2.4	13	13	0.13	0	0	6.8	1.7	48	0	267	25	0	15	0	0	0	166	0	0	0	0	0	0	0	0	0	0	0	1	1	24	146	0	15	0	0	0	0	0	0	10	0	0	55	0	0	0	0		
TOTAL	60.0	316	316	3.2	13.0	5	128.3	31.5	974	19	6,235	245	5	241	38	26	43	4,631	17.4	17	8	109	31	1	5	7	1	26	49	5	5	24	1,270	343	276	169	37	1,183	3	5	1,420	113	357	47	725	275	17	181			

<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	USER NAME = Rana,Kalo	DESIGNED -	REVISED -	SCHEDULE OF QUANTITIES VARIOUS ROUTES – VARIOUS LOCATIONS				F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					VAR.	2021-123-SW	LAKE	69	8
	PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -	CONTRACT NO. 62P40								
	PLOT DATE = 7/11/2024	DATE -	REVISED -	SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			
										ILLINOIS	FED. AID PROJECT	



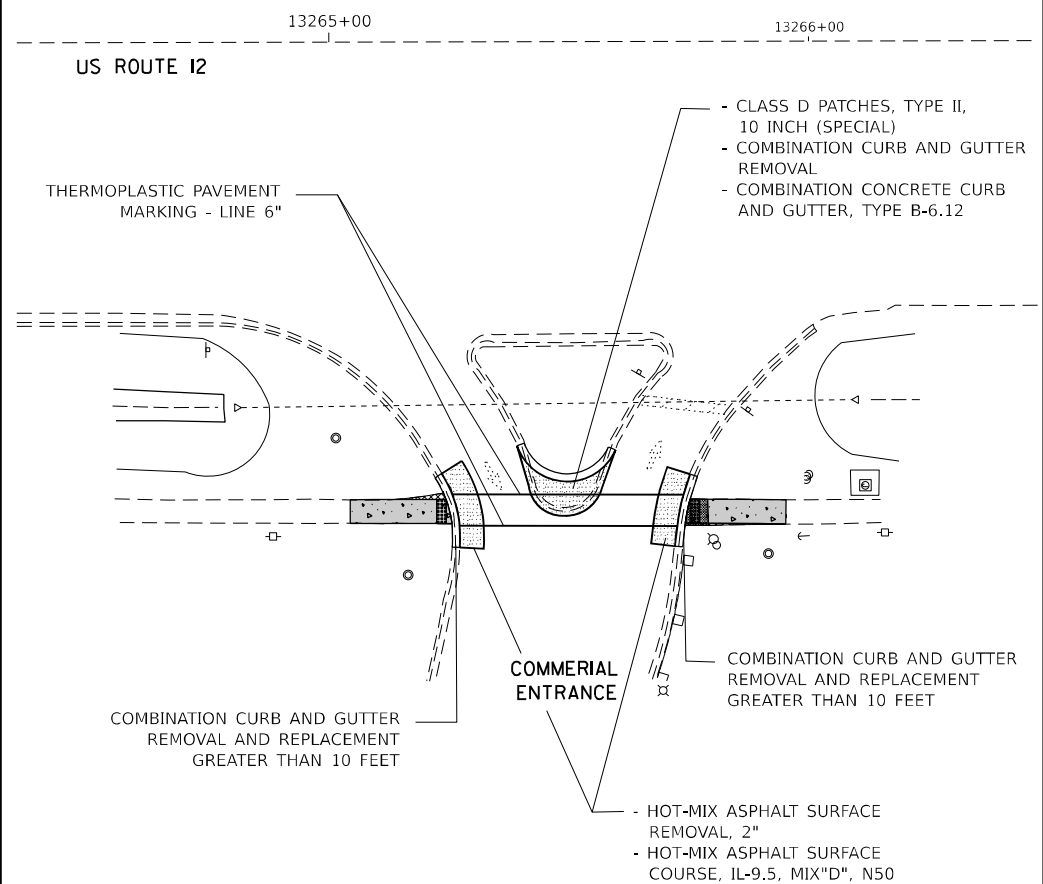
USER NAME = Rana,Kalo	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 40,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

ADA RAMPS PROPOSED PLANS

US ROUTE 12 AT IL ROUTE 22 AND AT ENTRANCE TO WALGREENS (NORTH OF MILLER STREET)

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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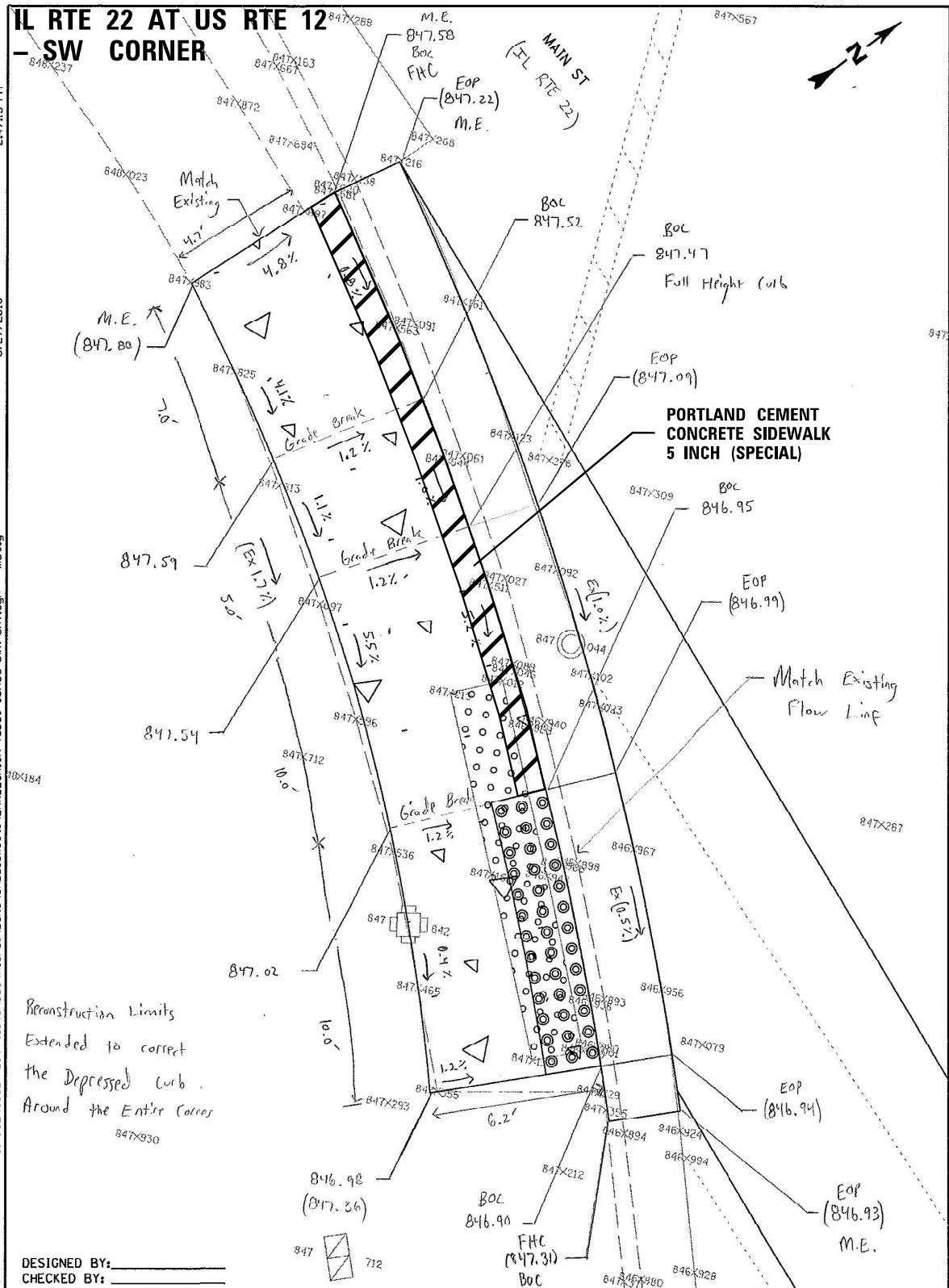
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	9
		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		



REFERENCE BENCHMARK ELEVATION = 835.136

BENCHMARK = CUT SQ. NORTH IN CONC CURB

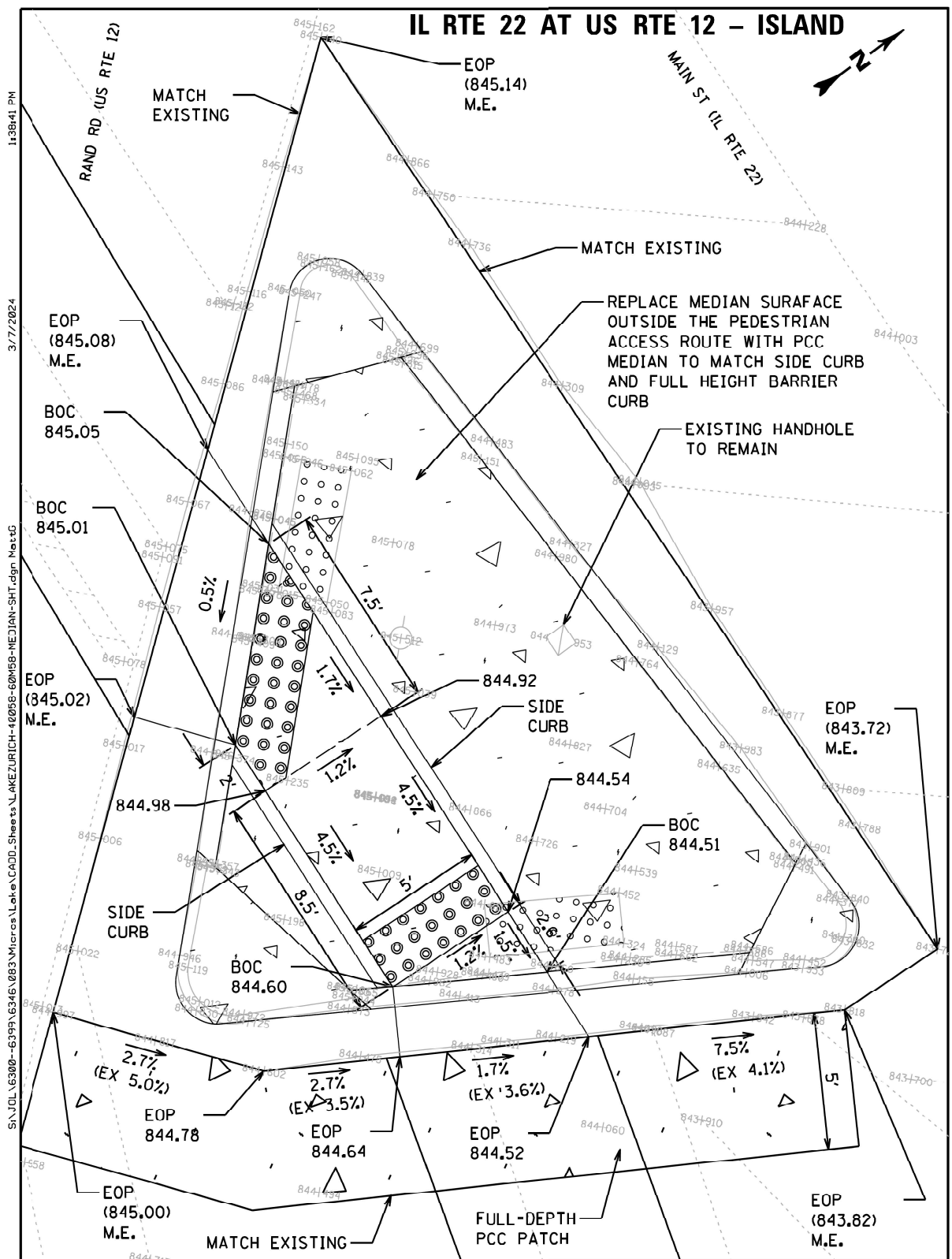
LOCATION = TRIANGULAR ISLAND AT US ROUTE 12 AND WALGREENS ENTRANCE



DESIGNED BY:
CHECKED BY:

LEGEND
XX.XX' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE
PROPOSED SIDEWALK
PROPOSED SIDE CURB
TOP OF PROPOSED SIDE CURB
HMA SURFACE REMOVAL, 2" HMA SURFACE COURSE, IL 9.5, MIX"D", N50
DETECTABLE WARNINGS
SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

STRAND ASSOCIATES®



LEGEND
XX.XX' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE
PROPOSED SIDEWALK
PROPOSED SIDE CURB
TOP OF PROPOSED SIDE CURB
HMA SURFACE REMOVAL, 2" HMA SURFACE COURSE, IL 9.5, MIX"D", N50
DETECTABLE WARNINGS
SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

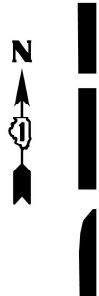
STRAND ASSOCIATES®

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
DRAWN -	REVISED -	REVISED -
PLOT SCALE = 100,000' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS IL ROUTE 22 AND US ROUTE 12			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	10
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				



IL RTE 59



XX.XX'

()



.....

.....

SC



SA
STRAND
ASSOCIATES[®]

1170 SOUTH HOMBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

IL ROUTE 59 AND RAILROAD CROSSING (SO FRANKLIN STREET)

0/

S	STA
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T

VAR.

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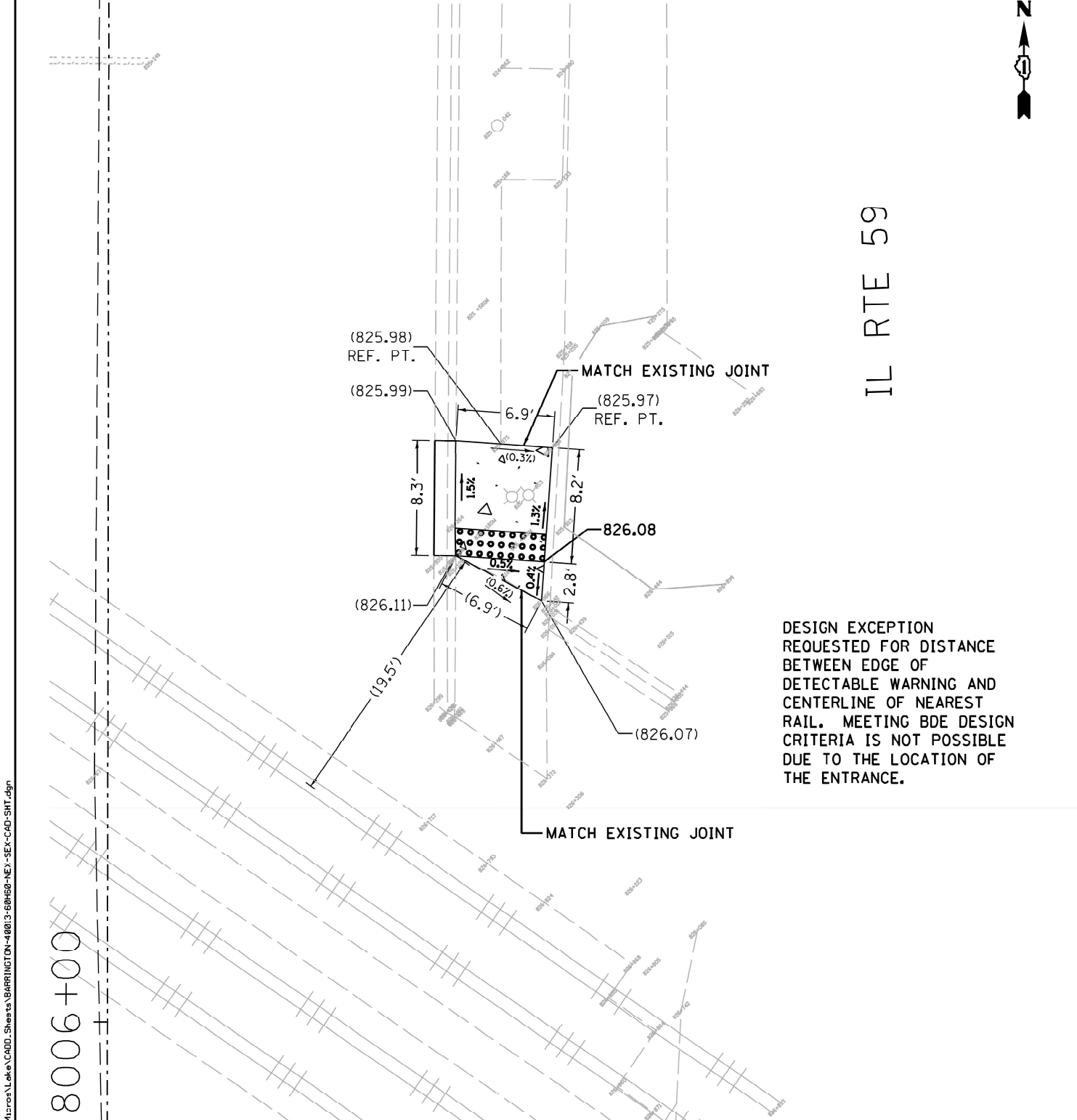
2021-123-SV

LAKE

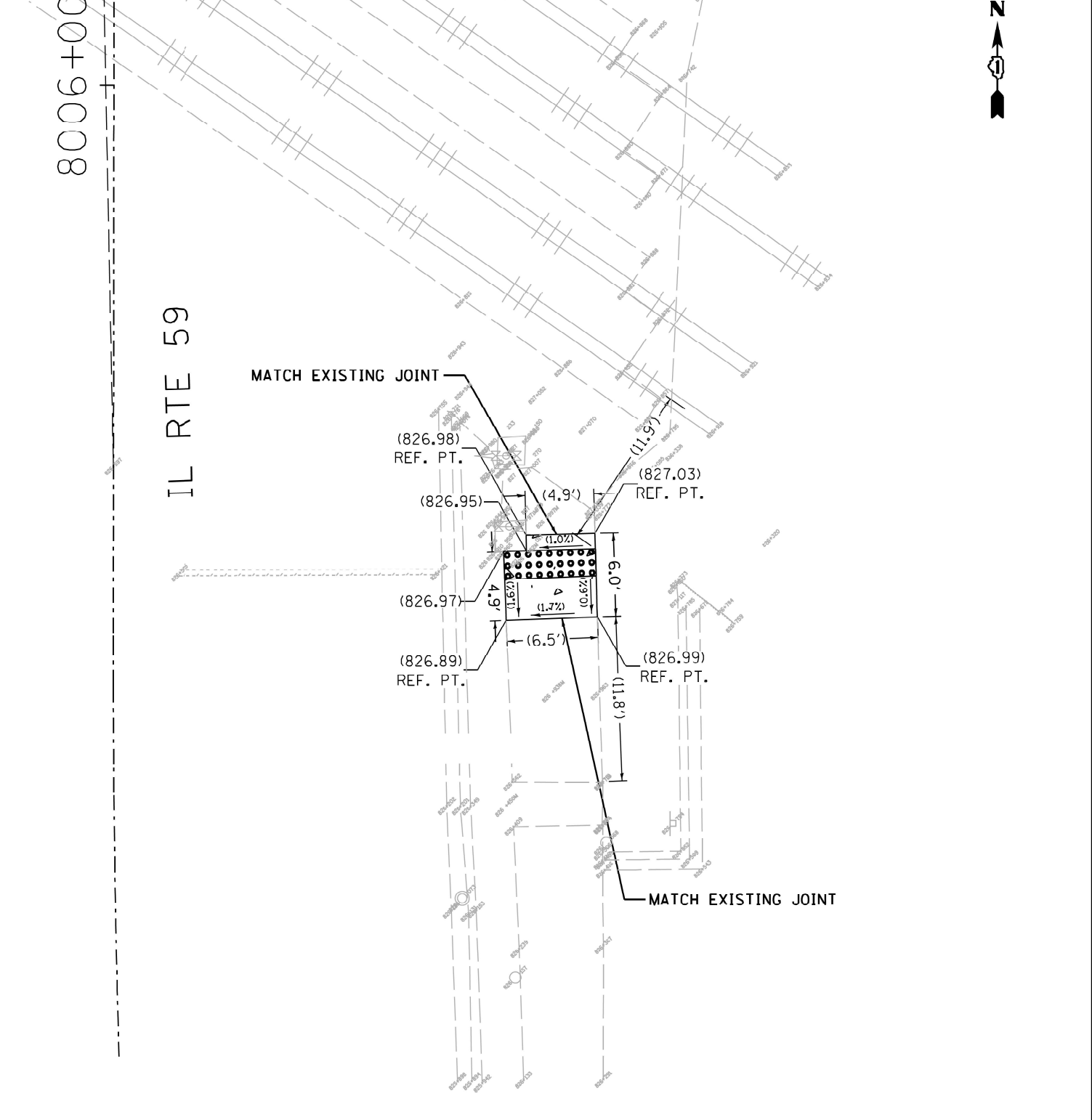
CONTRACT

CONTRACT	
PROJECT	

IL RTE 59 AT RAILROAD CROSSING
(S/O FRANKLIN ST) - NE CORNER



IL RTE 59 AT RAILROAD CROSSING
(S/O FRANKLIN ST) - SE CORNER



NOTE: REMOVE ALL BRICK PAVERS FROM THE PROPOSED IMPROVEMENT AREA AND REPLACE THEM FULLY WITH CONCRETE. IF ANY NEW CONCRETE AREA ADJACENT TO EXISTING PAVER AREA, A SOLDIER COURSE OF PAVERS SHALL BE INSTALLED BETWEEN THE CONCRETE AND AGAINST CLEANLY SAWCUT PAVERS. THE EXCESS OF THE BRICK PAVERS SHALL BE DISPOSED OF OR TRANSFERRED BACK TO THE VILLAGE OF BARRINGTON. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE VILLAGE OF BARRINGTON REGARDING THE DISPOSAL OF THE BRICK PAVERS.

SA

1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

STRAND
ASSOCIATES

USER NAME = MattG
MODEL NAME = Default
PLOT SCALE = 10.0000' / in.
PLOT DATE = 3/29/2023

DESIGNED - MJG
DRAWN - MJG
CHECKED - DWG
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS
IL ROUTE 59 AND RAILROAD CROSSING (S/O FRANKLIN STREET)

SCALE: 1"=10' SHEET OF SHEETS STA. TO STA.

F.A. RTE.
VAR.

SECTION
2021-123-SW

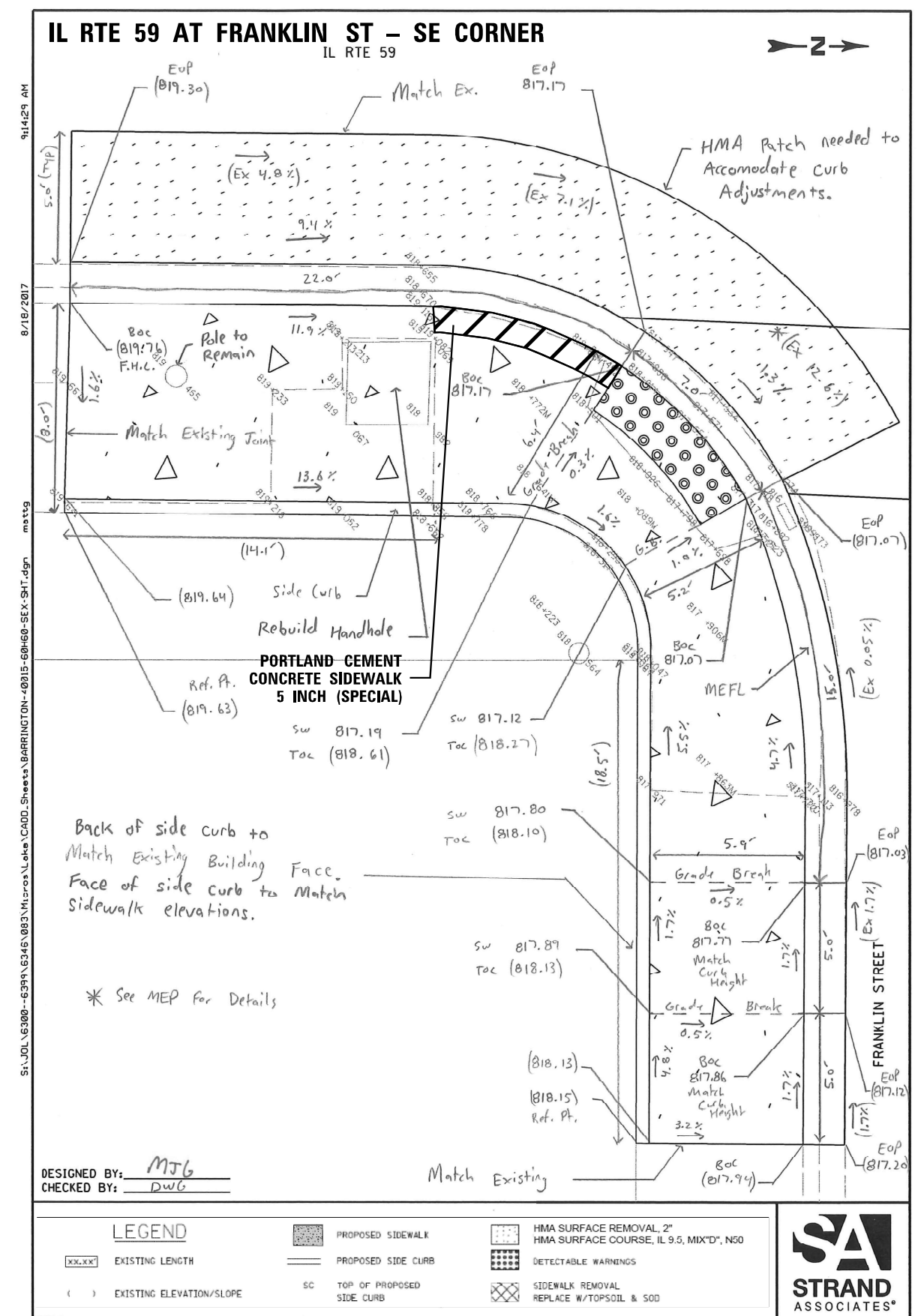
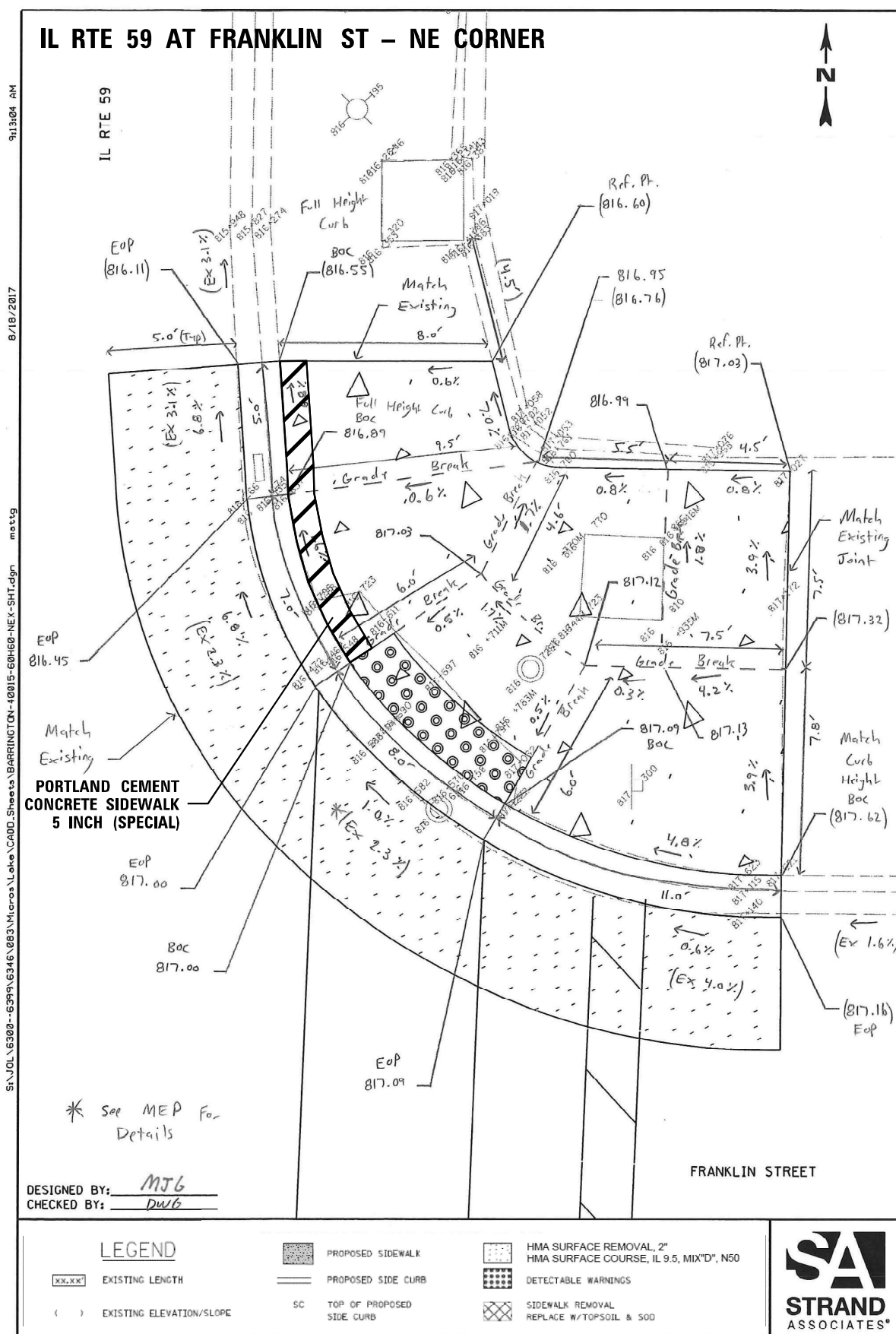
COUNTY
LAKE

TOTAL SHEETS
69

SHEET NO.
16

CONTRACT NO.
62P40

ILLINOIS FED. AID PROJECT



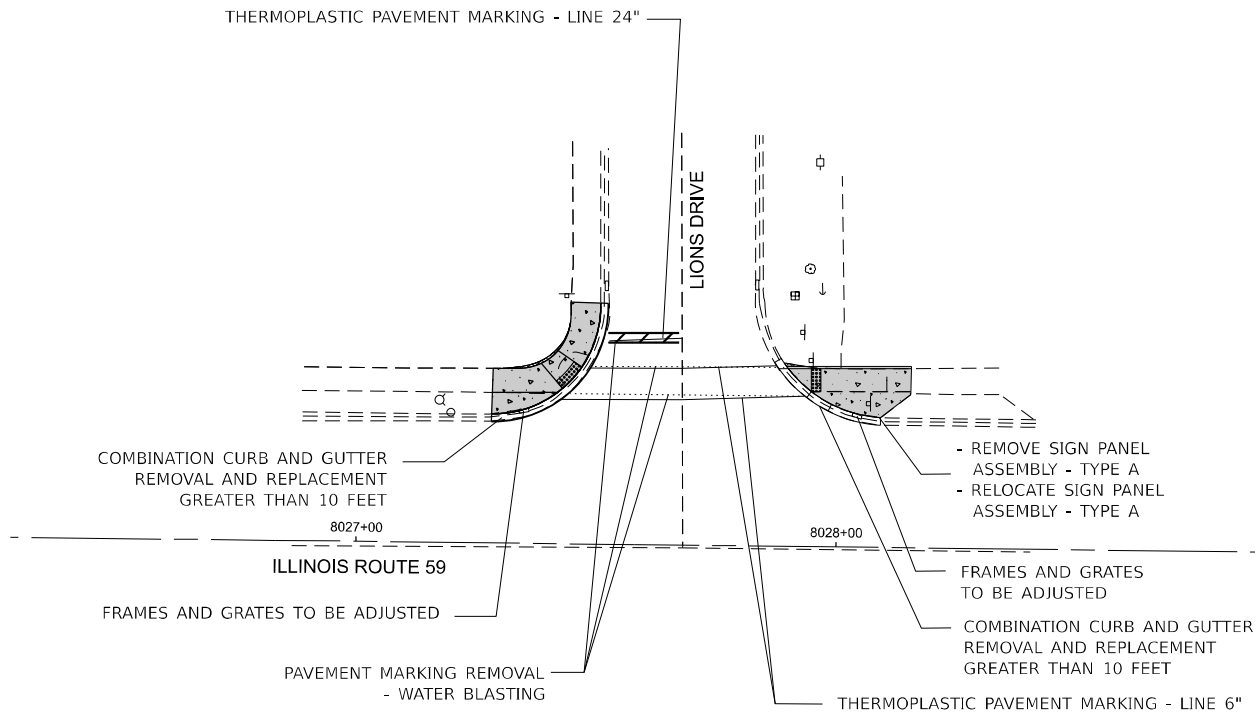
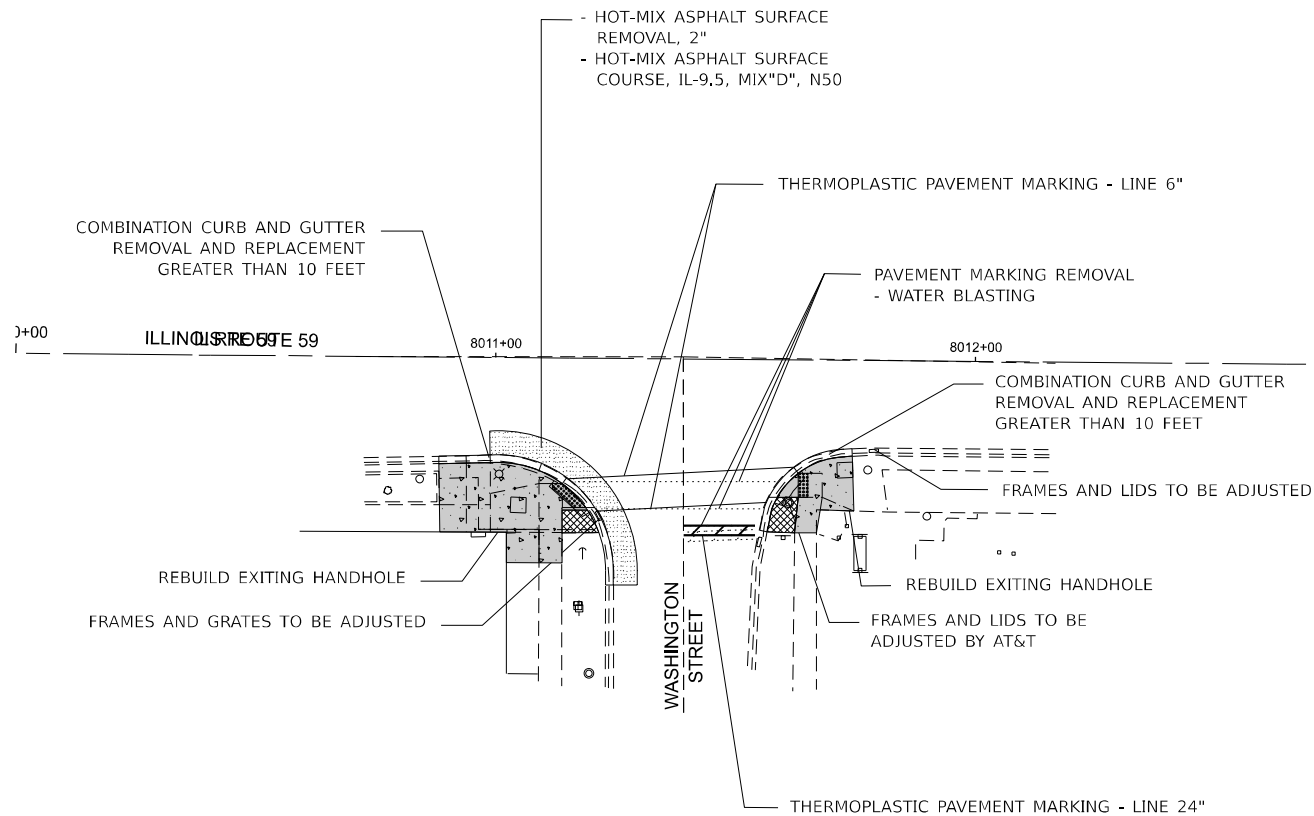
USER NAME = Rana,Kalo	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS
IL ROUTE 59 AND FRANKLIN STREET

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	17
		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	



LEGEND

XX.XX' EXISTING LENGTH

() EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED
SIDE CURB



HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, IL 9.5, MIX"D", N50



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEVATION = 814.97

BENCHMARK = NW BOLT FH, W. SIDE HOUGH ST @ NE COR. BLOG OPP WASHINGTON ST

LOCATION = NORTHEAST CORNER OF HOUGH STREET AND WASHINGTON STREET

REFERENCE BENCHMARK ELEVATION = 812.40

BENCHMARK = TAG BOLT ON FIRE HYDRANT SOUTH OF LIONS DR. ON WEST SIDE

LOCATION = SOTHWEST CORNER OF IL 59 AND LIONS DRIVE

MODEL: Default
FILE: \\net-cw.bentley.com\\P\\V\\DOT\\Documents\\DOT 0 cas\\District 1\\OR0 Projects\\ID121021\\CAD\\Data\\CAD\\Sheets\\ID121021-Left-Info-SW Lake Ccdgn

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 40,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

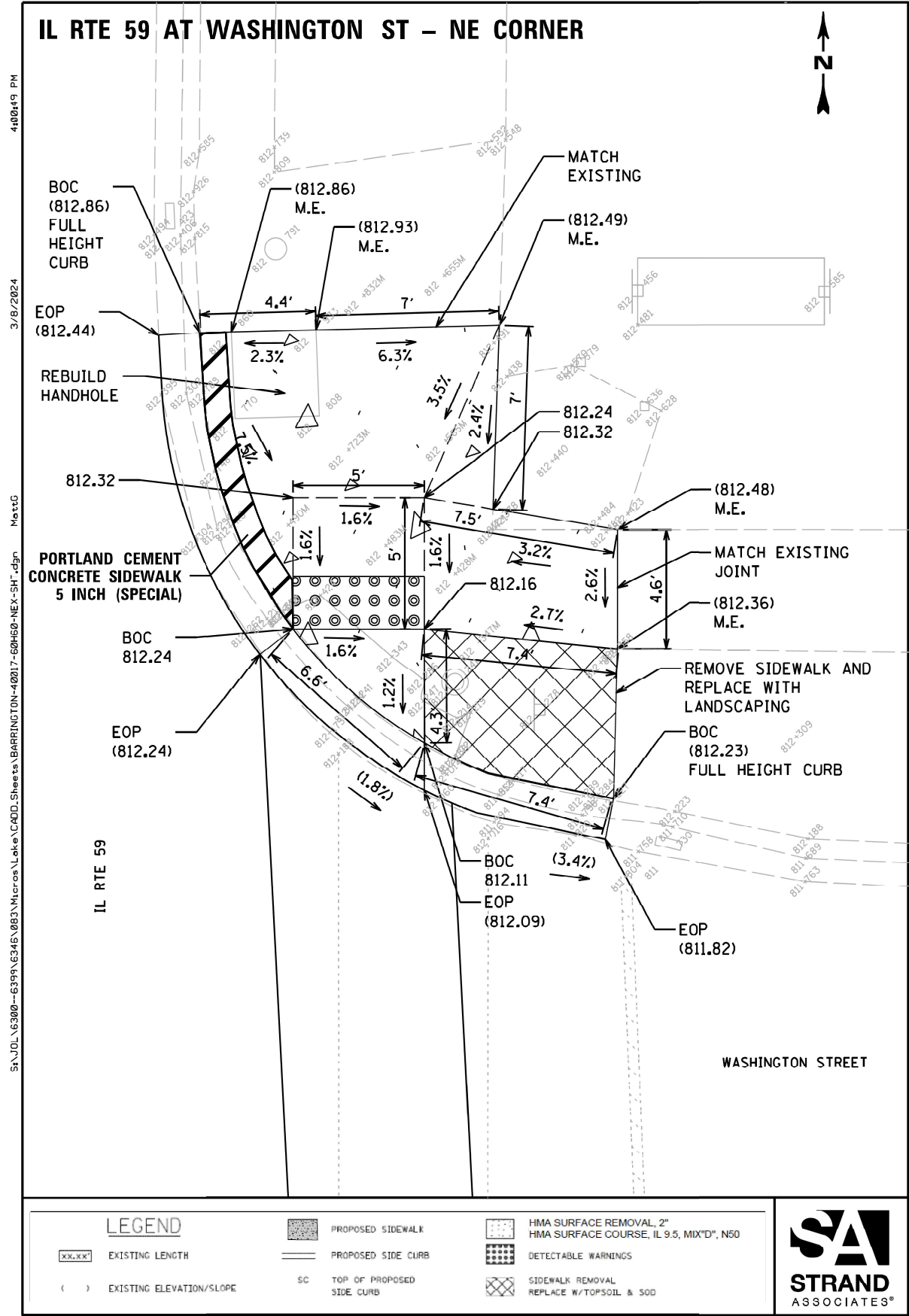
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS PROPOSED PLANS
IL ROUTE 59 AT WASHINGTON STREET AND AT LIONS DRIVE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	18
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

MODEL: Default
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DATE: 7/11/2024



NOTE: REMOVE ALL BRICK PAVERS FROM THE PROPOSED IMPROVEMENT AREA AND REPLACE THEM FULLY WITH CONCRETE. IF ANY NEW CONCRETE AREA ADJACENT TO EXISTING PAVER AREA, A SOLDIER COURSE OF PAVERS SHALL BE INSTALLED BETWEEN THE CONCRETE AND AGAINST CLEANLY SAWCUT PAVERS. THE EXCESS OF THE BRICK PAVERS SHALL BE DISPOSED OF OR TRANSFERRED BACK TO THE VILLAGE OF BARRINGTON. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE VILLAGE OF BARRINGTON REGARDING THE DISPOSAL OF THE BRICK PAVERS.

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
DRAWN -	REVISOR -	
PLOT SCALE = 100,000 * / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS
IL ROUTE 59 AND WASHINGTON STREET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	19
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

IL RTE 59 AT WASHINGTON ST
- SE CORNER

6:11:19 PM

3/8/2024

Met&G

S:\JOL\6300--6399\6346\083\MicroStation\CAD\Sheets\BARRING\ON-40017-60450-SEX-SHT.dgn

IL RTE 59

PORTLAND CEMENT
CONCRETE SIDEWALK
5 INCH (SPECIAL)

BOC
813.35
FULL
HEIGHT
CURB

BOC
812.56
2.5 INCH
TALL
CURB

BOC
813.45
EOP
(813.10)
M.E.

EOP
(813.03)
M.E.

EOP
(812.93)
M.E.

BOC
(813.52)
M.E.

LEGEND

XX.XX' EXISTING LENGTH

() EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK

PROPOSED SIDE CURB

SC TOP OF PROPOSED SIDE CURB

HMA SURFACE REMOVAL, 2" HMA SURFACE COURSE, IL 9.5, MIX"D", N50

DETECTABLE WARNINGS

SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS
IL ROUTE 59 AND WASHINGTON STREET

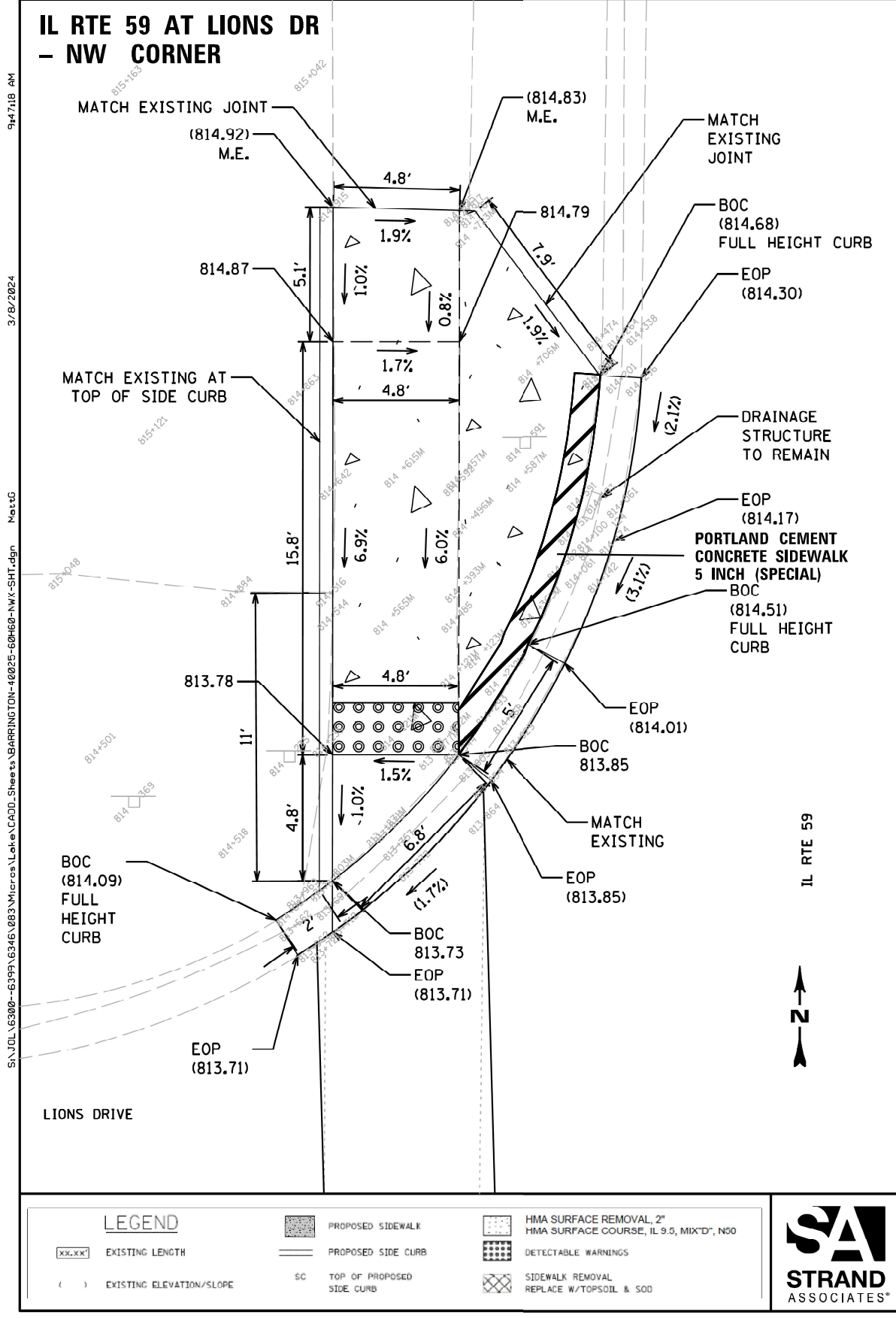
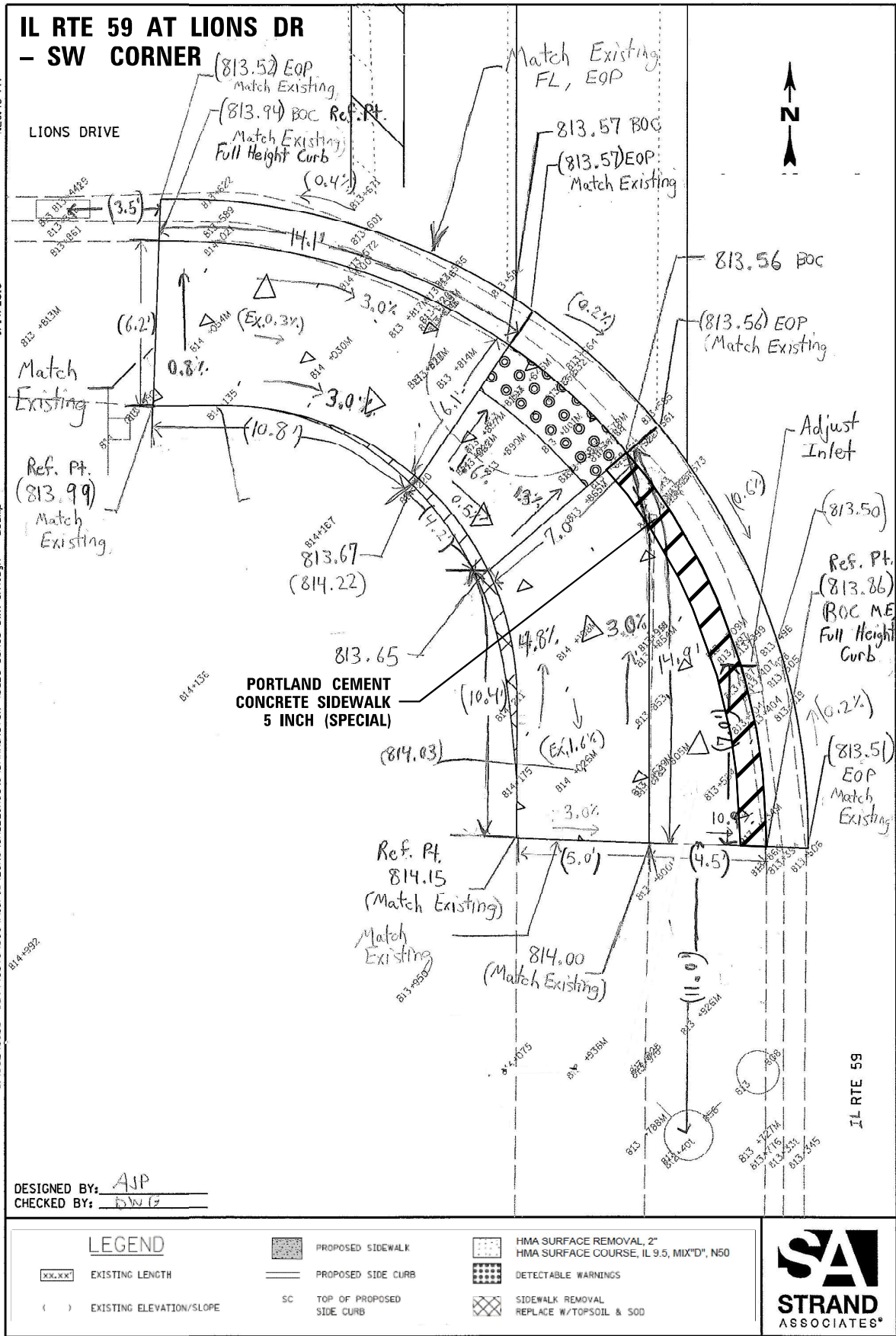
SCALE: SHEET OF SHEETS STA. TO STA.



NOTE: REMOVE ALL BRICK PAVERS FROM THE PROPOSED IMPROVEMENT AREA AND REPLACE THEM FULLY WITH CONCRETE. IF ANY NEW CONCRETE AREA ADJACENT TO EXISTING PAVER AREA, A SOLDIER COURSE OF PAVERS SHALL BE INSTALLED BETWEEN THE CONCRETE AND AGAINST CLEANLY SAWCUT PAVERS. THE EXCESS OF THE BRICK PAVERS SHALL BE DISPOSED OF OR TRANSFERRED BACK TO THE VILLAGE OF BARRINGTON. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE VILLAGE OF BARRINGTON REGARDING THE DISPOSAL OF THE BRICK PAVERS.

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
PLOT SCALE = 100,000' / in.	DRAWN -	REVISED -
PLOT DATE = 7/11/2024	CHECKED -	REVISED -
	DATE -	REVISED -

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	20
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				



NOTE: REMOVE ALL BRICK PAVERS FROM THE PROPOSED IMPROVEMENT AREA AND REPLACE THEM FULLY WITH CONCRETE. IF ANY NEW CONCRETE AREA ADJACENT TO EXISTING PAVER AREA, A SOLDIER COURSE OF PAVERS SHALL BE INSTALLED BETWEEN THE CONCRETE AND AGAINST CLEANLY SAWCUT PAVERS. THE EXCESS OF THE BRICK PAVERS SHALL BE DISPOSED OF OR TRANSFERRED BACK TO THE VILLAGE OF BARRINGTON. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE VILLAGE OF BARRINGTON REGARDING THE DISPOSAL OF THE BRICK PAVERS.

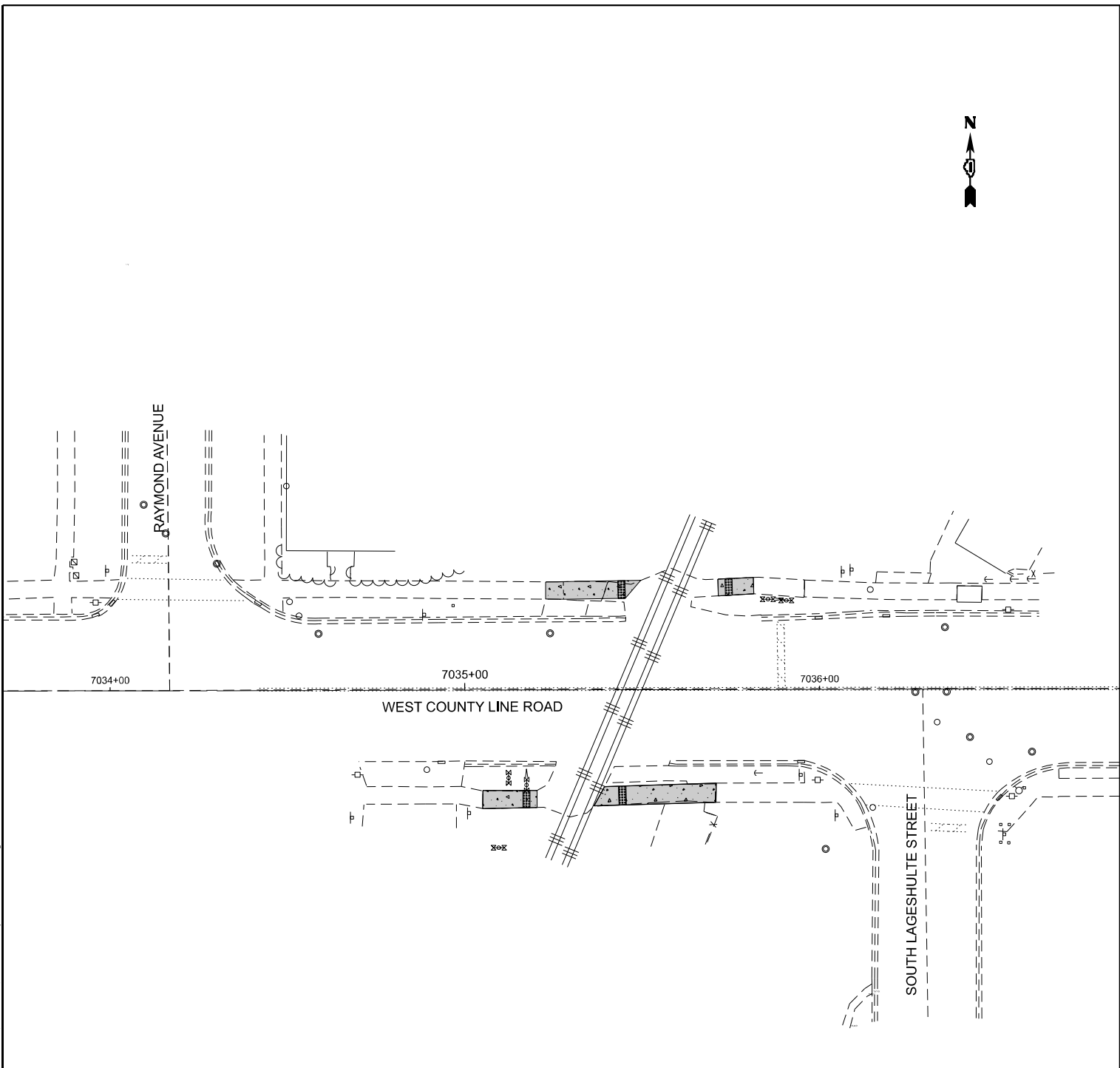
USER NAME = Rana,Kalo	DESIGNED -	REVISED -
PLOT SCALE = 100,000' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS IL ROUTE 59 AND LIONS DRIVE			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	21
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

Model: Default
FILE: \\net-cw.bentley.com\\P\\V\\DOT\\Documents\\DOT 0 cas\\District 1\\ORD Projects\\ID121021\\CADD\\Data\\CAD\\Sheets\\ID121021-Left-Info-SW Lake Caddn



REFERENCE BENCHMARK ELEVATION = 827.01
BENCHMARK = RECTANGULAR CUT IN TOP OF RECTANGULAR CONC SIGN BASE AT EAST END ASPH PKNG LOT
LOCATION = SOUTHEAST CORNER OF MAINT STREET AND SOUTH LAGESHULTE STREET

LEGEND

xx.xx'

EXISTING LENGTH

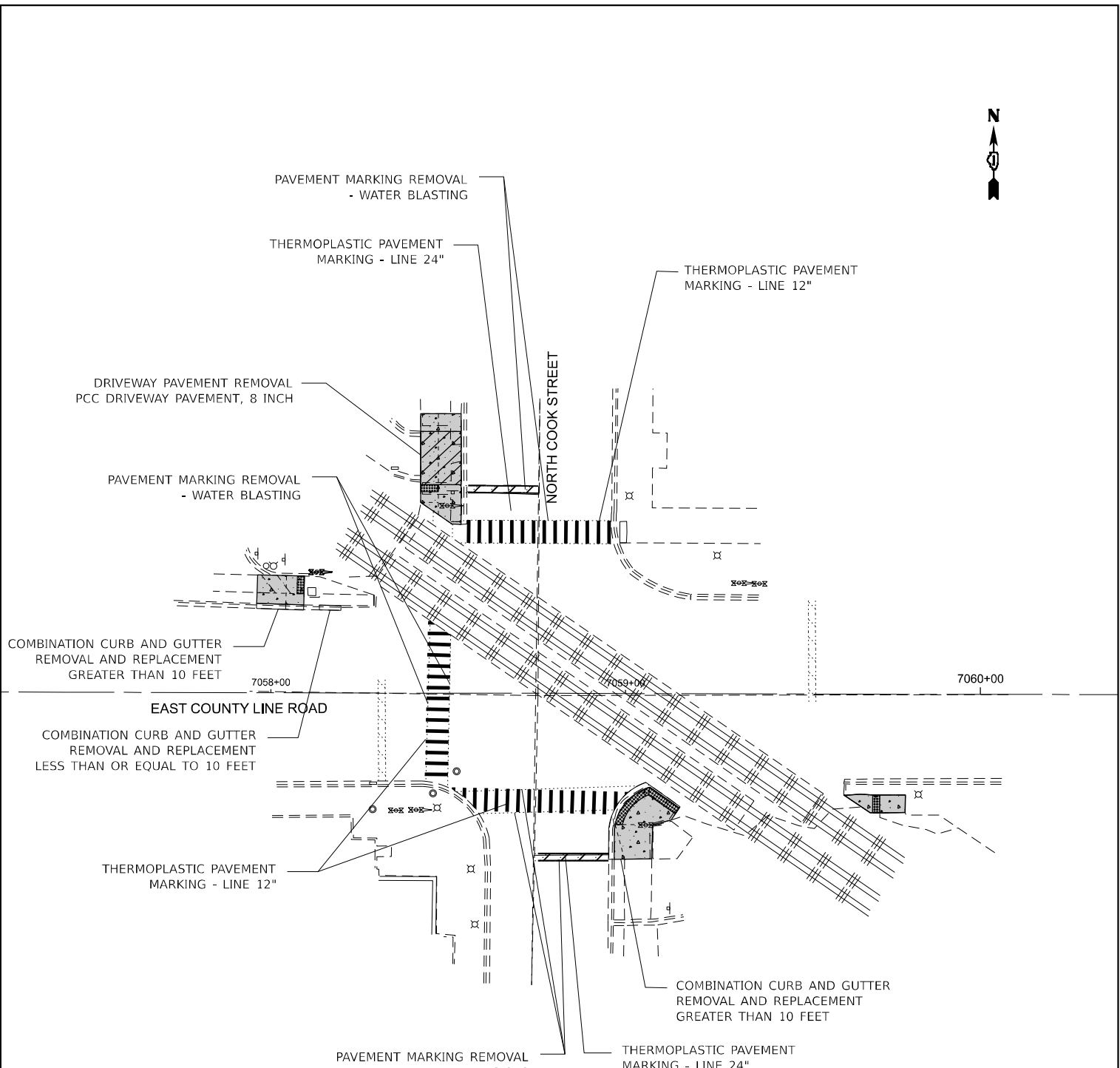
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EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK

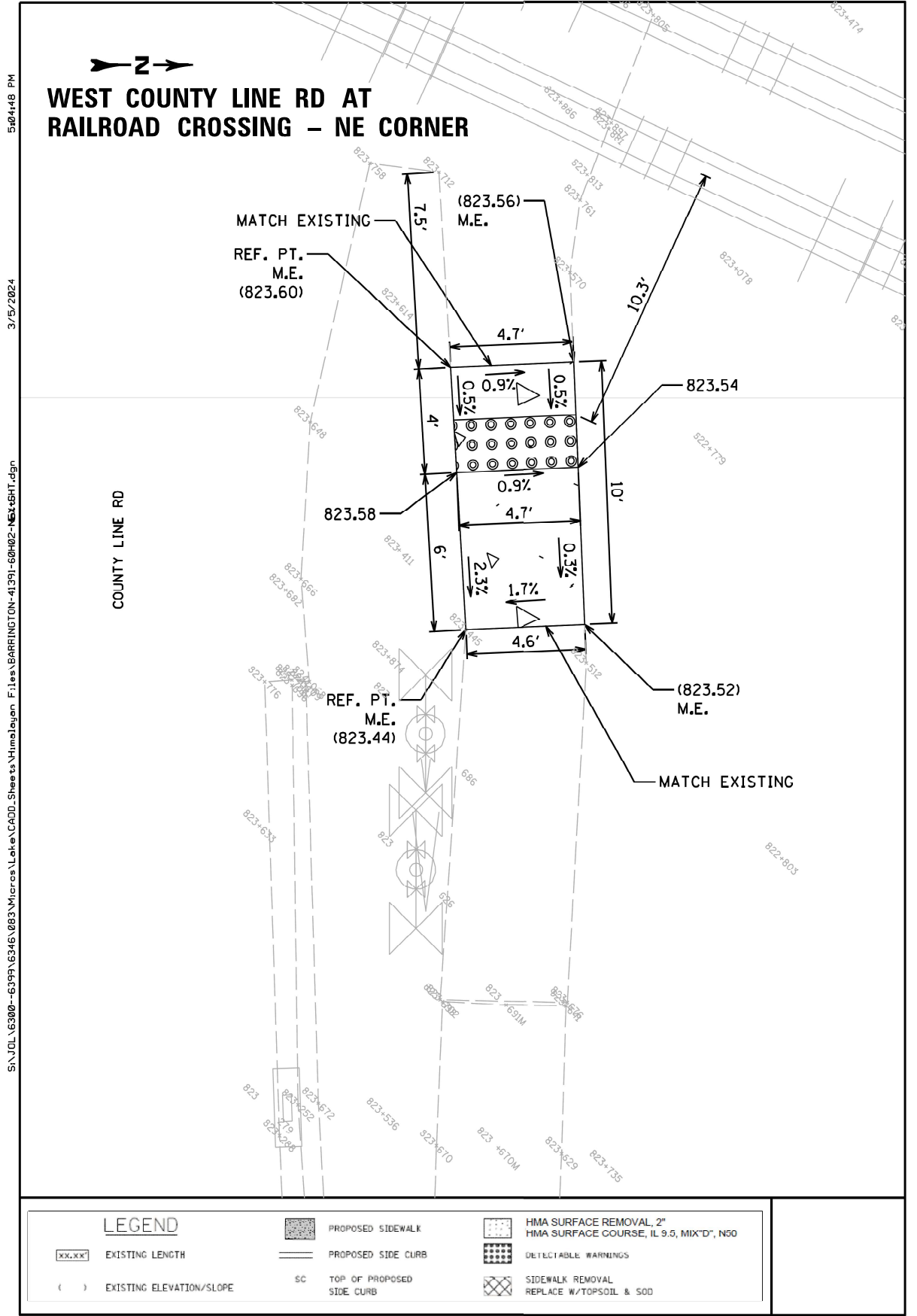
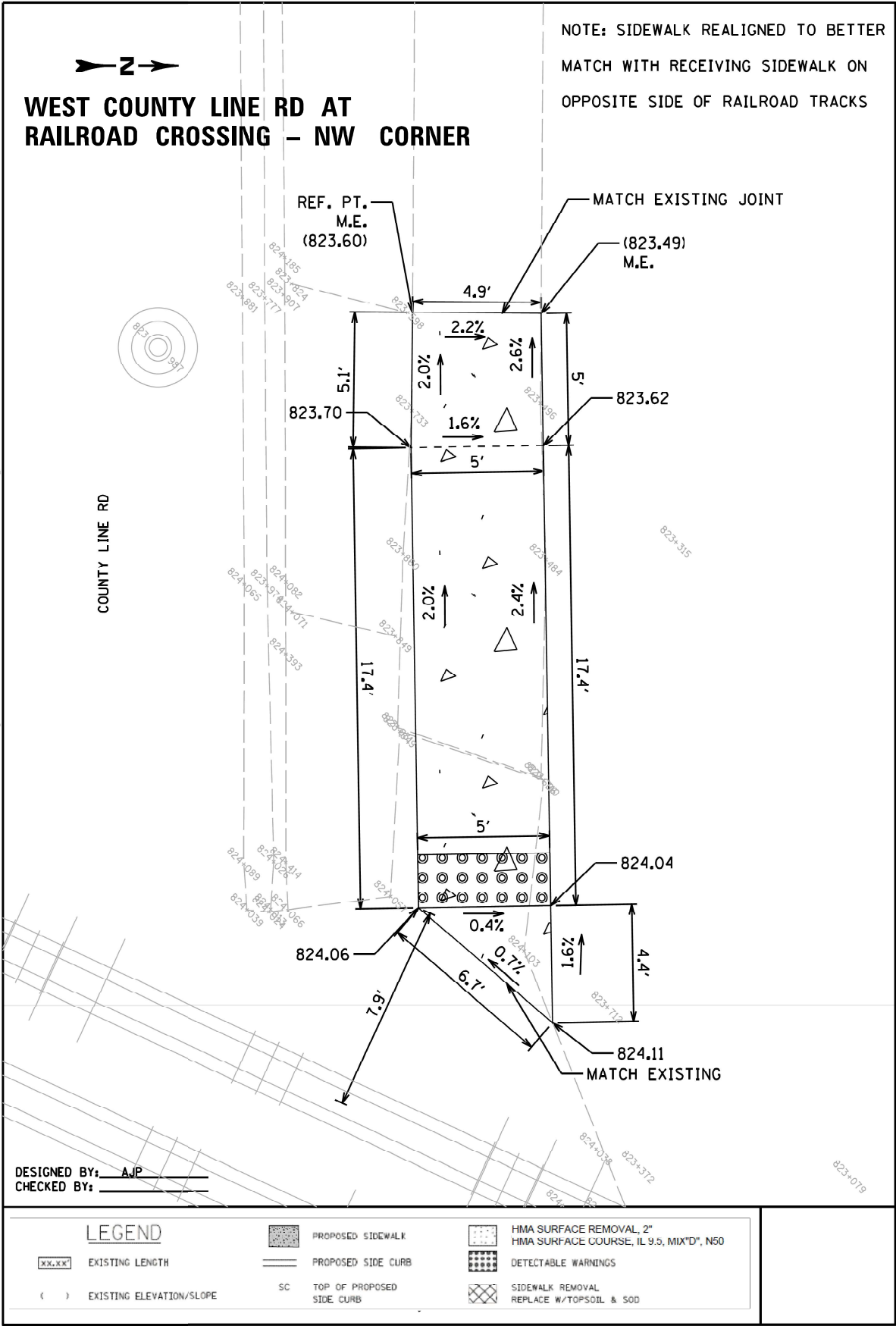
PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURBHMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, IL 9.5, MIX"D", N50DETECTABLE WARNINGSSIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEVATION = 830.24
BENCHMARK = NE BOLT FH AT PARKING END
LOCATION = NORTHEAST CORNER OF MAIN STREET AND NORTH COOK STREET

	USER NAME = Rana,Kalo		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMPS PROPOSED PLANS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -		CHECKED -	REVISED -		WEST COUNTY LINE RD AT RAILROAD CROSSING (EO RAYMOND AVE) AND EAST COUNTY LINE RD AT RAILROAD CROSSING (NORTH COOK ST)				VAR.	2021-123-SW	LAKE	69	26
	PLOT SCALE = 40,000 ' / in.		DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 62P40				
	PLOT DATE = 7/11/2024									ILLINOIS FED. AID PROJECT				



MODEL: Default
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USER NAME	= Rana,Kalo	DESIGNED -	REVISED -
DRAWN -		REVISOR -	
PLOT SCALE = 100,000' = 1 in.		CHECKED -	REVISED -
PLOT DATE = 7/11/2024		DATE -	REVISED -

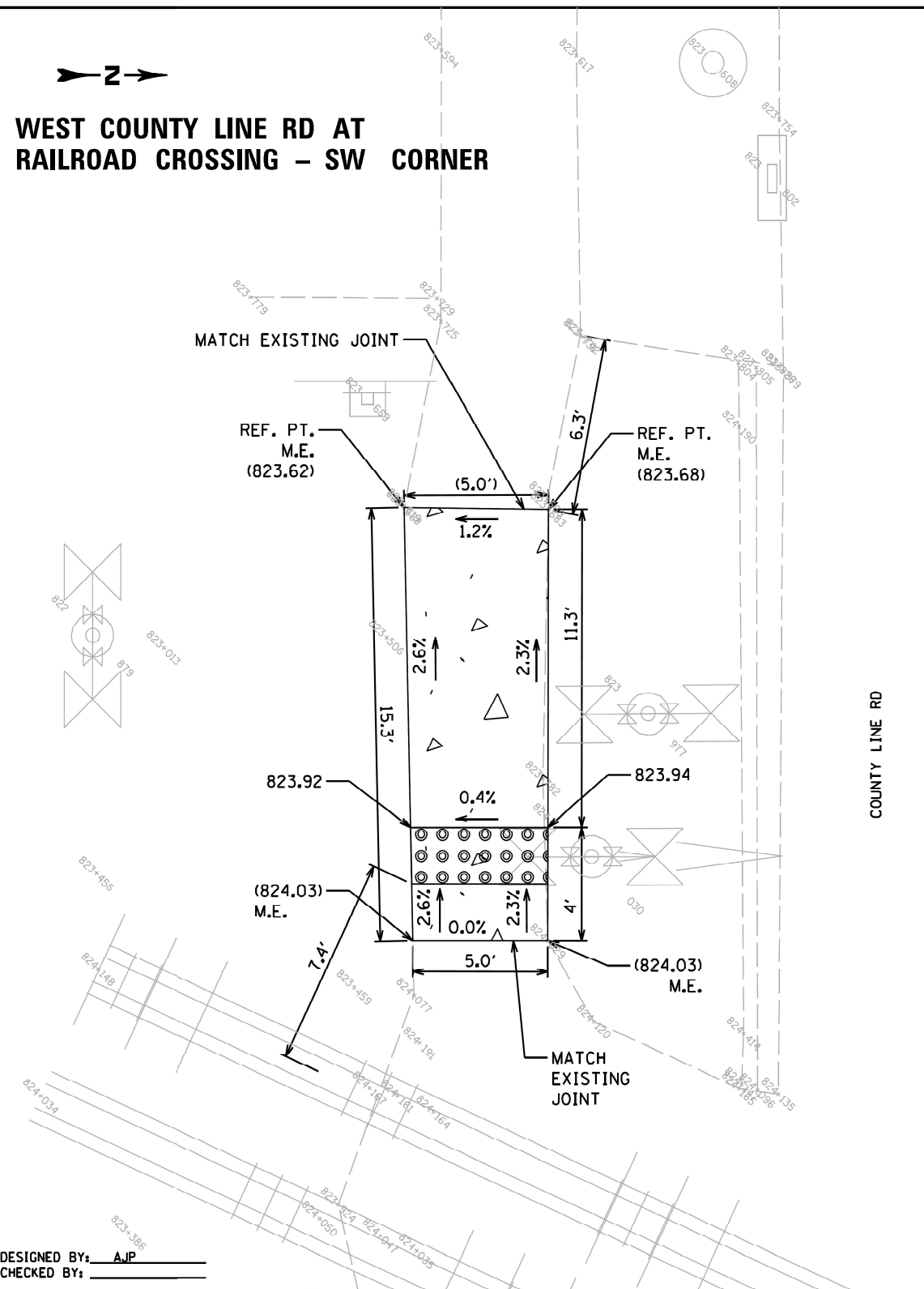
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS			
WEST COUNTY LINE RD AND RAILROAD CROSSING (E/O RAYMOND AVE)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	27
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

5:58 PM 6/30/2006 1:39:50 PM C:\Program Files\Microsoft Office\Office11\Excel.exe



**WEST COUNTY LINE RD AT
RAILROAD CROSSING – SW CORNER**



DESIGNED BY: AJP
CHECKED BY: _____

LEGEND

() EXISTING ELEVATION/SLOPE

 PROPOSED SIDEWALK
 PROPOSED SIDE CURB
 SC TOP OF PROPOSED
 SIDE CURB

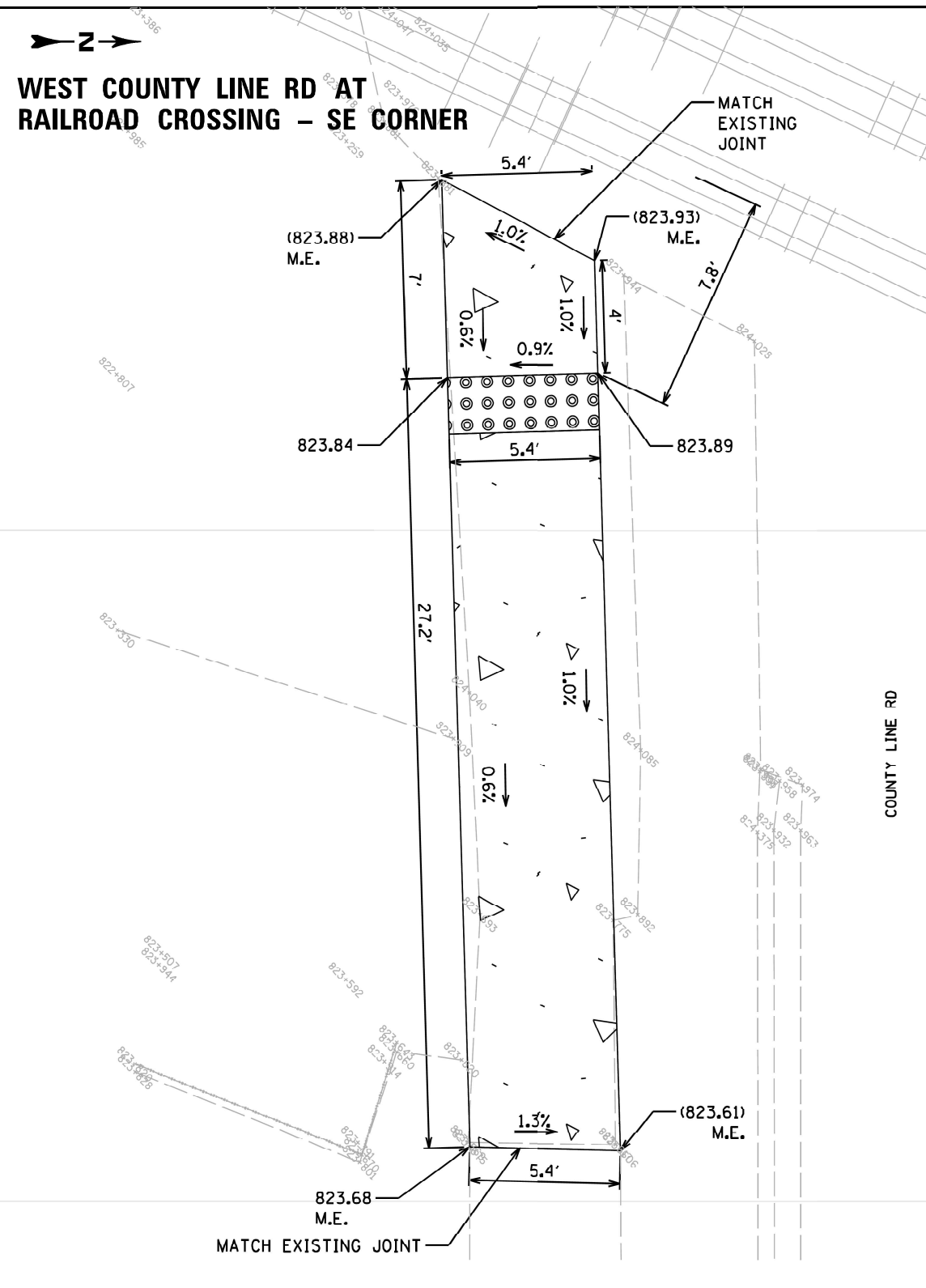
	HMA SURFACE REMOVAL, 2"
	HMA SURFACE COURSE, IL 9.5, MIX"D", N50
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL
	REPLACE W/TOPSOIL & SOD

COUNTY LINE RD

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

55\JUL\6300--6399\6346\083\Micros\Himalayan Files\BARRINGTON-41391-60H02-55x5HT.dgn



**WEST COUNTY LINE RD AT
RAILROAD CROSSING - SE CORNER**



MATCH EXISTING JOINT

LEGEND

EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE

 PROPOSED SIDEWALK
 PROPOSED SIDE CURB
 SC TOP OF PROPOSED SIDE CURB

 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, IL 9.5, MIX "D", N50

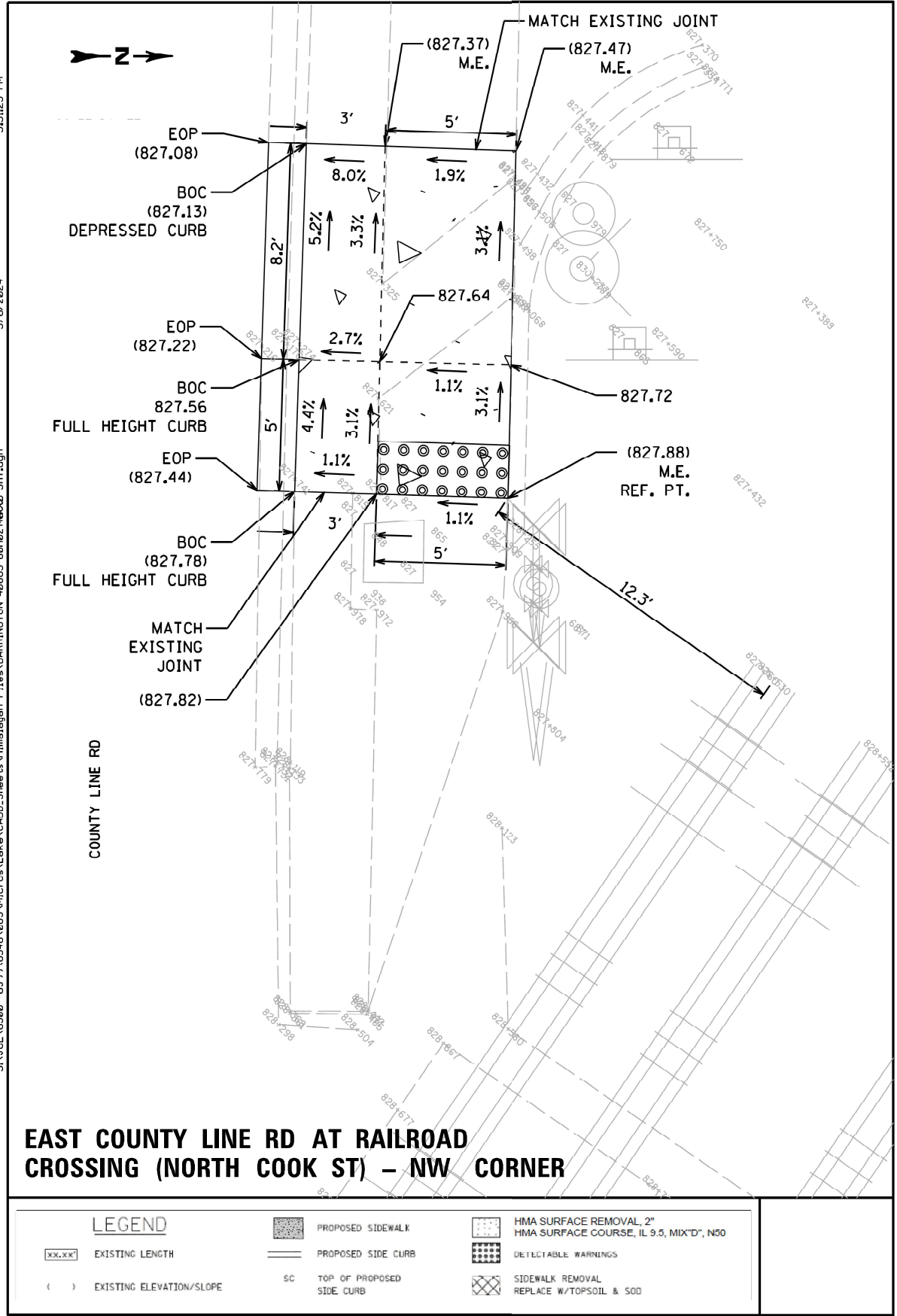
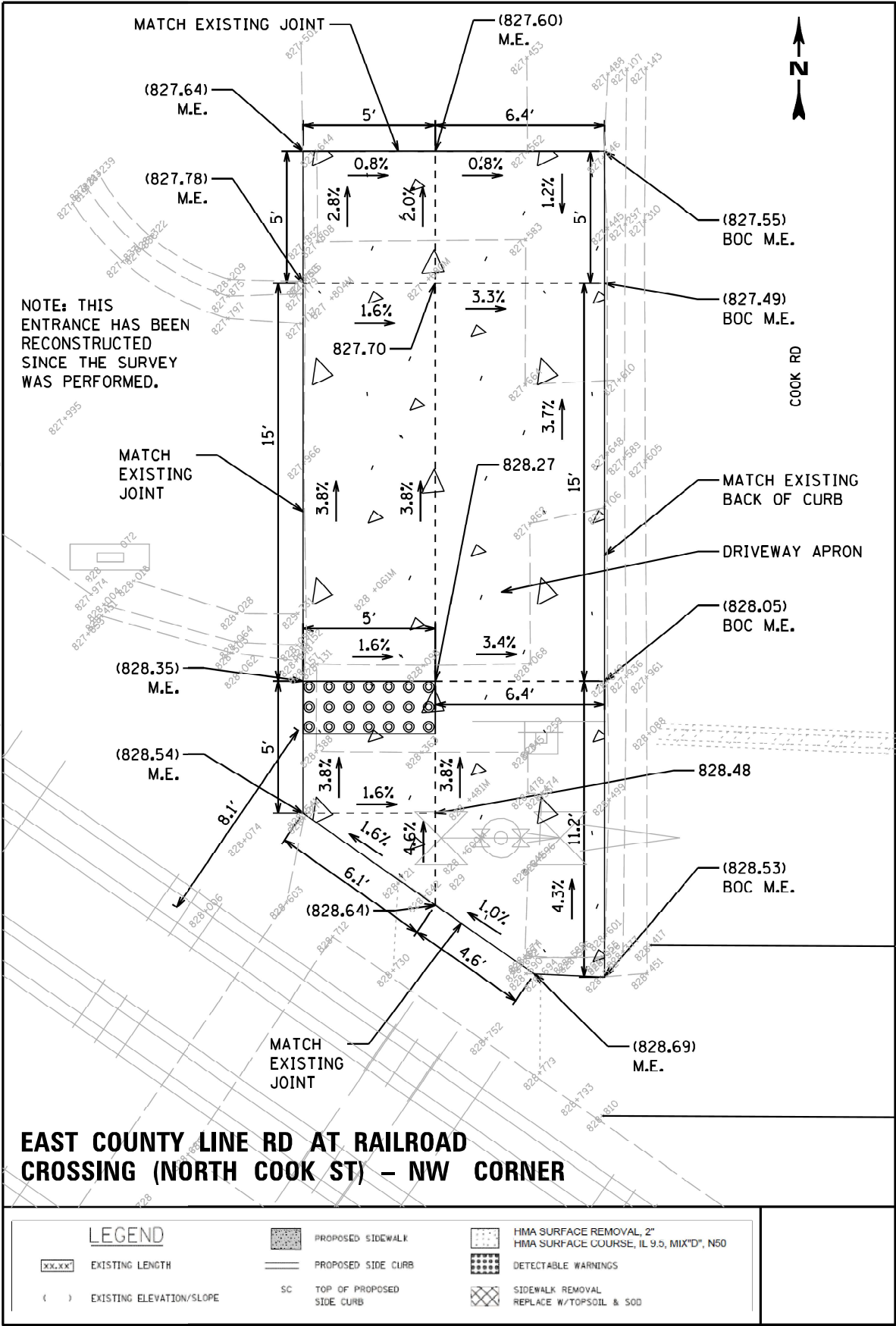
 DETECTABLE WARNINGS

 SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

COUNTY LINE RD

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

ADA RAMPS EXISTING AND PROPOSED PLANS WEST COUNTY LINE RD AND RAILROAD CROSSING (E/O RAYMOND AVE)				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				VAR.	2021-123-SW	LAKE	69	28
				CONTRACT NO. 62P40				
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			
					ILLINOIS	FED. AID PROJECT		



NOTE: REMOVE ALL BRICK PAVERS FROM THE PROPOSED IMPROVEMENT AREA AND REPLACE THEM FULLY WITH CONCRETE. IF ANY NEW CONCRETE AREA ADJACENT TO EXISTING PAVER AREA, A SOLDIER COURSE OF PAVERS SHALL BE INSTALLED BETWEEN THE CONCRETE AND AGAINST CLEANLY SAWCUT PAVERS. THE EXCESS OF THE BRICK PAVERS SHALL BE DISPOSED OF OR TRANSFERRED BACK TO THE VILLAGE OF BARRINGTON. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE VILLAGE OF BARRINGTON REGARDING THE DISPOSAL OF THE BRICK PAVERS.

USER NAME	= Rana,Kalo	DESIGNED	-	REVISED	-
DRAWN	-	CHECKED	-	REVISED	-
PLOT SCALE	= 100,000' = 1 in.	DATE	-	REVISED	-
PLOT DATE	= 7/11/2024				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS EXISTING AND PROPOSED PLANS
EAST COUNTY LINE RD AND RAILROAD CROSSING (NORTH COOK ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	29
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				



- LEGEND:**

- STATE OF ILLINOIS**
DEPARTMENT OF TRANSPORTATION

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	32
		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

MODEL: Default
FILE NAME: pww:///ldot-pw.bentley.com/PW/IDOT/Documents/IDOT Offices/District 1/ORD Projects/D121021/CADDData/CADsheets/D121021-sht-plan-NE Lake Co.dgn



LOCATION = NORTHWEST CORNER OF IL 120 AND RUTH WILCOX AVENUE

LEGEND:

- STATE OF ILLINOIS**
DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS

IL 120 AT KNIGHT AVENUE/RUTH WILCOX AVENUE, OLD PLANK ROAD AT RUTH WILCOX AVENUE

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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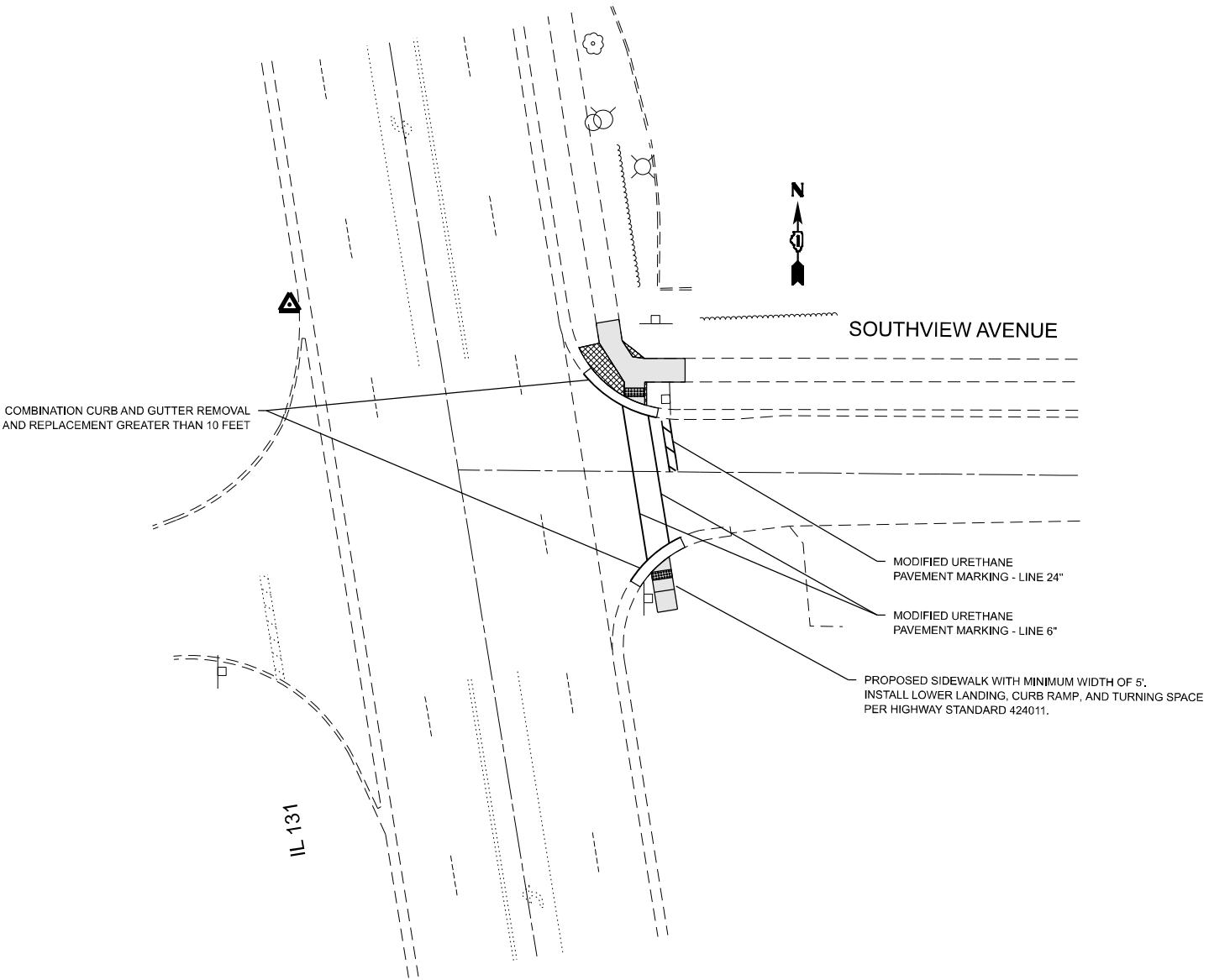
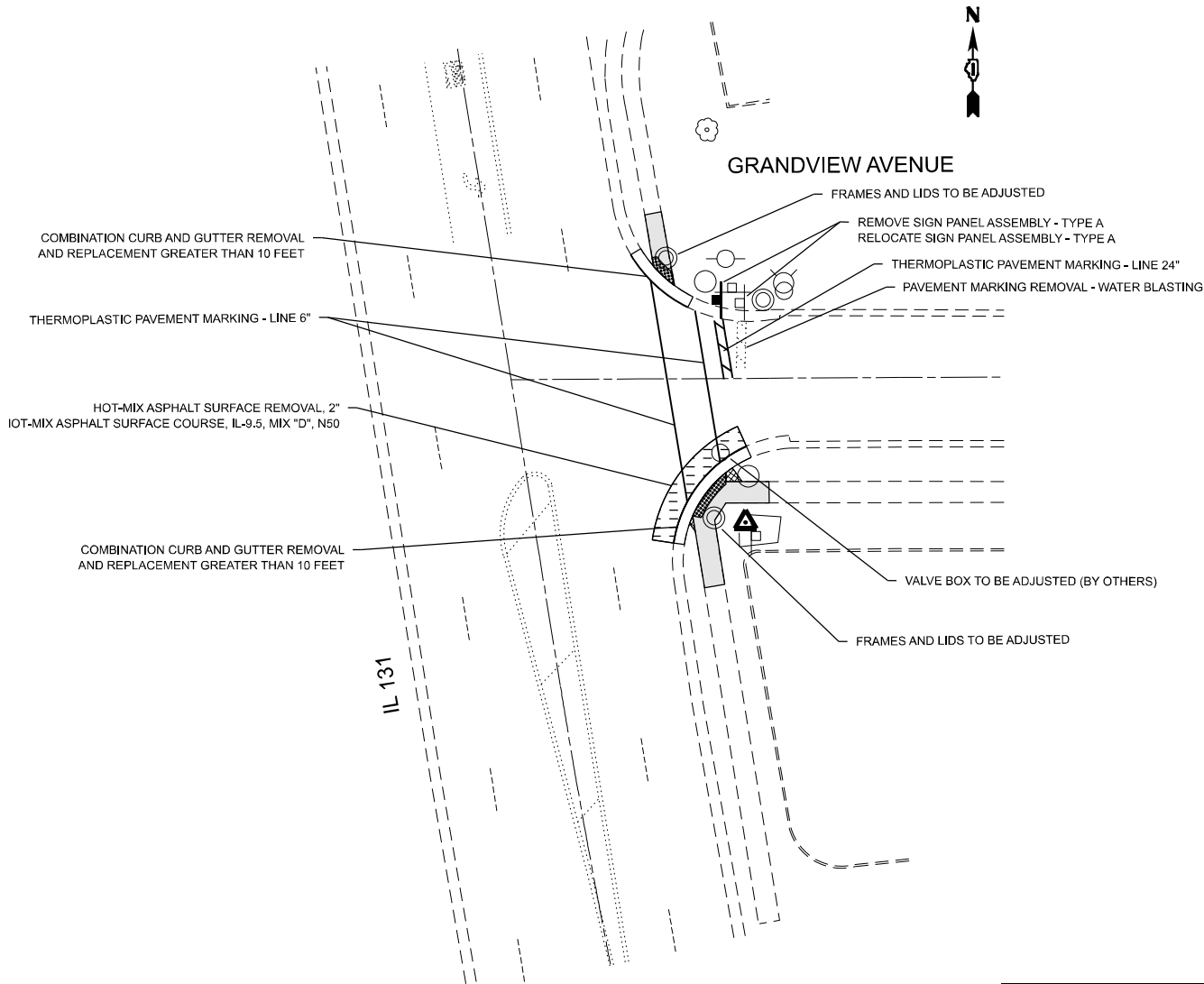
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	33
		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	34
		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	

IL 131 AT GRANDVIEW AVENUE

IL 131 AT SOUTHVIEW AVENUE



LEGEND:

- PROPOSED SIDEWALK
-
- PROPOSED SIDE CURB

()

BENCHMARK INFORMATION:

REFERENCE BENCHMARK ELEVATION = 714.03

BENCHMARK = CHISELED SQUARE CUT ON THE NORTHWEST CORNER OF CONCRETE SIGN FOUNDATION

LOCATION = SOUTHEAST CORNER OF IL 131 AND GRANDVIEW AVENUE

BENCHMARK INFORMATION:

REFERENCE BENCHMARK ELEVATION = 713.14

BENCHMARK = CHISELED SQUARE CUT ON THE SOUTHEAST CORNER OF THE TRAFFIC SIGNAL HANDHOLE

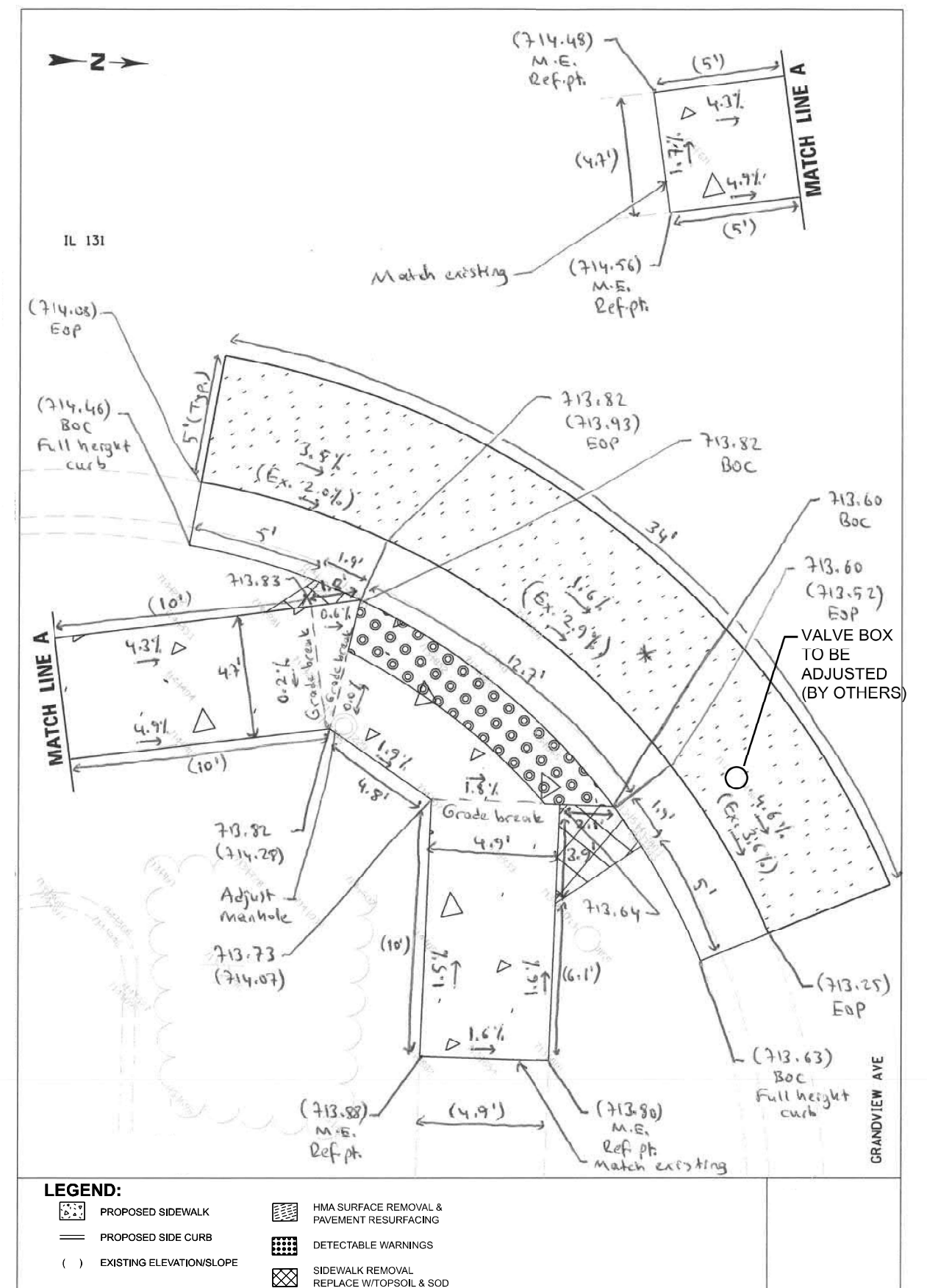
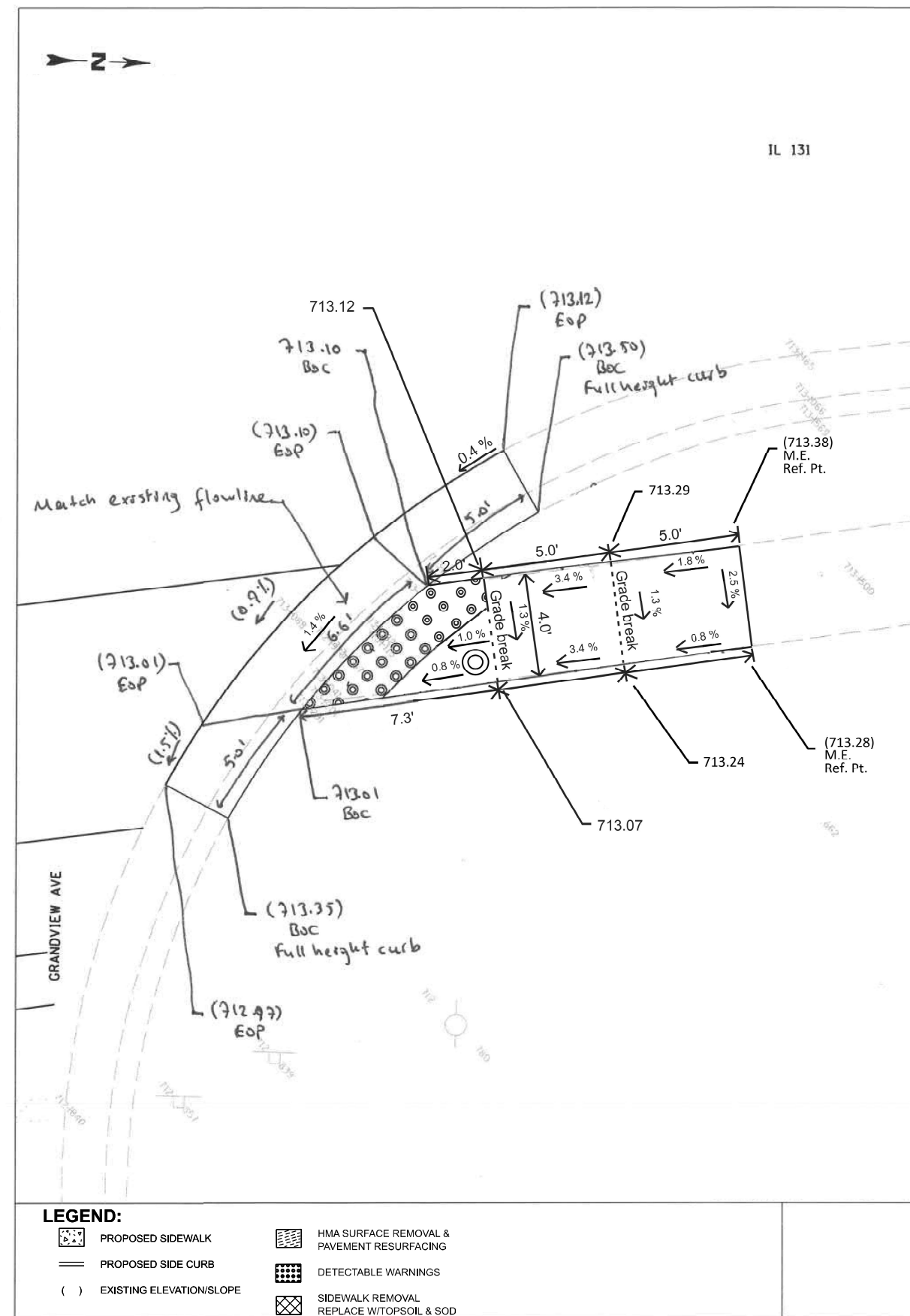
LOCATION = NORTHWEST CORNER OF IL 131 AND SOUTHVIEW AVENUE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS
IL 131 AT GRANDVIEW AVENUE AND AT SOUTHVIEW AVENUE

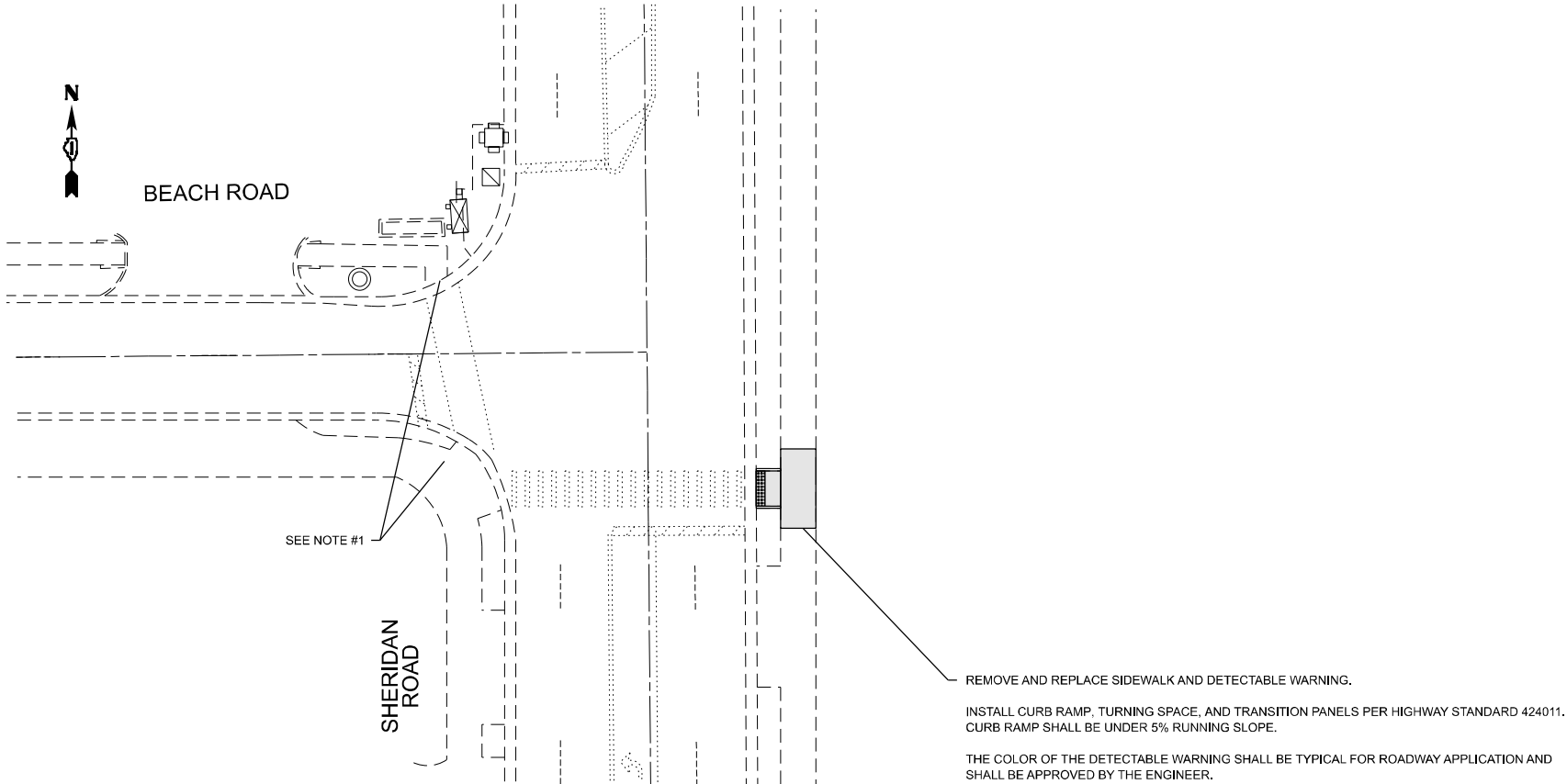
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	35
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				










SHERIDAN ROAD AT BEACH ROAD



NOTE:

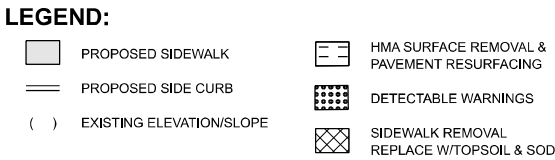
1. EXISTING SIDEWALK/ISLANDS/MEDIANS AND PAVEMENT MARKING IMPACTED DUE TO PROPOSED TRAFFIC SIGNAL WORK SHALL BE RESTORED TO EXISTING CONDITIONS. GRADES AND ELEVATIONS OF RESTORED SIDEWALK/ISLANDS/MEDIANS SHALL MATCH EXISTING.

LEGEND:

- | | | | |
|---|--------------------------|---|---|
|  | PROPOSED SIDEWALK |  | HMA SURFACE REMOVAL & PAVEMENT RESURFACING |
| | PROPOSED SIDE CURB |  | DETECTABLE WARNINGS |
|  | EXISTING ELEVATION/SLOPE |  | SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD |

	USER NAME = Farhan,Tariq	DESIGNED -	REVISED - FT, 7/19/2024	<div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div>	CURB RAMP DETAILS					F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -		SHERIDAN ROAD AT BEACH ROAD					VAR.	2021-123-SW	LAKE	69	38
	PLOT SCALE = 40,000 ' / in.	CHECKED -	REVISED -							CONTRACT NO. 62P40				
	PLOT DATE = 7/19/2024	DATE -	REVISED -											
					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.				

MODEL: Default
FILE NAME: pw://lido-pw.bentley.com/PW/IDOT/Documents/IDOT Offices/District 1/ORD Projects/D121021/CADData/CADsheets/D121021-sst-plbn-NE Lake Co.dgn



REFERENCE BENCHMARK ELEVATION = 632.42

BENCHMARK = CHISELED SQUARE CUT ON THE WEST SIDE OF BODY SHOP SIGN

LOCATION = NORTHWEST CORNER OF SHERIDAN ROAD AND 6TH STREET

USER NAME = Farhan,Tariq	DESIGNED -	REVISED - FT, 7/19/2024
	DRAWN -	REVISED -
PLOT SCALE = 40,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 7/19/2024	DATE -	REVISED -

CURB RAMP DETAILS
AT 6TH STREET AND AT PARK AVENUE

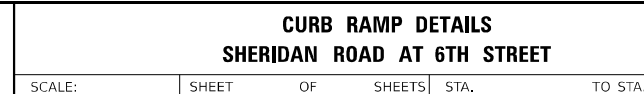
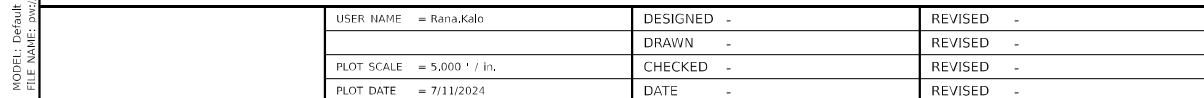
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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REFERENCE BENCHMARK ELEVATION = 632.24

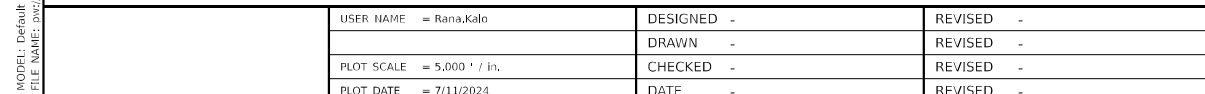
BENCHMARK = CHISELED SQUARE CUT ON THE TOP OF CURB

LOCATION = SOUTH SIDE OF PARK AVENUE IN FRONT OF STOP SIGN ON THE SOUTHWEST CORNER OF SHERIDAN ROAD AND PARK AVENUE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	39
		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	

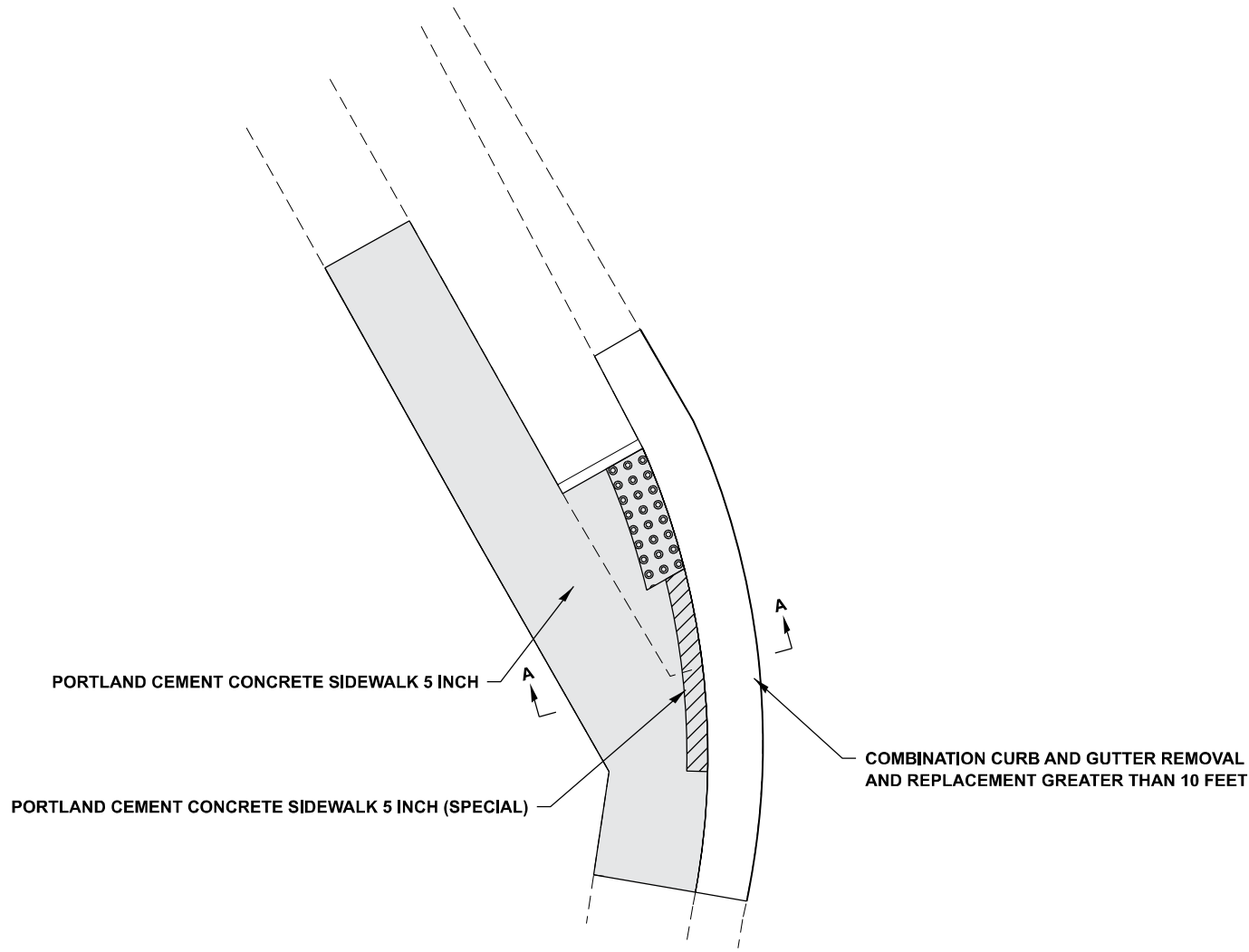


F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	40
		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

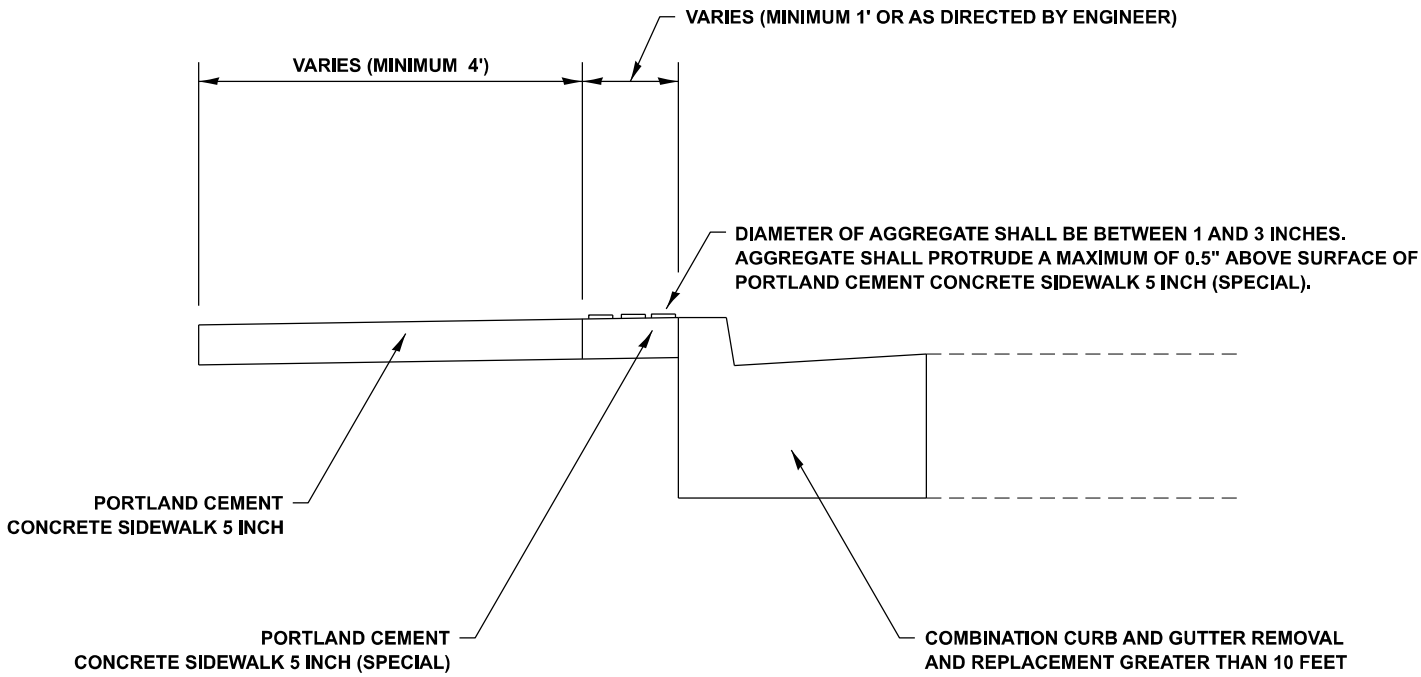


F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	41
		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

Model: Default
File Name: s:\projects\2021-123-SW\2021-123-SW.dwg
User: Farhan, Tariq
Date: 7/11/2024
Scale: 1" = 100'-0"



TYPICAL PLAN FOR PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)



SECTION A-A

USER NAME = Farhan, Tariq	DESIGNED -	REVISED -
DRAWN -	REVISED -	
PLOT SCALE = 100,000 * / in.	CHECKED -	REVISED -
PLOT DATE = 7/11/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**






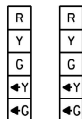








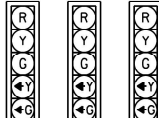
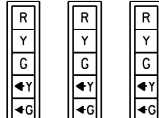








































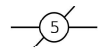







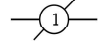



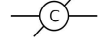





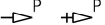

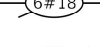

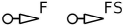

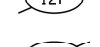

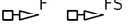

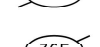











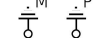























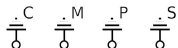
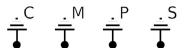










**DETAIL FOR PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)
VARIOUS ROUTES – VARIOUS LOCATIONS**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. * RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	42
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

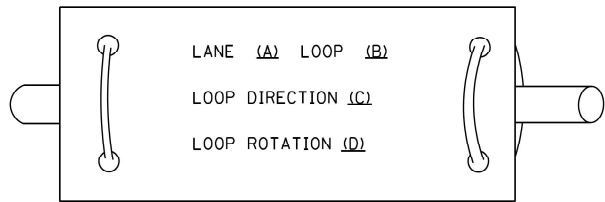
ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND					
MASTER CONTROLLER			DOUBLE HANDHOLE			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER MASTER CONTROLLER			JUNCTION BOX					
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM					
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL					
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE					
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM	S	SP	NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
WOOD POLE			INTERSECTION ITEM	I	IP	GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
GUY WIKE			REMOVE ITEM		R	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
SIGNAL HEAD			RELOCATE ITEM		RL	COAXIAL CABLE		
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM		A	VENDOR CABLE		
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF	COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF	FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
			SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF			
PEDESTRIAN SIGNAL HEAD			DETECTOR LOOP, TYPE I					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			PREFORMED DETECTOR LOOP					
RADAR DETECTION SENSOR			SAMPLING (SYSTEM) DETECTOR					
VIDEO DETECTION CAMERA			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			WIRELESS DETECTOR SENSOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS ACCESS POINT					
CONFIMATION BEACON								
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	STANDARD TRAFFIC SIGNAL DESIGN DETAILS		VAR.	2021-123-SW	LAKE	69	43		
			TS-05		CONTRACT NO. 62P40				
	SCALE: NONE	SHEET 1 OF 7 SHEETS	STA.	TO STA.					
			ILLINOIS FED. AID PROJECT						

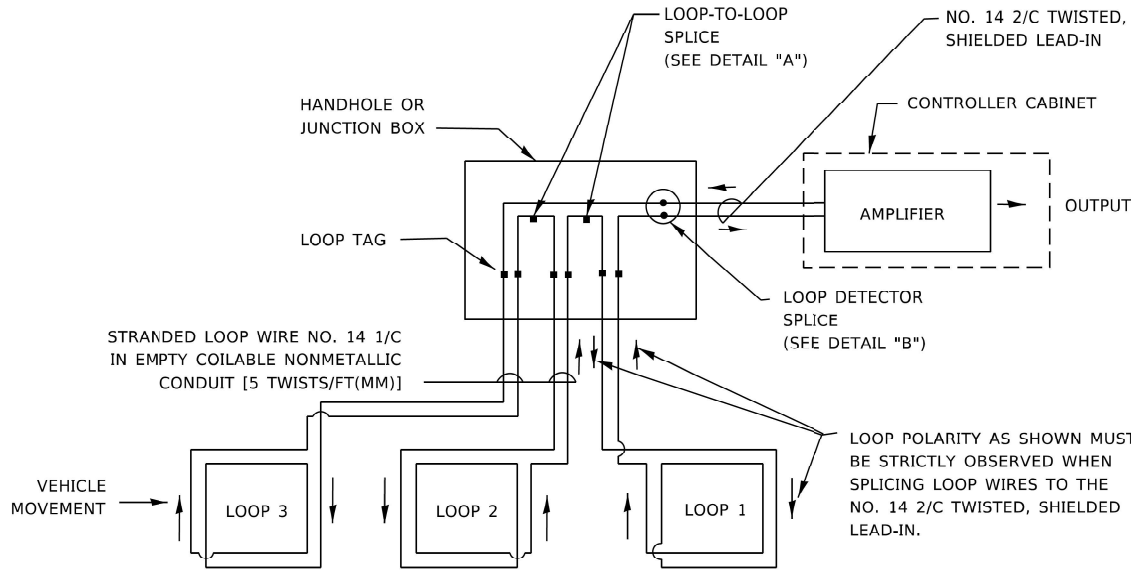
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

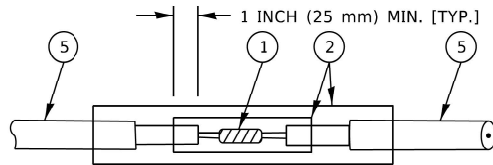


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

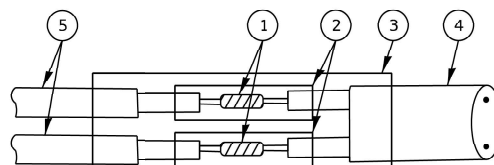


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

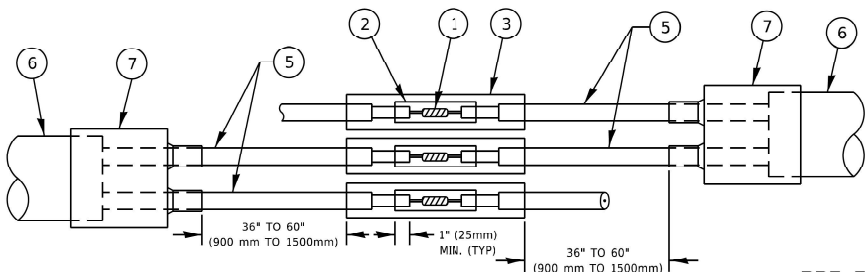


DETAIL "A"
LOOP-TO-LOOP SPLICE

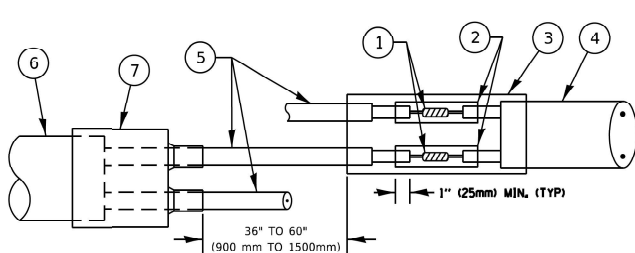


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
- ⑥ XL POLYOLEFIN 2 CONDUCTOR
- ⑦ BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

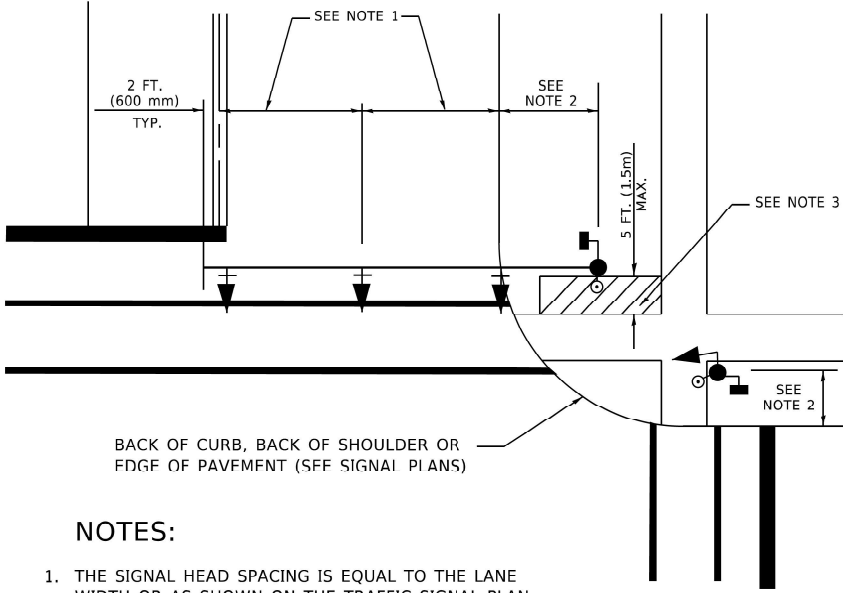
SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	44
TS-05		CONTRACT NO. 62P40		
		ILLINOIS FED. AID PROJECT		

MODEL Default
FILE NAME P102_2109_002_IDOT_PFB_173_12 Traffic Signal Design\Design\013-AP5 Upgrades\CADD\CADD_Sheets\DT22P40-Detail_TS01-1-003.dgn

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



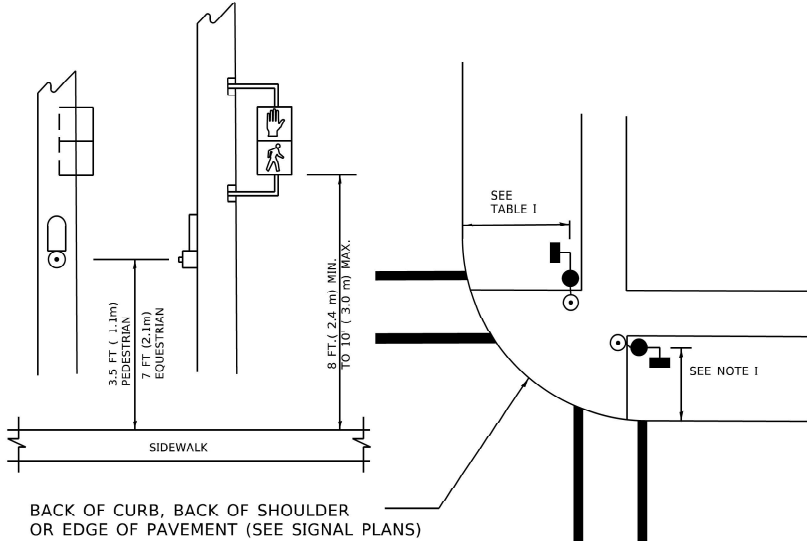
NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST

AND

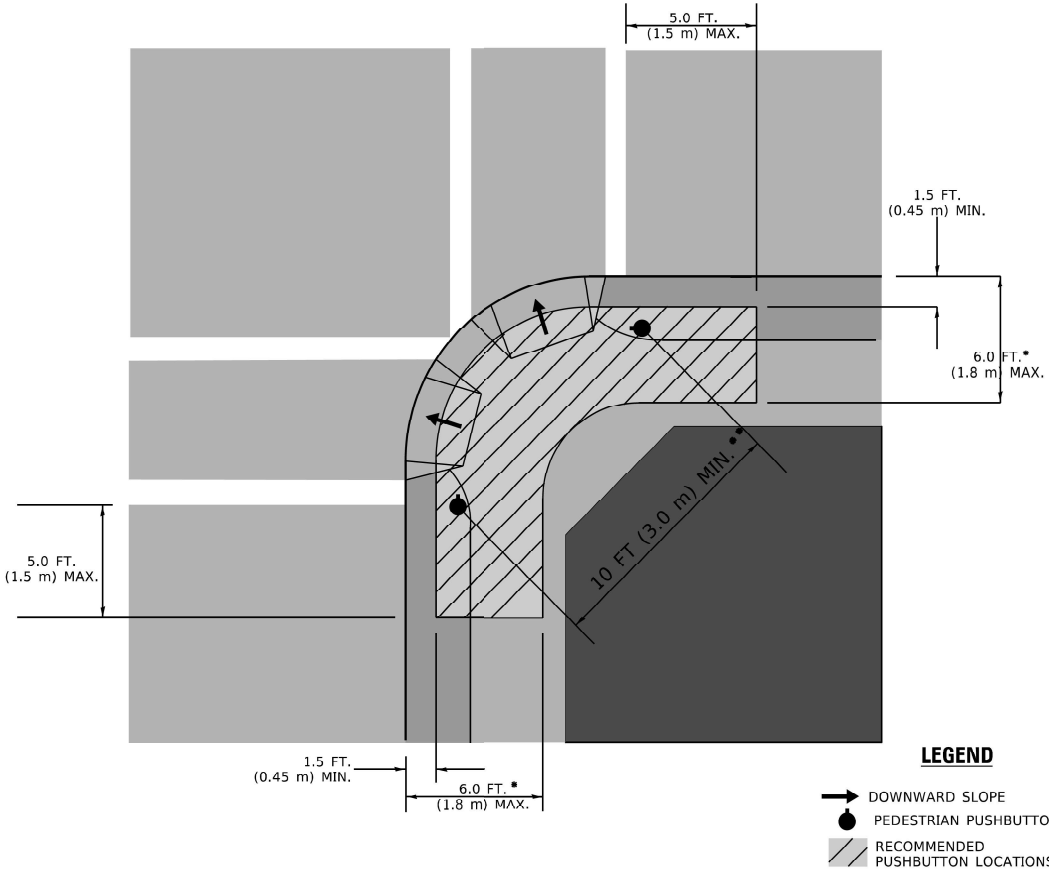
PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RIATFD ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

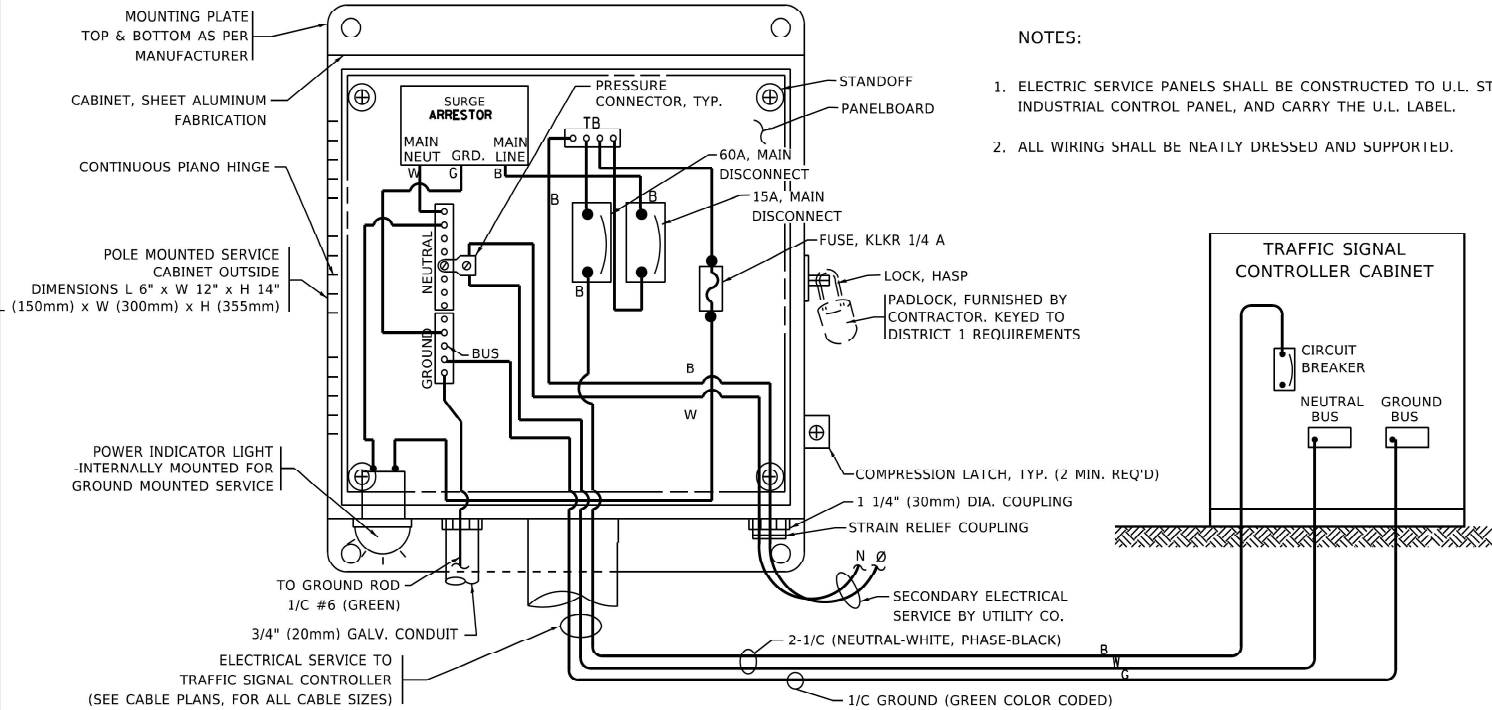
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

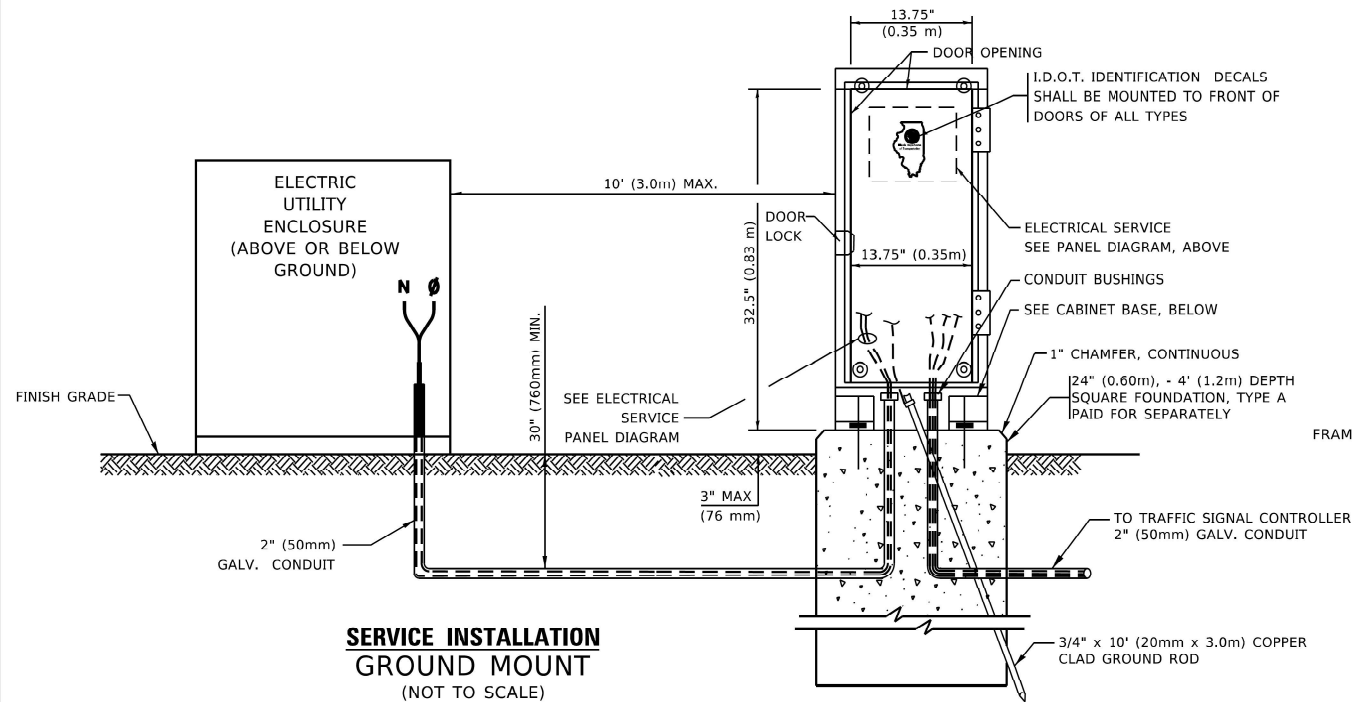
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 3 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	45
TS-05		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

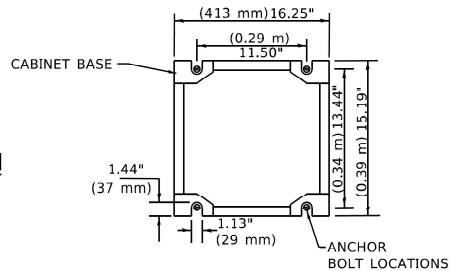


ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



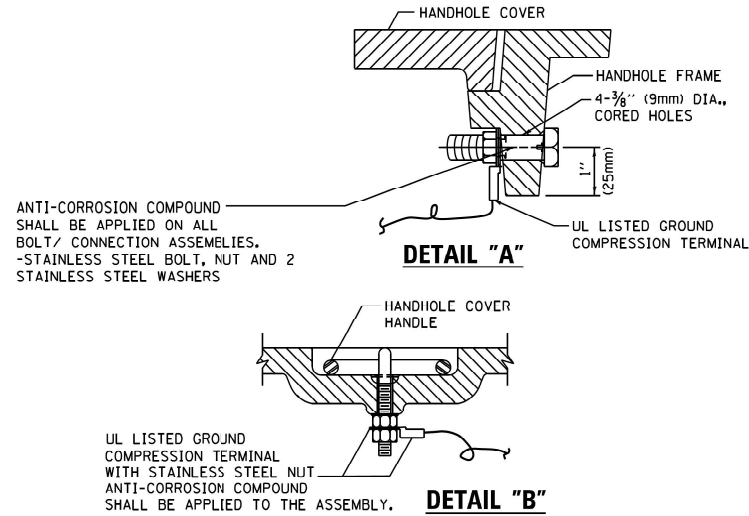
SERVICE INSTALLATION
GROUND MOUNT
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN
(NOT TO SCALE)

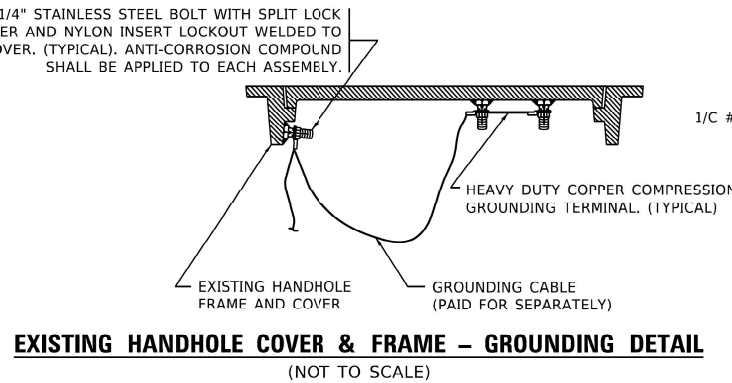


NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



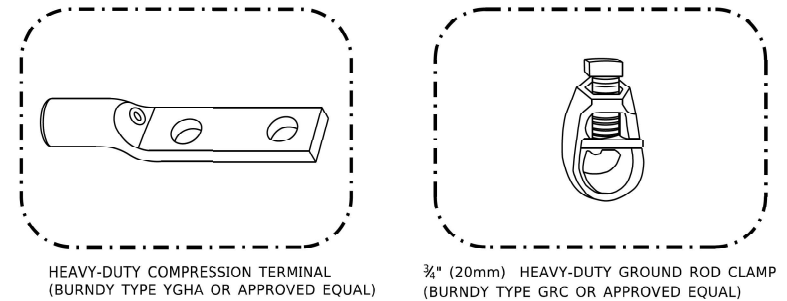
HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)

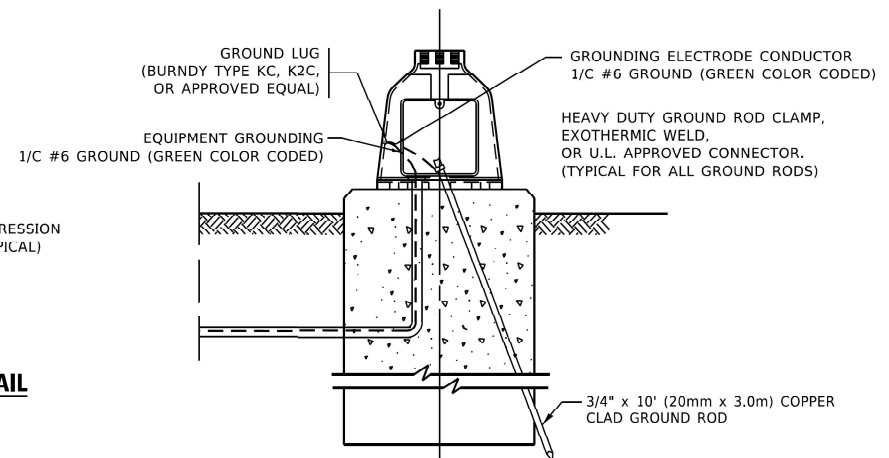
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC. ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE /POST-GROUNDING DETAIL
(NOT TO SCALE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

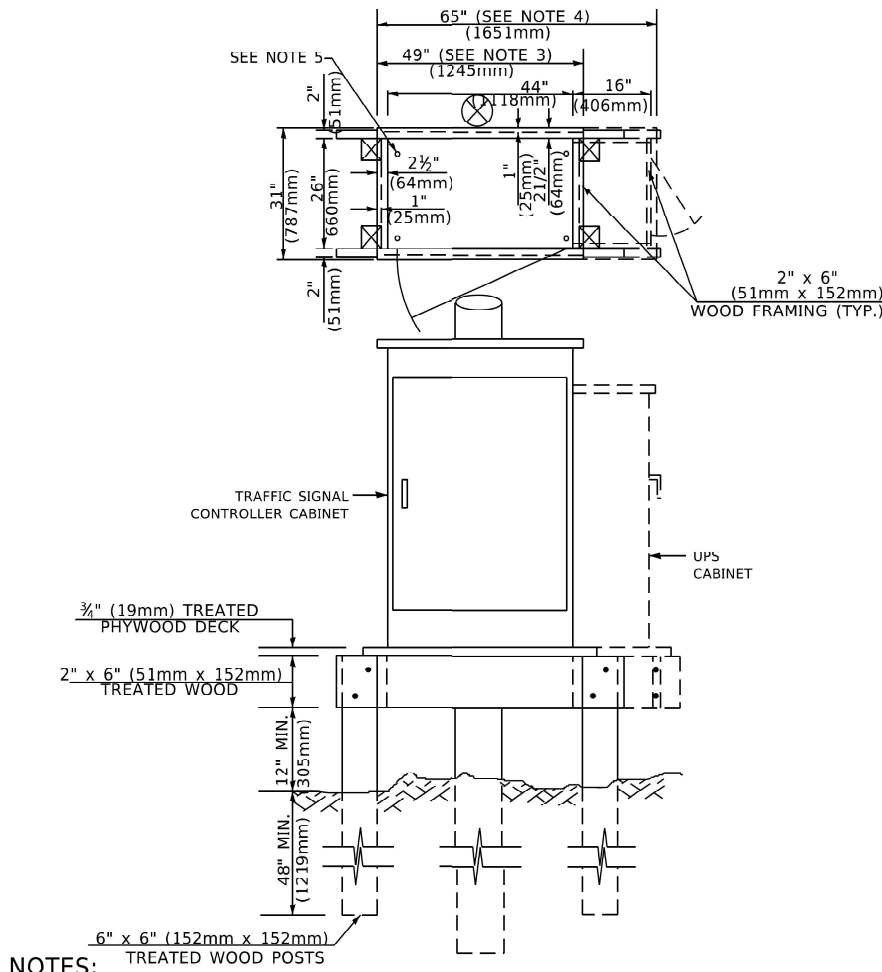
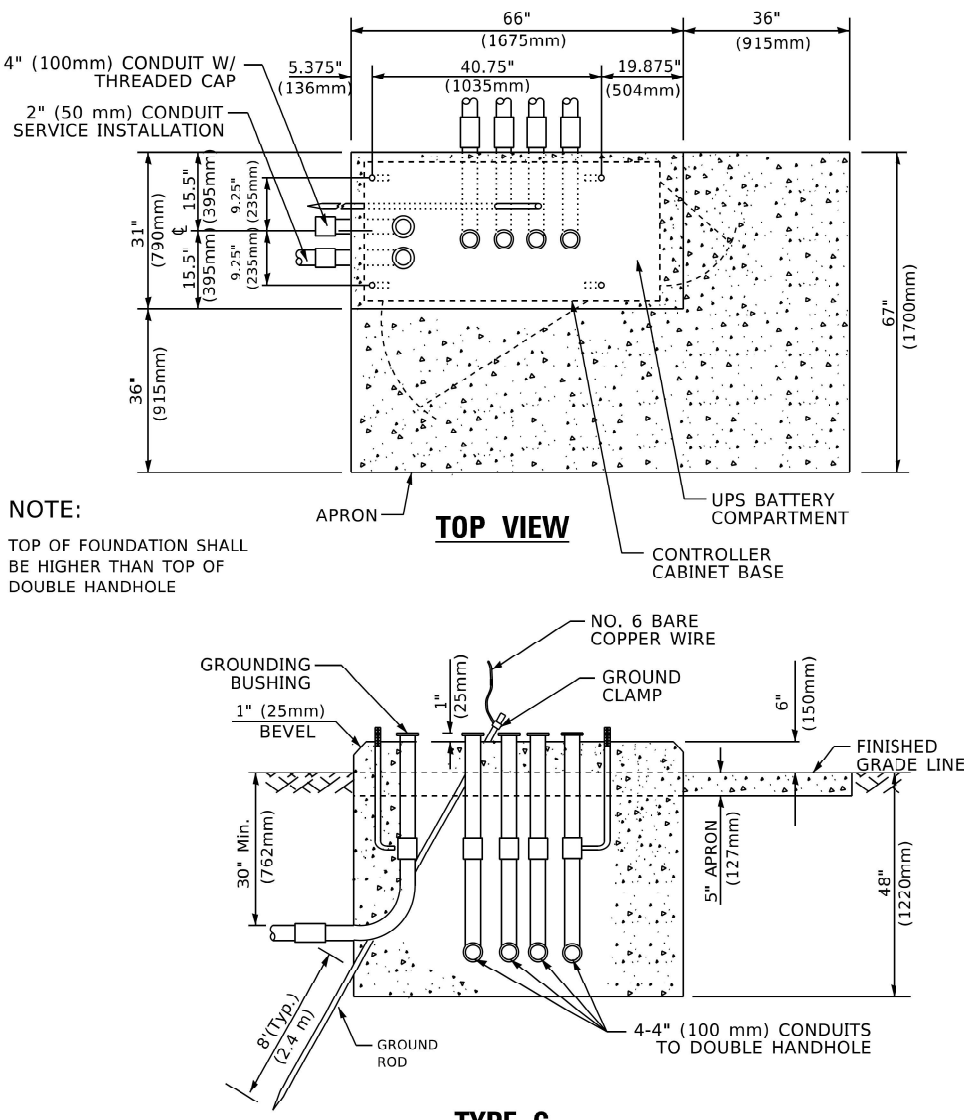
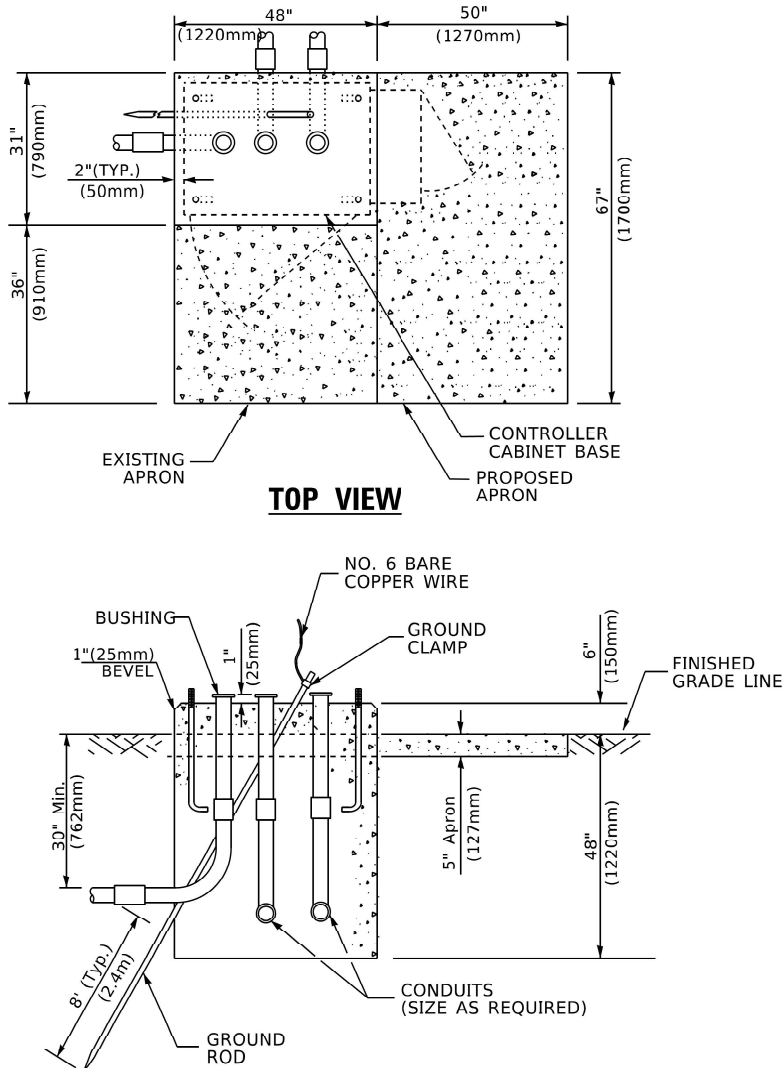
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	46
TS-05		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH



- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

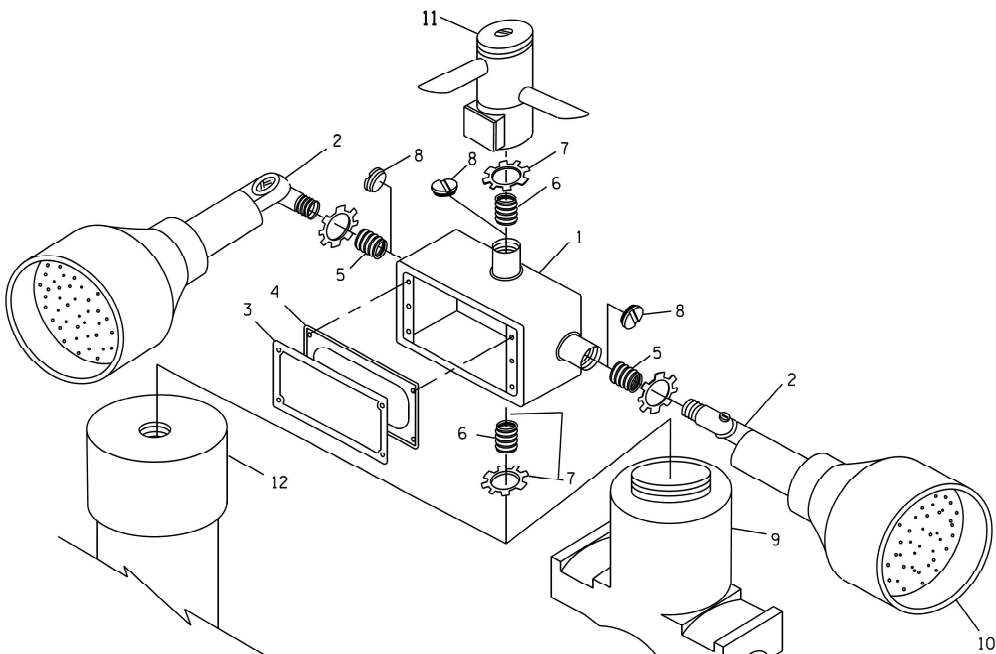
DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)		12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
 4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				VAR.	2021-123-SW	LAKE	69	47
				TS-05		CONTRACT NO. 62P40		
	SCALE: NONE			SHEET 5	OF 7 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	



POST CAP MOUNT

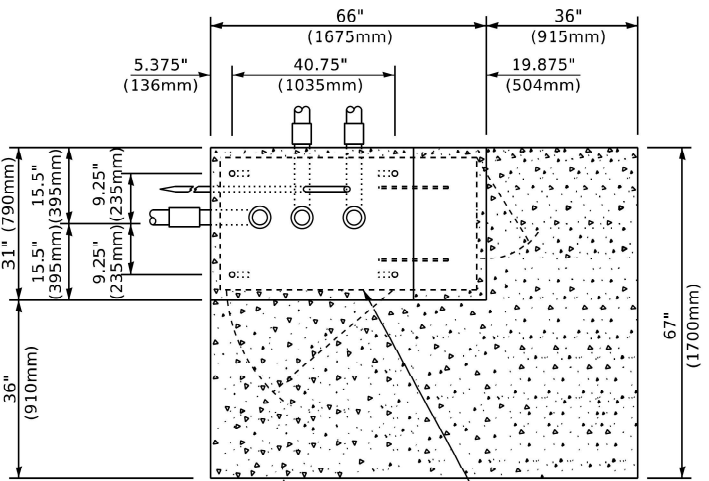
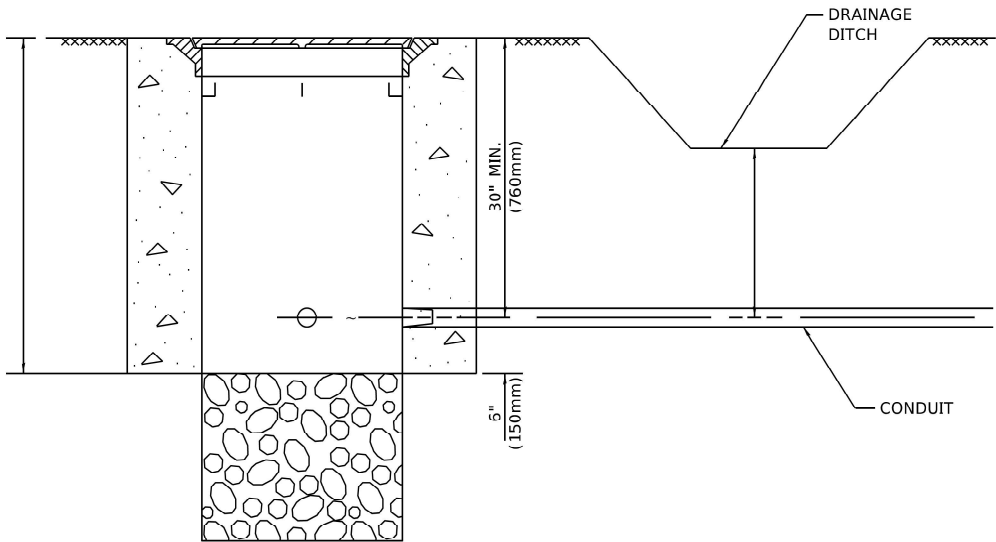
MAST ARM MOUNT

**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION
BEACON MOUNTING DETAIL**

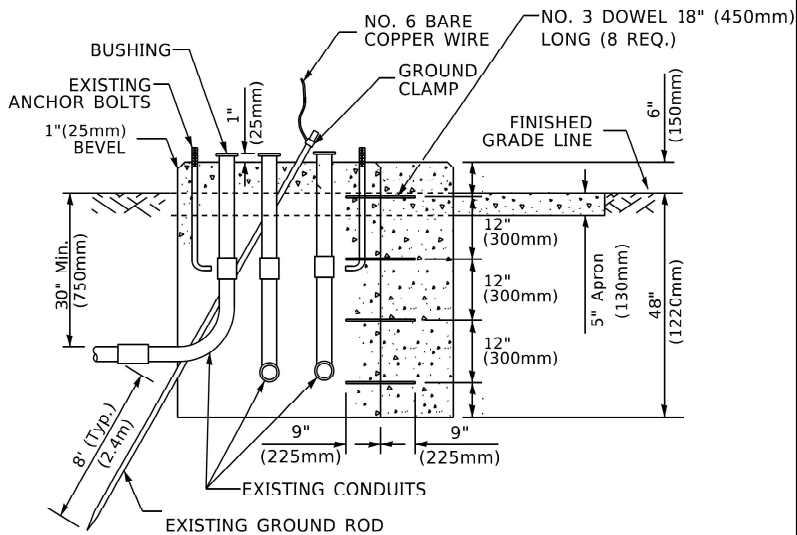
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

**HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)**



**TOP VIEW
(NOT TO SCALE)**

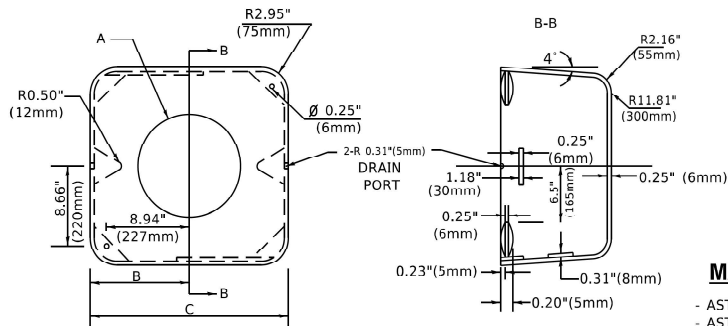


**MODIFY EXISTING TYPE "D" FOUNDATION
TO TYPE "C" FOUNDATION
(NOT TO SCALE)**

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 " (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

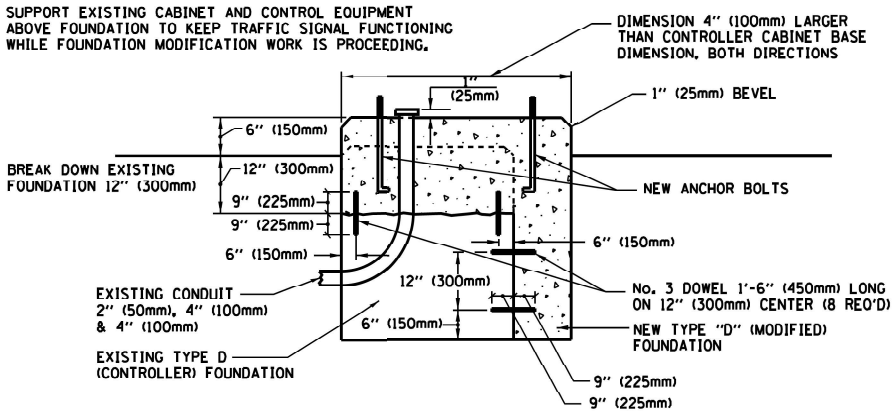
SHROUD

NOTES:

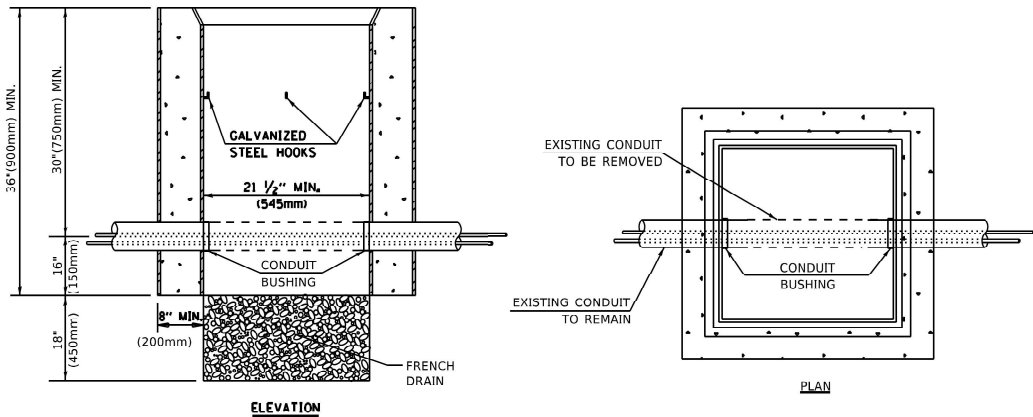
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

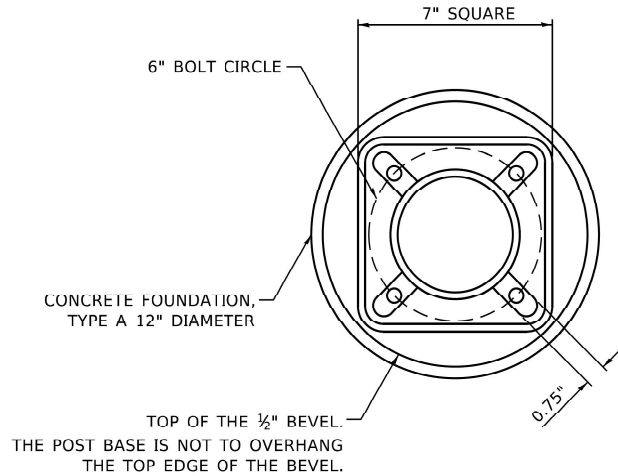
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DRAWN		REVIS	-	REVISED	-
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PLOT DATE	= 2/9/2023	DATE	-	REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

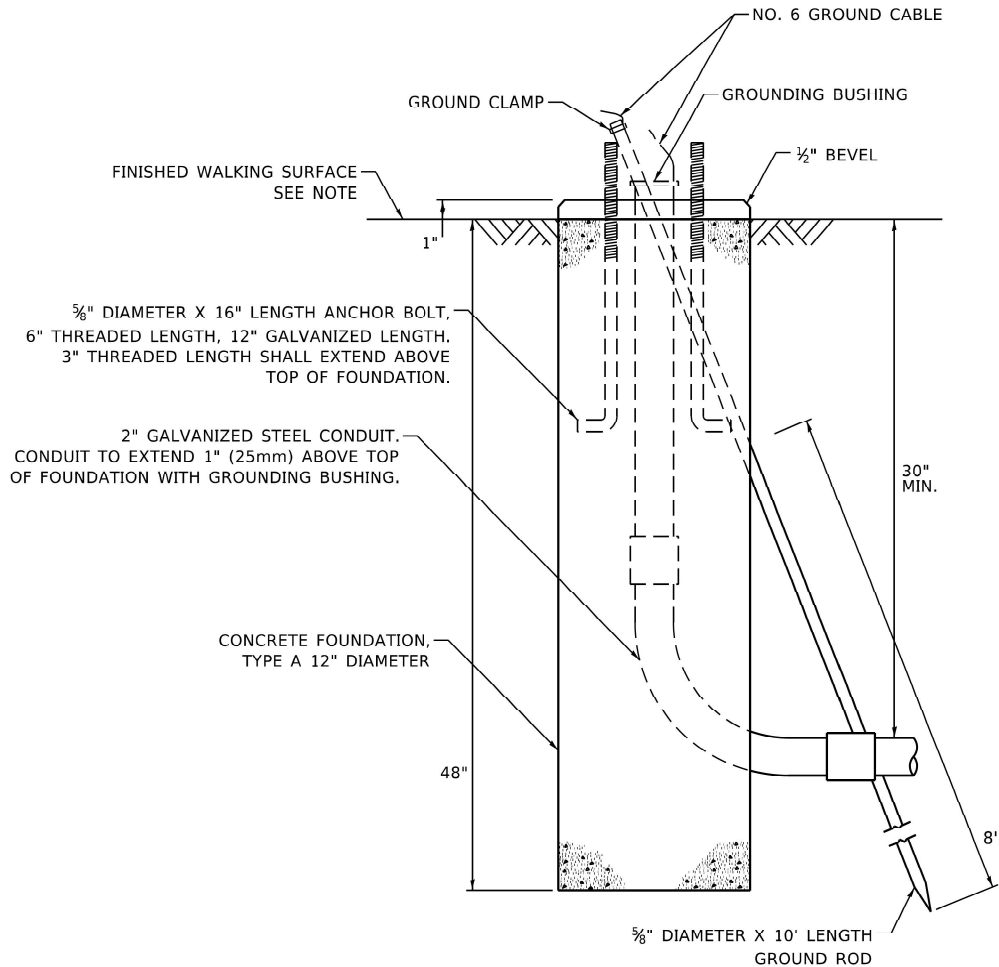
SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	48
TS-05		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

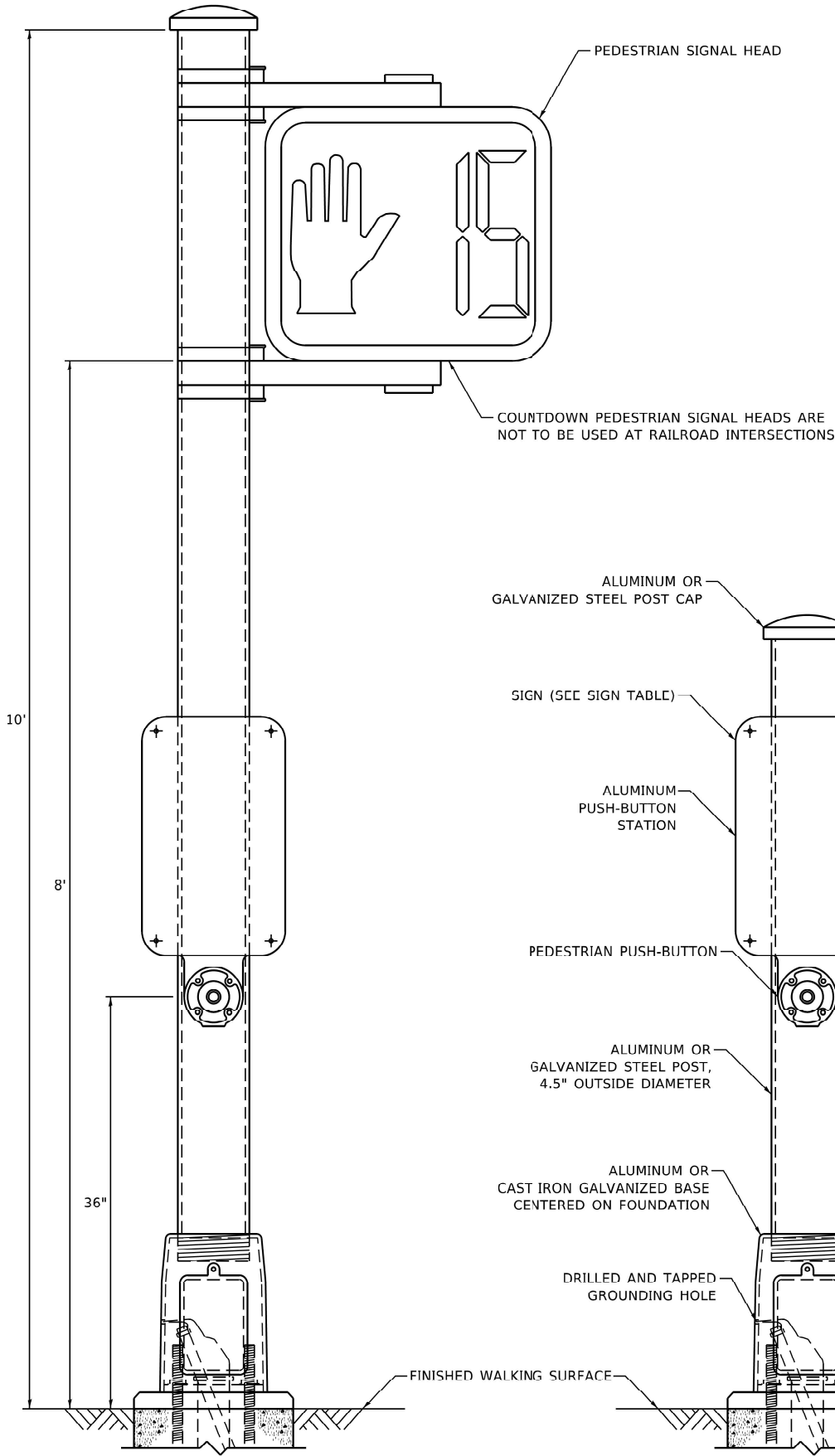


BOLT PATTERN

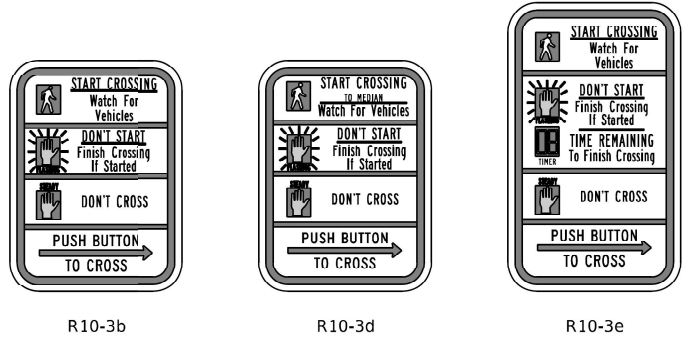
NOTE:
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



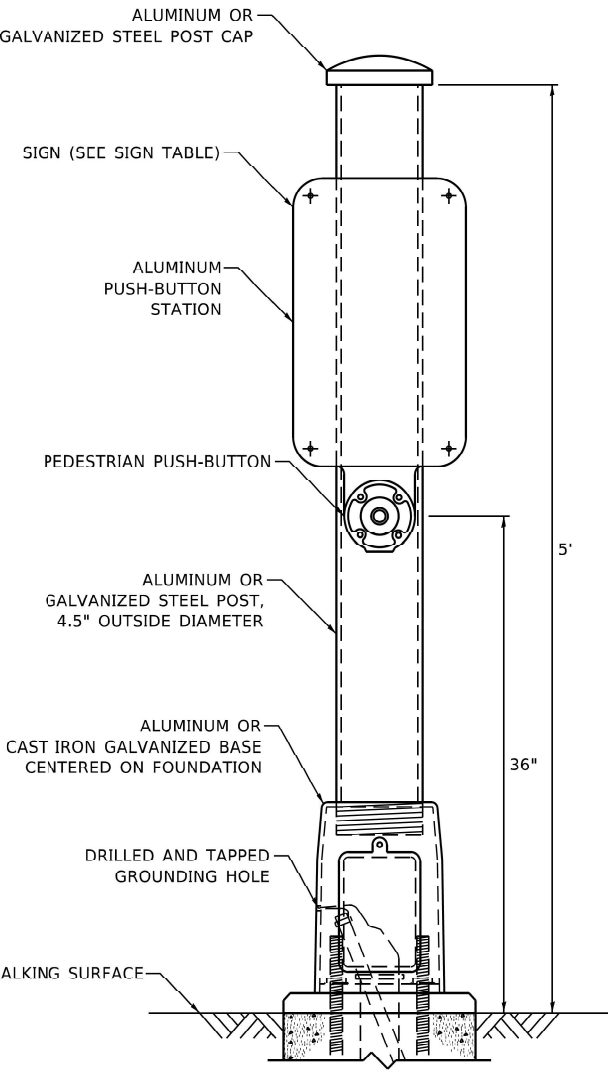
PEDESTRIAN SIGNAL POST, 10 FT.



SIGN TABLE

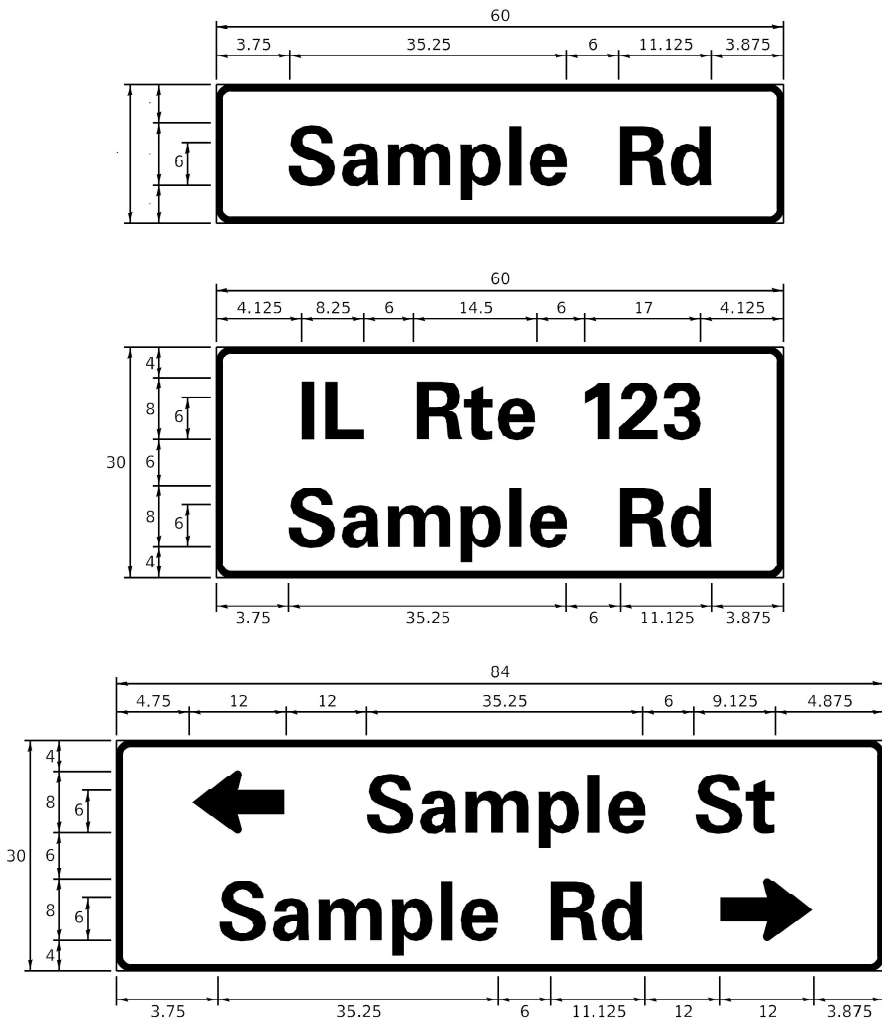
SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 15"

NOTES:
1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.



PEDESTRIAN SIGNAL POST, 5 FT.

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA

- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

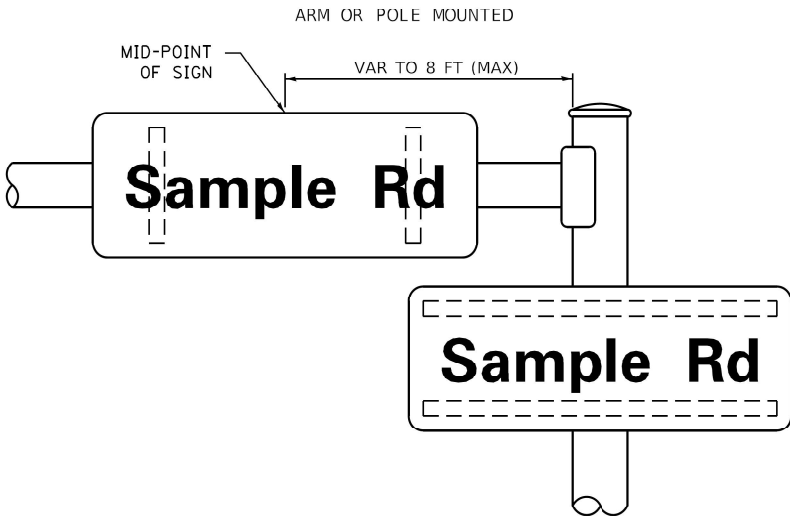
SIGN CHANNEL
SIGN SCREWS

BRACKETS

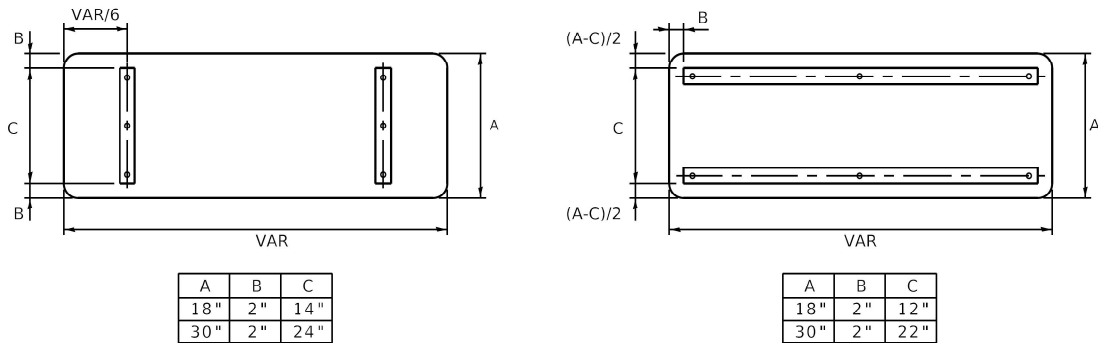
PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

USER NAME = dbennett	DESIGNED - LP/IP	REVISED - LP 07/01/2015
	DRAWN - LP	REVISED -
	CHECKED - IP	REVISED -
	DATE - 10/01/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	50
TS-02		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		



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DESIGNED - WL
DRAWN - WL
CHECKED - DB
DATE - 06/21/2022

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL PLAN
US ROUTE 12 (RAND RD) AT IL ROUTE 22 (MAIN ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	51
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				

NOTE:

1. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
2. EACH PEDESTRIAN PUSH-BUTTON
3. ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
4. CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
5. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
6. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK
7. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
8. STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.
9. WHERE TRAFFIC SIGNAL EQUIPMENT WAS REMOVED AND THE EXISTING CABLE IS NOT BEING REUSED, THE EXISTING ELECTRICAL CABLE SHALL BE REMOVED FROM CONDUIT.

9. PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.

REMOVE EXISTING PED PUSHBUTTON

EXISTING EQUIPMENT
(TYPICAL THIS SHEET ONLY)

DRILL EXISTING HANDHOLE

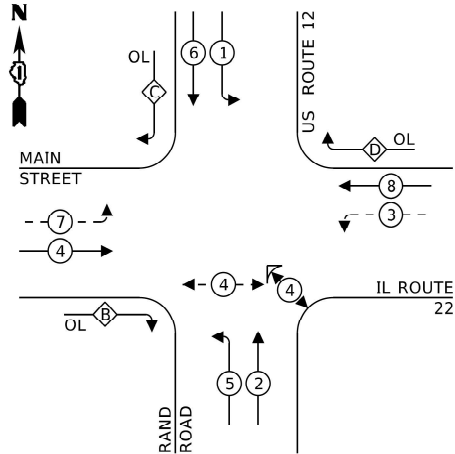
17'-UC-2"

5' PED POST
APPROX. STA 13178+88, 42.7 RT

REMOVE EXISTING PED PUSHBUTTON

TS 940
ECON 148

EXISTING CONTROLLER SEQUENCE



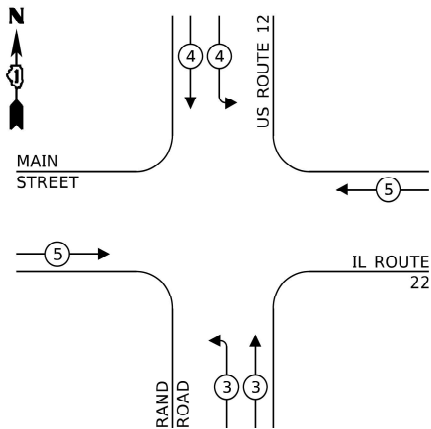
LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

RIGHT TURN OVERLAP
PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	4	5
C	6	7
D	8	1

EXISTING EMERGENCY VEHICLE
PREEMPTION SEQUENCE



TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	20	11	50	110.0
(YELLOW)	20	20	5	20.0
(GREEN)	20	12	45	108.0
PERMISSIVE ARROW	20	10	10	20.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				463.0

ENERGY COSTS TO:

LAKE ZURICH
70 E MAIN ST
LAKE ZURICH, IL 60047

ENERGY SUPPLY: CONTACT: TERRI BLECK
PHONE: 847-816-5239
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: #21701-02004



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ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	17
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	417
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	25
DRILL EXISTING HANDHOLE	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

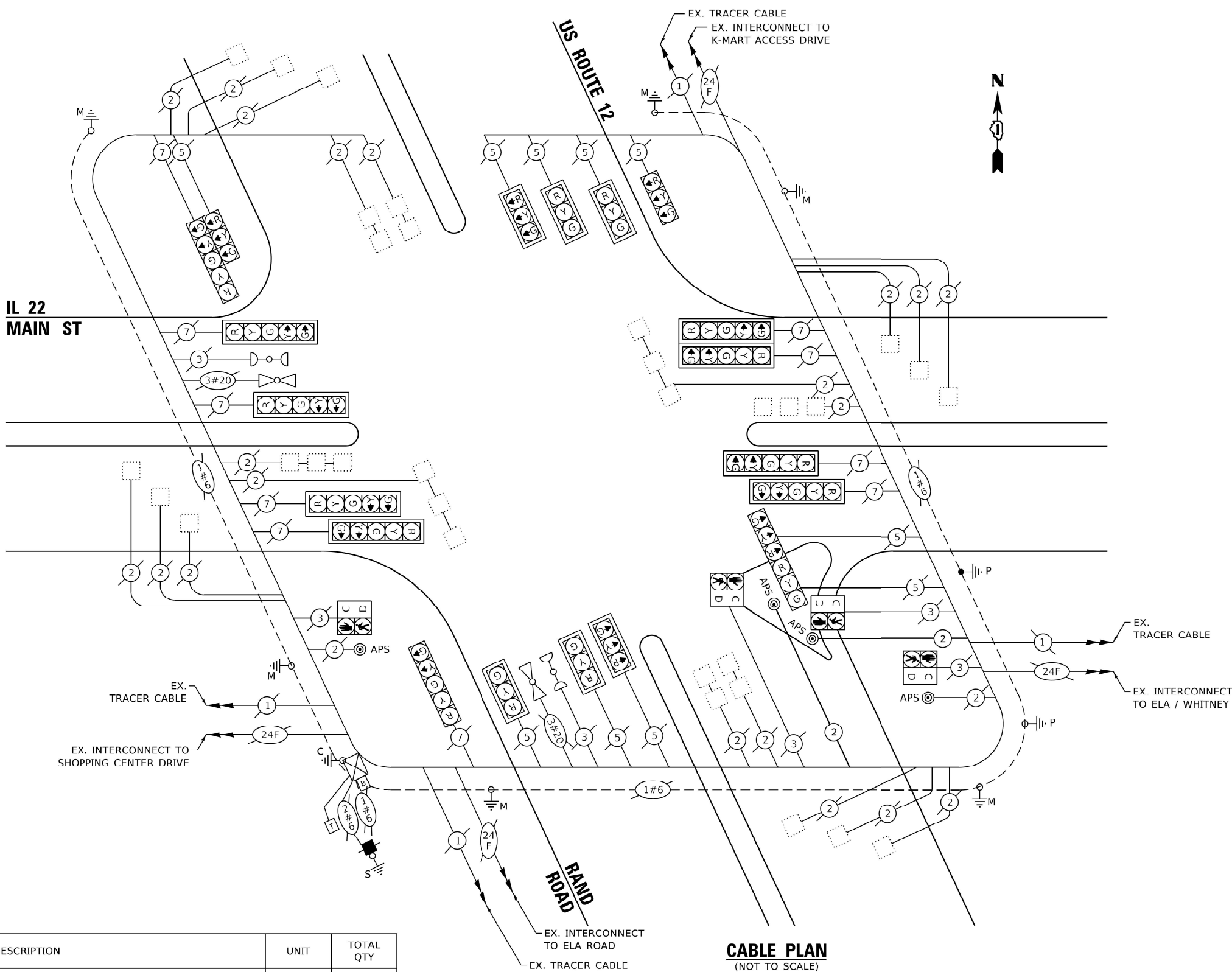
CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
US ROUTE 12 (RAND RD) AT IL ROUTE 22 (MAIN ST)

SCALE: SHEET OF SHEETS STA. TO STA.

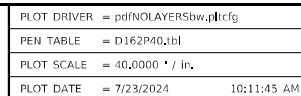
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	52
CONTRACT NO. 62P40				

ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$

TS 940
ECON 148



CABLE PLAN
(NOT TO SCALE)



DESIGNED	-	WL
DRAWN	-	WL
CHECKED	-	DB
DATE	-	06/21/2022

REVISED	-
REVISED	-
REVISED	-
REVISED	-

TRAFFIC SIGNAL PLAN
IL ROUTE 137 (BUCKLEY RD) AT MERIDIAN DR.

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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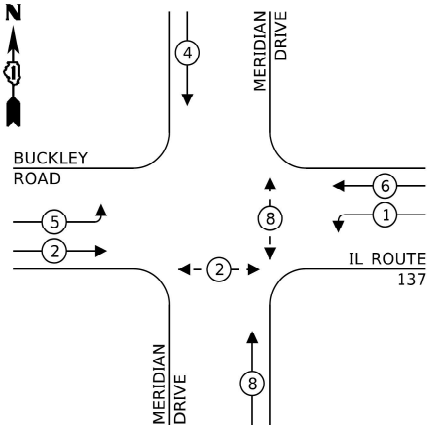
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	53
		CONTRACT NO. 62P40		
	ILLINOIS	FED. AID PROJECT	SFEDAIDPROJIS	

NOTE:

1. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
- 3 EACH PEDESTRIAN PUSH-BUTTON
2. ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
3. CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
5. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK
6. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
7. STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.
8. WHERE TRAFFIC SIGNAL EQUIPMENT WAS REMOVED AND THE EXISTING CABLE IS NOT BEING REUSED, THE EXISTING ELECTRICAL CABLE SHALL BE REMOVED FROM CONDUIT.
9. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
10. PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.
11. EXISTING SIDEWALK/ISLANDS/MEDIANS AND PAVEMENT MARKING IMPACTED DUE TO PROPOSED TRAFFIC SIGNAL WORK SHALL BE RESTORED TO EXISTING CONDITIONS, GRADES AND ELEVATIONS OF RESTORED SIDEWALK/ISLANDS/MEDIANS SHALL MATCH EXISTING.

MODEL: Default
FILE NAME: P102_2109_002_IDOT_PTB_173_12_Traffic_Signal_Design\Design\013-Aps_Upgrade\CD\CD\CD\Sheet\012P40-CablePlan-MeridianAtBuckley.dgn

PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← (*) → PROTECTED PHASE
- ← (*) - - PROTECTED/PERMITTED PHASE
- ← (*) → PEDESTRIAN PHASE
- ← (*) OL OVERLAP

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	30
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	115
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	360
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1060
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	44
DRILL EXISTING HANDHOLE	EACH	5
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	501
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	22	11	50	121.0
(YELLOW)	22	20	5	22.0
(GREEN)	22	12	45	118.8
PERMISSIVE ARROW	0	10	10	0.0
PED. SIGNAL	4	20	100	80
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				476.8

ENERGY COSTS TO:

NORTH CHICAGO
1850 LEWIS AVE
NORTH CHICAGO, IL 60064

ENERGY SUPPLY: CONTACT: TERRI BLECK
PHONE: 847-816-5239
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: #11831-41069



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DRAWN -	WL
CHECKED -	DB
DATE -	06/21/2022

REVISED -	
REVISED -	
REVISED -	
REVISED -	

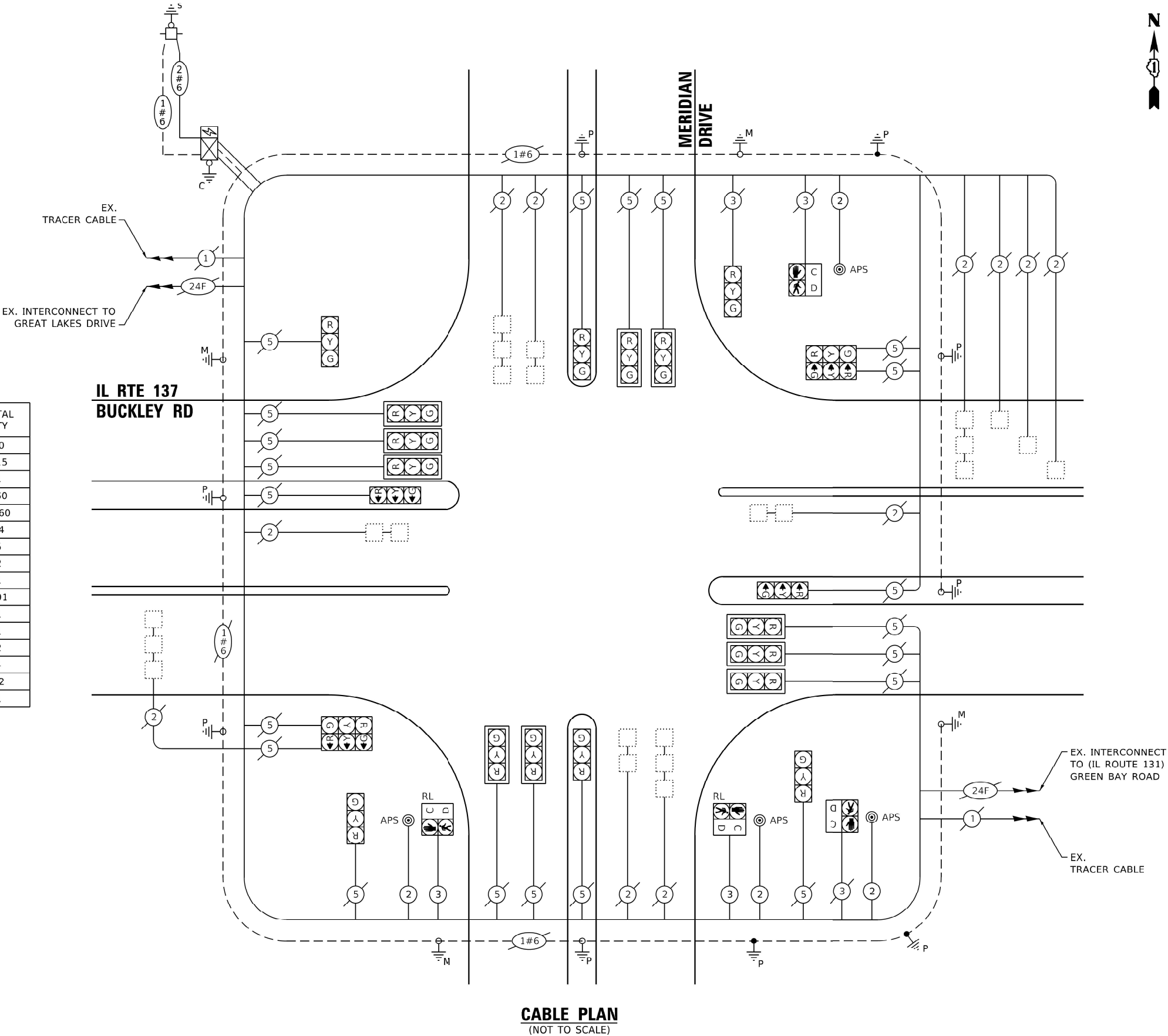
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM,
IL ROUTE 137 (BUCKLEY RD) AT MERIDIAN DR.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	54
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				

TS 7100
ECON 20





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DESIGNED - WL
DRAWN - WL
CHECKED - DB
DATE - 06/21/2022

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

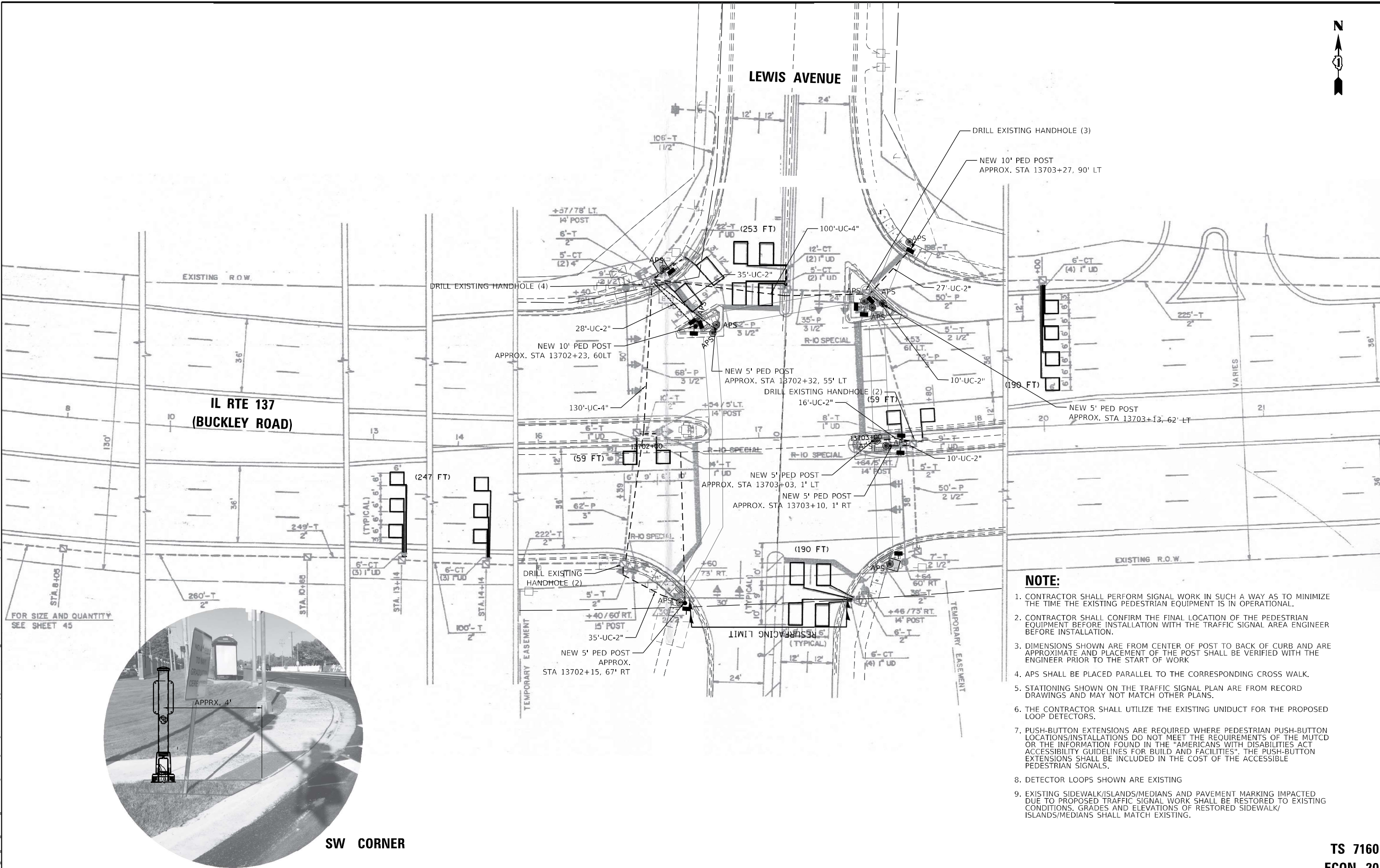
TRAFFIC SIGNAL PLAN

IL ROUTE 137 (BUCKLEY ROAD) AT LEWIS AVENUE

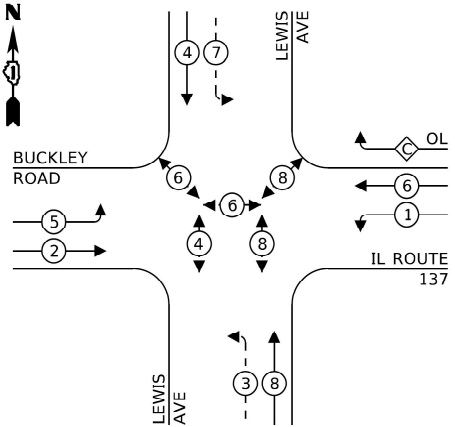
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	55
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ5				

TS 7160
ECON 20



PROPOSED CONTROLLER SEQUENCE



LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

RIGHT TURN OVERLAP
PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
C	6	7

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	161
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	230
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALATION	FACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1840
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1785
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	160
DRILL EXISTING HANDHOLE	EACH	11
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	12
MODIFY EXISTING CONTROLLER CABINET	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	5
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	12
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	28
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	17	11	50	93.5
(YELLOW)	17	20	5	17.0
(GREEN)	17	12	45	91.8
PERMISSIVE ARROW	12	10	10	12.0
PED. SIGNAL	12	20	100	240.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				579.3

ENERGY COSTS TO:

NORTH CHICAGO

1850 LEWIS AVE

NORTH CHICAGO, IL 60064

ENERGY SUPPLY: CONTACT: TERRI BLECK

PHONE: 847-816-5239

COMPANY: COMMONWEALTH EDISON

ACCOUNT NUMBER: #11831-41069



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DESIGNED - WL	REVISED -
DRAWN - WL	REVISED -
CHECKED - DB	REVISED -
DATE - 06/21/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN
(NOT TO SCALE)

CABLE PLAN, PHASE DESIGNATION DIAGRAM, IL ROUTE 137 (BUCKLEY ROAD) AT LEWIS AVENUE			
SCALE:	SHEET	OF	SHEETS
STA.	TO	STA.	TO

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	56
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT SFEDAIDPROJ5				

TS 7160
ECON 20

1. CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
3. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK
4. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
5. STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.
6. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
7. PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.

8. EXISTING SIDEWALK/
ISLANDS/MEDIANS AND PAVEMENT MARKING
IMPACTED DUE TO
PROPOSED TRAFFIC SIGNAL WORK SHALL BE
RESTORED TO EXISTING
CONDITIONS. GRADES AND ELEVATIONS OF
RESTORED SIDEWALK/
ISLANDS/MEDIANS SHALL MATCH EXISTING.



TS SHT NO. 13

MODEL: Default
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL PLAN

IL 137 (BUCKLEY ROAD) AT RAY STREET

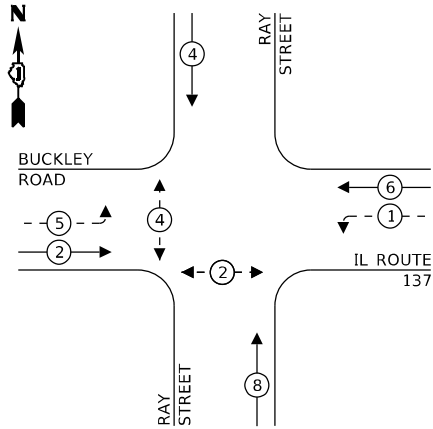
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	57
		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	\$FEDAIDPROJS

TS 7175
ECON 20

MODEL: Default
FILE NAME: TSUG_209_002_IDOT_PTB_173_12_Traffic_Signal_Design\Drawn\013_Aps_Upgrade\CD\CD\CADD_Sheets\1D152700-CablePlan-RayAtBuckley.dgn

PROPOSED CONTROLLER SEQUENCE



LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	10	11	50	55.0
(YELLOW)	10	20	5	10.0
(GREEN)	10	12	45	54.0
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				332.0

ENERGY COSTS TO:

NORTH CHICAGO

1850 LEWIS AVE
NORTH CHICAGO, IL 60064

ENERGY SUPPLY: CONTACT: TERRI BLECK
PHONE: 847-816-5239
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: #11831-41069

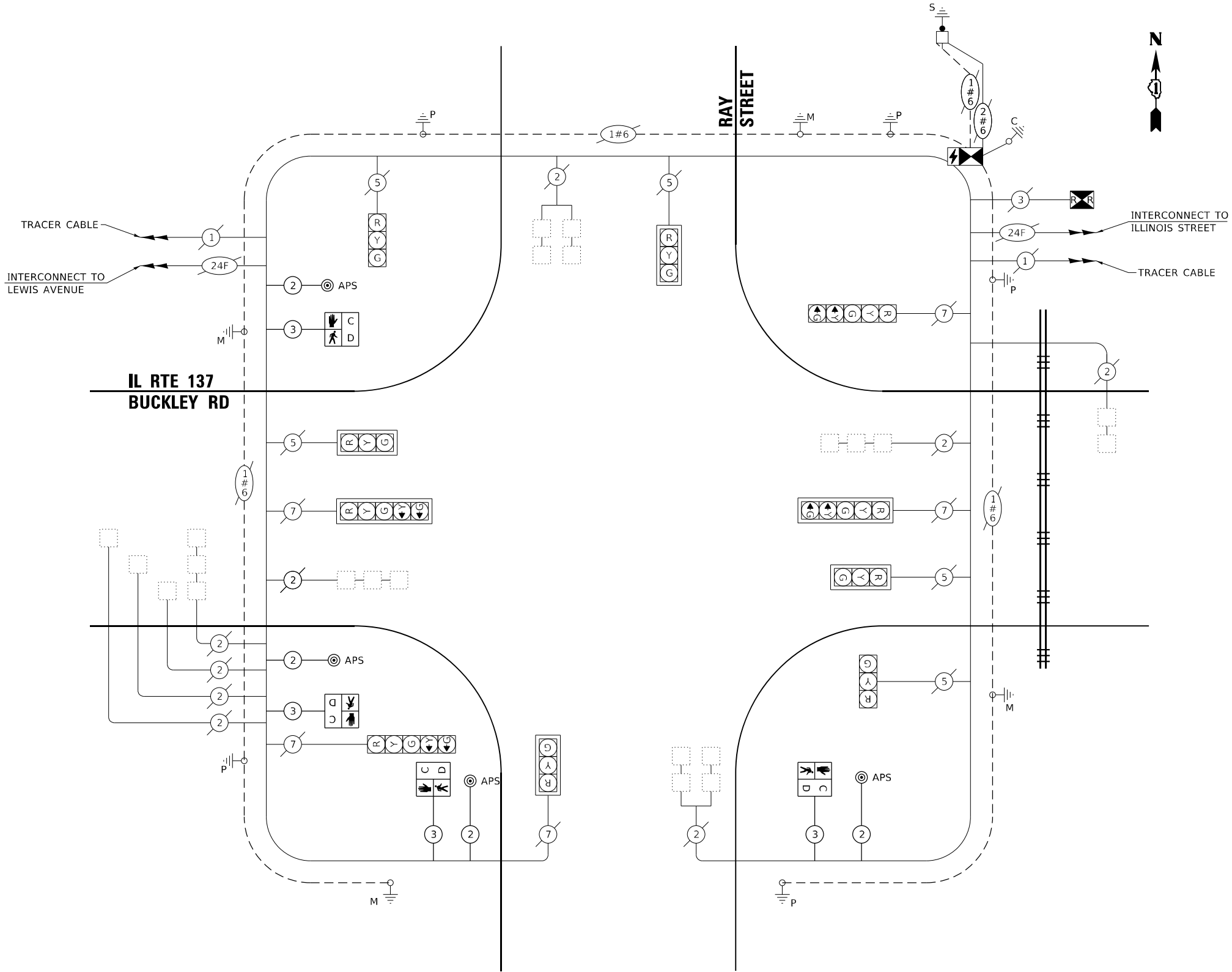
ITEM DESCRIPTION	UNIT	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	825
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	825
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
MODIFY EXISTING CONTROLLER CABINET	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
UPGRADE EXISTING CONTROLLER TO NTCIP SPECIAL	EACH	1



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN
(NOT TO SCALE)



TS 7175
ECON 20

F.A.U. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			LAKE	69	58
CONTRACT NO. 62P40					
SCALE:		SHEET	OF	SHEETS	STA. TO STA.
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$					

MODEL Default
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SEQUENCE OF OPERATION

MOVEMENT	N ↑																	FLASH
PHASE		1+5				1+6		2+5		2+6				4+8				
INTERVAL		1	2	3	4	5	6	7	8	9	10	11	12a	12b	13	14	15a	
CHANGE TO			1+G	2+5	2+G		2+G		θ	θ	2+G			4+8			1+5,1+6 2+5,2+6	
IL 137 (BUCKLEY RD) FAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	G	G	G	G	G	Y	R	R	R	R	R
IL 137 (BUCKLEY RD) END MAST ARM AND FAR LEFT SIGNALS	E/B	R ← G	R ← Y	R ← G	R ← Y	R	R	G ← G	G ← G	G ← Y	G	G	Y	R	R	R	R	R
IL 137 (BUCKLEY RD) FAR RIGHT MAST ARM SIGNAL	W/B	R	R	R	R	G	G	R	R	R	G	G	Y	R	R	R	R	R
IL 137 (BUCKLEY RD) END MAST ARM AND FAR LEFT SIGNALS	W/B	R ← G	R ← G	R ← Y	R ← Y	G ← G	G ← Y	R	R	R	G	G	Y	R	R	R	R	R
RAY ST ALL SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R
RAY ST ALL SIGNALS	S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R
PEDESTRIAN SIGNALS CROSSING IL 137 ON THE WEST SIDE OF RAY ST		H	H	H	H	H	H	H	H	H	H	H	H	H	P *	FH **	H	H
PEDESTRIAN SIGNALS CROSSING RAY ST ON THE SOUTH SIDE OF IL 137		H	H	H	H	H	H	P *	FH **	H	P *	FH **	H	H	H	H	H	H

Revised: 9/16/2022

PHASE 2 + 6 TO BE SET ON RECALL

* TO APPEAR ONLY UPON PUSHBUTTON ACTUATION

** FLASHING HAND IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE

WALK AND FLASHING DON'T WALK TIMINGS TO BE SET ONLY ON PHASES WHERE WALK AND FLASHING DON'T WALK ARE INDICATED IN THE SEQUENCE OF OPERATION

θ THIS WALK OR FLASHING DON'T WALK INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE THE WALK OR FLASHING DON'T WALK INTERVALS

P= ILLUMINATED PERSON=WALK

FH= ILLUMINATED FLASHING HAND=FLASHING DON'T WALK

H= ILLUMINATED SOLID HAND=DON'T WALK

D= DARK

RAILROAD PREEMPTION SEQUENCE OF OPERATION

										PREEMPTOR NUMBER 2					
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	7		10		13								
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	2	3	4	5	CLEAR TO NORMAL SEQUENCE		
CHANGE TO RAILROAD PREEMPTION SEQUENCE INTERVAL NUMBER	2	2	1D	2	1F	2	1H	2	3	4	5				
IL 137 (BUCKLEY RD) FAR RIGHT MAST ARM SIGNAL	E/B	R	R	Y	R	Y	R	R	R	R	R	R	R	Δ	
IL 137 (BUCKLEY RD) END MAST ARM AND FAR LEFT SIGNALS	E/B	R	R	Y	R	Y	R	R	R	R	R	R	R	Δ	
IL 137 (BUCKLEY RD) FAR RIGHT MAST ARM SIGNAL	W/B	R	G	R	R	G	G	R	R	G	Y	R	R	Δ	
IL 137 (BUCKLEY RD) END MAST ARM AND FAR LEFT SIGNALS	W/B	R	G	R	R	G	G	R	R	G	Y	R	R	Δ	
RAY ST ALL SIGNALS	N/B	R	R	R	R	R	R	Y	R	R	R	R	G	Δ	
RAY ST ALL SIGNALS	S/B	R	R	R	R	R	R	Y	R	R	R	R	G	Δ	
PEDESTRIAN SIGNALS CROSSING IL 137 ON THE WEST SIDE OF RAY ST		H	H	H	H	H	H	FH	H	H	H	H	H	Δ	
PEDESTRIAN SIGNALS CROSSING RAY ST ON THE SOUTH SIDE OF IL 137		H	H	FH	H	FH	H	H	H	H	H	H	H	Δ	

Revised: 9/16/2022

Δ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.



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PLOT DATE = 2/9/2023 9:15:44 AM	DATE - 06/21/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAILROAD SEQUENCING
IL ROUTE 137 (BUCKLEY ROAD) AT RAY STREET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	59
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				



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DRAWN - WL
CHECKED - DB
DATE - 06/21/2022

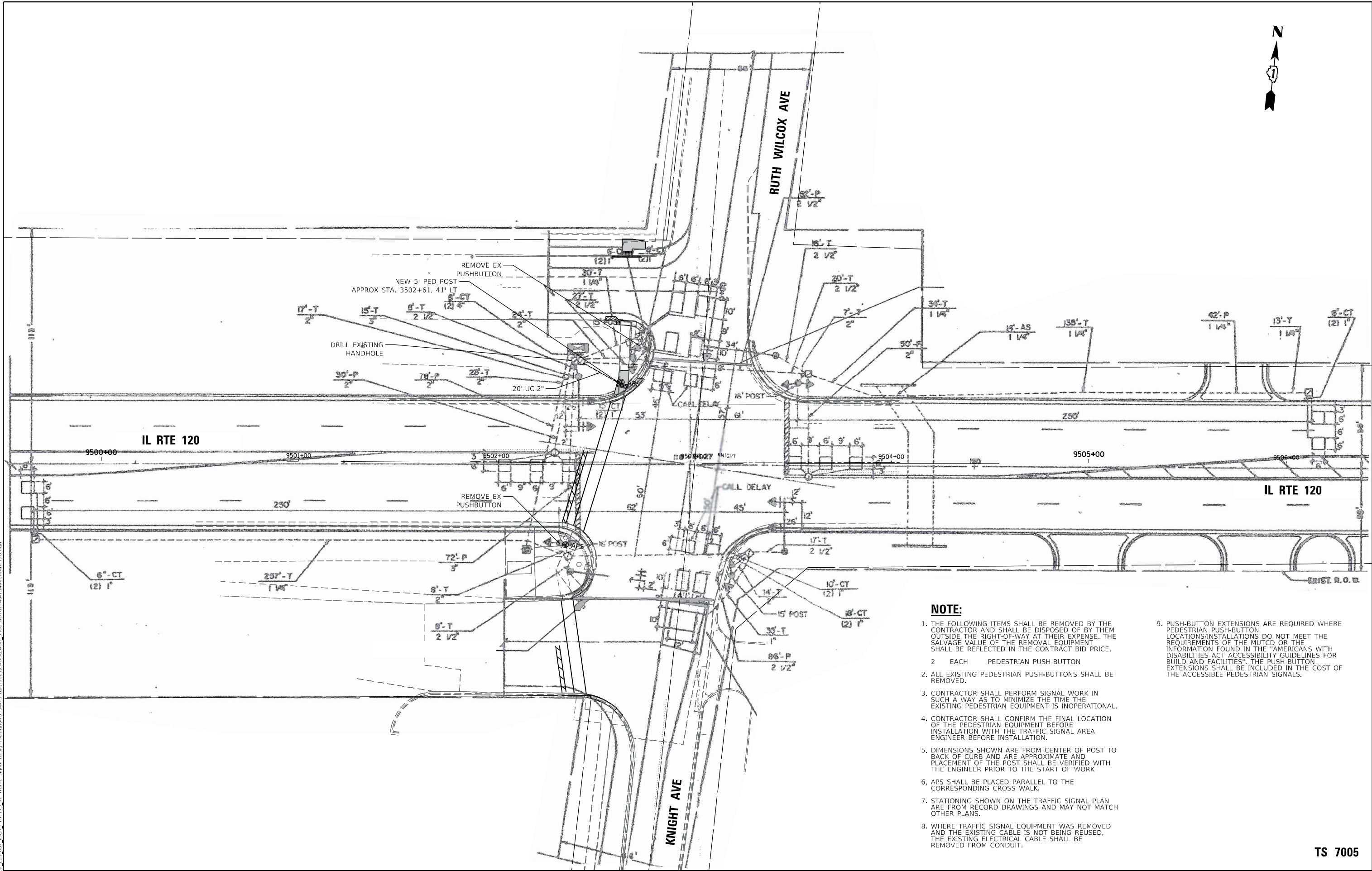
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

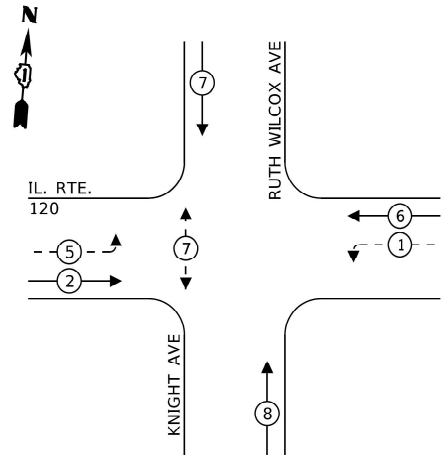
TRAFFIC SIGNAL PLANS
IL ROUTE 120 AT KNIGHT AVENUE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	60
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ5				



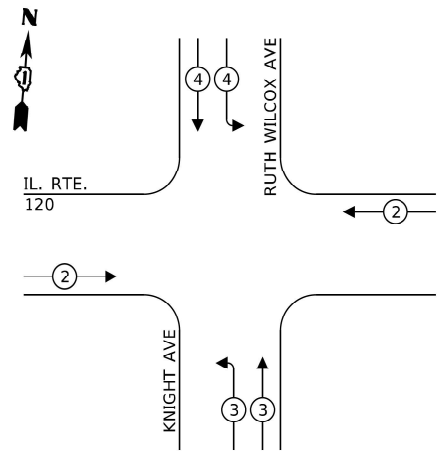
EXISTING CONTROLLER SEQUENCE



LEGEND:

- ← * → PROTECTED PHASE
- ← * - PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * OL OVERLAP

EXISTING EMERGENCY VEHICLE
PREEMPTION SEQUENCE



TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	11	50	77.0
(YELLOW)	14	20	5	14.0
(GREEN)	18	12	45	97.2
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	2	20	100	40.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				361.2

ENERGY COSTS TO:

WAUKEGAN

100 N MARTIN LUTHER KING JR. AVE
WAUKEGAN, IL 60085

ENERGY SUPPLY: CONTACT: TERRI BLECK
PHONE: 847-816-5239
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: #01781-48055

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	20
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	55
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	27
DRILL EXISTING HANDHOLE	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	53
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	FOOT	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1



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DESIGNED -	WL	REVISED -
DRAWN -	WL	REVISED -
CHECKED -	DB	REVISED -
DATE -	06/21/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
IL ROUTE 120 AT KNIGHT AVENUE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	61
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				

TS 7005

IL RTE 120

OLD PLANK RD

RUTH WILCOX AVE

FRONT.
ROAD

KNIGHT AVENUE

CABLE PLAN
(NOT TO SCALE)



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DRAWN - WL	REVISED -
CHECKED - DB	REVISED -
DATE - 06/21/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL EQUIPMENT REMOVAL AND MODIFICATION PLAN
IL RTE 137 (SHERIDAN RD) AT BEACH RD

SCALE: SHEET OF SHEETS STA. TO STA.

TS 7190
ECON LAKE COUNTY SYSTEM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	69	62
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				

NOTE:

1. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
 - 3 EACH PEDESTRIAN PUSH-BUTTON
2. ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
3. CONTRACTOR SHALL PERFORM SIGNAL WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
5. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK
6. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
7. STATIONING SHOWN ON THE TRAFFIC SIGNAL PLAN ARE FROM RECORD DRAWINGS AND MAY NOT MATCH OTHER PLANS.
8. WHERE TRAFFIC SIGNAL EQUIPMENT WAS REMOVED AND THE EXISTING CABLE IS NOT BEING REUSED, THE EXISTING ELECTRICAL CABLE SHALL BE REMOVED FROM CONDUIT.
9. PUSH-BUTTON EXTENSIONS ARE REQUIRED WHERE PEDESTRIAN PUSH-BUTTON LOCATIONS/INSTALLATIONS DO NOT MEET THE REQUIREMENTS OF THE MUTCD OR THE INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILD AND FACILITIES". THE PUSH-BUTTON EXTENSIONS SHALL BE INCLUDED IN THE COST OF THE ACCESSIBLE PEDESTRIAN SIGNALS.
10. THE SOUTH BOUND MAST ARM IS SHOWN AT A LOCATION INCONSISTENT WITH ACTUAL FIELD CONDITIONS. THE PUSH-BUTTON IS SHOWN WHERE THE MAST ARM IS PRESENT IN FIELD CONDITIONS. THE CONTRACT IS RESPONSIBLE FOR VERIFYING FIELD CONDITIONS AND CONFIRM FINAL EQUIPMENT LOCATION AS DIRECTED IN NOTE 4 OF THIS SHEET
11. EXISTING SIDEWALK/ISLANDS/MEDIANS AND PAVEMENT MARKING IMPACTED DUE TO PROPOSED TRAFFIC SIGNAL WORK SHALL BE RESTORED TO EXISTING CONDITIONS. GRADES AND ELEVATIONS OF RESTORED SIDEWALK/ISLANDS/MEDIANS SHALL MATCH EXISTING.

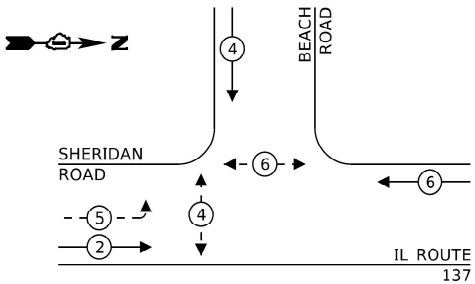


BEACH RD

IL RTE 137 (SHERIDAN RD)

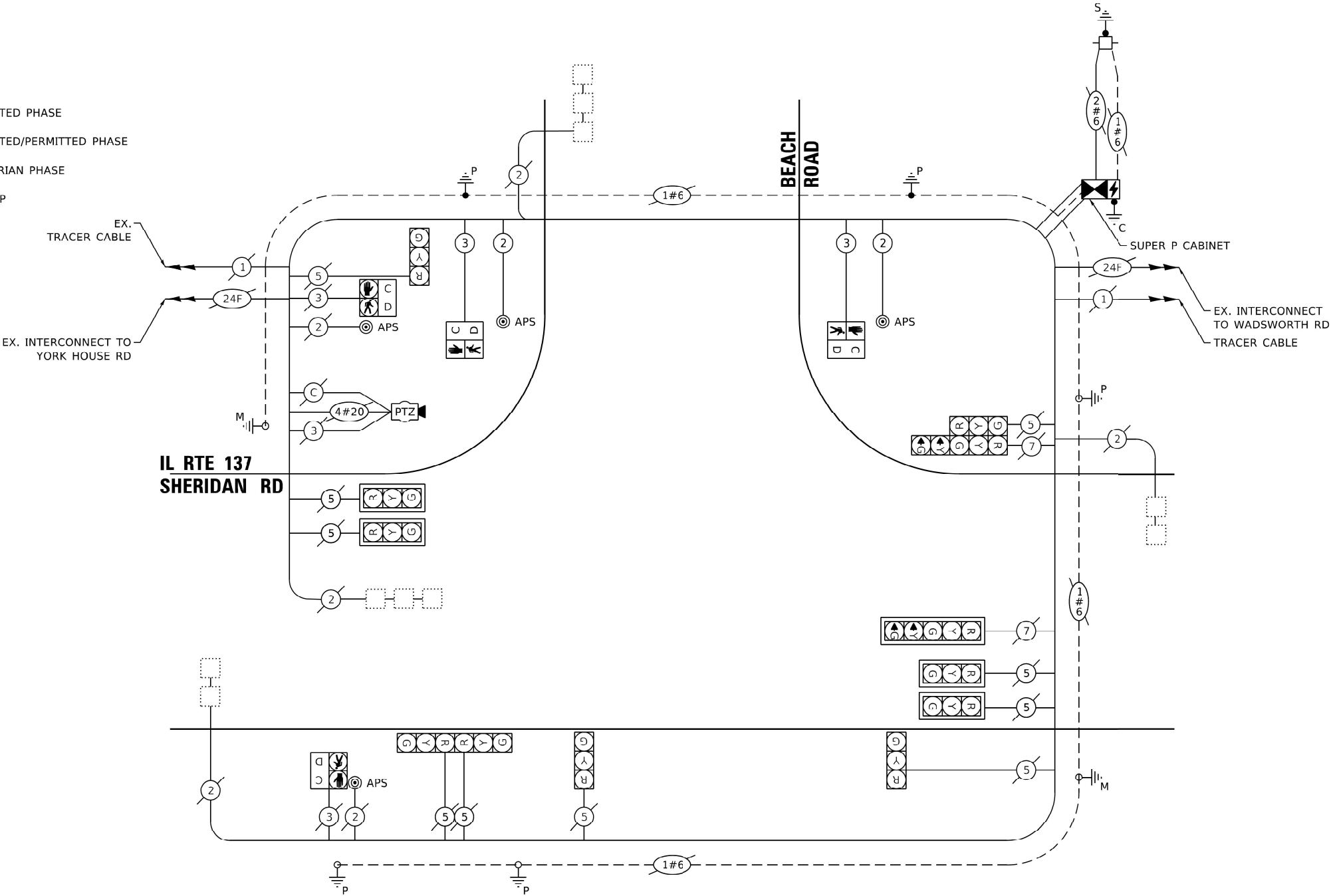
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PROPOSED CONTROLLER SEQUENCE



LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP



CABLE PLAN
(NOT TO SCALE)

TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	4	10	10	4.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				351.8

ENERGY COSTS TO:

BEACH PARK
11270 W. WADSWORTH ROAD
BEACH PARK, IL 60099

ENERGY SUPPLY: CONTACT: TERRI BLECK
PHONE: 847-816-5239
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: #04081-67019

ITEM DESCRIPTION	UNIT	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	35
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	170
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	155
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	50
DRILL EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1



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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

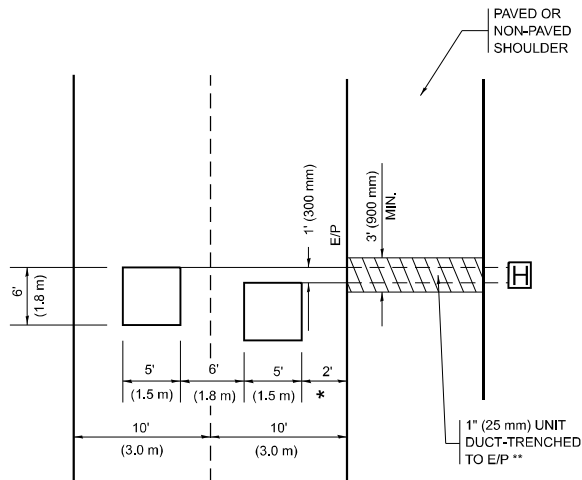
CABLE PLAN, PHASE DESIGNATION DIAGRAM,
IL RTE 137 (SHERIDAN ROAD) AT BEACH ROAD

SCALE: SHEET OF SHEETS STA. TO STA.

TS 7190
ECON LAKE COUNTY SYSTEM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	63
CONTRACT NO. 62P40				
ILLINOIS FED. AID PROJECT \$FEDAIDPROJ\$				

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

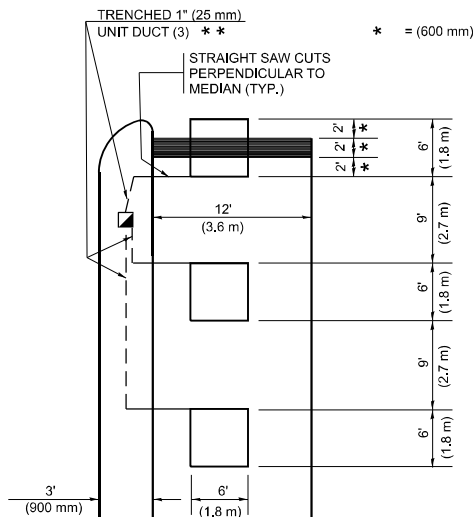


* = (600 mm)

* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

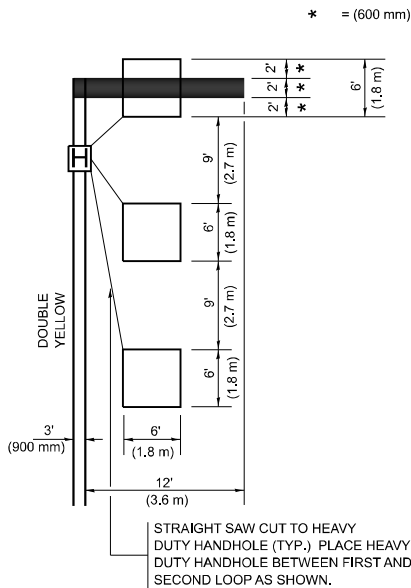
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARE SAW-CUT
EDGE OF
UNIT
RUN BETWEEN
OF PAVEMENT
ANDHOLE.
OR LOOPS
ERMINATE
DHOLES
DE PAVEMENT)

DO NOT INSTALL
CALLING LOOP IN
RIGHT TURN LANE.

N.T.S.

DETAIL 1
N.T.S.

[illegible]

DETAIL 2
N.T.S.

USER NAME = Rana,Kalo	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 0.16666633 / in.	CHECKED - R,K.F.	REVISED -
PLOT DATE = 6/21/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	64
TS-07		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

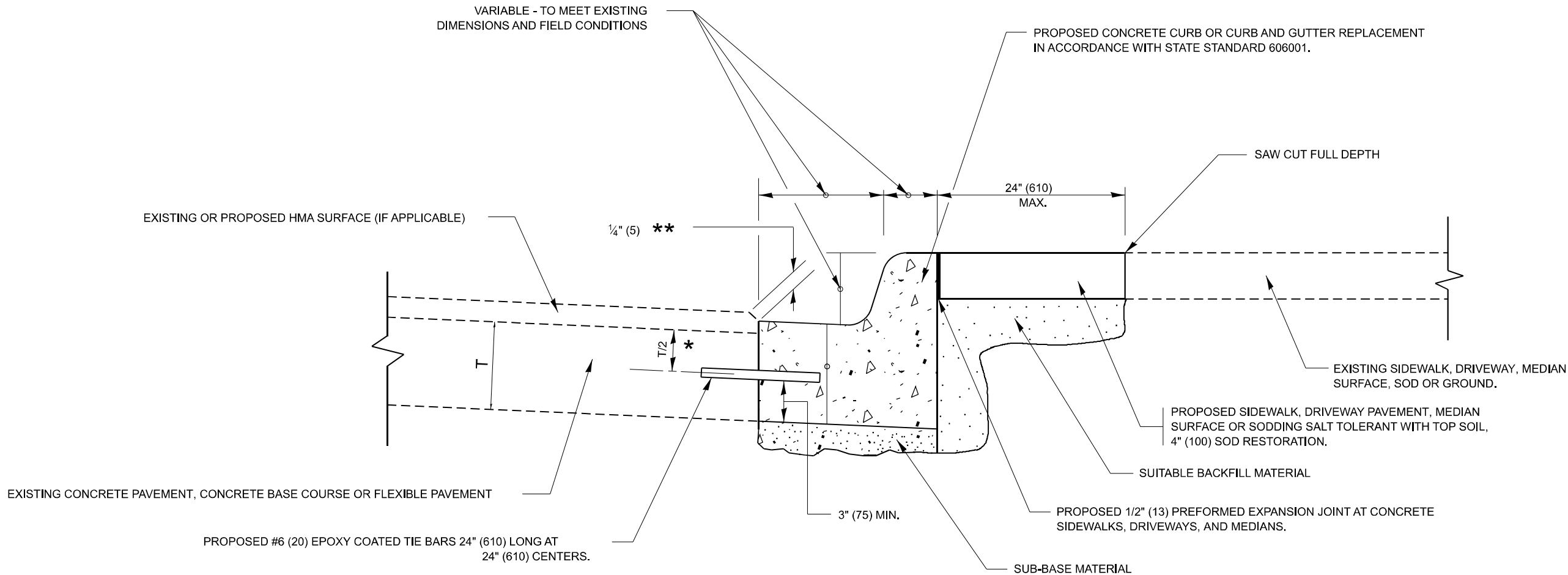
NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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MODEL: BD-24 [Sheet]
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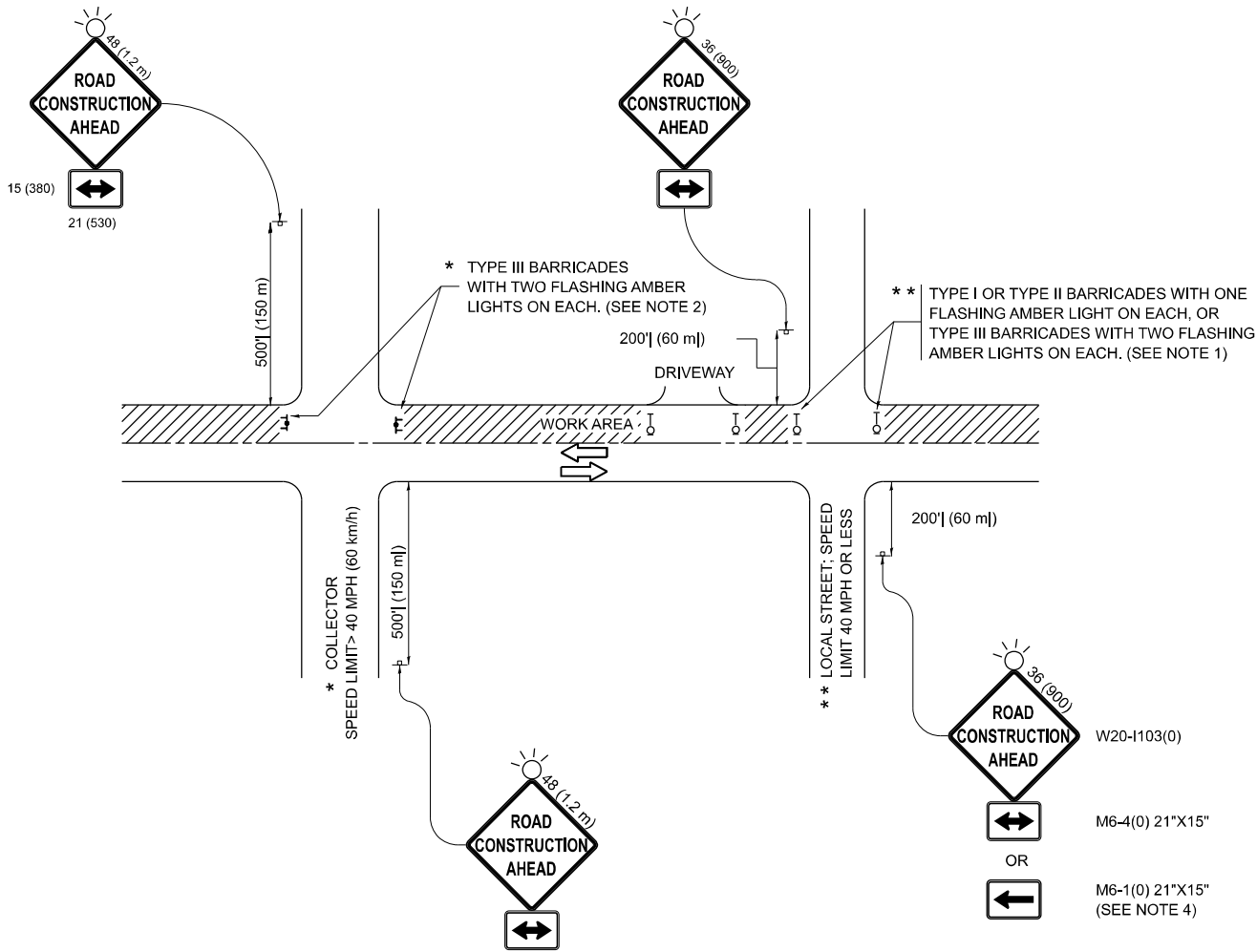
- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = Rana.Kalo	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - M. GOMEZ 01-22-01						VAR.	2021-123-SW	LAKE	69	65
	PLOT SCALE = 0.16666633" / in.	CHECKED -	REVISED - R. BORO 12-15-09		SCALE: NONE			SHEET 1 OF 1 SHEETS STA. TO STA.					
	PLOT DATE = 6/21/2024	DATE - 03-11-94	REVISED - K. SMITH 07-11-19					BD600-06 (BD-24) CONTRACT NO. 62P40					
											ILLINOIS	FED. AID PROJECT	

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NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters)
unless otherwise shown.

USER NAME	= Rana.Kalo	DESIGNED	- L.H.A.	REVISED	- T. RAMMACHER 01-06-00
DRAWN	-	DRAWN	-	REVISED	- A. SCHUETZE 07-01-13
PLOT SCALE	= 0.16666633 "/in.	CHECKED	-	REVISED	- A. SCHUETZE 09-15-06
PLOT DATE	= 6/21/2024	DATE	- 06-89	REVISED	- D. SENDERAK 05-03-24

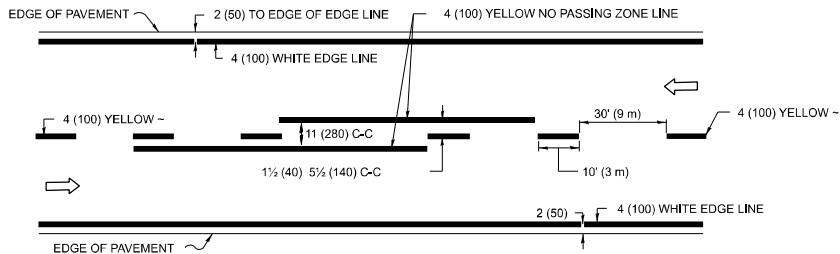
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

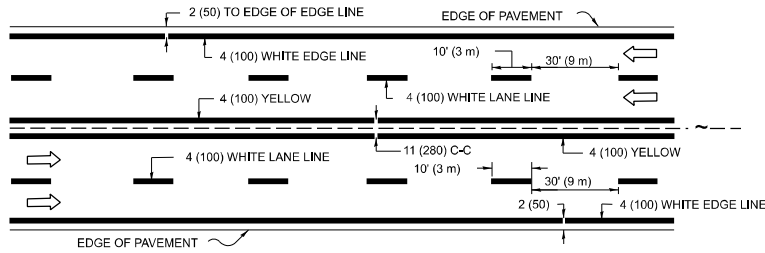
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	66
TC-10		CONTRACT NO. 62P40		
		ILLINOIS	FED. AID PROJECT	

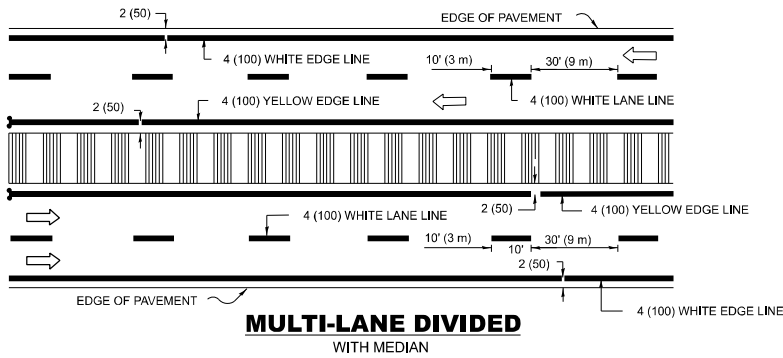
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2-LANE ROADWAY

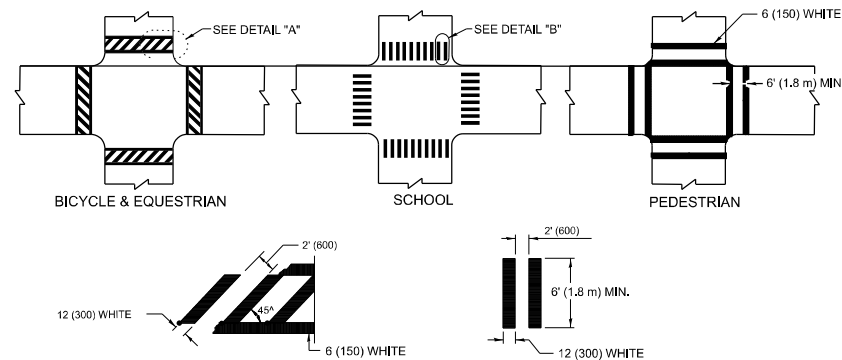


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

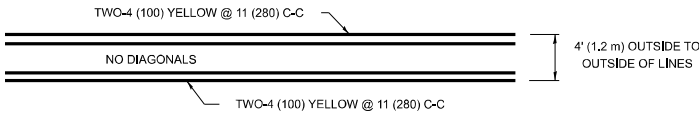


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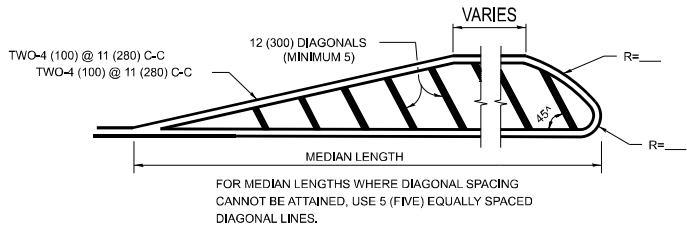
DETAIL "B"

TYPICAL CROSSWALK MARKING

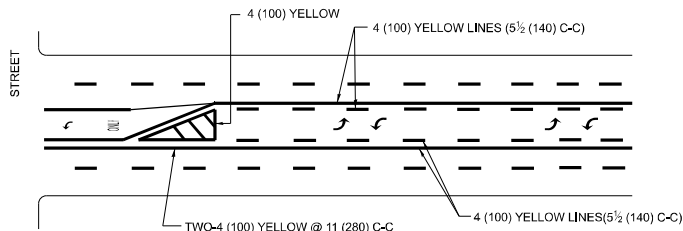
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

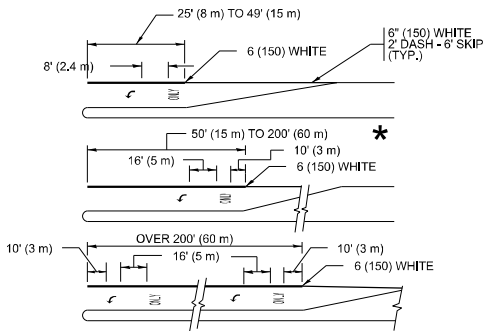


MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

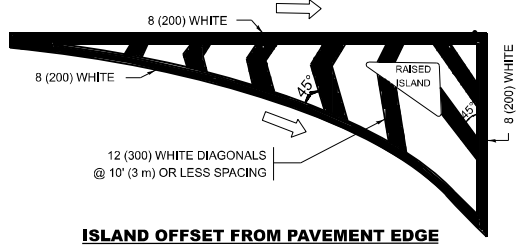


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

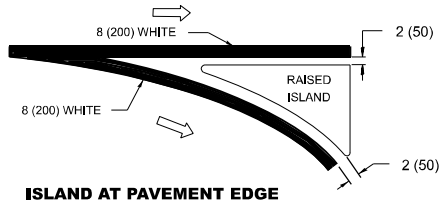
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

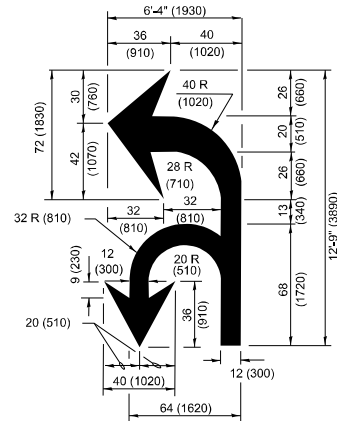


ISLAND OFFSET FROM PAVEMENT EDGE

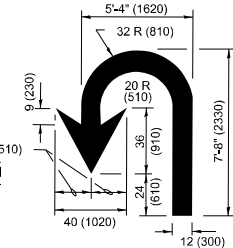


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Rana.Kalo	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
DRAWN -	REVISED - C. JUCIUS 07-01-13	
PLOT SCALE = 0.16666633" / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 6/21/2024	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	67
TC-13		CONTRACT NO. 62P40		
		ILLINOIS FED. AID PROJECT		

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

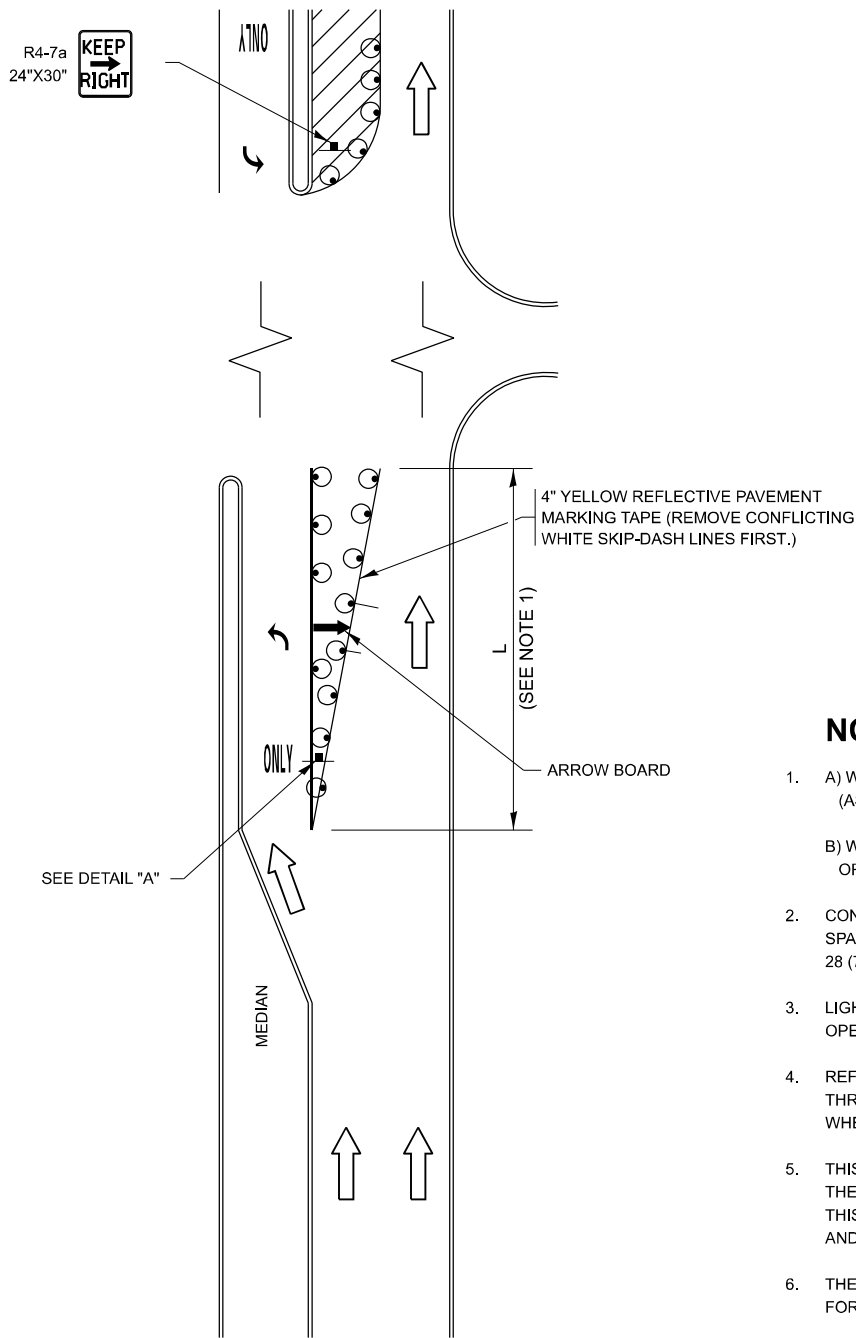
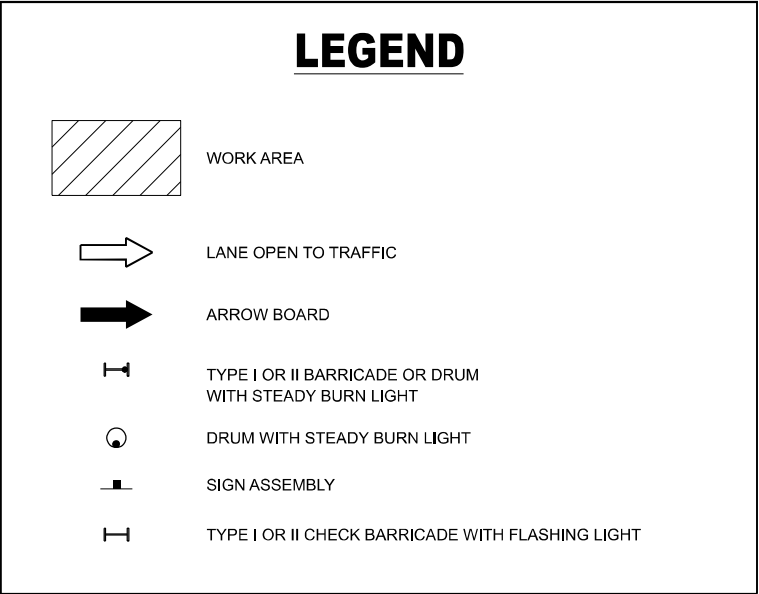


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

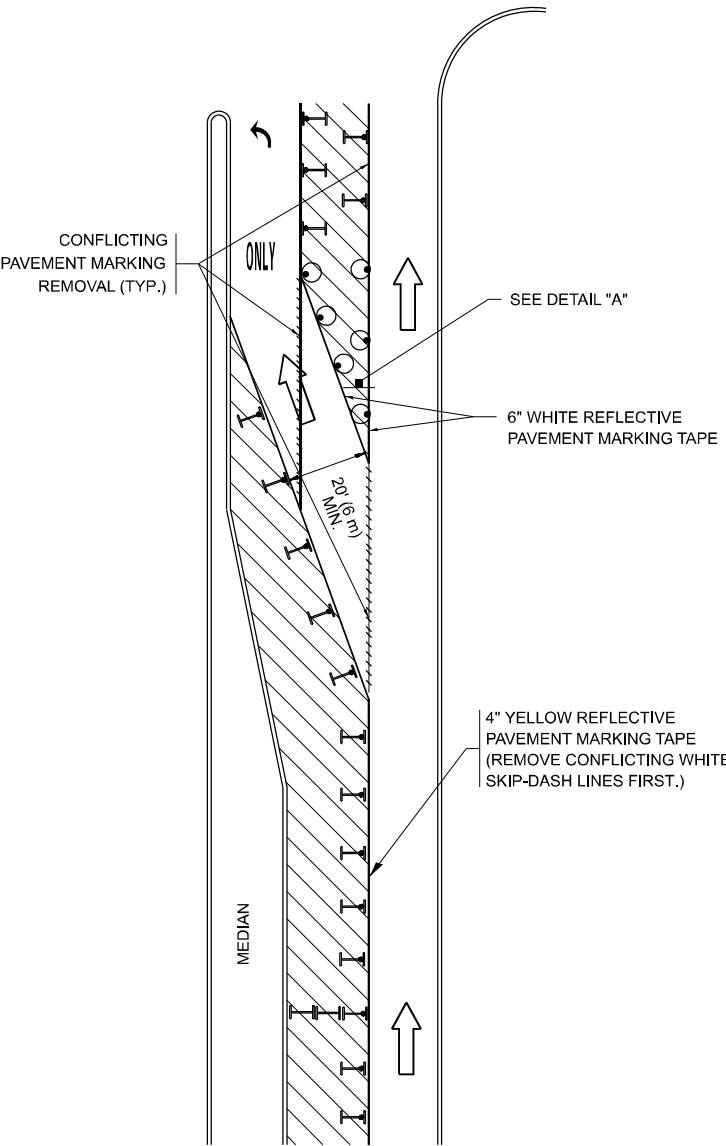
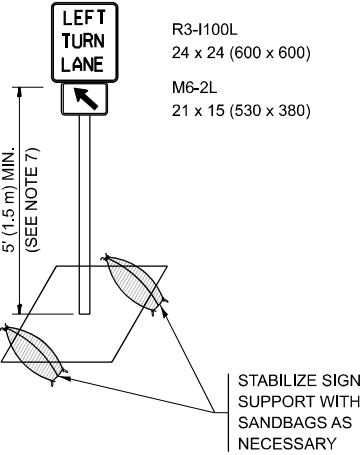


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-14 [Sheet]
FILE NAME: c:\p\work\kalkabom\0986322_0121021-sh-Dts\ds.dgn

USER NAME	= Rana.Kalo
DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
PLOT SCALE	= 0.16666633" / in.
PLOT DATE	= 6/21/2024

DESIGNED	- T. RAMMACHER 09-08-94	REVISED	- R. BORO 09-14-09
DRAWN	- A. HOUSEH 11-07-95	REVISED	- A. SCHUETZE 07-01-13
CHECKED	- A. HOUSEH 10-12-96	REVISED	- A. SCHUETZE 09-15-16
DATE	- T. RAMMACHER 01-06-00	REVISED	-

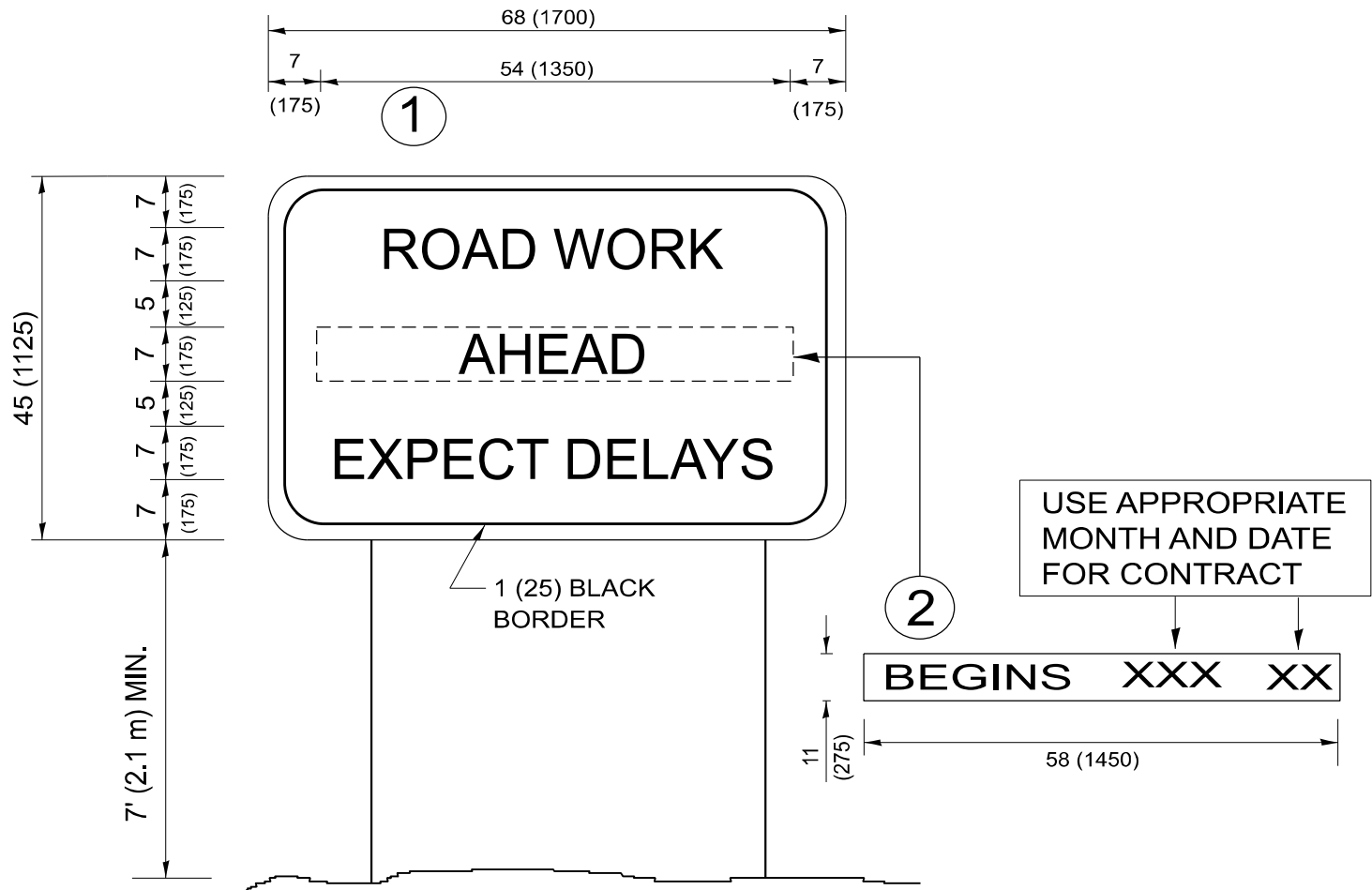
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2021-123-SW	LAKE	69	68
TC-14		CONTRACT NO. 62P40		
ILLINOIS		FED. AID PROJECT		

MODEL: TC-22 [Sheet]
FILE NAME: c:\p\work\work\kalor\0986322.D121021-sh-Dls\ds.dgn



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME	= Rana.Kalo	DESIGNED	-		REVISED	-	R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN					F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN	-		REVISED	-	R. MIRS 12-11-97		VAR.	2021-123-SW	LAKE	69	69					
	PLOT SCALE	= 0.16666633 "/in.	CHECKED	-		REVISED	-	T. RAMMACHER 02-02-99		TC-22					CONTRACT NO. 62P40				
	PLOT DATE	= 6/21/2024	DATE	-		REVISED	-	C. JUCIUS 01-31-07		ILLINOIS FED. AID PROJECT									
										SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				