

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	11-00049-00-SW	COOK	28	1

CONTRACT NO. 63732

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

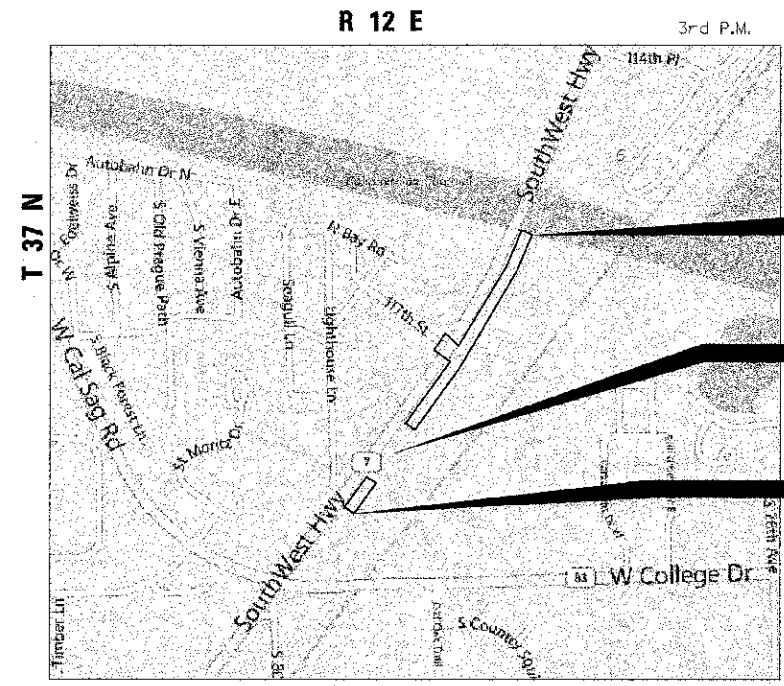
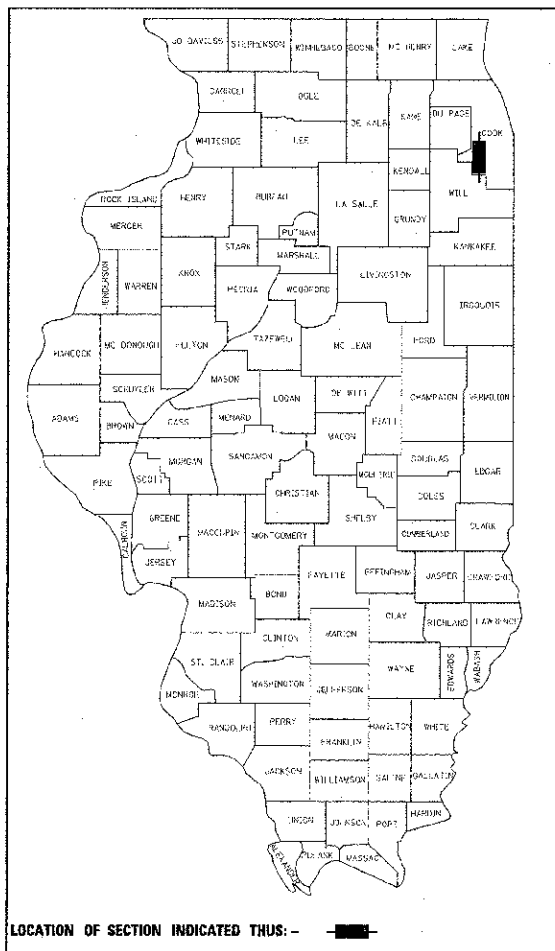
FAU RTE 3578 (SOUTHWEST HIGHWAY)
SOUTH OF 117TH STREET TO CAL-SAG CHANNEL
NEW SIDEWALK AND TRAFFIC SIGNAL MODIFICATION

SECTION 11-00049-00-SW
PROJECT M-9003(843)
CITY OF PALOS HEIGHTS
COOK COUNTY
JOB NO: C-91-641-11

PROJECT LOCATED IN
CITY OF PALOS HEIGHTS

- INDEX OF SHEETS**
- 1 COVER SHEET/INDEX OF SHEETS
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 - 20-22 DISTRICT 1 STANDARDS
 - 23-29 CROSS SECTIONS

DESIGN DESIGNATION = MINOR ARTERIAL
TRAFFIC VOLUME = 23,300 VPD
POSTED SPEED = 45 MPH



PROJECT ENDS
STATION 27+54.57

BEGIN OMISSION
STATION 11+69.19
END OMISSION
STATION 15+53.12

PROJECT BEGINS
STATION 10+18.05

LOCATION MAP
NOT TO SCALE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

VERTICAL CONTROL DATUM IS NGVD 29
AND BENCHMARKS APPLY TO THIS
PLAN SET ONLY.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

DESIGN DIG NO. A1372782:
T37N R9E: SW 1/4 SEC 1; SE 1/4 SEC 2; NE 1/4 SEC 11; NW 1/2 SEC 12

DESIGN DIG NO. A1372786:
T37N R9E: SE 1/4 SEC 12; NE 1/4 SEC 13
T37N R10E: SW 1/4 SEC 7; NW 1/4 SEC 18

PROJECT LENGTH	GROSS	NET
SOUTHWEST HWY SIDEWALK	1,736.52 FT	1,352.60 FT
TOTAL NET PROJECT LENGTH	1,736.52 FT (0.33 MILES)	1,352.60 FT (0.26 MILES)



DAVID D. LANDEWEER, P.E.
LICENSE NO.: 062-042363
EXPIRES: NOVEMBER 30, 2013
DATE: 6/29/12

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: June 29, 2012
Seth Smith, Dir. of P.W.
CITY OF PALOS HEIGHTS

Passed: July 23, 2012
C. J. Holt, District 1 Engineer of Local Roads and Streets

Releasing for Bid Based on Limited Review: July 24, 2012
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PLANS PREPARED BY:
URS 100 S. WACKER DR., SUITE 500 TEL (312)-939-1000
CHICAGO IL, 60606 FAX (312)-939-4198

PROGRAM AND OFFICE ENGINEER: Charles F. Riddle, P.E. (84 D) 705-4406 Schaumburg, IL

CONTRACT NO. 63732

GENERAL PLAN NOTES

GENERAL NOTES BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITY FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

ALL STATION-OFFSET CALL OUTS AND CURVE DATA ON THE PLANS REFER TO THE PROPOSED CENTERLINE UNLESS OTHERWISE SHOWN.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS

SPECIFICATIONS ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", MAY 1996 EDITION, THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", LATEST EDITION, THE DETAILS INCLUDED IN THE PLANS, AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

STANDARDS ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE CURRENT STANDARD OF IDOT AS LISTED ON THIS SHEET.

COORDINATION THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK, AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE CITY OF PALOS HEIGHTS. THE CONTRACTOR SHALL ALSO OBTAIN ANY AND ALL NECESSARY PERMITS REQUIRED BEFORE THE START OF ANY CONSTRUCTION.

PUBLIC OR PRIVATE UTILITIES THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND THE DEPARTMENT DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATIONS OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE "STANDARD SPECIFICATIONS", THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE UTILITY OWNERS SO THAT THEIR FACILITIES MAY BE ADJUSTED OR RELOCATED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS UNLESS OTHERWISE NOTED IN THE PLANS. ALL RELOCATION WORK ON EXISTING PRIVATE UTILITIES WILL BE DONE BY THE OWNER OF THAT UTILITY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE PROPERTY, AND SHALL RESTORE SUCH PROPERTY AT HIS/HER OWN EXPENSE IN ACCORDANCE WITH ARTICLE 107.20.

SURVEY MONUMENTS THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WHEN SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE MONUMENTS ARE REMOVED.

DISPOSAL OF MATERIALS THE CONTRACTOR WILL BE REQUIRED TO DISPOSE OF ALL SIDEWALK, CURB AND GUTTER, PAVEMENT, AND ALL OTHER MATERIALS EXCAVATED OR REMOVED DUE TO CONSTRUCTION OPERATIONS, AT HIS EXPENSE. NO PAYMENT WILL BE MADE FOR HAULING OR TRUCKING MATERIAL TO LOCATIONS PROVIDED BY THE CONTRACTOR, OUTSIDE THE LIMITS OF THE IMPROVEMENT IS INCLUDED IN THE COST OF THE INDIVIDUAL REMOVAL ITEMS.

EXISTING DRAINAGE STRUCTURES DURING CONSTRUCTION OPERATIONS, WHENEVER ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED BY THE CONTRACTOR AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE/SHE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS, AND DISCHARGE SAME. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

UNSLITABLE MATERIAL BEFORE REMOVAL OF ANY UNSUITABLE MATERIAL, THE CONTRACTOR SHALL TREAT THE SUBGRADE AS PER ARTICLE 301.03 OF THE "STANDARD SPECIFICATIONS" TO THE SATISFACTION OF THE ENGINEER. UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT OR FILL UNDER THE PROPOSED PATH AS SHOWN ON THE TYPICAL CROSS SECTIONS.

STOCKPILES STOCKPILES OF TOPSOIL AND OTHER MATERIALS SHALL NOT BE LOCATED WITHIN A WETLAND AND/OR FLOOD PLAIN. APPROVAL OF THE LOCATION MUST BE OBTAINED FROM THE ENGINEER PRIOR TO PLACEMENT. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, THE EROSION CONTROL MEASURES SHALL BE PROVIDED. STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

TREE TRIMMING ALL BRANCHES THAT ARE LESS THAN 12 FEET ABOVE THE SURFACE OF THE SIDEWALK SHALL BE REMOVED BY THE CONTRACTOR. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH ANSI STANDARD A300-2001 (PART 1) FOR TREE, SHRUB AND OTHER WOODY PLANT MAINTENANCE AND ANSI Z133.1-1994 FOR TREE CARE OPERATIONS PRUNING, TRIMMING, REPAIRING, MAINTAINING AND REMOVING TREES AND CUTTING BRUSH. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION AND WILL NOT BE PAID FOR SEPARATELY.

ACCESS TO ADJUTING PROPERTY THE CONTRACTOR SHALL MAINTAIN ACCESS TO ADJUTING PROPERTY DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER.

SAW CUTTING THE LIMITS OF REMOVAL OF ALL CONCRETE OR BITUMINOUS PAVEMENTS, CURBING OR SIDEWALK ADJACENT TO EXISTING LIKE PAVEMENTS, CURBING OR SIDEWALKS SHALL BE SAWCUT IN ACCORDANCE WITH SECTION 440 OF THE "STANDARD SPECIFICATIONS" AND AT THE DIRECTION OF THE ENGINEER. THE SAW CUTTING OF BITUMINOUS PAVEMENT, DRIVEWAYS, CURBING OR SIDEWALK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT AND WILL NOT BE PAID FOR AS SEPARATE ITEMS.

RESTORATION GRASS AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED WITH SIX INCHES TOPSOIL AND SEED OR SOD AS SPECIFIED.

PRESERVATION OF TREES AND SHRUBS THE CONTRACTOR SHALL REMOVE ONLY THOSE TREES AND SHRUBS SO DESIGNATED BY THE ENGINEER, OR THOSE WHICH DIRECTLY INTERFERE WITH THE SAFETY OR QUALITY OF CONSTRUCTION PRACTICES. THE CONTRACTOR SHALL EXERCISE EXTREME CARE WHEN WORKING NEAR EXISTING TREES AND SHRUBS TO AVOID DAMAGING THOSE NOT SCHEDULED FOR REMOVAL, AND SHALL REPLACE ANY DAMAGED PLANTS AT HIS/HER OWN EXPENSE.


EXISTING PAVEMENT AND APPURTENANCES WHEN PORTIONS OF EXISTING PAVEMENTS OR APPURTENANCES ARE TO REMAIN IN PLACE, OR ADJACENT EXISTING PAVEMENTS OR APPURTENANCES ARE TO REMAIN IN PLACE, THE CONTRACTOR SHALL FORM A PERPENDICULAR STRAIGHT JOINT BY FULL-DEPTH MACHINE SAWING AT THE ENDS AND ALL EDGES OF PORTIONS TO BE REMOVED TO PREVENT SURFACE SPALLING WHEN THE EXISTING PAVEMENT OR APPURTENANCE IS REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT OR APPURTENANCE TO REMAIN IN PLACE SHALL BE REPAIRED OR REMOVED AND REPLACED BY THE CONTRACTOR AT HIS/HER OWN EXPENSE, AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE MEASURED OR PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 280001-06 TEMPORARY EROSION CONTROL SYSTEMS
- 424001-06 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
- 424006 DIAGONAL CURB RAMPS FOR SIDEWALKS
- 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701606-08 URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701901-02 TRAFFIC CONTROL DEVICES
- 857001 01 STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
- 880006-01 TRAFFIC SIGNAL MOUNTING DETAILS

SEDIMENTATION AND EROSION CONTROL NOTES

- A. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- B. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE.
- C. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DISTURBED AREAS WITHIN 14 CALENDAR DAYS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCE. PERMANENT STABILIZATION SHALL BE DONE WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING OF THE SOIL.
- D. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED, OR OTHERWISE TREATED TO REMOVE SEDIMENT.
- E. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- F. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE GENERAL CONTRACTOR WILL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.
- G. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.
- H. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL PRIOR TO THE START OF ANY EARTHWORK.

 100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 939-4999	USER NAME: RUSBY# FIRST NAME: RUSBY# LAST NAME: RUSBY# PILOT DATE: RUSBY#	DESIGNED: KLM DRAWN: KLM CHECKED: DD DATE: JUN 29, 2012	REVISED: REVISED: REVISED: REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK GENERAL NOTES & STANDARDS	F.A.D. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 3578 11-00049-00-SW COOK 29 2	SCALE: N/A DRAWING NO. 1 OF 1 STA. TO STA. ILLINOIS FED. AID PROJECT CONTRACT NO. 63732
	URS PROJECT: 11-00049-00-SW						

TREE REMOVAL SCHEDULE			
LOCATION	OFFSET (FT)	6 TO 15 UNITS	
STATION	LT	RT	DIAMETER
18+40	1.5		9
22+50	8.8		12
23+35	9		12
		TOTAL	33

PROPOSED TREE SCHEDULE			
ITEM	LOCATION	OFFSET LT	NUMBER
	STATION	(FT)	(EACH)
TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEE TREE), 3" CALIPER, BALLED AND BURLAPPED	18+60	6.5	1
TREE, ACER RUBRUM (RED MAPLE), 3" CALIPER, BALLED AND BURLAPPED	19+70	6.5	1
TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEE TREE), 3" CALIPER, BALLED AND BURLAPPED	21+75	10	1
TREE, ACER RUBRUM (RED MAPLE), 3" CALIPER, BALLED AND BURLAPPED	22+25	10	1
TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEE TREE), 3" CALIPER, BALLED AND BURLAPPED	22+75	10	1
TREE, ACER RUBRUM (RED MAPLE), 3" CALIPER, BALLED AND BURLAPPED	23+25	10	1

EARTHWORK TABLE			
UNSUITABLE EXCAVATION (REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL)	UNSUITABLE MATERIAL ADJUSTED FOR SHRINKAGE	TOPSOIL FURNISH AND PLACE, 4"	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (UNSUITABLE MATERIAL)
CU YD	CU YD	CU YD	CU YD
217	162.75	70	92.75

SUMMARY OF QUANTITIES			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY 0021
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	33
20101100	TREE TRUNK PROTECTION	EACH	3
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	217
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	152
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	623
* 25000210	SEEDING, CLASS 2A	ACRE	0.25
* 25000350	SEEDING, CLASS 7	ACRE	0.25
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	22.5
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	22.5
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	22.5
* 25100115	MULCH, METHOD 2	ACRE	0.25
* 25200110	SODDING, SALT TOLERANT	SQ YD	418
28000400	PERIMETER EROSION BARRIER	FOOT	875
28000500	INLET AND PIPE PROTECTION	EACH	2
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	115
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	17
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	19
42001300	PROTECTIVE COAT	SQ YD	59
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5,896
42400800	DETECTABLE WARNINGS	SQ FT	186
44000100	PAVEMENT REMOVAL	SQ YD	36
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	256
44000300	CURB REMOVAL	FOOT	17
44000600	SIDEWALK REMOVAL	SQ FT	59
* 50601720	BICYCLE RAILING	FOOT	335
60255500	MANHOLES TO BE ADJUSTED	EACH	2
60600605	CONCRETE CURB, TYPE B	FOOT	22
67100100	MOBILIZATION	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1

SUMMARY OF QUANTITIES			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY 0021
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	201
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	140
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	152
* 78000850	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	124
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA	FOOT	8
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	391
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	405
* 87501200	TRAFFIC SIGNAL POST, 16 FT.	EACH	1
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	4
* 87900200	DRILL EXISTING HANDHOLE	EACH	1
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	2
* 89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	1
* 89502200	MODIFY EXISTING CONTROLLER	EACH	1
X4400220	CURB REMOVAL AND REPLACEMENT	FOOT	106
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	49
X8140115	HANDHOLE TO BE ADJUSTED	EACH	3
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	127
Z0013302	SEGMENTAL CONCRETE BLOCK WALL	SQ FT	892
Z0300001	Aggregate Subgrade Improvement	CU YD	44
Z0042300	PORTLAND CEMENT CONCRETE SIDEWALK CURB	FOOT	12
* A2001024	TREE, ACER RUBRUM (RED MAPLE), 3" CALIPER, BALLED AND BURLAPPED	EACH	3
* A2005024	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEE TREE), 3" CALIPER, BALLED AND BURLAPPED	EACH	3

* SPECIALTY ITEMS



100 S. WACKER DR., SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 935-4989

DESIGNED: KLM
DRAWN: KLM
CHECKED: TTT
DATE: JUN 11, 2011

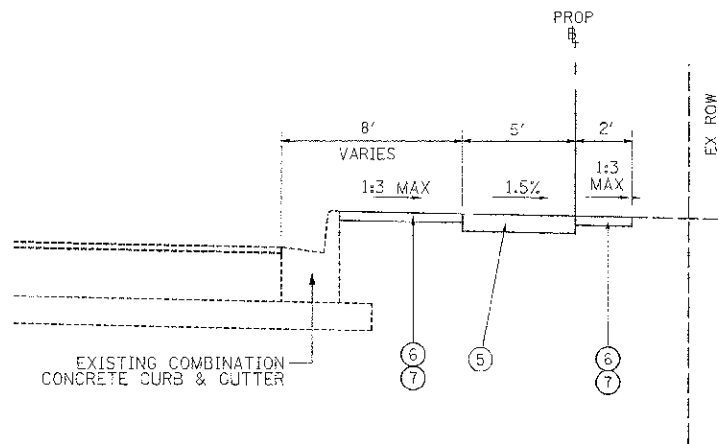
REVISIONS:
REVISED
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REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTHWEST HIGHWAY SIDEWALK
SUMMARY OF QUANTITIES AND SCHEDULES

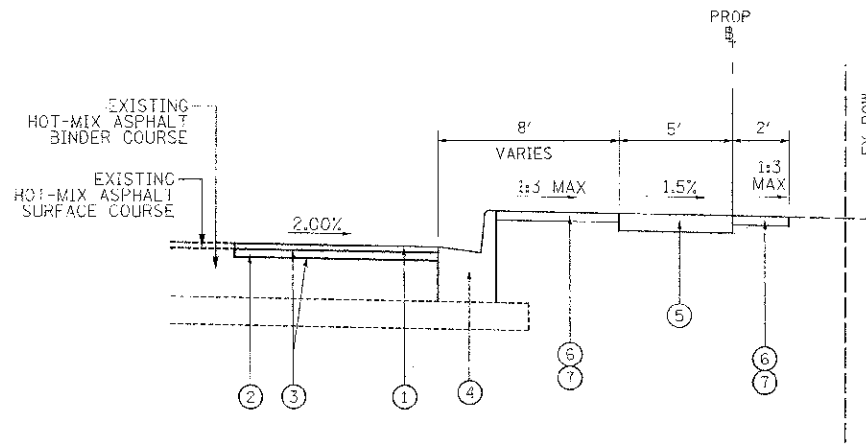
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F.A.B. R.E. SECTION COUNTY TOTAL SHEET NO.
3578 11-C0049-00-SW COOK 29 3
CONTRACT NO. 63732



PROPOSED TYPICAL SECTION
N.T.S.

STA. 10+18.05 TO STA. 11+69.19, SOUTHWEST HWY
 STA. 15+53.12 TO STA. 20+07.82, SOUTHWEST HWY
 STA. 21+21.37 TO STA. 23+85, SOUTHWEST HWY
 STA. 27+20 TO STA. 27+54.57, SOUTHWEST HWY



PROPOSED TYPICAL SECTION
N.T.S.

STA. 20+07.82 TO STA. 21+21.37, SOUTHWEST HWY

LEGEND

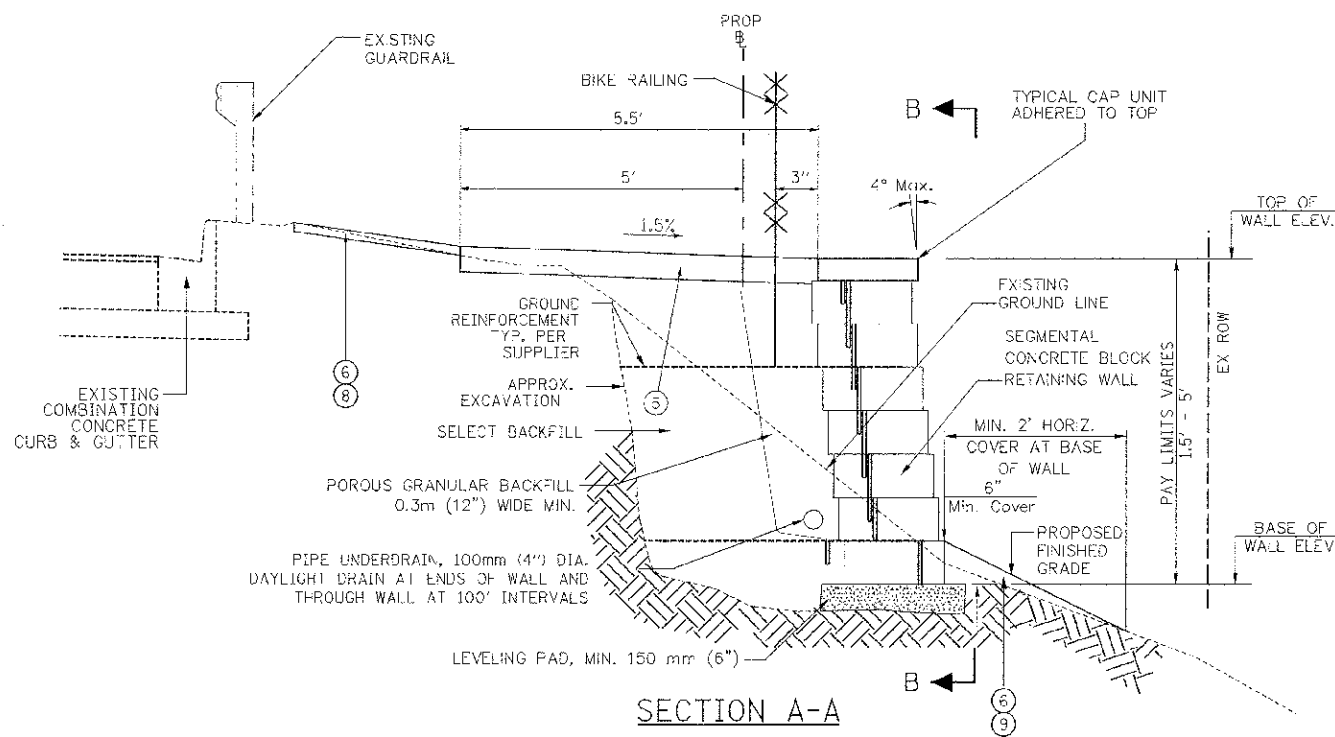
- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2")
- ② LEVELING BINDER (MACHINE METHOD), N70
- ③ BITUMINOUS MATERIALS (PRIME COAT)
- ④ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑤ PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- ⑥ TOPSOIL EXCAVATION AND PLACEMENT (4" OR AS NOTED)
- ⑦ SODDING, SALT TOLERANT
- ⑧ SEEDING, CLASS 2A
- ⑨ SEEDING, CLASS 7

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	AIR VOIDS & NDES
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70 (IL 9.5mm; 1 1/2")	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD) MIX "D", N70 (3/4" TO 1 1/2")	4% @ 70 Gyr.

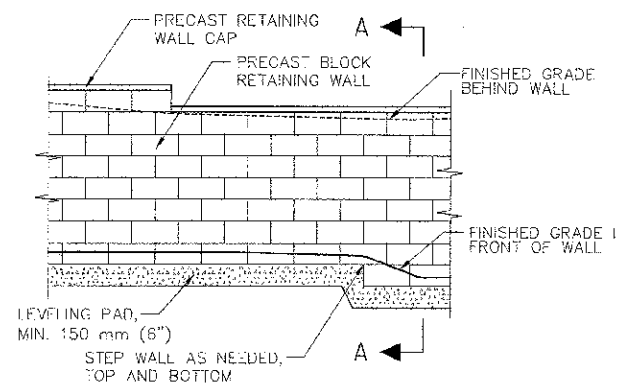
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



SECTION A-A
SEGMENTAL CONCRETE BLOCK
RETAINING WALL DETAIL
N.T.S.

STA. 23+85 TO STA. 27+20, SOUTHWEST HWY



SECTION B-B
ELEVATION
N.T.S.

SEGMENTAL CONCRETE BLOCK WALL NOTES

1. THE CONTRACTOR SHALL SUBMIT DESIGN, CONSTRUCTION PLANS WITH MATERIAL INFORMATION & SHOP DRAWINGS FOR CONNECTION DETAILS SIGNED BY A STRUCTURAL ENGINEER LICENSED IN THE STATE OF ILLINOIS TO THE ENGINEER FOR APPROVAL BEFORE ORDERING MATERIALS.
2. RETAINING WALL EQUIVALENT FLUID PRESSURE = 50(PCF)
3. REINFORCING BARS SHALL BE GRADE 60
4. GROUT MORTAR, f'm = 2500 psi (MIN.)
5. THE DESIGN OF THE RETAINING WALL SHALL BE IN COMPLIANCE WITH THE GUIDELINES FOR THE DESIGN OF MECHANICALLY STABILIZED EARTH WALLS AS DEVELOPED BY AASHTO-AGC-ARTA JOINT COMMITTEE TASK FORCE 27 GROUND MODIFICATION SYSTEMS.
6. EXCAVATION, LEVELING PAD, BACKFILL, PIPE UNDERDRAIN, GROUND REINFORCEMENT, GROUND RESTORATION, AND SEEDING OF RESTORED GROUND ARE INCLUDED IN COST OF CONCRETE BLOCK RETAINING WALL.
7. INSTALL BIKE RAILING POST SLEEVE DURING RETAINING WALL CONSTRUCTION TO PREVENT DAMAGE TO GROUND REINFORCEMENT.

SEGMENTAL CONCRETE BLOCK
RETAINING WALLS SITTING DATA

STATION	TOP OF WALL ELEV. FEET	BASE OF WALL ELEV. FEET	WALL HEIGHT FEET
23+89	604.36	600.40	3.96
24+00	604.08	600.40	3.68
24+75	604.08	600.61	3.47
25+00	604.81	601.52	3.29
26+00	605.54	603.51	2.03
27+27	608.08	607.05	1.03



100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 833-4100
FAX (312) 833-4198

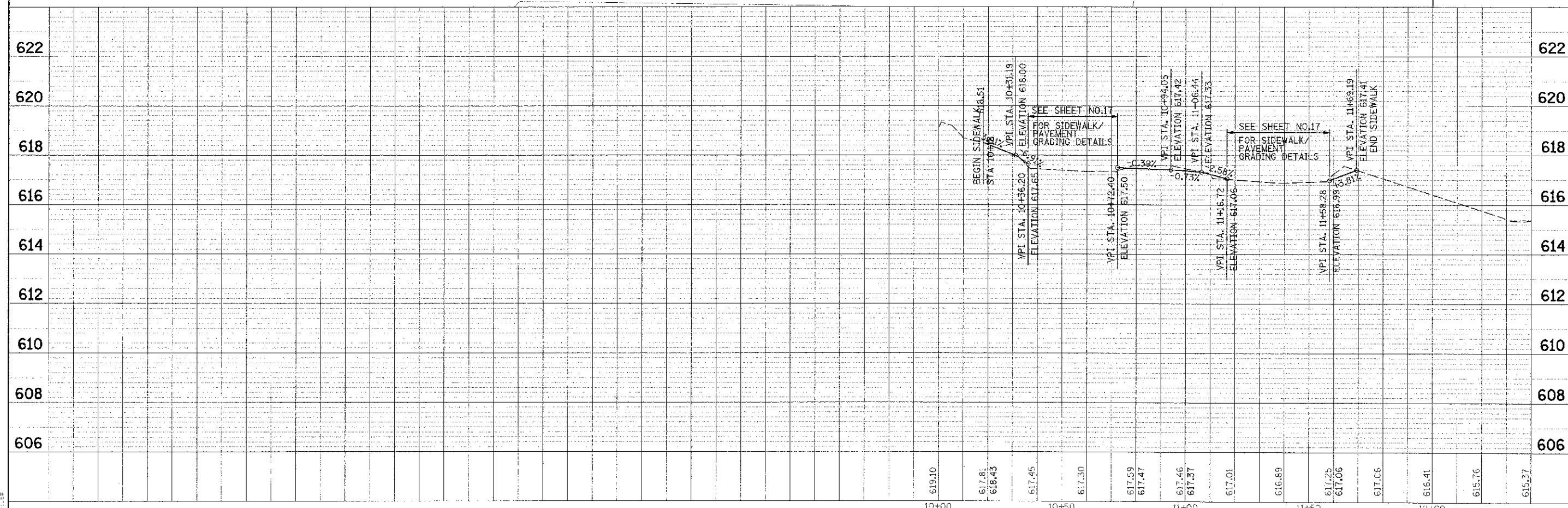
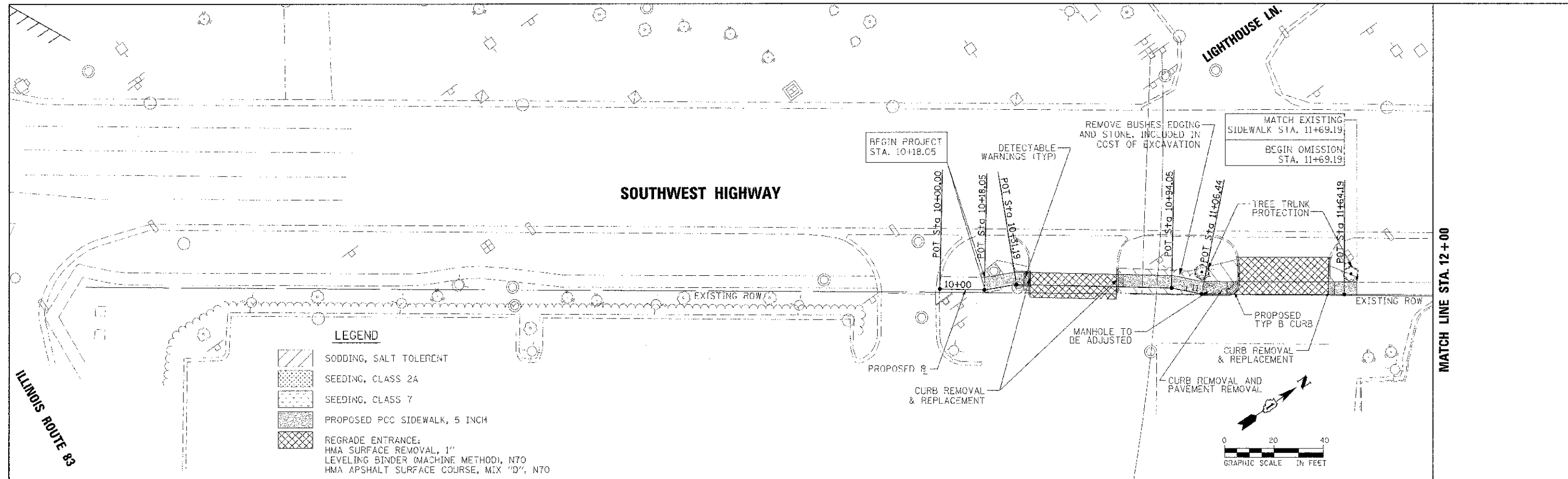
DESIGNED: KLM
DRAWN: KLM
CHECKED: DDL
DATE: JUNE 29, 2012

REVISIONS:
REVISED: -
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REVISED: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTHWEST HIGHWAY SIDEWALK
TYPICAL SECTIONS

SCALE: DRAWING NO. OF STA. TO STA.
F.A.U. RTE. 3578 SECTION 11-00049-00-54 COUNTY COOK TOTAL SHEETS 29 NO. 4 CONTRACT NO. 63732



PLAN
 DATE: 06/29/12
 DRAWN: KLM
 CHECKED: DDL
 DATE: 06/29/12

PROFILE
 DATE: 06/29/12
 DRAWN: KLM
 CHECKED: DDL
 DATE: 06/29/12



DESIGNED: KLM
 DRAWN: KLM
 CHECKED: DDL
 DATE: JUN 29, 2012

REVISED: -
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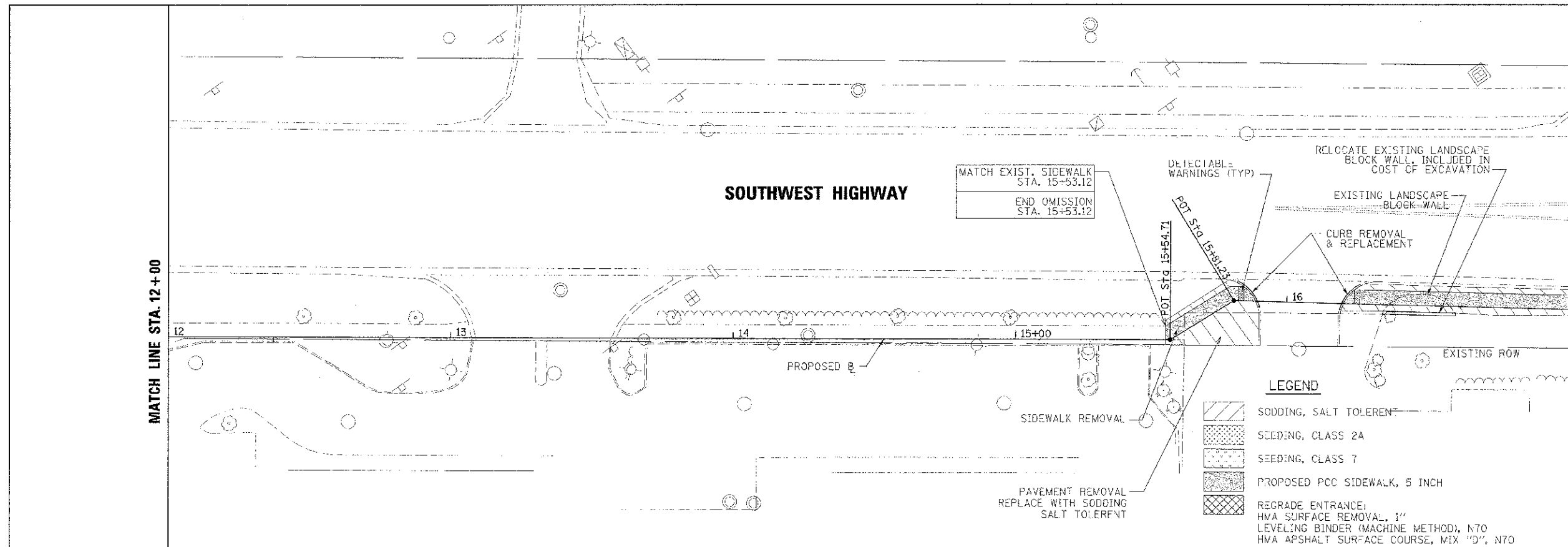
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SOUTHWEST HIGHWAY SIDEWALK
 PLAN & PROFILE - 1
 SCALE: H=2" V=2" SHEET NO. 1 OF 4 SHEETS STA. 10+00 TO STA. 12+00

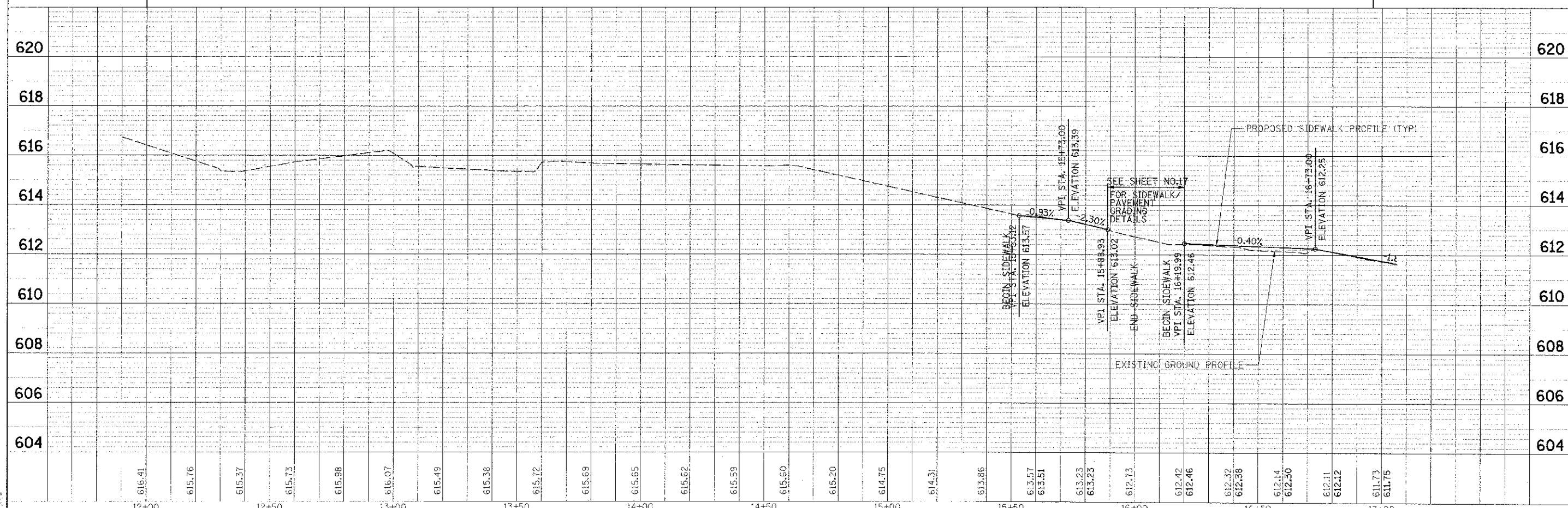
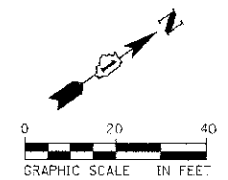
F.A.U. NO. 3578 SECTION 11-00049-00-SW COUNTY COOK TOTAL SHEETS 29 SHEET NO. 5 CONTRACT NO. 63732

PLAN	DATE
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	
NO. 6	
NO. 7	
NO. 8	
NO. 9	
NO. 10	

DESCRIPTION	DATE
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	
NO. 6	
NO. 7	
NO. 8	
NO. 9	
NO. 10	



MATCH LINE STA. 17+00



	USER NAME	DESIGNED <i>KLM</i>	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK PLAN & PROFILE - 2	FAU	SECTION	COUNTY	TOTAL SHEETS
	PRINT SCALE	DRAWN <i>KLM</i>	REVISED			3578	11-00049-00-SW	COOK	29
DATE	CHECKED <i>DDI</i>	DATE <i>JUN 29, 2012</i>	REVISED	SCALE: H=20' V=2' SHEET NO. 2 OF 4 SHEETS STA. 12+00 TO STA. 17+00		CONTRACT NO. 63732			

PROPOSED CURVE DATA

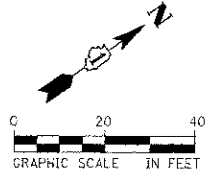
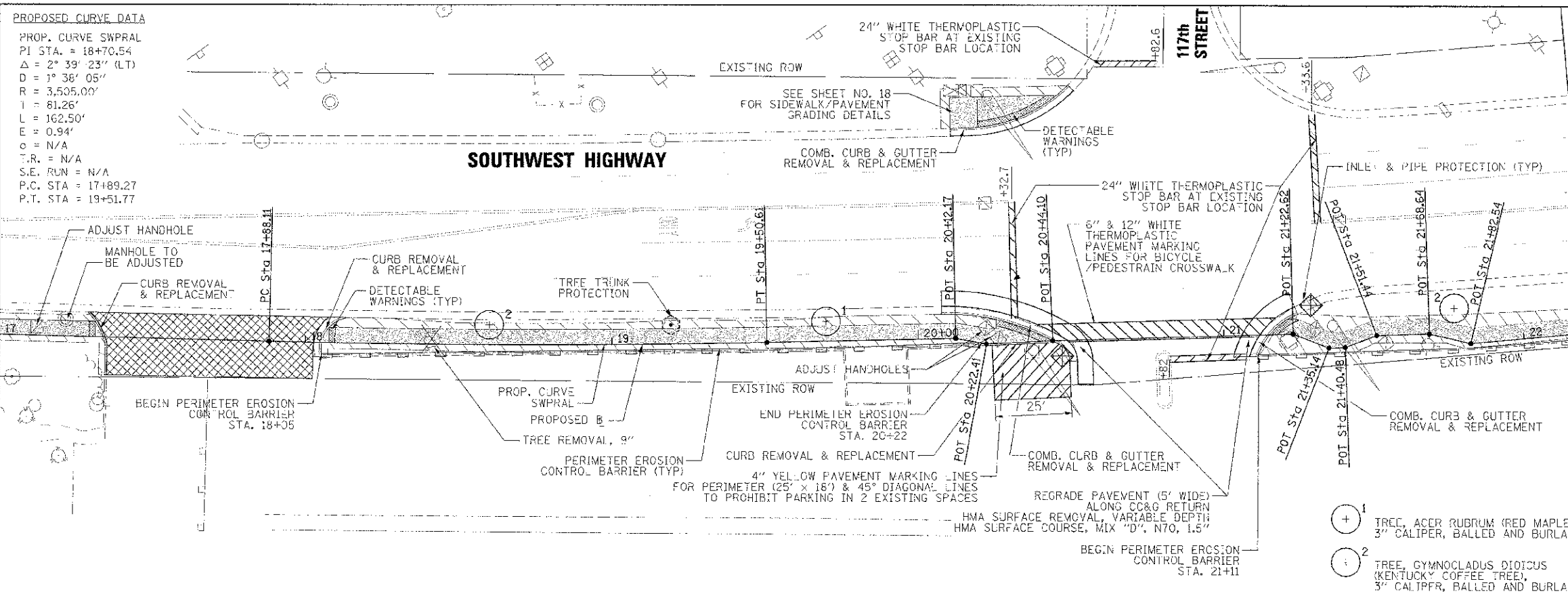
PROP. CURVE SWPRAL
 PI STA. = 18+70.54
 $\Delta = 2^\circ 39' 23''$ (LT)
 $D = 1^\circ 36' 05''$
 $R = 3,505.00'$
 $L = 162.50'$
 $E = 0.94'$
 $c = N/A$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 P.C. STA = 17+89.27
 P.T. STA = 19+51.77

LEGEND

- SODDING, SALT TOLERANT
- SEEDING, CLASS 2A
- SEEDING, CLASS 7
- PROPOSED PCC SIDEWALK, 5 INCH
- REGRADE ENTRANCE: HMA SURFACE REMOVAL, 1" LEVELING BINDER (MACHINE METHOD), N70
- HMA ASPHALT SURFACE COURSE, MIX "D", N70

MATCH LINE STA. 17+00

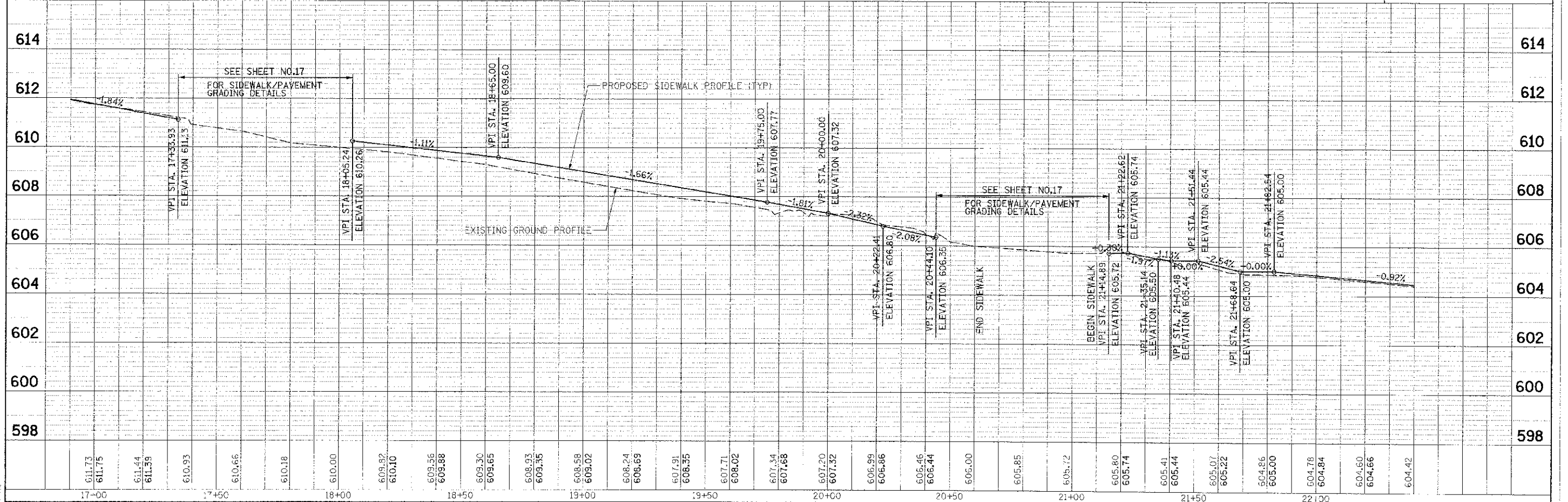
MATCH LINE STA. 22+25



- 1 TREE, ACER RUBRUM (RED MAPLE), 3" CALIPER, BALLED AND BURLAPPED
- 2 TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEE TREE), 3" CALIPER, BALLED AND BURLAPPED

DATE	
SCALE	
PROJECT	
NO.	
DATE	
SCALE	
PROJECT	
NO.	
DATE	

DATE	
SCALE	
PROJECT	
NO.	
DATE	
SCALE	
PROJECT	
NO.	
DATE	



DESIGNED	KLM	REVISED	
DRAWN	KLM	REVISED	
CHECKED	DDI	REVISED	
DATE	JUNE 29, 2012	REVISED	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

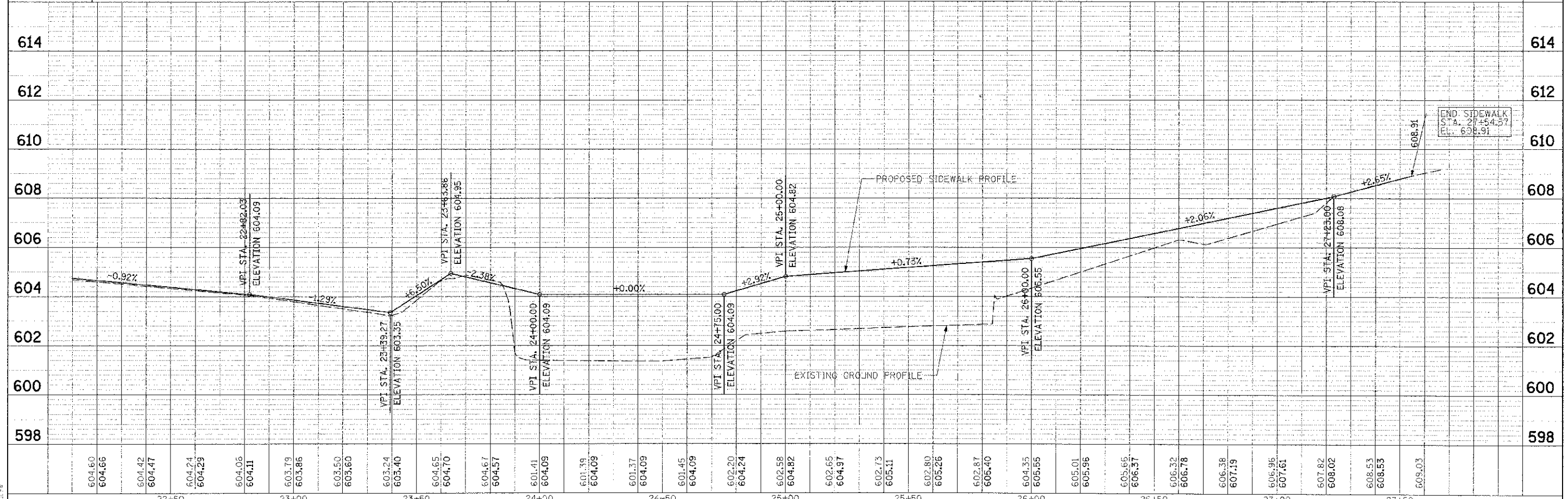
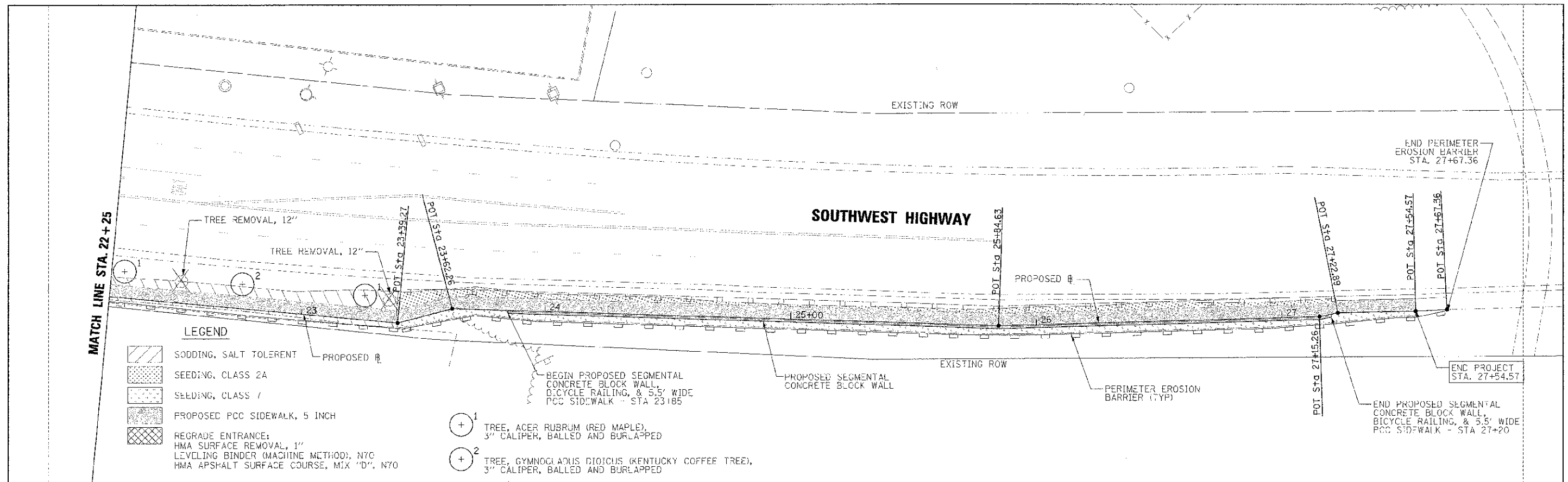
**SOUTHWEST HIGHWAY SIDEWALK
 PLAN & PROFILE - 3**

SCALE: H=20' V=2' SHEET NO. 3 OF 4 SHEETS STA. 17+00 TO STA. 22+25

PLAN NO.	SECTION	COUNTY	TOTAL SHEETS
3373	11-00049-00-5W	COOK	29
			7
			CONTRACT NO. 63732

PROJECT: SW HIGHWAY SIDEWALK
 DATE: 06/28/2012
 DRAWN BY: KLM
 CHECKED BY: DDI
 DATE: 06/28/2012

PROJECT: SW HIGHWAY SIDEWALK
 DATE: 06/28/2012
 DRAWN BY: KLM
 CHECKED BY: DDI
 DATE: 06/28/2012



604.60	604.66	604.42	604.47	604.24	604.29	604.06	604.11	603.79	603.86	603.50	603.60	603.24	603.40	604.65	604.70	604.67	604.57	601.41	604.09	601.39	604.09	601.37	604.09	601.45	604.09	602.20	604.24	602.58	604.82	602.65	604.97	602.73	605.11	602.80	605.26	602.87	605.40	604.35	605.55	605.01	605.96	605.66	606.37	606.32	606.78	606.38	607.19	606.96	607.61	607.82	608.02	608.53	608.53	609.03
22+50		23+00		23+50		24+00		24+50		25+00		25+50		26+00		26+50		27+00		27+50																																		



DESIGNED: KLM
 DRAWN: KLM
 CHECKED: DDI
 DATE: JUNE 28, 2012

REVISIONS:

1	REVISED	
2	REVISED	
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4	REVISED	

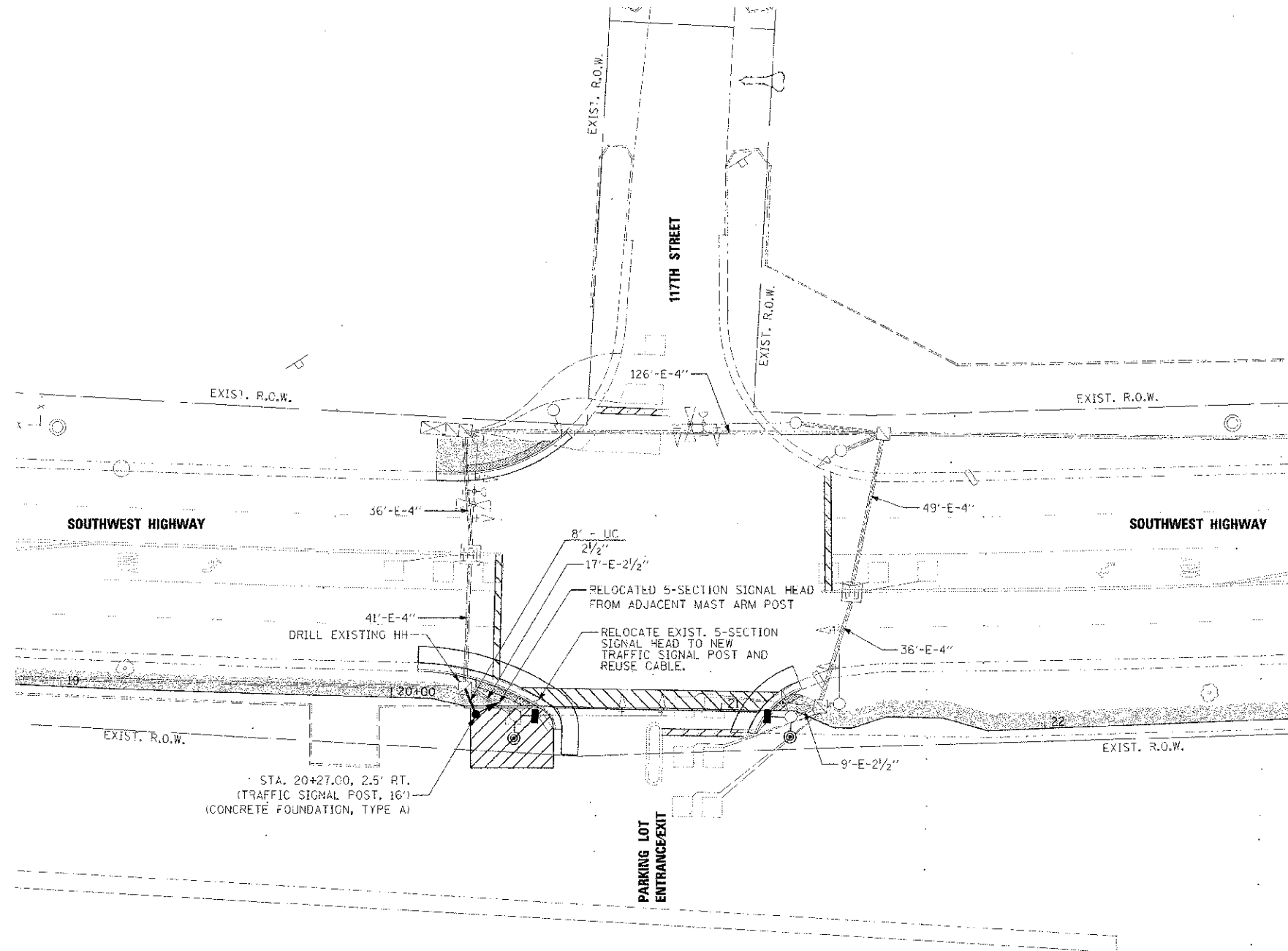
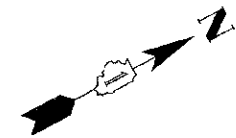
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SOUTHWEST HIGHWAY SIDEWALK
 PLAN & PROFILE - 4

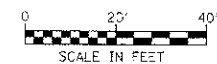
SCALE: H=20' V=2' SHEET NO. 4 OF 4 SHEETS STA. 22+25 TO STA. 27+59

SECTION	COUNTY	TOTAL SHEETS
11-00049-00-SW	COOK	29
CONTRACT NO. 63732		

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOG, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



1. ALL PEDESTRIAN SIGNAL HEADS TO BE L.E.D. (LIGHT EMITTING DIODE).
2. ANY DAMAGE TO THE PROPOSED SIDEWALKS SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT NO ADDITIONAL COST.
3. FOR DETAILS SEE DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS.



P.L.T. NAME - 01151



100 S. WACKER DR.
SUITE 500
CHICAGO IL 60606
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FAX (312) 433-4196

DESIGNED	KLM
DRAWN	KLM
CHECKED	DDL
DATE	JUNE 29, 2012

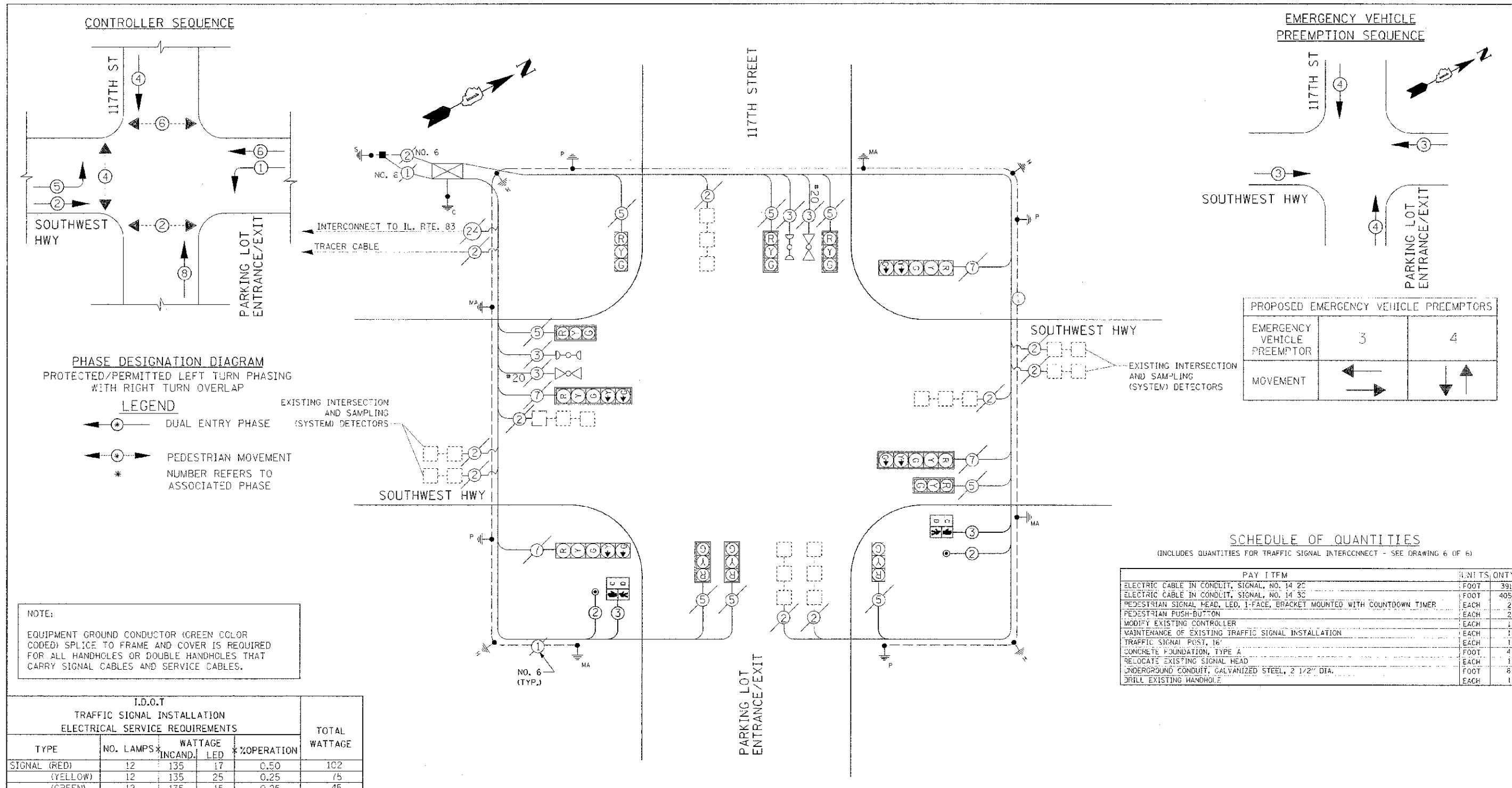
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

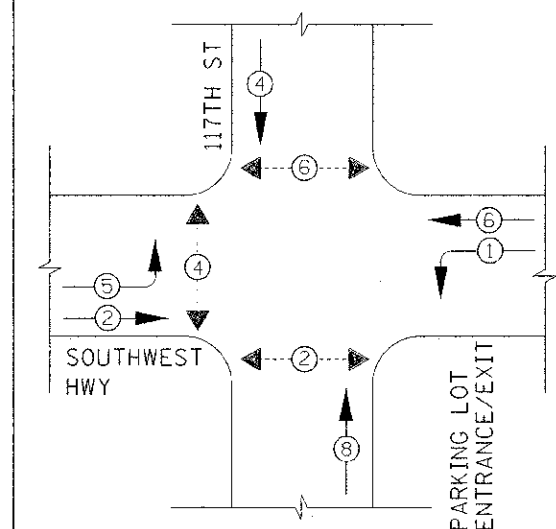
TRAFFIC SIGNAL PLAN
SOUTHWEST HIGHWAY AND 117TH STREET

SCALE: 1" = 20' DRAWING NO. OF ST4. TO STA.

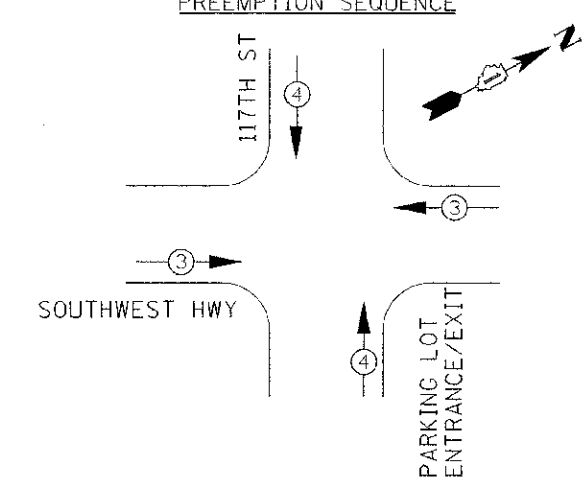
F.A.U. RT. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3378	11-00049-00-SW	COOK	29	9
				CONTRACT NO. 63732
ILLINOIS FED. AID PROJECT				



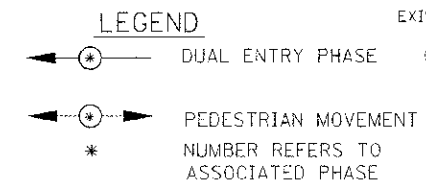
CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE



PHASE DESIGNATION DIAGRAM
PROTECTED/PERMITTED LEFT TURN PHASING WITH RIGHT TURN OVERLAP



EXISTING INTERSECTION AND SAMPLING (SYSTEM) DETECTORS

EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

SCHEDULE OF QUANTITIES

(INCLUDES QUANTITIES FOR TRAFFIC SIGNAL INTERCONNECT - SEE DRAWING 6 OF 6)

DESCRIPTION	UNITS	QTY.
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	391
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	405
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
PEDESTRIAN PUSH-BUTTON	EACH	2
MODIFY EXISTING CONTROLLER	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
TRAFFIC SIGNAL POST, 16"	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	4
RELOCATE EXISTING SIGNAL HEAD	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	8
DRILL EXISTING HANDHOLE	EACH	1

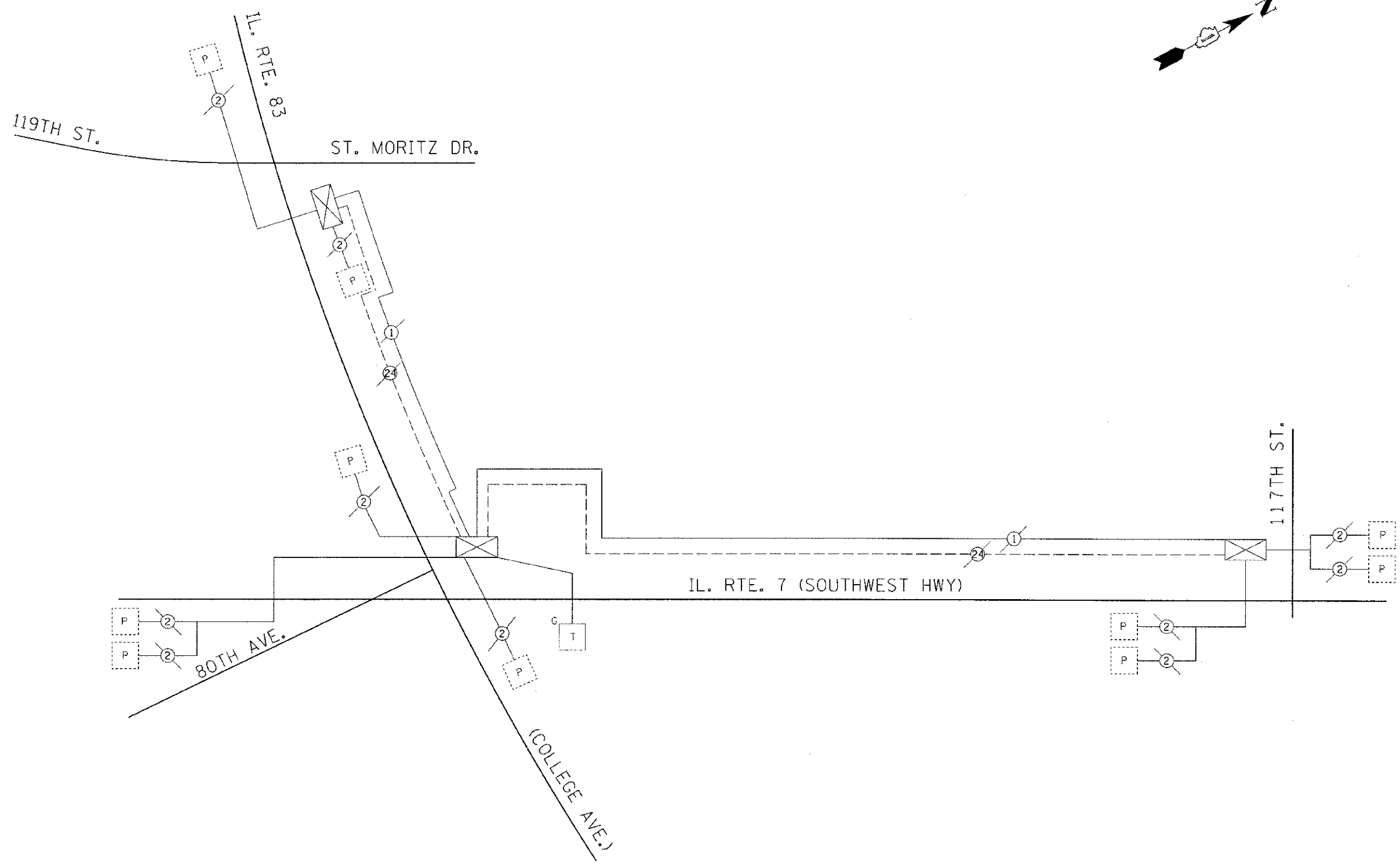
NOTE:
EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	8	135	12	0.10	9.6
PED. SIGNAL	2	90	25	1.00	50
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	-	84	-	0.50	-
FLASHER LED	-	-	-	0.50	-
ENERGY COSTS TO:				TOTAL =	381.6

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: RON RITIER
PHONE: (630) 420-4183
COMPANY: NAPERVILLE DPU-E

NOTES:
1. ALL PEDESTRIAN SIGNAL LENSES SHALL BE L.E.D. TYPE.

RESTORATION OF WORK AREA RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



URS | 100 S. WACKER DR. | CHICAGO, IL 60606 | TEL: (312) 939-1000 | FAX: (312) 939-4938
 USER NAME = 405834
 DESIGNED KLM
 DRAWN KLM
 CHECKED DDL
 DATE JUNE 29, 2012
 REVISIONS
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 SCALE: N.T.S. | DRAWING NO. OF STA. TO STA. | INTERCONNECT SCHEMATIC | SOUTHWEST HIGHWAY AND 117TH STREET | F.A.I.L. RTE. 3578 | SECTION 11-00049-00-SW | COUNTY COOK | TOTAL SHEETS 29 | SHEET NO. 11 | CONTRACT NO. 63732 | ILLINOIS FED. AID PROJECT

URS
 100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60606
 TEL: (312) 939-1000
 FAX: (312) 939-4938

USER NAME = 405834
 DESIGNED KLM
 DRAWN KLM
 CHECKED DDL
 DATE JUNE 29, 2012

REVISIONS
 REVISED -
 REVISED -
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 REVISED -

SCALE: N.T.S. | DRAWING NO. OF STA. TO STA. | INTERCONNECT SCHEMATIC | SOUTHWEST HIGHWAY AND 117TH STREET | F.A.I.L. RTE. 3578 | SECTION 11-00049-00-SW | COUNTY COOK | TOTAL SHEETS 29 | SHEET NO. 11 | CONTRACT NO. 63732 | ILLINOIS FED. AID PROJECT

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

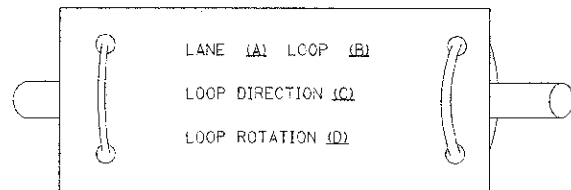
**INTERCONNECT SCHEMATIC
 SOUTHWEST HIGHWAY AND 117TH STREET**

F.A.I.L. RTE. 3578 | SECTION 11-00049-00-SW | COUNTY COOK | TOTAL SHEETS 29 | SHEET NO. 11 | CONTRACT NO. 63732 | ILLINOIS FED. AID PROJECT

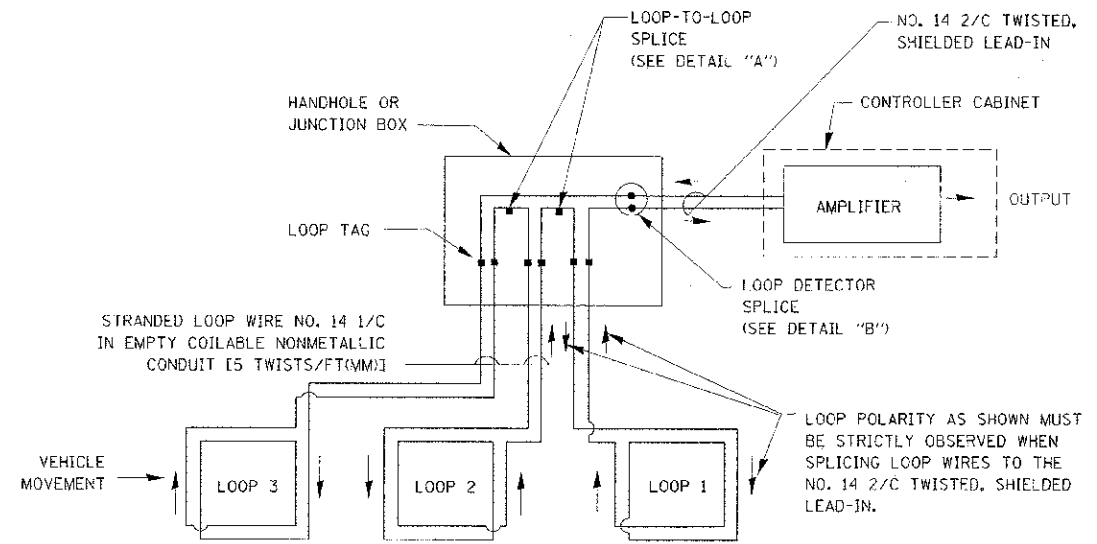
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

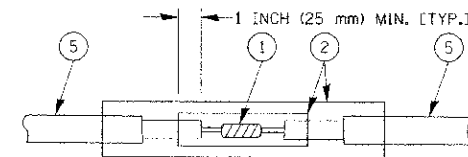


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

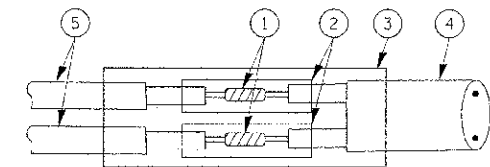


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

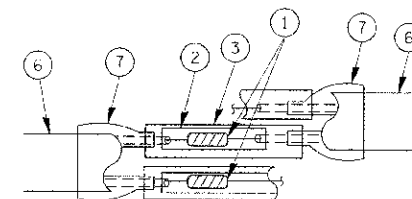


**DETAIL "A"
LOOP-TO-LOOP SPLICE**



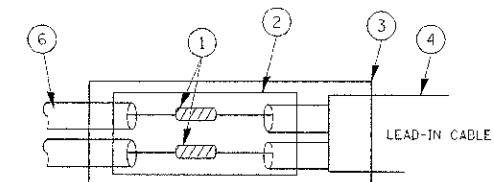
**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**

PREFORMED LOOP

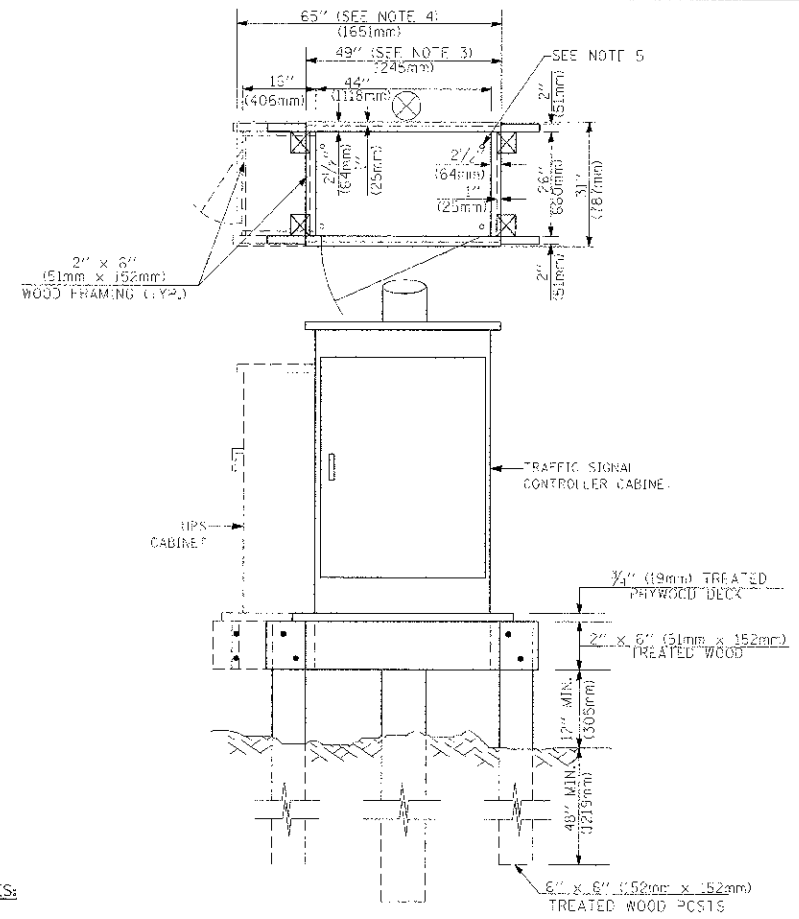
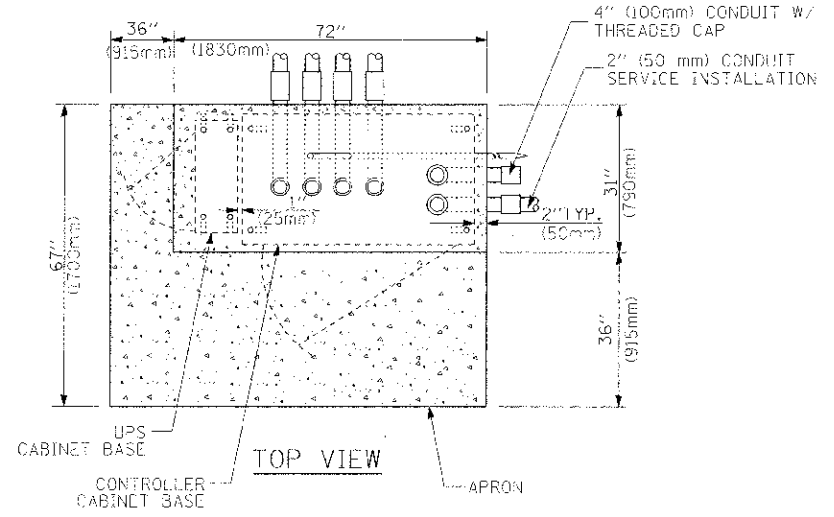
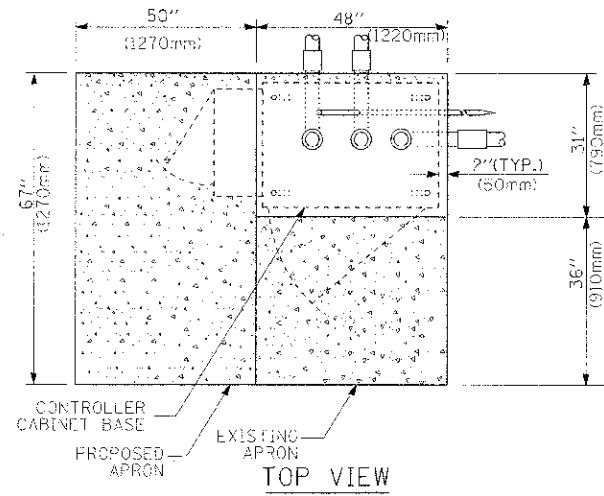


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

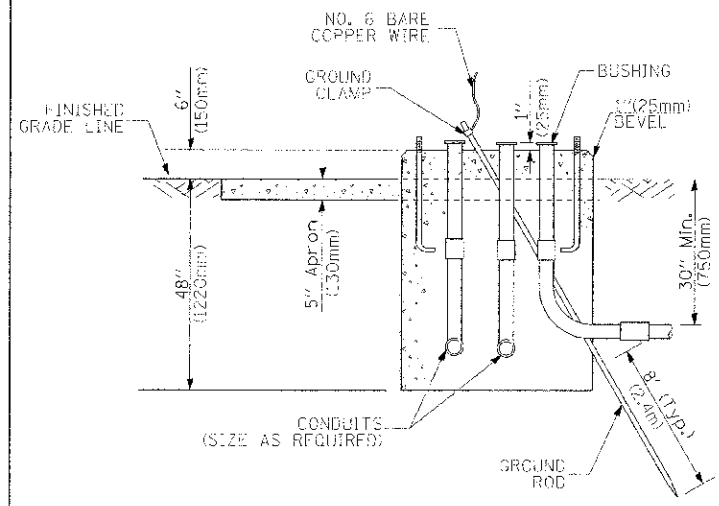
- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME -	USER NAME - bjaurel	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. R.T.E. 3578	SECTION 11-00049-00-SW	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 12	
PLT SCALE = 50:0000 1/2" = 1'	CHECKED - DAD	REVISED -	SCALE: NONE			SHEET NO. 1 OF 6 SHEETS	STA. TO STA.	TS-05		CONTRACT NO. 63732	
PLT DATE = 11/17/2009	DATE - 10-28-09	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

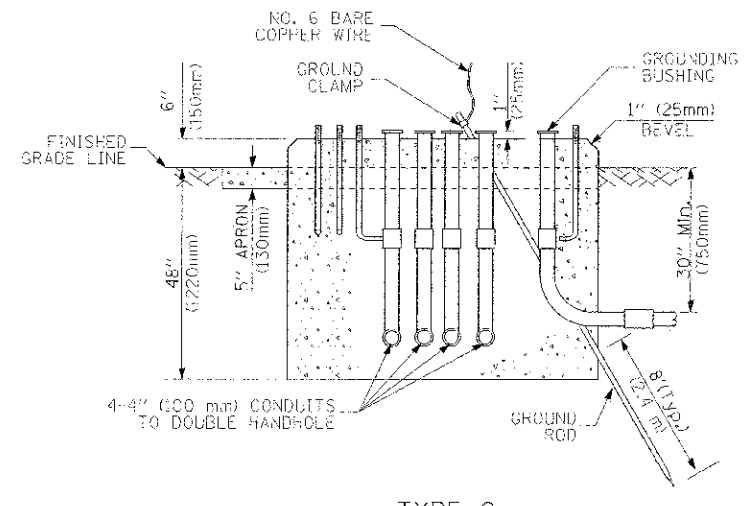


- NOTES:**
- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 - DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 - FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM



TYPE D FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET



TYPE C FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

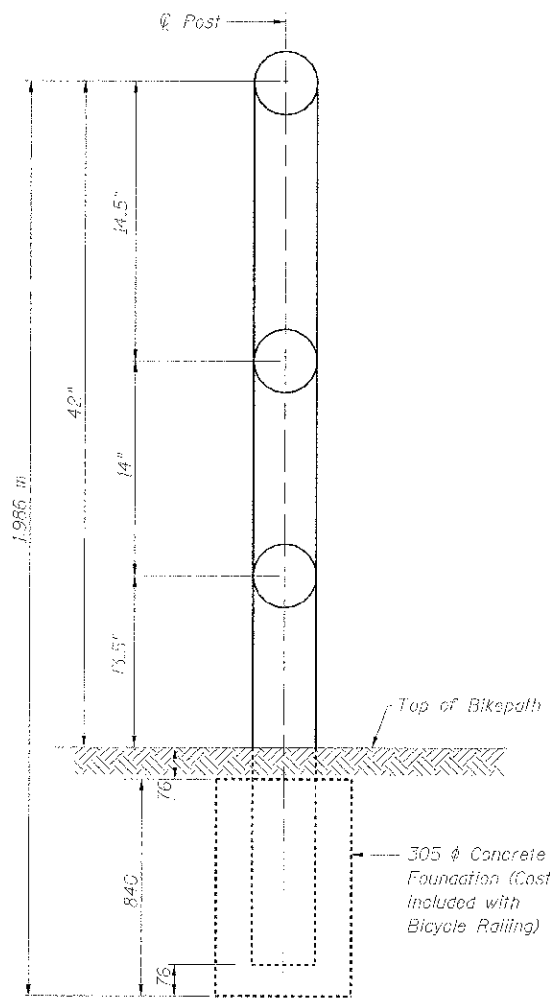
MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	36" (915mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	36" (915mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	17'-0" (5.1 m)	36" (915mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and less than 55' (16.8 m)	13'-0" (4.0 m)	36" (915mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m)	15'-0" (4.6 m)	36" (915mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 - Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (915 mm) diameter foundations.
 - Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 - For mast arm assemblies with dual arms refer to state standard 878301.

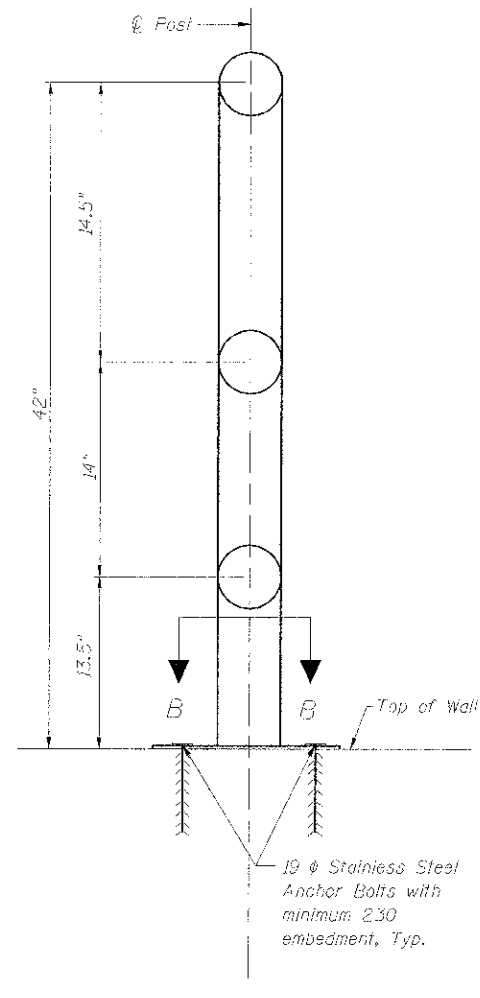
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

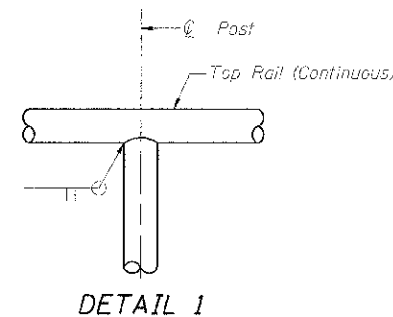
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																	
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																				
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																				
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																				
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																				
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																				
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F 5M12F																				
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F 5M12F																				
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)																				
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																				
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																				
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED																				
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR																				
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				EXISTING PREFORMED INTERSECTION LOOP DETECTOR																				
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				PREFORMED SAMPLING (SYSTEM) DETECTOR																				
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 50%;"></th> <th style="width: 25%;">EXISTING</th> <th style="width: 25%;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td></td> <td></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td></td> <td></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td></td> <td></td> </tr> <tr> <td>CROSSING GATE</td> <td></td> <td></td> </tr> <tr> <td>CROSSBUCK</td> <td></td> <td></td> </tr> </tbody> </table>				EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
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CROSSBUCK																												
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT																								
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER																								
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED																								
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																								
MICROWAVE VEHICLE SENSOR																												
VIDEO DETECTION CAMERA																												
VIDEO DETECTION ZONE																												
PAN, TILT, ZOOM CAMERA																												
WIRELESS DETECTOR SENSOR																												
WIRELESS ACCESS POINT																												



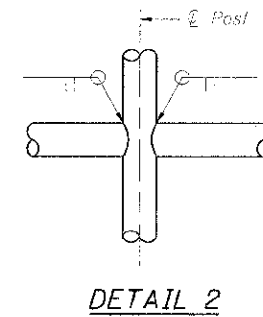
DETAIL - GROUND MOUNTED RAIL POST



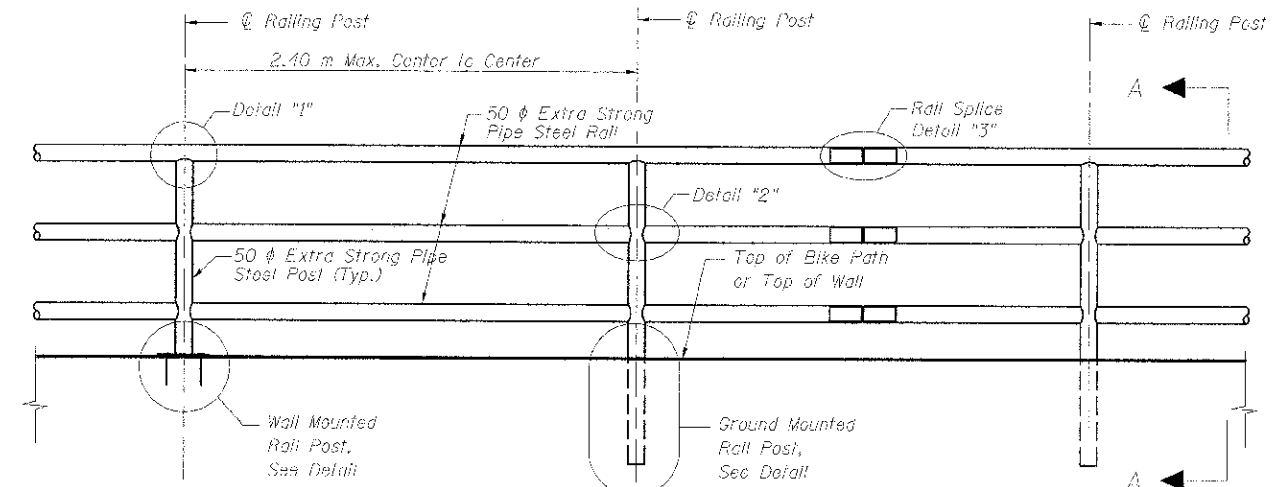
DETAIL - WALL MOUNTED RAIL POST



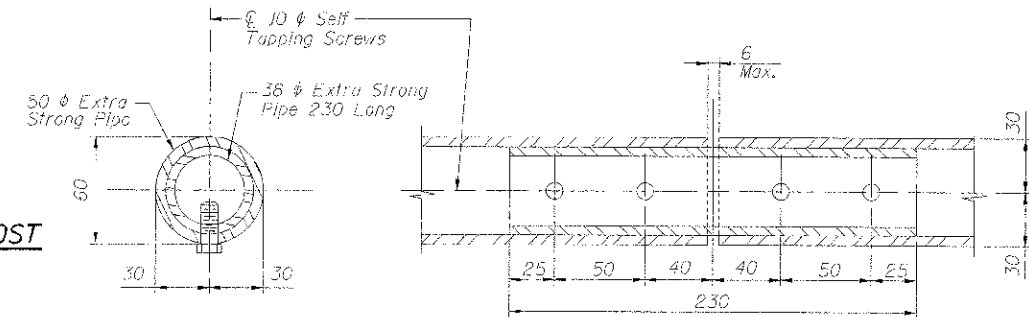
DETAIL 1



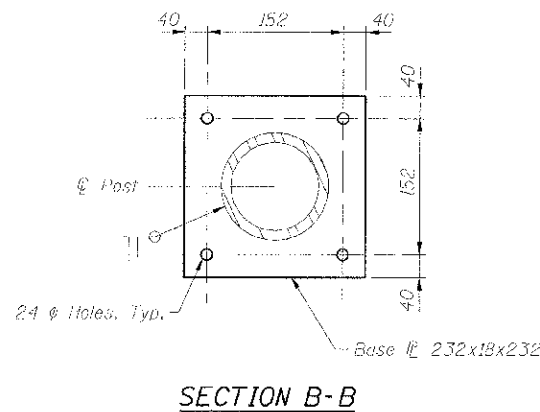
DETAIL 2



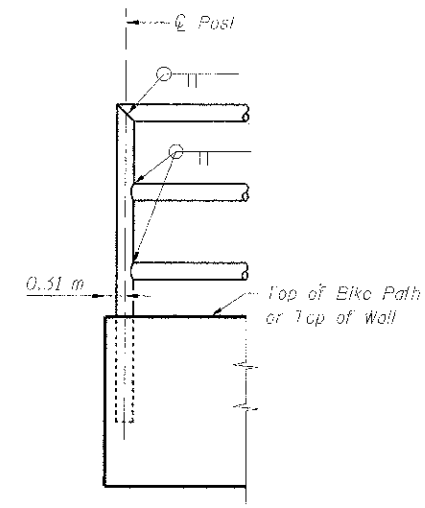
TYPICAL BICYCLE RAILING ELEVATION



DETAIL 3 - RAIL SPLICE



SECTION B-B



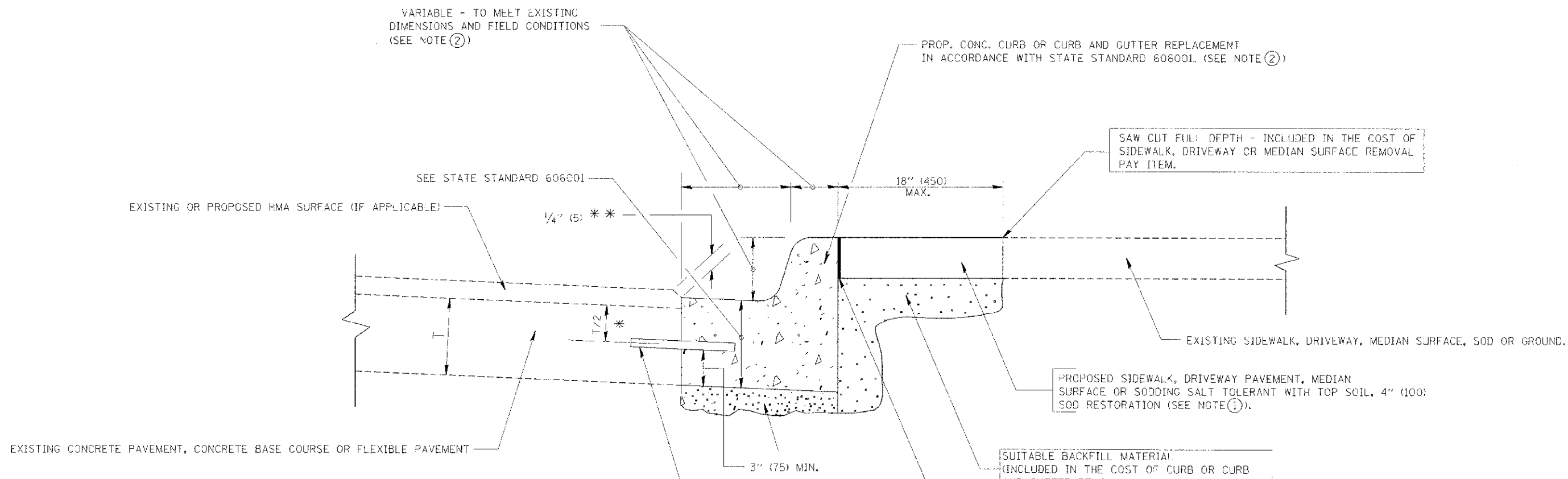
ELEVATION SHOWING END POST

NOTES:

1. The Contractor shall submit design, construction plans with material information, and shop drawings, including connection details, signed by a Structural Engineer licensed in the state of Illinois to the Engineer for approval before ordering materials.
2. Railing shall be according to Section 509 of the Standard Specifications, except as noted, and will be paid for at the contract unit price per meter.
3. Hollow structural steel tubing shall conform to the requirements of ASTM designation A 500, Grade B, Structural Steel Tubing.
4. All other steel shapes and plates shall conform to the requirements of AASHTO M-270M, Grade 345.
5. All posts, railing, splices, anchor devices, and bent plates shall be galvanized after shop fabrication according to AASHTO M-111 and ASTM A-385. All bolts, nuts and washers shall be galvanized according to AASHTO M-232 except stainless steel bolts as noted.
6. Vent holes for galvanizing shall be placed in the posts and rails at locations that will not allow the accumulation of moisture in the members.
7. Rail to match profile of Sidewalk.

FILE NAME: \\snp\p01\projects\11-00049-00-SW\Drawings\Railings\Bike Railing Details.dgn

URS 100 SWACKER DR. SUITE 500 CHICAGO, IL 60605 TEL (312) 939-1000 FAX (312) 939-4998	USER NAME: KJSEPK FILE SCALE: 24x36mm = 1/2" = 1" PLOT DATE: 7/27/2012	DESIGNED: KLM DRAWN: KLM CHECKED: DDI DATE: JUNE 29, 2012	REVISED: - REVISED: - REVISED: - REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK BIKE RAILING DETAILS	F.A.J. RTE. 3578 SECTION 11-00049-00-SW COUNTY COOK TOTAL SHEETS 29 SHEET NO. 19 CONTRACT NO. 63732 ILLINOIS FED. AID PROJECT
	SCALE: 1" = 1'-0" DRAWING NO. OF STA. TO STA.					



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

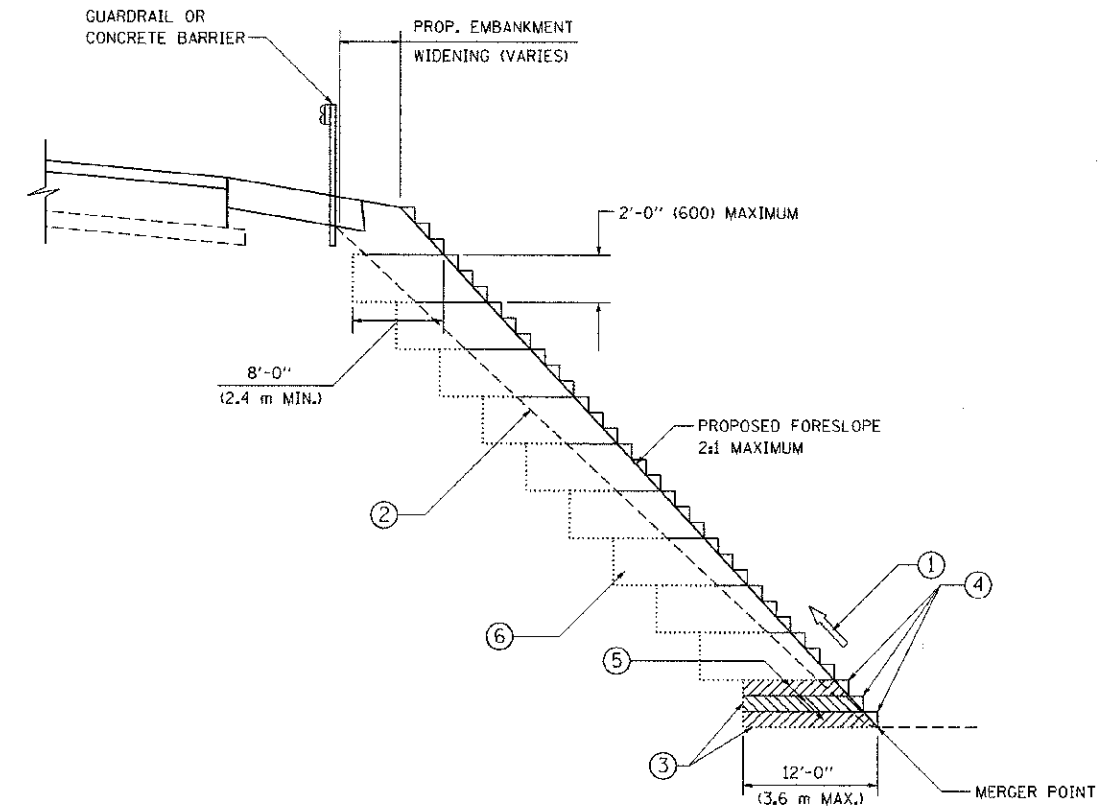
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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PLGT SCALE: AS SHOWN	CHECKED: -	REVISED: M. GOMEZ 01-22-01	REVISED: R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. _____	TO STA. _____	CONTRACT NO. 63732		
PLGT DATE: 12/15/2009	DATE: 03-11-94	REVISED: -	REVISED: -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR 3578 THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

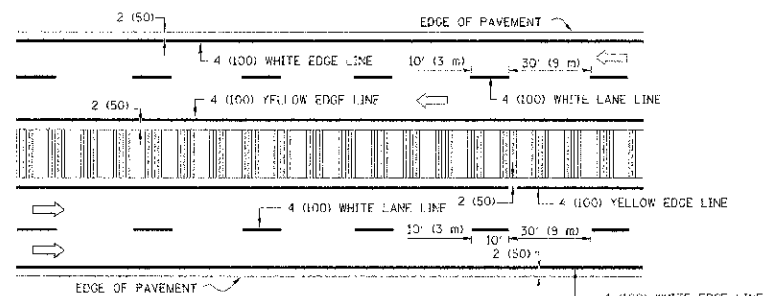
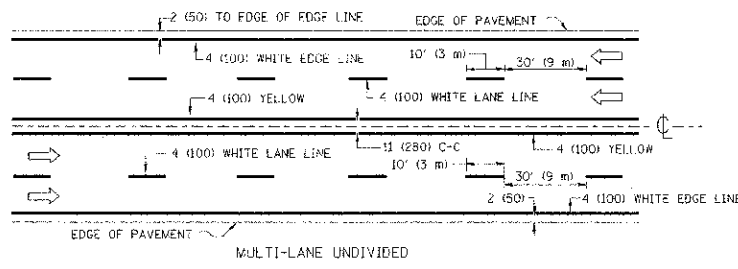
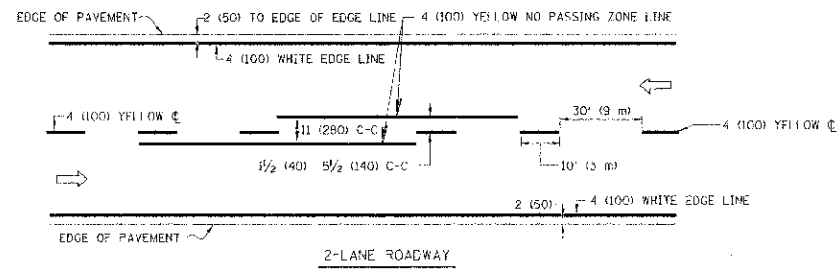
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL
FOR EMBANKMENT WIDENING**

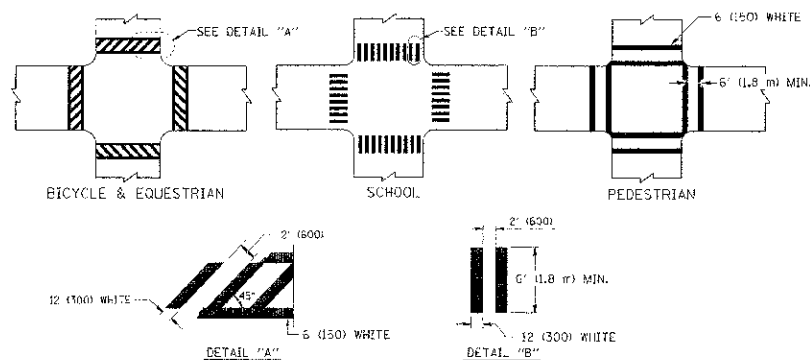
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	11-00049-00-SW	COOK	29	21
BD-51			CONTRACT NO. 63732	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

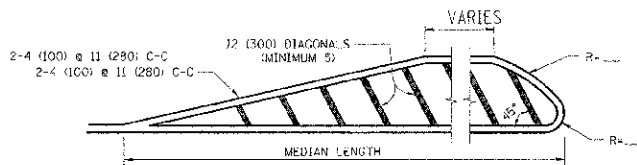
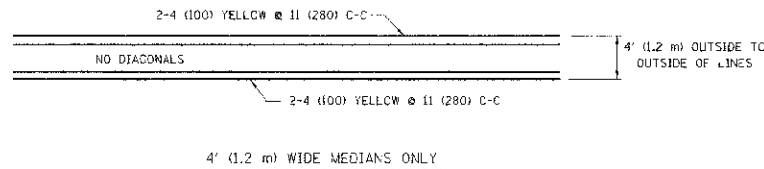


NOTE: MEDIANS WITH BARRIER CURBS DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

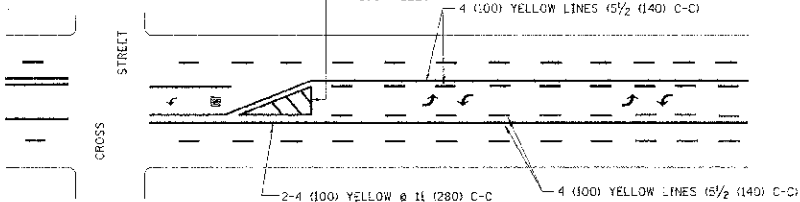


TYPICAL CROSSWALK MARKING

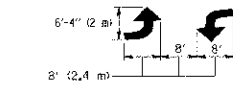


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (OVER 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

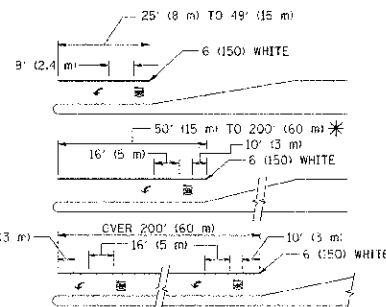


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

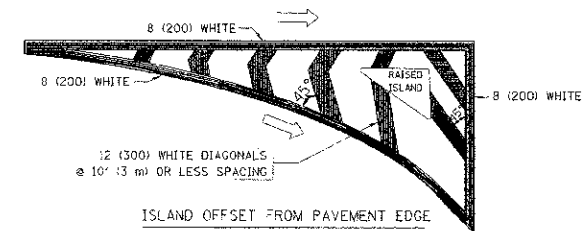


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

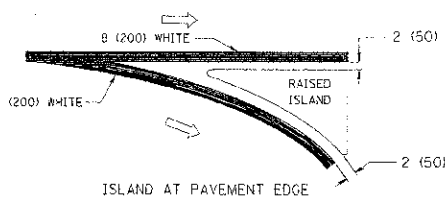
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

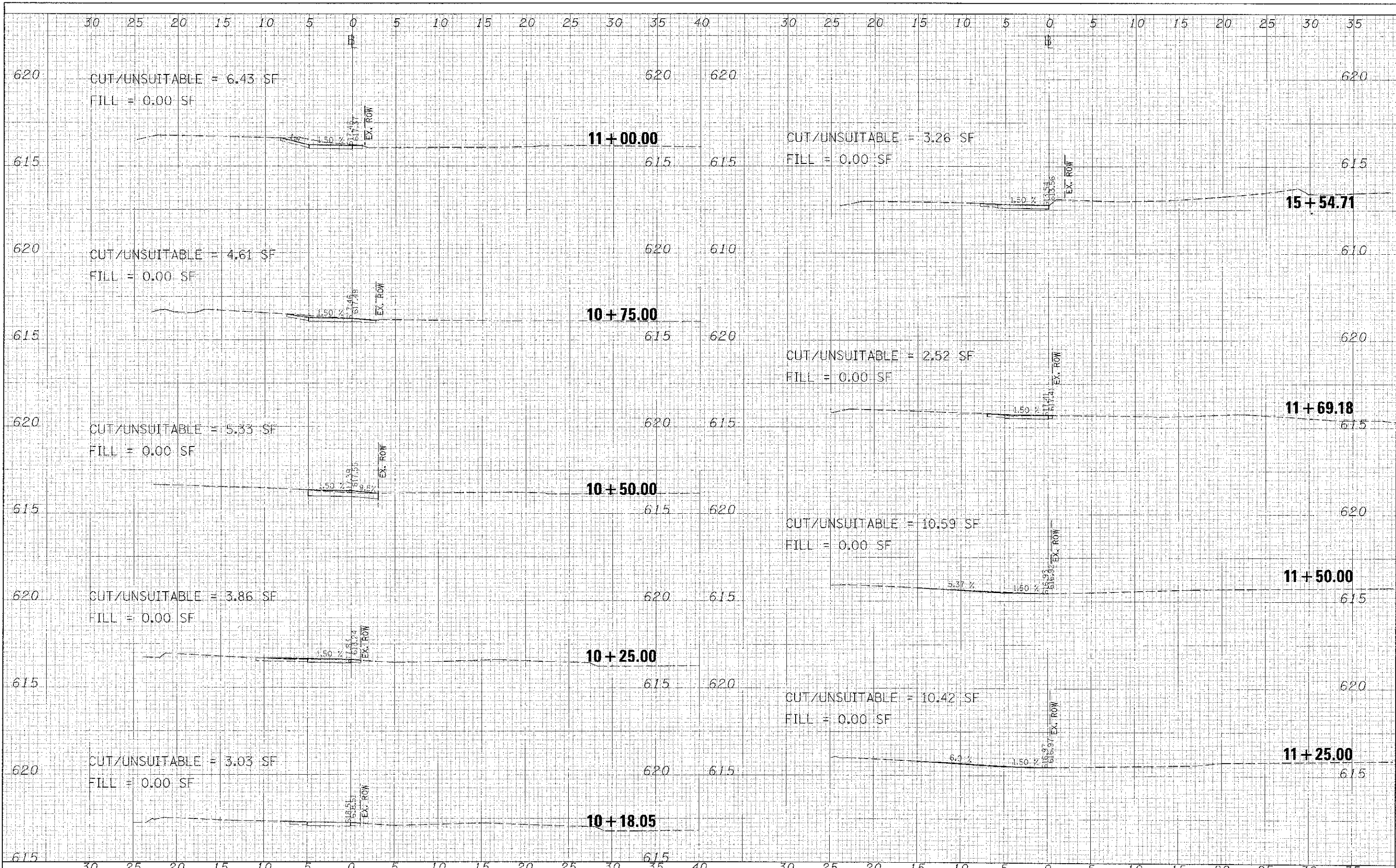
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (DIVE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACED 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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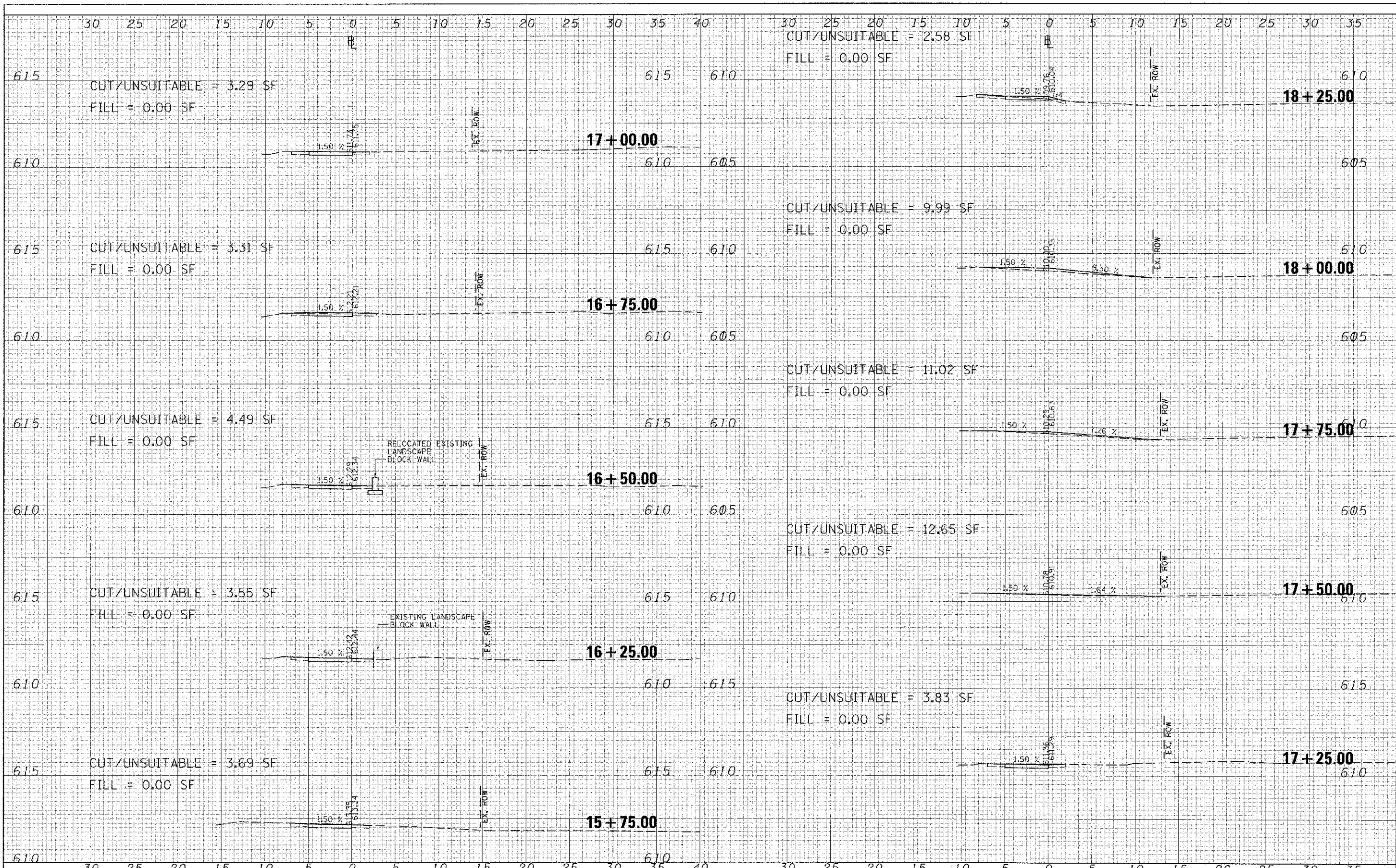
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FILE NAME DATE SCALE	USER NAME P. 01. SCALE P. 01. DATE	DESIGNED <i>KLM</i> DRAWN <i>KLM</i> CHECKED <i>DUL</i> DATE <i>JUNE 29, 2012</i>	REVISED REVISED REVISED REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK CROSS SECTIONS	F.A.U. RTE. 3578	SECTION 11-00049-00-SW	COUNTY COCK	TOTAL SHEETS 29	SHEET NO. 23	CONTRACT NO. 63732	ILLINOIS' FED. AID PROJECT
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PROJECT	DATE
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AREA	DATE
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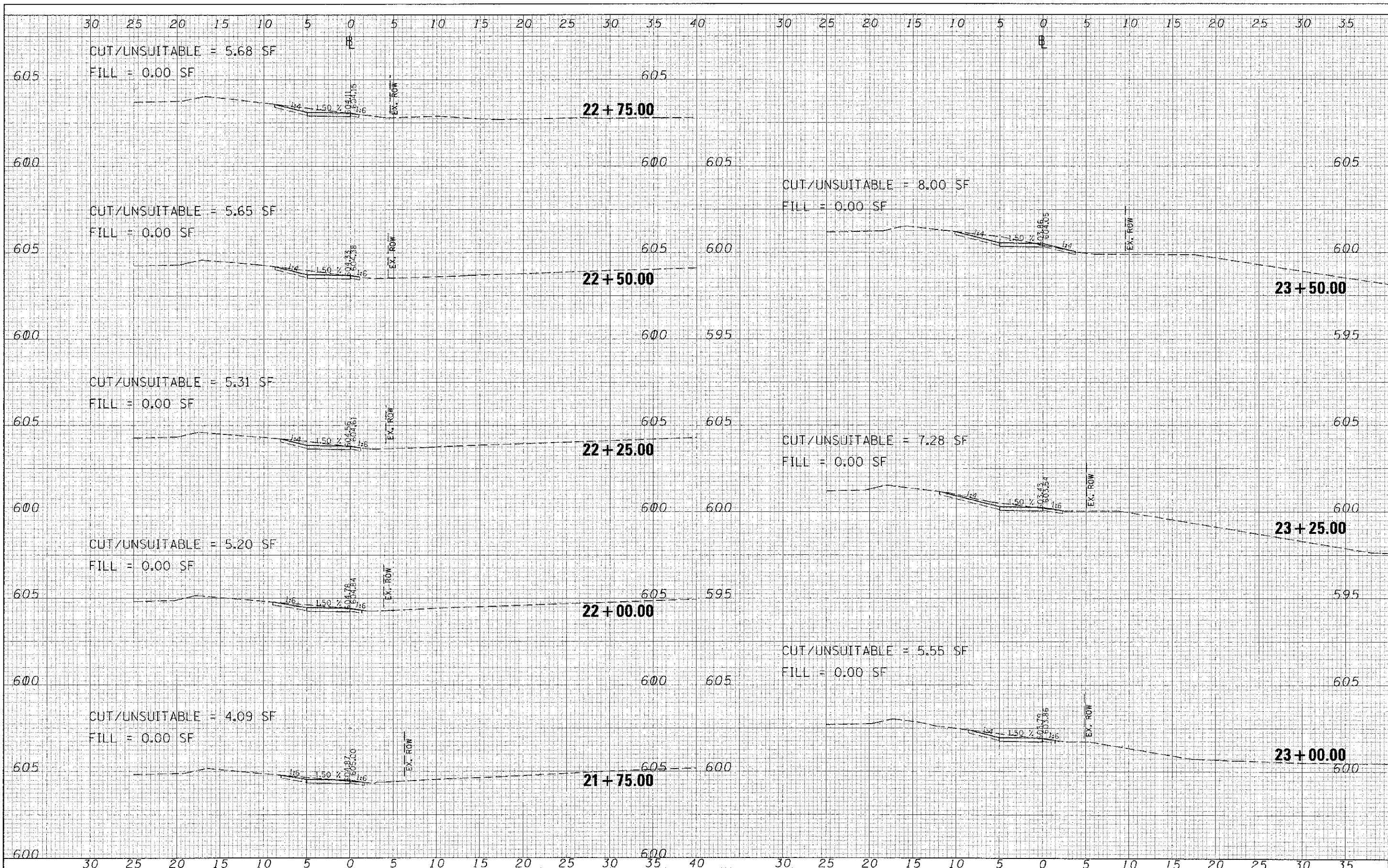
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NOTE BOOK	DATE
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FILE NAME	USER NAME	DESIGNED	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		SOUTHWEST HIGHWAY SIDEWALK CROSS SECTIONS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
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NO. SHEET	PLLOT SCALE	CHECKED	REVISED								24
	PLLOT DATE	DATE	REVISED								CONTRACT NO. 63732
		JUNE 29, 2012				SCALE: SHEET OF SHEETS STA. 15+75.00 TO STA. 18+25.00		ILLINOIS FED. AID PROJECT			

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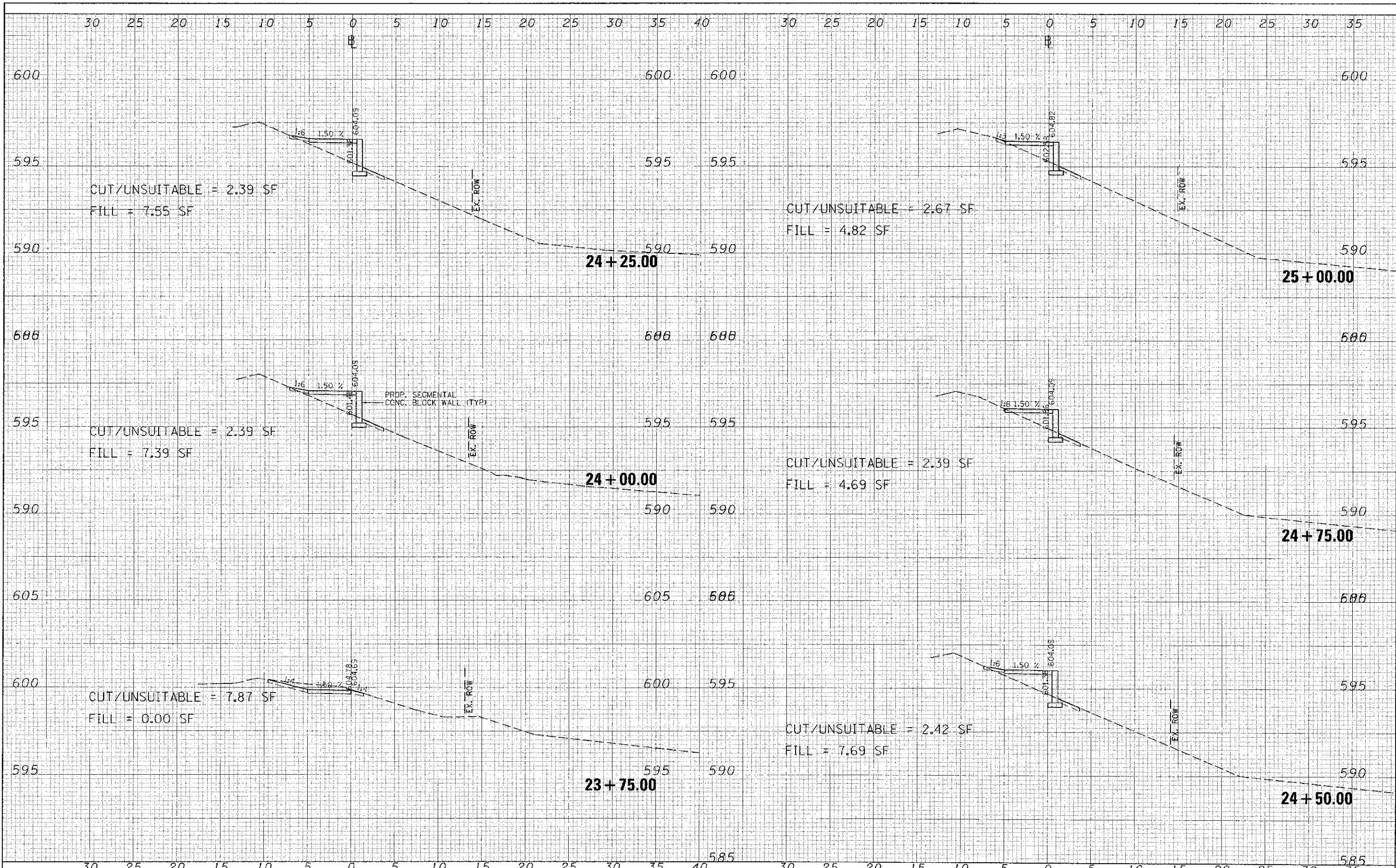
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FILE NAME	USER NAME	DESIGNED	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK CROSS SECTIONS	P.A.U. RTE. 3578 SECTION 11-00049-00-SW COUNTY COOK TOTAL SHEETS 29 SHEET NO. 26 CONTRACT NO. 63732
DATE	DRAWN	CHECKED	REVISED			
DATE	CHECKED	DATE	REVISED			
DATE	DATE	DATE	DATE			
DATE	DATE	DATE	DATE			
SCALE	SHEET OF SHEETS: STA. 21+75.00 TO STA. 23+50.00			ILLINOIS FED. AID PROJECT		

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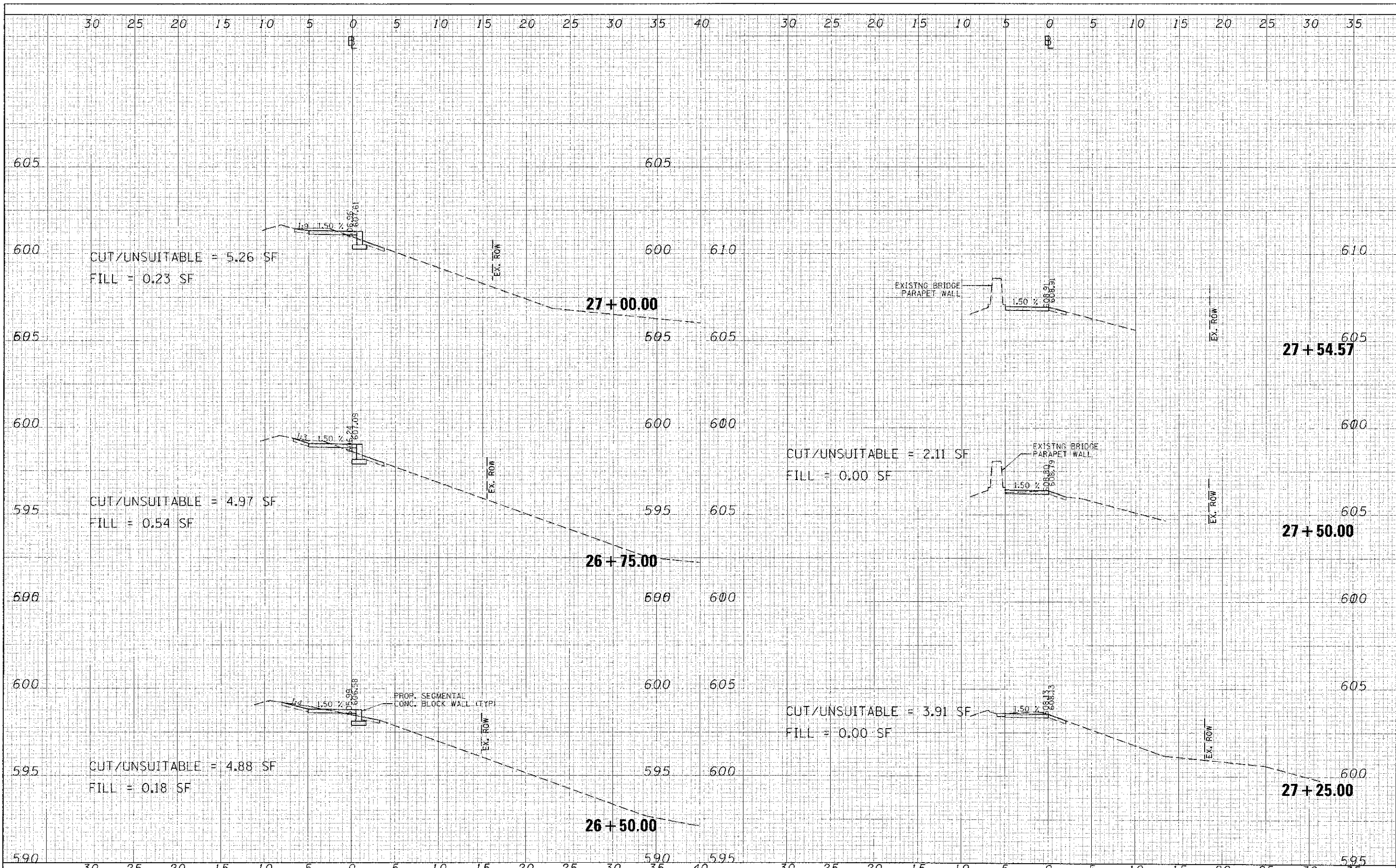
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FILE NAME =	USER NAME = #15296	DESIGNED <i>KLM</i>	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOUTHWEST HIGHWAY SIDEWALK CROSS SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILE #	PL01 SCALE = 1/8"=1'-0"	DRAWN <i>KLM</i>	REVISED -			3578	11-00049-00-SW	COOK	29	27	
KS. SHEET	PL01 DATE = 7/2/2012	CHECKED <i>DDL</i>	REVISED -			SCALE: SHEET OF SHEETS STA. 23+75.00 TO STA. 25+00.00		CONTRACT NO. 63732			
		DATE <i>JUNE 29, 2012</i>	REVISED -			ILLINOIS FED. AID PROJECT					

DATE	3/12/12
SCALE	1" = 10'
PROJECT	SW HIGHWAY
SECTION	29
SHEET	29
CONTRACT	63732
DESIGNED	KLM
DRAWN	KLM
CHECKED	DDJ
DATE	JUNE 29, 2012

DESIGNED	KLM
DRAWN	KLM
CHECKED	DDJ
DATE	JUNE 29, 2012



PROJECT NAME	USER NAME	DESIGNED	REVISIONS	STATE OF ILLINOIS	SOUTHWEST HIGHWAY SIDEWALK	P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	11-00049-00-SW	COOK	29	DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS	3578	11-00049-00-SW	COOK	29	29
AD. SUBJECT	PLT. SCALE	PLT. DATE	DATE	SCALE:	SHEET OF SHEETS	STA. 26+50.00 TO STA. 27+54.57	CONTRACT NO. 63732		ILLINOIS FED. AID PROJECT	