

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2006-031 BY	WILL	137	34
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

### SUGGESTED STAGES OF CONSTRUCTION

#### PRE-STAGE

1. INSTALL "CHANGEABLE MESSAGE BOARDS" PRIOR TO THE START OF CONSTRUCTION ACTIVITY ON FAI-55.
2. CONSTRUCT PROPOSED CULVERT BENEATH THE FRONTAGE ROAD NEAR THE C.S.X. RAILROAD USING STANDARD 701201.
3. BEGIN SUBSTRUCTURE WIDENING FOR THE I-55 BRIDGE OVER THE C.S.X. RAILROAD.
4. CONSTRUCT THE PERMANENT PROTECTIVE SHIELD AT IL RTE 59.
5. MILL, PATCH, AND RESURFACE THE OUTSIDE SHOULDERS OF THE SB LANES SOUTH OF THE I & M CANAL USING STANDARDS 701400 AND 701401 DURING ALLOWABLE PERIODS FOR LANE CLOSURES SPECIFIED IN THE SPECIAL PROVISIONS.

#### STAGE I

1. ESTABLISH TRAFFIC CONTROL AND INSTALL TEMPORARY CONCRETE BARRIER AND TEMPORARY PAVEMENT MARKINGS AS SHOWN ON STAGE I TYPICAL SECTIONS AND STAGING AND TRAFFIC CONTROL PLAN SHEETS. ALL LANE CLOSURES SHALL USE STANDARDS 701400 AND 701401. THESE LANE CLOSURES WILL ONLY BE ALLOWED DURING THE OFF PEAK HOURS AS DETAILED IN THE SPECIAL PROVISION FOR "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC".
2. I-55 BRIDGE OVER SUNNYLAND DRAIN - REMOVE EXISTING SLAB AND MEDIAN RETAINING WALLS, CONSTRUCT ABUTMENT WIDENING, AND CONSTRUCT BRIDGE SLAB FOR THE INSIDE PORTION OF THE BRIDGE AS SHOWN ON THE CONSTRUCTION STAGING SECTIONS SHEET 87.
3. I-55 SB BRIDGE OVER CSX RAILROAD - REMOVE EXISTING DECK, COMPLETE PROPOSED SUBSTRUCTURE WIDENING, ERECT BEAMS, AND CONSTRUCT DECK FOR THE INSIDE PORTION OF THE BRIDGE AS SHOWN ON THE CONSTRUCTION STAGING SECTIONS SHEET 65.
4. CONSTRUCT STABILIZED CONSTRUCTION ENTRANCES.
5. REMOVE SHOULDER AND GUARDRAIL FROM WITHIN THE MEDIAN.
6. CONSTRUCT ALL DRAINAGE ITEMS WITHIN STAGE I WORK AREA.
7. EXCAVATE AND CONSTRUCT AGGREGATE SUBGRADE, STABILIZED SUBBASE, PAVEMENT, SHOULDERS, AND GUARDRAIL WITHIN THE STAGE I WORKING AREA.

#### STAGE II

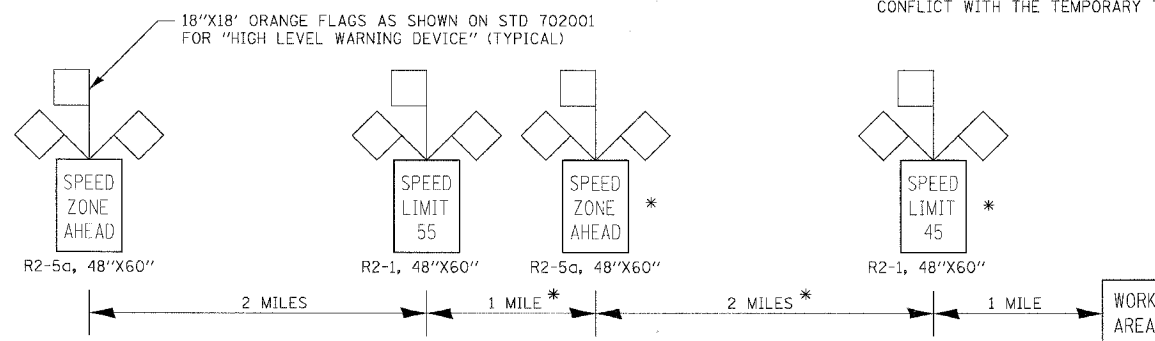
1. ESTABLISH TRAFFIC CONTROL AS SHOWN ON STAGE II TYPICAL SECTIONS AND SUGGESTED STAGING AND TRAFFIC CONTROL PLAN SHEETS. ALL LANE CLOSURES SHALL USE STANDARDS 701400 AND 701401. THESE LANE CLOSURES WILL ONLY BE ALLOWED DURING OFF PEAK HOURS AS DETAILED IN THE SPECIAL PROVISION FOR "KEEPING EXPRESSWAY OPEN TO TRAFFIC".
2. I-55 SB BRIDGE OVER CSX RAILROAD - REMOVE EXISTING DECK, COMPLETE PROPOSED SUBSTRUCTURE WIDENING, ERECT BEAM, AND CONSTRUCT DECK FOR THE OUTSIDE PORTION OF THE BRIDGE AS SHOWN ON THE CONSTRUCTION STAGING SECTIONS SHEET 65.
3. COMPLETE ALL GUARDRAIL WORK AND SHOULDER WIDENING ON OUTSIDE SHOULDERS.

#### STAGE III

1. USING FREEWAY STANDARD 701400, 701401 AND 701446 FOR ONE LANE AND TWO LANE CLOSURES, DURING THE OFF PEAK HOURS AS DETAILED IN THE SPECIAL PROVISIONS FOR "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC", CONSTRUCT PERMANENT PAVEMENT MARKINGS, AND REPLACE THE REFLECTIVE ELEMENTS IN THE RAISED REFLECTIVE PAVEMENT MARKERS.

### TRAFFIC CONTROL - GENERAL NOTES

1. ONCE THE OUTSIDE SHOULDER OF THE SB LANES SOUTH OF THE I & M CANAL ARE MILLED, THE CONTRACTOR SHALL PATCH THE SHOULDER AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FILL ALL PATCH HOLES BEFORE OPENING THE ADJACENT LANE CLOSURE TO TRAFFIC. THIS WORK SHALL BE PAID FOR AS CLASS D PATCHES OF THE TYPE REQUIRED.
2. AT THE END OF EACH NIGHTLY LANE CLOSURE, THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN AND THEN OPEN THE ADJACENT LANE TO TRAFFIC BY THE TIME SPECIFIED IN THE SPECIAL PROVISION "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC." SHOULDERS WHICH HAVE BEEN MILLED SHALL REMAIN CLOSED USING STANDARD 701101 FROM THE START OF SHOULDER MILLING OPERATIONS UNTIL THE SHOULDER RESURFACING IS COMPLETED, OR AS DIRECTED BY THE ENGINEER.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING AND TRAFFIC CONTROL OPERATIONS WITH ADJACENT CONTRACTS. THE COST OF ANY ADDITIONAL TRAFFIC CONTROL AND/OR TEMPORARY CONSTRUCTION ITEMS REQUIRED FOR SUCH COORDINATION WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)."
4. PRIOR TO REMOVING TRAFFIC CONTROL FOR LANE CLOSURES, THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN.
5. THE CONTRACTOR SHALL INSTALL TEMPORARY W21-1108 SIGNS ON BOTH SIDES OF THE PAVEMENT IN ADVANCE OF AREAS WITH A GRADE DIFFERENTIAL BETWEEN LANES AND AFTER EACH ENTRANCE RAMP. SIGN LOCATIONS SHALL BE APPROVED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)." THESE SIGNS SHALL BE 48" X 48".
6. THE REGULATORY SPEED LIMIT SHALL BE REDUCED TO 55 MPH DURING STAGE I ADJACENT TO AND BEFORE THE WORK AREAS WHERE TEMPORARY CONCRETE BARRIER IS INSTALLED AND TO 45 MPH DURING STAGE I AND II ADJACENT TO WORK AREAS WHERE TEMPORARY CONCRETE BARRIER IS NOT INSTALLED AS FOLLOWS:
  - A. IN CONJUNCTION WITH IMPLEMENTING TRAFFIC CONTROL AS SHOWN ON THE PLANS FOR STAGES I AND II, THE CONTRACTOR SHALL:
    - SIMULTANEOUSLY COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS AND ERECT 55 OR 45 MPH REGULATORY SPEED LIMIT SIGNS (R2-1, 48"X60") ADJACENT TO ALL EXISTING SPEED LIMIT SIGNS WITHIN THE WORK AREA LIMITS AS DIRECTED BY THE ENGINEER.
    - SIMULTANEOUSLY ERECT SIGNS AS SHOWN IN THE "SPEED LIMIT REDUCTION SIGNING" DIAGRAM ON BOTH SIDES OF THE TRAVELED WAY AND COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS WITHIN THE LIMITS OF THE "SPEED LIMIT REDUCTION SIGNING."
  - B. THE EXISTING SPEED LIMIT SIGNING OUTSIDE THE WORK AREA LIMITS AND BEYOND THE "SPEED LIMIT REDUCTION SIGNING" SHALL BE MAINTAINED, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
  - C. THE CONTRACTOR SHALL UNCOVER THE EXISTING SPEED LIMIT SIGNS AND REMOVE THE 55 OR 45 MPH REGULATORY SPEED LIMIT SIGNS SIMULTANEOUS WITH THE REMOVAL OF TRAFFIC CONTROL DEVICES IN ANY SEGMENT OF THE ROADWAY AS DIRECTED BY THE ENGINEER.
  - D. THE COST OF THIS WORK (NOTE 6) SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)."
7. DURING STAGE I, THE CONTRACTOR SHALL ERECT REGULATORY HIGHWAY SIGNS R4-5 (48" X 60") "TRUCKS USE LEFT LANE" ON BOTH SIDES OF THE TRAVEL LANES AT 1/2 MILE AND 1 MILE IN ADVANCE OF THE WORK AREA, AT 1/2 MILE INTERVALS ADJACENT TO THE WORK AREA, AND AT EACH ENTRANCE RAMP OF THE PROJECT AS DIRECTED BY THE ENGINEER. THE SIGNS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "TEMPORARY INFORMATION SIGNING".
8. SINCE THIS IS AN EXPRESSWAY FACILITY, THE CONTRACTOR MAY HAVE CREWS WORKING IN THE NORTHBOUND AND SOUTHBOUND DIRECTIONS AT THE SAME TIME.
9. SEE GENERAL NOTES ON SHEET 2 FOR TREATMENT OF PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES.



\* THE SIGNS AND SEGMENTS MARKED WITH AN ASTERISK ARE NOT REQUIRED WHEN THE SPEED LIMIT IS ONLY REDUCED TO 55 M.P.H.

#### SPEED LIMIT REDUCTION SIGNING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (OVER CSX RR AND SUNNYLAND DRAIN)  
BRIDGE WIDENING

### STAGING AND TRAFFIC CONTROL SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL GENERAL NOTES

SCALE:      DRAWN BY DP  
DATE 07/07/06      CHECKED BY DDH

**TENG**      TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 08/07/06  
 PLOT SCALE = AS CALLED  
 USER NAME = BUSERR  
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