

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2006-032 BY	WILL	505	103
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
	MOT-TYP-02 OF 05			

TRAFFIC CONTROL GENERAL NOTES

1. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING AND TRAFFIC CONTROL OPERATIONS WITH ADJACENT CONTRACTS. THE COST OF ANY ADDITIONAL TRAFFIC CONTROL AND/OR TEMPORARY CONSTRUCTION ITEMS REQUIRED FOR SUCH COORDINATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)"

2. PRIOR TO REMOVING TRAFFIC CONTROL FOR LANE CLOSURES THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN.

3. THE CONTRACTOR SHALL INSTALL TEMPORARY W21-1108 SIGNS ON BOTH SIDES OF THE ROADWAY IN ADVANCE OF THE AREAS WITH A GRADE DIFFERENTIAL BETWEEN LANES AND AFTER EACH ENTRANCE RAMP. SIGN LOCATIONS SHALL BE APPROVED BY THE ENGINEER. THE COST OF THE SIGNS WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)". THESE SIGNS SHALL BE 48X48 INCHES.

4. THE REGULATORY SPEED LIMIT SHALL BE REDUCED TO 55 MPH DURING STAGE I ADJACENT TO AND BEFORE THE WORK AREAS WHERE TEMPORARY CONCRETE BARRIER IS INSTALLED AND TO 45 MPH DURING STAGE I AND II ADJACENT TO WORK AREAS WHERE TEMPORARY CONCRETE BARRIER IS NOT INSTALLED AS FOLLOWS:

A. IN CONJUNCTION WITH IMPLEMENTING TRAFFIC CONTROL AS SHOWN ON THE PLANS FOR STAGES I AND II, THE CONTRACTOR SHALL:

- SIMULTANEOUSLY COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS AND ERECT 55 OR 45 MPH REGULATORY SPEED LIMIT SIGNS (R2-1, 48"X60") ADJACENT TO ALL EXISTING SPEED LIMIT SIGNS WITHIN THE WORK AREA LIMITS AS DIRECTED BY THE ENGINEER.

- SIMULTANEOUSLY ERECT SIGNS AS SHOWN IN THE "SPEED LIMIT REDUCTION SIGNING" DIAGRAM ON BOTH SIDES OF THE TRAVELED WAY AND COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS WITHIN THE LIMITS OF THE "SPEED LIMIT REDUCTION SIGNING."

B. THE EXISTING SPEED LIMIT SIGNING OUTSIDE THE WORK AREA LIMITS AND BEYOND THE "SPEED LIMIT REDUCTION SIGNING" SHALL BE MAINTAINED, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

C. THE CONTRACTOR SHALL UNCOVER THE EXISTING SPEED LIMIT SIGNS AND REMOVE THE 55 OR 45 MPH REGULATORY SPEED LIMIT SIGNS SIMULTANEOUS WITH THE REMOVAL OF TRAFFIC CONTROL DEVICES IN ANY SEGMENT OF THE ROADWAY AS DIRECTED BY THE ENGINEER.

D. THE COST OF THIS WORK (NOTE 5) SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)."

5. THE PROJECT WORK AREA CONSISTS OF ROADWAY AND BRIDGE WORK WITHIN THE FOLLOWING THREE MAIN CONSTRUCTION SEGMENTS:

- U.S. ROUTE 30, E.J. & E. RAILROAD, AND MINK CREEK
- MATERIAL SERVICE RAILROAD (ABANDONED)
- LILY CACHE SLOUGH

WORK ZONE TRAFFIC CONTROL DEVICES AND SIGNING SHALL BE PLACED WITHIN AND BETWEEN THESE CONSTRUCTION SEGMENTS AS SHOWN ON THE MOT PLANS OR AS DIRECTED BY THE ENGINEER.

6. DURING STAGE I THE CONTRACTOR SHALL ERECT REGULATORY HIGHWAY SIGNS R4-5 (48"X60") "TRUCKS USE LEFT LANE" ON BOTH SIDES OF THE TRAVEL LANES AT 1/2 MILE AND 1 MILE IN ADVANCE OF THE WORK ZONE, AT 1/2 MILE INTERVALS ADJACENT TO THE WORK ZONE, AND AT EACH ENTRANCE RAMP OF THE PROJECT AS DIRECTED BY THE ENGINEER. THE SIGNS SHALL ONLY BE PLACED ADJACENT TO THE ROADWAY, WHICH IS LOCATED WITHIN THE WORK ZONE AREAS. THE SIGNS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "TEMPORARY INFORMATION SIGNING".

7. DRUMS SHALL NOT BE PLACED IN THE LANE WHICH IS OPEN TO TRAFFIC. DURING MILLING AND PAVING OPERATIONS, THE CONTRACTOR SHALL USE ONE FOOT VERTICAL BARRICADES WITH STEADY BURN LIGHTS ALONG THE MILLING AND PAVING OPERATIONS. THE TYPE OF VERTICAL BARRICADES SHALL BE APPROVED IN WRITING BY THE ENGINEER PRIOR TO BEING USED ON THE PROJECT. DRUMS OR TYPE II BARRICADES SHALL BE USED TO DELINEATE ALL OPEN RAMPS THROUGHOUT THE WORK ZONE.

8. SINCE THIS IS AN EXPRESSWAY FACILITY, THE CONTRACTOR MAY HAVE CREWS WORKING IN BOTH DIRECTIONS AT THE SAME TIME.

9. SEE GENERAL NOTES FOR TREATMENT OF RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES.

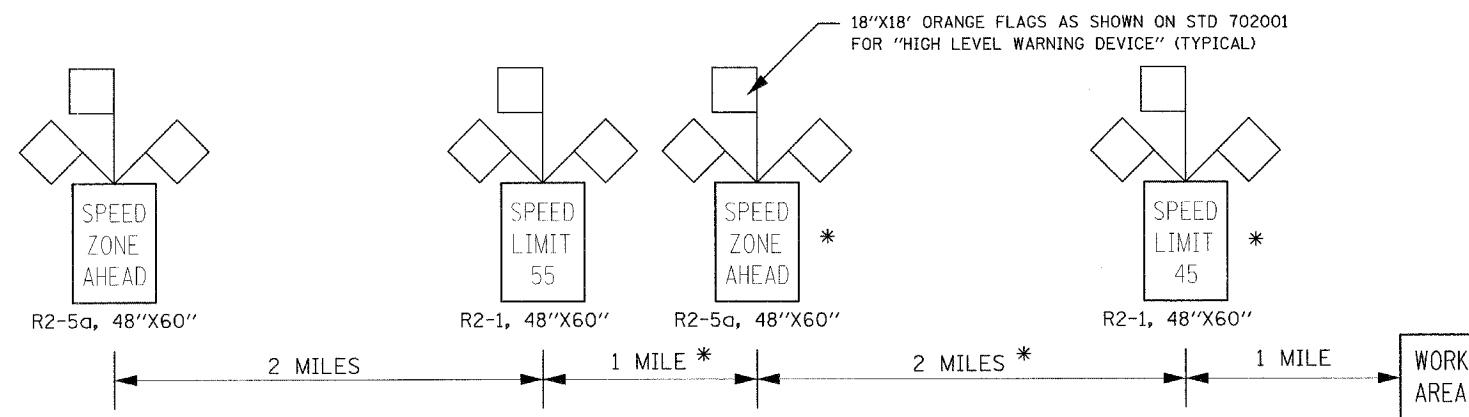
LIST OF TRAFFIC CONTROL STATE AND DISTRICT 1 STANDARDS

STATE

- 701101 OFF-ROAD OPERATIONS, MULTILANE, 4.5 m (15') TO 600 mm (24") FROM EDGE OF PAVEMENT
- 701106 OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 4.5 m (15') AWAY
- 701201 APPROACH TO LANE CLOSURE FREEWAY/EXPRESSWAY
- 701400 LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEED > OR = 45 MPH
- 701401 LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701402 LANE CLOSURE, FREEWAY/EXPRESSWAY WITH BARRIER
- 701411 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > OR = 45 MPH
- 701446 TWO LANE CLOSURE FREEWAY/EXPRESSWAY
- 701601 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN.
- 702001 TRAFFIC CONTROL DEVICES
- 704001 TEMPORARY CONCRETE BARRIER

DISTRICT 1

- TC09 TRAFFIC CONTROL DETAILS FRO FREEWAY SINGLE AND MULTI-LANE WEAVE
- TC11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- TC12 MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
- TC13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC17 TRAFFIC CONTROL DETAILS FOR SHOULDER CLOSURES PARTIAL RAMP CLOSURES
- TC18 SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS



SPEED LIMIT REDUCTION SIGNING

* THE SIGNS AND SEGMENTS MARKED WITH AN ASTERISK ARE NOT REQUIRED WHEN THE SPEED LIMIT IS ONLY REDUCED TO 55 M.P.H.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI ROUTE 55
 US 30 (PLAINFIELD ROAD) TO LILY CACHE SLOUGH
 SUGGESTED STAGES OF
 CONSTRUCTION AND TRAFFIC
 CONTROL GENERAL NOTES

SCALE: NONE
 DATE: 07-21-06

DRAWN BY: DM
 CHECKED BY: DVS

