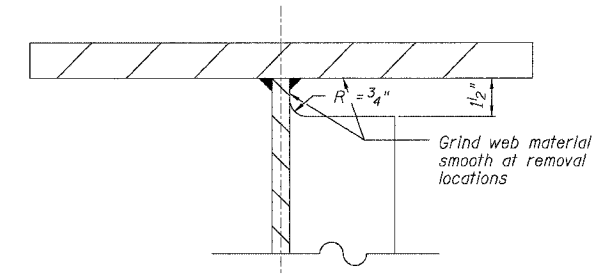


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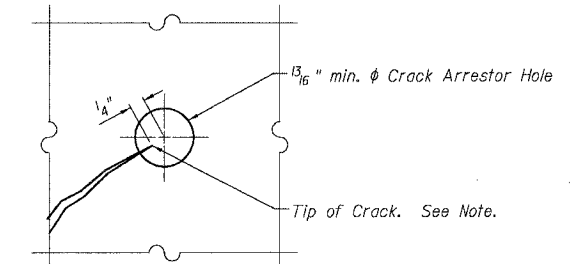
STRUCTURAL STEEL REPAIR GENERAL NOTES

- Existing structural steel shall be painted in accordance with Guide Bridge Special Provisions for Cleaning and Painting Contact Surface Areas of Existing Steel Structures.
- New structural steel shall conform to AASHTO M-270 Gr. 36, and be shop painted with inorganic zinc rich primer per AASHTO M300 Type I. Spot paint with zinc silicate primer applied on heads of field welds and all areas where paint has been removed or damaged shall be required. Cost included in Structural Steel Repair.
- All new fasteners shall be $\frac{3}{4}$ " ϕ H.S. bolts unless otherwise noted. Minimum edge distance for bolts shall be $1\frac{1}{2}$ ".
- Grinding shall be done parallel to the beam direction. Cost of grinding and field drill shall be included with Structural Steel Repair.
- All structural steel repairs shall be coordinated with deck MOT to assure live load is not directly transmitted through members being repaired.
- Costs of furnishing, erecting and removal of required Traffic Control and Protection Standards for steel repairs shall be included in the cost of "Structural Steel Repair" unless otherwise noted.
- Cost of removal and disposal of existing steel plates shall be included in the cost of "Structural Steel Repair".



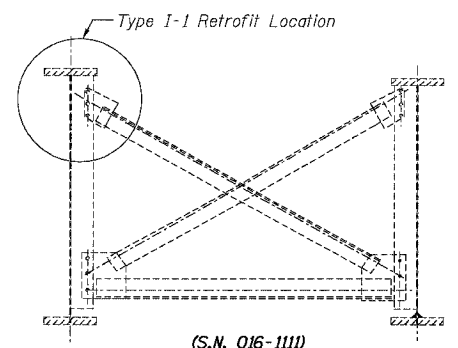
CUT WEB STIFFENER DETAIL

- Note: 1. Field cut web stiffener using saw, drill and / or grinding. Flame cutting will not be permitted within 2" of girder web or flange. Grind surfaces after cutting to a surface roughness of (Ra) 250u in. or less.
2. Contractor shall fabricate retrofit hardware providing holes subpunched for attachment to girder flange. Holes shall be reamed full size for attachment to the flange and drilled for attachment in the field.

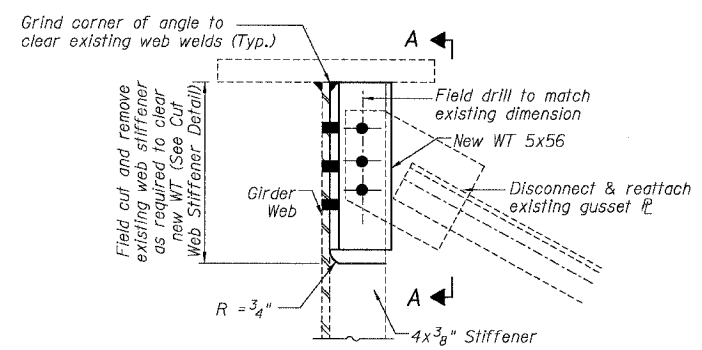


CRACK ARRESTOR HOLE DETAIL

- Note: Locate crack tip using liquid dye penetrant or magnetic particle testing. Drill $1\frac{3}{16}$ " min. ϕ Crack Arrestor hole at the crack tip. After crack arrestor hole has been drilled, dye penetrant or magnetic particle testing shall be used to verify that the drilled hole has captured the crack tip. Cost shall be included in the cost of "Structural Steel Repair".

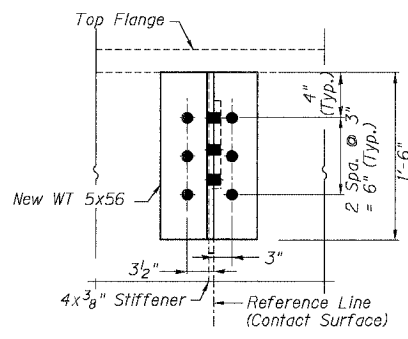


(S.N. 016-1111)

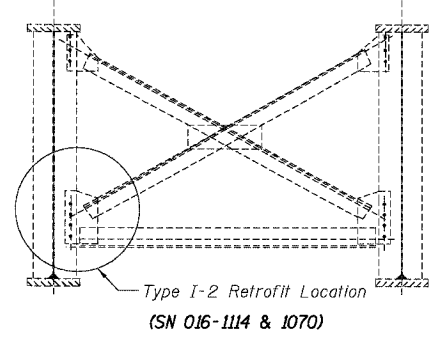


PARTIAL ELEVATION

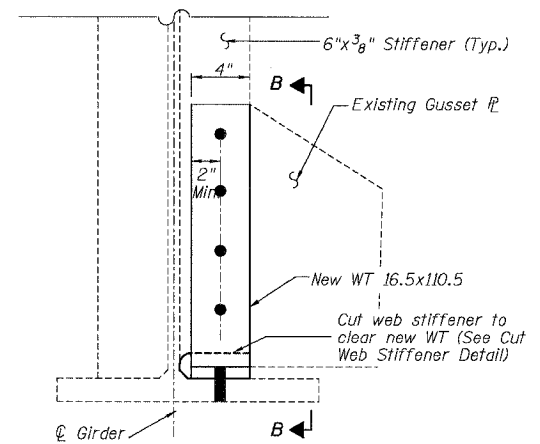
TYPE I-1



SECTION A-A

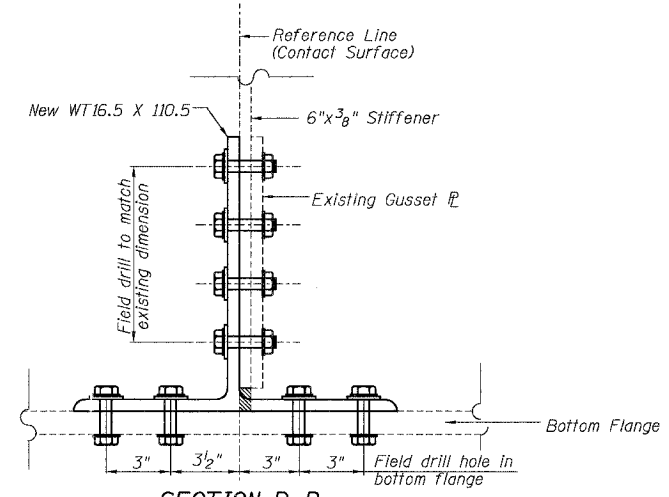


(SN 016-1114 & 1070)

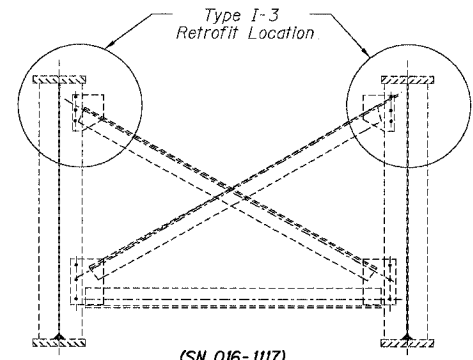


PARTIAL ELEVATION

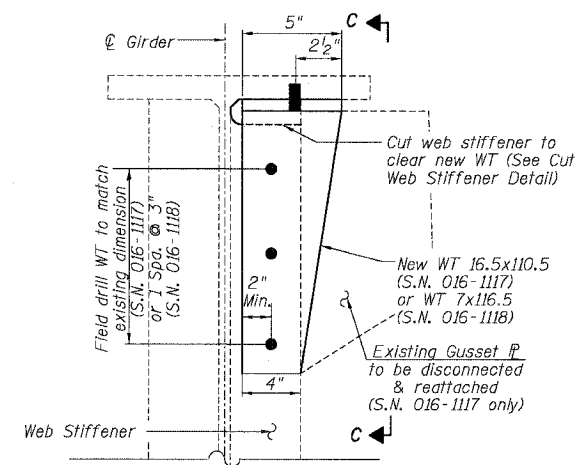
TYPE I-2



SECTION B-B

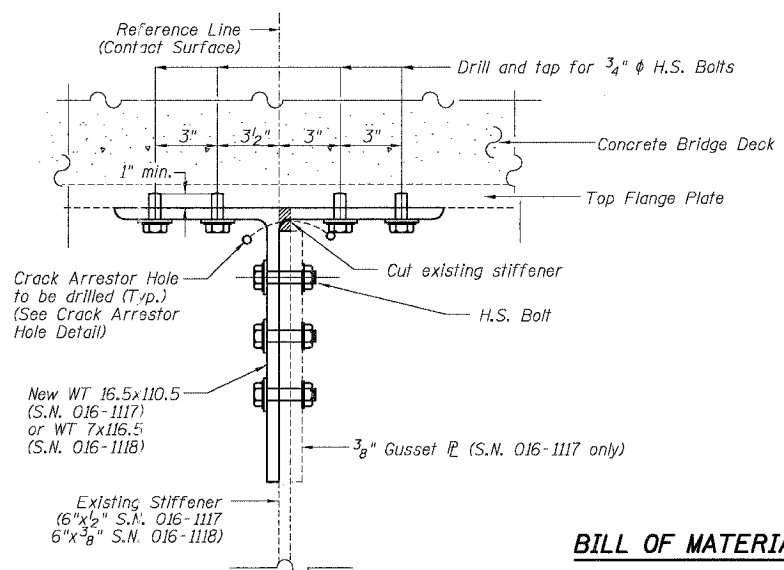


(SN 016-1117)



PARTIAL ELEVATION

TYPE I-3

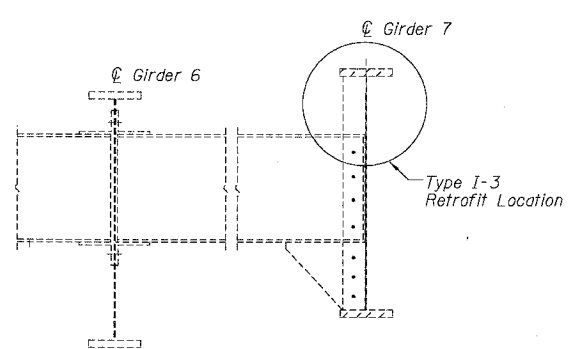


SECTION C-C

BILL OF MATERIAL

Item	Unit	Quantity
Structural Steel Repair	Pound	1,085

EXISTING TYPICAL INTERIOR CROSS FRAME



EXISTING TYPICAL INTERIOR DIAPHRAGM
(SN 016-1118)

Notes: This repair work at S.N. 016-1117 shall require temporary lane closure of West 26th Street in accordance with Highway Standard 701501.

DETAIL A FATIGUE RETROFIT TYPE I REPAIR

BOWMAN, BARRETT & ASSOCIATES INC
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbainc.com

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
NB DAN RYAN ELEVATED BRIDGE
REPAIR FROM 15TH TO 28TH STREETS
STRUCTURAL STEEL REPAIR
DETAIL A

SCALE: NTS
DATE: 7/22/2005

DRAWN BY: BDC
CHECKED BY: BLU

7/21/2005 3:46:02 PM