09-23-11 LETTING ITEM 006

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	Mc HENRY	34	1
	ILLINOIS	CONTRACT NO. 60M53		

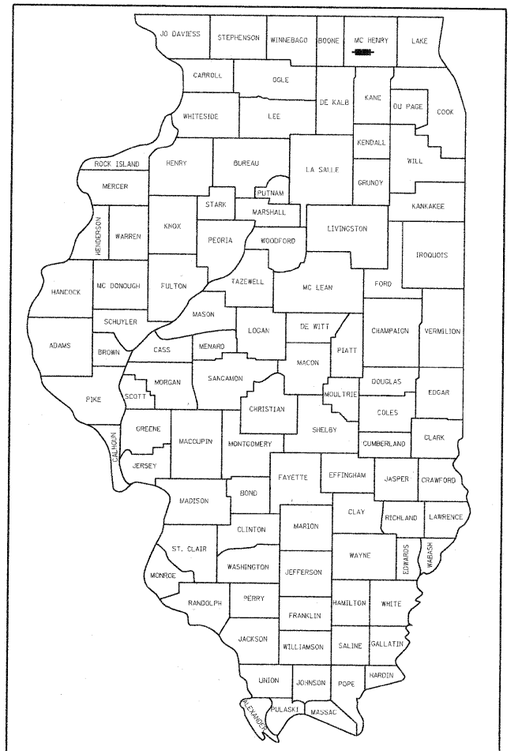
D-91-125-11

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAP ROUTE 525 – US ROUTE 20 (GRANT HWY.)
OVER DRAINAGE DITCH
SECTION 11 – T – 1
PROJECT: F-0525(117)
CULVERT REPLACEMENT
Mc HENRY COUNTY
C–91–125–11

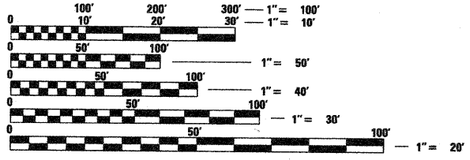


LOCATION OF SECTION INDICATED THUS: –■–

TRAFFIC DATA
****

EXISTING ADT = 7,900 (2009)
POSTED SPEED = 55 MPH

PROJECT IS LOCATED IN CORAL TOWNSHIP



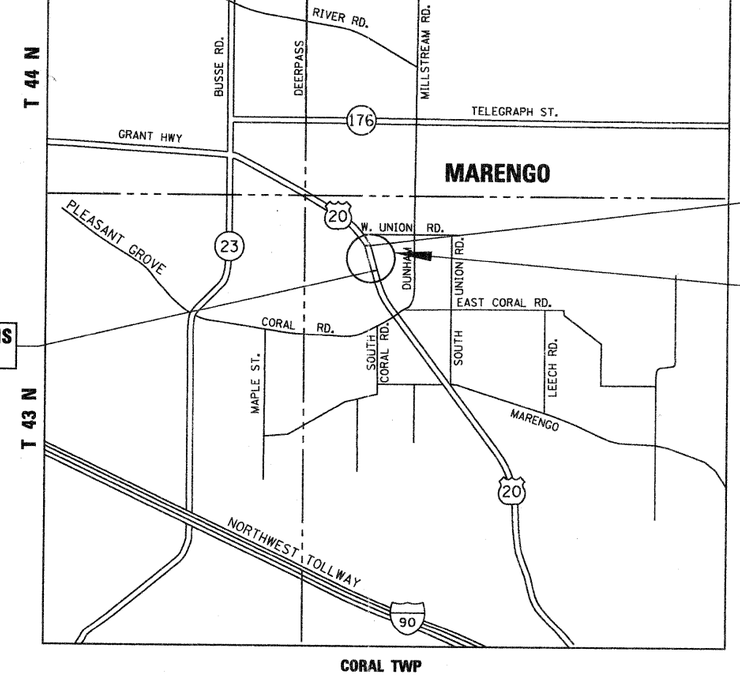
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

PROJECT ENGINEER ROBERT BORO
PROJECT MANAGER ISSAM RAYYAN

CONTRACT NO. 60M53

ENTRAN
5440 N. CUMBERLAND AVE, SUITE 111
CHICAGO, IL 60656



PROJECT BEGINS STA. 111+19.70

PROJECT ENDS STA. 117+30.40

PROJECT LOCATION
US ROUTE 20 (GRANT HWY.)
OVER DRAINAGE DITCH
EXISTING SN. 056–0088
PROPOSED SN. 056–0276

CORAL TWP
LOCATION MAP
NOT TO SCALE
GROSS LENGTH = 610.70 FT. = 0.116 MILE
NET LENGTH = 610.70 FT. = 0.116 MILE

LICENSED PROFESSIONAL ENGINEER
THOMAS M. HEIN
062-053199
STATE OF ILLINOIS

THOMAS M. HEIN, P. E.
IL. LIC. NO. 062-053199
EXP 11-30-2011
DATE 06-29-2011

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED July 6, 20 11
Dawn M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
August 19 20 11
acting Scott E. Stitt, P.E. /a
ENGINEER OF DESIGN AND ENVIRONMENT
August 19 20 11
Christine M. Reed /a
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

COMMITMENTS

NONE

GENERAL NOTES - MISCELLANEOUS

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

TEN FOOT (10-FT) TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE RESPECTIVE UTILITIES, THE CITY OF MARENGO, AND MCHENRY COUNTY.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKINGS OUTSIDE THE REMOVAL LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONSTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFELCTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT ON ANY TEMPORARY TRAFFIC CONTROL DEVICES.

THE RESIDENT ENGINEER SHALL CONTACT MS. DEBBIE HANLON, AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

WET REFLECTIVE TEMPORARY TAPE, TYPE 3 SHALL BE USED FOR TEMPORARY PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE TEMPORARY PAVEMENT MARKING TAPE REMOVAL SHALL BE INCLUDED IN THE COST OF WORK ZONE PAVEMENT MARKING REMOVAL.

SAW CUTS WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS. THE ENGINEER, OR AN AUTHORIZED SURVEYOR OR AGENT WILL WITNESS OR OTHERWISE REFERENCE AND RESET MONUMENTS AS NECESSARY. ALL PROPERTY CORNERS EXCEPT THOSE WITHIN AREAS WHERE THE SCHEDULE SHOWS PLACEMENT OF R.O.W. MARKERS SHALL REMAIN UNDISTURBED.

THE CONTRACTOR AS REQUIRED, SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO COMMENCING WITH CONSTRUCTION.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH STATE OR LOCAL REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION.

THE CONTRACTOR'S OPERATIONS AND TEMPORARY STORAGE ACTIVITIES SHALL BE LIMITED TO THE WORK AREA AND/OR CONSTRUCTION LIMITS. ANY ADDITIONAL STAGING AREAS ADJACENT TO THE PROJECT ARE SUBJECT TO PRIOR APPROVAL BY THE ENGINEER. NO ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR COMPLIANCE WITH THE ABOVE REQUIREMENTS.

THE CONTRACTOR'S PERSONNEL SHALL NOT BE ALLOWED TO PARK PERSONAL VEHICLES IN THE WORK AREA AND/OR CONSTRUCTION LIMITS.

THE REMOVAL OF EXISTING ENTRANCE CULVERTS SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ACCESS SHALL BE PROVIDED AT ALL TIMES TO PROPERTIES ABUTTING THE PROPOSED IMPROVEMENT.

ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR FLUORESCENT ORANGE, FLUORESCENT YELLOW/GREEN OR A COMBINATION OF FLUORESCENT ORANGE AND FLUORESCENT YELLOW/GREEN VESTS AND HARD HATS AT ALL TIMES WHILE ON THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GENERAL NOTES - ROADWAY

THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE.

ANY MAIL BOXES REQUIRING RELOCATION TO CONSTRUCT PORTIONS OF THE CONTRACT SHALL BE MOVED BY THE CONTRACTOR. THIS WORK SHALL BE DONE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND BITUMINOUS TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE INSTALLATION OF GUARDRAIL OVER THE PROPOSED CULVERT SHALL BE MOUNTED IN ACCORDANCE WITH STANDARD 630101. THE COST OF THE INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE UNIT PRICE OF STEEL PLATE BEAM GUARDRAIL, TYPE A.

THE REMOVAL OF SINGLE RAIL, DOUBLE RAIL, RUB RAIL, AND TERMINAL SECTIONS (TAPERS AND END SECTIONS) SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE COST PER LINEAL FOOT FOR STEEL PLATE BEAM GUARD RAIL REMOVAL.

THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.

GENERAL NOTES - DRAINAGE

THE COST OF MAKING ANY CONNECTIONS TO EXISTING DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.

LENGTHS AND SIZES OF STORM SEWERS AS SHOWN ON THE PLANS AND DRAINAGE STRUCTURE ELEVATIONS SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO ORDERING AND INSTALLATION OF DRAINAGE ITEMS. THE INVERTS OF THE PROPOSED DRAINAGE STRUCTURES MAY REQUIRE REVISIONS TO MEET EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL MAINTAIN THE SURFACE DRAINAGE OF THE ROAD DURING CONSTRUCTION OF THIS PROJECT. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

STORM SEWER SHALL BE BACKFILLED IN ACCORDANCE WITH ARTICLE 550.07 METHOD 1 ONLY.

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS, CROSSROAD PIPES, OR DRAINAGE STRUCTURES DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GENERAL NOTES - UTILITIES

THE CONTRACTOR SHALL PROTECT EXISTING AND NEW UTILITIES. WHEN REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL BRACE AND SUPPORT THE UTILITIES PROPERLY IN ORDER TO PREVENT SETTLEMENT, DISPLACEMENT, OR DAMAGE TO THE UTILITIES. THE PROTECTION OF THE UTILITIES AS SPECIFIED HEREIN WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED. IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

GENERAL NOTES - LANDSCAPING

FERTILIZER NUTRIENTS SHALL BE APPLIED AT THE FOLLOWING RATES PER ACRE:

NITROGEN FERTILIZER NUTRIENT - 90 LBS./ACRE
PHOSPHORUS FERTILIZER NUTRIENT - 90 LBS./ACRE
POTASSIUM FERTILIZER NUTRIENT - 90 LBS./ACRE

GENERAL NOTES - EROSION AND SEDIMENT CONTROL

TEMPORARY EROSION CONTROL SEEDING SHALL BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS. REFER TO SPECIAL PROVISIONS.

ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH A TIME AS DIRECTED BY THE ENGINEER.

AT ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND COVER SHALL REMAIN.

HIGHWAY STANDARDS

280001-05 TEMPORARY EROSION CONTROL SYSTEMS

406201-01 MAILBOX TURNOUT

482001-02 HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT

482011-03 HMA SHLD. STRIPS / SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS

542301-03 PRECAST REINFORCED CONCRETE FLARED END SECTION

630001-09 STEEL PLATE BEAM GUARDRAIL

630101-09 GUARDRAIL MOUNTED ON EXISTING CULVERTS

630301-05 SHOULDER WIDENING FOR TYPE 1(SPECIAL) GUARDRAIL TERMINALS

701006-03 OFF-ROAD OPERATIONS, 2L, 2W, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE

701201-04 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH

701306-03 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH

701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY

701321-11 LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER

701326-04 LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS ≥ 45 MPH

701901-01 TRAFFIC CONTROL DEVICES

704001-06 TEMPORARY CONCRETE BARRIER

862001-01 UNINTERRUPTABLE POWER SUPPLY (UPS)

880001-01 SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALL

FILE NAME = g:\projects\2102155.002\cadd\civil\sht\D1660M53-02ant-gennotes.dgn

ENTRAN

USER NAME = 2kujawc	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 06/22/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
INDEX OF SHEETS, GENERAL NOTES, COMMITMENTS, AND STANDARDS

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	2
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

80% FED./20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE ROADWAY 0004	CONSTRUCTION TYPE CODE BRIDGE 0040
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	272	272	0
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	20	20	0
20200100	EARTH EXCAVATION	CU YD	438	438	0
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	379	303	76
20300100	CHANNEL EXCAVATION	CU YD	6	6	0
20400800	FURNISHED EXCAVATION	CU YD	708	708	0
20700220	POROUS GRANULAR EMBANKMENT	CU YD	76	0	76
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	317	317	0
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,682	1,682	0
25000210	SEEDING, CLASS 2A	ACRE	0.32	0.32	0.00
25000312	SEEDING, CLASS 4A	ACRE	0.03	0.03	0.00
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	31	31	0
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	31	31	0
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	31	31	0
25100630	EROSION CONTROL BLANKET	SQ YD	1,682	1,682	0
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	26	26	0
28000305	TEMPORARY DITCH CHECKS	FOOT	140	140	0
28000400	PERIMETER EROSION BARRIER	FOOT	1,051	1,051	0
28100107	STONE RIPRAP, CLASS A4	SQ YD	74	0	74
28200200	FILTER FABRIC	SQ YD	74	74	0
31101000	SUBBASE GRANULAR MATERIAL, TYPE B	TON	270	270	0
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	162	162	0
35501329	HOT-MIX ASPHALT BASE COURSE, 11 1/4"	SQ YD	190	190	0
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	261	261	0
40600300	AGGREGATE (PRIME COAT)	TON	6.5	6.5	0.0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	24	24	0
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	18	18	0
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	182	182	0
44000100	PAVEMENT REMOVAL	SQ YD	830	830	0
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	1,438	1,438	0
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	152	152	0
48101498	AGGREGATE SHOULDERS, TYPE B 4"	SQ YD	349	349	0
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	985	985	0
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	0	1
50104400	CONCRETE HEADWALL REMOVAL	EACH	1	1	0
50800105	REINFORCEMENT BARS	POUND	14,420	0	14,420

80% FED./20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE ROADWAY 0004	CONSTRUCTION TYPE CODE BRIDGE 0040
51500100	NAME PLATES	EACH	1	0	1
54003000	CONCRETE BOX CULVERTS	CU YD	72.9	0.0	72.9
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	8	8	0
5422D024	PIPE CULVERTS, CLASS D, TYPE 2 24" (TEMPORARY)	FOOT	20	20	0
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	250	250	0
* 63000003	STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS	FOOT	550	550	0
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	0
63200310	GUARDRAIL REMOVAL	FOOT	876	876	0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	0
67100100	MOBILIZATION	L SUM	1.0	1.0	0.0
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1	0
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	729	729	0
70400100	TEMPORARY CONCRETE BARRIER	FOOT	248	248	0
* 70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	240	240	0
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1	0
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,496	2,496	0
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	14	14	0
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	0
78300100	PAVEMENT MARKING REMOVAL	SQ FT	608	608	0
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	14	14	0
X0322672	SPLIT RAIL FENCE TO BE REMOVED AND RE-ERECTED	FOOT	62	62	0
* X0326276	TEMPORARY LIGHTING FOR SINGLE LANE STAGING	L SUM	1.0	1.0	0.0
X0426200	DEWATERING	L SUM	1.0	1.0	0.0
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1.0	1.0	0.0
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	2,024	2,024	0
X7030055	WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH	FOOT	27	27	0
Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	317	317	0
Z0013798	CONSTRUCTION LAYOUT	L SUM	1.0	1.0	0.0
Z0026407	TEMPORARY SHEET PILING	SQ FT	1,275	0	1,275
Z0030280	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE,NARROW), TEST LEVEL 3	EACH	2	2	0
Z0030370	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, NARROW), TEST LEVEL 3	EACH	2	2	0
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	0
Z0062456	TEMPORARY PAVEMENT	SQ YD	76	76	0

* SPECIALTY ITEMS

ENTRAN	USER NAME = 2bookd	DESIGNED - DJB	REVISED -
	PLOT SCALE = 50.00 ' / IN.	DRAWN - ENTRAN	REVISED -
	PLOT DATE = 7/7/2011	CHECKED - TMH	REVISED -
		DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
SUMMARY OF QUANTITIES

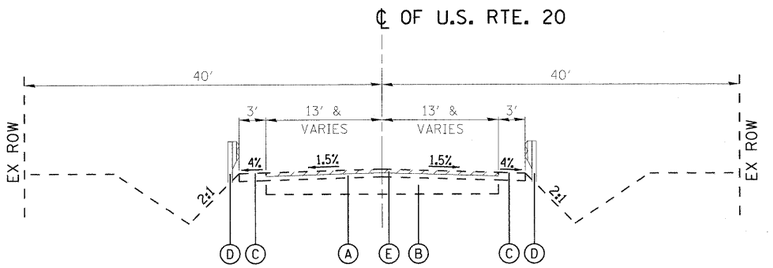
SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	3

CONTRACT NO. 60M53

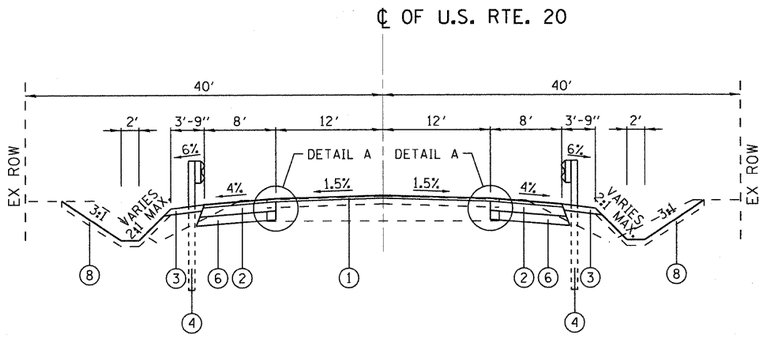
FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

FILE NAME = G:\project\2102165_003\CADD\Civil\Sht\D160M53-03sht-SQ.dgn



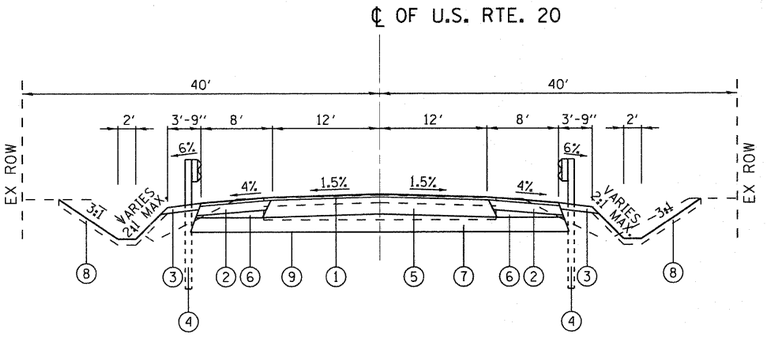
EXISTING TYPICAL SECTION

FACING NORTH
N.T.S.



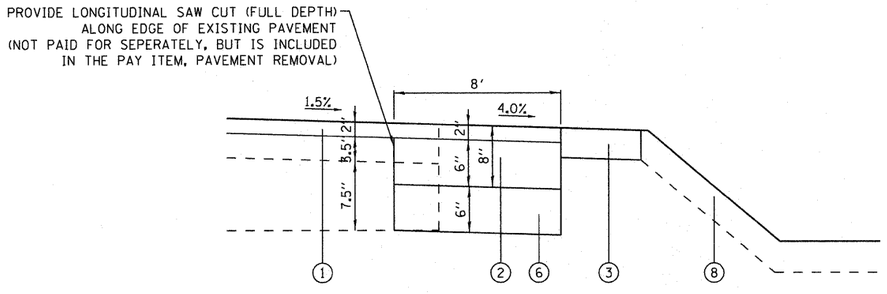
PROPOSED TYPICAL SECTION US ROUTE 20

FACING NORTH
RESURFACING WITH SHOULDER WIDENING
STA. 111+19.7 TO STA. 114+79.8 AND
STA. 115+51.2 TO STA. 117+30.4
N.T.S.



PROPOSED TYPICAL SECTION US ROUTE 20

FACING NORTH
RECONSTRUCTION AREA WITH SHOULDER WIDENING
STA. 114+79.8 TO STA. 115+51.2
N.T.S.



DETAIL A

EXISTING LEGEND

- (A) EXIST. HMA SURFACE, ± 5"
- (B) EXIST. PCC PAVEMENT, ± 7.5"
- (C) AGGREGATE SHOULDER, VARIABLE DEPTH
- (D) EXIST. GUARDRAIL
- (E) HMA SURFACE REMOVAL, 1.5"

PROPOSED LEGEND

- (1) HMA SURFACE COURSE, MIX 'D', N70 (2.0")
- ++ (2) HMA SHOULDERS, 8"
- (3) AGGREGATE SHOULDERS, TYPE B 4"
- (4) STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FT POSTS
- (5) HMA BASE COURSE, 11 1/4"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B (6.0" AND VARIES)
- (7) AGGREGATE SUBGRADE 12"
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (9) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

++ THE TOP 2.0 INCHES OF THE HMA SHOULDER SHALL BE HMA SURFACE COURSE AND IS INCIDENTAL TO THE COST OF HMA SHOULDERS.

HMA MIXTURE REQUIREMENTS

MIX DESIGNS	AIR VOIDS
PAVEMENT RECONSTRUCTION	
HMA SURFACE COURSE, MIX 'D', N70 (IL 9.5 mm); 2"	4% @ 70 GYRATIONS
HMA BASE COURSE, 11 1/4" (HMA BINDER IL-19 mm)	4% @ 70 GYRATIONS
PAVEMENT RESURFACING	
HMA SURFACE COURSE, MIX 'D', N70 (IL 9.5 mm); 2"	4% @ 70 GYRATIONS
SHOULDERS	
HMA BINDER COURSE, IL 19.0, N70; 8"	4% @ 70 GYRATIONS
DRIVEWAYS	
HMA SURFACE COURSE, MIX 'C', N50 (IL 9.5 mm); 2"	4% @ 50 GYRATIONS
HMA BASE COURSE, 6" (HMA BINDER IL-19mm)	4% @ 50 GYRATIONS
TEMPORARY PAVEMENT	
HMA BINDER COURSE, IL-19 mm; 8"	4% @ 50 GYRATIONS

NOTE: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

TEMPORARY PAVEMENT NOTE: IF CONTRACTOR CHOOSES TO USE CONCRETE, THE THICKNESS WILL BE 8"

FILE NAME = G:\projects\2102156.083\CADD\civil\Shts\D160M53-04sht-typicaldgn

ENTRAN

USER NAME = 2bookd	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 6/24/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
TYPICAL SECTIONS

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	4
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SCHEDULE OF QUANTITIES

20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER)

STATION	OFFSET(FT)	UNIT
111+45.0	34.0 LT	8.00
111+70.0	38.0 LT	14.00
111+77.0	33.0 LT	8.00
112+22.0	29.0 LT	7.00
112+38.0	35.0 LT	6.00
112+43.0	32.0 LT	8.00
112+80.0	34.0 LT	11.00
113+20.0	34.0 LT	8.00
113+42.0	36.0 LT	7.00
113+67.4	36.8 LT	10.00
113+80.0	36.0 LT	8.00
113+95.0	36.0 LT	7.00
114+40.0	34.4 LT	15.00
114+63.0	35.7 LT	14.00
115+25.0	36.2 LT	11.00
115+40.0	36.0 LT	8.00
115+57.0	33.4 LT	13.00
115+61.7	37.8 LT	12.00
115+79.5	32.1 LT	15.00
115+90.0	35.0 LT	7.00
116+02.5	31.1 LT	8.00
112+55.0	38.0 RT	9.00
112+85.0	36.0 RT	12.00
113+65.0	32.0 RT	6.00
114+22.0	28.0 RT	7.00
114+22.0	28.0 RT	6.00
114+25.0	34.0 RT	8.00
114+80.2	25.2 RT	12.00
114+95.0	26.0 RT	7.00
	TOTAL =	272

20100210 TREE REMOVAL (OVER 15 UNITS DIAMETER)

STATION	OFFSET(FT)	UNIT
111+70.0	38.0 LT	20.00
	TOTAL =	20

20300100 CHANNEL EXCAVATION

STATION	CUT/FILL(SF)	-	STATION	CUT/FILL(SF)	CU YD
LEFT SIDE - WEST ROW LINE TOWARDS CULVERT					
+39.8	0.0	-	+46.3	10.0	1.2
+46.3	10.0	-	+52.9	7.3	2.1
RIGHT SIDE - EAST CULVERT TOWARDS ROW LINE					
1+07.2	14.8	-	1+13.7	4.5	2.3
1+13.7	4.5	-	1+20.2	0.0	0.6
				TOTAL =	6

CROSS SECTIONS TAKEN ALONG THE CENTERLINE OF THE CULVERT

21101615 TOPSOIL FURNISH AND PLACE, 4"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
STAGE I - LEFT SIDE					
110+50.0	0.0	-	110+70.4	0.0	0.0
110+70.4	0.0	-	111+00.0	11.4	18.8
111+00.0	11.4	-	111+50.0	18.8	83.9
111+50.0	18.8	-	111+65.7	19.0	32.9
111+65.7	19.0	-	112+00.0	17.8	70.2
112+00.0	17.8	-	112+50.0	12.8	84.9
112+50.0	12.8	-	113+00.0	15.7	79.2
113+00.0	15.7	-	113+50.0	15.7	87.4
113+50.0	15.7	-	114+00.0	15.4	86.5
114+00.0	15.4	-	114+50.0	14.4	82.8
114+50.0	14.4	-	115+00.0	12.2	73.8
115+00.0	12.2	-	115+07.9	14.7	11.8
CULVERT LOCATION					
115+23.4	14.1	-	115+50.0	12.5	39.4
115+50.0	12.5	-	116+00.0	12.2	68.7
116+00.0	12.2	-	116+50.0	6.7	52.4
116+50.0	6.7	-	116+58.1	0.0	3.0
116+58.1	0.0	-	117+00.0	3.7	8.5
117+00.0	3.7	-	117+11.9	2.2	3.9
117+11.9	2.2	-	117+30.4	0.0	2.3
117+30.4	0.0	-	117+69.8	0.0	0.0
STAGE II - RIGHT SIDE					
110+50.0	0.0	-	110+70.4	0.0	0.0
110+70.4	0.0	-	111+00.0	11.3	18.6
111+00.0	11.3	-	111+50.0	10.5	60.6
111+50.0	10.5	-	111+65.7	0.0	9.1
111+65.7	0.0	-	112+00.0	16.7	31.9
112+00.0	16.7	-	112+50.0	18.4	97.4
112+50.0	18.4	-	113+00.0	12.8	86.6
113+00.0	12.8	-	113+50.0	10.3	64.4
113+50.0	10.3	-	114+00.0	11.7	61.3
114+00.0	11.7	-	114+50.0	12.3	66.8
114+50.0	12.3	-	115+00.0	10.7	63.9
115+00.0	10.7	-	115+07.9	13.3	10.5
CULVERT LOCATION					
115+23.4	14.4	-	115+50.0	10.6	36.9
115+50.0	10.6	-	116+00.0	9.2	54.9
116+00.0	9.2	-	116+50.0	9.0	50.6
116+50.0	9.0	-	116+58.1	8.1	7.7
116+58.1	8.1	-	117+00.0	17.2	58.8
117+00.0	17.2	-	117+11.9	0.0	11.4
117+11.9	0.0	-	117+30.4	0.0	0.0
117+30.4	0.0	-	117+69.8	0.0	0.0
				TOTAL =	1,682

25000210 SEEDING, CLASS 2A

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	ACRE
REFER TO ITEM "21101615 TOPSOIL F & P 4" FOR QTY					0.347
LESS ITEM "25000312 SEEDING CL 4A"					-0.031
				TOTAL =	0.32

25000312 SEEDING, CLASS 4A

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	ACRE
LEFT					
114+75.0	14.5	-	114+97.9	14.4	0.008
114+97.9	8.4	-	115+07.0	0.5	0.001
115+19.1	0.5	-	115+25.3	13.7	0.001
115+25.3	13.7	-	115+50.0	13.1	0.008
RIGHT					
114+80.0	11.6	-	115+05.7	11.5	0.007
115+05.7	8.6	-	115+12.0	0.5	0.001
115+24.1	0.5	-	115+33.2	11.0	0.001
115+33.2	11.0	-	115+50.0	10.9	0.004
				TOTAL =	0.03

28000250 TEMPORARY EROSION CONTROL SEEDING

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	POUND
RATE = 100.0 POUND / ACRE					
AFTER COMPLETION OF STAGE I					
110+70.4	16.0	-	117+69.8	16.0	25.7
				TOTAL =	26

28000305 TEMPORARY DITCH CHECKS

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
ASSUMED 10 FT LENGTH AT EACH LOCATION					
111+55.0	29.9 LT				10.0
111+90.0	30.9 LT				10.0
112+25.0	33.3 LT				10.0
112+60.0	35.7 LT				10.0
112+95.0	36.6 LT				10.0
113+30.0	37.3 LT				10.0
113+65.0	37.6 LT				10.0
114+00.0	37.3 LT				10.0
114+35.0	36.3 LT				10.0
114+70.0	35.8 LT				10.0
115+30.0	33.9 LT				10.0
116+05.0	32.3 LT				10.0
112+15.0	28.0 RT				10.0
112+50.0	32.0 RT				10.0
				TOTAL =	140

28000400 PERIMETER EROSION BARRIER

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
110+94.5	38.6 RT	-	111+59.7	38.6 RT	65.2
111+71.0	38.6 RT	-	114+00.0	38.6 RT	229.0
114+00.0	38.6 RT	-	115+05.7	38.6 RT	105.7
115+32.6	38.6 RT	-	116+81.2	38.6 RT	148.7
111+19.7	33.5 LT	-	112+50.0	38.6 LT	130.4
112+50.0	38.6 LT	-	114+00.0	38.6 LT	150.0
114+00.0	38.6 LT	-	114+97.9	38.6 LT	97.9
115+30.0	38.6 LT	-	116+54.0	38.6 LT	124.0
				TOTAL =	1.051

35501308 HOT-MIX ASPHALT BASE COURSE, 6"

STATION	AREA (SF)	SQ YD
PRIVATE ENTRANCES		
111+65.4 RT	530.7	59.0
116+58.4 LT	428.6	47.6
117+11.0 RT	495.2	55.0
	TOTAL =	162

35501329 HOT-MIX ASPHALT BASE COURSE, 11 1/4"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
114+79.8	24.0	-	115+51.2	24.0	190.4
				TOTAL =	190

40600982 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
111+19.7	24.0	-	111+24.2	24.0	12.0
117+25.9	24.0	-	117+30.4	24.0	12.0
				TOTAL =	24

40603310 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	TON
RATE = 112.0 POUND / SQ YD / INCH					
DEPTH = 2.0 INCH					
REFER TO ITEM "35501308 HMA BASE CSE 6"					
111+65.4 RT					6.6
116+58.4 LT					5.3
117+11.0 RT					6.2
				TOTAL =	18

40603340 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	TON
RATE = 112.0 POUND / SQ YD / INCH					
DEPTH = 2.0 INCH					
MAINLINE					
111+19.7	24.0	-	117+30.4	24.0	182.4
				TOTAL =	182

44000100 PAVEMENT REMOVAL

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
CULVERT REPLACEMENT AREA					
114+79.8	24.0	-	115+51.2	24.0	190.4
LEFT SHOULDER					
111+18.1	0.0	-	111+19.7	3.7	0.3
111+19.7	5.5	-	116+02.1	4.7	274.1
116+02.1	4.7	-	116+46.5	5.6	25.5
116+46.5	1.8	-	116+71.4	1.8	5.0
116+71.4	5.6	-	116+99.0	4.9	16.0
116+99.0	4.9	-	117+30.4	6.0	18.9
RIGHT SHOULDER					
111+19.7	4.8	-	111+42.7	5.2	12.7
111+42.7	1.0	-	111+90.8	1.3	6.1
111+90.8	4.4	-	116+01.4	2.9	164.7
116+01.4	2.9	-	116+49.3	3.6	17.1
116+49.3	3.6	-	117+00.6	4.3	22.6
REFER TO ITEM "Z0062456 TEMP PAVEMENT"					76.2
				TOTAL =	830

44000155 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
111+19.7	24.0	-	114+79.8	24.0	960.3
115+51.2	24.0	-	117+30.4	24.0	477.9
				TOTAL =	1.438

44000200 DRIVEWAY PAVEMENT REMOVAL

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
DRIVEWAY			111+65.4 RT		64.9
DRIVEWAY			116+58.4 LT		36.5
DRIVEWAY			117+11.0 RT		50.3
				TOTAL =	152

48101498 AGGREGATE SHOULDERS, TYPE B 4"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
LEFT SIDE					
112+21.7	0.0	-	112+45.7	4.0	5.3
112+45.7	4.0	-	112+80.7	4.0	15.6
112+80.7	4.0	-	112+82.2	3.8	0.7
112+82.2	3.8	-	116+16.7	3.8	139.4

FILE NAME = G:\project\2102155.003\CADD\Civil\Sht\D1608M53-09sht-schedule.dgn

ENTRAN

USER NAME = 2sookd	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 6/28/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
SCHEDULES OF QUANTITIES

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	5
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SCHEDULE OF QUANTITIES

116+16.7	3.8	-	116+18.2	4.0	0.7
116+18.2	4.0	-	116+52.3	4.0	15.2
RIGHT SIDE					
112+59.6	0.0	-	112+83.6	4.0	5.3
112+83.6	4.0	-	113+18.6	4.0	15.6
113+18.6	4.0	-	113+20.1	3.8	0.7
113+20.1	3.8	-	116+29.6	3.8	129.0
116+29.6	3.8	-	116+31.1	4.0	0.7
116+31.1	4.0	-	116+66.1	4.0	15.6
116+66.1	4.0	-	116+90.1	0.0	5.3
				TOTAL =	349

48203029 HOT-MIX ASPHALT SHOULDERS, 8"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
LEFT SIDE					
111+18.1	0.0	-	111+19.7	5.5	0.5
111+19.7	5.5	-	111+69.7	8.0	37.5
111+69.7	8.0	-	116+49.4	8.0	426.4
116+68.2	6.0	-	117+30.4	6.0	41.2
RIGHT SIDE					
111+19.7	4.8	-	111+46.4	5.1	14.7
111+79.1	8.0	-	117+02.3	8.0	465.1
				TOTAL =	985

50104400 CONCRETE HEADWALL REMOVAL

STATION	OFFSET(FT)	EACH
111+20.2	24.1 LT	1.0
	TOTAL =	1

54213660 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"

STATION	OFFSET(FT)	EACH
110+70.4	22.4 LT	1.0
111+20.3	28.0 LT	1.0
110+91.8	18.8 RT	1.0
112+00.0	26.6 RT	1.0
116+45.8	26.6 LT	1.0
116+67.4	25.0 LT	1.0
117+00.0	32.6 RT	1.0
117+69.5	28.6 RT	1.0
	TOTAL =	8

542A0220 PIPE CULVERTS, CLASS A, TYPE 1 15"

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
110+70.4	22.4 LT	-	111+20.3	28.0 LT	50.2
110+91.8	18.8 RT	-	112+00.0	26.6 RT	108.5
116+45.8	26.6 LT	-	116+67.4	25.0 LT	21.7
117+00.0	32.6 RT	-	117+69.5	28.6 RT	69.6
				TOTAL =	250

63000003 STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
113+05.7	20.0 LT	-	115+93.2	20.0 LT	287.5
113+43.6	20.0 RT	-	116+06.1	20.0 RT	262.5
				TOTAL =	550

63100167 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT

STATION	OFFSET(FT)	EACH
112+55.7	20.0 LT	1.0
112+93.6	20.0 RT	1.0
116+43.2	20.0 LT	1.0
116+56.1	20.0 RT	1.0
	TOTAL =	4

63200310 GUARDRAIL REMOVAL

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
111+39.1	17.5 LT	-	115+89.0	17.8 LT	449.9
111+95.9	15.9 RT	-	116+22.3	16.2 RT	426.4
				TOTAL =	876

70301000 WORK ZONE PAVEMENT MARKING REMOVAL

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ FT
LEFT AND RIGHT EDGE LINE					
REFER TO ITEM "70300520 PAVT MARK TAPE T3 4"					674.6
STOP BARS					
REFER TO ITEM "70300570 PAVT MARK TAPE T3 24"					54.0
				TOTAL =	729

70400100 TEMPORARY CONCRETE BARRIER

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
STAGE I					
113+80.0	4.0 LT	-	114+50.0	2.0 RT	70.3
114+50.0	2.0 RT	-	115+80.0	2.0 RT	130.0
115+80.0	2.0 RT	-	116+27.0	4.0 LT	47.4
				TOTAL =	248

70400200 RELOCATE TEMPORARY CONCRETE BARRIER

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
RELOCATED FROM STAGE I TO STAGE II PLACEMENT					
113+80.0	4.0 RT	-	114+75.0	4.0 LT	95.3
114+75.0	4.0 LT	-	115+70.0	4.0 LT	95.0
115+70.0	4.0 LT	-	116+19.0	4.0 RT	49.6
				TOTAL =	240

72400600 RELOCATE SIGN PANEL ASSEMBLY - TYPE B

STATION	OFFSET(FT)	EACH
116+00.0	20.0 LT	1.0
	TOTAL =	1

78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4"

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
SOLID WHITE EDGE LINE					
111+19.7	13.8 LT	-	111+69.7	12.0 LT	50.0
111+69.7	12.0 LT	-	116+80.4	12.0 LT	510.7
116+80.4	12.0 LT	-	117+30.4	13.7 LT	50.0
117+30.4	13.7 LT	-	117+48.0	13.7 LT	17.6
111+19.7	13.1 RT	-	111+69.7	12.0 RT	50.0
111+69.7	12.0 RT	-	116+80.4	12.0 RT	510.7
116+80.4	12.0 RT	-	117+30.4	13.1 RT	50.0
SOLID YELLOW NO PASSING ZONE					
111+19.7	0.5 LT	-	117+48.0	0.5 LT	628.3
111+19.7	0.5 RT	-	117+48.0	0.5 RT	628.3
				TOTAL =	2,496

78100100 RAISED REFLECTIVE PAVEMENT MARKER

STATION	OFFSET(FT)	EACH
TWO-WAY AMBER MARKER - 80' O.C.		
111+80.0	0.0 RT	2.0
112+60.0	0.0 RT	2.0
113+40.0	0.0 RT	2.0
114+20.0	0.0 RT	2.0
115+00.0	0.0 RT	2.0
115+80.0	0.0 RT	2.0
116+60.0	0.0 RT	2.0
	TOTAL =	14

78300100 PAVEMENT MARKING REMOVAL

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ FT
WHITE EDGE LINE					
LEFT EDGE					
112+65.0	0.3	-	117+48.0	0.3	152.2
RIGHT EDGE					
112+65.0	0.3	-	116+98.0	0.3	129.9
DOUBLE YELLOW NO PASSING ZONE					
112+05.0	0.3	-	117+48.0	0.3	162.9
112+05.0	0.3	-	117+48.0	0.3	162.9
				TOTAL =	608

78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

STATION	OFFSET(FT)	EACH
TWO-WAY AMBER MARKER - 80' O.C.		
111+80.0	0.0 RT	2.0
112+60.0	0.0 RT	2.0
113+40.0	0.0 RT	2.0
114+20.0	0.0 RT	2.0
115+00.0	0.0 RT	2.0
115+80.0	0.0 RT	2.0
116+60.0	0.0 RT	2.0
	TOTAL =	14

X0322672 SPLIT RAIL FENCE TO BE REMOVED AND RE-ERECTED

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
116+29.4	28.8 RT	-	116+80.4	28.9 RT	51.1
116+80.4	28.9 RT	-	116+81.3	40.0 RT	11.1
				TOTAL =	62

X7030030 WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
STAGE I					
WHITE EDGE LINE					
112+65.0	13.7 LT	-	114+00.3	2.9 RT	136.3
114+00.3	2.9 RT	-	116+05.3	2.9 RT	205.0
116+05.3	2.9 RT	-	117+38.0	13.7 LT	133.7
112+65.0	13.2 RT	-	114+00.3	13.9 RT	135.3
114+00.3	13.9 RT	-	116+05.3	13.9 RT	205.0
116+05.3	13.9 RT	-	117+38.0	13.1 RT	132.7
STAGE II					
WHITE EDGE LINE					
111+19.7	13.8 LT	-	112+83.6	13.8 LT	163.9
112+83.6	13.8 LT	-	114+20.0	16.0 LT	136.4
114+20.0	16.0 LT	-	115+90.0	16.0 LT	170.0
115+90.0	16.0 LT	-	116+23.5	13.5 RT	44.7
116+23.5	13.5 RT	-	116+99.0	13.5 RT	75.5
112+15.0	13.3 RT	-	114+20.0	5.0 LT	205.8
114+20.0	5.0 LT	-	115+90.0	5.0 LT	170.0
115+90.0	5.0 LT	-	116+98.0	13.0 RT	109.5
				TOTAL =	2,024

X7030055 WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH

STATION	OFFSET(FT)	-	STATION	OFFSET(FT)	FOOT
STAGE I & STAGE II					
WHITE STOP BAR					
112+05.0	0.0 RT	-	112+05.0	13.3 RT	13.3
117+48.0	0.0 RT	-	117+48.0	13.7 LT	13.7
				TOTAL =	27

Z0001050 AGGREGATE SUBGRADE 12"

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
114+79.8	40.0	-	115+51.2	40.0	317.3
***WIDTH INCLUDES 24' PAVEMENT AND 8' SHOULDERS					
				TOTAL =	317

Z0062456 TEMPORARY PAVEMENT

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ YD
STAGE I - RIGHT EDGE OF PAVEMENT					
113+80.0	2.7	-	116+25.0	2.9	76.2
				TOTAL =	76

EARTHWORK SCHEDULE

STAGE	EARTH EXCAVATION	EARTH EX. ADJUSTED FOR SHRINKAGE	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	FURNISHED EXCAVATION
STAGE I	263.7 CY	197.8 CY	449.1 CY	-251.3 CY	251.3 CY
STAGE II	174.7 CY	131.0 CY	587.9 CY	-456.9 CY	456.9 CY
TOTAL	438.4 CY	328.8 CY	1037.0 CY	-708.2 CY	708.2 CY

FURNISHED EX. = EMBANKMENT - [SUITABLE EX. x (1 - SHRINK FACTOR)]
SHRINKAGE = 25%

FILE NAME = G:\project\2102155.003\CADD\Civil\Sht\0160M53-05sht-schedule.dgn

ENTRAN

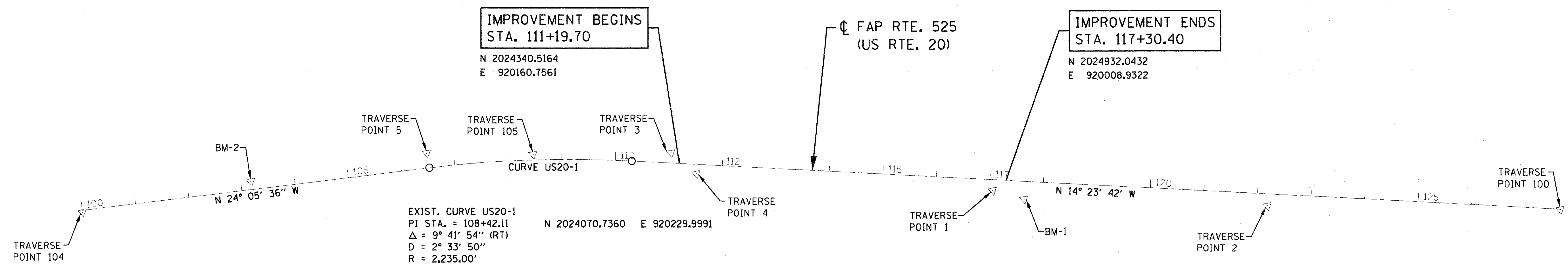
USER NAME = 2bockd	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 6/20/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

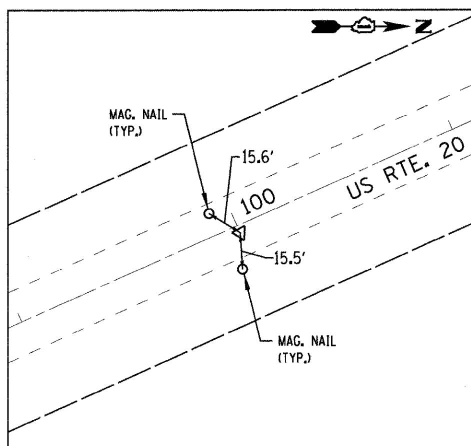
FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
SCHEDULES OF QUANTITIES

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

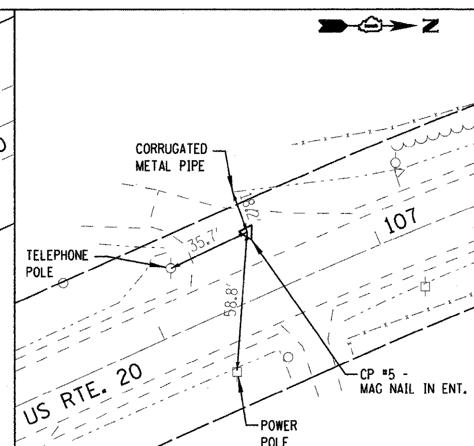
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	6
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



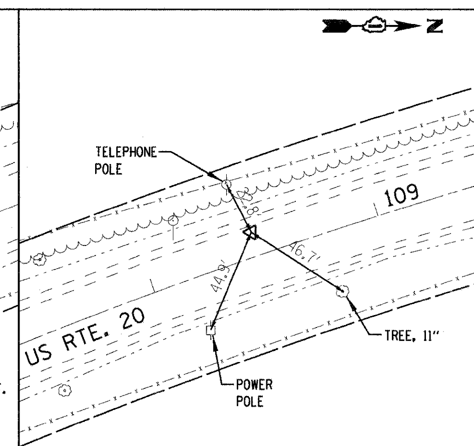
EXIST. CURVE US20-1
PI STA. = 108+42.11 N 2024070.7360 E 920229.9991
Δ = 9° 41' 54" (RT)
D = 2° 33' 50"
R = 2,235.00'
T = 189.61'
L = 378.28'
E = 8.03'
P.C. STA. = 106+52.50 N 2023897.6457 E 920307.4019
P.T. STA. = 110+30.78 N 2024254.3920 E 920182.8611



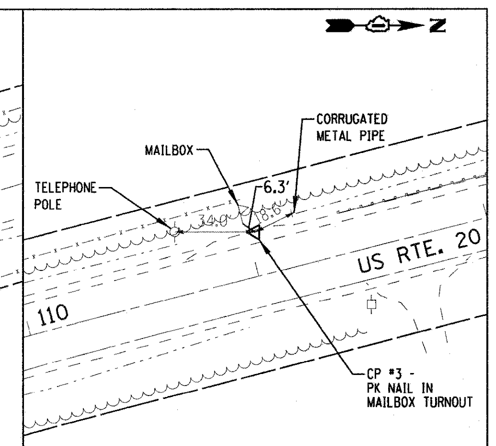
TRAVERSE POINT #104
N 2023303.3537
E 920576.7771
STA 100+00.00, OFFSET 3.30' RT



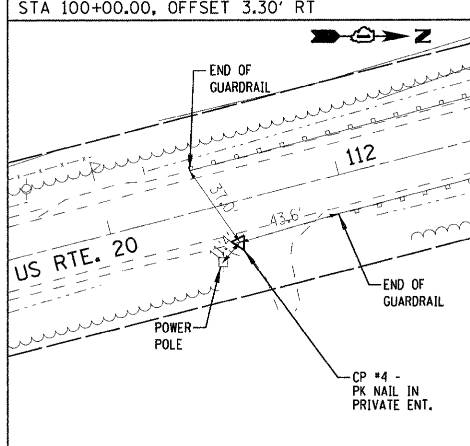
TRAVERSE POINT #5
N 2023884.7750
E 920283.1491
STA 106+50.66, OFFSET 27.39' LT



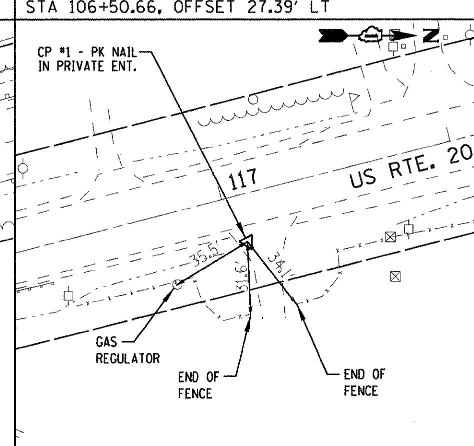
TRAVERSE POINT #105
N 2024075.0248
E 920226.0232
STA 108+46.98, OFFSET 10.37' LT



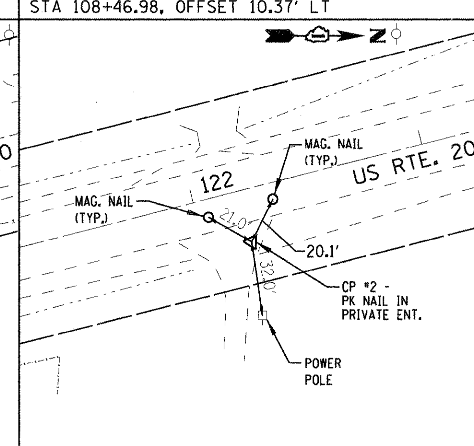
TRAVERSE POINT #3
N 2024319.8564
E 920146.8191
STA 111+03.15, OFFSET 18.64' LT



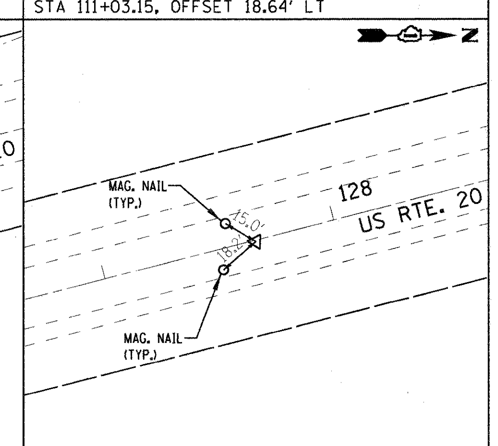
TRAVERSE POINT #4
N 2024376.3046
E 920169.2709
STA 111+52.25, OFFSET 17.14' RT



TRAVERSE POINT #1
N 2024913.6720
E 920034.1988
STA 117+06.32, OFFSET 19.91' RT



TRAVERSE POINT #2
N 2025411.6695
E 919908.6428
STA 122+19.90, OFFSET 22.10' RT



TRAVERSE POINT #100
N 2025935.0991
E 919752.0737
STA 127+65.82, OFFSET 0.57' RT

BENCHMARKS

BM-1 TOP OF ROW MARKER STA. 117+65.57, OFFSET 32.87' RT.
ELEV = 880.33

BM-2 SET CUT '□' IN CENTER OF 14' HEADWALL, ±1100' SOUTH OF BOX CULVERT.
STA. 103+18.59, OFFSET 14.90' LT.
ELEV = 922.06

NOTE:
VERTICAL AND HORIZONTAL CONTROL
GPS RELI-NET NETWORK
GRID SURVEY

ENTRAN

USER NAME = 2kujawa	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 100.00 '/ IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

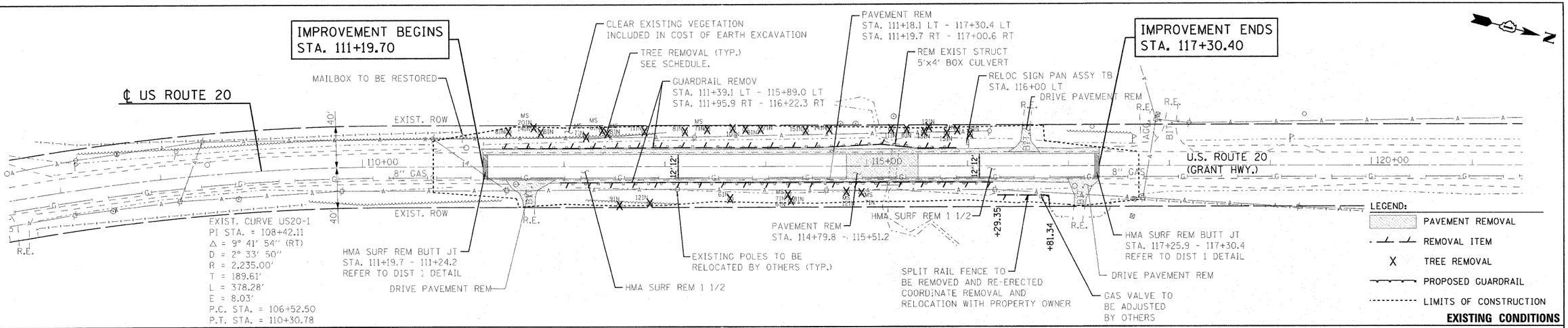
FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
ALIGNMENT, TIES & BENCHMARKS

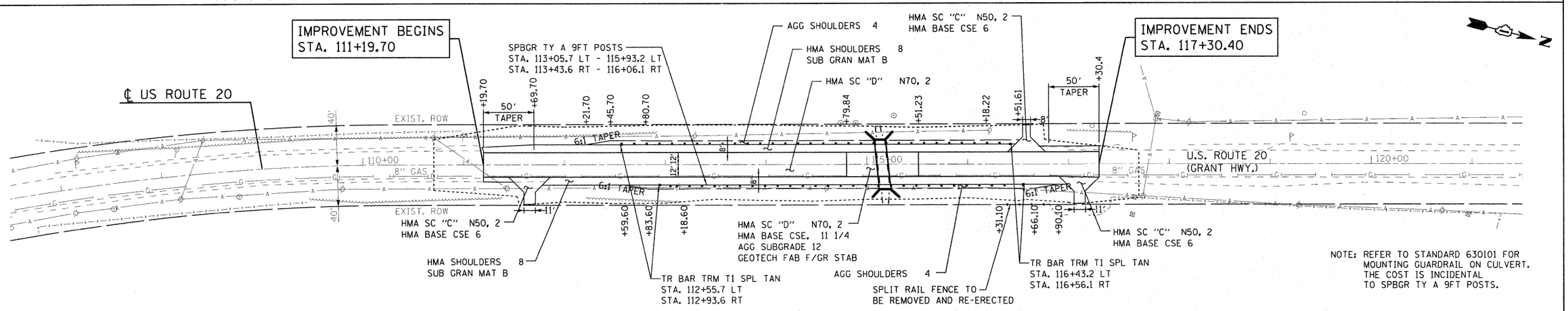
SCALE: 1" = 100' | SHEET NO. OF SHEETS | STA. TO STA.

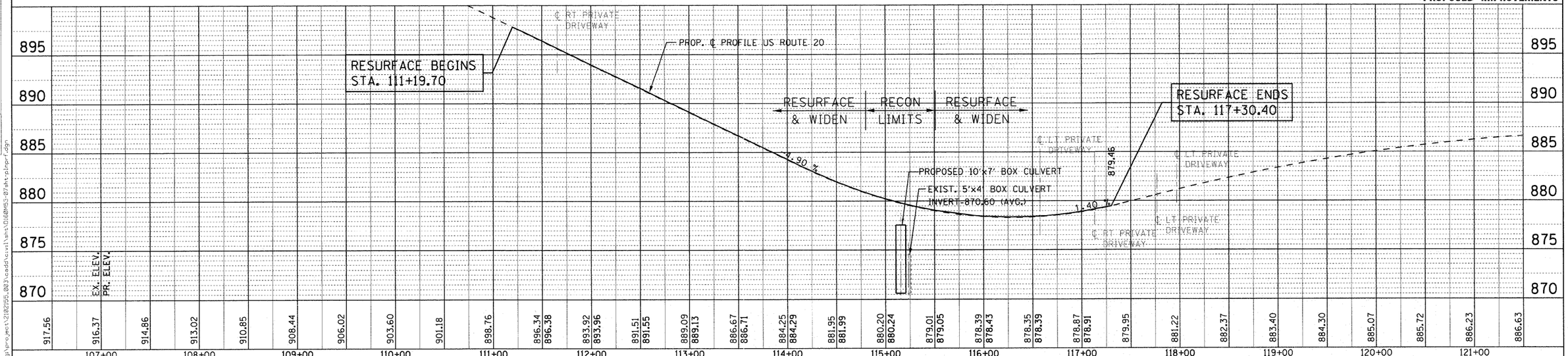
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	7
		CONTRACT NO. 60M53		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

FILE NAME = g:\projects\2102195.003\cadd\civil\sht\D160M53-06sht-ATB.dgn







ENTRAN	USER NAME = 2kuyawe	DESIGNED - DJB	REVISED -
		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -

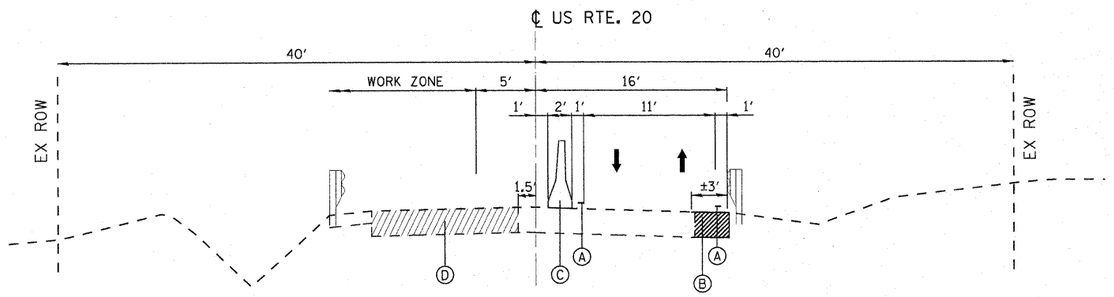
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
EXISTING AND PROPOSED PLAN AND PROFILE

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	8
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN: SURVEYED, PLOTTED, ROADWAY CHECKED, R.O.W. CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE
PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, MATERIALS CHKD, STRUCTURE NOTATNS CHKD, NOTE BOOK, NO., BY, DATE
FILE NAME = g:\projects\2102155.003\caddd\civil\shts\D160M53-07ahtplnprf.dgn



MOT STAGE 1

STA 114+79.8 TO STA 115+51.2

(LOOKING NORTHBOUND)

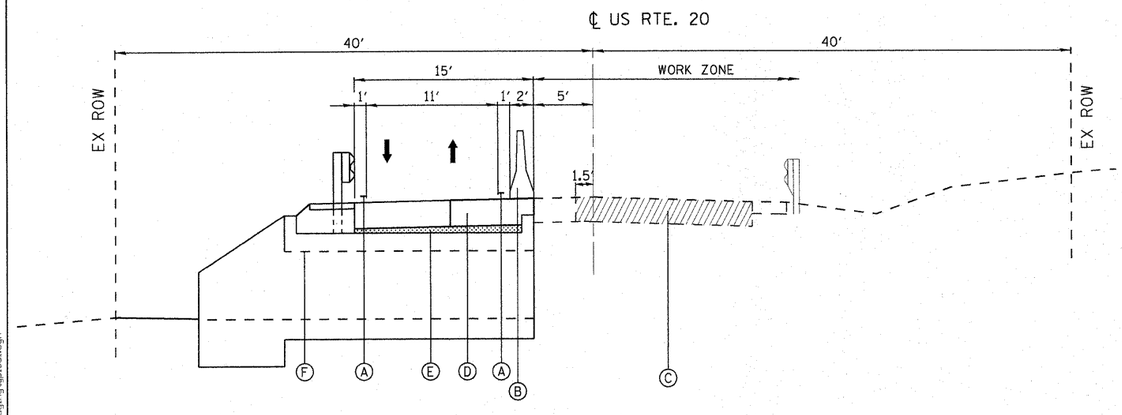
NOTE:

VERTICAL PANELS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE APPLICABLE HIGHWAY STANDARD.

ANY EARTH EXCAVATION REQUIRED TO CONSTRUCT THE TEMPORARY PAVEMENT WILL BE CONSIDERED INCIDENTAL TO THE PAY ITEM "TEMPORARY PAVEMENT."

MOT LEGEND

(A) TEMPORARY PAVEMENT MARKING 4", WHITE
(B) TEMPORARY PAVEMENT, 8"
(C) TEMPORARY CONCRETE BARRIER
(D) STAGE I REMOVAL



MOT STAGE 2

STA 114+79.8 TO STA 115+51.2

(LOOKING NORTHBOUND)

NOTE:

VERTICAL PANELS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE APPLICABLE HIGHWAY STANDARD.

MOT LEGEND

(A) TEMPORARY PAVEMENT MARKING 4", WHITE
(B) TEMPORARY CONCRETE BARRIER
(C) STAGE II REMOVAL
(D) HMA PAVEMENT (REFER TO TYPICAL SECTION)
(E) AGGREGATE SUBGRADE
(F) SEE STRUCTURAL PLANS FOR CULVERT DETAILS

GENERAL NOTES - TRAFFIC CONTROL

MAINTENANCE OF TRAFFIC DEVICES TO BE INSTALLED IN ACCORDANCE WITH APPLICABLE POTIONS OF STANDARDS 701201, 701306, 701321 AND 701326 AND DISTRICT ONE STANDARDS TC-11 AND TC-13. ADDITIONAL SIGNAGE MAY BE REQUIRED BY THE RESIDENT ENGINEER AT NO ADDITIONAL COST. THIS WORK AND SIGNAGE IS INCLUDED IN THE PAY ITEM FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

THE MAINTENANCE OF TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT, HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF THE PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

THE RESIDENT ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MOT PLANS.

EXISTING CONFLICTING PAVEMENT MARKINGS SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL.

REMOVAL OF THE TEMPORARY PAVEMENT MARKINGS SHALL BE PAID FOR UNDER THE PAY ITEM WORK ZONE PAVEMENT MARKING REMOVAL.

THE EXISTING PAVEMENT MARKINGS THAT HAVE BEEN REMOVED SHALL BE REPLACED IN-KIND.

THE CONTRACTOR SHALL NOT MOUNT SIGNS ON EXISTING SIGNS.

THE CONTRACTOR SHALL PLACE AN ARTERIAL ROAD INFORMATION SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACE TWO WEEKS BEFORE THE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PER SQUARE FOOT, TEMPORARY INFORMATION SIGNING.

SUGGESTED CONSTRUCTION SEQUENCING

PRE-STAGE

IMPLEMENT STAGE I MOT PAVEMENT MARKINGS, SIGNAGE, AND TEMPORARY SIGNALS. PERFORM PAVEMENT MARKING REMOVAL AS NECESSARY FOR STATE I.

REMOVE EXISTING NORTHBOUND PAVED SHOULDER AND CONSTRUCT TEMPORARY PAVEMENT USING STANDARD 701201.

STAGE 1

IMPLEMENT HIGHWAY STANDARD 701321 "LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER" AND SUGGEST MOT PROVIDED IN PLANS.

PERFORM WORK WITHIN OR ADJACENT TO US 20 SOUTHBOUND LANES: IMPLEMENT TEMPORARY EROSION CONTROL MEASURES, COMPLETE ROADWAY REMOVALS, PERFORM OFF-ROAD GRADING OPERATIONS, CONSTRUCT WEST HALF OF BOX CULVERT, CONSTRUCT PAVEMENT AND SHOULDER WIDENING UP TO BINDER COURSE, AND INSTALL GUARDRAIL.

STAGE 2

REFER TO STAGE 1 NOTES FOR US 20 NORTHBOUND LANES AND EAST HALF OF BOX CULVERT.
REMOVE TEMPORARY PAVEMENT (TO BE PAID FOR AS "PAVEMENT REMOVAL")

STAGE 3

INSTALL SURFACE COURSE USING STANDARD 701306.
INSTALL FINAL PAVEMENT MARKING AND RAISED REFLECTIVE MARKERS USING STANDARD 701311.
INSTALL FINAL LANDSCAPING USING STANDARD 701006.

THE CONTRACTOR SHALL COORDINATE THE EXACT PLACEMENT OF ADVANCED WARNING SIGNAGE WITH THE RESIDENT ENGINEER.

ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC, AS DETAILED ON THE PLANS, OR HIGHWAY STANDARDS SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN MAINTENANCE OF TRAFFIC SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

ALL DRUMS, VERTICAL PANELS, AND BARRICADES ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY-BURNING LIGHTS.

ALL EXISTING SIGNS WITHIN THE LIMITS OF MAINTENANCE OF TRAFFIC WHICH ARE OBSCURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THE WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL COORDINATE THE EXACT PLACEMENT OF ADVANCED WARNING SIGNAGE WITH THE RESIDENT ENGINEER

THE CONTRACTOR IS ADVISED THAT IN THE EVENT OF SNOW, HE WILL BE RESPONSIBLE FOR THE IMMEDIATE REMOVAL OF ANY MAINTENANCE OF TRAFFIC AND/OR PROTECTIVE DEVICES THAT WOULD INTERFERE WITH SNOW REMOVAL OPERATIONS PERFORMED BY THE STATE OR LOCAL AGENCY.

FOR ADDITIONAL CULVERT CONSTRUCTION STAGING INFORMATION AND SHEET PILING RECOMMENDATIONS, SEE STRUCTURAL PLANS.

FILE NAME = G:\project\2102155.003\CADD\Civil\Shn\D160M53-08ahrstagingtypsect.dgn

ENTRAN

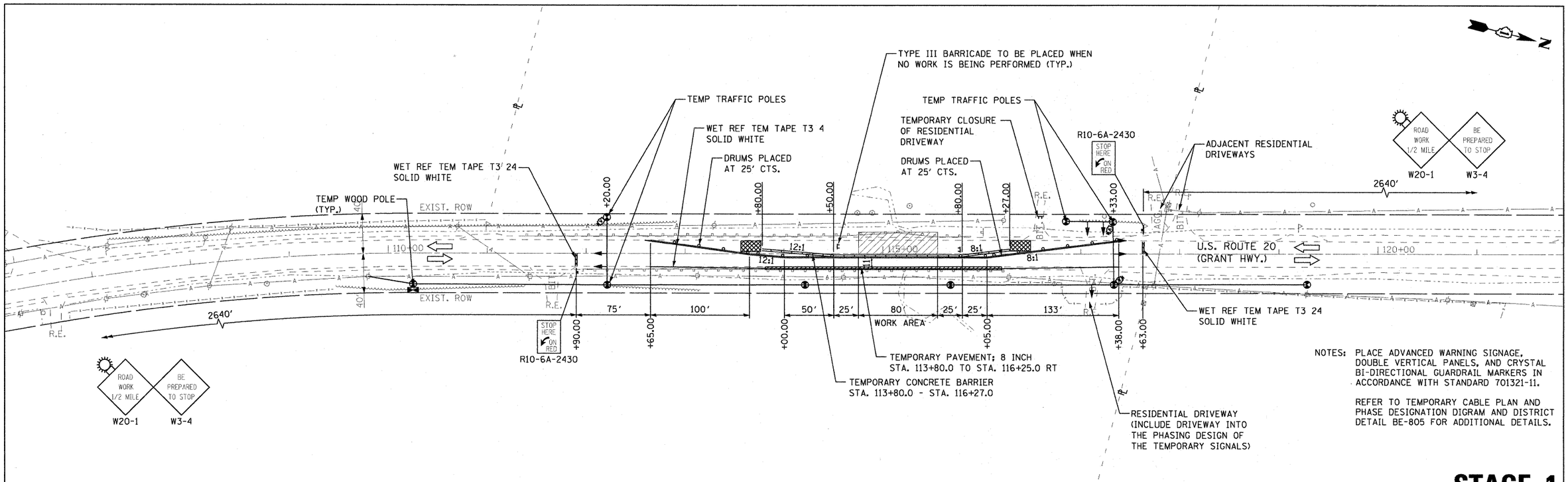
USER NAME = 2baokd	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 7/6/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

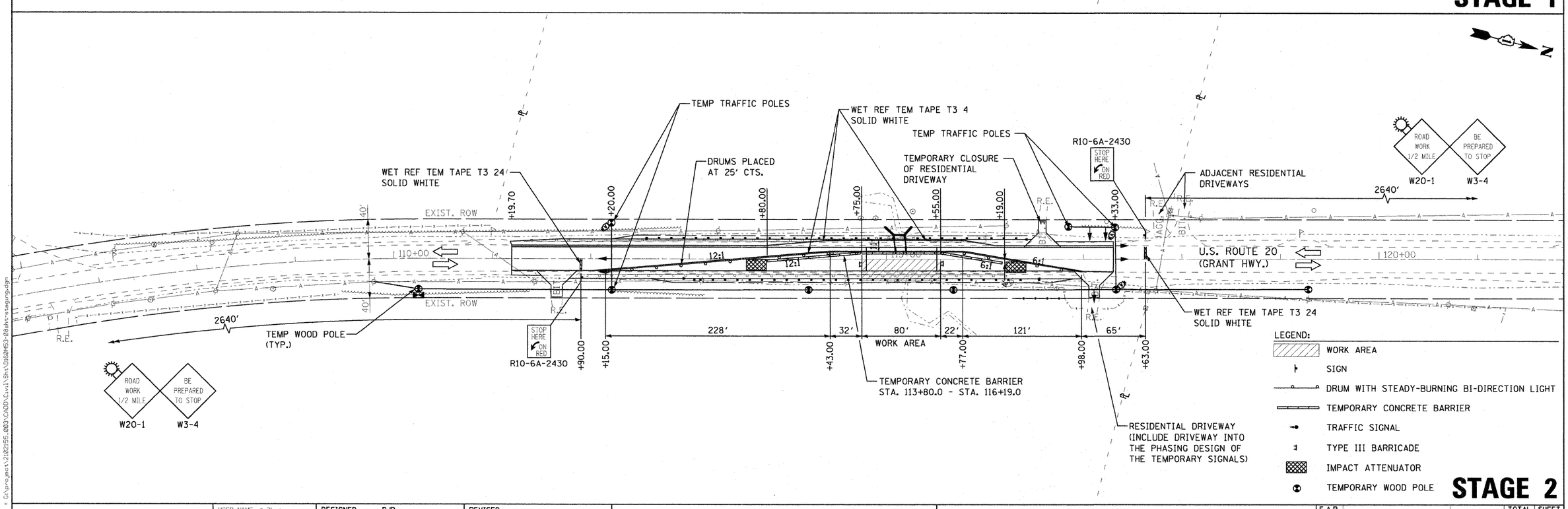
FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
MAINTENANCE OF TRAFFIC GENERAL NOTES & TYPICAL SECTIONS

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	9
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



STAGE 1



STAGE 2

FILE NAME = G:\projects\2102155.003\CADD\Civil\Sht\D160M53-08sht-staging.dgn

ENTRAN

USER NAME = 2kujawa	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 07/05/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
MAINTENANCE OF TRAFFIC – STAGE 1 AND 2

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	10
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

IMPROVEMENT BEGINS
STA. 111+19.70

℄ US ROUTE 20

ENTRANCE CULVERT
TO BE REMOVED
(PAID AS EARTH EX.)

CONC HDWL REM
STA. 111+20.2

EXIST. ROW

110+00

8" GAS

R.E.

ENTRANCE CULVERT
TO BE REMOVED
(PAID AS EARTH EX.)

ENTRANCE CULVERT
TO BE REMOVED
(PAID AS EARTH EX.)

REM EXIST STRUCT
5'x4' BOX CULVERT

115+00

IMPROVEMENT ENDS
STA. 117+30.40

U.S. ROUTE 20
(GRANT HWY.)

120+00

LEGEND:
- CULVERT REMOVAL
- REMOVAL ITEM
- PROPOSED DITCH (2 FT WIDE TYP)
- PROPOSED STORM SEWER
- LIMITS OF CONSTRUCTION

IMPROVEMENT BEGINS
STA. 111+19.70

℄ US ROUTE 20

1 PRC FLAR END SEC 15
STA. 110+70.4
OFF. 22.4' LT
EL.=898.10

P CUL CL A 1 15 (A)
50 LF @ 5.00%
(RCCP CLASS IV)

PRC FLAR END SEC 15 2
STA. 111+20.3
OFF. 28.0' LT
EL.=895.60

50'
TAPER

3 PRC FLAR END SEC 15
STA. 110+91.8
OFF. 18.8' RT
EL.=897.16

B P CUL CL A 1 15
108 LF @ 4.78%
(RCCP CLASS IV)

PRC FLAR END SEC 15 4
STA. 112+00.0
OFF. 26.6' RT
EL.=892.00

LIMITS OF CHANNEL EXCAVATION
(TYP.)

D P CUL CL A 1 15
22 LF @ 4.55%
(RCCP CLASS IV)

8 PRC FLAR END SEC 15
STA. 116+45.8
OFF. 26.6' LT
EL.=875.75

PROP. 10'x7'
BOX CULVERT

6 PRC FLAR END SEC 15
STA. 117+00.0
OFF. 32.6' RT
EL.=875.40

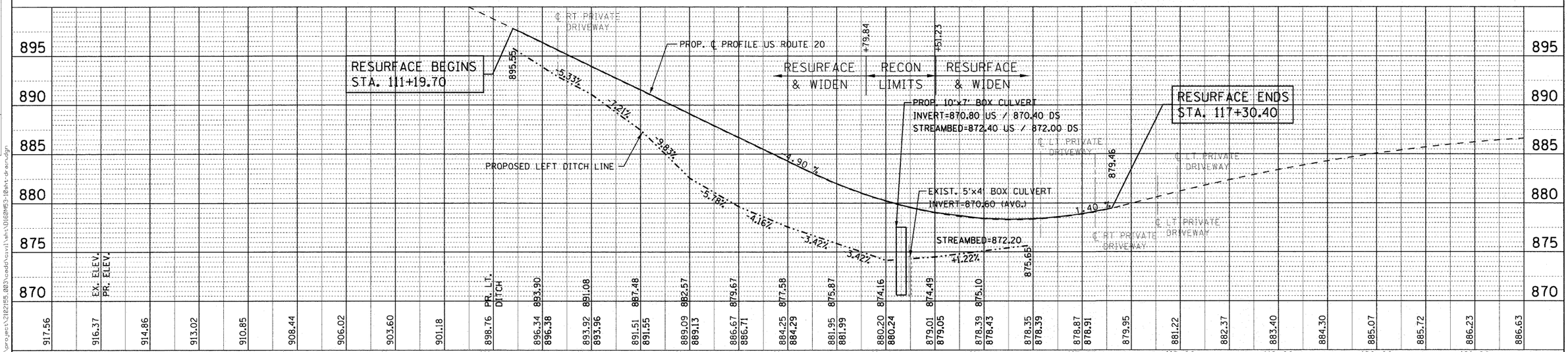
PRC FLAR END SEC 15 7
STA. 116+67.4
OFF. 25.0' LT
EL.=876.75

IMPROVEMENT ENDS
STA. 117+30.40

U.S. ROUTE 20
(GRANT HWY.)

PRC FLAR END SEC 15 5
STA. 117+69.5
OFF. 28.6' RT
EL.=877.27

P CUL CL A 1 15 (C)
70 LF @ 2.67%
(RCCP CLASS IV)



ENTRAN

USER NAME = 2kujawc	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
EXISTING AND PROPOSED DRAINAGE AND UTILITY PLAN

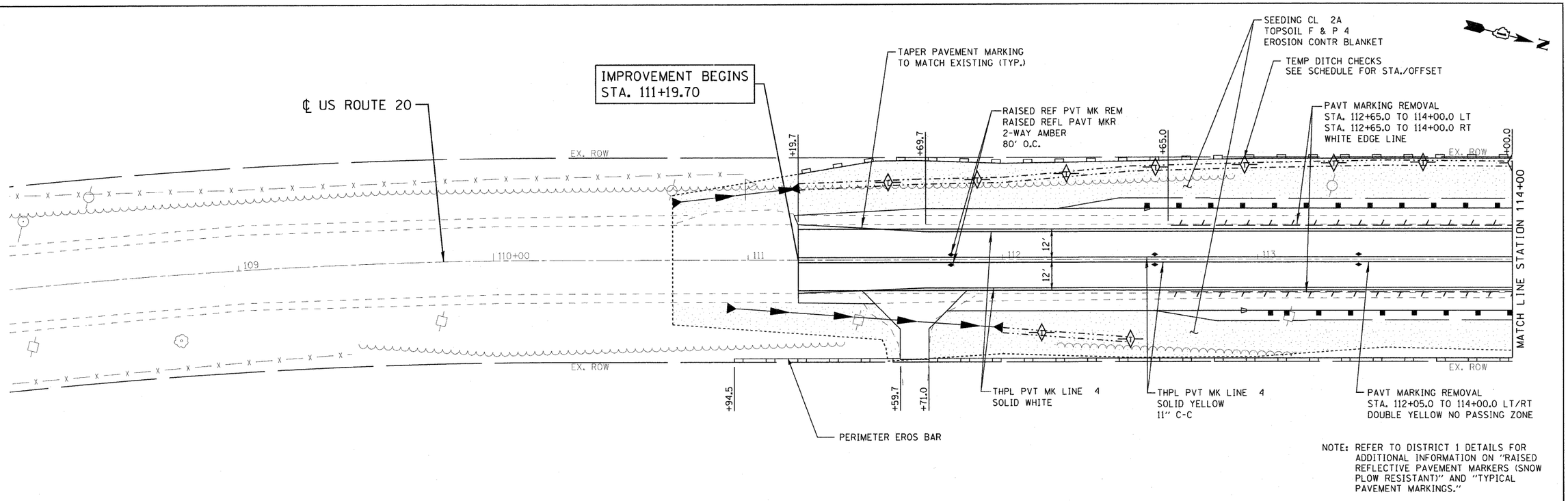
SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	11
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

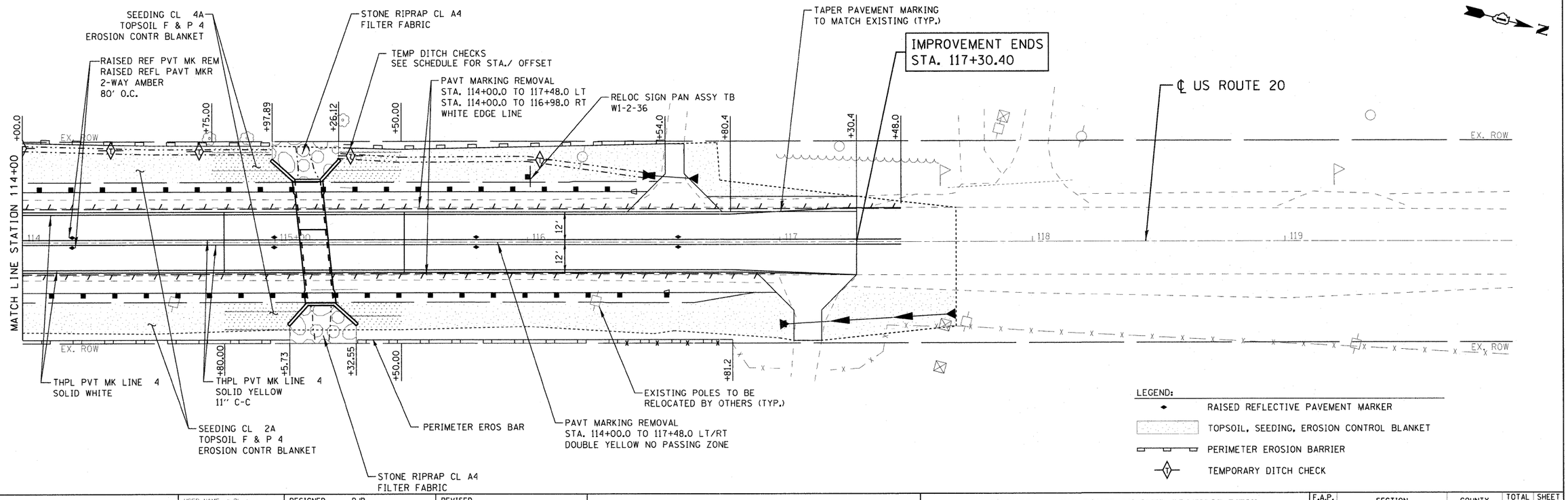
PLAN — SURVEYED, PLOTTED, GRADES CHECKED, R.O.W. CHECKED, CADD FILE NAME, NOTE BOOK, NO., STRUCTURE NO'S ATTN'S CHKD, BY, DATE

PROFILE — SURVEYED, PLOTTED, GRADES CHECKED, R.O.W. CHECKED, CADD FILE NAME, NOTE BOOK, NO., STRUCTURE NO'S ATTN'S CHKD, BY, DATE

FILE NAME = gh\projects\2102155.003\cadd\civil\shts\D160M53-10sht-dr-end.dgn



NOTE: REFER TO DISTRICT 1 DETAILS FOR ADDITIONAL INFORMATION ON "RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)" AND "TYPICAL PAVEMENT MARKINGS."



LEGEND:

- RAISED REFLECTIVE PAVEMENT MARKER
- TOPSOIL, SEEDING, EROSION CONTROL BLANKET
- PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECK

FILE NAME = s:\projects\2102156.003\cadd\sui\sht\D160M53-01sht-PMK.dgn

ENTRAN

USER NAME = 2kujawc	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 20.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
PAVEMENT MARKING, LANDSCAPING, & EROSION CONTROL PLAN

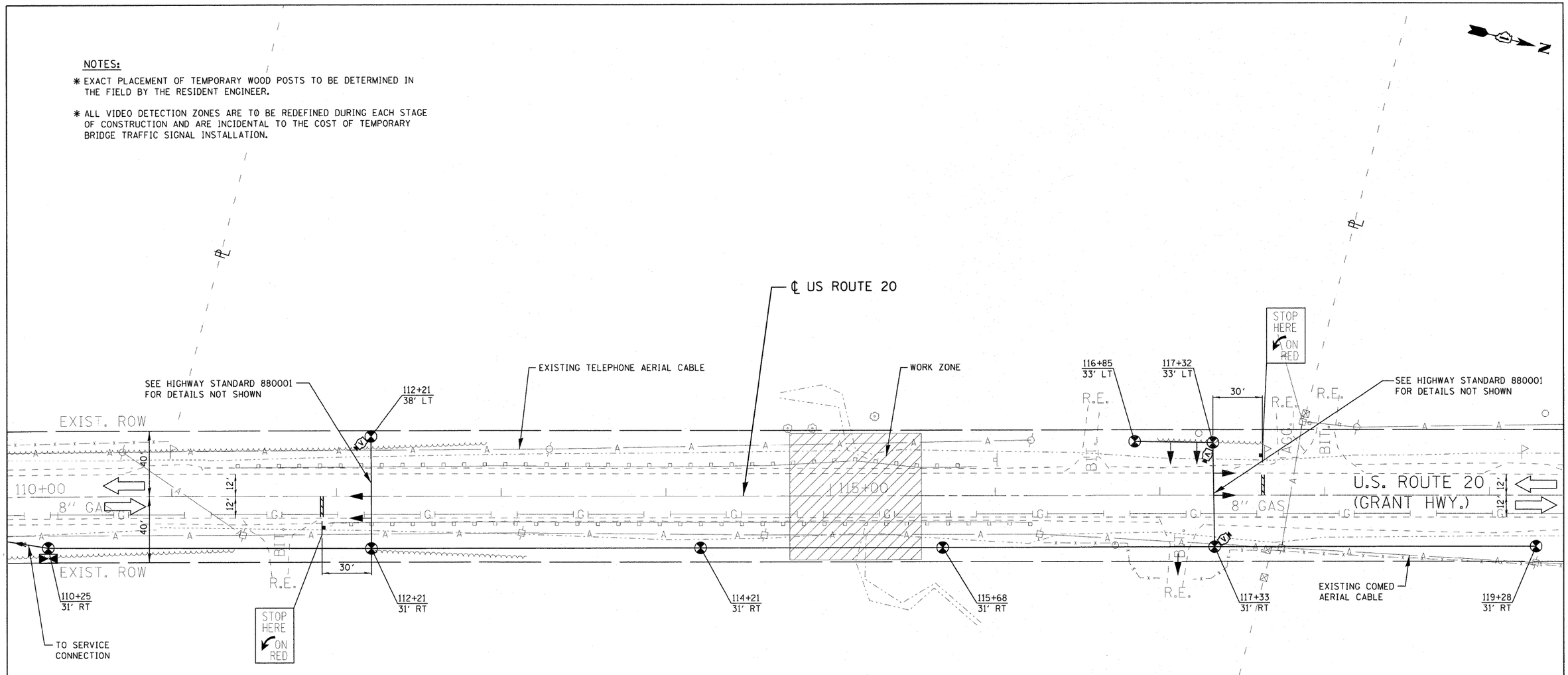
SCALE: 1" = 20' | SHEET NO. OF SHEETS | STA. TO STA.

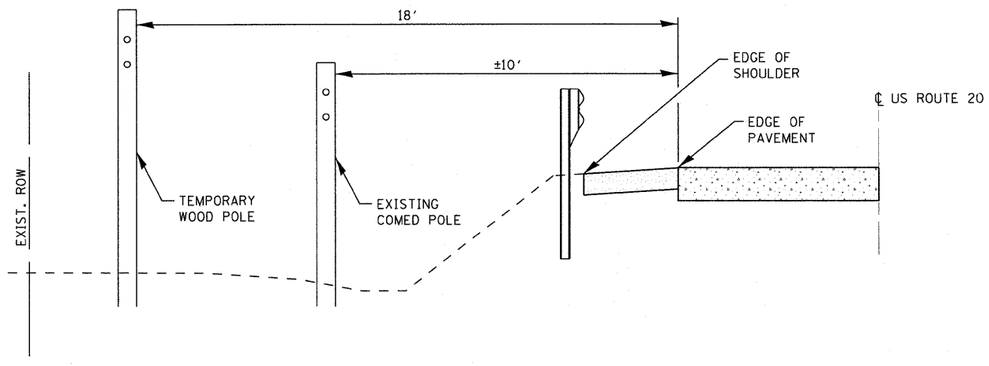
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	12
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

NOTES:

* EXACT PLACEMENT OF TEMPORARY WOOD POSTS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

* ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCIDENTAL TO THE COST OF TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION.





CROSS SECTION DETAIL

NOT TO SCALE
LOOKING SOUTH

THIS SHEET TO BE WORKED WITH DISTRICT 1 DETAIL "TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING" AND THE MAINTENANCE OF TRAFFIC PLANS.

TEMPORARY TRAFFIC SIGNAL LEGEND

- TEMPORARY WOOD POLE NOMINAL 60 FT, CLASS 4
- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED
- TEMPORARY SPAN WIRE TETHER WIRE, AND CABLE
- VIDEO DETECTION CAMERA
- TEMPORARY CONTROLLER CABINET

ENTRAN

USER NAME = 2KUJAWC	DESIGNED - DJB	REVISED -	
	DRAWN - ENTRAN	REVISED -	
PLOT SCALE = 30.00 ' / IN.	CHECKED - TMH	REVISED -	
PLOT DATE = 06/27/2011	DATE - 06/24/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
TEMPORARY TRAFFIC SIGNAL PLAN

SCALE: 1" = 30' | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	13
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FILE NAME = G:\project\2102155.003\CAD0\Cvil\Shts\D160#53-13sns-Signal01.dgn

NOTES FOR TEMPORARY TRAFFIC SIGNALS
****

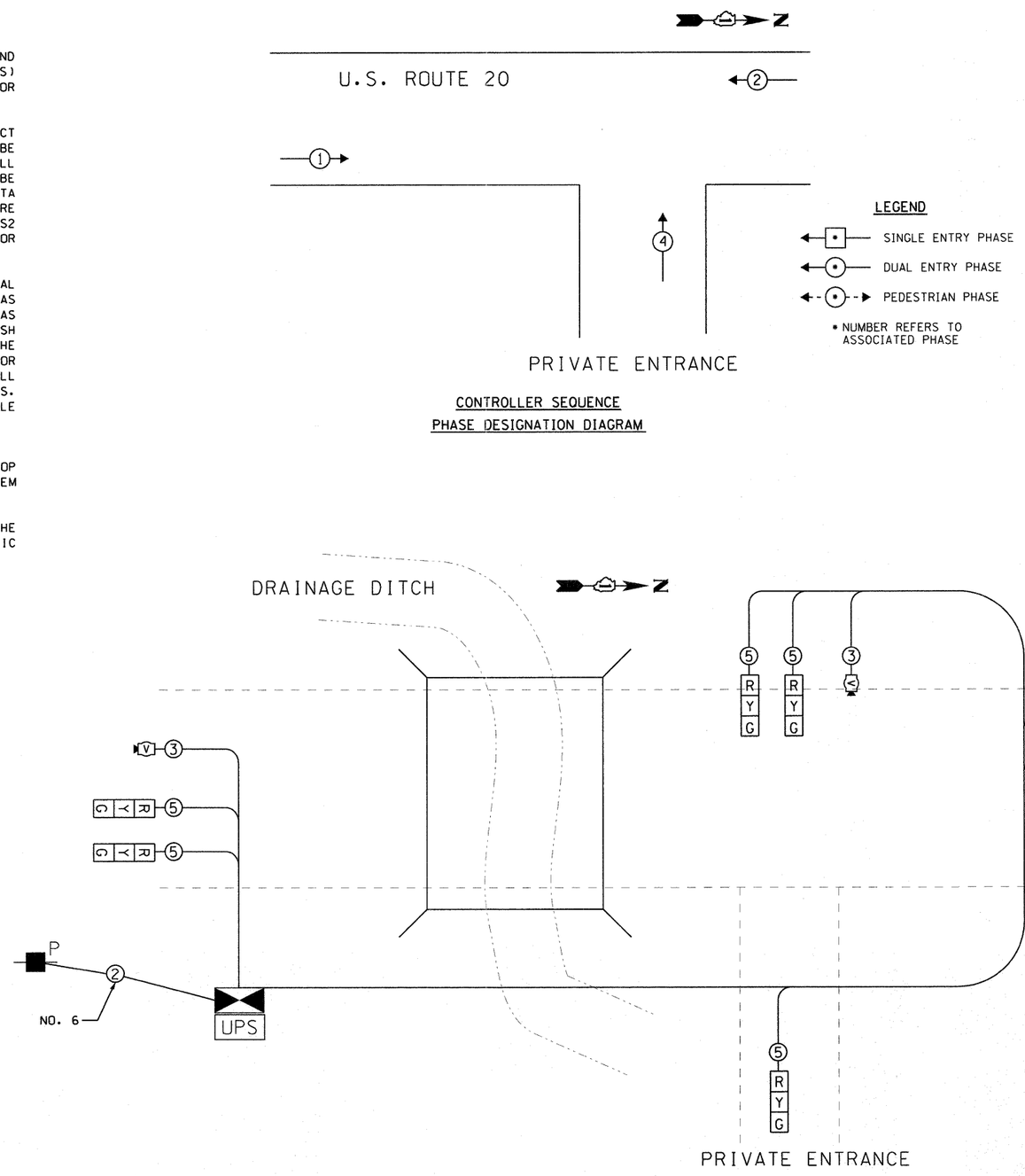
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR. EVP WILL BE PAID FOR SEPARATELY.

2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.

3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300MM). HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.

4. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.

5. 24" WHITE STOP BAR TO BE INSTALLED AFTER THE INSTALLATION AND IMPLEMENTATION OF THE TEMPORARY TRAFFIC SIGNALS.



GENERAL NOTES
****

1. CONTROLLER WITH STEEL BASE CABINET AND BATTERY BACK-UP CABINET SHALL BE MOUNTED ON A WOOD STAND.

2. UN-INTERRUPTABLE POWER SUPPLY (UPS) SHALL BE INCLUDED IN "TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION" PAY ITEM.

3. 9 WOOD POLES INSTALLED BY THE TRAFFIC CONTRACTOR (SEE TEMPORARY TRAFFIC SIGNAL PLANS).

4. PROPOSED WOOD POLES FOR ROADWAY LIGHTING SHALL BE USED TO SUPPORT TRAFFIC SIGNAL CABLE.

5. TRAFFIC SIGNAL AND ROADWAY LIGHTING SHALL UTILIZE A COMBINATION ELECTRIC SERVICE BOX. (SEE DETAIL)

6. ALL SIGNAL HEADS SHALL BE L.E.D.

TEMPORARY CABLE DIAGRAM LEGEND

- CONTROLLER CABINET
- UNINTERRUPTIBLE POWER SUPPLY (UPS)
- SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT
- VIDEO DETECTION CAMERA
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIREUNLESS OTHERWISE NOTED.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	*%OPERATION	
SIGNAL (RED)	7	135	17	0.50	59.5
(YELLOW)	7	135	25	0.25	43.8
(GREEN)	7	135	15	0.25	26.3
ARROW		135	12	0.10	
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN		84		0.05	
FLASHER				0.50	--
				TOTAL =	230.0

ENERGY COSTS TO:
ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: _____
PHONE: (708) 235-2327
COMPANY: COM. ED.

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1,2)	HANDHOLE	6,5 (2,0)	ALL FOUNDATIONS	3,5 (1,0)
D - CONTROLLER	4 (1,2)	DOUBLE HANDHOLE	13 (4,0)	MAST ARM (L) POLE	20'+L-2=
E - M. ARM POLE		SIGNAL POST	2 (1,0)		(6m+L-0.6m)=
24" (600mm)	10 (3,0)	CONTROLLER CAB.	1 (0,5)	BRACKET MOUNTED	13 (4,0)
30" (750mm)	15 (4,6)	FIBER OPTIC	13 (4,0)	PED. PUSHBUTTON	4 (1,2)
		ELECTRIC SERVICE	1 (0,5)	ELECTRIC SERVICE	13,5 (4,1)
		GROUND CABLE	1 (0,5)	SERVICE TO GROUND	13,5 (4,1)
				POST MOUNTED	6 (1,8)

THIS SHEET TO BE WORKED WITH DISTRICT 1 DETAIL "TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING" AND THE MAINTENANCE OF TRAFFIC PLANS.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	UNIT	QUANTITY
X8900005	TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION	EACH	1

ENTRAN

USER NAME = 2kujawc	DESIGNED - DJB		REVISED -
	DRAWN - ENTRAN		REVISED -
PLOT SCALE = 50.00 ' / IN.	CHECKED - TMH		REVISED -
PLOT DATE = 06/20/2011	DATE - 06/24/11		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM

SCALE: 1" = 50' | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	14
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FILE NAME = c:\projects\2012155.003\eadd\civil\sht\D160M53-13sht-Signal02.dgn

Benchmark: Top of Right of Way marker at approximate Sta. 117+65, offset 33' right of ℄ U.S. Route 20. Elev. 880.33.

Existing Structure: S.N. 056-0088 originally constructed in 1922 as Route 5 Section 11 is a reinforced concrete single box culvert with a 5' span and 4' rise. The culvert length is 39'-4" from headwall to headwall. No skew. One lane of traffic to be maintained utilizing stage construction.

No salvage.

INDEX OF SHEETS

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges 17th Edition

LOADING HS 20-44 & ALT.

DESIGN STRESSES

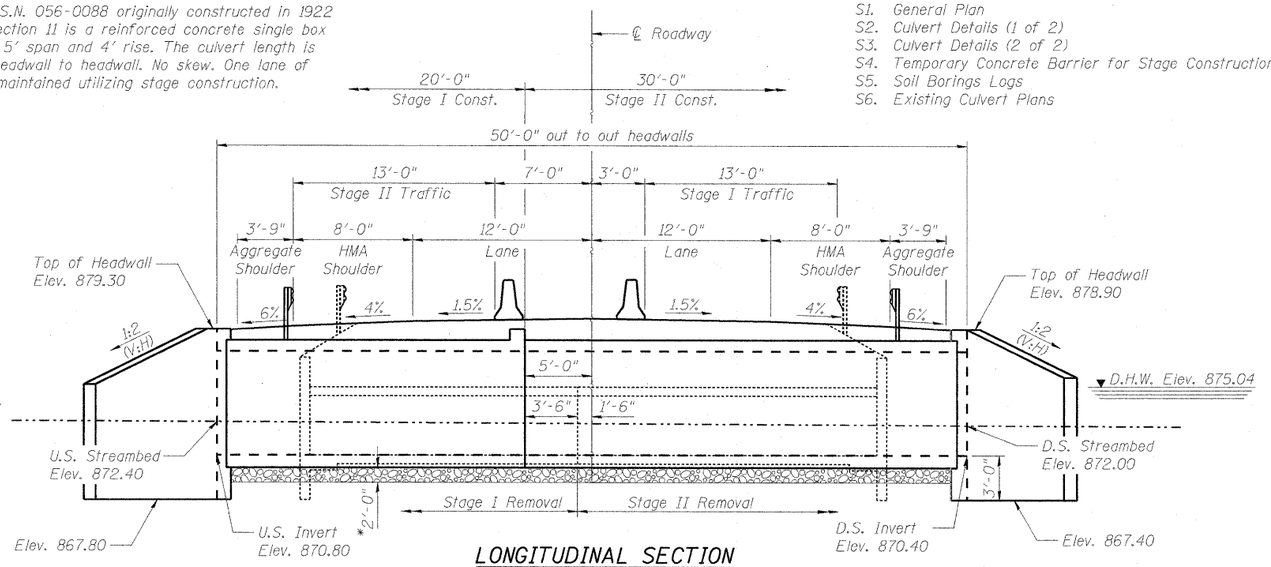
FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

STATION 115+15.54
BUILT 2011 BY
STATE OF ILLINOIS
F.A.P. RT. 525 SEC. 11-T-1
LOADING HS20 & ALT.
STR. NO. 056-0276

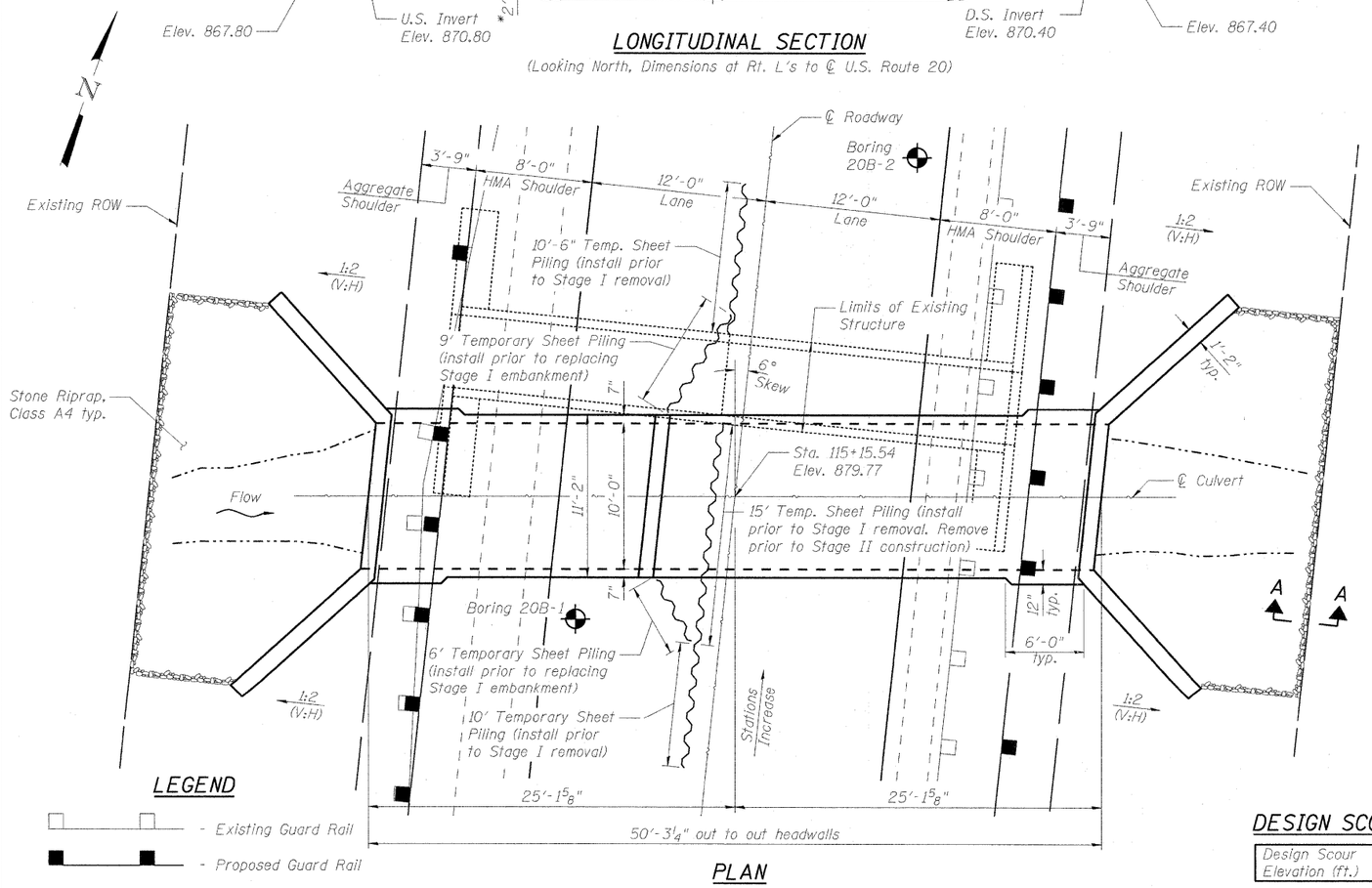
NAME PLATE

See Highway Standard 515001-03 for dimensions and placement



LONGITUDINAL SECTION

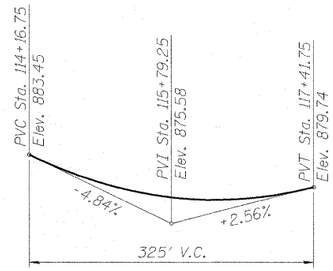
(Looking North, Dimensions at Rt. L's to ℄ U.S. Route 20)



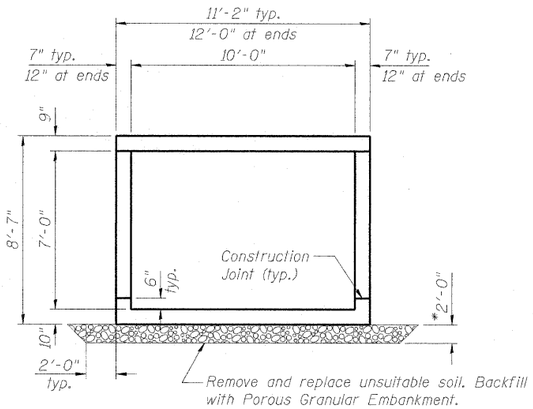
PLAN

LEGEND

- Existing Guard Rail
- Proposed Guard Rail



PROFILE GRADE

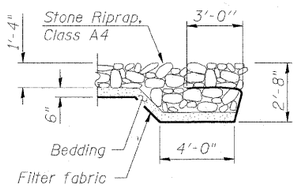


SECTION THROUGH CULVERT

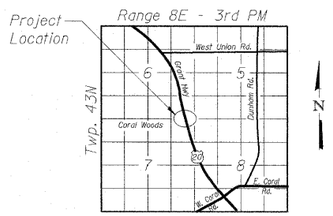
* Increase the removal and replacement of unsuitable soil to 3'-0" if the precast alternative is selected. The quantity of Removal and Disposal of Unsuitable Material and Porous Granular Embankment should be adjusted accordingly.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QTY.
Removal and Disposal of Unsuitable Material	Cu Yd	76
Porous Granular Embankment	Cu Yd	76
Stone Riprap, Class A4	Sq Yd	74
Removal of Existing Structures	Each	1
Reinforcement Bars	Pound	14,420
Name Plates	Each	1
Concrete Box Culverts	Cu Yd	72.9
Temporary Sheet Piling	Sq Ft	1275



SECTION A-A



LOCATION SKETCH

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	Upstream	Downstream
	867.80	867.40

WATERWAY INFORMATION

Drainage Area = 0.147 sq. mi. Low Grade Elev. 878.00 @ Sta. 116+40

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Opening Sq. Ft. Prop.	Nat. H.W.E.	Head - Ft. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
	10	77	20.0	33.3	874.67	0.82	0.00	875.49	874.67
Design	50	124	20.0	38.2	875.04	2.64	0.00	877.68	875.04
Base	100	143	20.0	40.4	875.13	2.82	0.00	877.95	875.13
Overtopping	>500								
Max. Calc.	500	191	20.0	45.7	875.33	2.81	0.04	878.14	875.37

FILE NAME = g:\projects\21202195.003\cadd\structure\B5602276_60M53_001_GPE.dgn

ENTRAN

USER NAME = 2sayerb	DESIGNED - TWO	REVISED -	
	CHECKED - BHS	REVISED -	
PLOT SCALE = N/A	DRAWN - TWO	REVISED -	
PLOT DATE = 7/6/2011	CHECKED - BHS	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
SHEET NO. S1 OF 6 SHEETS

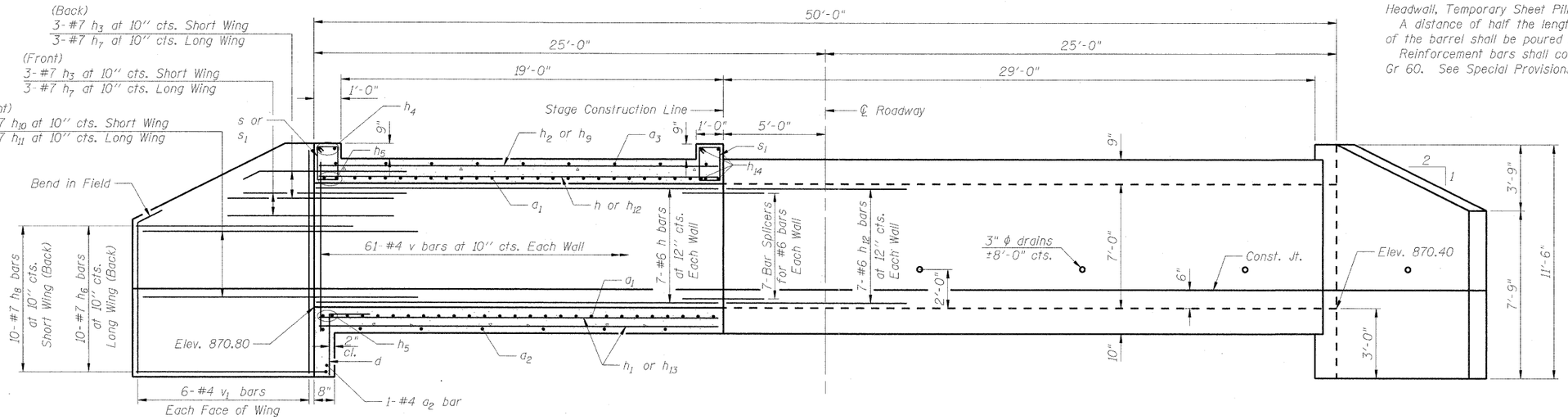
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	15
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

NOTES

See Sheet S3 for Section B-B, Section Thru Barrel, Section Thru Headwall, Temporary Sheet Piling, bar diagrams and Bill of Material.

A distance of half the length of the wingwall but not less than six feet of the barrel shall be poured monolithically with the wingwalls.

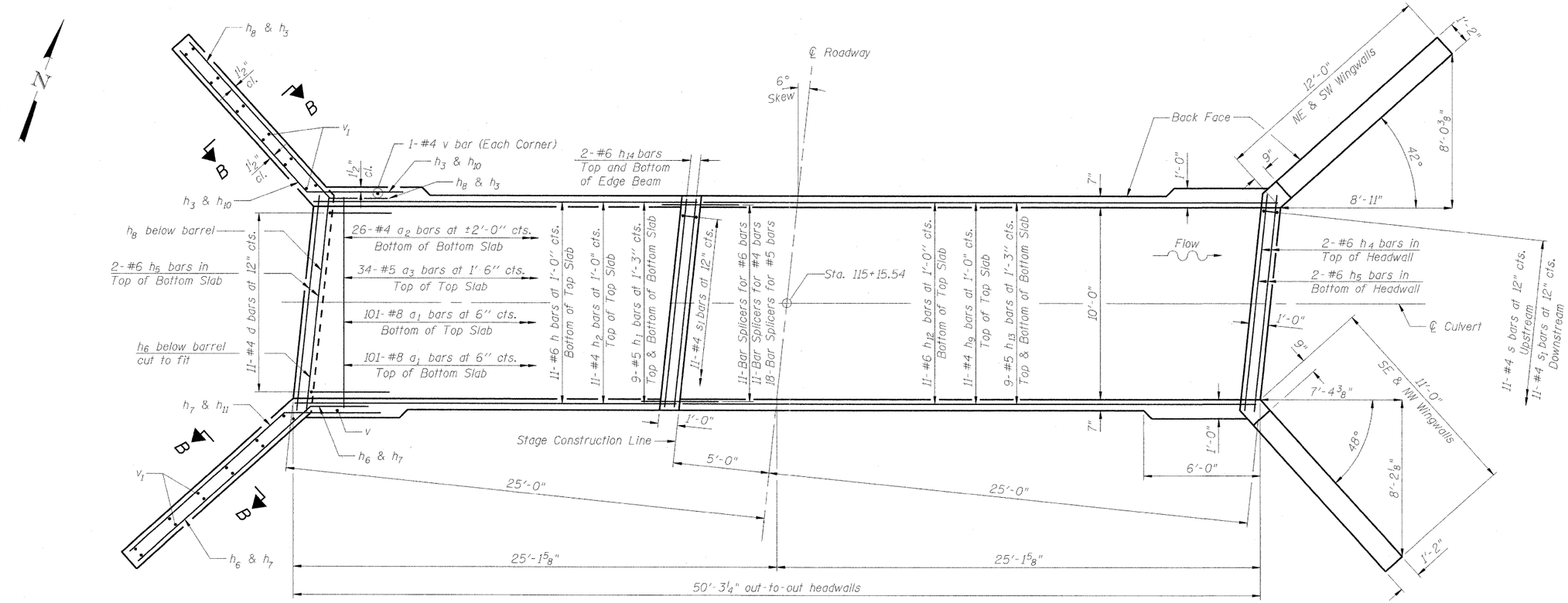
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.



HALF LONG. SECTION

HALF ELEVATION

Dimensions at Rt. L's to ℄ Roadway

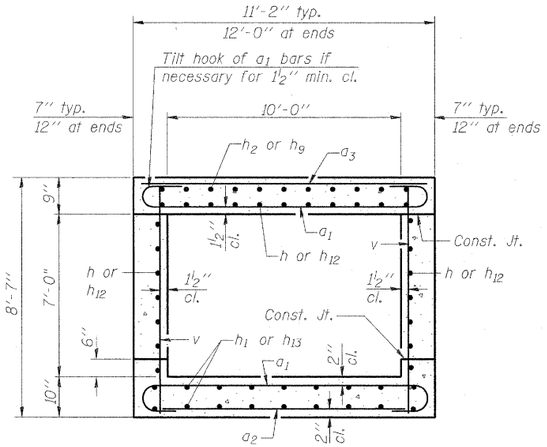


PLAN

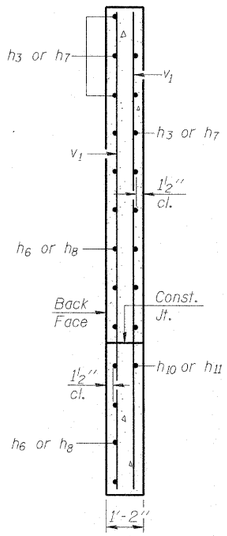
ENTRAN	USER NAME = 2sayerb	DESIGNED - TWO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CULVERT DETAILS (1 OF 2) S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
		CHECKED - BHS	REVISED -		
	PLOT SCALE = N/A	DRAWN - TWO	REVISED -		SHEET NO. S2 OF 6 SHEETS
	PLOT DATE = 7/6/2011	CHECKED - BHS	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	16
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

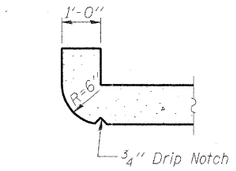
FILE NAME = g:\projects\2102155.003\cadd\structure\05602276_60M53_002_Culvert Details 1.dgn



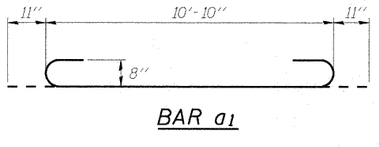
SECTION THRU BARREL



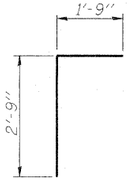
SECTION B-B



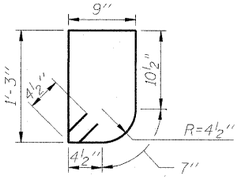
SECTION THRU HEADWALL
(Upstream End Only)



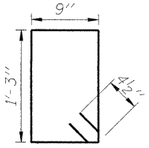
BAR a_1



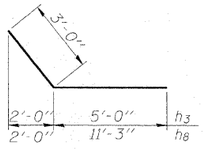
BAR d



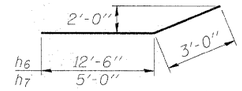
BAR s
(Upstream)



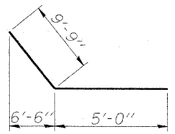
BAR s_1
(Downstream)



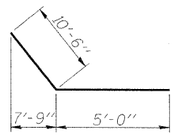
BARS h_3 & h_8



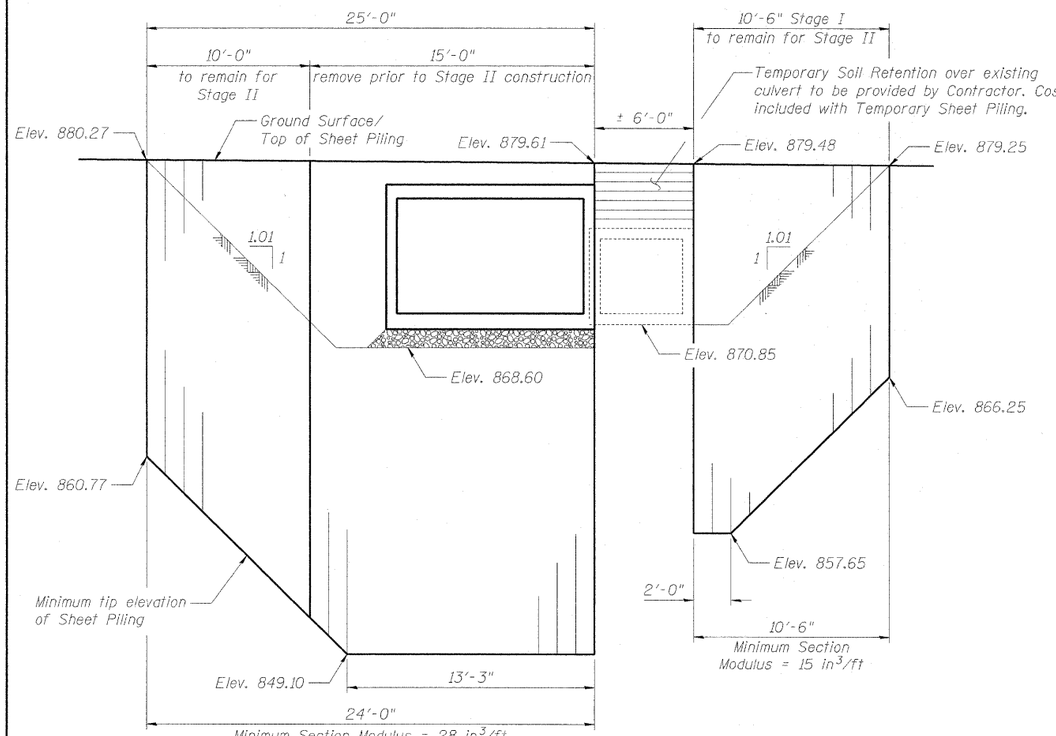
BARS h_6 & h_7



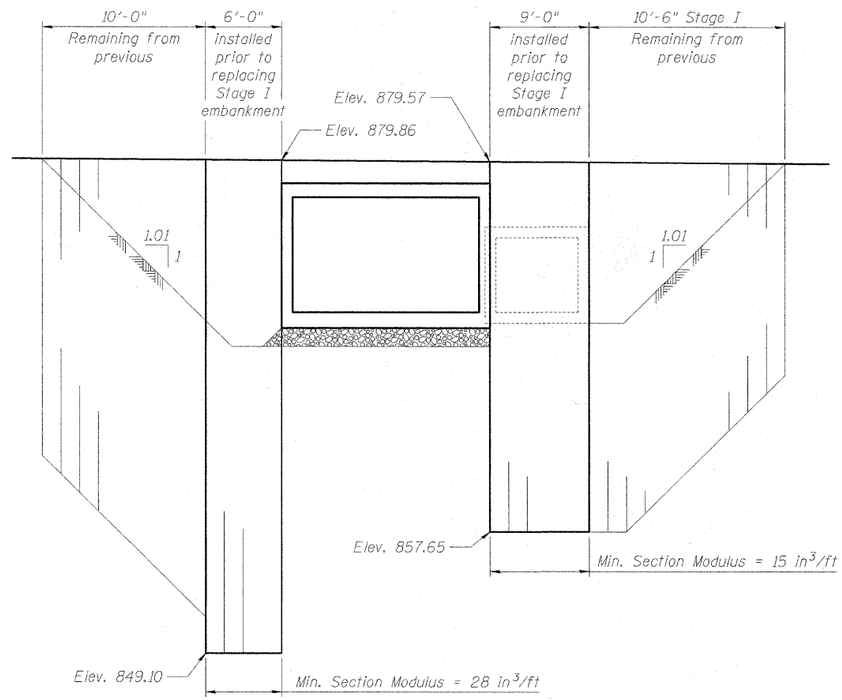
BARS h_{10}



BARS h_{11}



INSTALL PRIOR TO STAGE I EXCAVATION



INSTALL PRIOR TO REPLACING STAGE I EMBANKMENT

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a_1	202	#8	12'-8"	
a_2	28	#4	10'-10"	
a_3	34	#5	10'-10"	
d	22	#4	4'-6"	
h	25	#6	19'-9"	
h_1	18	#5	19'-9"	
h_2	11	#4	19'-9"	
h_3	12	#7	8'-0"	
h_4	4	#6	11'-4"	
h_5	8	#6	11'-9"	
h_6	20	#7	15'-6"	
h_7	12	#7	8'-0"	
h_8	20	#7	14'-3"	
h_9	11	#4	29'-10"	
h_{10}	12	#7	14'-9"	
h_{11}	12	#7	15'-6"	
h_{12}	25	#6	29'-10"	
h_{13}	18	#5	29'-10"	
h_{14}	4	#6	10'-10"	
s	11	#4	4'-7"	
s_1	22	#4	4'-9"	
v	126	#4	8'-3"	
v_1	48	#4	11'-2"	
Concrete Box Culverts			Cu. Yd.	72.9
Reinforcement Bars			Pound	14,420

SHEET PILE ELEVATION

See Sheet S1 for Plan layout of Sheet Piling

FILE NAME = gt\project\2102155.003\cadd\structure\056B276.60M53.003_Culvert Details 2.dgn

ENTRAN

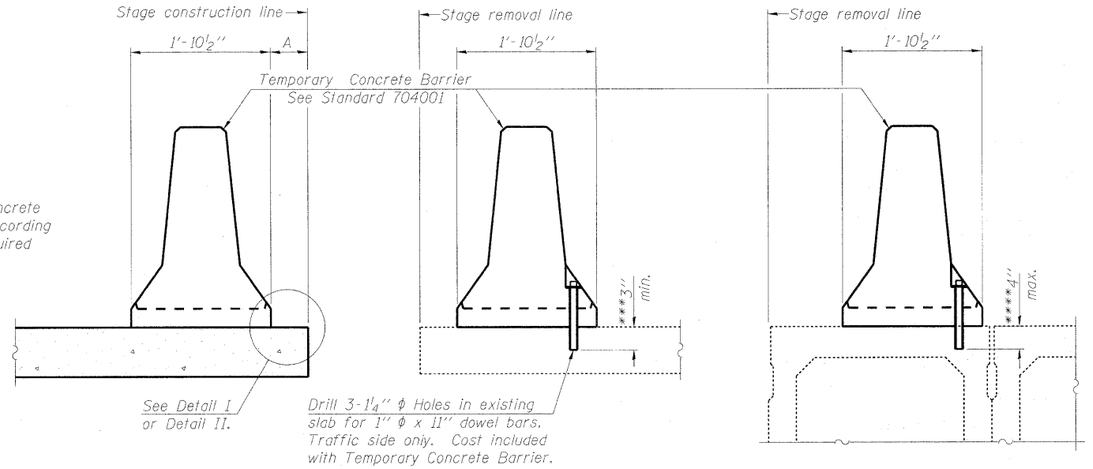
USER NAME = 2eayerb	DESIGNED - TWO	REVISED -	
	CHECKED - BHS	REVISED -	
PLOT SCALE = N/A	DRAWN - TWO	REVISED -	
PLOT DATE = 7/6/2011	CHECKED - BHS	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS (2 OF 2)
S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
SHEET NO. S3 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	17
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel ℙ to the top layer of couplers with 2-⅝" ϕ bolts screwed to coupler at approximate ℄ of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel ℙ to the concrete slab or concrete wearing surface with 2-⅝" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate ℄ of each barrier panel.

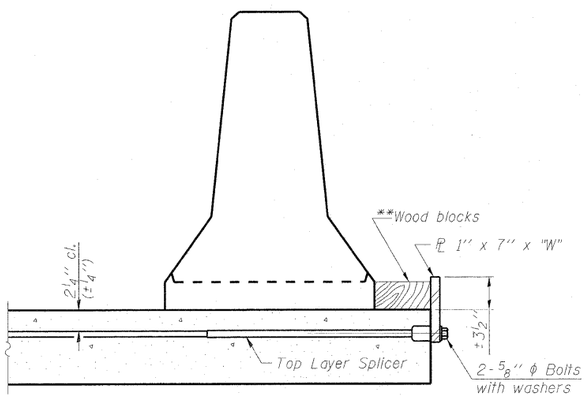
Cost of anchorage is included with Temporary Concrete Barrier.

The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

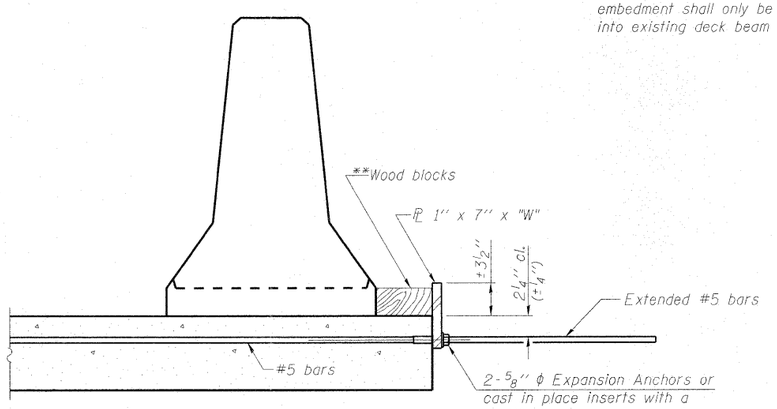
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



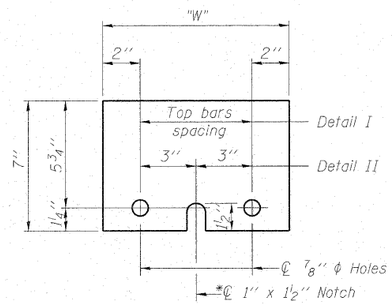
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER ℙ 1" x 7" x "W"

* Required only with Detail II

R-27

7-1-10

ENTRAN	USER NAME = 2sayerb	DESIGNED - TWO	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
		CHECKED - BHS	REVISED -		SHEET NO. 54 OF 6 SHEETS
	PLOT SCALE = N/A	DRAWN - TWO	REVISED -		
	PLOT DATE = 6/28/2011	CHECKED - BHS	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	18
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FILE NAME = gi\project\2102155.003\cadd\structure\X2502276_60M53_004_Temp Barrier.dgn

Wang Engineering, Inc.
Consulting Geotechnical and Environmental Engineers
wangeng@wangeng.com
1145 Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

BORING LOG 20B-1

WEI Job No.: 555-13-11

Client: **IDOT District One**
Project: **US Route 20 over Drainage Ditch**
Location: **T-43N, R-6E, Sect 7, McHenry County**

Datum: NGVD
Elevation: 879.44 ft
North: 2024714.55 ft
East: 920055.08 ft
Station: 115+06
Offset: 10LT

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	878.5	5-inch thick ASPHALT over 6-inch CONCRETE --PAVEMENT--							
		Very stiff to stiff, brown SANDY CLAY LOAM, trace gravel --FILL--				1	4 4 3	2.00 P	11
			5			2	2 2 2	1.00 P	16
	873.9	Medium stiff, black CLAY LOAM, trace organic matter				3	1 1 2	0.50 P	28
	872.1	Very soft, brown SANDY CLAY LOAM				4	1 2 1	<0.25 P	18
	869.2	Loose, brown SANDY LOAM	10			5	1 2 4	NP	15
	866.9	Hard, brown SILTY CLAY, trace gravel				6	3 5 7	4.26 B	11
	864.2	Medium dense, brown SILT	15			7	6 12 13	NP	15
	861.4	Very stiff to hard, brown SILTY CLAY, trace gravel				8	5 7 9	4.18 B	11
			20			9	5 8 9	3.94 B	11
			25			10	5 8 10	3.85 B	11
	842.9	Very stiff, gray CLAY LOAM	30			11	4 6 8	3.03 B	12
						12	6 9 9	3.53 B	9
			35			13	9 12 12	4.76 B	10
	839.4	Boring terminated at 40.00 ft	40			14	4 7 9	2.95 B	11
			45						
			50						

GENERAL NOTES

Begin Drilling: **11-25-2009** Complete Drilling: **11-25-2009**
Drilling Contractor: **WTS** Drill Rig: **Mobile B-57 TMR**
Driller: **K&J** Logger: **F.Bozga** Checked by: **N. Davis**
Drilling Method: **3.25 IDA HSA; 140# Mobile Automatic Hammer**

WATER LEVEL DATA

While Drilling: **9.00 ft**
At Completion of Drilling: **21.00 ft**
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

Wang Engineering, Inc.
Consulting Geotechnical and Environmental Engineers
wangeng@wangeng.com
1145 Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

BORING LOG 20B-2

WEI Job No.: 555-13-11

Client: **IDOT District One**
Project: **US Route 20 over Drainage Ditch**
Location: **T-43N, R-6E, Sect 7, McHenry County**

Datum: NGVD
Elevation: 878.84 ft
North: 2024748.09 ft
East: 920062.32 ft
Station: 115+40
Offset: 10LT

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	878.5	4.5-inch thick ASPHALT over 7.5-inch thick CONCRETE --PAVEMENT--							
		Loose, brown SANDY LOAM, trace gravel				1	5 4 4	NP	9
	876.1	--BASE COURSE-- Loose, brown SAND, trace gravel --FILL--	5			2	2 2 2	NP	13
	873.3	Soft, brown SANDY CLAY LOAM				3	2 2 4	0.25 P	16
	871.3	Loose, brown GRAVELLY SAND	10			4	3 3 3	NP	13
	867.8	Medium dense, brown SAND, trace gravel				5	5 6 5	2.25 P	15
	866.8	Stiff to hard, brown SILTY CLAY, trace gravel	15			6	2 4 5	2.25 P	13
						7	4 17 9	4.35 B	12
			20			8	3 6 8	3.53 B	12
			25			9	5 7 9	3.77 B	11
						10	4 5 9	3.61 B	11
	842.3	Stiff, gray CLAY LOAM	30			11	3 6 9	3.28 B	10
						12	6 8 11	3.20 B	11
			35			13	5 7 10	3.20 B	11
	838.8	Boring terminated at 40.00 ft	40			14	4 6 10	1.48 B	11
			45						
			50						

GENERAL NOTES

Begin Drilling: **11-25-2009** Complete Drilling: **11-25-2009**
Drilling Contractor: **WTS** Drill Rig: **Mobile B-57 TMR**
Driller: **K&J** Logger: **F.Bozga** Checked by: **N. Davis**
Drilling Method: **3.25 IDA HSA; 140# Mobile Automatic Hammer**

WATER LEVEL DATA

While Drilling: **8.50 ft**
At Completion of Drilling: **10.00 ft**
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

ENTRAN

USER NAME = 2sayerb	DESIGNED - TWO	REVISED -	
	CHECKED - BHS	REVISED -	
PLOT SCALE = N/A	DRAWN - TWO	REVISED -	
PLOT DATE = 6/28/2011	CHECKED - BHS	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

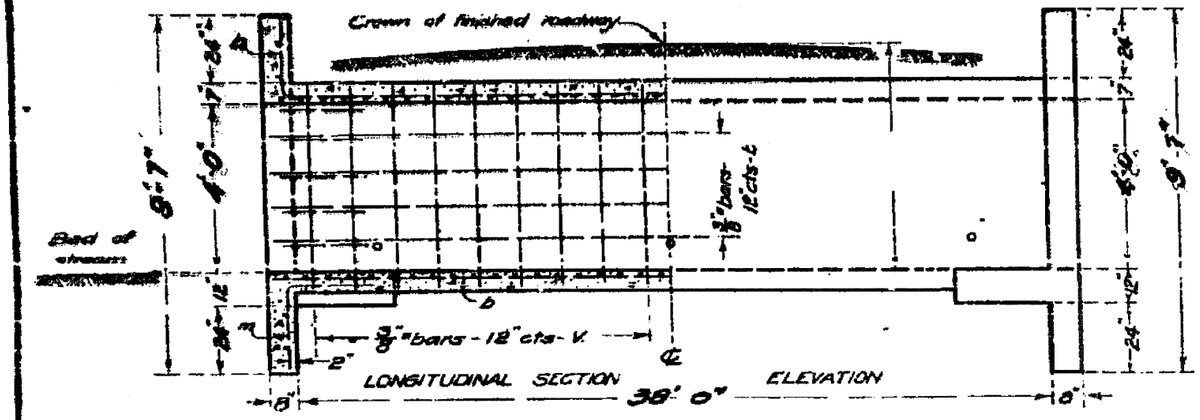
SOIL BORING LOGS
S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
SHEET NO. 55 OF 6 SHEETS

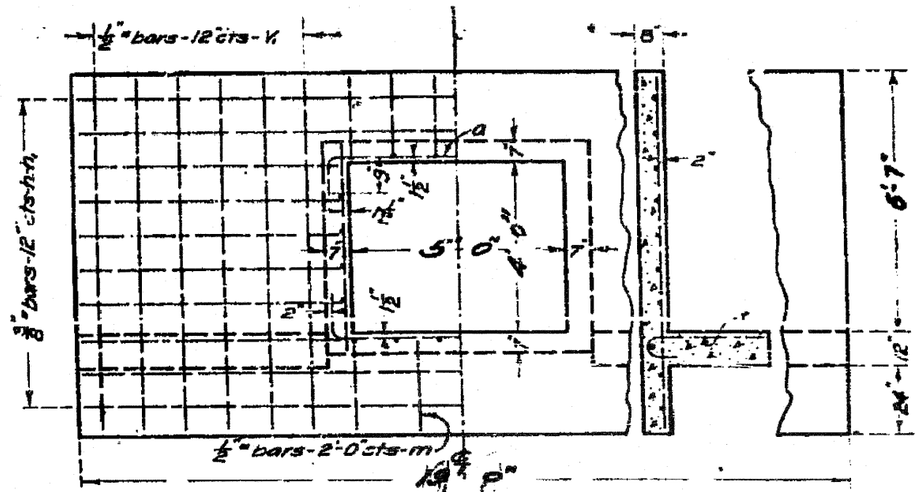
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	19
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
STATE HIGHWAY DEPARTMENT

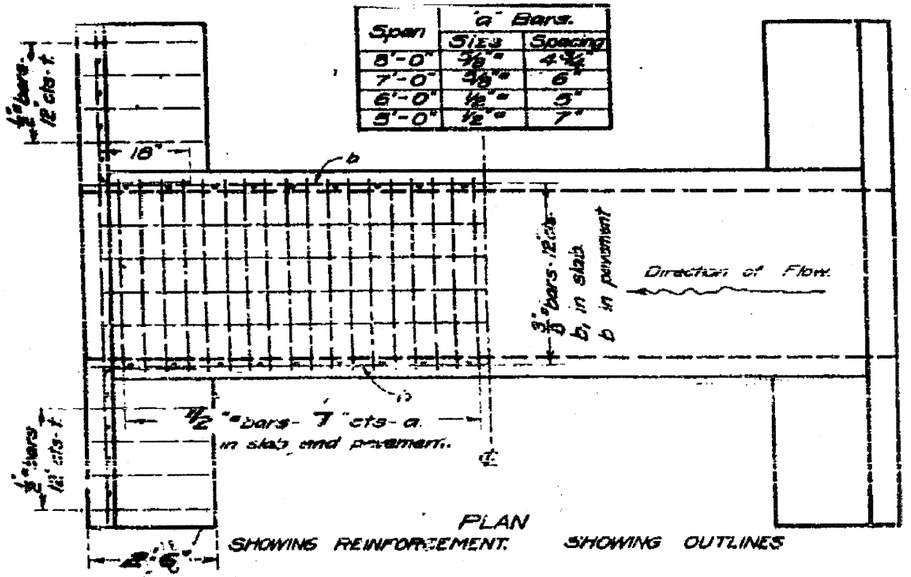
REINFORCED CONCRETE BOX CULVERT

BOND ISSUE ROUTE NO.	COUNTY	SEC.	TOTAL SHEETS	SHEET NO.
5	McHENRY	11	34	19





Span	"a" Bars. Size	Spacing
5'-0"	3/8"	4 3/4"
7'-0"	3/8"	6"
6'-0"	1/2"	5"
5'-0"	1/2"	7"



Note - Use "m" bars in downstream headwall only.
Box is designed for no fill.
Maximum clearance = 7'-6"

BILL OF MATERIAL

Bars	No.	Size	Length
V	36	3/8"	5'-0"
V,	24	1/2"	9'-0"
h	8	3/8"	18'-6"
h,	20	3/8"	8'-0"
a	132	1/2"	7'-6"
b	36	3/4"	14'-0"
b,	12	3/4"	15'-6"
t	24	1/2"	3'-0"
m	3	1/2"	5'-0"
Steel - Lbs.			1775
Concrete - Cu. Yds.			265

Class A concrete to be used throughout
Proportions 1 - 2½ - 4.

Route 5 Sec 11
Sta 80+37
McHenry Co.

STAMPED 2/28/11

Passed Frank J. Kotis
Engr. of Design

619

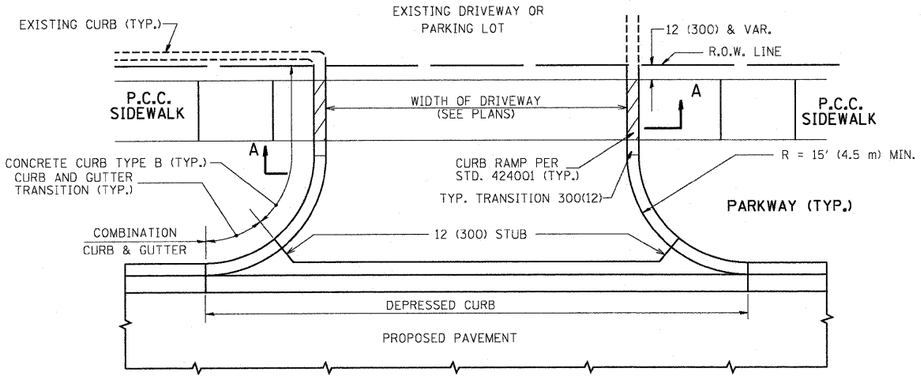
ENTRAN	USER NAME = 2sayerb	DESIGNED - TWO	REVISED -
		CHECKED - BHS	REVISED -
	PLOT SCALE = N/A	DRAWN - TWO	REVISED -
	PLOT DATE = 6/28/2011	CHECKED - BHS	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

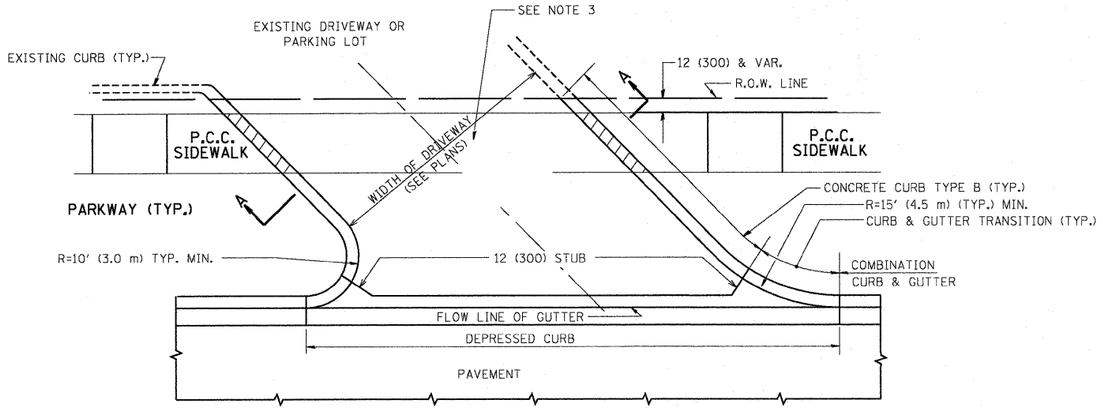
EXISTING CULVERT PLANS
S.N. 056-0279 – U.S. ROUTE 20 OVER DRAINAGE DITCH
SHEET NO. 56 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	20
		CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

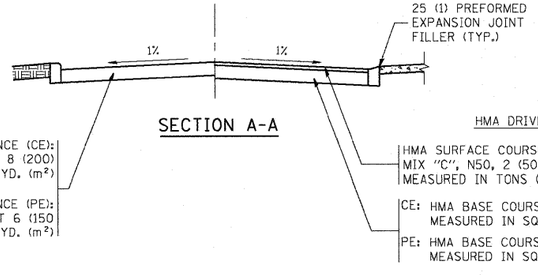
FILE NAME = g:\projects\21201155.003\caddd\structure\056D276.60M53.006.Exist Culvert.dgn

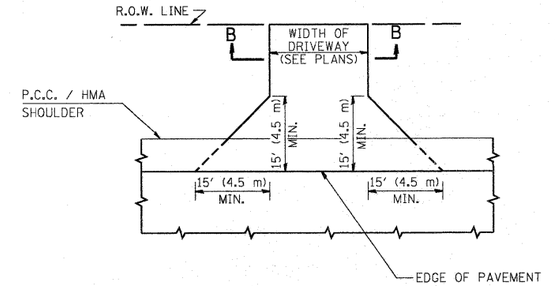


WITH CONCRETE CURB, TYPE B

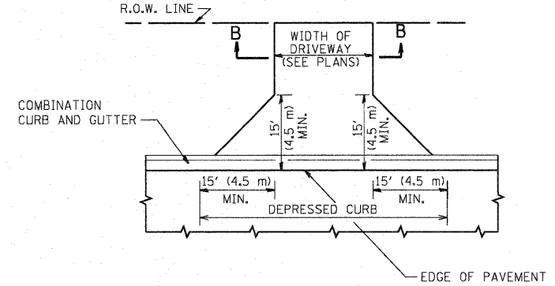


WITH CONCRETE CURB, TYPE B

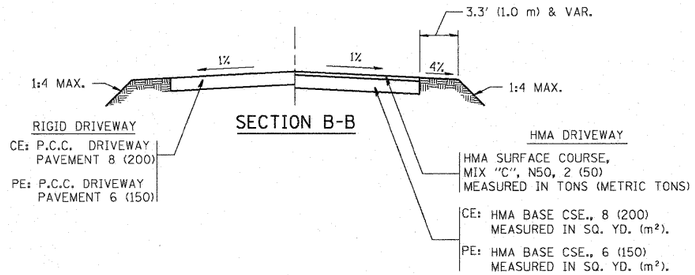




ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

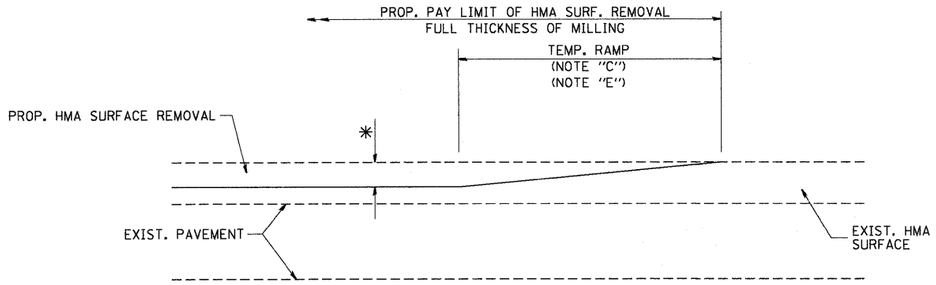
FILE NAME = c:\projects\diststd22x34\bd01.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
		DRAWN -	REVISED - P. LaFLUER 04-15-03
	PLOT SCALE = 49.9999 ' / IN.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 6/12/2008	DATE - 11-04-95	REVISED - R. BORO 06-11-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

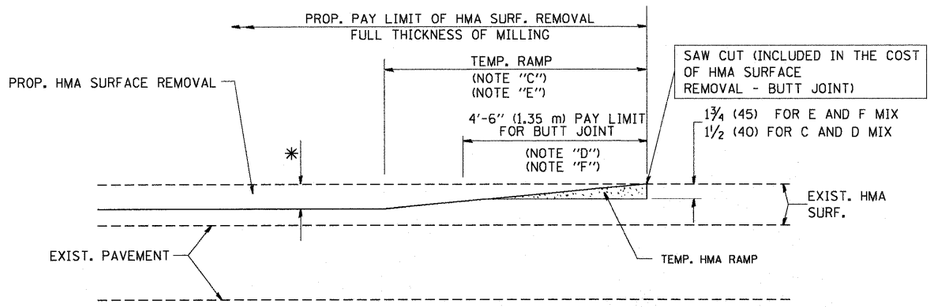
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	21
	BD0156-07 (BD-01)	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

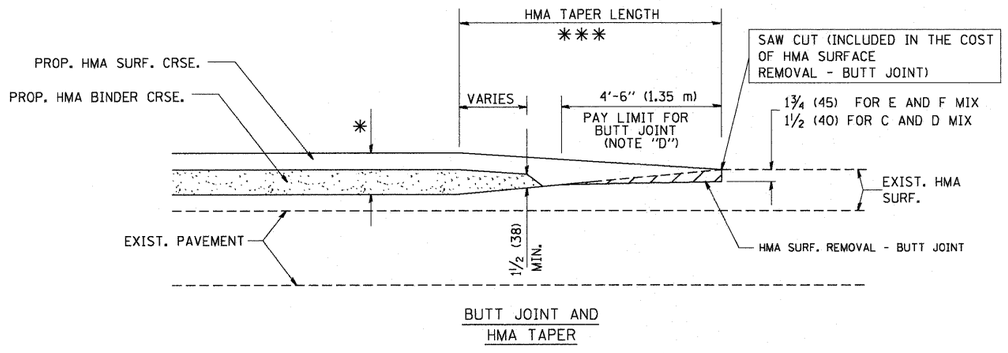


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

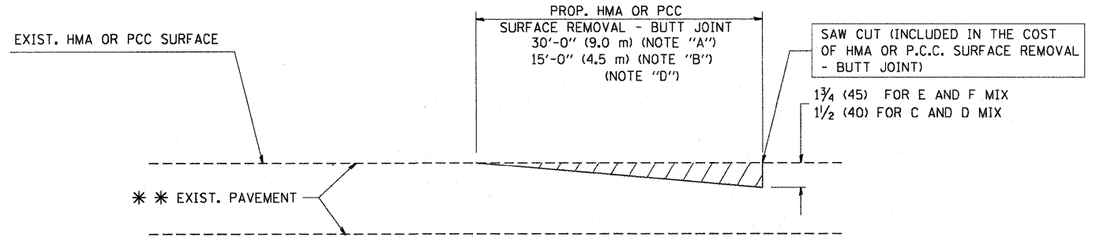
OPTION 2

TYPICAL TEMPORARY RAMP

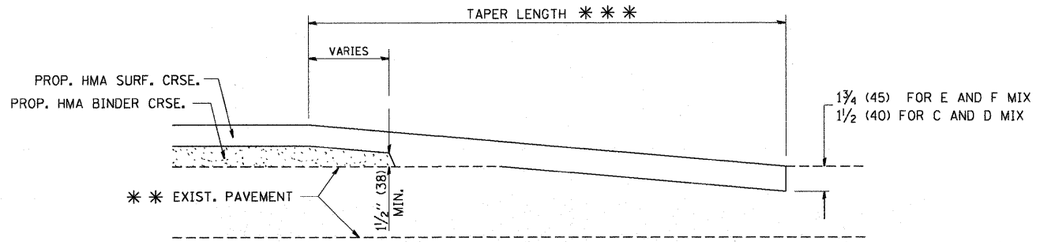


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

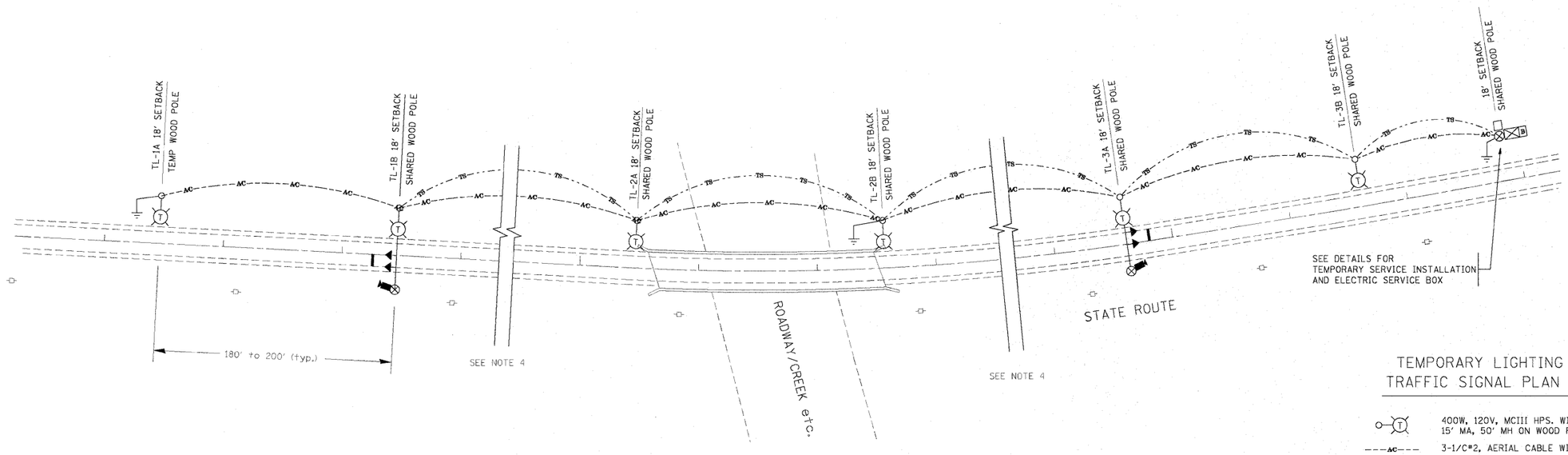
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	22
	BD400-05 BD32	CONTRACT NO.60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL LAYOUT FOR TEMPORARY LIGHTING AND TRAFFIC SIGNALS

NOT TO SCALE

TEMPORARY LIGHTING AND TRAFFIC SIGNAL PLAN LEGEND

T	400W, 120V, MCIII HPS. WITH PHOTO CELL 15' MA, 50' MH ON WOOD POLE, CLASS 4
---AC---	3-1/C#2, AERIAL CABLE WITH MESSENGER WIRE UNLESS OTHERWISE NOTED
TL-1A	TEMPORARY LIGHTING UNIT NUMBER - ONE CIRCUIT A
	GROUND ROD 5/8" DIA. x 10'
	COMBINATION LIGHTING AND TRAFFIC POLE MOUNTED ELECTRICAL SERVICE BOX
	TEMPORARY WOOD POLE - NOMINAL 60 FT., CLASS 4
	TEMPORARY LED TRAFFIC SIGNAL HEAD, NUMBER OF SECTION AND DISPLAY AS REQUIRED.
---TS---	TEMPORARY TRAFFIC SIGNAL SPAN WIRE, NUMBER OF CONDUCTORS AS REQUIRED.
TB	TEMPORARY TRAFFIC CONTROLLER WITH UPS AND BOTTOM PLATE MOUNTED TO WOOD POLE
	TEMPORARY VIDEO DETECTOR

GENERAL NOTES:

1. CONTACT TO THE ELECTRIC UTILITY SHALL BE INITIATED BEFORE THE PRECONSTRUCTION MEETING, AND DOCUMENTATION OF CONTACT SHALL BE PRESENTED AT THAT MEETING. NO PLACEMENT OF POLES WILL BE ALLOWED WITHOUT EVIDENSE OF A SIGNED AGREEMENT WITH THE ELECTRIC UTILITY, FURNISHED TO THE ENGINEER.

2. UNLESS OTHERWISE INDICATED, AND EXCEPT AS OTHERWISE NOTED, THIS STANDARDIZED LAYOUT SHALL APPLY FOR BRIDGES NOT EXCEEDING A 250-FOOT SPAN. FOR BRIDGE SPANS IN EXCESS OF 250 FEET, THE POLES IMMEDIATELY ADJACENT TO THE BRIDGE SHALL BE 100-FOOT POLES (90-FOOT MOUNTING HEIGHT), WITH 750-WATT TYPE III HIGH PRESSURE SODIUM HIGH-MAST LUMINAIRES AS APPROVED BY THE ENGINEER.

3. THE LAYOUT OF THE TEMPORARY EQUIPMENT WILL VARY BASED ON FIELD CONDITIONS, STAGING, UTILITY IMPACTS, AND THE ELECTRIC SERVICE LOCATION AS COORDINATED WITH THE ELECTRIC UTILITY. THE CONTRACTOR SHALL SUBMIT A PLAN INDICATING THE SETTING OF POLES, TRAFFIC SIGNALS, AND COMBINED SERVICE. THIS PLAN MUST BE APPROVED BY THE ENGINEER BEFORE ANY POLES ARE PLACED

4. THE ELECTRIC SERVICE SHALL BE 240/120V. WHERE 240V SERVICE IS NOT AVAILABLE, THE CONTRACTOR MAY SUBMIT A PROPOSAL FOR 120V SERVICE. DROP CABLE, MAIN BREAKER, AND ALL OTHER SERVICE APPURTENANCES SHALL BE APPROPRIATELY RATED AND INCLUDED REGARDLESS OF THE SERVICE VOLTAGE APPLIED

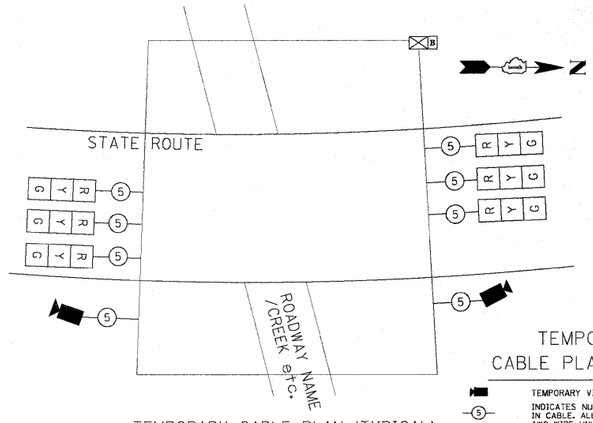
5. THE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATION SHALL SHARE ANY COMMON ELEMENTS SUCH AS WOOD POLES, ELECTRICAL SERVICE, ELECTRIC SERVICE BOX, CABLE, ETC. THE CONTRACTOR SHALL COORDINATE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATIONS.

6. THE LIGHT POLE SETBACK FROM THE EDGE OF TRAVEL PAVEMENT SHALL BE 18 FT. UNLESS THE LIGHT POLE IS BEHIND GUARDRAIL. THE LIGHT POLES INSTALLED BEHIND THE GUARDRAIL OR BARRIER WALL SHOULD HAVE AT LEAST 8 FT. SETBACK FROM THE BACK OF THE SHOULDER AND OR AS DIRECTED BY THE ENGINEER.

7. EACH LIGHTING UNIT SHALL BE CONTROLLED BY A PHOTO CELL MOUNTED ON EACH LUMINAIRE WITH THE LIGHTING CIRCUIT FED FROM THE TEMPORARY SERVICE DISCONNECT BOX. OTHER MEANS OF LUMINAIRE CONTROL CAN BE CONSIDERED IF APPROVED BY THE ENGINEER.

8. THE CONTRACTOR SHALL SPLICE AERIAL CABLE AT THE LIGHT POLE USING HEAT SHRINKABLE CAPS WITH THE FACTORY APPLIED WATERPROOF SEALENT OR AN APPROVED UL LISTED AERIAL TAP DEVICE.

9. ALL AREAS DISTURBED UNDER THIS CONTRACT SHALL BE RESTORED TO THE ORIGINAL CONDITION OR BETTER, TO THE SATISFACTION OF THE ENGINEER.



TEMPORARY CABLE PLAN (TYPICAL)

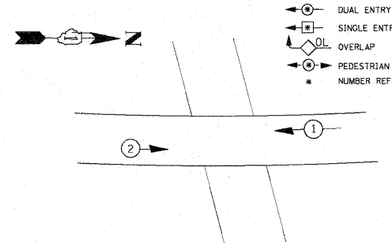
NOT TO SCALE

TEMPORARY CABLE PLAN LEGEND

	TEMPORARY VIDEO DETECTOR
5	INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
R	TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION, 12" (300 mm)

TEMPORARY PHASE DESIGNATION DIAGRAM LEGEND

	DUAL ENTRY PHASE
	SINGLE ENTRY PHASE
OL	OVERLAP
	PEDESTRIAN PHASE
*	NUMBER REFERS TO ASSOCIATED PHASE



TEMPORARY PHASE DESIGNATION DIAGRAM (TYPICAL)

NOT TO SCALE

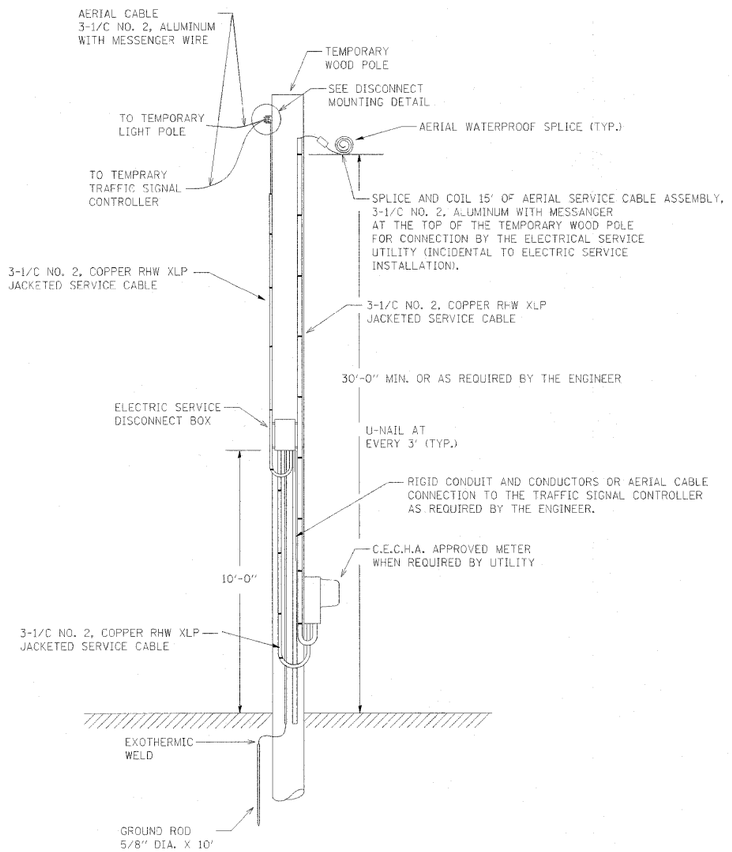
FILE NAME = c:\pw_work\PWIDOT\BAUERDL\d0108315\be805.dgn	USER NAME = bauerdl	DESIGNED - MP	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/14/2010	DATE - 01/14/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING

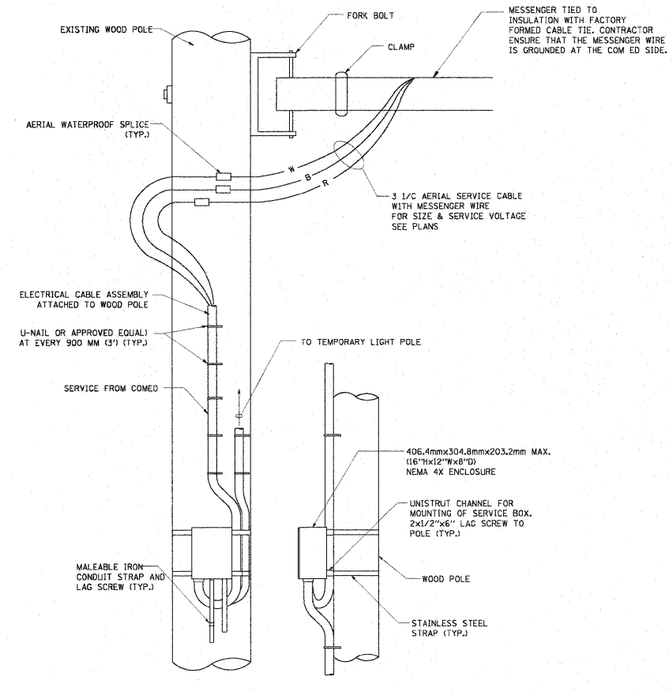
SCALE: NONE | SHEET NO. 1 OF 3 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	23
	BE-805	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



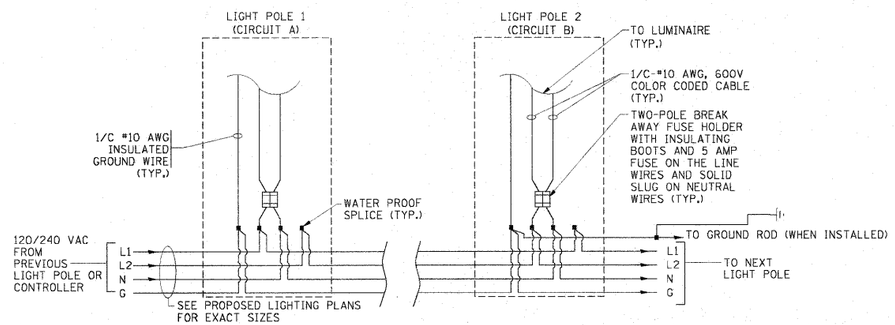
TEMPORARY SERVICE INSTALLATION DETAIL

NOT TO SCALE



DISCONNET MOUNTING DETAIL

NOT TO SCALE



LIGHT POLE WIRING DETAIL

NOT TO SCALE

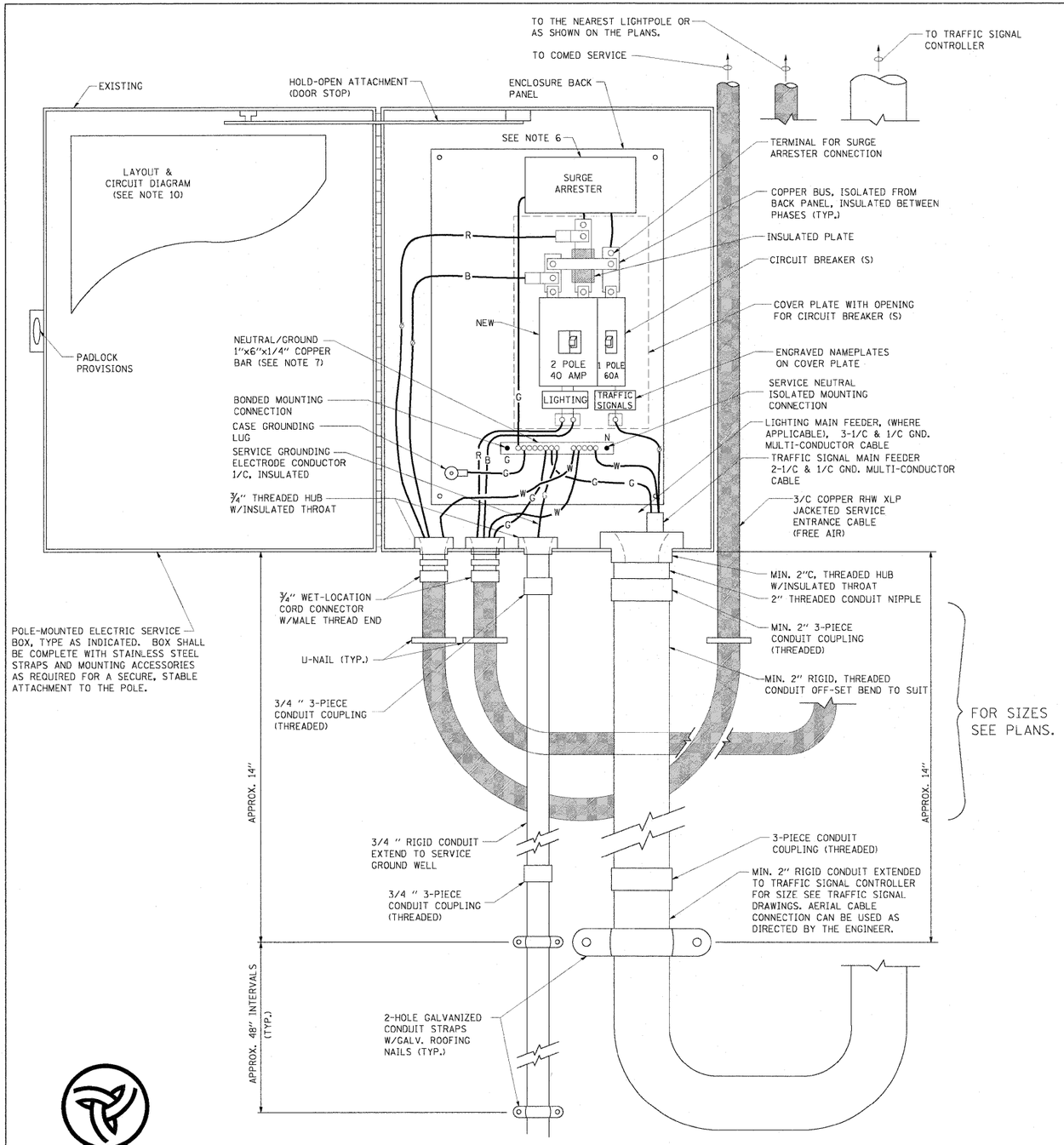
FILE NAME = c:\pw_work\PWIDOT\BAUERDL\d0108315\be805.dgn	USER NAME = bauerdl	DESIGNED - MP	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/14/2010	DATE - 01/14/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING

SCALE: NONE | SHEET NO. 2 OF 3 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	24
	BE-805	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

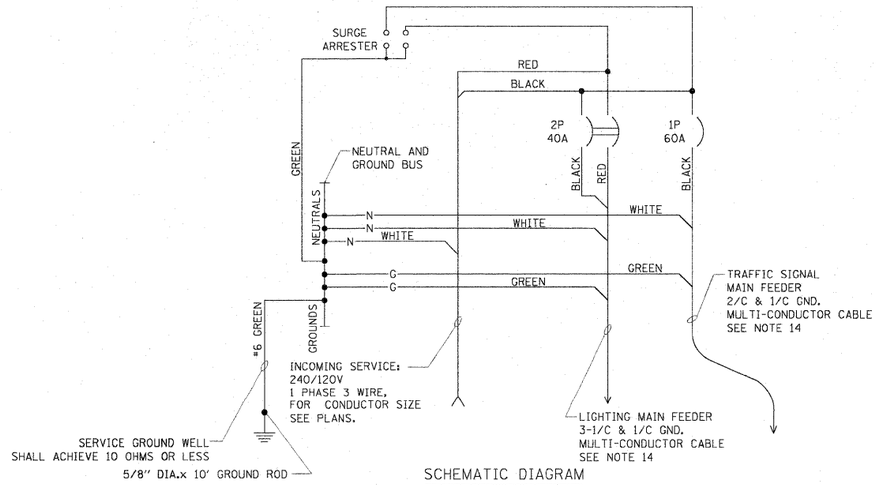


POLE-MOUNTED ELECTRIC SERVICE ENTRANCE
GENERAL LAYOUT DIAGRAM
NOT TO SCALE

6" DECAL ON FRONT COVER

NOTES:

1. ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED OR DESIGNATED BY THE ENGINEER, AND SERVICE DROP CABLE SHALL BE COMPATIBLE WITH THE SERVICE ACCORDINGLY. SOME INSTALLATIONS MAY CALL FOR SERVICE ENTRANCE EQUIPMENT SUITABLE FOR 3-WIRE SERVICE EVEN THOUGH INITIALLY WIRED FOR 2-WIRE SERVICE.
2. THE POLE-MOUNTED ELECTRIC SERVICE BOX SHALL BE CONFIGURED AND FULLY EQUIPPED FOR 240/120V 3W SERVICE, COMPLETE WITH LIGHTING MAIN BREAKER AND TRAFFIC SIGNALS MAIN BREAKER AS REQUIRED.
3. THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LISTED AS SUITABLE FOR USE AS SERVICE ENTRANCE EQUIPMENT.
4. THE ELECTRIC SERVICE EQUIPMENT ENCLOSURE SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12"W X 16"H X 8"D, WITH A PIANO-HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS AND DOOR STOP, HOFFMAN CATALOG NO. A-16H12085SSLP/A-16 P12/A-DSTOPK/C-PMK12, OR APPROVED EQUAL.
5. CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/ TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
6. THE SURGE PROTECTOR SHALL BE SUITABLE FOR THE SERVICE VOLTAGE SINGLE PHASE 60HZ AC, WITH A SURGE ENERGY CAPABILITY OF 2160 JOULES OR BETTER AT 8/20 MICRO-SECONDS, RATED -40 TO 60 DEGREES C., WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CMOV230L065XST OR APPROVED EQUAL.
7. BUS BARS, CONNECTORS, AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED, AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS, OR THE ASSEMBLY SHALL BE A MANUFACTURED SPECIALTY PANELBOARD, CUTLER-HAMMER PRL2A OR APPROVED EQUAL.
8. THE COMBINATION GROUND AND NEUTRAL BAR SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN. THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE. THE SERVICE NEUTRAL AND SERVICE GROUNDING ELECTRODE CONDUCTOR SHALL BE TERMINATED ADJACENT TO EACH OTHER AT THE DIVIDE BETWEEN THE SECTIONS AND WIRING SHALL BE TERMINATED ONLY UPON THE APPROPRIATE SECTION.
9. THE WIRING TERMINALS, INCLUDING THE GROUND/NEUTRAL BAR SHALL BE ARRANGED TO PROVIDE ADEQUATE ROOM FOR PERFORMING FIELD TERMINATIONS.
10. A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE MECHANICALLY SECURED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
11. A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
12. LUGS AND CONNECTORS SHALL BE RATED FOR 75 C CONDUCTOR.
13. THE EXACT MOUNTING HEIGHT OF THE BOX SHALL BE FIELD DETERMINED TO AVOID OBSTRUCTIONS AND PUBLIC ACCESS. TYPICAL HEIGHT SHALL BE APPROXIMATELY 10 FEET ABOVE GRADE.



SCHEMATIC DIAGRAM

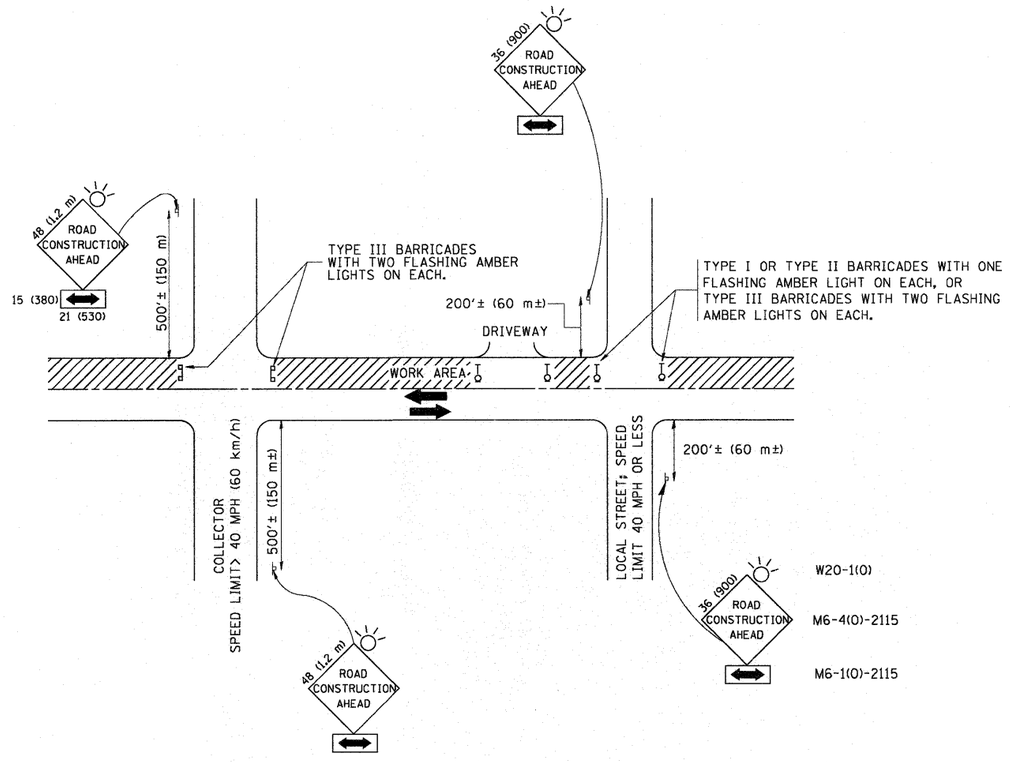
FILE NAME = c:\pw_work\PWIDOT\BAUERDL\d0108315\be805.dgn	USER NAME = bauerdl	DESIGNED - MP	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/14/2010	DATE - 01/14/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING

SCALE: NONE | SHEET NO. 3 OF 3 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	25
	BE-805	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

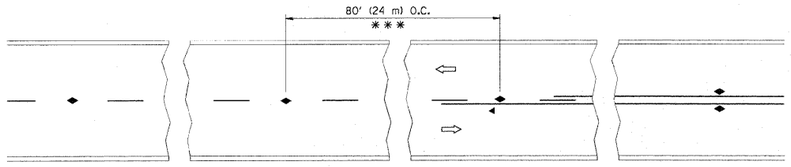
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglienobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

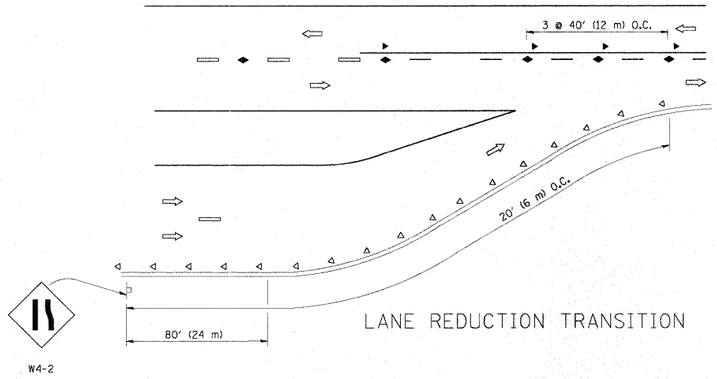
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	26
	TC-10	CONTRACT NO.60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

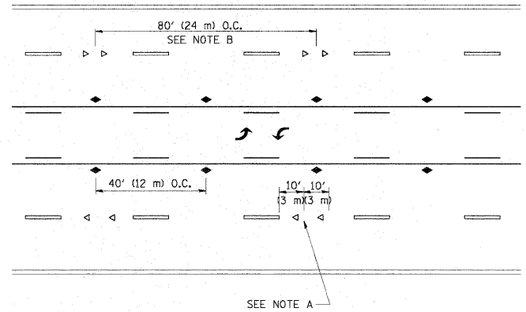


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

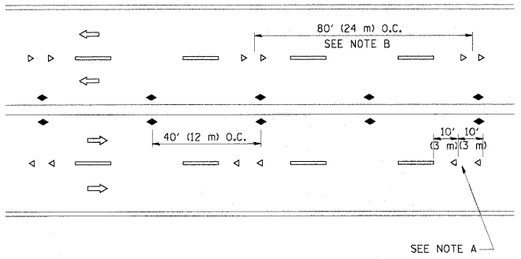
TWO-LANE/TWO-WAY



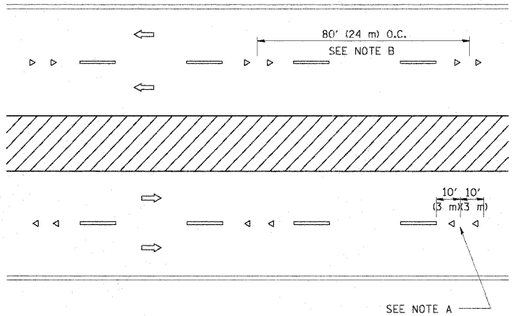
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

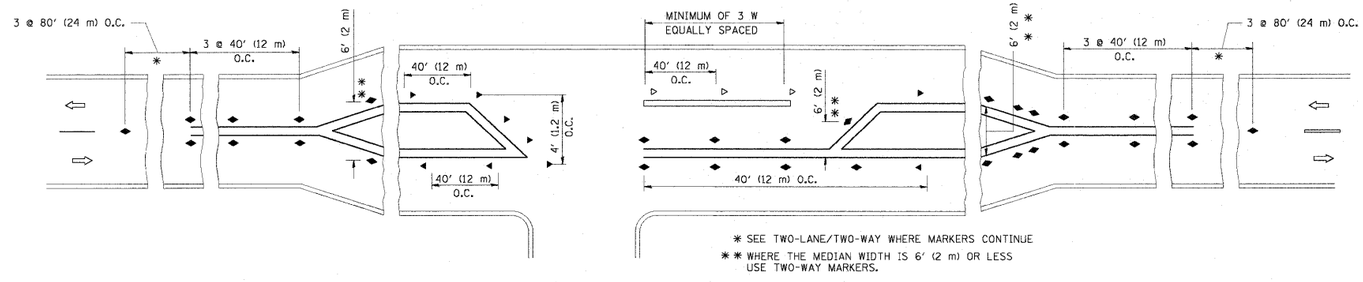
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

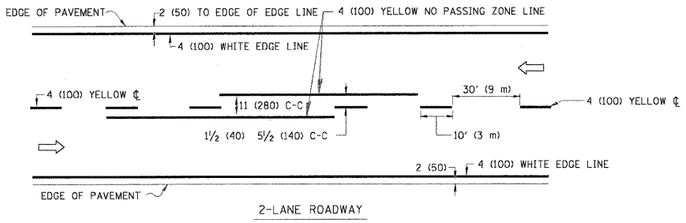
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0128315\tc11.dgn	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

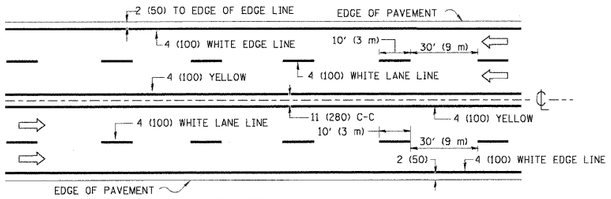
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

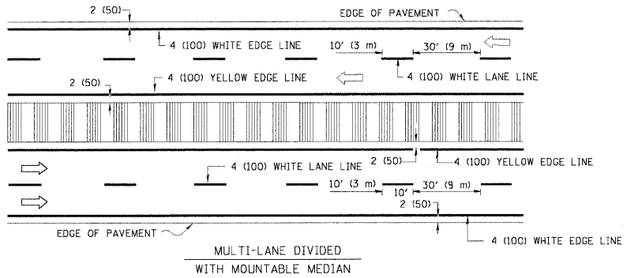
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	27
	TC-11	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



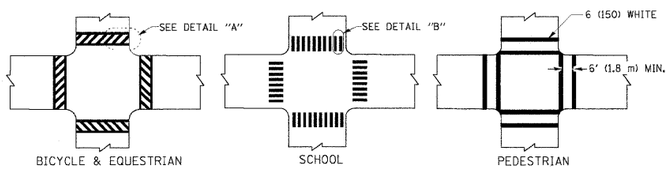
MULTI-LANE UNDIVIDED

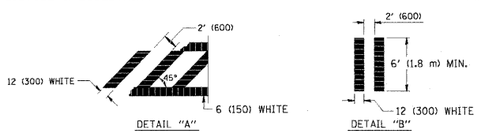


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

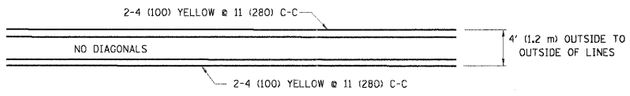
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

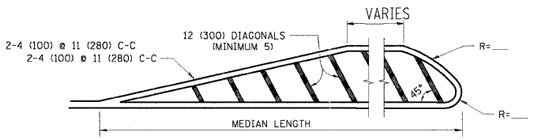




TYPICAL CROSSWALK MARKING



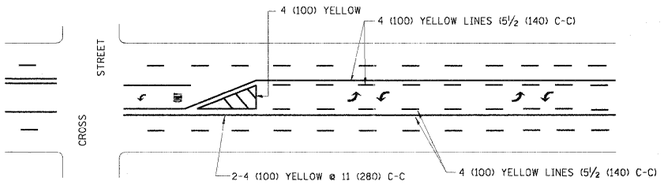
4' (1.2 m) WIDE MEDIANS ONLY



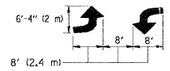
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

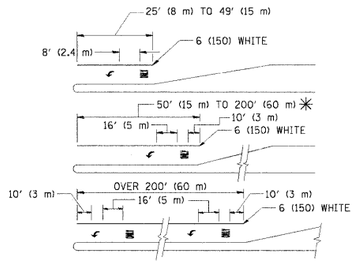


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

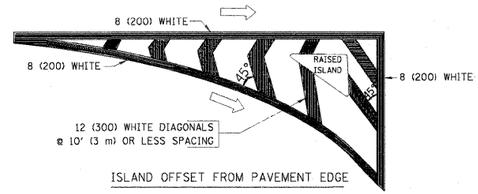


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

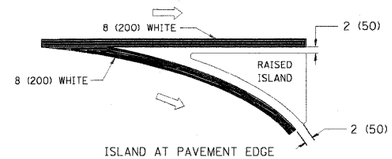
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

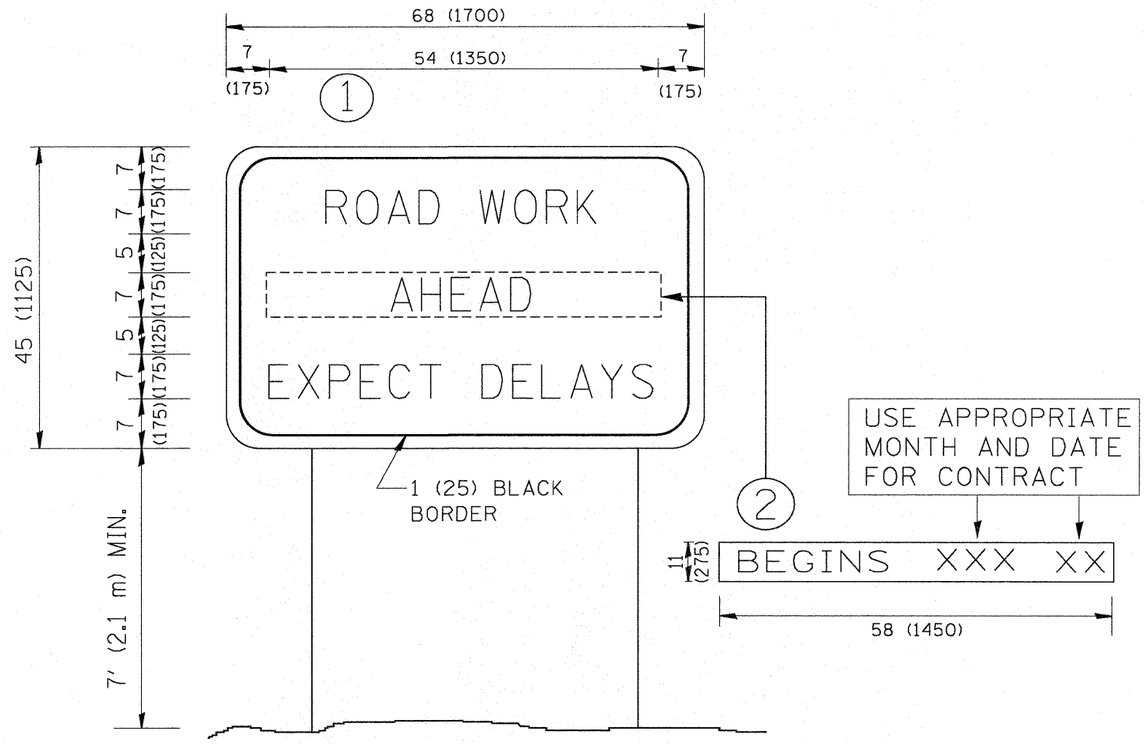
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0128315\tc13.dgn	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1	McHENRY	34	28
	TC-13	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

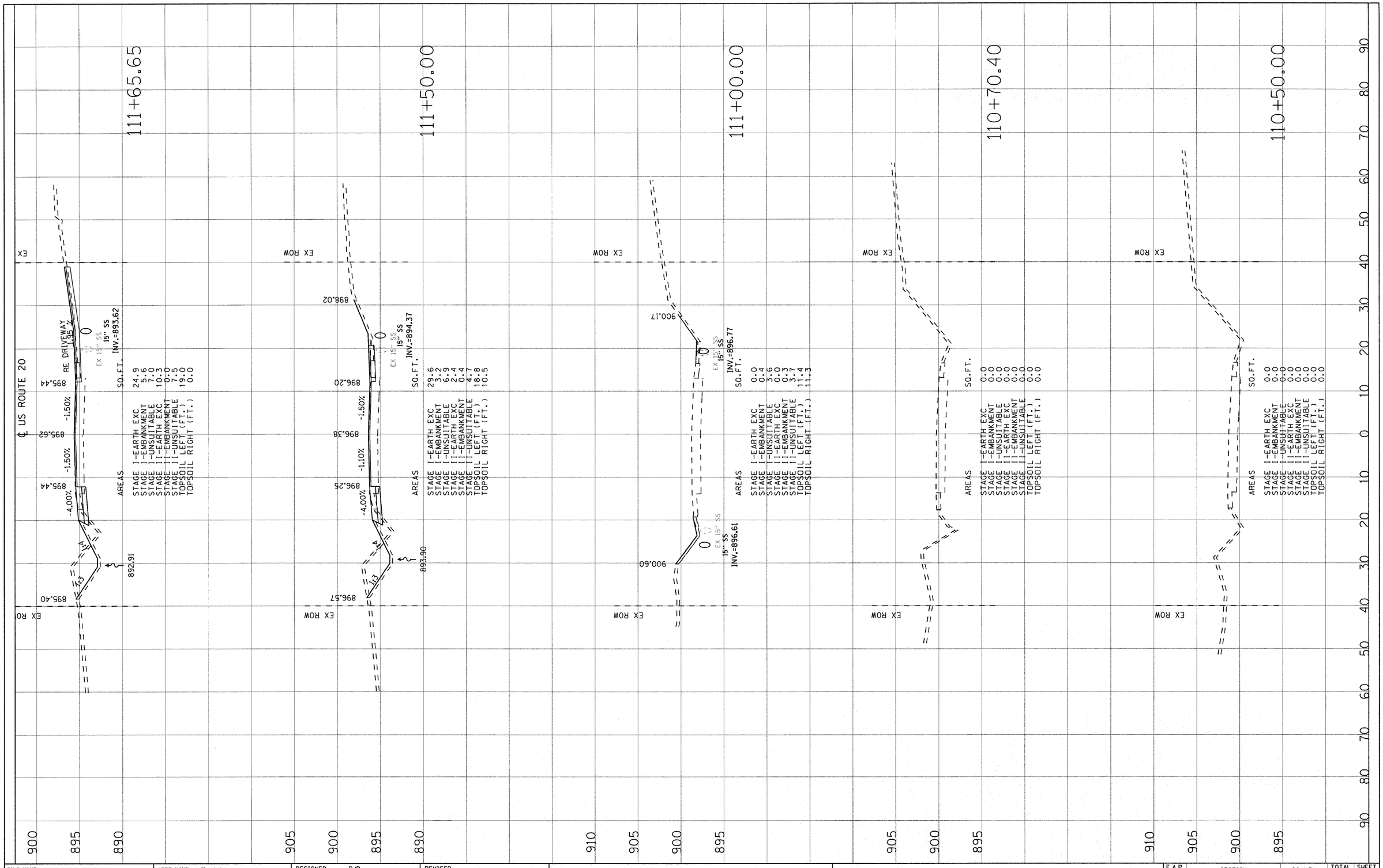
ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-7-1	McHENRY	34	29
	TC-22	CONTRACT NO. 60M53		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY	SURVEYED	BY	
	PLOTTED		DATE
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	
	PLOTTED		DATE
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		



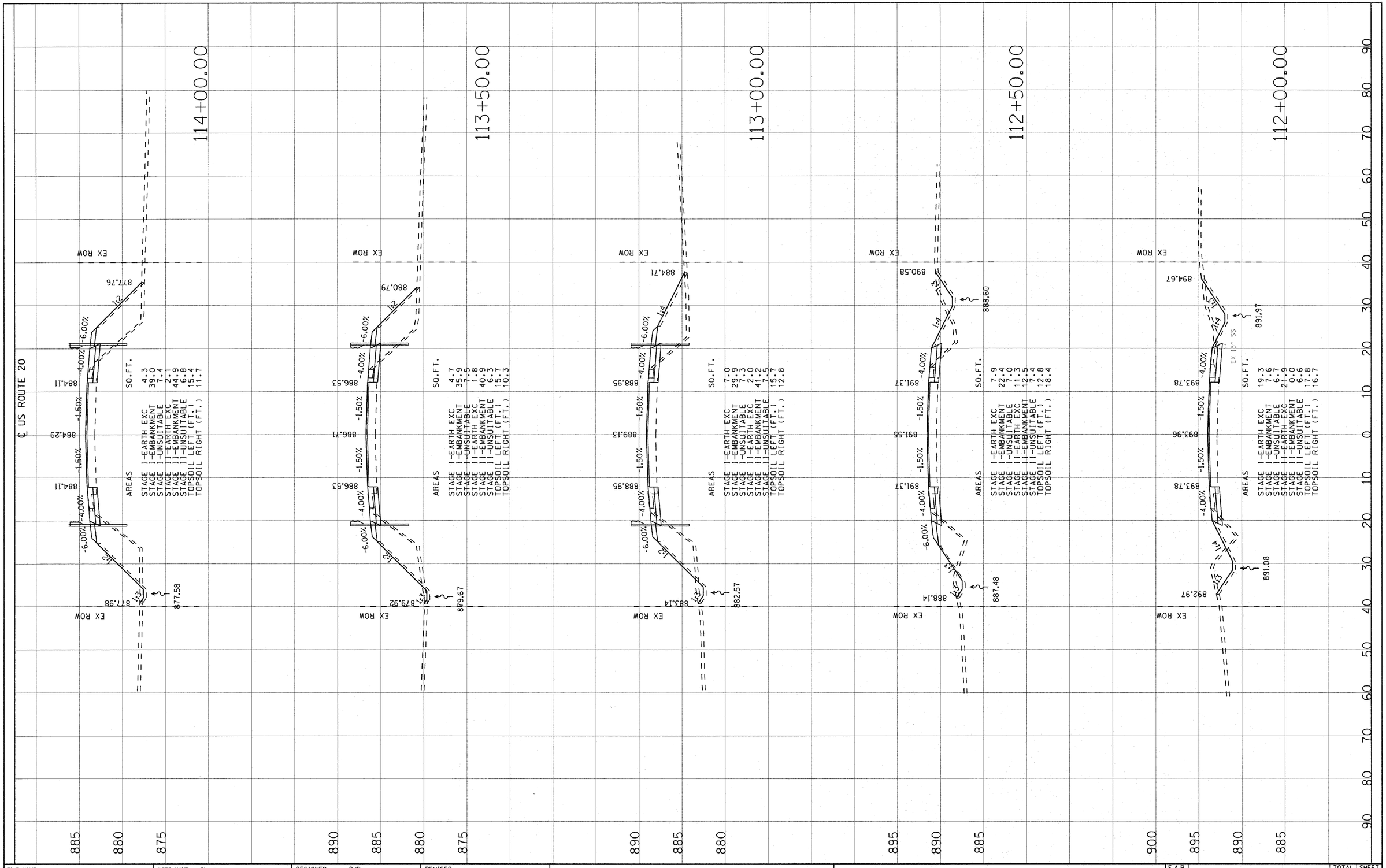
FILE NAME =	USER NAME = 2bookd	DESIGNED - DJB	REVISED -
G:\project\21021155_003\CADD\Civil\Sht\D160M53-12sht-xsec.dgn		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 6/20/2011	DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
US ROUTE 20 CROSS SECTIONS

SCALE: 1"=5'V/10'H | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1		34	30
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FILE NAME = g:\project\2102156.003\cadd\civil\sht\D160M53-12sht-xsec.dgn	USER NAME = 2kujawc	DESIGNED - DJB	REVISED -
		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

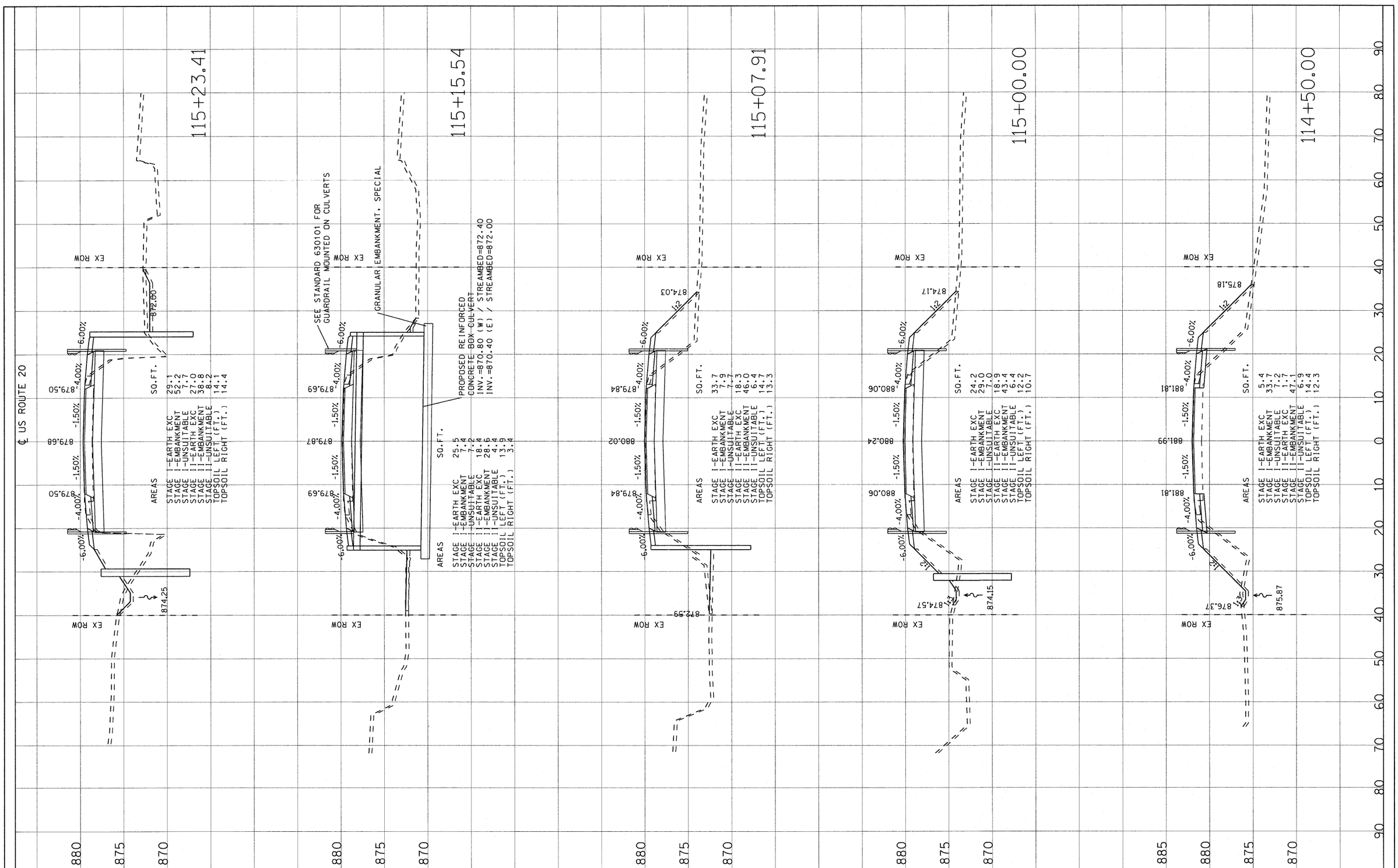
FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
US ROUTE 20 CROSS SECTIONS

SCALE: 1"=5'V/10'H | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1		34	31
			CONTRACT NO.60M53	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY — NOTE BOOK — NO. — BY — DATE — SURVEYED — PLOTTED — TEMPLATE — AREAS — AREAS CHECKED

ORIGINAL SURVEY — NOTE BOOK TEMPLATE TEMPLATE — NO. — BY — DATE — SURVEYED — PLOTTED — TEMPLATE — AREAS — AREAS CHECKED



FINAL SURVEY — NOTE BOOK — NO. — SURVEYED — PLOTTED — TEMPLATE — AREAS — CROSS CHECKED — BY — DATE

ORIGINAL SURVEY — NOTE BOOK — NO. — SURVEYED — PLOTTED — TEMPLATE — AREAS — CROSS CHECKED — BY — DATE

FILE NAME = g:\project\2102155_003\cadd\civil\sht\D160M53-12sht-xsec.dgn	USER NAME = 2kujawa	DESIGNED - DJB	REVISED -
		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 06/22/2011	DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

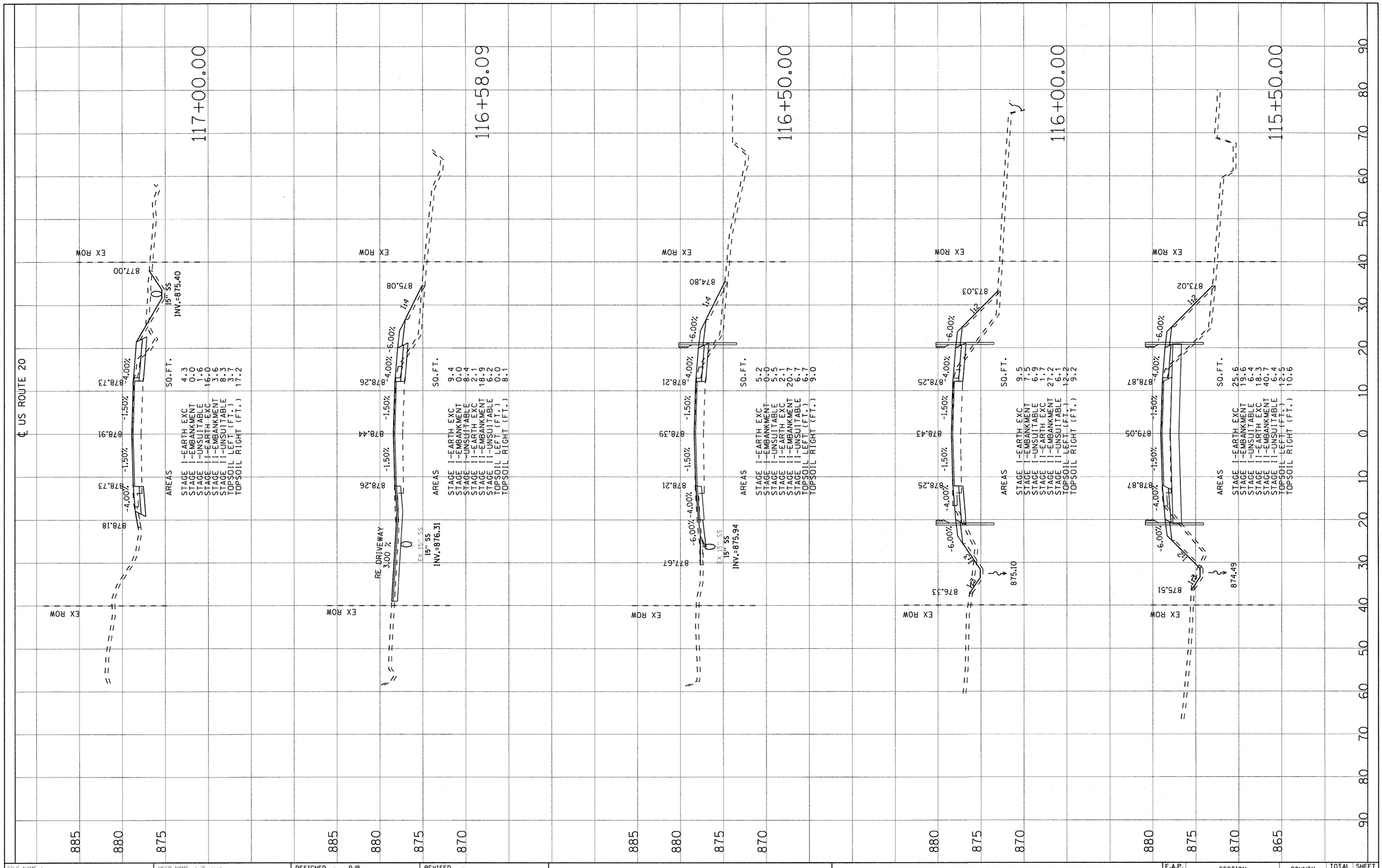
FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
US ROUTE 20 CROSS SECTIONS

SCALE: 1"=5'V/10'H | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1		34	32
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY	SURVEYED	BY	
	PLOTTED		DATE
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	
	PLOTTED		DATE
NOTE BOOK	TEST SITE		
NO.	AREAS CHECKED		



FILE NAME = g:\projects\2102155.003\cadd\civil\sht\D160M53-12sht-xsec.dgn	USER NAME = 2kujawc	DESIGNED - DJB	REVISED -
		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
US ROUTE 20 CROSS SECTIONS

SCALE: 1"=5'V/10'H | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1		34	33
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY	SURVEYED	BY	
	PLOTTED	DATE	
NOTE BOOK	TEMPLATE		
NO.	AREAS	AREAS CHECKED	

ORIGINAL SURVEY	SURVEYED	BY	
	PLOTTED	DATE	
NOTE BOOK	TEMPLATE		
NO.	AREAS	AREAS CHECKED	

℄ US ROUTE 20

117+75.92

EX ROW

EX 12" SS

117+69.79

EX ROW

EX 15" SS

117+30.40

EX ROW

EX 15" SS

INV.=876.75

AREAS	SO.FT.
STAGE I-EARTH EXC.	0.0
STAGE I-EMBANKMENT	0.0
STAGE I-UNSUITABLE	0.0
STAGE II-EARTH EXC	0.0
STAGE II-EMBANKMENT	0.0
STAGE II-UNSUITABLE	0.0
TOPSOIL LEFT (FT.)	0.0
TOPSOIL RIGHT (FT.)	0.0

117+11.93

EX ROW

EX 15" SS

INV.=875.74

RE DRIVEWAY

879.07

-1.50%

5.73 %

878.89

-4.00%

878.39

AREAS	SO.FT.
STAGE I-EARTH EXC.	4.2
STAGE I-EMBANKMENT	0.0
STAGE I-UNSUITABLE	1.0
STAGE II-EARTH EXC	13.7
STAGE II-EMBANKMENT	0.0
STAGE II-UNSUITABLE	0.0
TOPSOIL LEFT (FT.)	2.2
TOPSOIL RIGHT (FT.)	0.0

890 885 880 875 | 890 885 880 875 | 890 885 880 875 | 885 880 875

0 10 20 30 40 50 60 70 80 90

FILE NAME = g:\project\2102155.003\cadd\civil\sht\D160M53-12sht-xsec.dgn	USER NAME = 2kujawc	DESIGNED - DJB	REVISED -
		DRAWN - ENTRAN	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED - TMH	REVISED -
	PLOT DATE = 06/20/2011	DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE 525 (US ROUTE 20) OVER DRAINAGE DITCH
US ROUTE 20 CROSS SECTIONS

SCALE: 1"=5'V/10'H | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
525	11-T-1		34	34
		CONTRACT NO.60M53		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		