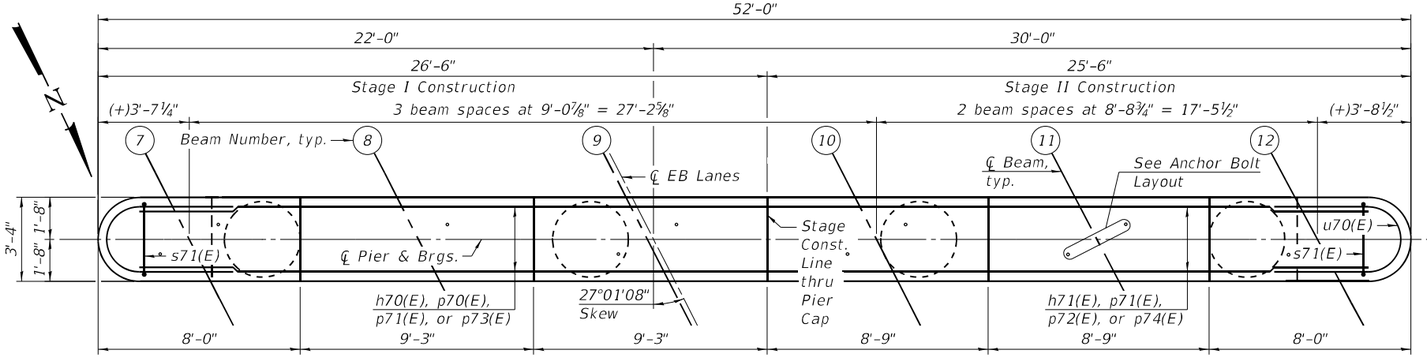
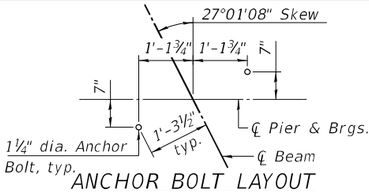
Notes:
Space reinforcement into cap to miss anchor bolts.
Pour steps monolithically with cap.
For details of piles, see sheet 53 of 65.
See sheet 52 of 65 for Bar Splicer details.

PILE DATA

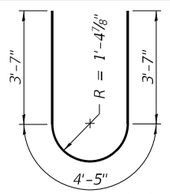
Type: HP14x89 with pile shoes
Nominal Required Bearing: 705 kips
Factored Resistance Available: 388 kips
Est. Length: 26 ft
No. Production Piles: 13
No. Test Piles: 1



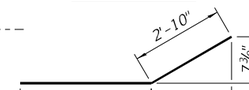
TOP PLAN



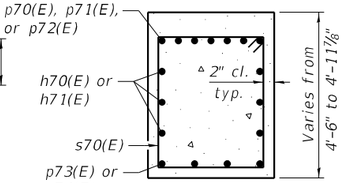
ANCHOR BOLT LAYOUT



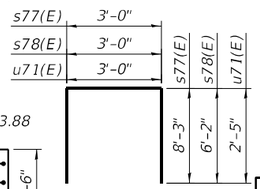
BAR u70(E)



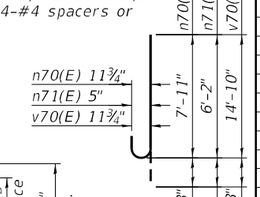
BAR p75(E)



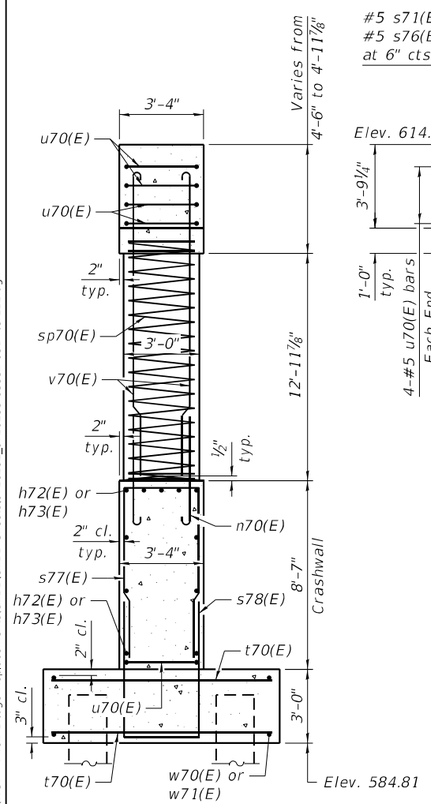
SECTION A-A



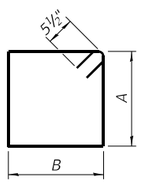
BARS s77(E) s78(E) & u71(E)



BARS n70(E), n71(E) & v70(E)



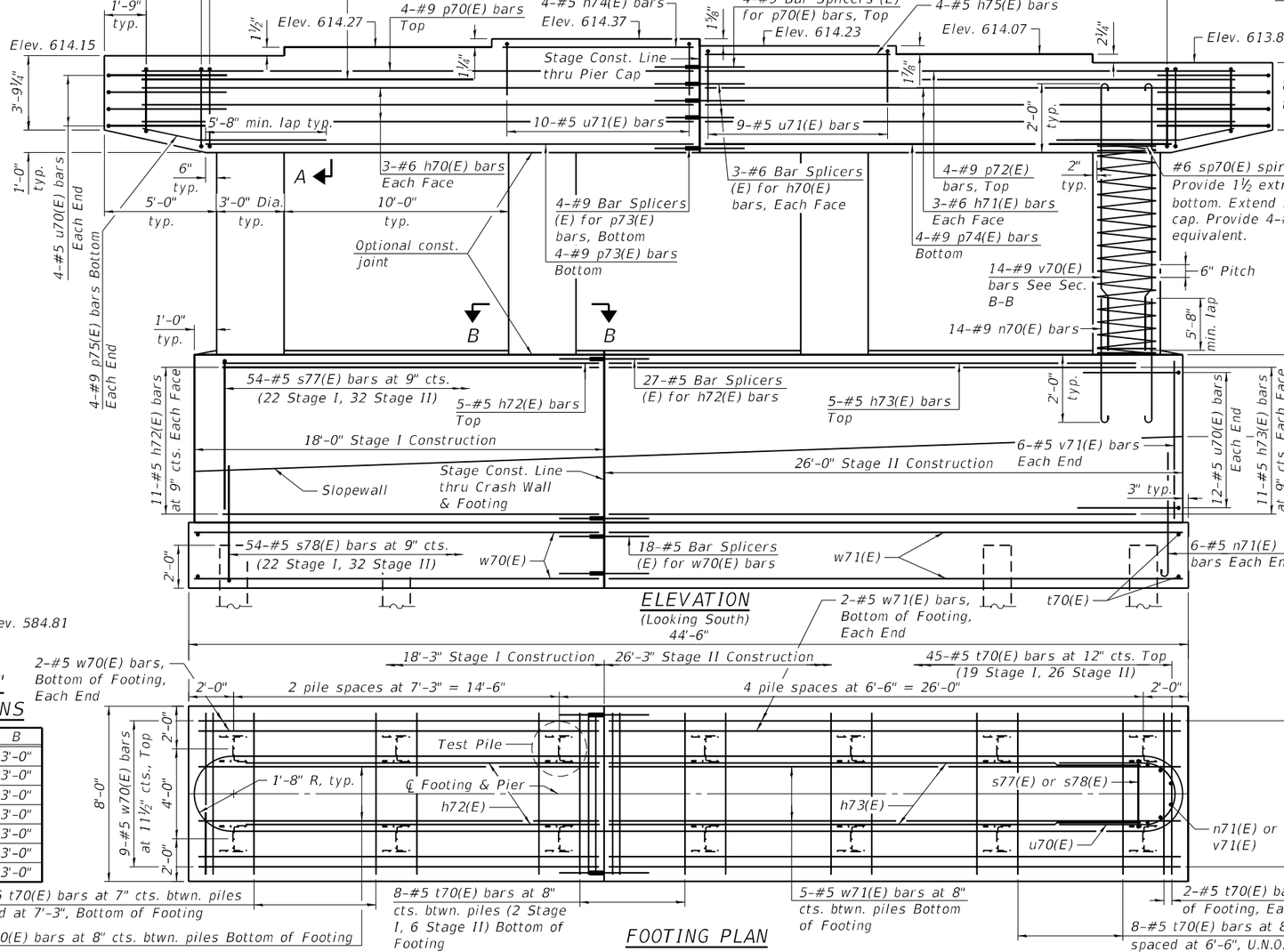
END VIEW



BARS s70(E) THRU s76(E)

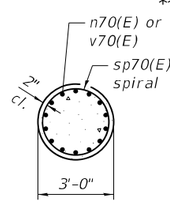
"A" & "B" DIMENSIONS

Bar	A	B
s70(E)	4'-2"	3'-0"
s71(E)	3'-6"	3'-0"
s72(E)	3'-8"	3'-0"
s73(E)	3'-9½"	3'-0"
s74(E)	3'-10½"	3'-0"
s75(E)	4'-0"	3'-0"
s76(E)	4'-1½"	3'-0"



ELEVATION (Looking South) 44'-6"

FOOTING PLAN

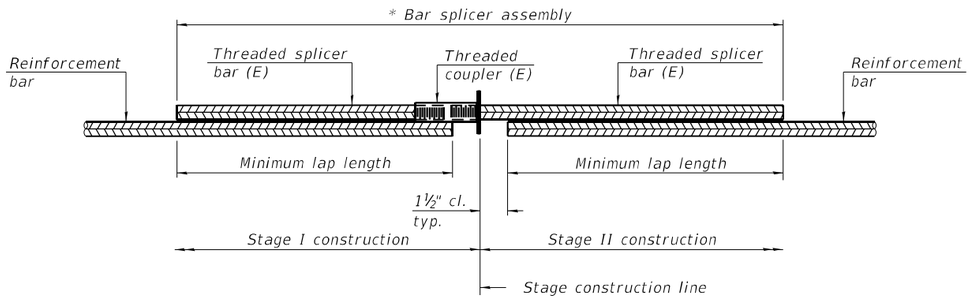


SECTION B-B

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h70(E)	6	#6	24'-7"	—
h71(E)	6	#6	23'-7"	—
h72(E)	27	#5	16'-1"	—
h73(E)	27	#5	24'-1"	—
h74(E)	4	#5	8'-11"	—
h75(E)	4	#5	8'-5"	—
n70(E)	56	#9	9'-2"	⌒
n71(E)	12	#5	6'-9"	⌒
p70(E)	4	#9	24'-7"	—
p71(E)	6	#9	12'-0"	—
p72(E)	4	#9	23'-7"	—
p73(E)	4	#9	21'-9"	—
p74(E)	4	#9	20'-9"	—
p75(E)	8	#9	8'-9"	⌒
s70(E)	86	#5	15'-3"	⊏
s71(E)	2	#5	13'-11"	⊏
s72(E)	2	#5	14'-3"	⊏
s73(E)	2	#5	14'-6"	⊏
s74(E)	2	#5	14'-8"	⊏
s75(E)	2	#5	14'-11"	⊏
s76(E)	2	#5	15'-2"	⊏
s77(E)	54	#5	19'-6"	⊔
s78(E)	54	#5	15'-4"	⊔
** sp70(E)	4	#6	13'-2"	⌇⌇⌇
t70(E)	103	#5	7'-8"	—
u70(E)	32	#5	11'-7"	⊃
u71(E)	19	#5	7'-10"	⊔
v70(E)	56	#9	16'-1"	⊃
v71(E)	12	#5	8'-3"	—
w70(E)	18	#5	17'-11"	—
w71(E)	18	#5	25'-11"	—
Structure Excavation		Cu. Yd.	218	
Concrete Structures		Cu. Yd.	129.0	
Reinforcement Bars, Epoxy Coated		Pound	15,530	
Furnishing Steel Piles HP14x89		Foot	338	
Driving Piles		Foot	338	
Test Pile Steel HP14x89		Each	1	
Pile Shoes		Each	14	

** Length is height of spiral.



STANDARD BAR SPLICER ASSEMBLY PLAN

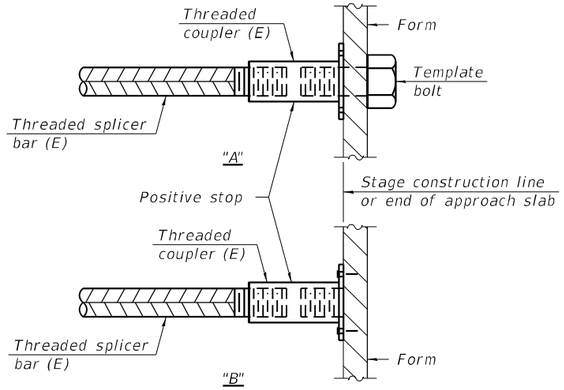
(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1½" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

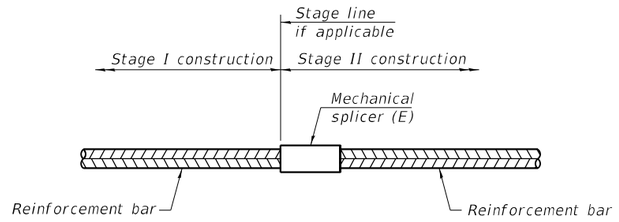
SN 072-0252 (EB)			
Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	867	3'-6"
N. Diaphragm	#6	7	4'-0"
S. Diaphragm	#6	7	4'-0"
N. Appr. Slab	#5	41	3'-4"
N. Appr. Slab	#8	54	4'-9"
N. Appr. Footing	#5	40	3'-2"
S. Appr. Slab	#5	41	3'-4"
S. Appr. Slab	#8	54	4'-9"
S. Appr. Footing	#5	40	3'-2"
N. Abut.	#7	10	5'-0"
S. Abut.	#7	10	5'-0"
Pier 1 Cap	#9	8	6'-5"
Pier 1 Cap	#6	6	4'-4"
Pier 1 Crashwall	#5	29	3'-7"
Pier 1 Footing	#5	18	3'-7"
Pier 2 Cap	#9	8	6'-5"
Pier 2 Cap	#6	6	4'-4"
Pier 2 Crashwall	#5	27	3'-7"
Pier 2 Footing	#5	18	3'-7"

SN 072-0253 (WB)			
Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	753	3'-6"
Diaphragm	#6	14	4'-0"
N. Appr. Slab	#5	41	3'-4"
N. Appr. Slab	#8	54	4'-9"
N. Appr. Footing	#5	40	3'-2"
S. Appr. Slab	#5	41	3'-4"
S. Appr. Slab	#8	54	4'-9"
S. Appr. Footing	#5	40	3'-2"
N. Abut.	#7	10	5'-0"
S. Abut.	#7	10	5'-0"
Pier 1 Cap	#9	12	6'-5"
Pier 1 Cap	#6	6	4'-4"
Pier 1 Crashwall	#5	27	3'-7"
Pier 1 Footing	#5	18	3'-7"
Pier 2 Cap	#9	12	6'-5"
Pier 2 Cap	#6	6	4'-4"
Pier 2 Crashwall	#5	23	3'-7"
Pier 2 Footing	#5	18	3'-7"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

1-1-2020

QUIGG ENGINEERING INC

USER NAME = zdavidson	DESIGNED - RPW	REVISED -
0720252_0720253-68884-052-BarSplicer.dgn	CHECKED - KWB	REVISED -
PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -
PLOT DATE =	CHECKED - MDC	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)

SHEET 52 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	102
		CONTRACT NO. 68884		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
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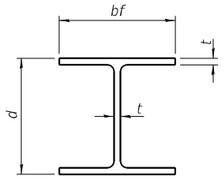
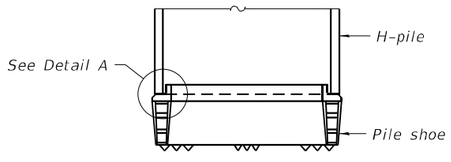
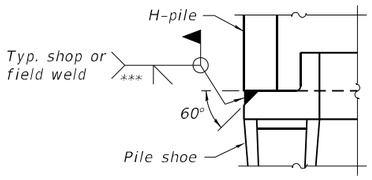


STEEL PILE TABLE

Designation	Depth d	Flange width bf	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	11/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	11/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



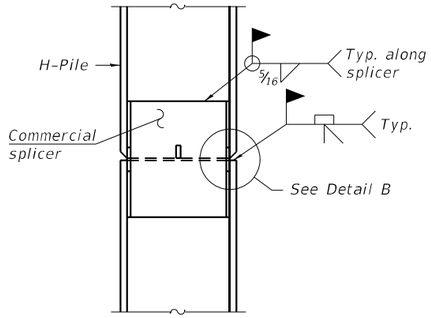
ELEVATION



DETAIL A

SHOE ATTACHMENT

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.



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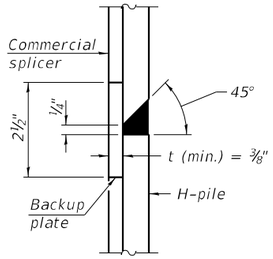
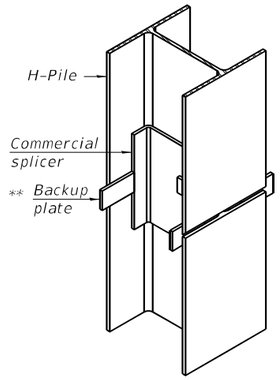
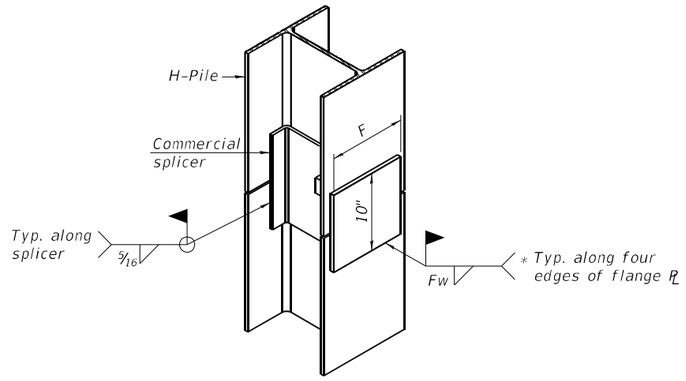


DETAIL "B"



ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE



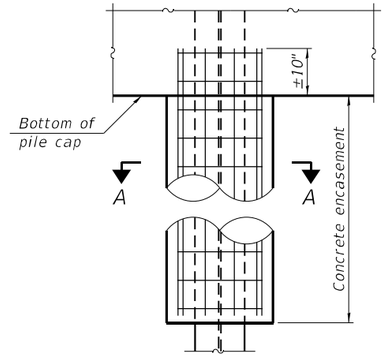
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

* Interrupt welds 1/4" from end of web and/or each flange.

** Remove portions of backup plates that extend outside the flanges.

*** Weld size per pile shoe manufacturer (5/16" min.).



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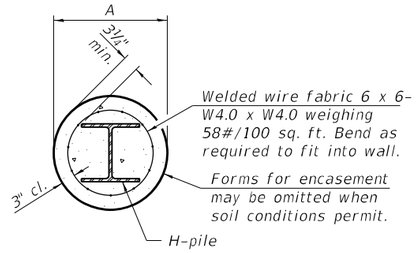
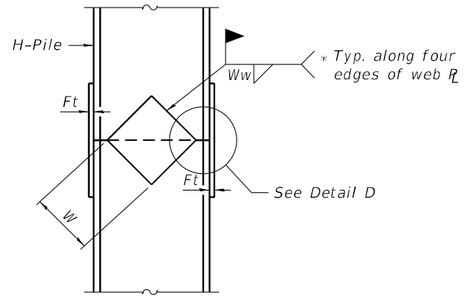


SECTION A-A

INDIVIDUAL PILE CONCRETE ENCASEMENT

(when specified)



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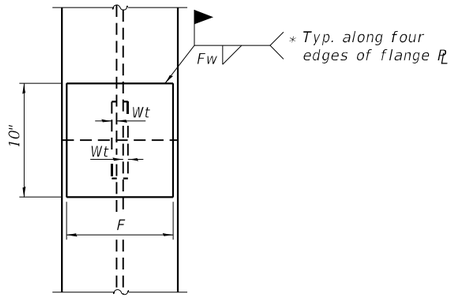
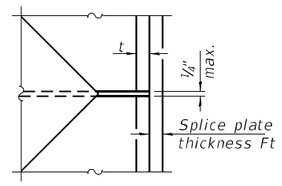


END VIEW



DETAIL D

Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	11/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	11/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	11/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

WELDED PLATE FIELD SPLICE

F-HP 1-1-2020

QUIGG ENGINEERING INC

USER NAME = zdavidson	DESIGNED - RPW	REVISED -	
0720252_0720253-68884-053-Piles.dgn	CHECKED - KWB	REVISED -	
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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HP PILE DETAILS
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 53 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	103
		CONTRACT NO. 68884		
		ILLINOIS	FED. AID PROJECT	

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-IIWO7 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-053-Piles.dgn
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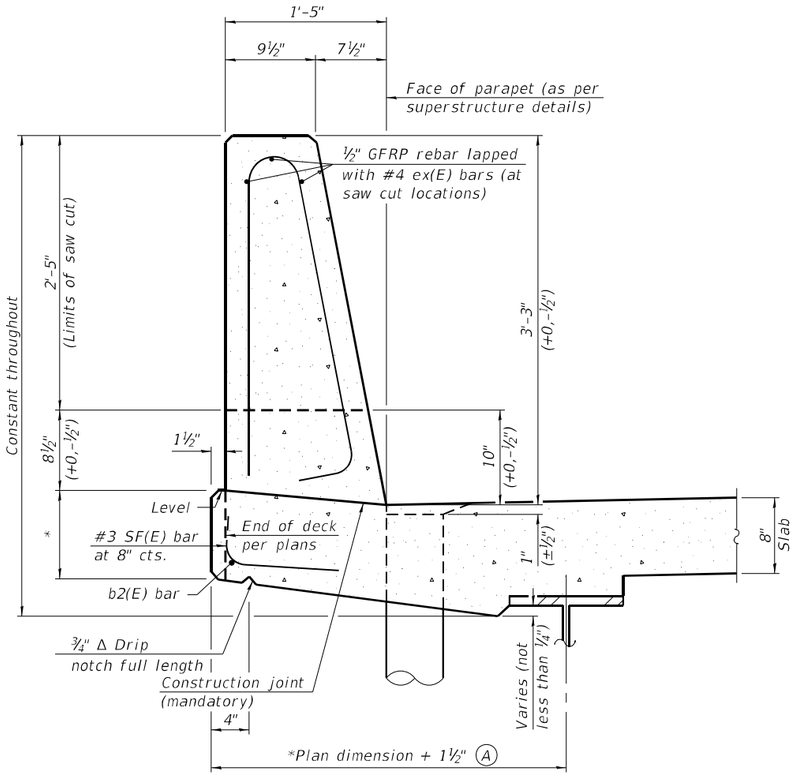
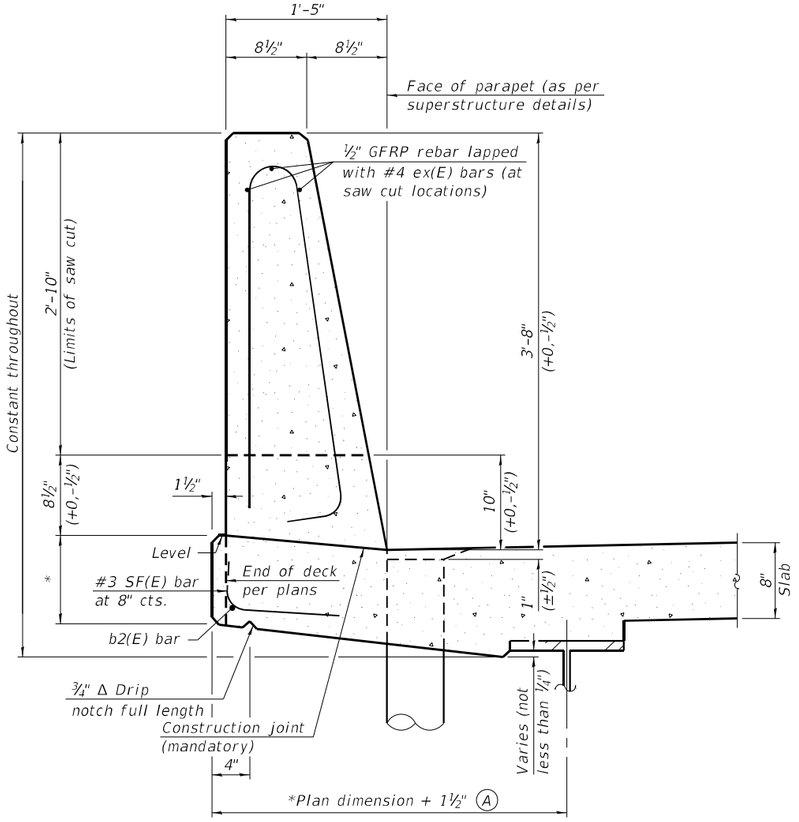


39" CONSTANT-SLOPE PARAPET SECTION

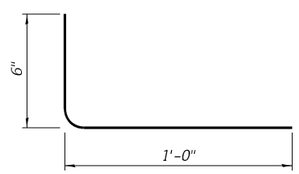
(Showing dimensions, d(E), and ½" Ø GFRP rebar)

*See Superstructure Details.

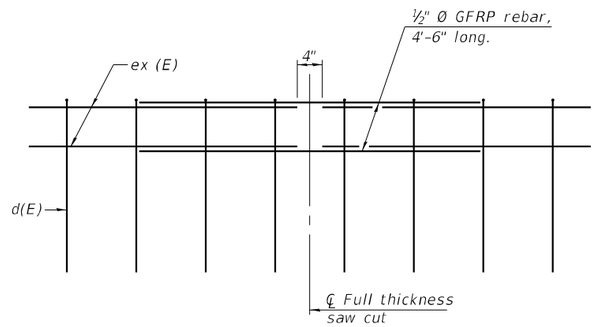


44" CONSTANT-SLOPE PARAPET SECTION

(Showing dimensions, d(E), and ½" Ø GFRP rebar)



#3 (E) BAR



GFRP REBAR STIFFENING DETAIL

(Place as shown in parapet section at each parapet joint location.)

Notes:

All dimensions shall remain the same as shown on superstructure details, except dimension A which is to be revised as shown. Additional concrete needed to revise dimension A = 0.00348 cu. yds./ft. for 39" and 44" parapets.

Place full depth aluminum sheets as shown on superstructure details.

Replace all cork joint filler locations with a full thickness saw cut.

Steel superstructure shown. Other superstructure types similar.

SFP 39-44 1-1-2020

QUIGG ENGINEERING INC	USER NAME = zdavidson	DESIGNED - RPW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONCRETE PARAPET SLIPFORMING OPTION STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
	0720252_0720253-68884-054-SFP.dgn	CHECKED - KWB	REVISED -		SHEET 54 OF 65 SHEETS
	PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -		
	PLOT DATE =	CHECKED - MDC	REVISED -		

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	104
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
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8/3/2022 10:05:29 AM

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/15/19

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY BI (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM, Latitude 40°41'18.74"N, Longitude 89°40'33.41"W

COUNTY Peoria DRILLING METHOD Solid Stem/ Rotary HAMMER TYPE AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-1
Station 222+95
Offset 100.5 ft RT
Ground Surface Elev. 596.28 ft

Surface Water Elev. ft
Stream Bed Elev. ft
Groundwater Elev.:
First Encounter ft
Upon Completion ft
After 96 Hrs. 581.2 ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
TOPSOIL 595.28			11	SILTY CLAY LOAM: gray, moist, very stiff, with traces of gravel			
CLAY LOAM: brown, moist, very stiff, with traces of sand and gravel	3				6		
	4 11	1.7	18		13 50/4	3.5 P	19
	3			573.28 MUDSTONE: gray, moist, completely weathered, soft	35		
-5	4 3	1.5	20	-25	50/5		13
590.28				Borehole continued with rock coring.			
CLAY LOAM: brown, moist, very soft to soft, with traces of sand and gravel	1						
	3 4	0.2	23 16				
	3						
586.28 -10	5 4	0.4	22	-30			
CLAY LOAM: brown and grayish brown, moist, medium stiff, with traces of sand and gravel	1						
	3 4	0.8	26				
	4						
▽15	4 5	0.8	31	-35			
	2						
	3 6	0.5	22				
578.28							
CLAY LOAM: grayish brown, moist, stiff, with sand and gravel	5						
576.28 -20	6 7	1.2	22	-40			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 7/15/19

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY BI (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM, Latitude 40°41'18.74"N, Longitude 89°40'33.41"W

COUNTY Peoria CORING METHOD Rotary Wash

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2
Core Diameter 1.9 in
Top of Rock Elev. 573.28 ft
Begin Core Elev. 571.28 ft

BORING NO. SB-1
Station 222+95
Offset 100.5 ft RT
Ground Surface Elev. 596.28 ft

DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
MUDSTONE: gray, moist, completely weathered, soft *(continued)*	RUN 1	72	43		
-30					
Mosture Content: 6.7%; Dry Density: 135.9 pcf					3.5
565.28 SHALE: gray, moist, highly weathered, soft					
Mosture Content: 4.8%; Dry Density: 142.6 pcf					71.7
-35	RUN 2	92	78		
Mosture Content: 4%; Dry Density: 147.0 pcf					47.9
Mosture Content: 2.8%; Dry Density: 153.4 pcf					132.3
-40					
Mosture Content: 2.4%; Dry Density: 151.4 pcf					168.0
554.28 LIMESTONE: gray, weak					
Mosture Content: 0.7%; Dry Density: 151.4 pcf	RUN 3	69	72		289.4
551.28 -45					

End of Boring

Color pictures of the cores Yes
Cores will be stored for examination until
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

RCCK CORE I-474 OVER PLANK RD.GPJ IL_DOT.GDT 5/13/20

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase HIW07 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-055-BoringSB1.dgn
8/3/2022 10:05:41 AM

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0720252_0720253-68884-055-BoringSB1.dgn	CHECKED - KWB	REVISED -	
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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-1)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 55 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	105
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/15/19

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY BI (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM,
Latitude 40°41'17.35"N, Longitude 89°40'30.96"W

COUNTY Peoria DRILLING METHOD Solid Stem/ Rotary HAMMER TYPE AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-2
Station 224+30.5
Offset 85.5 ft LT
Ground Surface Elev. 582.38 ft

Surface Water Elev. ft
Stream Bed Elev. ft
Groundwater Elev.:
First Encounter 572.4 ft ▼
Upon Completion ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
ASPHALT PAVEMENT: about 6 inches 581.88			2	CLAY LOAM: brown, moist, stiff, with traces of sand and gravel *(continued)*	9		
FILL - CLAY LOAM: brown and dark gray, moist, medium stiff, with traces of sand and gravel 580.38	2		27		8		34
SILTY CLAY LOAM: brown, moist, stiff, with traces of sand and gravel	2 3			559.38	4		
	4			MUDSTONE: gray, moist, completely weathered, soft Borehole continued with rock coring.	50/5		
-5	6 6		17	-25			
	4						
	5 9		18				
	4						
572.38 ▼ -10	6 4		21	-30			
CLAY LOAM: brown, moist, stiff, with traces of sand and gravel	2						
	3 4	1.2	25				
	2						
-15	3 4	1.2	27	-35			
	2						
	4 6	0.8	21 35				
brownish gray	4						
-20	3 6	0.7	28	-40			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 7/15/19

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY BI (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM,
Latitude 40°41'17.35"N, Longitude 89°40'30.96"W

COUNTY Peoria CORING METHOD Rotary Wash

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2

BORING NO. SB-2
Station 224+30.5
Offset 85.5 ft LT
Ground Surface Elev. 582.38 ft

Core Diameter 1.9 in
Top of Rock Elev. 558.38 ft
Begin Core Elev. 558.38 ft

DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
MUDSTONE: gray, moist, completely weathered, soft *(continued)*	RUN 1	82	79		
-25 Mosture Content: 5.8%; Dry Density: 139.3 pcf					4.2
555.38 SHALE: gray, most, highly weathered, soft					
Mosture Content: 5.2%; Dry Density: 144.7 pcf					14.4
-30 550.88 LIMESTONE: gray, weak					
Mosture Content: 0.3%; Dry Density: 163.9 pcf					645.1
-35 Mosture Content: 0.2%; Dry Density: 164.5 pcf	RUN 2	93	73		640.9
546.38 COAL: black, moist, highly weathered					
542.38 -40 SHALE: gray, highly weathered, weak, trace sand seams					
Mosture Content: 10.3%; Dry Density: 121.1 pcf					1.9
Mosture Content: 11.0%; Dry Density: 119.2 pcf					0.9
538.38 End of Boring					

Color pictures of the cores Yes
Cores will be stored for examination until _____
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

RCCK CORE I-474 OVER PLANK RD.GPJ IL_DOT.GDT 5/13/20

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-IW07 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-056-BoringSB2.dgn
8/3/2022 10:06:28 AM

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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 56 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	106
		CONTRACT NO. 68884		
	ILLINOIS FED. AID PROJECT			

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/17/19

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** BI (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40°41'19.12"N, **Longitude** 89°40'32.23"W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-4
Station 222+53
Offset 11.0 ft RT
Ground Surface Elev. 618.31 ft

Surface Water Elev. ft
Stream Bed Elev. ft

Groundwater Elev.:
First Encounter ft
Upon Completion ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
TOPSOIL 617.31			14	CLAY LOAM: brown, moist, stiff, with traces of sand and gravel *(continued)*			
FILL - CLAY LOAM: dark brown, moist, medium stiff to stiff, with traces of sand and gravel	4			brown	4		
	4 6	0.9	18		6 9	1.5	20
brownish gray	3				4		
	8 8	1.4	17		6 9	1.7	20
-5				-25			
	3				4		
	5 7	0.9	18		9 9	1.9	19
610.31							
POSSIBLE FILL - CLAY LOAM: brown and dark gray, moist, very stiff, with traces of sand and gravel	4			with silt seams 28.5 to 30 feet	3		
	8 12	1.4	23		7 7	1.2	30
-10				-30			
	3						
	6 5	1.5	24				
605.81							
CLAY LOAM: brown, moist, stiff, with traces of sand and gravel	2			reddish brown	4		
	5 7	2.0	28		12 10	1.1	21 17
-15				-35			
brownish gray	2						
	5 5	1.4	28				
	3				4		
	5 7	1.6	23 23		8 13	1.4	22
-20				-40			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/17/19

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** BI (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40°41'19.12"N, **Longitude** 89°40'32.23"W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-4
Station 222+53
Offset 11.0 ft RT
Ground Surface Elev. 618.31 ft

Surface Water Elev. ft
Stream Bed Elev. ft

Groundwater Elev.:
First Encounter ft
Upon Completion ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
CLAY LOAM: brown, moist, stiff, with traces of sand and gravel *(continued)*				SHALE: gray, weak, highly weathered			
	4				50/1		
	11 16	1.4	26 20				18
-45				-65			
	10				50/1		
	8 11	0.5	22 16				8
-50				548.31 -70			
	6			End of Boring			
	10 18	1.3	65				
-55				-75			
560.31							
LIMESTONE FRAGEMENTS: gray	50/5		18				
558.31 -60				-80			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-4)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 57 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	107
		CONTRACT NO. 68884		
	ILLINOIS FED. AID PROJECT			

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/17/19

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** BI (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**, **Latitude** 40°41'16.73"N, **Longitude** 89°40'32.09"W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-5
Station 225+02
Offset 0.0 ft
Ground Surface Elev. 617.10 ft

Surface Water Elev. ft
Stream Bed Elev. ft
Groundwater Elev.:
First Encounter ft
Upon Completion ft
After 108 **Hrs.** 594.2 ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	Description
			22	TOPSOIL
				616.10
	3			CLAY LOAM: brown, moist, medium stiff to stiff, with traces of sand and gravel
	5	1.4	25	
	6			
	3			
	4		29	
-5	6			
	4			SILTY CLAY LOAM: brownish gray, moist, medium stiff to very stiff, with traces gravel
	5	1.2	34	
	7			
	4			
	5	0.9	22	
-10	6			
	4			reddish brown
	5	1.5	19	
	5			
	5	0.7	27	
-15	6		29	
	3			
	5	1.4	25	
	6			
	5			brown
	7	0.7	21	
-20	7			

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	Description
				CLAY LOAM: brown, moist, medium stiff to stiff, with traces of sand and gravel *(continued)*
	5			
	8	0.7	20	
	15		12	
	28			
	24	0.8	12	
-25	29			
	14			
	22	0.7	21	
	36			
	28			
	21	2.4	17	
-30	50/4			
	8			
	16	1.6	17	
-35	19			
				579.10
	25			SHALE: gray, weak, highly weathered
	50/5	3.2	11	
-40				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/17/19

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** BI (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**, **Latitude** 40°41'16.73"N, **Longitude** 89°40'32.09"W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-5
Station 225+02
Offset 0.0 ft
Ground Surface Elev. 617.10 ft

Surface Water Elev. ft
Stream Bed Elev. ft
Groundwater Elev.:
First Encounter ft
Upon Completion ft
After 108 **Hrs.** 594.2 ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	Description
				SHALE: gray, weak, highly weathered *(continued)*
	50/2			
			18	
				572.10
				End of Boring
-50				
-55				
-60				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

MODEL: Default
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8/3/2022 10:08:00 AM

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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-5)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 58 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	108
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 12/29/21

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY MC (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM,
Latitude 40°41'18.27"N, Longitude 89°40'32.12"W

COUNTY Peoria DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTO

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-6
Station 223+52
Offset 0.0 ft
Ground Surface Elev. 586.00 ft

Surface Water Elev. ft
Stream Bed Elev. ft
Groundwater Elev.:
First Encounter 563.0 ft
Upon Completion 563.0 ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
ASPHALT 585.30				GRAVELLY CLAY, trace sand, grayish brown, stiff			
GRAVELLY SANDY CLAY, brown, stiff	2						
	3 5	1.6					
					6		
				-25	6 8	1.2	
-5							
				559.25			
				SANDY LOAM, gray, moist, very dense			
	3				22		
RIMAC not tested due to presence of large gravels	3				50/3"		
-10	3			-30			
574.25				554.25			
SANDY CLAY, trace gravel, grayish brown, very stiff				CLAYEY SHALE, gray, very dense			
	3			552.00	50/2"		
	3 5	2.0		Borehole continued with rock coring.			
-15				-35			
	3						
	4 6	2.3					
566.00 -20				-40			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 12/29/21

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY MC (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM,
Latitude 40°41'18.27"N, Longitude 89°40'32.12"W

COUNTY Peoria CORING METHOD

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2
Core Diameter 2 in
Top of Rock Elev. 552.00 ft
Begin Core Elev. 552.00 ft

BORING NO. SB-6
Station 223+52
Offset 0.0 ft
Ground Surface Elev. 586.00 ft

DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
SHALE, gray, moderately to completely weathered, weak rock, very closely fractured, laminated 552.00	1	100	67		89.6
-35 Moisture Content = 5.1%; Dry Density = 147.3 pcf					
	2	90	85		
-40					
544.00	3	40	20		
COAL, black, highly weathered, very weak, very closely fractured					
-45 540.50					
LIMESTONE, gray, slightly weathered, medium strong, moderately fractured, thin Moisture Content = 1.3%; Dry Density = 162.9 pcf	4	70	42		470.5
536.00 -50					
LIMESTONE, gray, completely weathered, very weak	5	100	0		

Color pictures of the cores Yes
Cores will be stored for examination until
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

RCCK CORE I-474 OVER PLANK RE.GPJ IL_DOT.GDT 2/9/22

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-III\WO7 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-059-BoringSB6 1of2.dgn

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QUIGG ENGINEERING INC

USER NAME = zdavidson	DESIGNED - RPW	REVISED -	
0720252_0720253-68884-059-BoringSB6 1of2.dgn	CHECKED - KWB	REVISED -	
PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -	
PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-6) (1 OF 2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 59 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	109
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 12/29/21

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** MC (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40°41'18.27"N, **Longitude** 89°40'32.12"W

COUNTY Peoria **CORING METHOD**

STRUCT. NO. 072-0121 & 0122 EX, 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2
Core Diameter 2 in
Top of Rock Elev. 552.00 ft
Begin Core Elev. 552.00 ft

BORING NO. SB-6
Station 223+52
Offset 0.0 ft
Ground Surface Elev. 586.00 ft

	DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
LIMESTONE, gray, completely weathered, very weak *(continued)*	-55					
		6	97	63		
	528.00					
LIMESTONE, gray, moderately weathered, weak rock, close to moderately fractured, thin	-60					
Moisture Content = 6.0%; Dry Density = 147.8 pcf		7	100	100		99.1
	-65					
	520.50					
End of Boring	-70					

Color pictures of the cores Yes
Cores will be stored for examination until
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

ROCK CORE I-474 OVER PLANK RD.GPJ IL_DOT.GDT 2/9/22

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-IIWO7 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-060-BoringSB6 2of2.dgn

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QUIGG ENGINEERING INC

USER NAME = zdavidson	DESIGNED - RPW	REVISED -	
0720252_0720253-68884-060-BoringSB6 2of2.dgn	CHECKED - KWB	REVISED -	
PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -	
PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-6) (2 OF 2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 60 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	110
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 12/29/21

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY MC (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM, Latitude 40°41'17.75"N, Longitude 89°40'32.16"W

COUNTY Peoria DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTO

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

BORING NO. SB-7
Station 223+92
Offset 3.0 ft RT
Ground Surface Elev. 586.00 ft

Surface Water Elev. ft
Stream Bed Elev. ft

Groundwater Elev.:
First Encounter 563.0 ft
Upon Completion 563.0 ft
After Hrs. ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)
ASPHALT 585.40				SILT, trace gravel, gray and brown, moist, medium dense *(continued)*			
SILTY LOAM, light brown, dry, loose				564.25			
				SANDY LOAM, with gravel, grayish brown, moist, very dense			
	3				50/5"		
-5	3 3			-25			
579.25							
SILTY LOAM, trace gravel, light brown, dry, loose							
	3			557.00	50/1"		
-10	2 4			Borehole continued with rock coring. -30			
574.25							
SILTY CLAY, light brown to brown, very soft							
	WOH						
-15	WOH 1			-35			
569.25							
SILT, trace gravel, gray and brown, moist, medium dense							
	3						
-20	5 10			-40			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 12/29/21

ROUTE FAI 474 (I-474) DESCRIPTION Structure boring for bridge replacement LOGGED BY MC (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over IL 116 (Plank Rd), SEC. 11, TWP. 8N, RNG. 7E, 4th PM, Latitude 40°41'17.75"N, Longitude 89°40'32.16"W

COUNTY Peoria CORING METHOD

STRUCT. NO. 072-0121 & 0122 EX 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2
Core Diameter 2 in
Top of Rock Elev. 557.00 ft
Begin Core Elev. 557.00 ft

BORING NO. SB-7
Station 223+92
Offset 3.0 ft RT
Ground Surface Elev. 586.00 ft

DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
SHALE, gray, highly to moderately weathered, weak rock, very closely fractured, thin 557.00	1	93	67		88.1
Moisture Content = 7.4%; Dry Density = 147.1 pcf -30					
	2	100	83		
-35					
Moisture Content = 11.1%; Dry Density = 141.2 pcf	3	98	62		74.7
-40					
	4	93	38		
542.00					
COAL, black, highly weathered, very weak, very closely fractured 541.00 -45					
LIMESTONE, gray, highly weathered, extremely weak					
	5	97	0		

Color pictures of the cores Yes
Cores will be stored for examination until
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

RCCK CORE I-474 OVER PLANK RD.GPJ IL_DOT.GDT 2/9/22

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase III\W07 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253+68884-061-BoringSB7 1of2.dgn
8/3/2022 10:10:21 AM

QUIGG ENGINEERING INC

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0720252_0720253-68884-061-BoringSB7 1of2.dgn	CHECKED - KWB	REVISED -	
PLOT SCALE = 0:2.0000 ':' / in.	DRAWN - LMC	REVISED -	
PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-7) (1 OF 2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 61 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	111
		CONTRACT NO. 68884		
		ILLINOIS	FED. AID PROJECT	

Illinois Department of Transportation
Division of Highways
Terracon

ROCK CORE LOG

Date 12/29/21

ROUTE FAI 474 (I-474) **DESCRIPTION** Structure boring for bridge replacement **LOGGED BY** MC (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over IL 116 (Plank Rd), **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40°41'17.75"N, **Longitude** 89°40'32.16"W

COUNTY Peoria **CORING METHOD**

STRUCT. NO. 072-0121 & 0122 EX, 072-0252 & 0253 PR
Station 223+71 (CL median)

CORING BARREL TYPE & SIZE NX-2
Core Diameter 2 in
Top of Rock Elev. 557.00 ft
Begin Core Elev. 557.00 ft

BORING NO. SB-7
Station 223+92
Offset 3.0 ft RT
Ground Surface Elev. 586.00 ft

	DEPTH (ft)	CORE (#)	RECOVERY (%)	R.Q.D. (%)	CORE TIME (min/ft)	STRENGTH (tsf)
LIMESTONE, gray, highly weathered, extremely weak *(continued)*	-50					
	533.50					
LIMESTONE, gray, slightly weathered, weak rock, moderately fractured	-55	6	100	80		
Moisture Content = 5.3%; Dry Density = 149.5 pcf	-60	7	100	88		89.6
	524.50					
End of Boring	-65					

Color pictures of the cores Yes
Cores will be stored for examination until
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)

BBS, form 138 (Rev. 8-99)

ROCK CORE I-474 OVER PLANK RD.GPJ IL_DOT.GDT 2/9/22

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-III\WO7 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-062-BoringSB7 2of2.dgn
8/3/2022 10:11:07 AM

QUIGG ENGINEERING INC

USER NAME = zdavidson	DESIGNED - RPW	REVISED -	
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PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -	
PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (SB-7) (2 OF 2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 62 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	112
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/6/20

ROUTE FAI 474 (I-474) DESCRIPTION Rock probe boring LOGGED BY BT (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over Plank Road, **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40d 41' 17" N, **Longitude** 89d 40' 33" W

COUNTY Peoria DRILLING METHOD Solid Stem/ Rotary HAMMER TYPE AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX / 072-0252 & 0253 PR
Station 223+71

BORING NO. RP-1
Station 224+58
Offset 68.0 ft RT
Ground Surface Elev. 618.51 ft

Surface Water Elev. ____ ft
Stream Bed Elev. ____ ft

Groundwater Elev.:
First Encounter ____ ft
Upon Completion ____ ft
After ____ **Hrs.** ____ ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	
				BLANK DRILLED TO 33.5 FEET
-5				
-10				
-15				
-20				
-25				BLANK DRILLED TO 33.5 FEET *(continued)*
-30				
				585.21
	53			COAL: black, moist, completely weathered, soft
-35	50		37	583.51
	37			SILTY CLAY LOAM: gray, moist, very stiff, with sand
	8			
	10		17	
	17			580.51
	22			SHALE: gray, completely weathered, exteremely weak
-40	100/3		13	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/6/20

ROUTE FAI 474 (I-474) DESCRIPTION Rock probe boring LOGGED BY BT (Terracon)

SECTION 73-3HB-2 LOCATION I-474 over Plank Road, **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40d 41' 17" N, **Longitude** 89d 40' 33" W

COUNTY Peoria DRILLING METHOD Solid Stem/ Rotary HAMMER TYPE AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX / 072-0252 & 0253 PR
Station 223+71

BORING NO. RP-1
Station 224+58
Offset 68.0 ft RT
Ground Surface Elev. 618.51 ft

Surface Water Elev. ____ ft
Stream Bed Elev. ____ ft

Groundwater Elev.:
First Encounter ____ ft
Upon Completion ____ ft
After ____ **Hrs.** ____ ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	
	40			SHALE: gray, completely weathered, exteremely weak *(continued)*
	100/2		18	
	100			
			14	
-45				573.51
				End of Boring
-50				
-55				
-60				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

SCIL BORING WO 50 PLANK RD RCCK 2RCBES.GPJ IL_DOT.GDT 7/17/20

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-IIW07 - I-464 Bridge Replacements\CADD\CADD Sheets\0720252_0720253-68884-063-BoringRP1.dgn

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QUIGG ENGINEERING INC

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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (RP-1)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 63 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	113
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/7/20

ROUTE FAI 474 (I-474) **DESCRIPTION** Rock probe boring **LOGGED BY** BT (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over Plank Road, **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40d 41' 16" N, **Longitude** 89d 40' 31" W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX
072-0252 & 0253 PR
Station 223+71

BORING NO. RP-2
Station 225+52
Offset 62.0 ft LT
Ground Surface Elev. 619.39 ft

Surface Water Elev. ____ ft
Stream Bed Elev. ____ ft

Groundwater Elev.:
First Encounter ____ ft
Upon Completion ____ ft
After ____ **Hrs.** ____ ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	Description
				BLANK DRILLED TO 38.5 FEET
-5				
-10				
-15				
-20				
-25				BLANK DRILLED TO 38.5 FEET *(continued)*
-30				
-35			19	
580.89				
	2			CLAY LOAM: brown and light gray, moist, stiff, with traces of sand and gravel
579.39 -40	4 6		21	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/7/20

ROUTE FAI 474 (I-474) **DESCRIPTION** Rock probe boring **LOGGED BY** BT (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over Plank Road, **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40d 41' 16" N, **Longitude** 89d 40' 31" W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX
072-0252 & 0253 PR
Station 223+71

BORING NO. RP-2
Station 225+52
Offset 62.0 ft LT
Ground Surface Elev. 619.39 ft

Surface Water Elev. ____ ft
Stream Bed Elev. ____ ft

Groundwater Elev.:
First Encounter ____ ft
Upon Completion ____ ft
After ____ **Hrs.** ____ ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	Description
	1			CLAY LOAM: brown, moist, stiff to medium stiff, with traces of sand and gravel
-45	2 7		19	
	3			- gray 48.5 to 53.5 feet
-50	3 2		23	- with shale fragments 48.5 to 50 feet
565.89				
	50/1			SHALE: gray, completely weathered, exteremely weak
-55			7	
	100/3			
	50/3			
559.39 -60			18	

End of Boring

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

QUIGG ENGINEERING INC

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PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (RP-2)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 64 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	114
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

Illinois Department of Transportation
Division of Highways
Terracon

SOIL BORING LOG

Date 7/6/20

ROUTE FAI 474 (I-474) **DESCRIPTION** Rock probe boring **LOGGED BY** BT (Terracon)

SECTION 73-3HB-2 **LOCATION** I-474 over Plank Road, **SEC.** 11, **TWP.** 8N, **RNG.** 7E, 4th **PM**,
Latitude 40d 41' 18" N, **Longitude** 89d 40' 32" W

COUNTY Peoria **DRILLING METHOD** Solid Stem/ Rotary **HAMMER TYPE** AUTO SPT Hammer

STRUCT. NO. 072-0121 & 0122 EX, 072-0252 & 0253 PR
Station 223+71

BORING NO. RP-3
Station 223+94
Offset 2.0 ft LT
Ground Surface Elev. 586.79 ft

Surface Water Elev. ____ ft
Stream Bed Elev. ____ ft

Groundwater Elev.:
First Encounter ____ ft
Upon Completion ____ ft
After ____ **Hrs.** ____ ft

DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIST (%)	DESCRIPTION
				BLANK DRILLED TO 23.5 FEET
-5				
-10				
-15				
-20				
				BLANK DRILLED TO 23.5 FEET *(continued)*
563.29				
	100/4			SHALE: light gray, completely weathered, extremely weak
-25			14	
	100/3			
			15	
	42			
556.79 -30	74 26/3		13	
				End of Boring
-35				
-40				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

SOIL BORING WO 50 PLANK RD ROCK PROBES.GPJ IL_DOT.GDT 7/17/20

MODEL: Default
FILE NAME: S:\2020\20IL006 - PTB 194-35 D4 - Upchurch - Various Phase I-IIWO7 - I-464 Bridge Replacements\CADD\CADD Sheets\07202520720253-68884-065-BoringRP3.dgn

8/3/2022 10:13:09 AM

QUIGG ENGINEERING INC

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PLOT SCALE = 0:2.0000 ':" / in.	DRAWN - LMC	REVISED -	
PLOT DATE =	CHECKED - MDC	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS (RP-3)
STRUCTURE NO. 072-0252 (EB) & 072-0253 (WB)
SHEET 65 OF 65 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	115
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		

BRIDGE PLANS
PAGE HOLDER

USER NAME = $USER$	DESIGNED -	REVISED -	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**
	DRAWN -	REVISED -	
PLOT SCALE = 1:100	CHECKED -	REVISED -	
PLOT DATE = 7/1/2022	DATE -	REVISED -	

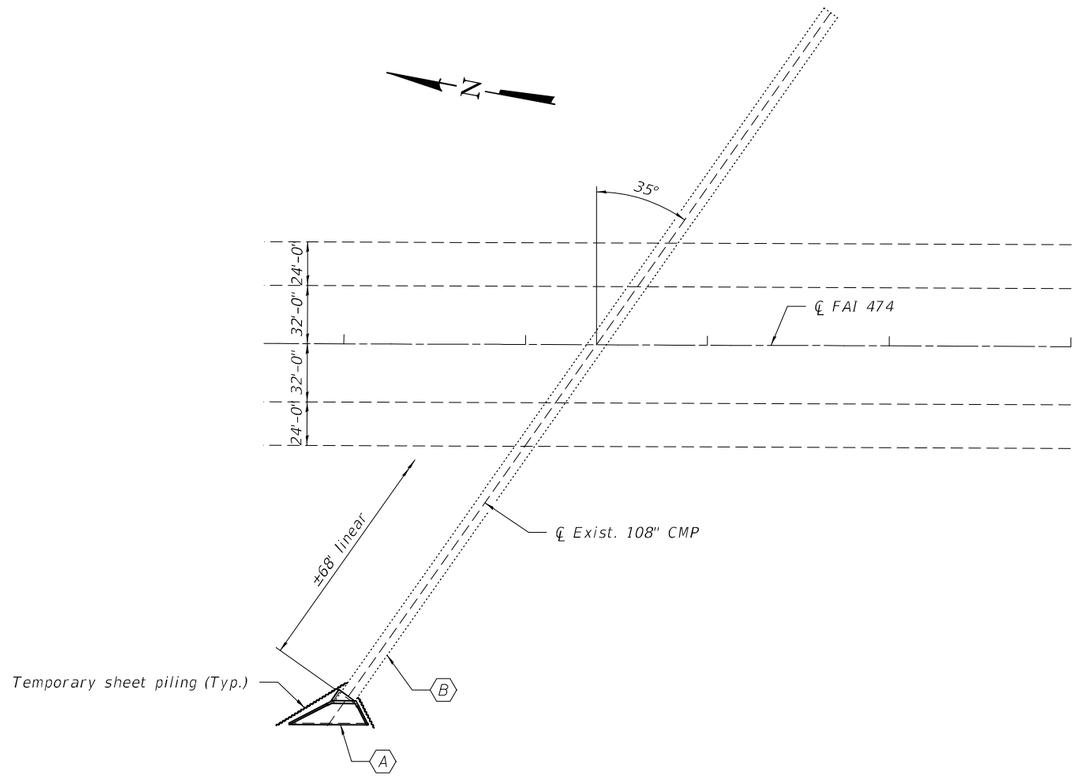
SCALE:	SHEET OF SHEETS	STA.	TO STA.	F.A. RTE.	SECTION / COUNTY / TOTAL SHEETS / SHEET NO.
					CONTRACT NO.
				ILLINOIS	FED. AID PROJECT

MODEL: Default
FILE NAME: pw:\\dot-pw.bentley.com:PWIDOT\Documents\IDOT Offices\District 4\Projects\104_68884\CADD\sts\CADD\sts\CADD\sheets\D468884-sht-bridge.dgn

BRIDGE PLANS
PAGE HOLDER

USER NAME = $USER$	DESIGNED -	REVISED -	**STATE OF ILLINOIS**
	DRAWN -	REVISED -	**DEPARTMENT OF TRANSPORTATION**
PLOT SCALE = 1:100	CHECKED -	REVISED -	
PLOT DATE = 7/1/2022	DATE -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		



GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

(A) - Remove and replace existing concrete end section and replace with a new cast in place concrete end section. (West end only)

(B) - Install a new culvert liner. Per material properties and installation requirements, see Art. 543 of the Standard Specifications. Estimated fill height = 50'-0".

LICENSED STRUCTURAL ENGINEER
JAYME F. SCHIFF
081-005540
STATE OF ILLINOIS

EXPIRES 11-30-2022

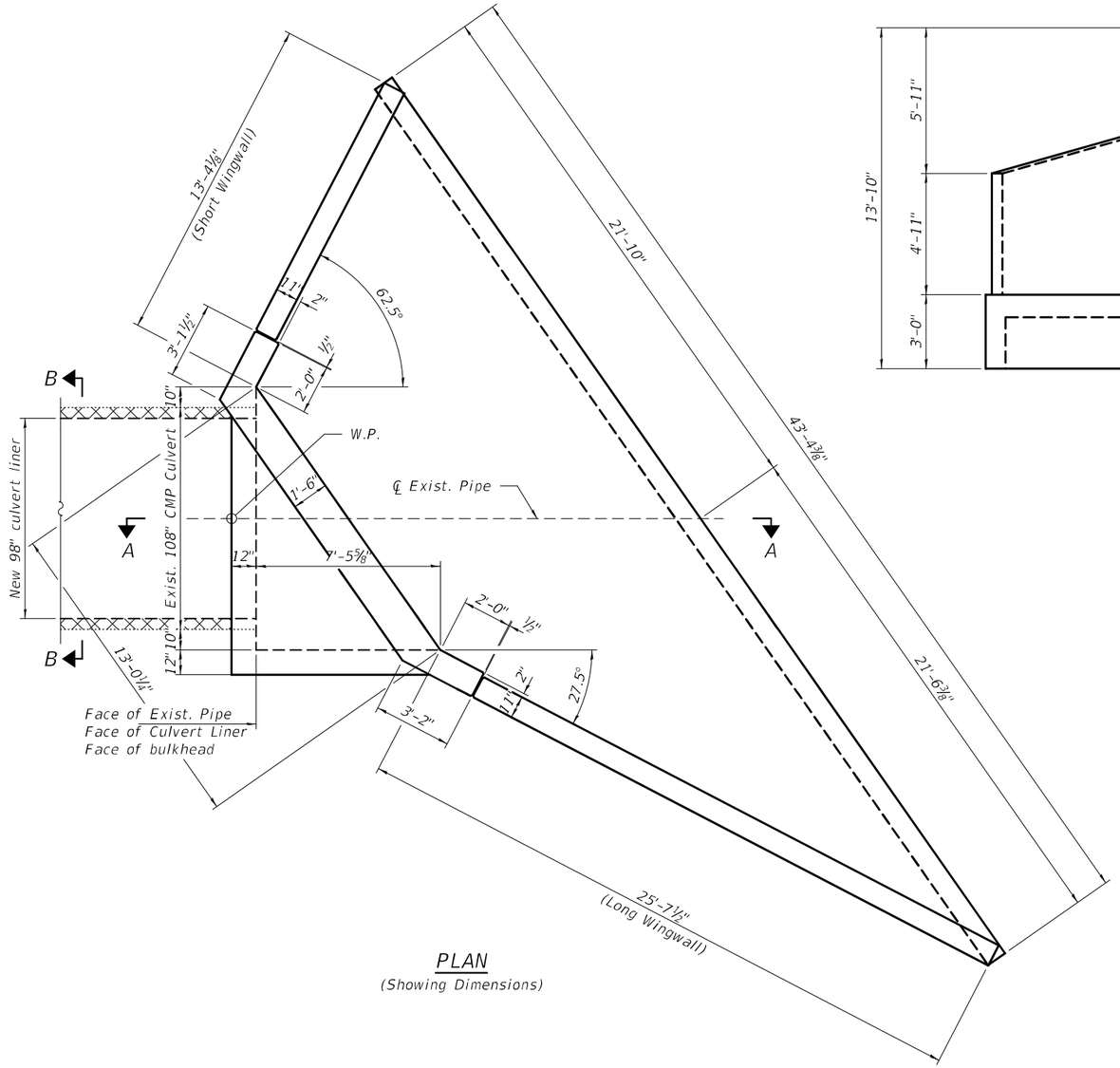
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Insertion Culvert Liner 108"	Foot	68
Concrete Removal	Cu. Yd.	22.8
Concrete Box Culvert	Cu. Yd.	42.6
Reinforcent Bars, Epoxy Coated	Pound	12850
Temporary Sheet Piling	Foot	660

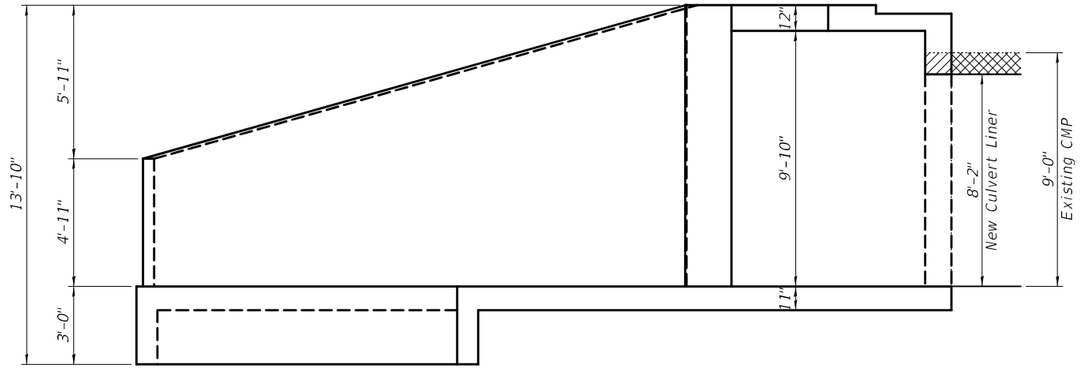
DESIGNED - Victor H. Veliz	EXAMINED	DATE - AUGUST 19, 2022	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**
CHECKED - Jeffrey S. Burke			
DRAWN - *daburdell*	PASSED	REVISED -	
CHECKED - VHV JSB	ENGINEER OF STRUCTURAL SERVICES ENGINEER OF BRIDGES AND STRUCTURES	REVISED -	

GENERAL PLAN
FAI 474 OVER PIPE CULVERT
SN 072-1008
SHEET NO. 1 OF 5 SHEETS

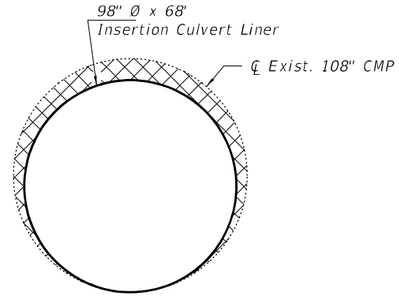
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	118
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		



PLAN
(Showing Dimensions)



SECTION A-A



SECTION B-B

Grout or cellular concrete.
See Standard Specifications.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
b(E)	12	#6	30'-2"	
b1(E)	12	#6	32'-11"	
b2(E)	4	#6	23'-2"	
b3(E)	2	#6	33'-5"	
b4(E)	2	#6	14'-4"	
b5(E)	14	#6	11'-11"	
d(E)	43	#4	4'-9"	
h(E)	28	#6	23'-2"	
h1(E)	13	#6	23'-7"	
h2(E)	2	#6	23'-11"	
h3(E)	28	#6	10'-11"	
h4(E)	13	#6	11'-0"	
h5(E)	2	#6	12'-4"	
h6(E)	3	#6	12'-10"	
h7(E)	3	#7	12'-10"	
h8(E)	11	#6	9'-0"	
h9(E)	10	#6	9'-2"	
h10(E)	11	#6	9'-8"	
h11(E)	8	#6	10'-6"	
h12(E)	18	#6	10'-2"	
h13(E)	22	#6	4'-2"	
h14(E)	11	#6	4'-9"	
h15(E)	6	#6	13'-4"	
h16(E)	16	#6	6'-2"	
h17(E)	8	#5	10'-9"	
h18(E)	14	#6	3'-2"	
h19(E)	4	#6	1'-10"	
s(E)	13	#5	4'-7"	
v(E)	84	#6	15'-0"	
v1(E)	84	#6	19'-8"	
v2(E)	84	#6	5'-10"	
v3(E)	84	#6	6'-6"	
w(E)	21	#6	13'-7"	
w1(E)	5	#6	31'-5"	
w2(E)	8	#6	31'-7"	
w(3E)	7	#6	15'-0"	
Concrete Box Culverts			Cu. Yd.	42.6
Reinforcement Bars, Epoxy Coated			Pound	12850

DESIGNED - VHV
CHECKED - JSB
DRAWN - *daburdell*
CHECKED - VHV JSB

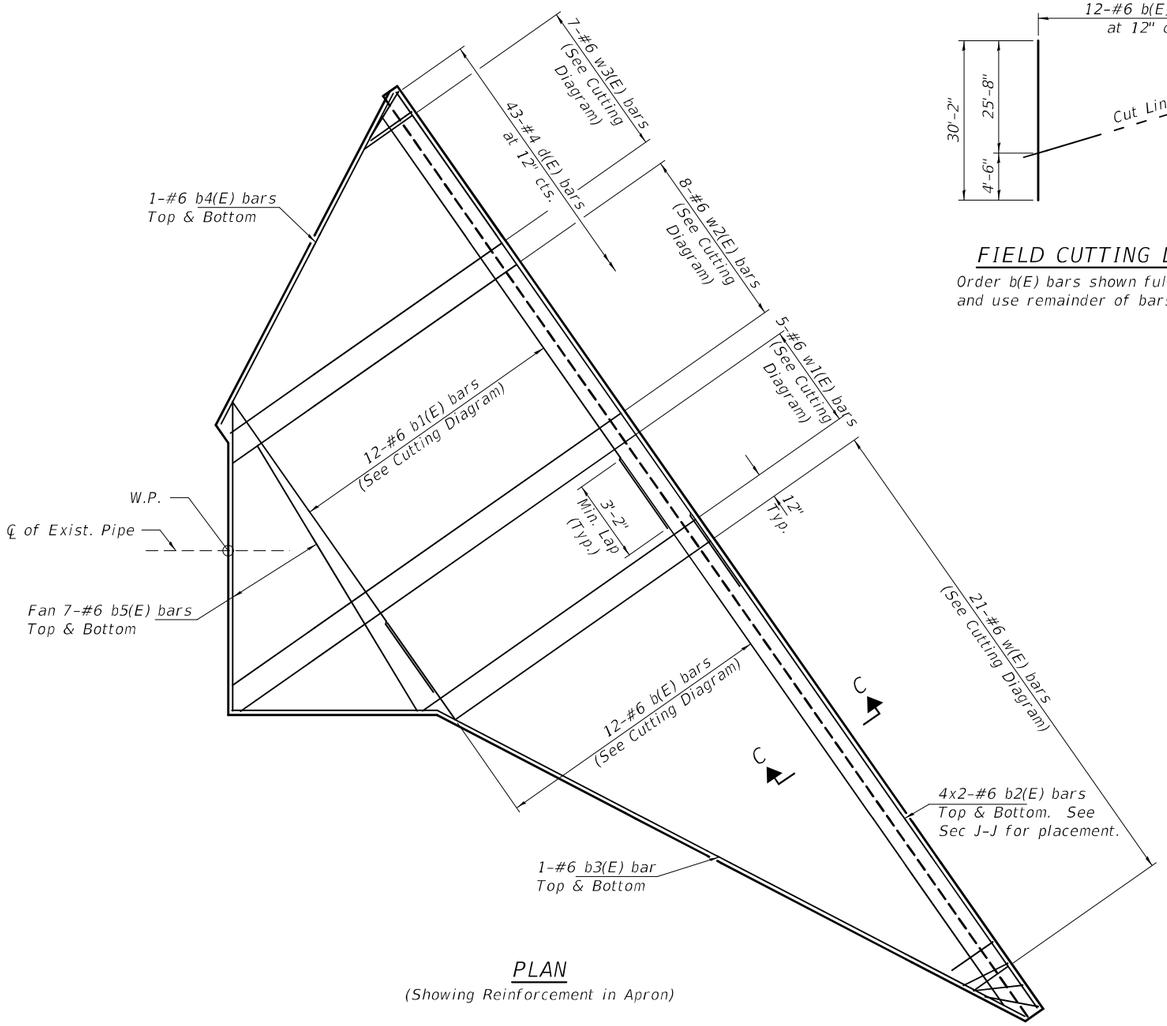
EXAMINED
Timothy A. Bridgewater
ENGINEER OF STRUCTURAL SERVICES
PASSED
Joyce F. Duffy
ENGINEER OF BRIDGES AND STRUCTURES

DATE - AUGUST 19, 2022
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS
SN 072-1008
SHEET NO. 2 OF 5 SHEETS

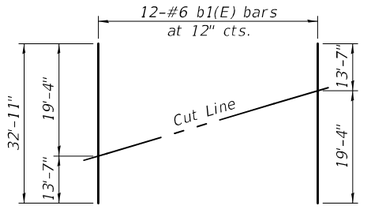
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	119
		CONTRACT NO. 68884		
	ILLINOIS FED. AID PROJECT			



PLAN
(Showing Reinforcement in Apron)

Note:
Bars indicated thus 4x2-#6 etc. indicates 4 line of bars with 2 lengths per line.

FIELD CUTTING DIAGRAM
Order b(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



FIELD CUTTING DIAGRAM
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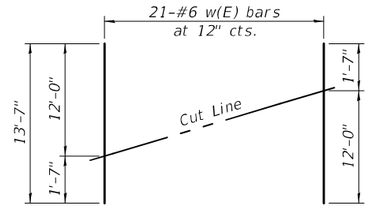
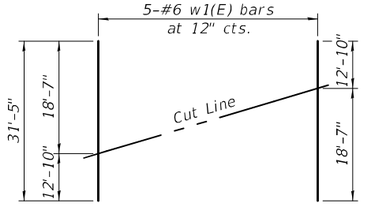
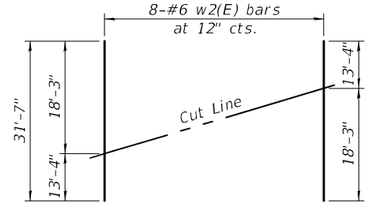


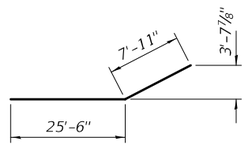
FIELD CUTTING DIAGRAM
Order w(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



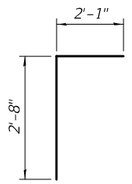
FIELD CUTTING DIAGRAM
Order w1(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



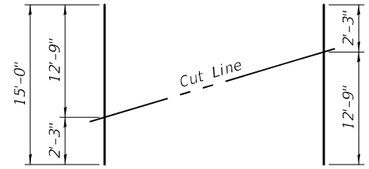
FIELD CUTTING DIAGRAM
Order w2(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



BAR b3(E)

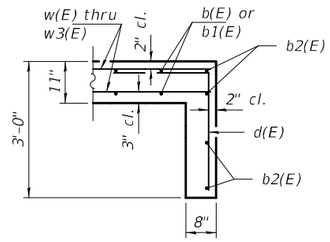


BAR d(E)



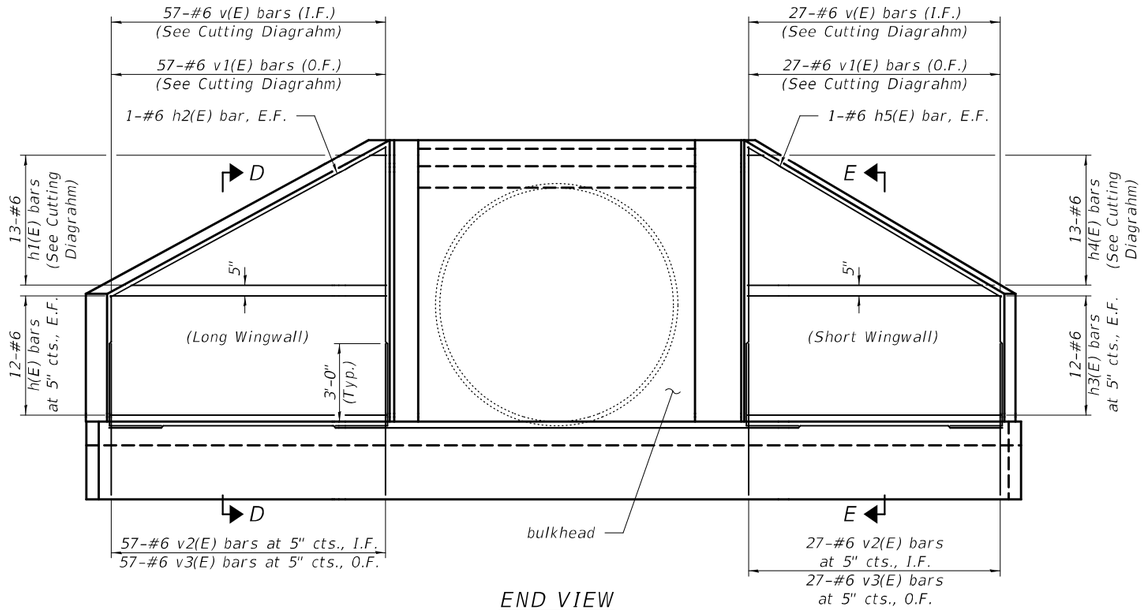
FIELD CUTTING DIAGRAM
Order w3(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.

MIN. BAR LAP
#6 - 3'-2"



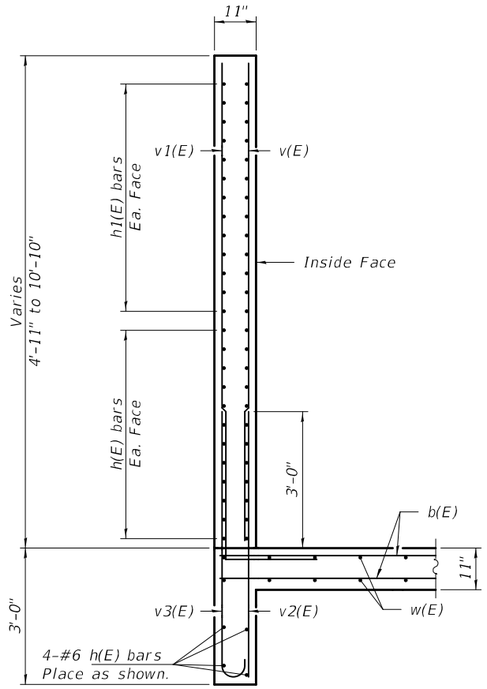
SECTION C-C

DESIGNED - VHV	EXAMINED	DATE - AUGUST 19, 2022	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CULVERT DETAILS - APRON SN 072-1008	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED - JSB	ENGINEER OF STRUCTURAL SERVICES				474	(72-3HB-2)BR	PEORIA	126	119A
DRAWN - dahurdell	PASSED	REVISED -		SHEET NO. 3 OF 5 SHEETS			CONTRACT NO. 68884		
CHECKED - VHV JSB	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -					ILLINOIS	FED. AID PROJECT	

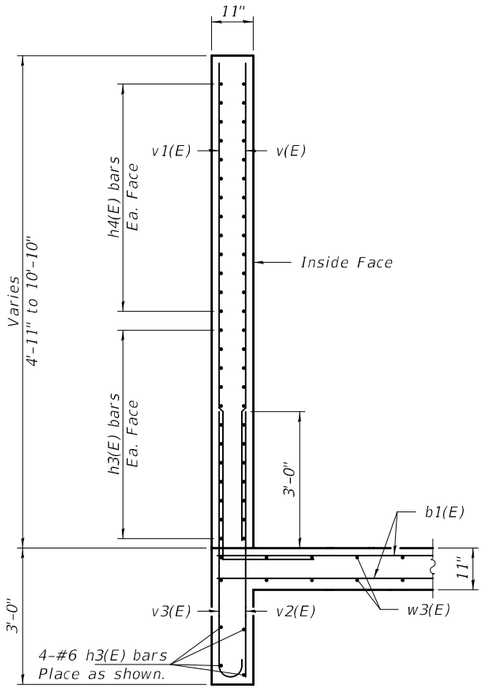


END VIEW
(Showing reinforcement in wingwalls only)

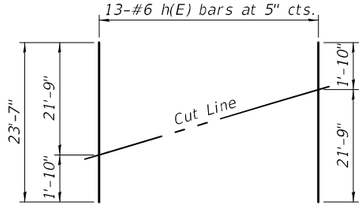
Note:
For bulkhead reinforcement, see End View on sheet 5 of 5.



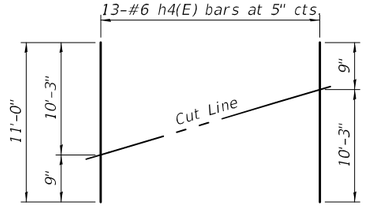
SECTION D-D
(Long Wingwall)



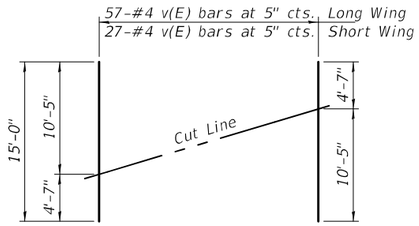
SECTION E-E
(Short Wingwall)



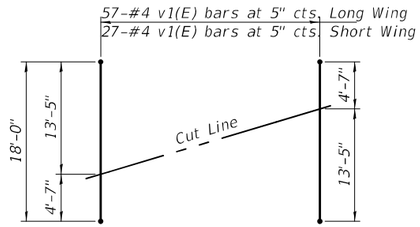
FIELD CUTTING DIAGRAM
Order h(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



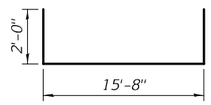
FIELD CUTTING DIAGRAM
Order h4(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



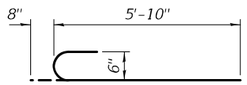
FIELD CUTTING DIAGRAM
Order v(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



FIELD CUTTING DIAGRAM
Order v1(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



BAR v1(E)

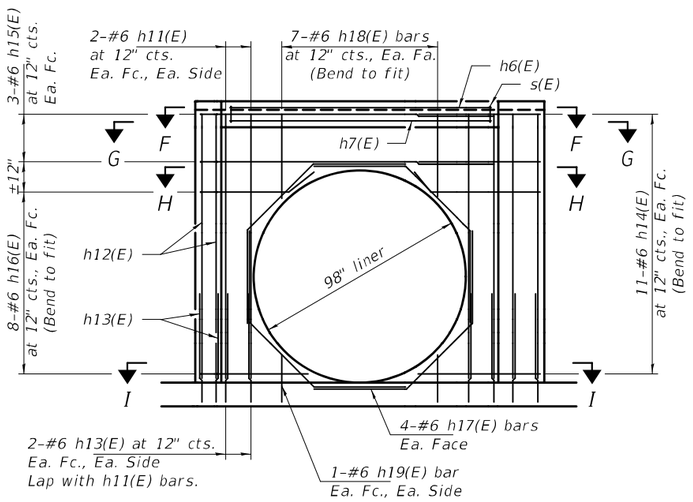


BAR v3(E)

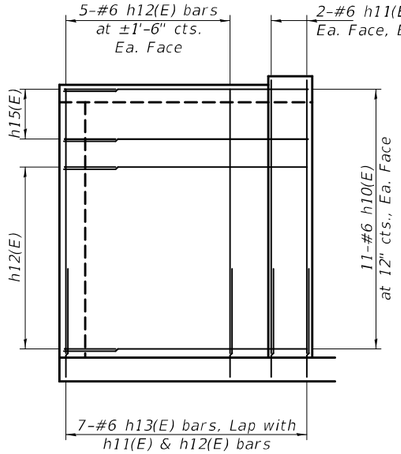
DESIGNED - VHV	EXAMINED	DATE - AUGUST 19, 2022	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**CULVERT DETAILS - WINGWALLS SN 072-1008**	
CHECKED - JSB					
DRAWN - *dahurdell*	PASSED	REVISED -		SHEET NO. 4 OF 5 SHEETS	
CHECKED - VHV JSB		REVISED -			

Timothy A. Daly
ENGINEER OF STRUCTURAL SERVICES
Joyce F. Duffy
ENGINEER OF BRIDGES AND STRUCTURES

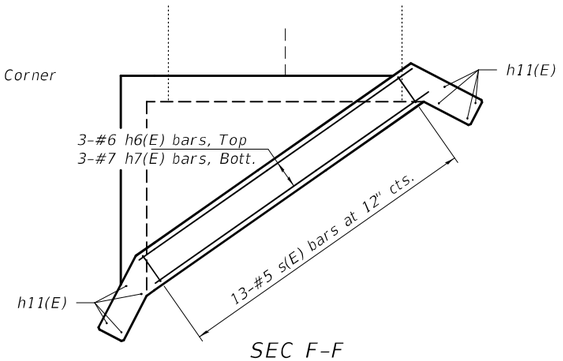
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	119B
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		



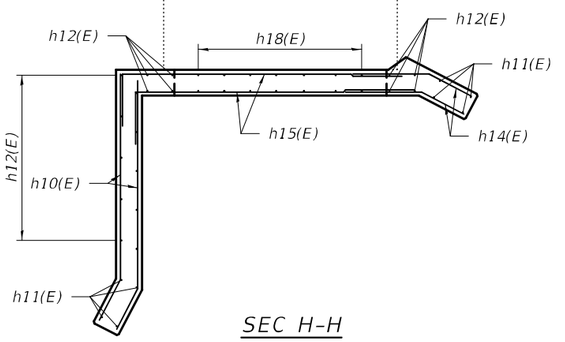
END VIEW



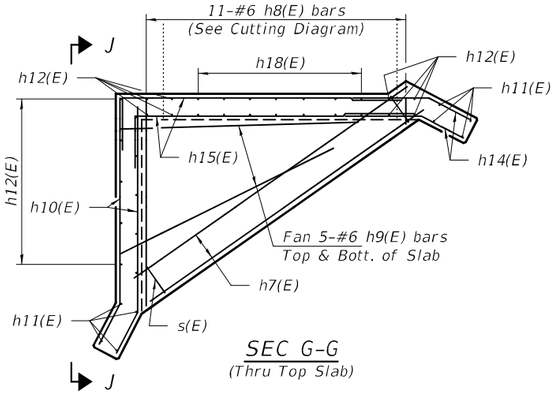
SEC J-J



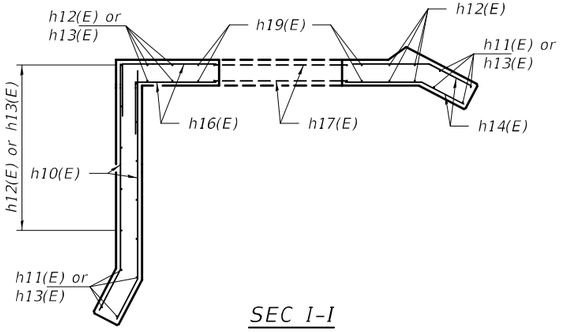
SEC F-F
(Thru Headwall)



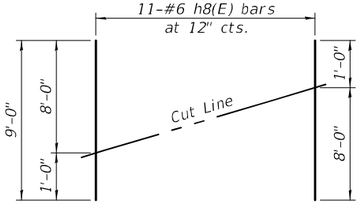
SEC H-H



SEC G-G
(Thru Top Slab)

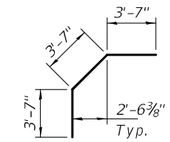


SEC I-I

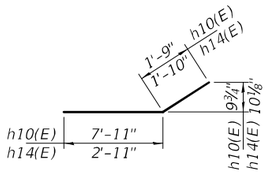


FIELD CUTTING DIAGRAM

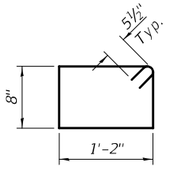
Order h8(E) bars shown full length. Cut as shown and use remainder of bars in opposite face.



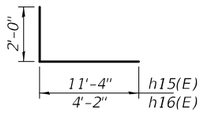
BAR h17(E)



BARS h10(E) & h14(E)



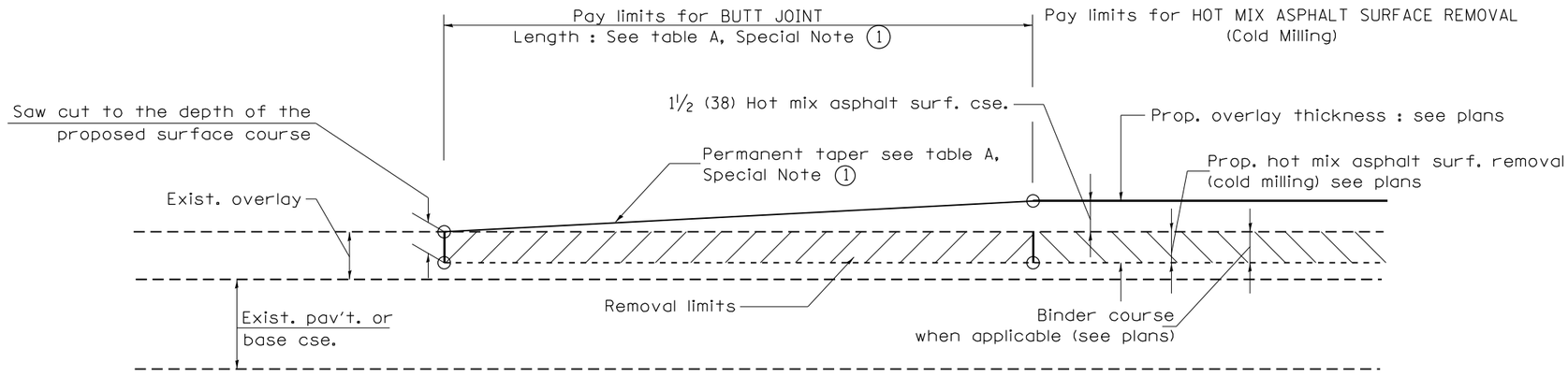
BAR s(E)



BARS h15(E) & h16(E)

DESIGNED - VHV	EXAMINED	DATE - AUGUST 19, 2022	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CULVERT DETAILS - BOX SN 072-1008	
CHECKED - JSB	ENGINEER OF STRUCTURAL SERVICES				
DRAWN - daburdell	PASSED	REVISED -		SHEET NO. 5 OF 5 SHEETS	
CHECKED - VHV JSB	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -			

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	119C
		CONTRACT NO. 68884		
	ILLINOIS	FED. AID PROJECT		



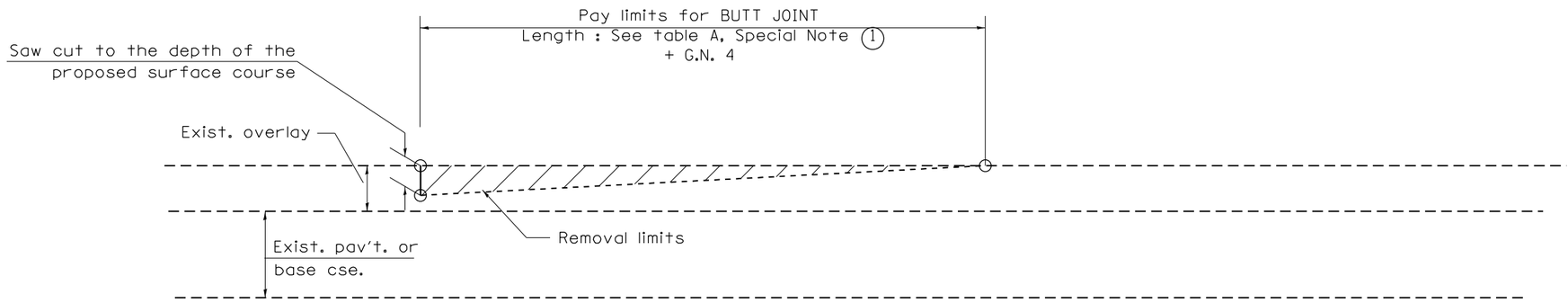
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

TABLE A
TAPER RATES

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
(1)	BUTT JOINT TAPER RATE	1:480	1:240
(2)	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.



CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.
04-01-97	CORRECTION TO DEPTH	J.A.
09-15-05	REVISED DESIGNER NOTE	M.M.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.
08-21-13	MAJOR MODIFICATIONS	R.D.
04-12-16	MINOR CORRECTIONS	R.D.
02-14-17	ADDED NOTE 5	R.D.
07-16-19	Wording and Spelling corrections	R.D.

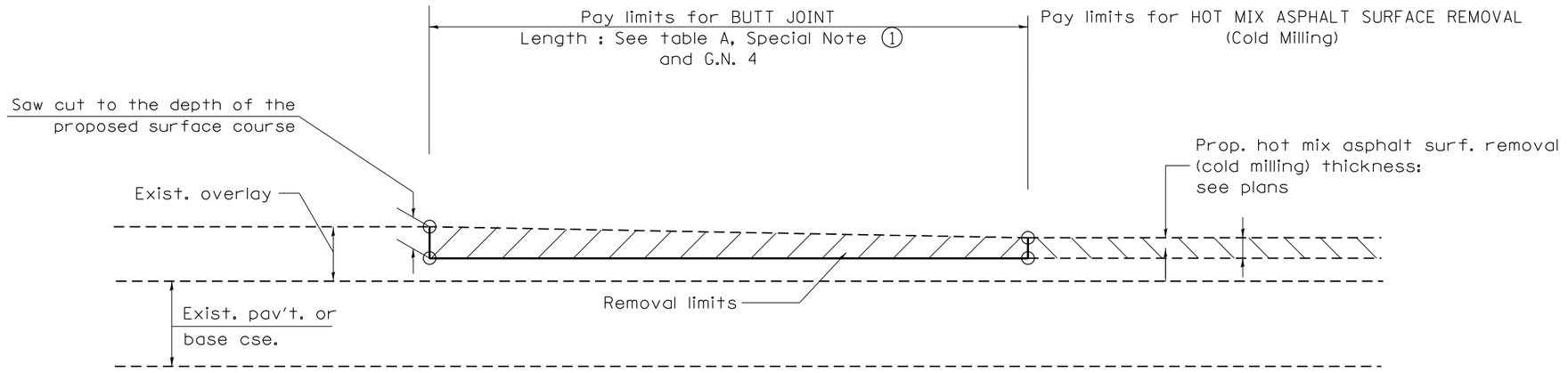
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

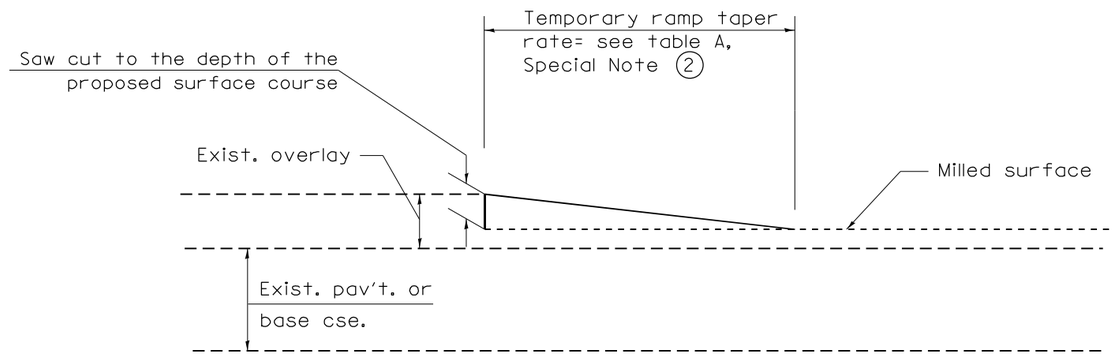
NOT TO SCALE

SHT. 1 OF 3
CADD STD. 406101-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	120
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



DETAIL TEMPORARY RAMP

All dimensions are in inches (millimeters) unless otherwise noted.

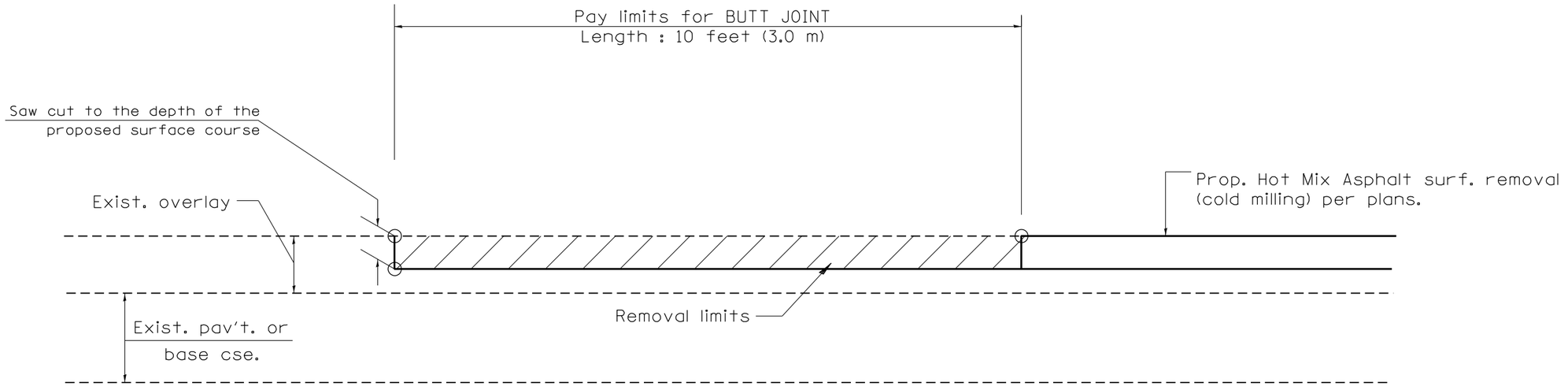
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

NOT TO SCALE

SHT. 2 OF 3
CADD STD. 406101-D4

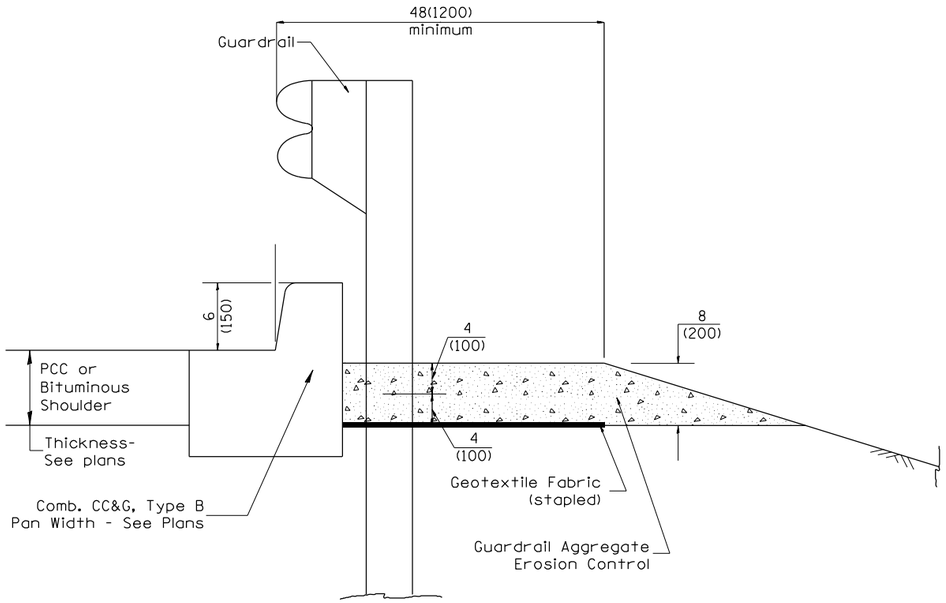
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	121
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



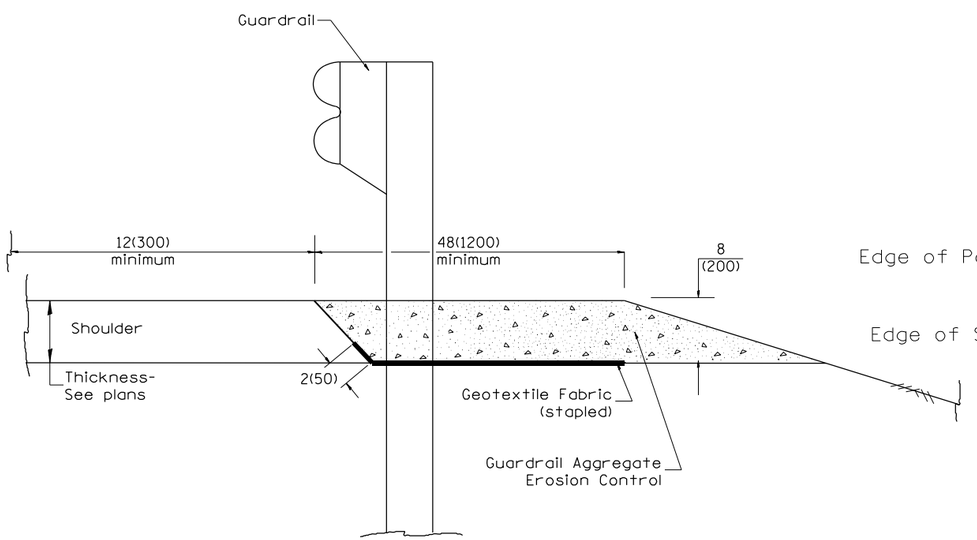
CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING) TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS					
	NOT TO SCALE	SHT. 3 OF 3 CADD STD. 406101-D4	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			474	(72-3HB-2)BR	PEORIA	126	122
					CONTRACT NO. 68884		
			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION WITH COMBINATION CONCRETE CURB & GUTTER



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.

2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.

3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.

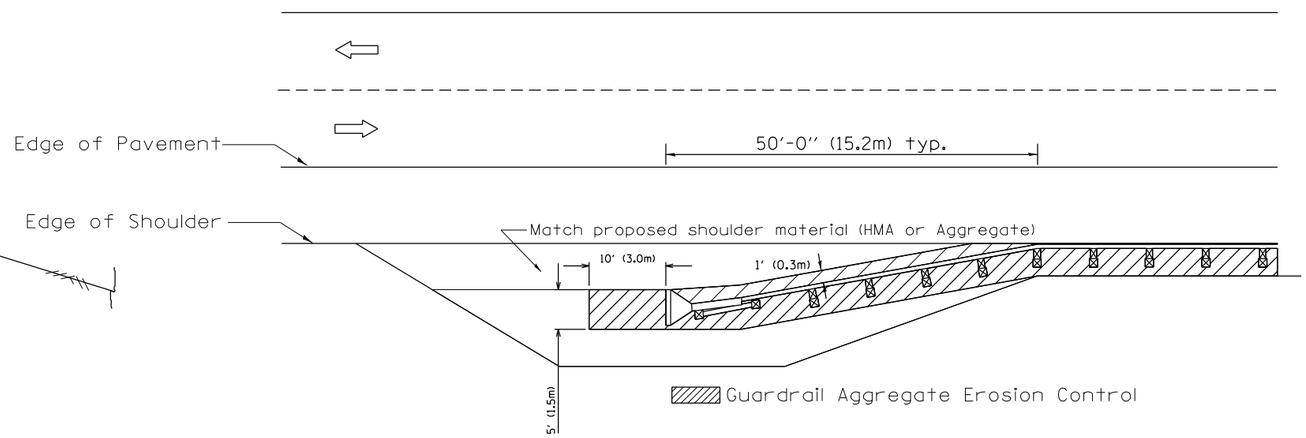
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.

5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.

6. Materials shall meet the following requirements:

   A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.

   B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



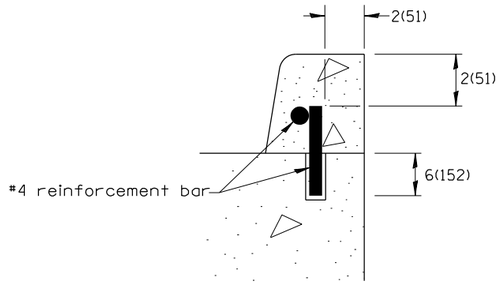
All dimensions are in inches (millimeters) unless otherwise noted.

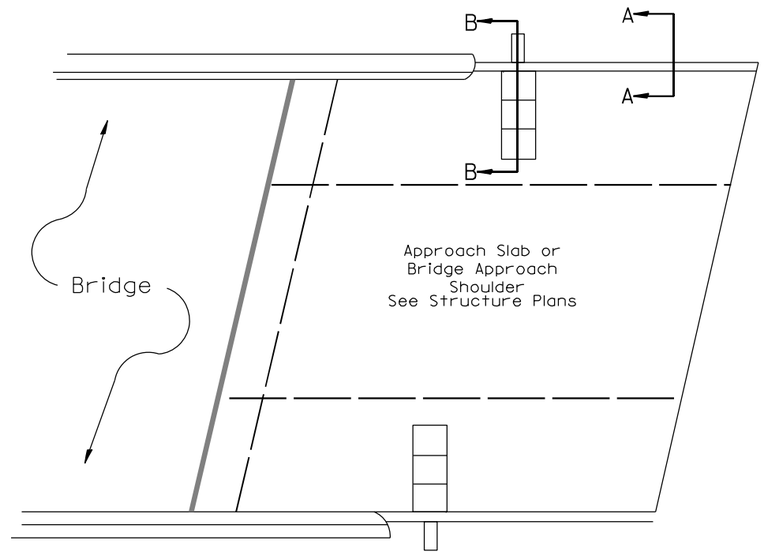
03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.	5-30-18	CHANGE B CURB TO CC&G	R.D.	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**GUARDRAIL EROSION CONTROL TREATMENTS**	SHT. 1 OF 2
08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.	07-16-19	SPELLING CORRECTIONS	R.D.		NOT TO SCALE	CADD STD. 630101-D4
07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.						
01-26-17	REVISED	R.D.						

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	123
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

DETAIL "A"

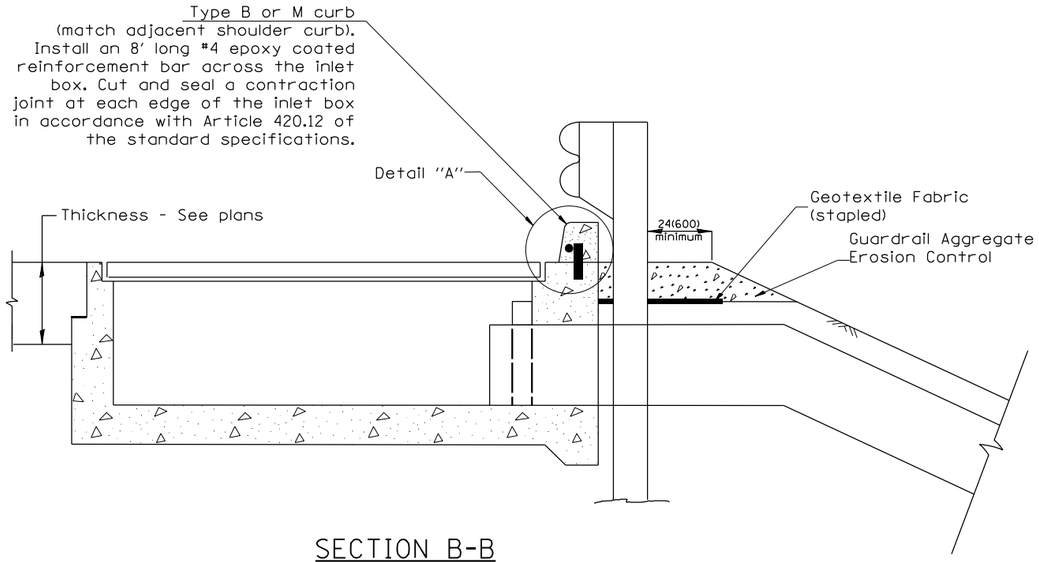
#4 reinforcement bars may be cast in PCC apron and precast inlet box or drilled and anchored with an approved chemical adhesive at 2' spacing. Cost of curb and reinforcement are included in the cost of shoulder inlet.





PLAN VIEW

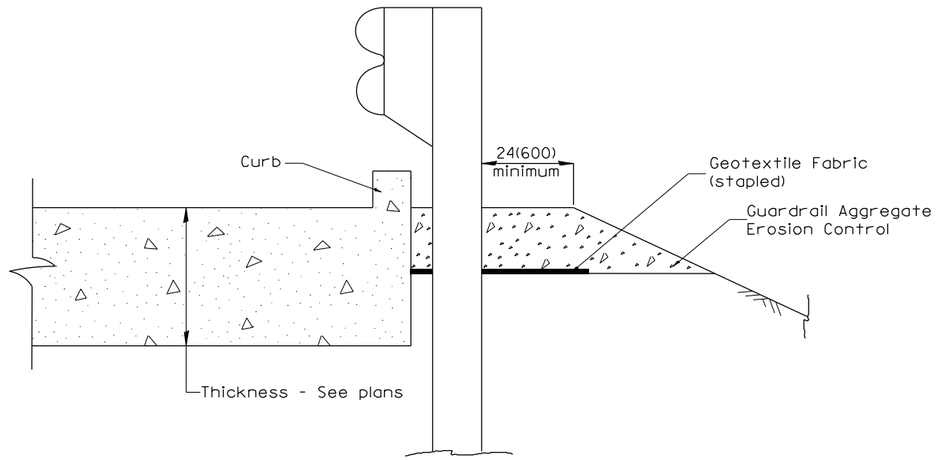
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B

TYPICAL SECTION AT INLETS

TYPE E, F & G (HIGHWAY STANDARD 610001)



SECTION A-A

TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

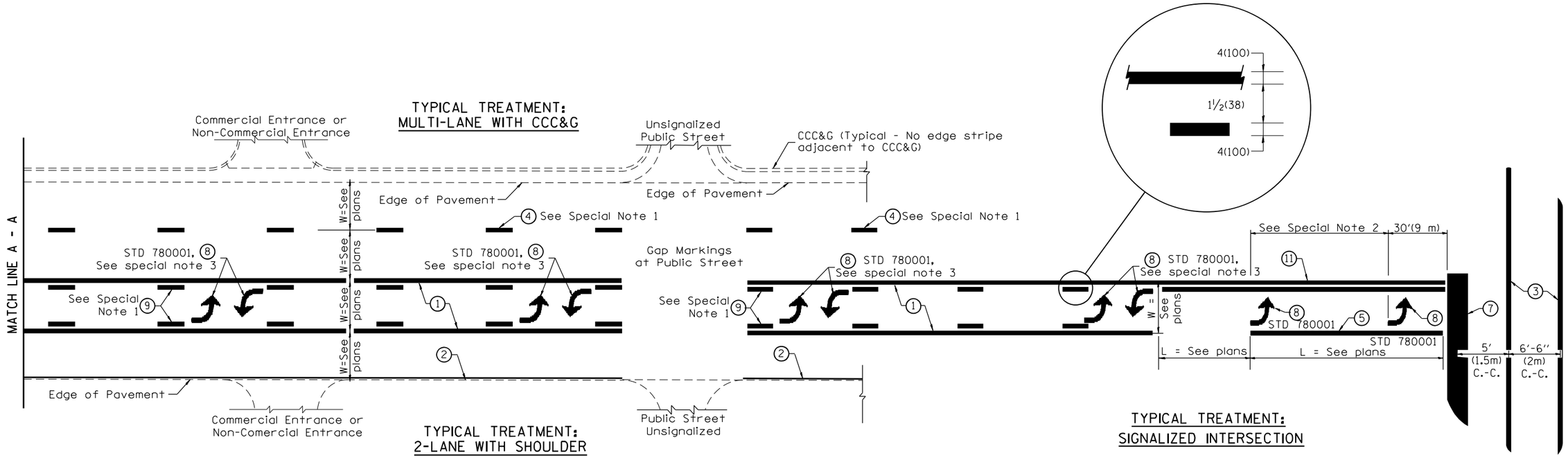
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GUARDRAIL EROSION CONTROL TREATMENTS

NOT TO SCALE

SHT. 2 OF 2
CADD STD. 630101-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	124
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend.
Some elements may not apply to specific project.)

1. 4(100) Solid (Yellow)
2. 4(100) Solid (White)
3. 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
   2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
4. 6(150) Skip-Dash (White) — 10' (3.05m) / 30' (9.14m) / 10' (3.05m) (See Special Note 1)
5. 8(200) Solid (White)
6. 12(300) Diagonal (White) (Item 6 is shown on Std. 780001)
7. 24(600) Stop Bar (White)
8. Letters & Arrows — 33' (10m) (See Std. 780001 and Special Notes 2 & 3)
9. 4(100) Skip-Dash (Yellow) — 10' (3.05m) / 30' (9.14m) / 10' (3.05m) (See Special Note 1)
10. 12(300) Diagonal (Yellow) (See Table A) — 45°
11. 4(100) Double Solid (Yellow) — 11(280) C.-C. — See Table A

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
   A. A minimum of two (2) arrows is required.
   B. The maximum spacing between arrows is 80' (24 m).
   C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
   A. A minimum of two (2) arrow pairs is required.
   B. The maximum spacing between arrow pairs is 200' (61 m).
   C. Arrow pairs shall be evenly spaced if three (3) or more are required.
   D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

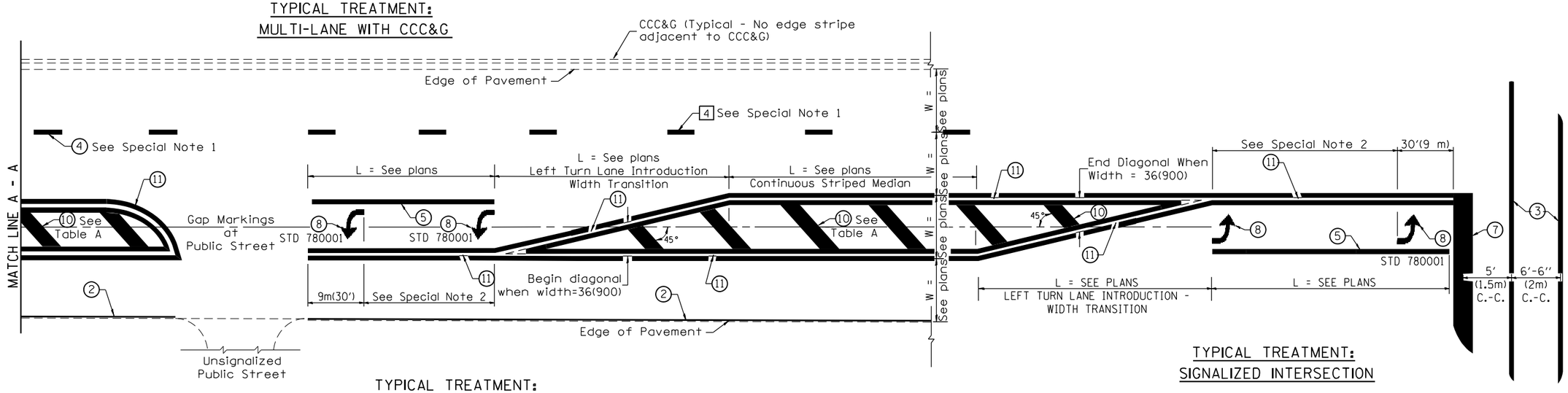
1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
   Through Arrow= 14.8 sq. ft.
   Large Left or Right Arrow= 21.9 sq. ft.
   2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
   Wrong Way Arrow= 29.5 sq. ft.
   Railroad Crossing Symbol= 69.8 sq. ft.
   (For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.	
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.	
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL PAVEMENT MARKINGS
NOT TO SCALE
SHT. 1 OF 2
CADD STD. 780001-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3H8-2)BR	PEORIA	126	125
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



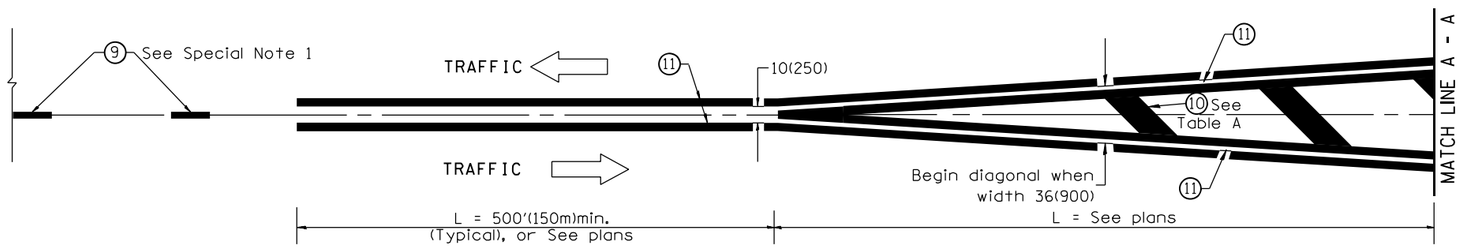
TYPICAL MEDIAN TRANSITIONS

FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A

RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	CONTINUOUS	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



MEDIAN INTRODUCTION - WIDTH TRANSITIONS

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL PAVEMENT MARKINGS

NOT TO SCALE

SHT. 2 OF 2
CADD STD. 780001-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	(72-3HB-2)BR	PEORIA	126	126
		CONTRACT NO. 68884		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		