

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDE, P.E. 847-705-4406 SCHAUMBURG, IL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2562	10-00104-00-RS	COOK	21	1
FED. ROAD DIST NO. 1 ILLINOIS		CONTRACT NO. 63500		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

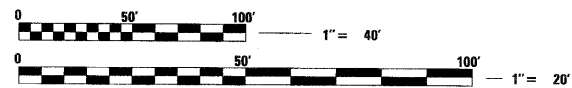
**FAU 2562 (SPRINGINGGUTH ROAD)  
FROM FAU 1338 (WISE ROAD)  
TO FAU 1322 (SCHAUMBURG ROAD)  
RESURFACING  
SECTION 10-00104-00-RS  
PROJECT NO. M-9003(703)  
COOK COUNTY  
VILLAGE OF SCHAUMBURG  
JOB #: C-91-761-10**



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701901-01	TRAFFIC CONTROL DEVICES

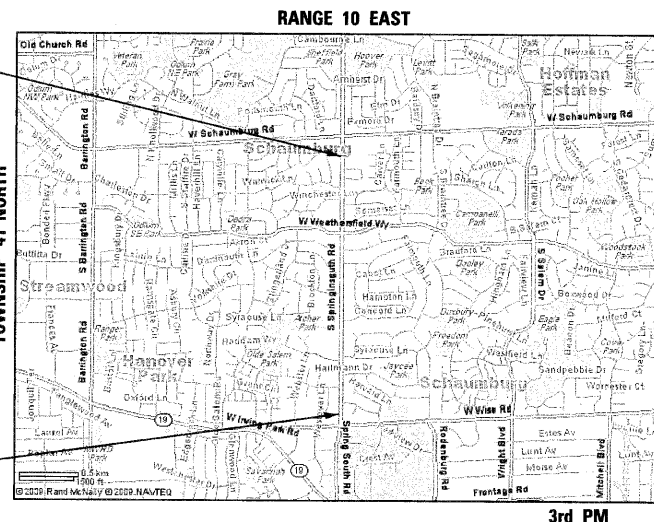


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

**ADT = 15,300 (2007)  
POSTED SPEED LIMIT = 30 MPH  
DESIGN SPEED = 30 MPH  
DESIGN DESIGNATION = URBAN COLLECTOR  
CONTRACT NO. 63500**

SPRINGINGGUTH ROAD  
IMPROVEMENT ENDS  
STATION 76 + 40



SPRINGINGGUTH ROAD  
IMPROVEMENT BEGINS  
STATION 5 + 63

LOCATION MAP  
SCALE: 1" = 2500'  
SCHAUMBURG TOWNSHIP  
GROSS LENGTH = 7077 FT. = 1.34 MILES  
NET LENGTH = 7077 FT. = 1.34 MILES

**PROJECT LOCATED IN THE  
VILLAGE OF SCHAUMBURG**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

APPROVED August 3, 2010  
*Steven R. Weinstock*  
STEVEN R. WEINSTOCK, PE  
DIRECTOR OF ENGINEERING / PUBLIC WORKS VILLAGE OF SCHAUMBURG

PASSED August 31, 2010  
*Charles F. Ridde*  
CHARLES F. RIDDE, P.E.  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW August 31, 2010  
*Diane M. O'Keefe*  
DIANE M. O'KEEFE, PE  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

DATE: 8-04-2010

MARK A. STOTT  
PROFESSIONAL ENGINEER  
STATE OF ILLINOIS  
NO. 0435

**HAMPTON, LENZINI AND RENWICK, INC.**  
CIVIL ENGINEERS • STRUCTURAL ENGINEERS • LAND SURVEYORS  
**HLR**  
380 SHEPARD DRIVE  
ELGIN, ILLINOIS 60123  
847.697.6700 www.hirengineering.com  
184.000950  
ILLINOIS PROFESSIONAL DESIGN FIRM L3 / PE / SE CORPORATION

EXPIRES: 11-30-2011 PROJECT NUMBER: 10.0038.330 DATE: 4/6/2010

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED JANUARY 1, 2010; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION; THE DETAILS IN THE PLANS; AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE PLANS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE, AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER IN ACCORDANCE WITH 105.07 AND 107.31.

STAKING

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, BACKS OF CURB, ETC. ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

SEWERS AND WATER MAINS

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT, UNLESS OTHERWISE NOTED IN THE PLANS.

THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF SCHLAUMBURG PUBLIC WORKS DEPARTMENT ONE WEEK IN ADVANCE OF ALL WATER MAIN SHUT DOWNS. UNDER NO CIRCUMSTANCE SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

SIGNS

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR, ENGINEER AND VILLAGE MAINTENANCE PERSONNEL SHALL INVENTORY THE LOCATION, SIZE, TYPE AND CONDITION OF ALL EXISTING SIGNS. ANY SIGN DAMAGED DURING CONSTRUCTION OR STORAGE SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REPLACE SIGNS WHICH INTERFERE WITH HIS CONSTRUCTION OPERATIONS AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS. THIS WORK WILL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

ALL WORK INVOLVING SIGNS SHALL BE GOVERNED BY THE FOLLOWING REQUIREMENTS:

1. SIGNS SHALL NOT BE MOVED UNTIL PROGRESS OF WORK NECESSITATES IT.
2. EVERY SIGN REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.
3. ALL SIGNS SHALL BE RE-ERECTED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. HORIZONTAL LOCATION FROM THE EDGE OF PAVEMENT SHALL BE AS DESIGNATED BY THE ENGINEER.
4. ALL UNUSED SIGNS WILL BE RETURNED TO THE VILLAGE OR COUNTY, AS APPLICABLE.
5. LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS.

MISCELLANEOUS

THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET ACCESS, EXISTING DRIVEWAY ACCESS, AND PEDESTRIAN ACCESS TO ADJUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT.

THIS WORK SHALL BE PERFORMED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER. THE CONTRACTOR SHALL CUT THE JOINT BETWEEN THE PORTION OF THE ITEM TO BE REMOVED AND THAT TO BE LEFT IN PLACE WITH A SAWING MACHINE TO PREVENT SPALLING WHEN THE ITEM IS BROKEN OUT. THIS WORK SHALL BE DONE IN A MANNER THAT A STRAIGHT AND PERPENDICULAR JOINT WILL BE SECURED. ALL SAW CUTTING SHALL BE TO THE FULL DEPTH OF THE PAVEMENT, DRIVEWAY, SIDEWALK, OR CURB TO BE REMOVED.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING ITEM BEING REMOVED AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.

THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEMS TO BE REMOVED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR SAWING REINFORCEMENT.

AT ALL BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF ONE AND ONE HALF (1½) INCHES. THE THICKNESSES OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF CONTRACT.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

ALL TYPE I AND II BARRICADES SHALL BE WEIGHTED DOWN WITH TWO SANDBAGS EACH. (ONE WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL). ALL TYPE III BARRICADES SHALL REQUIRE FOUR SANDBAGS EACH.

THE CONTRACTOR SHALL DISPOSE OF ALL SIDEWALK, CURB AND GUTTER, PAVEMENT, AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE AT HIS EXPENSE. ALL EXCESS EXCAVATED MATERIAL SHALL BE REMOVED FROM THE SITE EACH DAY. THIS SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

PAVEMENT MARKING PAINT

IN ADDITION TO THE REQUIREMENTS OF ARTICLE 105.09 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL FURNISH, AT HIS EXPENSE, WHITE, PINK OR PURPLE PAVEMENT MARKING PAINT IN AEROSOL CANS, FOR USE BY THE ENGINEER. THE CONTRACTOR AND SUBCONTRACTORS SHALL ONLY USE THESE SAME COLORS FOR THEIR OWN MARKINGS, THEREFORE NOT USING JULIE UTILITY COLORS.

PROTECTION OF EXISTING DRAINAGE FACILITIES DURING CONSTRUCTION

ALL EXISTING DRAINAGE STRUCTURES ARE TO BE KEPT FREE OF DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS. ALL WORK AND MATERIAL NECESSARY TO PREVENT ACCUMULATION OF DEBRIS IN THE DRAINAGE STRUCTURES WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT. ANY DEBRIS IN THE DRAINAGE STRUCTURES RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE CONTRACTOR'S OWN EXPENSE, AND NO EXTRA COMPENSATION WILL BE ALLOWED. SHOULD RECONSTRUCTION OR ADJUSTMENT OF A DRAINAGE STRUCTURE BE REQUIRED BY THE ENGINEER IN THE FIELD, THE NECESSARY WORK AND PAYMENT SHALL BE DONE IN ACCORDANCE WITH SECTION 602 AND ARTICLE 104.02 RESPECTIVELY OF THE STANDARD SPECIFICATIONS.

DURING CONSTRUCTION, IF THE CONTRACTOR'S FORCES ENCOUNTER OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS, OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, THEY SHALL INFORM THE ENGINEER. THE ENGINEER SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE, AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE DONE IN ACCORDANCE WITH SECTIONS 550 AND 601 AND ARTICLE 104.02 RESPECTIVELY OF THE STANDARD SPECIFICATIONS.

BITUMINOUS MATERIALS (PRIME COAT)

PRIME COAT SHALL MEET THE SPECIFICATIONS OF ARTICLE 406.05(B) OF THE STANDARD SPECIFICATIONS WITH THE FOLLOWING REVISIONS AND ADDITIONS: PRIME COAT SHALL BE APPLIED AT A RATE OF 0.1 GALLON PER SQUARE YARD. BITUMINOUS MATERIAL SHALL BE SS 1, UNLESS OTHERWISE REQUIRED BY ARTICLE 403.05 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR WILL BE REQUIRED TO PRESENT A WEIGHT TICKET OF THE TRUCKLOAD PRIOR TO APPLYING THE PRIME COAT. AFTER APPLICATION OF THE TRUCK SHALL THEN BE WEIGHED AGAIN IN ORDER TO DETERMINE THE NET WEIGHT OF PRIME COAT THAT HAS BEEN PLACED. BOTH TICKETS SHALL BE STAMPED BY THE CERTIFIED WEIGH MASTER.

THE CONTRACTOR SHALL ERECT (TO THE ENGINEER'S SATISFACTION) 48" BY 48" FRESH OIL AHEAD SIGNS 24 HOURS PRIOR TO THE PRIME COAT APPLICATION, WHICH SIGNS SHALL REMAIN UNTIL THE PRIME COAT HAS ADEQUATELY CURED. THE FRESH OIL AHEAD SIGNS SHALL BE INSTALLED IN ADVANCE OF THE AREA TO BE PRIMED. THIS INCLUDES SIGNS ON ADJACENT STREETS, AND NO PARKING SIGNS SHALL BE PLACED THROUGHOUT THE AREA TO BE PRIMED.

THE CONTRACTOR SHALL CONTACT THE VILLAGE 72 HOURS IN ADVANCE OF BEGINNING WORK.

THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR BITUMINOUS MATERIALS (PRIME COAT).

AGGREGATE (PRIME COAT)

THIS WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE 406.05(B) OF THE STANDARD SPECIFICATIONS INSOFAR AS APPLICABLE AND THE FOLLOWING PROVISIONS.

THE FINE AGGREGATE SHALL BE MECHANICALLY SPREAD AT A UNIFORM RATE OF 2 POUNDS PER SQUARE YARD.

INCIDENTAL HOT-MIX ASPHALT SURFACING

THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS INSOFAR AS APPLICABLE AND THE FOLLOWING PROVISIONS.

THIS ITEM IS TO BE USED AS: TEMPORARY ASPHALT RAMPS DURING STAGE CONSTRUCTION; TEMPORARY PATCHES ON ALL SEWER OR WATER MAIN TRENCHES; TEMPORARY ASPHALT RAMPS PLACED AROUND PROTRUDING FRAMES AND LIDS PRIOR TO THE PLACEMENT OF THE FINAL ASPHALT CONCRETE SURFACE COURSE; OR AS DIRECTED BY THE ENGINEER.

PAYMENT FOR THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, WHICH PRICE SHALL INCLUDE ALL MATERIAL, LABOR, AND EQUIPMENT NECESSARY FOR A COMPLETE INSTALLATION.

CLEANING EXISTING DRAINAGE STRUCTURES

ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED OR RECONSTRUCTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR AND THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER SHALL INSPECT ALL STRUCTURES PRIOR TO CONSTRUCTION, NOTING THE EXISTING CONDITION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).

FRAMES AND GRATES OR FRAMES AND LIDS

THIS WORK SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 604 OF THE STANDARD SPECIFICATIONS.

A NEENAH FRAME AND OPEN LID, R-2040, OR APPROVED EQUAL, SHALL BE USED FOR FRAMES AND LIDS, TYPE 1, OPEN LID. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR FRAMES AND LIDS, TYPE 1, OPEN LID, WHICH SHALL INCLUDE REMOVAL AND DISPOSAL OF EXISTING CASTING.

AN EAST JORDAN HEAVY-DUTY 7510 CURB INLET WITH TYPE M1 GRATE AND T1 BACK (PER IDOT STANDARD 604051 WITH THE ADDITION OF THE CURB BACK LETTERED DUMP NO WASTE AND WITH A CAST-IN FISH LOGO) SHALL BE USED FOR FRAMES AND GRATES, TYPE 11, SPECIAL. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR FRAMES AND GRATES, TYPE 11, SPECIAL, WHICH SHALL INCLUDE THE REMOVAL AND DISPOSAL OF THE EXISTING CASTING.

DETECTABLE WARNINGS

THE MANUFACTURER FOR THE DETECTABLE WARNINGS SHALL BE ENGINEERED PLASTICS INC. (800-682-2525) OR APPROVED EQUAL

CONCRETE BREAKERS

WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS THAT MAY DAMAGE UNDERGROUND PUBLIC AND/OR PRIVATE UTILITIES WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

THE CONTRACTOR IS PROHIBITED FROM BREAKING UP CONCRETE BY DROPPING IT ON PAVEMENT OR IN ANY OTHER MANNER THAT, IN THE OPINION OF THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER, MAY DAMAGE EXISTING OR PROPOSED PAVEMENTS OR OTHER ROADWAY APPURTENANCES.

DRIVEWAY ACCESS

THE CONTRACTOR SHALL, WHERE REQUIRED BY THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER, PROVIDE IMMEDIATE ACCESS TO DRIVEWAYS AND INTERSECTING STREETS. THE CONTRACTOR SHALL AT ALL TIMES PROVIDE ACCESS FOR EMERGENCY VEHICLES DURING THE TIME OF CONSTRUCTION.

ANY DRIVEWAY APRON ADJACENT TO THE CURB AND GUTTER THAT IS REMOVED OR DISTURBED SHALL BE RESTORED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER AFTER THE NEW CURB AND GUTTER HAS BEEN CONSTRUCTED. THE CONTRACTOR MUST SCHEDULE THIS WORK SO THAT ONLY ONE SIDE OF ANY STREET WILL BE UNDER CONSTRUCTION AT ANY ONE TIME. IN NO CASE SHALL AN OPEN EXCAVATION CAUSED BY REMOVAL OF EXISTING CURB AND GUTTER, DRIVEWAY, OR SIDEWALK, WHETHER FORMED OR NOT FORMED, REMAIN OPEN MORE THAN 3 WORKING DAYS.

PRIOR TO REMOVING ANY DRIVEWAY, THE CONTRACTOR SHALL PROVIDE THE VILLAGE SUFFICIENT TIME TO PROVIDE 24 HOURS' ADVANCE WRITTEN NOTICE TO THE RESIDENT/OWNER OF THE DRIVEWAY, ALLOWING THE RESIDENT/OWNER TIME TO REMOVE ANY VEHICLES. ACCESS SHALL BE RESTORED NO LESS THAN 4 DAYS AFTER CURB AND GUTTER HAS BEEN PLACED. IF NECESSARY, THE CONTRACTOR SHALL PLACE TEMPORARY AGGREGATE BEHIND THE NEW CURB AND GUTTER UNTIL THE DRIVEWAY IS RESTORED. THIS TEMPORARY AGGREGATE SHALL BE CONSIDERED AS INCLUDED IN THE DRIVEWAY REPLACEMENT PAY ITEM.

DATE	BY	REVISIONS	CHECKED
		PLOTTED	FILED
		ALIGNMENT	FILED
		NOTE BOOK	NO.
		PLAN	NO.

DATE	BY	REVISIONS	CHECKED
		PLOTTED	FILED
		ALIGNMENT	FILED
		NOTE BOOK	NO.
		PROFILE	NO.

FILE NAME -	USER NAME -	DESIGNED -	REVISED -	<p align="center"><b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b></p> 	<p align="center">SPRINGINGUTH ROAD IMPROVEMENTS GENERAL NOTES AND LEGEND</p>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -			2562	10-00104-00-RS	COOK	21	2
PLOT SCALE -		CHECKED -	REVISED -			SPRINGINGUTH ROAD		CONTRACT NO. 63500		
PLOT DATE -		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT W-90037(03)		
					SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS STA.		TO STA.		

PLAN	DESIGNED	DATE
	CHECKED	
	PLATTED	
	ALIGNMENT CHECKED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	FILE NAME	

PROFILE	DESIGNED	DATE
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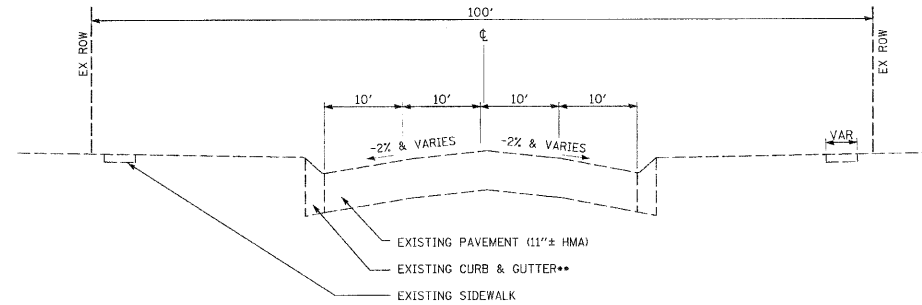
**SUPPLEMENTAL LEGEND**  
SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- 211 STREET ADDRESS
- TUD TELEPHONE CABLE (TUC) OR DUCT (TUD)
- SL EXISTING STREET LIGHTING CABLE
- PROPOSED STREET LIGHTING OR TRAFFIC SIGNAL CABLE
- ===== EXISTING CURB OR CURB & GUTTER
- ===== PROPOSED CURB OR CURB & GUTTER
- [Hatched Box] EXISTING CURB, CURB & GUTTER, DRIVEWAY PAVEMENT AND SIDEWALK TO BE REMOVED
- [Hatched Box] EXISTING CURB, CURB & GUTTER, DRIVEWAY PAVEMENT AND SIDEWALK TO BE REPLACED
- [Cross-hatched Box] CLASS D PATCHES
- [Dotted Box] HOT-MIX ASPHALT SURFACE REMOVAL

STRUCTURE ADJUSTMENT/RECONSTRUCTION/REMOVAL NOTATION

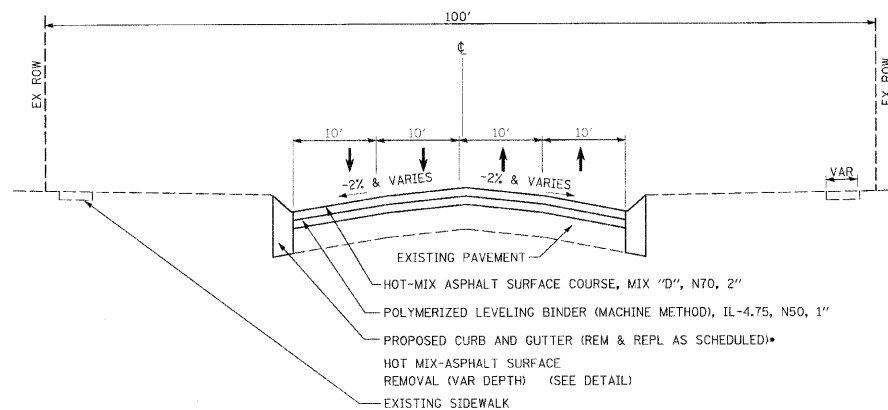
"A" FOR ADJUST "C" FOR CLOSED  
"R" FOR RECONSTRUCT "P" FOR OPEN

- FRAME & GRATE/LID TYPE
- [Symbol] DENOTES STRUCTURE TO BE REMOVED
  - [Symbol] DENOTES STRUCTURE TO BE FILLED



\*\* NORTH OF WEATHERSFIELD WAY, TYPE M-3.12  
SOUTH OF WEATHERSFIELD WAY, TYPE B-6.12

**EXISTING TYPICAL SECTION**  
SPRINGSGUTH ROAD  
STA 1+00 TO 80+00



\* NORTH OF WEATHERSFIELD WAY, TYPE B (MODIFIED)  
SOUTH OF WEATHERSFIELD WAY, TYPE B-6.12

**PROPOSED TYPICAL SECTION**  
SPRINGSGUTH ROAD  
STA 5+63 TO 76+40

CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 2" (IL 9.5mm)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"	4% @ 50 GYR.
<b>DRIVEWAY</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm)	4% @ 50 GYR.
<b>PATCHING</b>	
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR PG 70-22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAPP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME =	USER NAME =	DESIGNED --	REVISED --	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		SPRINGSGUTH ROAD IMPROVEMENTS TYPICAL SECTIONS	F.A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN --	REVISED --				2562	10-00104-00-RS	COOK	21	3
PLOT SCALE =		CHECKED --	REVISED --				SPRINGSGUTH ROAD				CONTRACT NO. 63500
PLOT DATE =		DATE --	REVISED --				FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-90637031		

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

Code No.	Items	Unit	Total Quantity	Roadway 1000
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100	100
<del>Z0042002</del>	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	100	100
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3355	3355
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	42	42
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	42	42
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	42	42
25200100	SODDING	SQ YD	3355	3355
25200200	SUPPLEMENTAL WATERING	UNIT	40	40
28000510	INLET FILTERS	EACH	47	47
35101400	AGGREGATE BASE COURSE, TYPE B	TON	68	68
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	28	28
40600300	AGGREGATE (PRIME COAT)	TON	68	68
<del>X406026</del>	POLYMERIZED LEVELING BINDER(MACHINE METHOD), IL-4.75, N50	TON	2028	2028
40600895	CONSTRUCTION TEST STRIP	EACH	1	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINT	SQ YD	286	286
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	25	25
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4056	4056
42300100	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 5 INCH	SQ YD	624	624
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	13621	13621
42400800	DETECTABLE WARNINGS	SQ FT	712	712
<del>X4401198</del>	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	33805	33805
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	764	764
44000500	COMBNATION CURB AND GUTTER REMOVAL	FOOT	5527	5527
44000600	SIDEWALK REMOVAL	SQ FT	15081	15081
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	323	323
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	396	396

Code No.	Items	Unit	Total Quantity	Roadway 1000
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	2440	2440
60202405	CATCH BASIN, TYPE A, 4'- DIAMETER	EACH	1	1
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	3	3
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2
<del>X6030310</del>	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	29	29
60404800	FRAMES AND GRATES, TYPE 11	EACH	31	31
60406000	FRAMES AND LIDS, TYPE 1,OPEN LID	EACH	2	2
60406100	FRAMES AND LIDS, TYPE 1,CLOSED LID	EACH	2	2
60500060	REMOVING INLETS	EACH	1	1
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1594	1594
60611610	COMBINATION CONCRETE CURB AND GUTTER, TYPE B(MODIFIED)	FOOT	3933	3933
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3
67100100	MOBILIZATION	L SUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2560	2560
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	640	640
* 70300220	TEMPORARY PAVEMENT MARKING LINE 4"	FOOT	18072	18072
* 78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	210	210
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	16140	16140
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	124	124
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	516	516
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	150	150
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	400	400
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51	51
△ Z0076600	TRAINEES	HOUR	500	500

\* SPECIALTY ITEMS △ Y080

PLAN  
 DESIGNED  
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 ALIGNMENT  
 CHECKED  
 NOTE BOOK  
 NO.  
 FILE NAME

PROFILE  
 DESIGNED  
 CHECKED  
 PLOTTED  
 GRADES  
 CHECKED  
 NOTE BOOK  
 NO.  
 STRUCTURE  
 NOTATIONS  
 CHECKED

FILE NAME =	USER NAME =	DESIGNED --	REVISED --
		DRAWN --	REVISED --
		CHECKED --	REVISED --
		DATE --	REVISED --

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION



SPRINGINGUTH ROAD IMPROVEMENTS  
 SUMMARY AND SCHEDULES

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 4
SPRINGINGUTH ROAD		CONTRACT NO. 63500		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT M-9003(703)		

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 REVIEWED \_\_\_\_\_  
 PLAN \_\_\_\_\_  
 CHECKED \_\_\_\_\_  
 DATE \_\_\_\_\_

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 REVIEWED \_\_\_\_\_  
 PROFILE \_\_\_\_\_  
 CHECKED \_\_\_\_\_  
 DATE \_\_\_\_\_

**ADA**

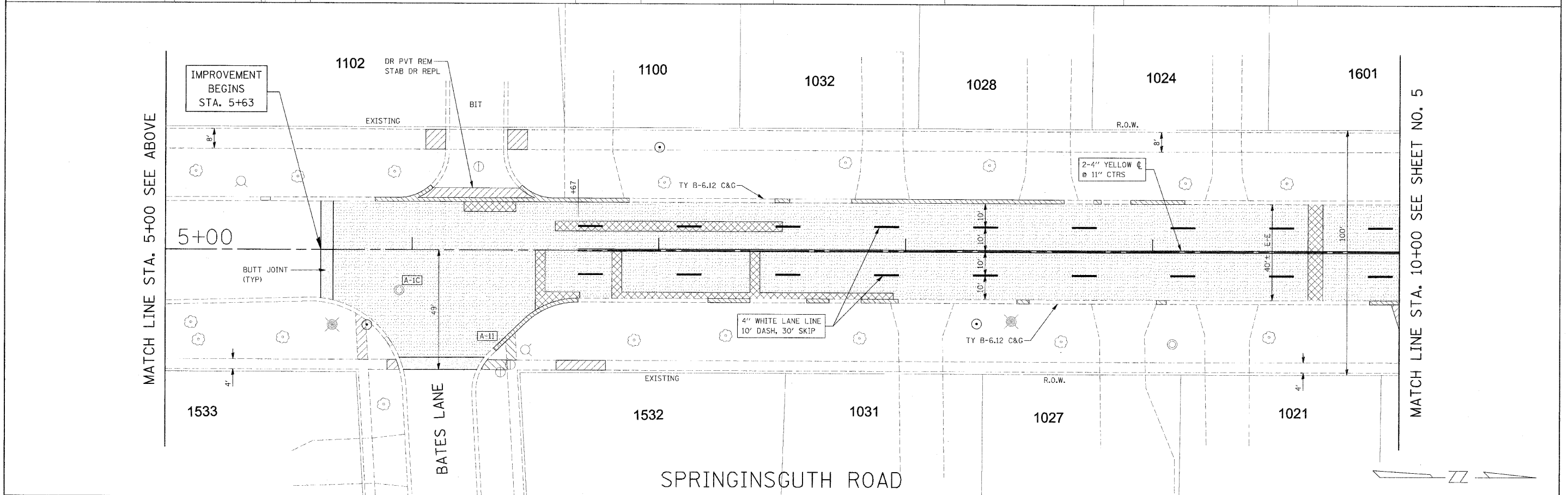
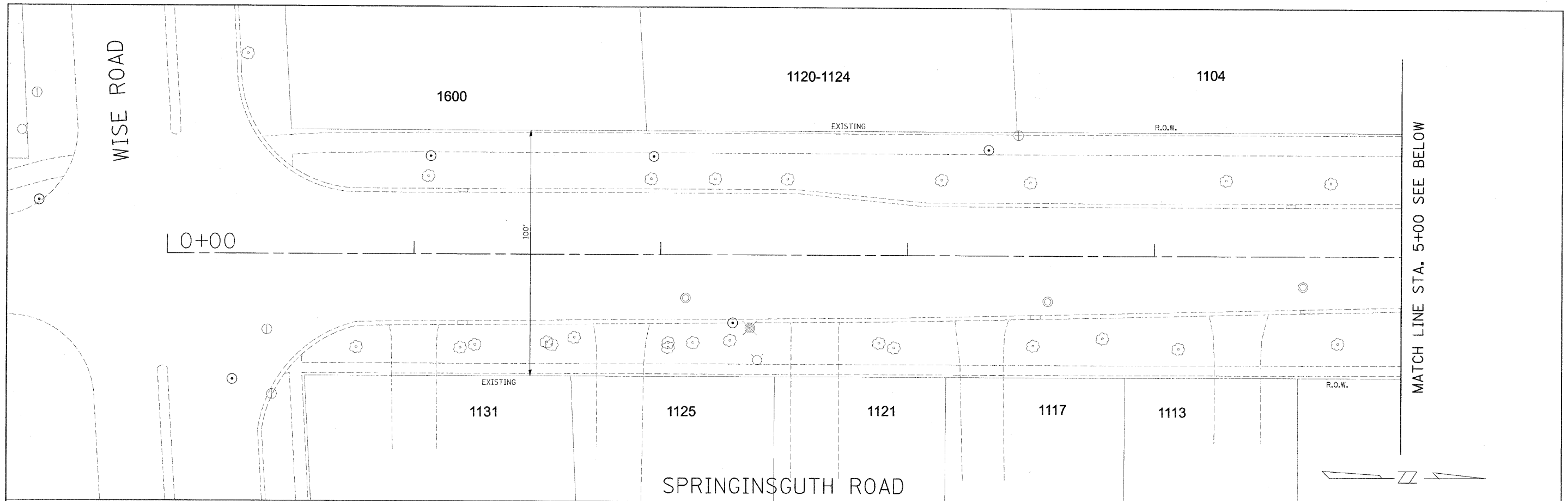
**CLASS D PATCHES**

DRIVEWAYS			PCC,5"	HMA
Address	Reference	Size	Sq Yd	Sq Yd
1102	L	4X52		23.1
1017	R	12+18/2X11	18.3	
916	L	24+20/2X18	44.0	
328	L	17+12/2X20	32.2	
327	R	14X19	29.6	
324	L	15+10/2X24	33.3	
323	R	9+14/2X24 + 4X11	35.6	
319	R	8X11+8X20	27.6	
316	L	5X20	11.1	
309	R	12X19	25.3	
305	R	8X16	14.2	
301	R	16X23	40.9	
230	L	12+18/2X8	13.3	
221	R	9+11/2X24	26.7	
215	R	9+11/2X16 + 8X18	33.8	
211	R	11X22	26.9	
207	R	12X15	20.0	
134	L	21+28/2X9	24.4	
133	R	5X29	16.1	
130	L	20+18/2X15	31.7	
127,125	R	16X24		42.7
124	L	9+13/2X23		28.1
123,121	R	5X34	18.9	
120	L	10+12/2X23	28.1	
119	R	7X23		17.9
118	L	5X21		11.7
114	L	12+15/2X11		16.4
110	L	11X21	25.7	
100	L	16+23/2X14	30.3	
30	L	10X14	15.6	
TOTALS			623.6	139.9

Street	Location	Size	Sq Ft
Bates Lane	SE Corner	2x4	8
	NE Corner	2x4	8
Hartmann Drive	SW Corner	2x8	16
	NW Corner	2x8	16
Church Entrance	SW Corner	2x8	16
	NW Corner	2x8	16
Hartmann Drive	SE Corner	2x4x2	16
	NE Corner	2x4x2	16
	NW Corner	2x4	8
Syracuse Lane	SW Corner	2x8 + 2x4	24
	NW Corner	2x8	16
	SE Corner	2x4x2	16
	NE Corner	2x4	8
Church Entrance	SW Corner	2x8	16
	NW Corner	2x8	16
Church Entrance	SW Corner	2x8	16
	NW Corner	2x8	16
Water Park	SW Corner	2x8	16
	NW Corner	2x8	16
Darymouth Lane	SE Corner	2x4	8
	NE Corner	2x4x2	16
School X-ing	NW Corner	2x4	8
School Entrance	SW Corner	2x8	16
	NW Corner	2x8	16
Norwell Lane	SW Corner	2x8	16
	NW Corner	2x8	16
Dedham Lane	SE Corner	2x4	8
	NE Corner	2x4	8
Kingston Lane	SW Corner	2x4	8
	NW Corner	2x4	8
	SE Corner	2x4	8
	NE Corner	2x4	8
Weathersfield Way	SW Corner	2x4x2	16
	NW Corner	2x4x2	16
	SE Corner	2x4x2	16
	NE Corner	2x4x2	16
School Entrance	SW Corner	2x4	8
	NW Corner	2x4	8
School X-ing	NW Corner	2x4	8
School Entrance	SW Corner	2x4	8
	NW Corner	2x4	8
Arlington Lane	SE Corner	2x4	8
	NE Corner	2x4	8
Winchester Lane	SW Corner	2x4	8
	NW Corner	2x4	8
Warwick Lane	SW Corner	2x4	8
	NW Corner	2x4	8
Revere Circle(South)	SE Corner	2x4	8
	NE Corner	2x4	8
Revere Circle ( North)	SE Corner	2x4	8
	NE Corner	2x4	8
Shopping Center Entrance (South)	SE Corner	2x4	8
	NE Corner	2x8	16
Shopping Center Entrance (North)	SE Corner	2x8	16
	NE Corner	2x8	16
Winthrop Lane	SW Corner	2x4x2	16
	NW Corner	2x4x2	16
Winthrop Lane X-ing	SE Corner	2x4	8
	NE Corner	2x4	8

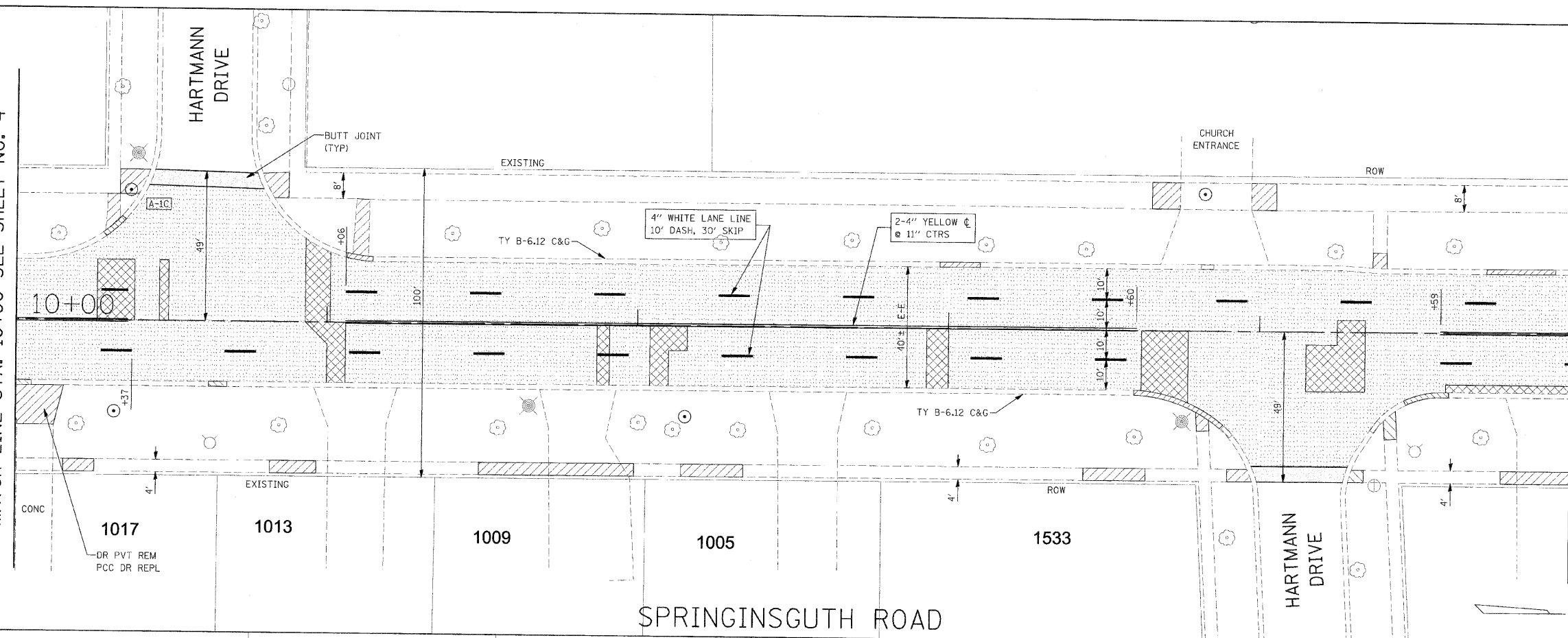
Location	Reference	Size	Sq Yd Ty II	Sq Yd Ty III	Sq Yd Ty IV
6+21	L	4x15	6.7		
6+50	L	4x92			40.9
6+58	R	4x25 + 2.5x15		15.3	
6+81	R	4x18 + 2.5x114			40.0
7+37	R	4x18	8.0		
9+63	L-R	6x40			26.7
10+26	L	12x20			26.7
10+46	L	3x20	6.7		
10+93	L	8x20		17.8	
11+00	R	6x20 + 6x6/2		15.3	
11+87	R	6x20	13.3		
12+04	R	6x20 + 8x6		18.7	
12+93	R	7x20		15.6	
13+62	R	15x20			33.3
14+15	R	13x10 + 9x23			37.4
14+60	R	2.5x232			64.4
16+11	L	2.5x32	8.9		
18+34	R	6x20	13.3		
18+65	L-R	6x40			26.7
19+15	L	7x20		15.6	
	R	7+3/2x20	11.1		
19+43	L	14x29 + 6x13			53.8
20+32	L	6x20	13.3		
	R	15x20			33.3
20+92	L	6x28		18.7	
	R	4x12	5.3		
21+05	L	8x24		21.3	
21+93	R	4x20 + 5x6	12.2		
22+42	R	5x24	13.3		
23+70	L	3x16 + 3x6	7.3		
27+85	R	6x20		17.8	
28+20	L	14x34			52.9
28+29	R	7x20		15.6	
29+45	R	14x20			31.1
29+98	L	6x20	13.3		
30+42	L	20x29			64.4
31+11	R	6x20	13.3		
31+17	L	17X20			37.8
31+48	R	2.5X122			33.9
33+65	L	3X16	5.3		
36+90	L	4X12	5.3		
37+12	L	17X20			37.8
	R	5X20	11.1		
	L	6X13	8.7		
38+08	L	5X17	9.4		
38+28	L	12X18		24.0	
38+44	R	4X26	11.6		
38+70	R	2.5X85		23.6	
39+75	L	12X28			37.3
40+53	L	13X28			40.4
	L	19X20			42.2
	L	12X36			48.0
41+83	L	6X90			60.0
42+78	L	8X20		17.8	
44+15	R	8X20		20.0	
44+25	L	9X20			88.3
44+68	L	15X53			26.7
45+42	R	12X20			81.4
45+50	L	5X20	11.1		
45+59	L	7X79			34.7
	L	12X26			62.0
	L-R	9X82			135.6
46+72	R	4X17	7.6		
46+96	R	3X26	8.7		
48+04	R	20X61			19.7
51+13	L	5X29		16.1	
51+23	R	4X26	11.6		
52+85	L-R	5X40		22.2	
53+78	R	7X29		22.6	
53+96	L	4X438			75.3
54+17	R	3X226			
54+90	L	5X16	8.9		
55+20	L	5X16	8.9		
56+43	R	4X30	13.3		
56+73	R	3X107			35.7
57+80	R	4X85			37.8
58+65	R	2X58		19.3	
58+23	R	4X214			95.1
59+15	L	3X360			120.0
61+91	L	4X16	7.1		
62+17	R	4X590			262.2
62+50	L	4X16	7.1		
62+67	L	8X16	14.2		
63+61	L	3X20	6.7		
63+89	L	4X72			32.0
65+06	L-R	4X28	12.4		
65+38	L	3+4/2X55		21.4	
66+48	L	3X440			146.7
67+30	L-R	5X33		18.3	
67+85	L-R	8X33			29.3
68+90	R	3X251			83.7
69+92	L-R	8X34			30.2
70+20	L-R	5X34		18.8	
71+07	L	4X32	14.2		
TOTALS			322.5	395.8	2440.4

712



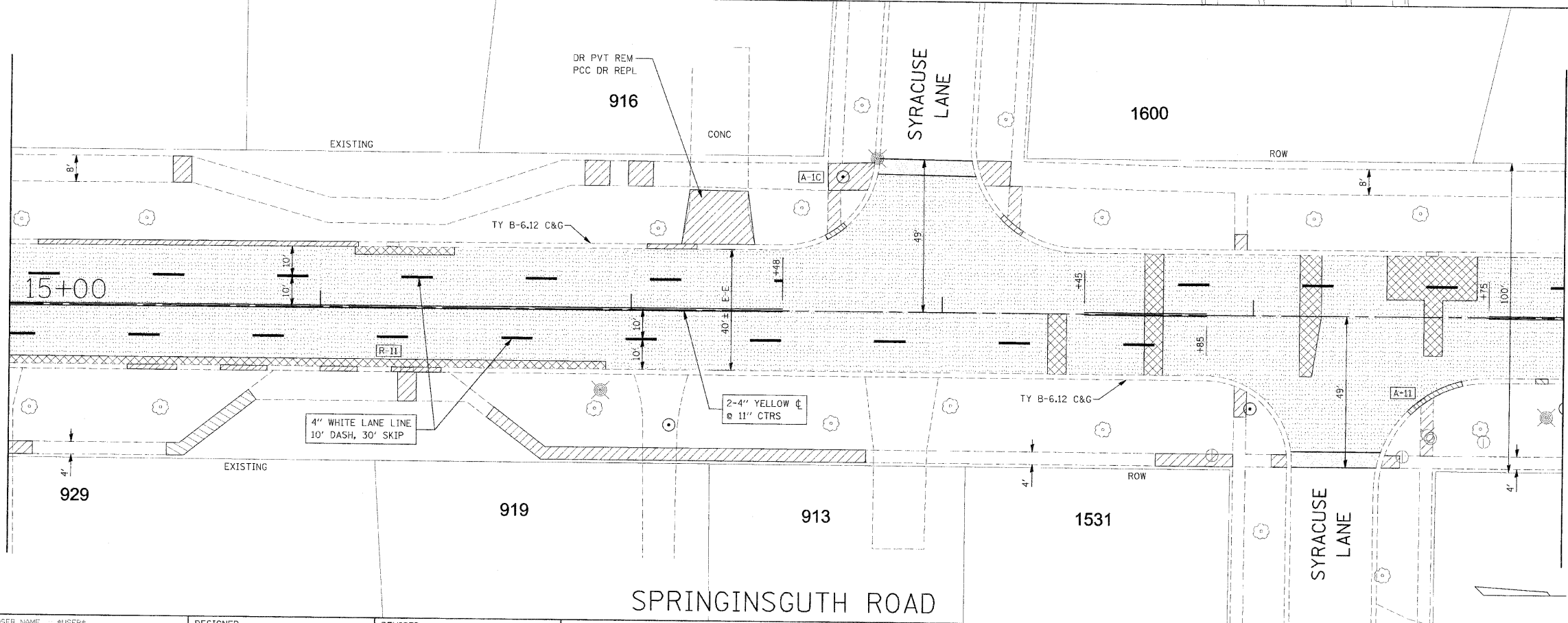
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	PLOT DATE = 8/2/2018	CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(703)							
		DATE -	REVISED -									

MATCH LINE STA. 10+00 SEE SHEET NO. 4



MATCH LINE STA. 15+00 SEE BELOW

MATCH LINE STA. 15+00 SEE ABOVE



MATCH LINE STA. 20+00 SEE SHEET NO. 6

SPRINGINGUTH ROAD

SPRINGINGUTH ROAD

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

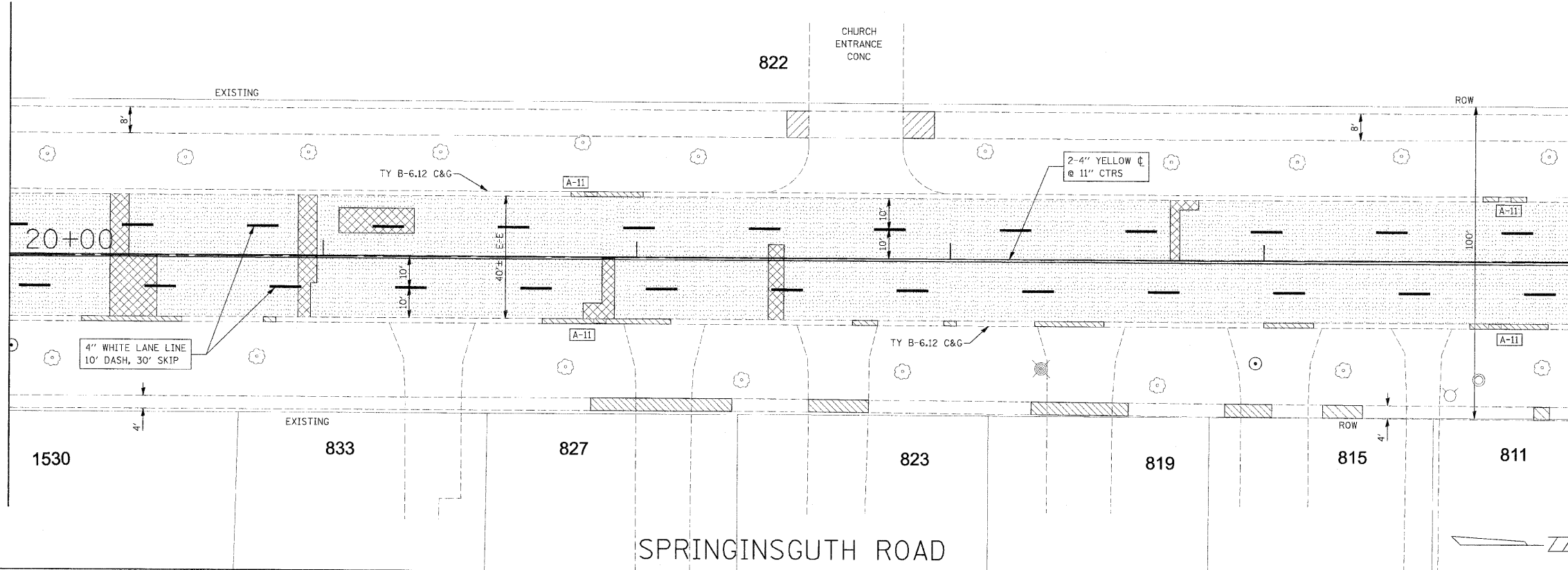
SPRINGINGUTH ROAD  
STA. 10+00 TO STA. 20+00

SCALE: 1"=20' SHEET NO. 2 OF 8 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2562	10-00104-00-RS	COOK	21	7
SPRINGINGUTH ROAD			CONTRACT NO. 63500	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90037031				

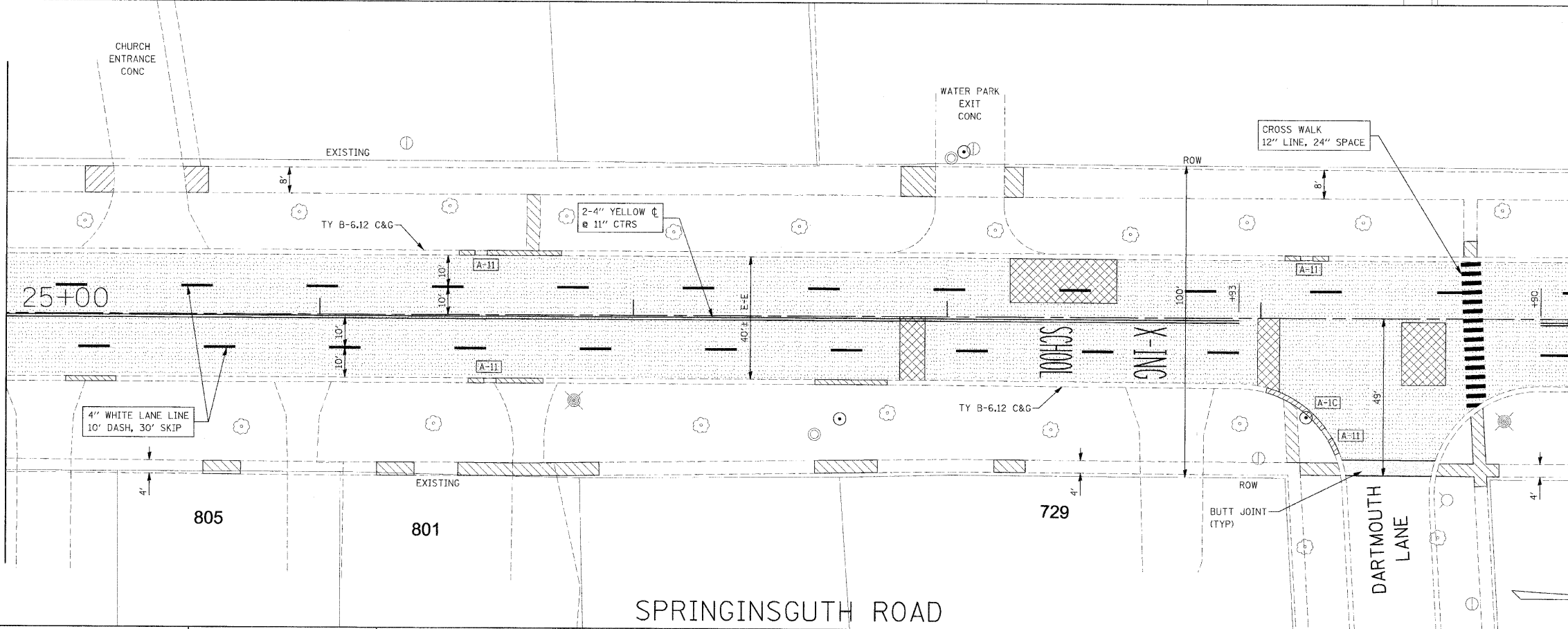


MATCH LINE STA. 20+00 SEE SHEET NO. 5



MATCH LINE STA. 25+00 SEE BELOW

MATCH LINE STA. 25+00 SEE ABOVE



MATCH LINE STA. 30+00 SEE SHEET NO. 7

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

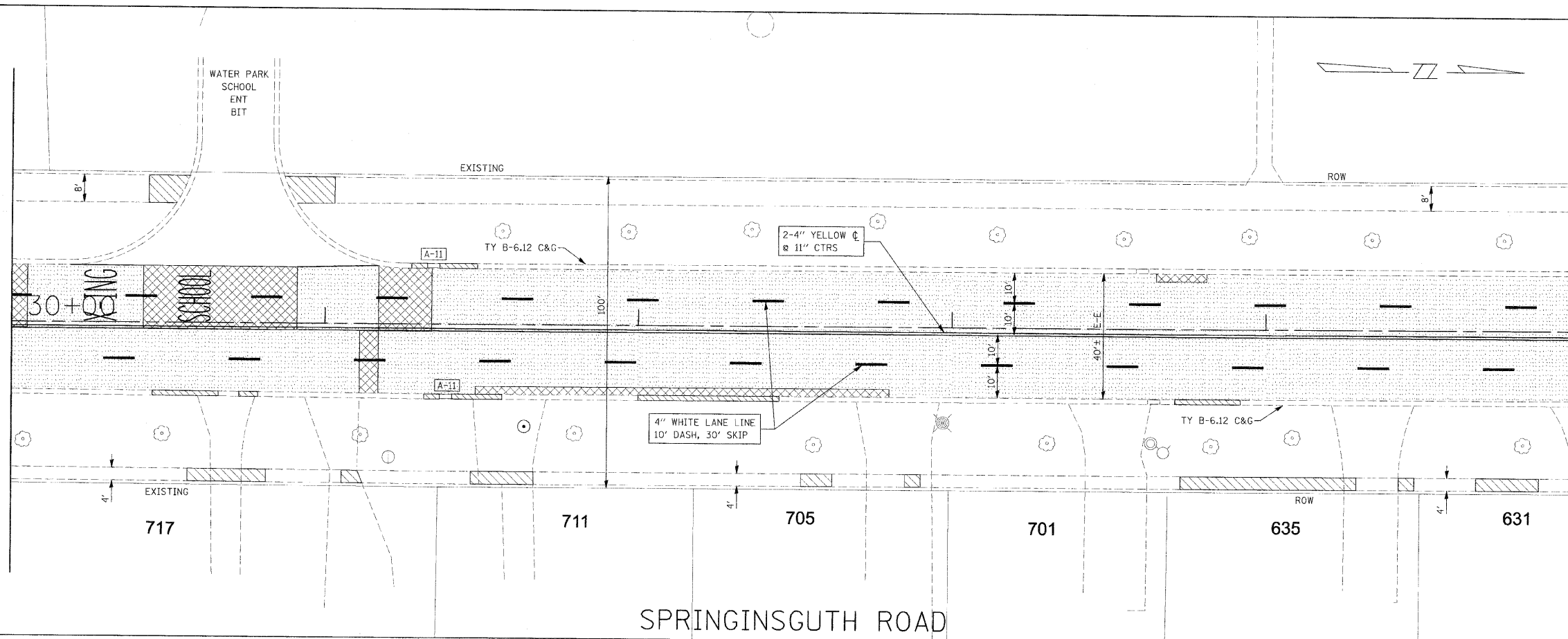
SPRINGSGUTH ROAD  
STA. 20+00 TO STA. 30+00

SCALE: 1"=20' SHEET NO. 3 OF 8 SHEETS STA. TO STA.

F.A.U. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 8
SPRINGSGUTH ROAD		CONTRACT NO. 63500		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(703)				



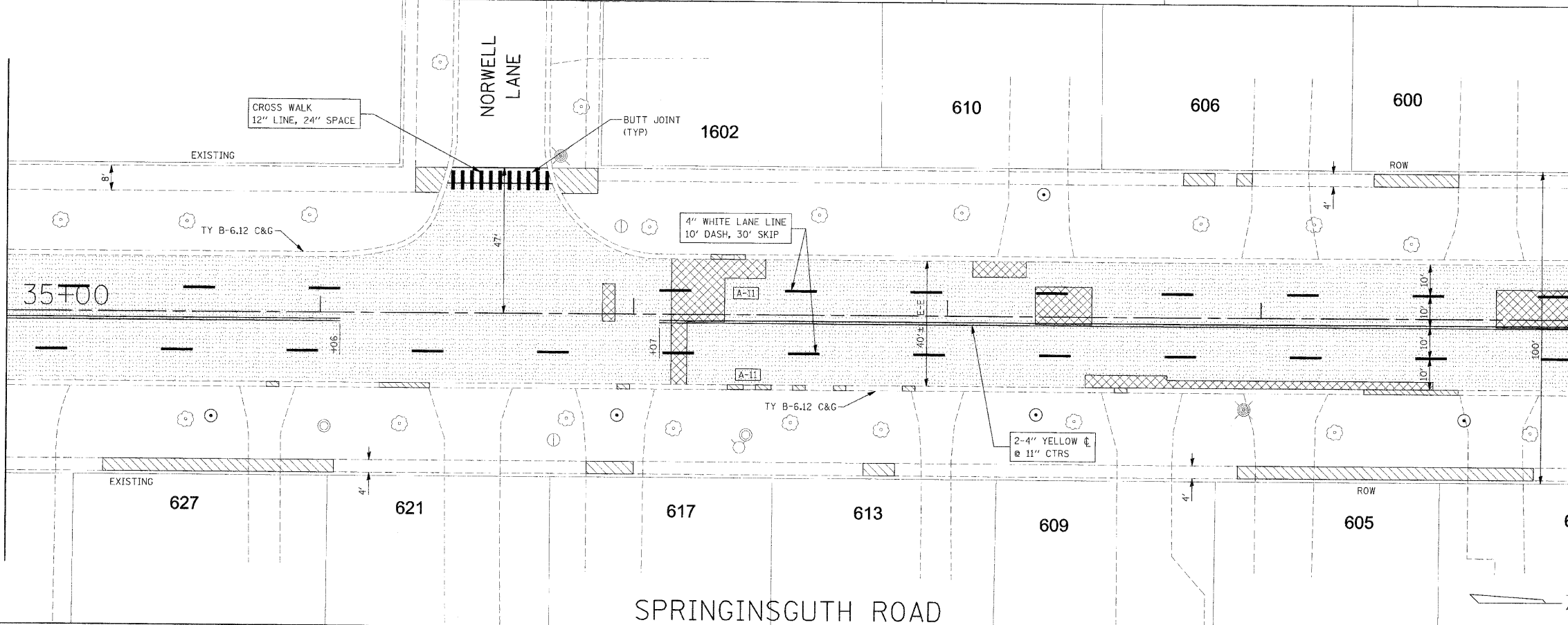
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MATCH LINE STA. 35+00 SEE BELOW

SPRINGINGUTH ROAD

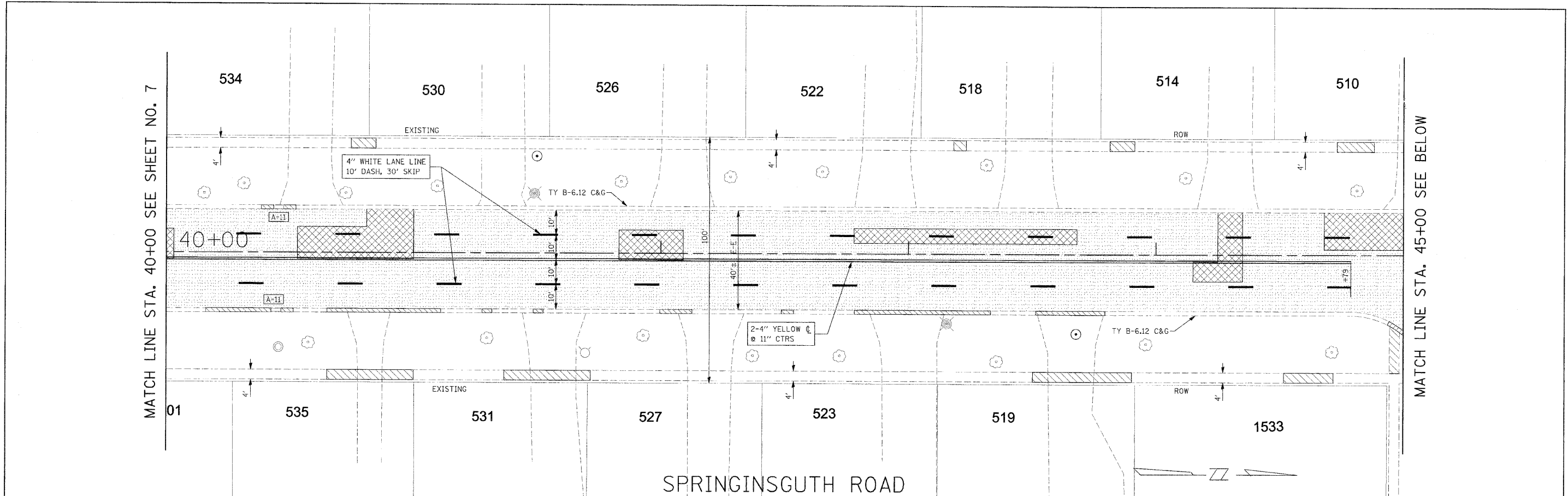
MATCH LINE STA. 35+00 SEE ABOVE



MATCH LINE STA. 40+00 SEE SHEET NO. 8

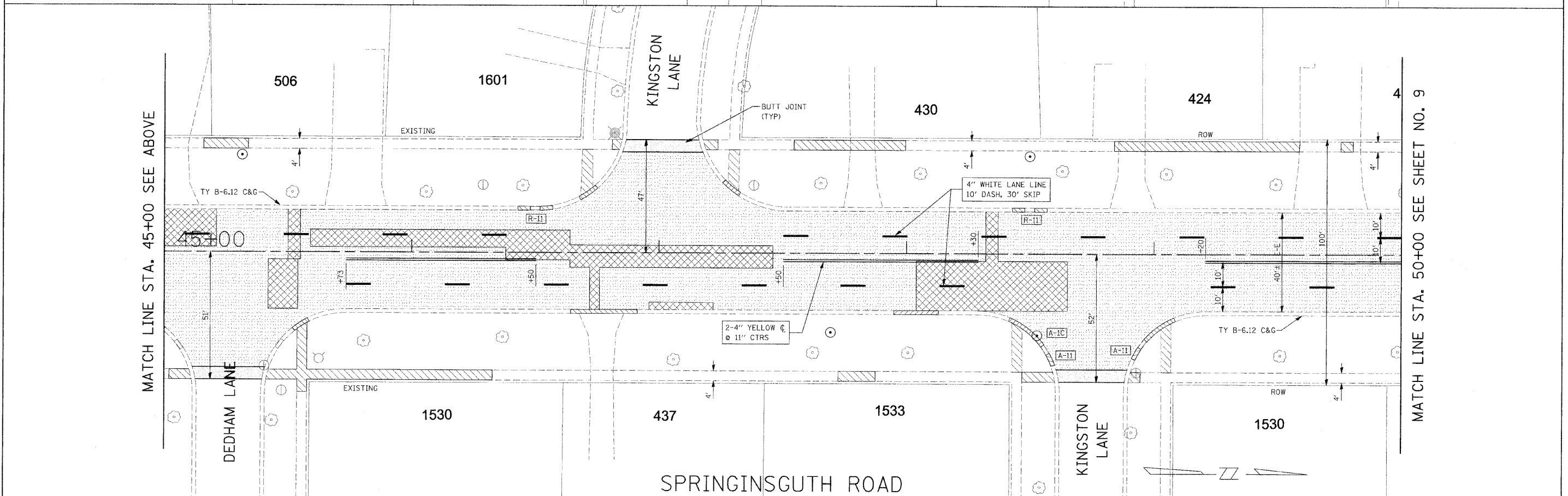
SPRINGINGUTH ROAD

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					SCALE: 1"=20'	SHEET NO. 4 OF 8 SHEETS STA.		TO STA.			



MATCH LINE STA. 40+00 SEE SHEET NO. 7

MATCH LINE STA. 45+00 SEE BELOW

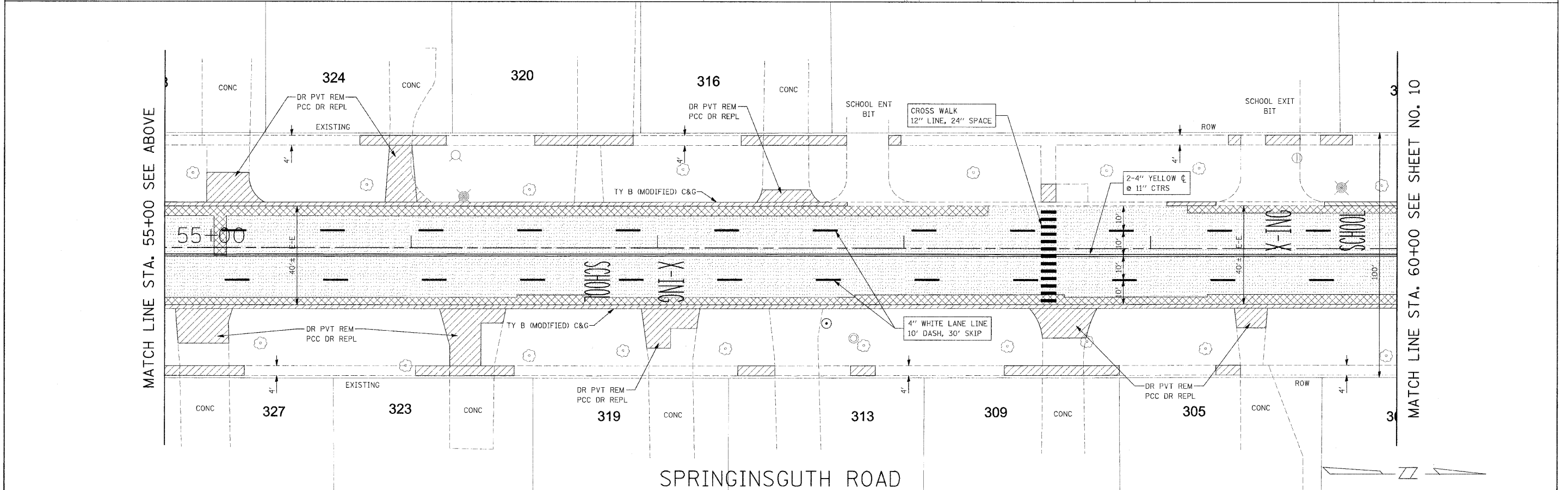
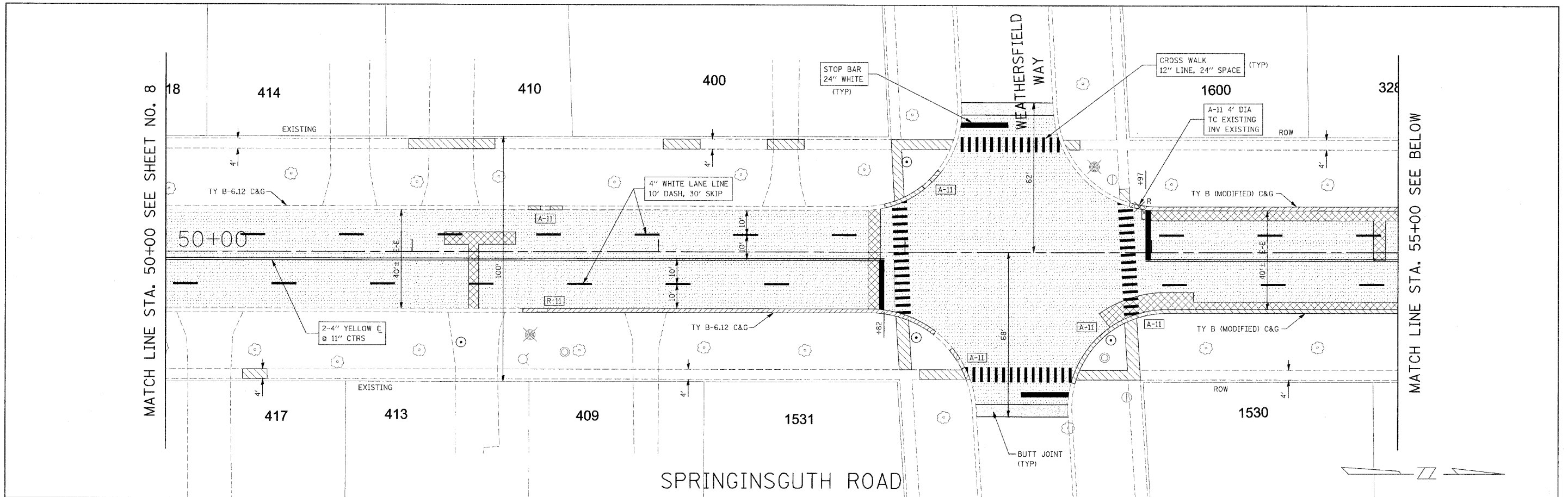


MATCH LINE STA. 45+00 SEE ABOVE

MATCH LINE STA. 50+00 SEE SHEET NO. 9

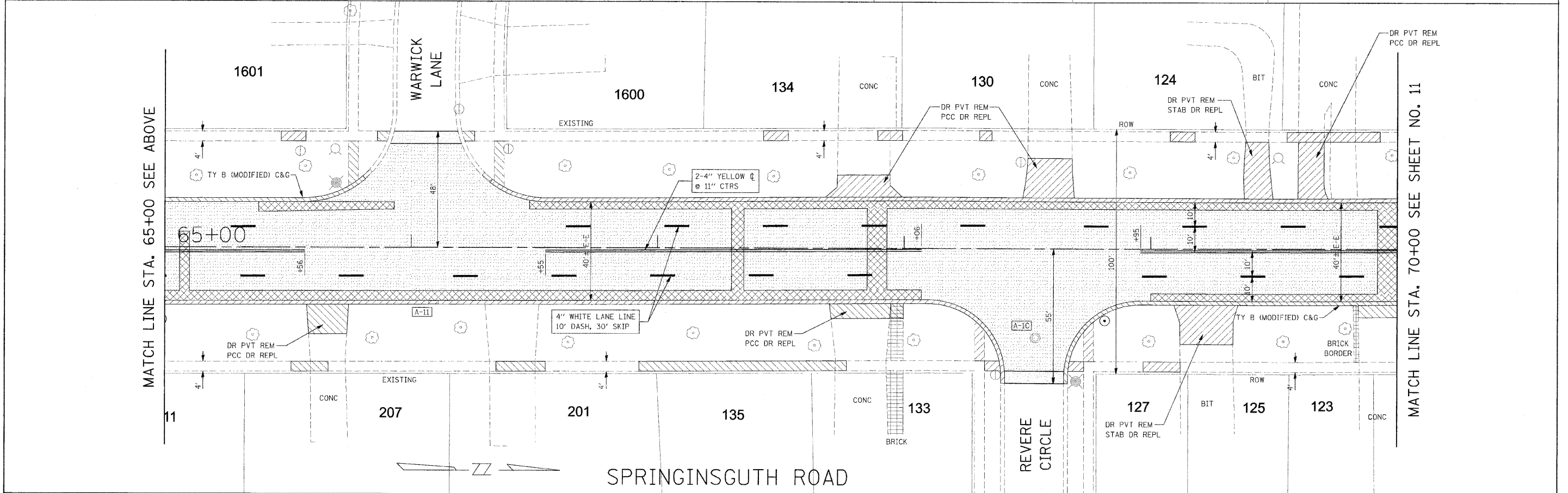
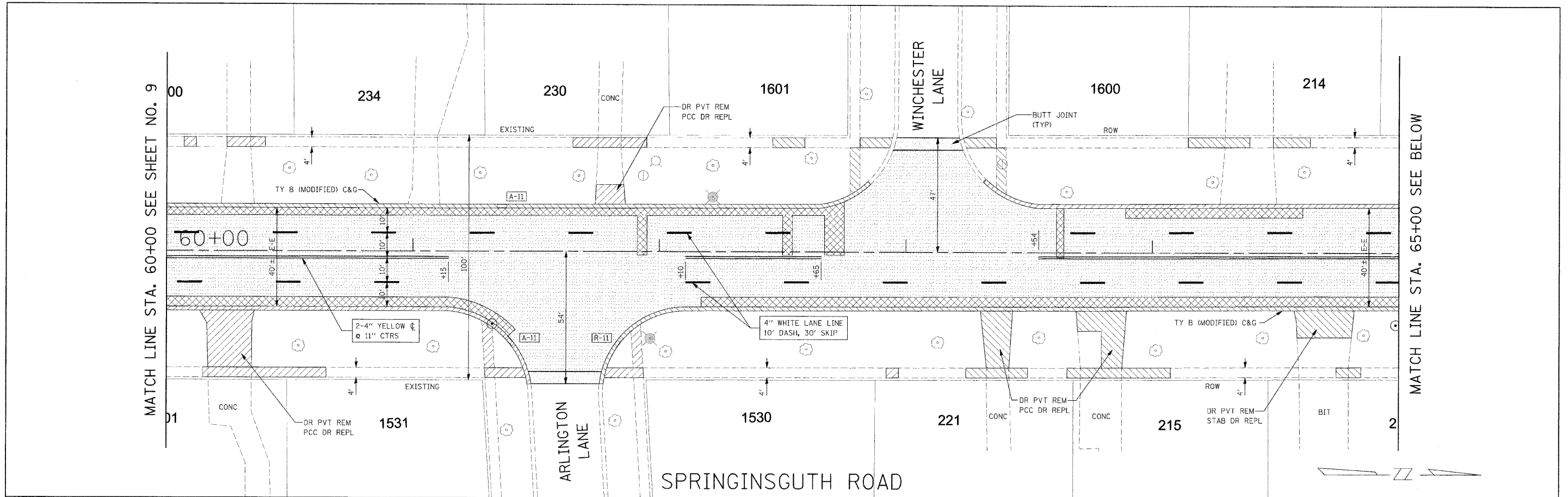
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PLOT DATE = 01/27/2010		DATE -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT M-90037031					

SCALE: 1"=20' SHEET NO. 5 OF 8 SHEETS STA. TO STA.

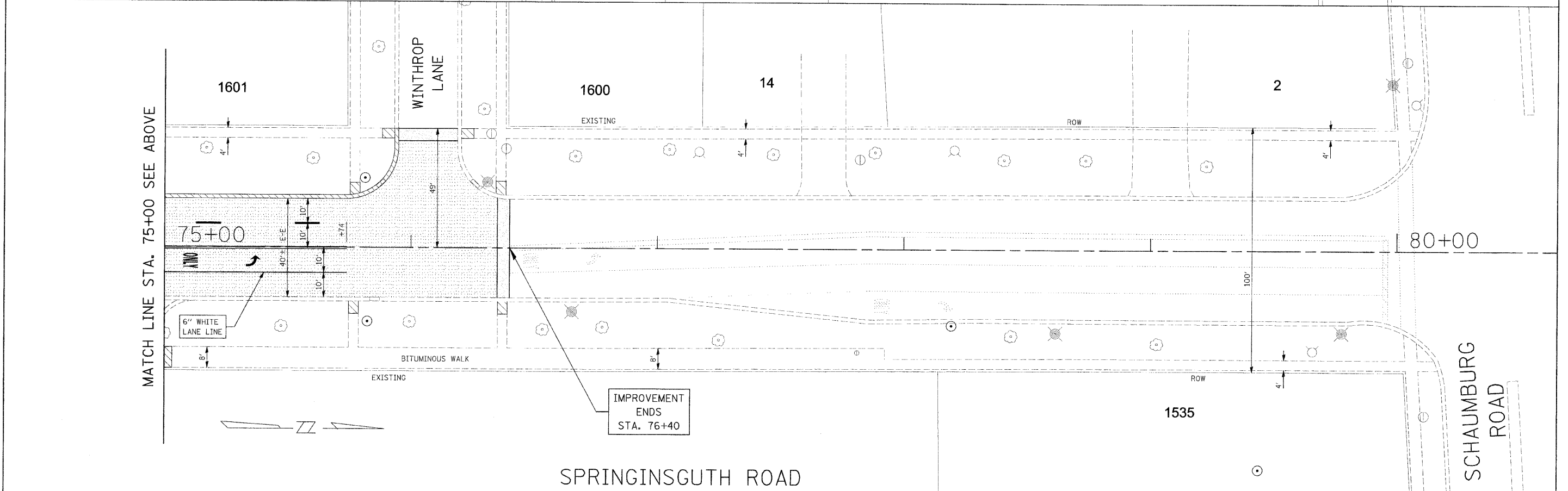
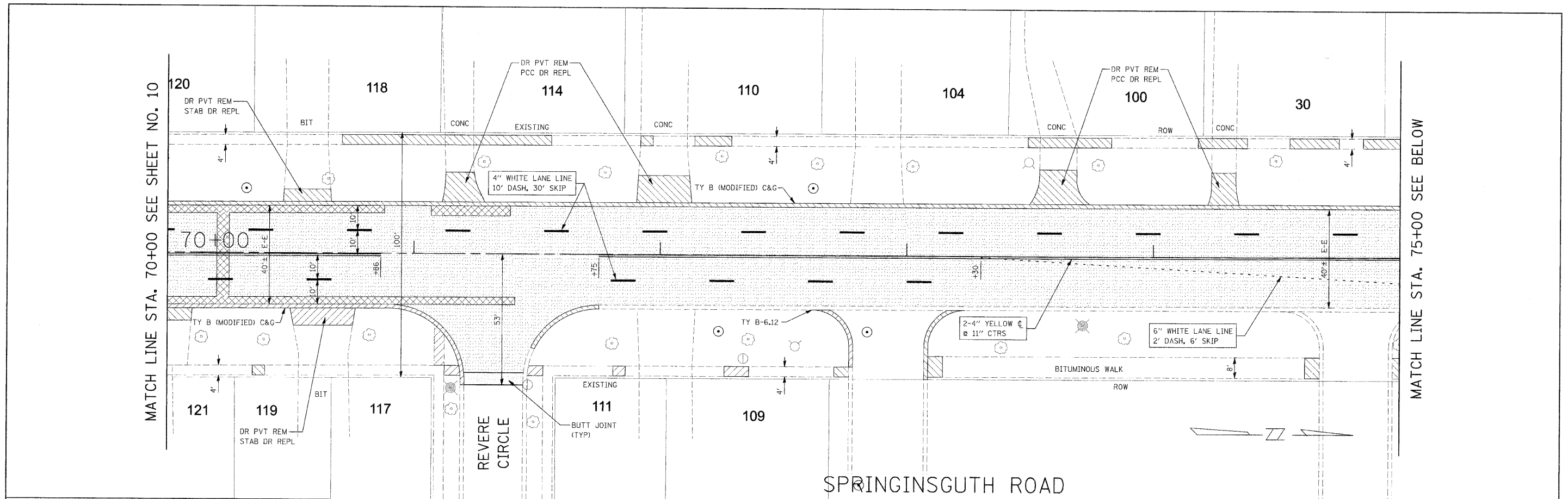


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PLOT DATE = 8/2/2010		DATE -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT M-90031703					

SCALE: 1"=20' SHEET NO. 6 OF 8 SHEETS STA. TO STA.



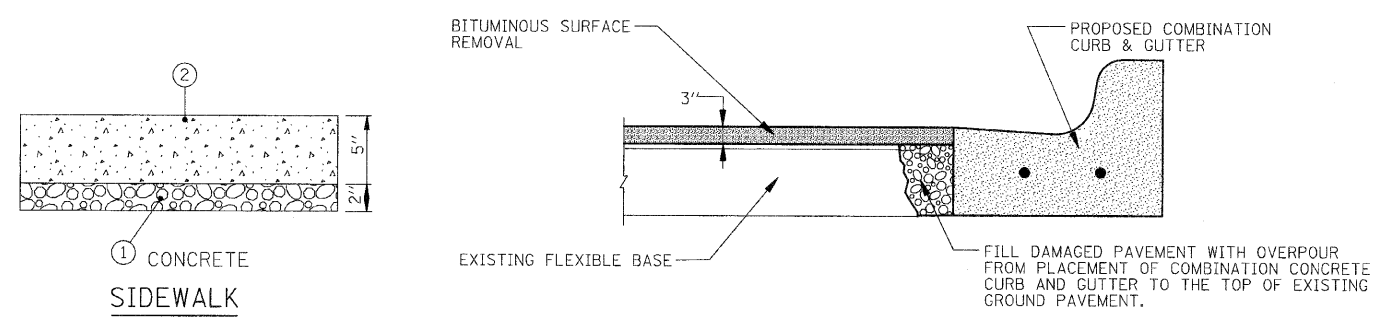
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PLOT SCALE = #SCALE#	PLOT DATE = 8/2/2018	DRAWN -	REVISED -			SCALE: 1"=20'	SHEET NO. 7 OF 8 SHEETS	SPRINGSGUTH ROAD	CONTRACT NO. 63500		
		CHECKED -	REVISED -			FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT M-9003(703)					
		DATE -	REVISED -								



FILE NAME - P:\2010\1000030\phase 2\dwg\100030.dgn	USER NAME - #UGER#	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SPRINGINGUTH ROAD STA. 70+00 TO STA. 80+00</b>	F.A.U. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 13	
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PLOT DATE - 8/27/2010	DATE -	REVISED -	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-9003(703)								

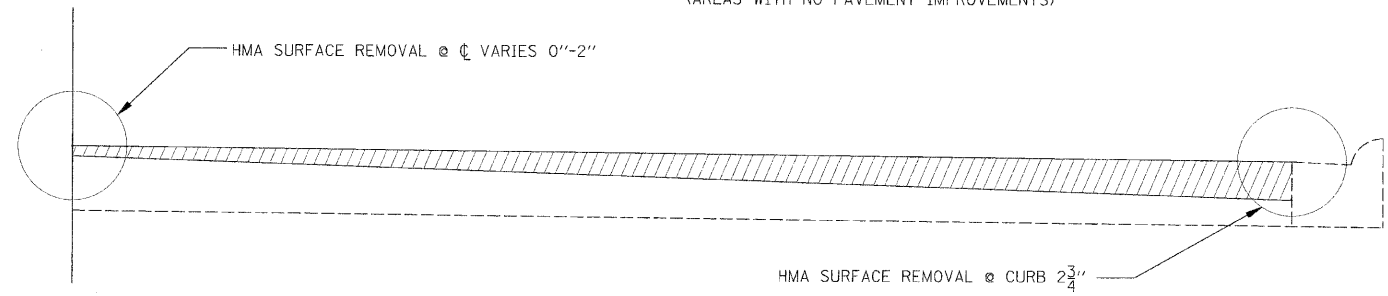
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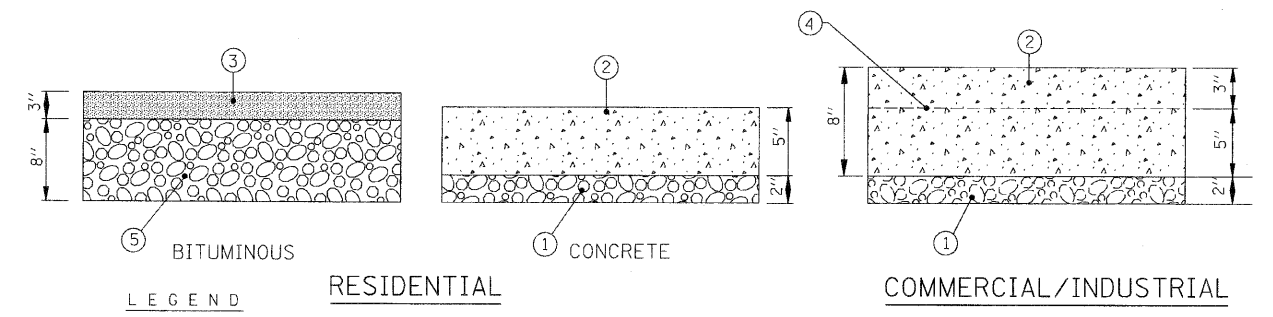


**PAVEMENT RESTORATION DETAIL  
 ADJACENT TO REMOVED AND REPLACED CURB & GUTTER**

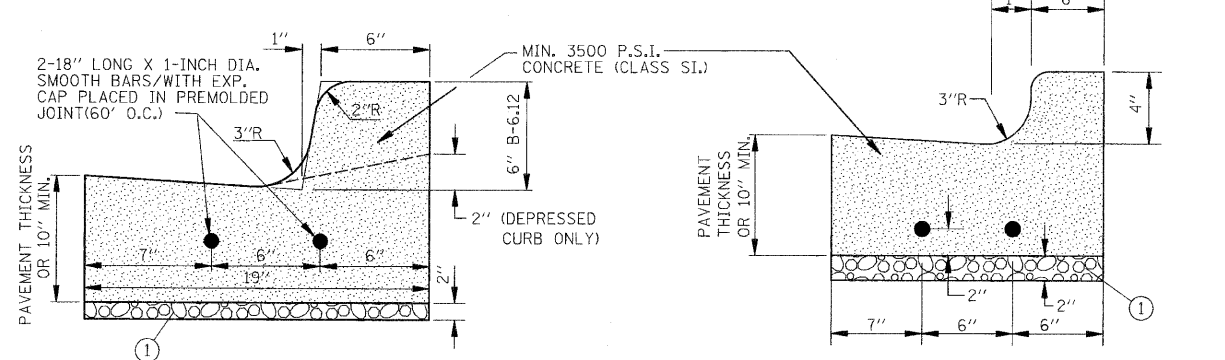
ALL WORK INCLUDED IN THE COST OF THE ASSOCIATED CURB AND GUTTER AND CURB AND GUTTER REMOVAL PAY ITEMS. (AREAS WITH NO PAVEMENT IMPROVEMENTS)



**HMA SURFACE REMOVAL DETAIL  
 NOT TO SCALE**



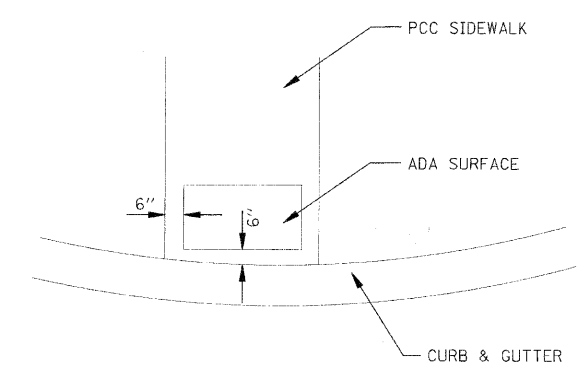
- LEGEND**
- ① SUB-BASE GRANULAR MATERIAL, TYPE B
  - ② CLASS SI CONCRETE
  - ③ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50
  - ④ 6" x 6" #6 WIRE MESH
  - ⑤ AGGREGATE BASE COURSE, TYPE B



**NOTE:**  
 USE TWO NO. 5 RE-BARS FOR 10' ON EITHER SIDE OF ALL UTILITY TRENCHES. INSTALL 1" EXPANSION JOINT AT POINTS OF CURVATURE, AT CONSTRUCTION JOINTS AND AT A DISTANCE NOT TO EXCEED 60'. CONTRACTION JOINTS SHALL BE PLACED BETWEEN EXPANSION JOINTS AT DISTANCES NOT TO EXCEED 15'.

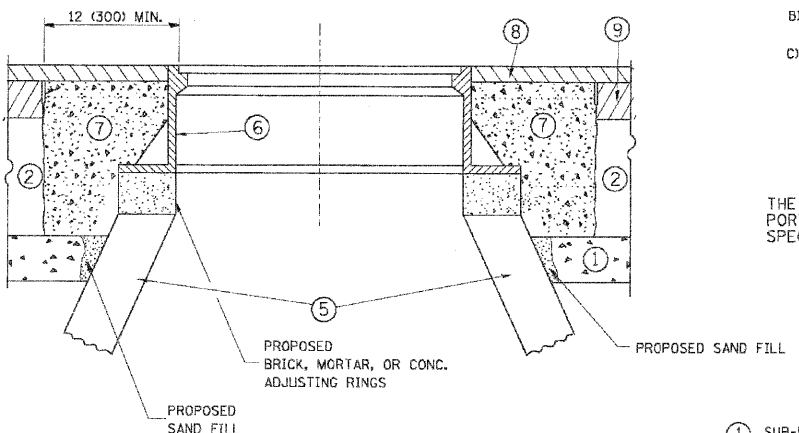
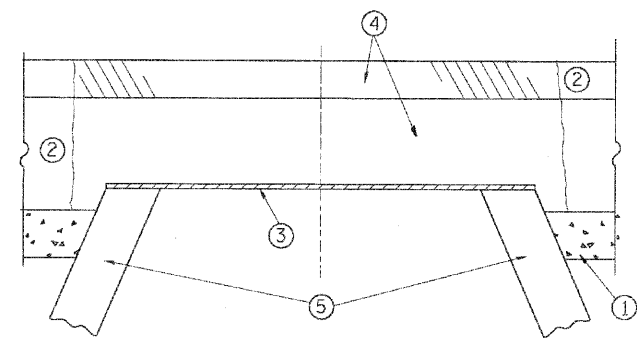
**COMBINATION CURB & GUTTER  
 TYPE B-6.12**

**COMBINATION CURB & GUTTER  
 TYPE B (MODIFIED)**



**DETECTABLE WARNING DETAIL  
 NOT TO SCALE**

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>HLR</b>	SPRINGSGUTH ROAD IMPROVEMENTS DETAILS	F.A.U. R/FE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 14
PLOT SCALE =	CHECKED -	REVISOR -	SPRINGSGUTH ROAD				CONTRACT NO. 63500				
PLOT DATE =	DATE -	REVISOR -	FED. ROAD DIST. NO. 1				ILLINOIS FED. AID PROJECT M-9003(T03)				
			SCALE:				SHEET NO. 1 OF 1 SHEETS	STA. TO STA.			



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

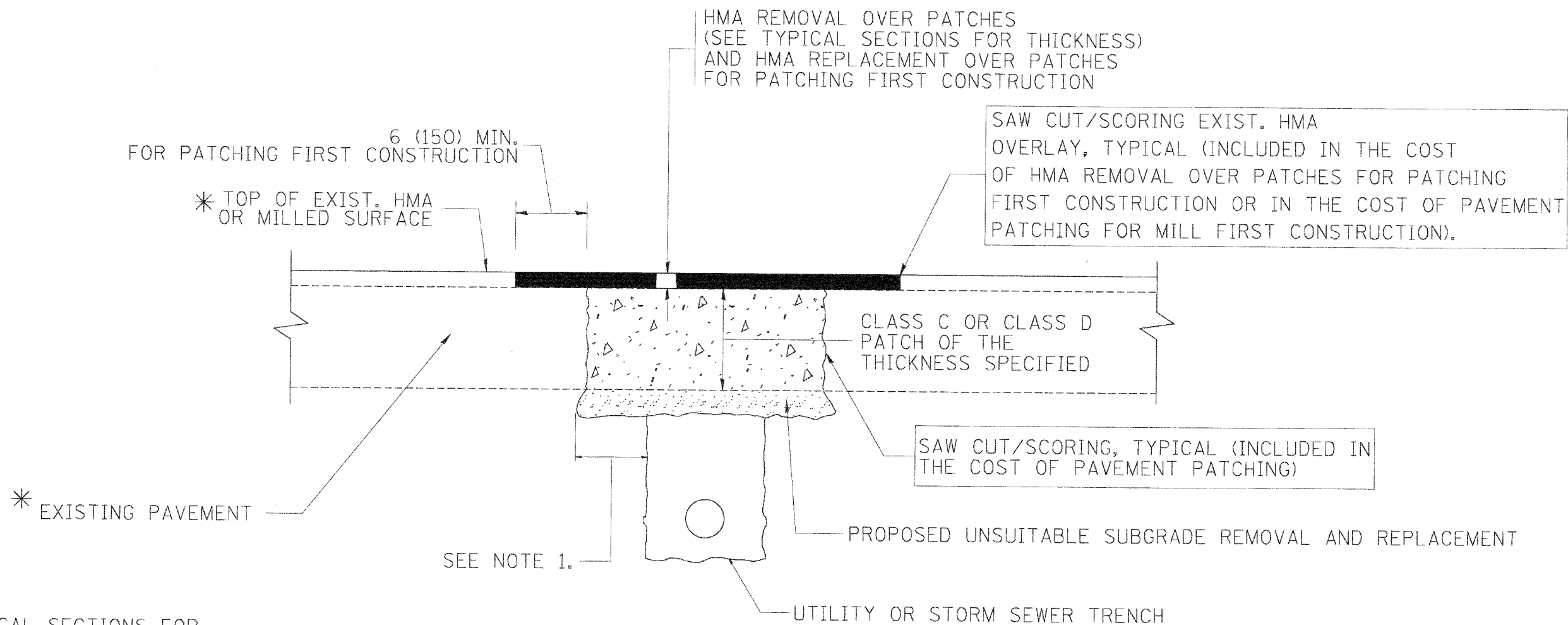
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = N:\data\22x34\ba00.dgn	USER NAME = gaglianob	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>		F.A.J. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 15
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	<b>BD600-03 (BD-8)</b>		CONTRACT NO. 63500	
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. WIEDEMAN 05-14-04					FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-9003(703)			
		DATE - 10-25-94	REVISED - R. BORO 01-01-07								





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

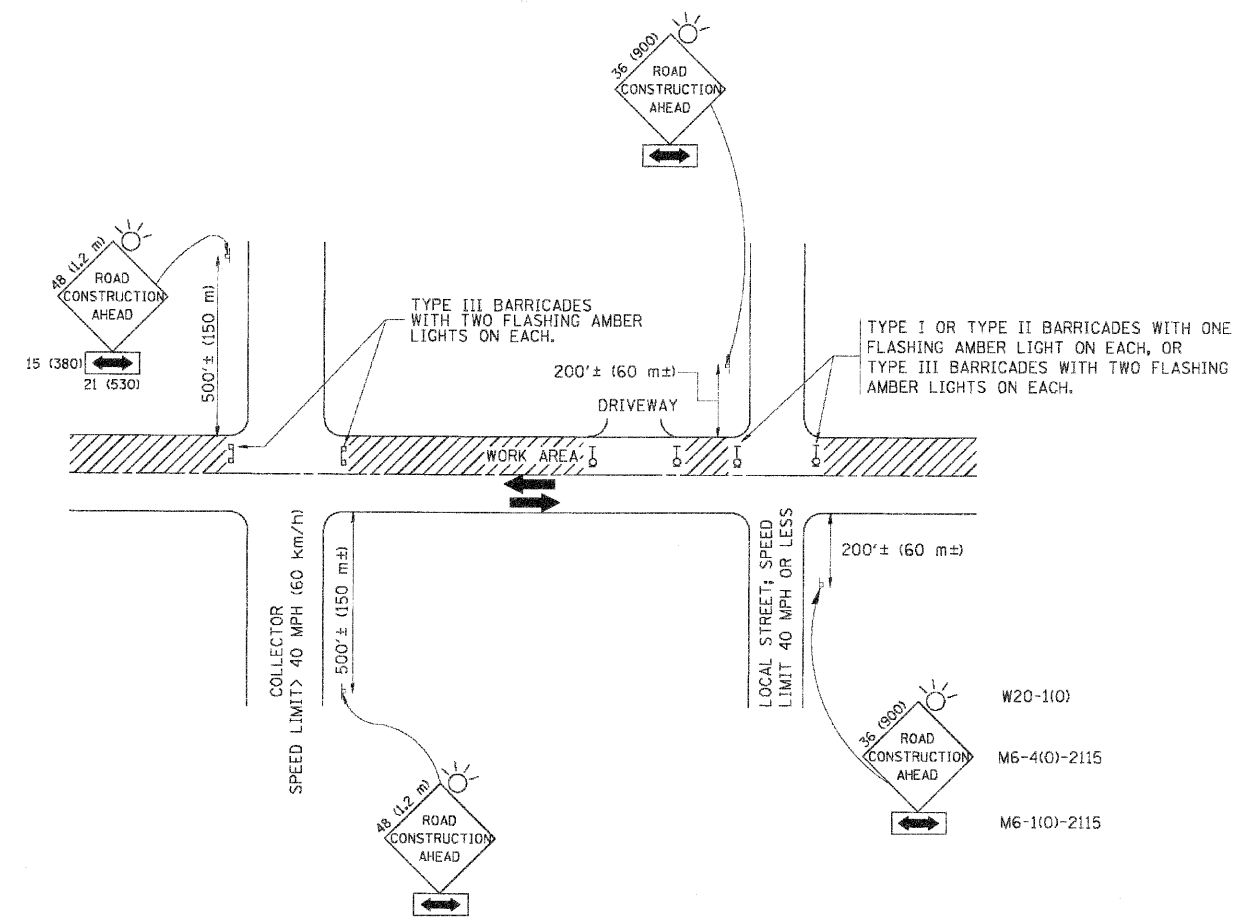
**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = bauer-dl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
m:\pave\pave\datatd22x34\bd22.dgn		DRAWN -	REVISED - R. BORO 01-01-07		2562	10-00104-00-RS	COOK	21	16		
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		PLT DATE = 10/27/2006	DATE - 10-25-94		REVISED - K. ENG 10-27-08	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-900317031					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
  - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

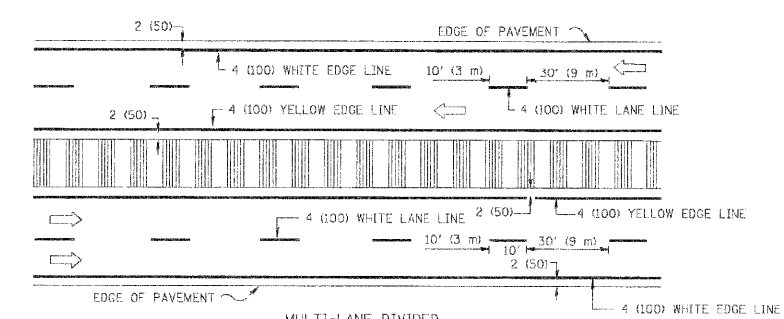
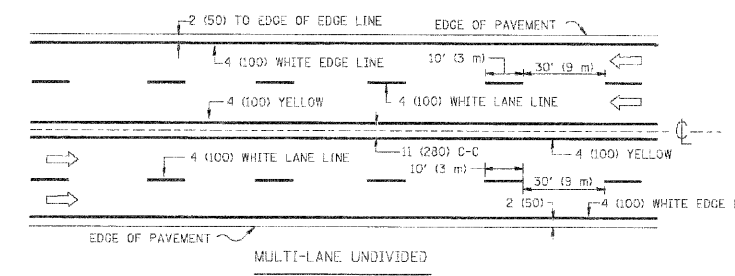
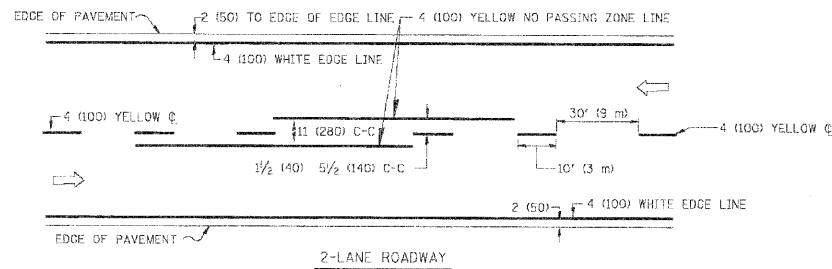
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = goglianob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
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	PLOT SCALE = 50,000 1/1 IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

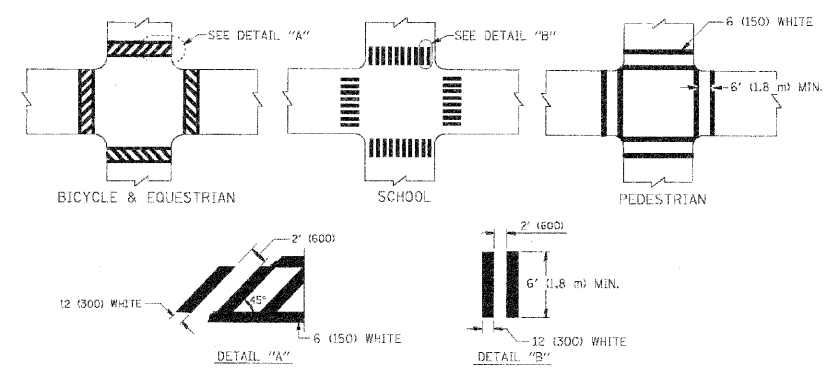
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 18
TC-10			CONTRACT NO. 63500	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(703)				

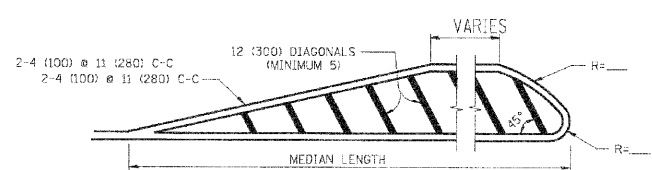
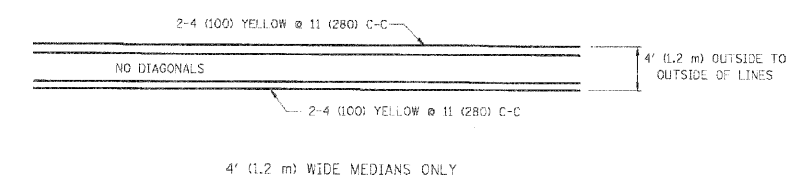


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

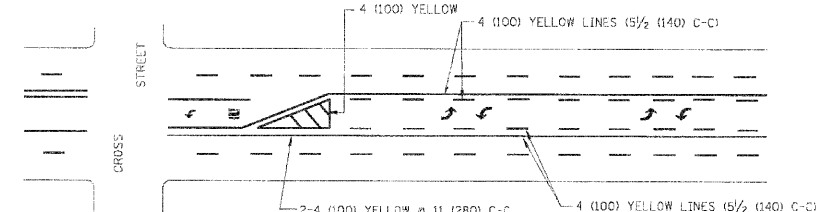


TYPICAL CROSSWALK MARKING

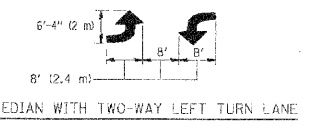


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (23 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

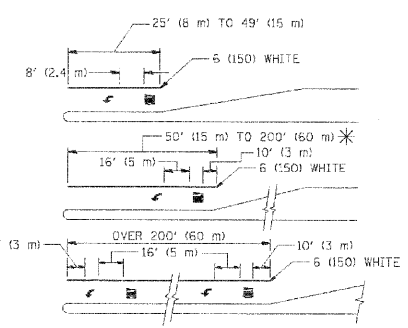
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



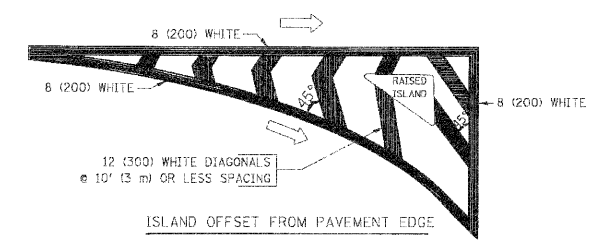
TYPICAL PAINTED MEDIAN MARKING



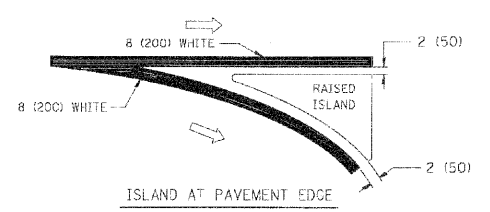
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



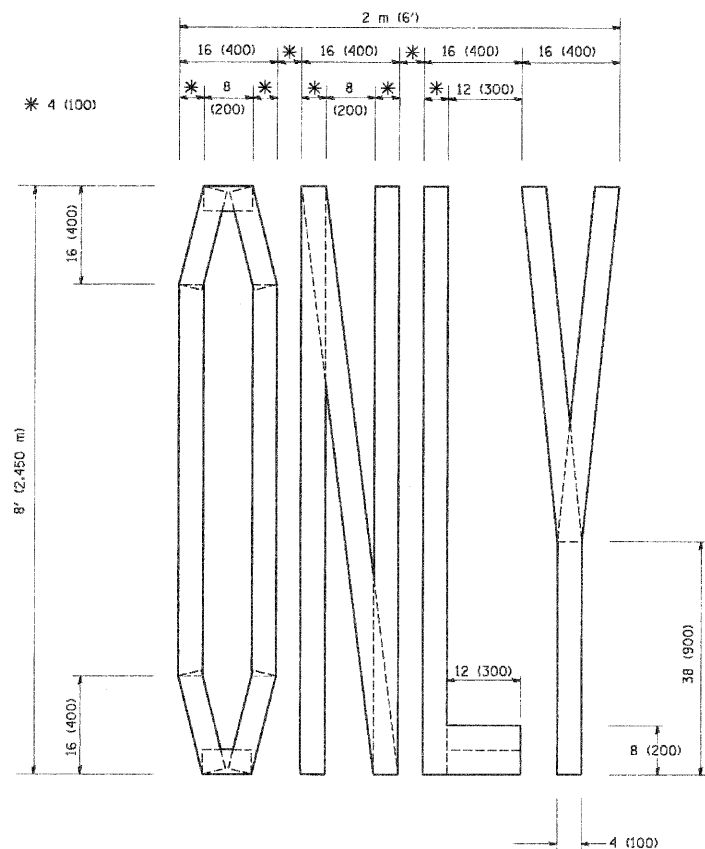
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

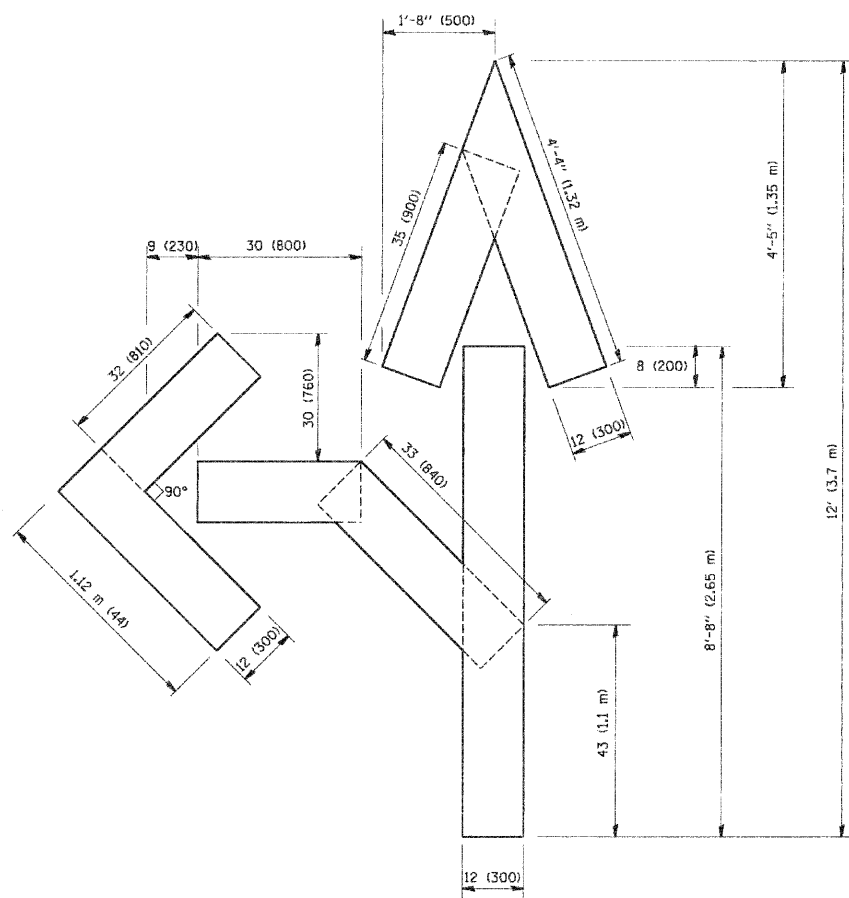
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/4 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 13' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINE; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

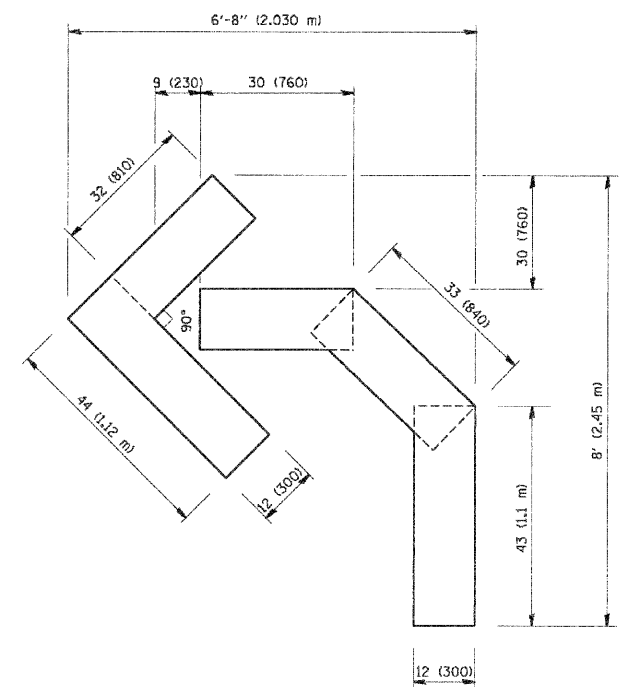
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



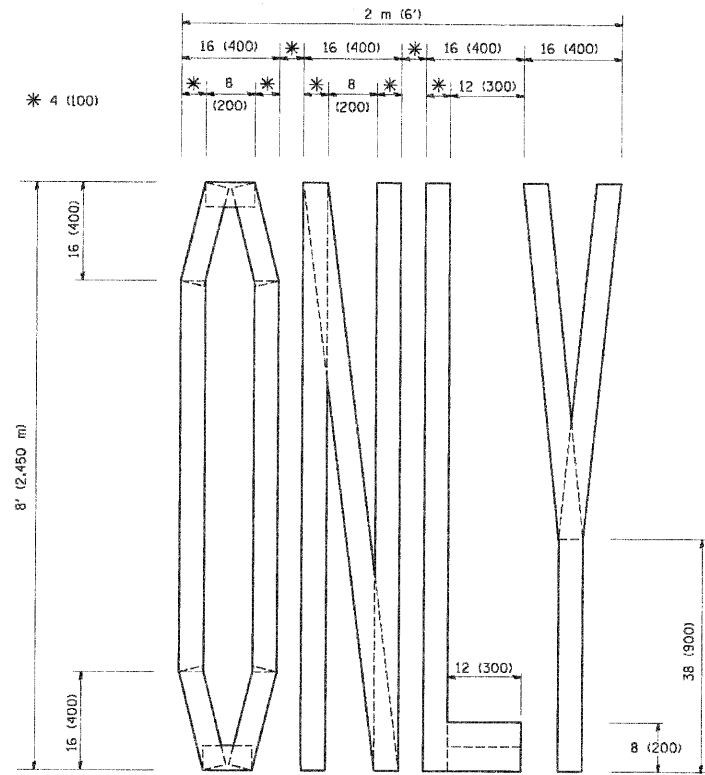
QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



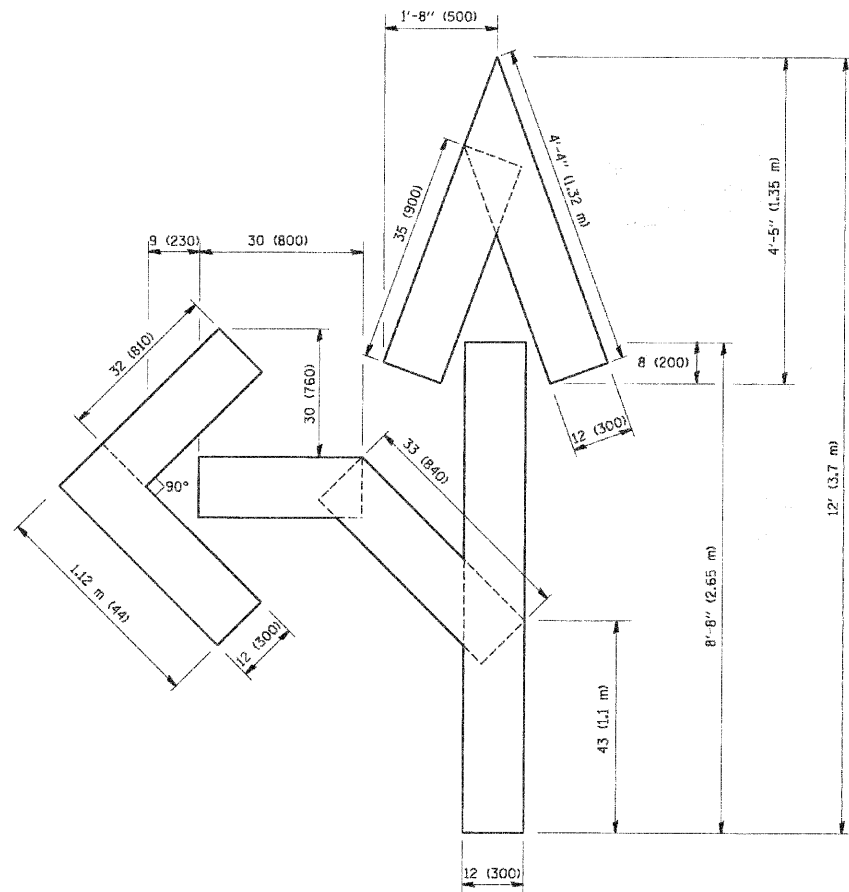
QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

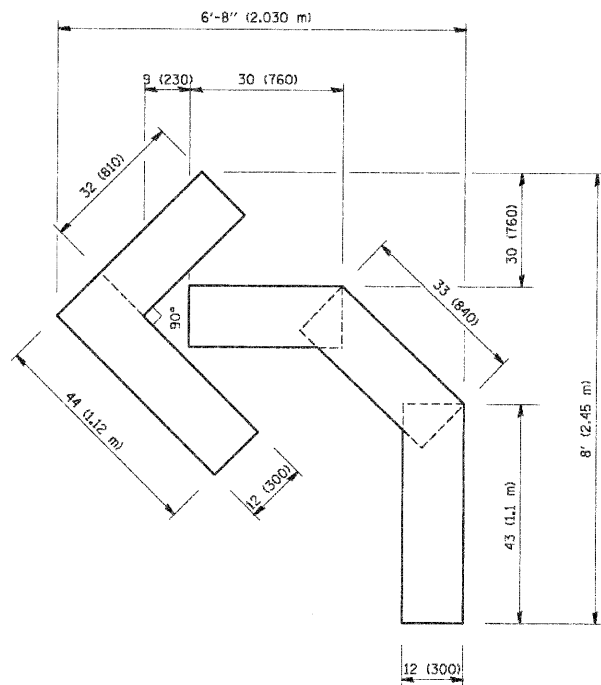
FILE NAME = W:\d:\state\22x34\w1b.dgn	USER NAME = gaglianoib	DESIGNED -	REVISED - T. RAMMACHER 06-05-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING</b>			F.A.J. RTE. 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEET 21	SHEET NO. 20
	PLOT SCALE = 5/8" = 1' IN.	DRAWN -	REVISED - T. RAMMACHER 11-04-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-16</b>			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 03-02-98		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(703)							
		DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00		CONTRACT NO. 63500							
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 4 (100) LINE = 82.5 ft. (25.3 m)  
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QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to16.dgn	USER NAME = gaqlanobt	DESIGNED -	REVISED - T. RAMMACHER 06-05-96
		DRAWN -	REVISED - T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED - T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE: 2562	SECTION 10-00104-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 21
TC-16			CONTRACT NO. 63500	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(703)				