

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

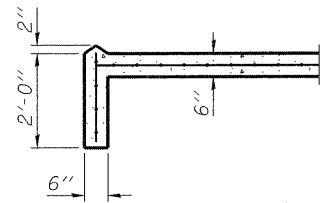
**GENERAL NOTES:**

- Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts Bolts 7/8 in.  $\phi$ , holes 15/16 in.  $\phi$ , unless otherwise noted.
- Calculated weight of Structural Steel = 17,800 pounds (AASHTO, M270, Grade 36).
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.  
Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- "Slip forming" of the concrete parapets is not allowed.
- Concrete Sealer shall be applied to the designated areas of the abutments.
- The Contractor shall submit Structural Assessment Report(s) as required for the Contractor's means and methods of construction. "See Special Provisions"
- Bridge Rail Removal included in the cost for removal of existing concrete deck.

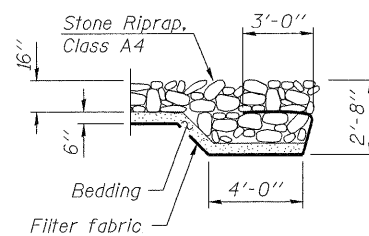
- Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All beams, bearings and other structural steel within 5 ft (measured along the beam) of either side of deck joints shall be cleaned per Near White Blast Cleaning - SSPC-SP10. The exterior surfaces and bottom of the bottom flange of the fascia beams shall be cleaned per Power Tool Cleaning - Commercial Grade.  
The designated areas cleaned per Near White Blast Cleaning - SSPC-SP10 and per Power Tool Cleaning - Commercial Grade shall be painted according to the requirements of Paint System 3 - EM/EM/AC. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No 2.5YR 3/4.
- The SSPC-QP1 and SSPC-QP2 Painting Contractor Certifications will be required for this bridge.
- A minimum of 3 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues".
- The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No. 2.5YR 3/4. See Special Provision for "Cleaning and Painting New Metal Structures".
- All new structural steel shall be shop painted with an inorganic zinc rich primer per AASHTO M 300, Type 1.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the engineer.

**TOTAL BILL OF MATERIAL**

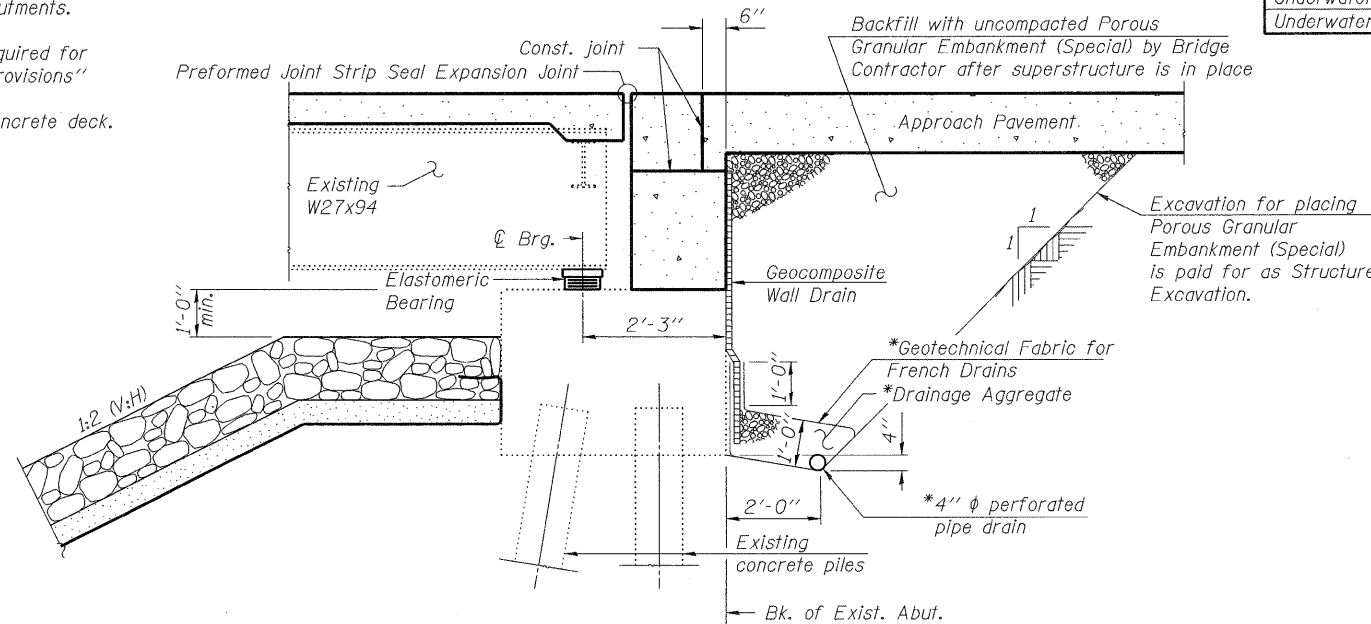
ITEM	UNIT	SUPER.	SUB.	TOTAL
Porous Granular Embankment, Special	Cu Yd	-	137	137
Stone Riprap, Class A4	Sq Yd	-	554	554
Filter Fabric	Sq Yd	-	554	554
Approach Slab Removal	Sq Yd	300	-	300
Concrete Removal	Cu Yd	-	23	23
Slope Wall Removal	Sq Yd	-	807	807
Structure Excavation	Cu Yd	-	111	111
Floor Drains	Each	6	-	6
Concrete Structures	Cu Yd	-	44	44
Concrete Superstructure	Cu Yd	303	-	303
Bridge Deck Grooving	Sq Yd	560	-	560
Protective Coat	Sq Yd	667	-	667
Furnishing and Erecting Structural Steel	L Sum	0.2	-	0.2
Stud Shear Connectors	Each	2,730	-	2,730
Jack and Remove Existing Bearings	Each	12	-	12
Jack and Reposition Bearings	Each	12	-	12
Structural Steel Removal	Pound	14,340	-	14,340
Cleaning And Painting Steel Bridge	L Sum	1	-	1
Containment And Disposal Of Lead Paint Cleaning Residues	L Sum	1	-	1
Reinforcement Bars, Epoxy Coated	Pound	69,640	6,640	76,280
Bar Splacers	Each	-	66	66
Slope Wall 6 Inch	Sq Yd	-	508	508
Name Plates	Each	1	-	1
Preformed Joint Strip Seal	Foot	86	-	86
Elastomeric Bearing Assembly, Type I	Each	7	-	7
Elastomeric Bearing Assembly, Type II	Each	7	-	7
Anchor Bolts, 1"	Each	32	-	32
Concrete Sealer	Sq Ft	-	475	475
Geocomposite Wall Drain	Sq Yd	-	64	64
Pipe Underdrains for Structures, 4"	Foot	-	123	123
Drainage Scuppers, DS-11	Each	1	-	1
Structural Repair of Concrete (Depth Greater Than 5")	Sq Ft	-	21	21
Structural Repair of Concrete (Depth Equal To or Less Than 5")	Sq Ft	-	12	12
Removal of Existing Concrete Deck	Each	1	-	1
Underwater Structure Excavation Protection - Location 1	Each	-	0.5	0.5
Underwater Structure Excavation Protection - Location 2	Each	-	0.5	0.5



SECTION A-A



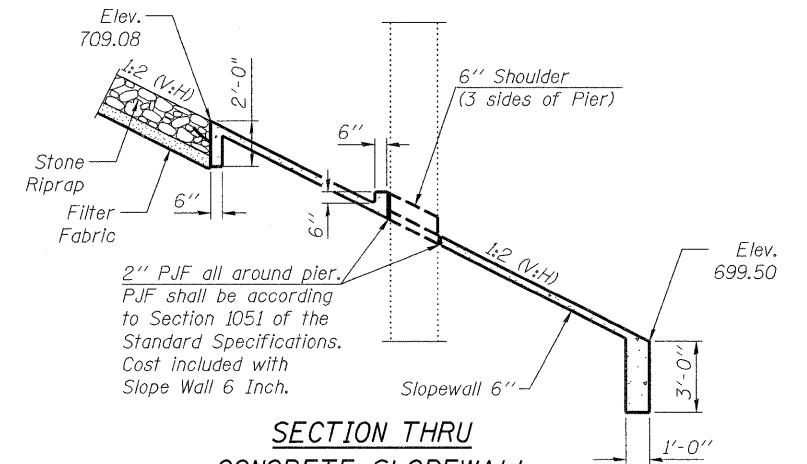
SECTION B-B



SECTION THRU EXISTING ABUTMENT

\* Included in the cost of Pipe Underdrains for Structures, 4".

Note:  
All drainage system components shall extend parallel to the abutment back wall until they intersect the wingwalls or 2'-0" from the end of the wingwalls when the wings are parallel to the abutment. The pipe shall extend under the wingwall, if necessary, until intersecting the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



SECTION THRU CONCRETE SLOPEWALL

Slope wall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. per 100 sq. ft.

**GENERAL NOTES, TOTAL BILL OF MATERIAL AND SECTION THRU EXISTING ABUTMENT SOUTHBOUND ILLINOIS ROUTE 394 OVER PLUM CREEK STATION 20+07.55**

DESIGNED	SK/GMK/LCM
CHECKED	GBC/GMK/SMK
DRAWN	RR/LCM/SK
CHECKED	GBC/GMK/SMK

SHEET NO. 2	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	332	2002-113R	WILL		173
23 SHEETS	SN-099-0183		CONTRACT NO. 62542		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

