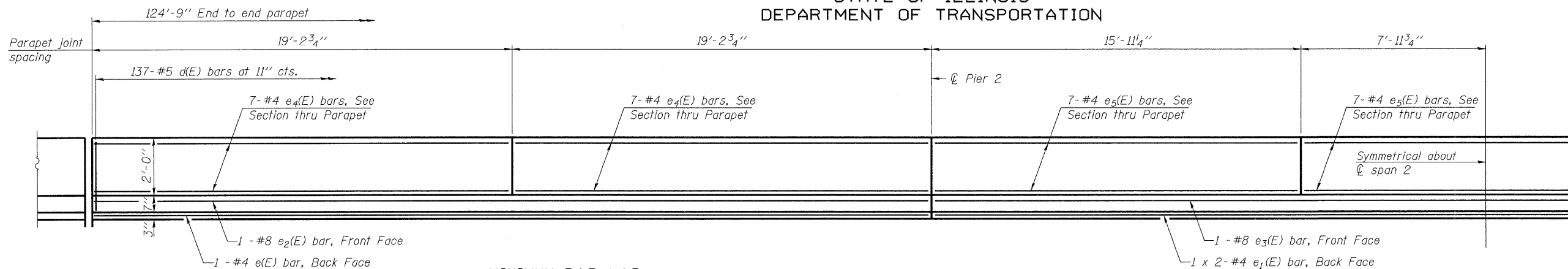


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

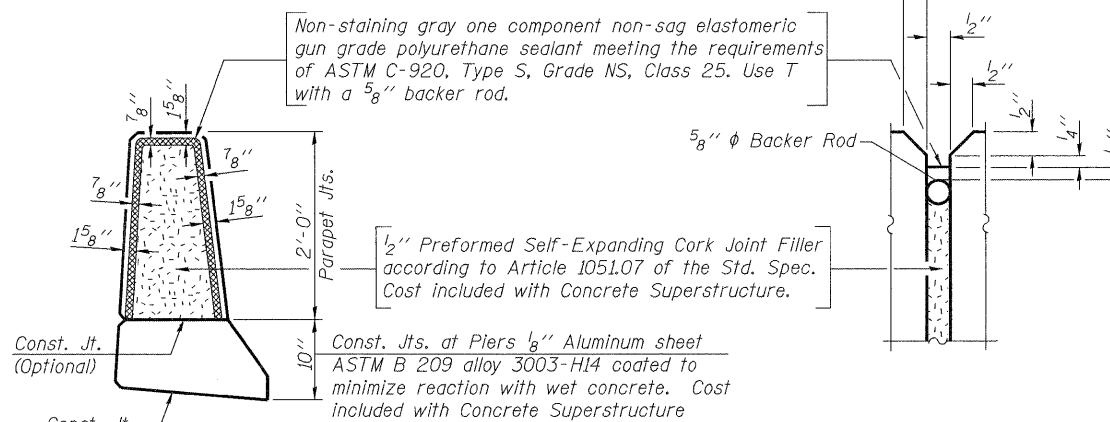
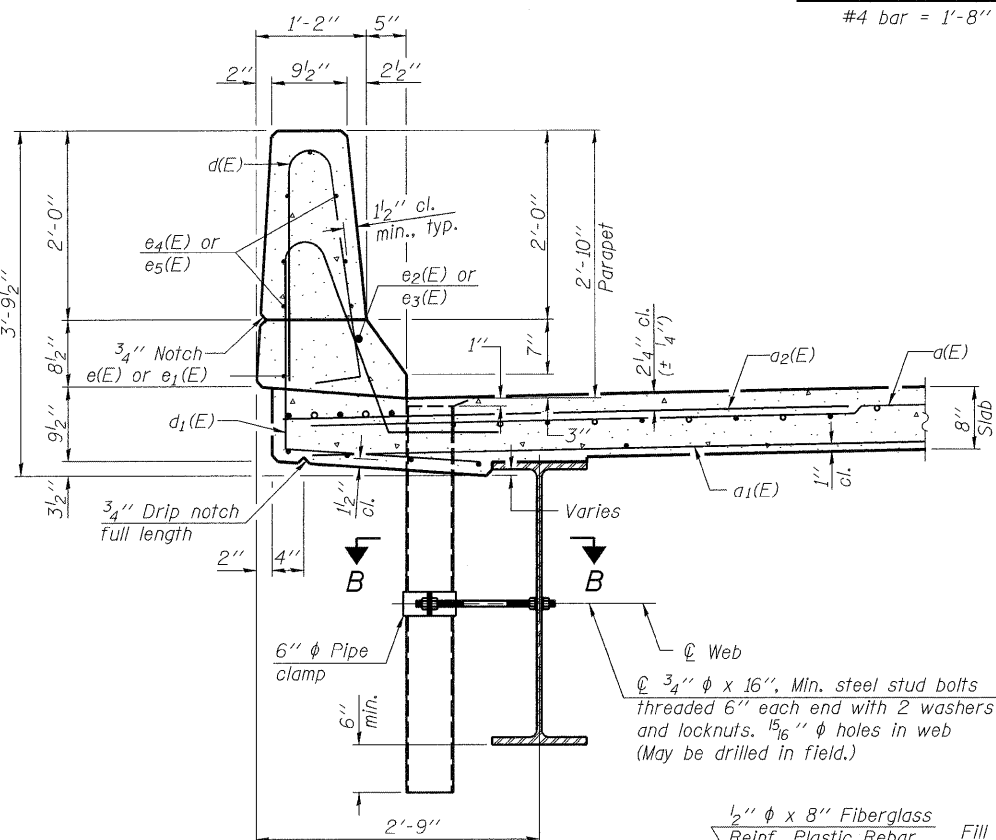


**SUPERSTRUCTURE  
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	183	#5	42'-11"	—
a1(E)	151	#5	42'-3"	—
a2(E)	354	#6	6'-0"	—
a3(E)	8	#5	2'-0"	—
b(E)	188	#5	32'-5"	—
b1(E)	88	#6	28'-6"	—
b2(E)	210	#5	26'-3"	—
d(E)	274	#5	5'-7"	⌒
d1(E)	274	#5	7'-6"	⌒
e(E)	4	#4	38'-2"	—
e1(E)	4	#4	24'-8"	—
e2(E)	4	#8	38'-2"	—
e3(E)	2	#8	47'-7"	—
e4(E)	56	#4	18'-11"	—
e5(E)	42	#4	15'-8"	—
x(E)	84	#4	3'-2"	—
Reinforcement Bars, Epoxy Coated		Pound	40,620	
Concrete Superstructure		Cu. Yds.	170	

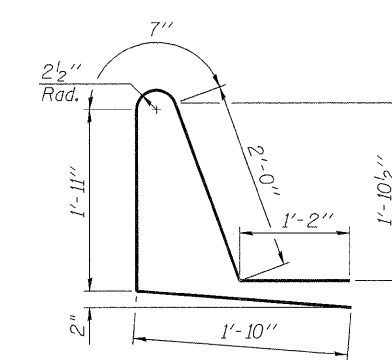
**MINIMUM BAR LAP**  
#4 bar = 1'-8"

**INSIDE ELEVATION OF PARAPET**

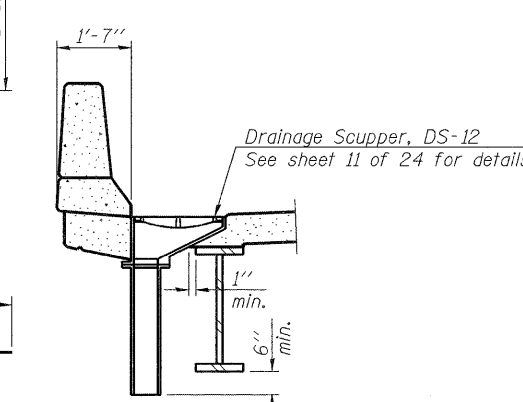


**PARAPET JOINT DETAILS**

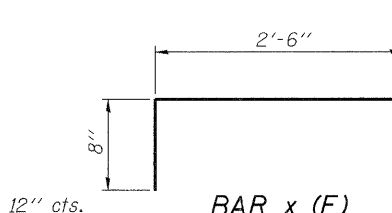
Notes:  
Drains shall be located clear of all diaphragms.  
The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to Steel Structures Painting Council's Spec. SSPC-SPI prior to painting. Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.



**BAR d1(E)**

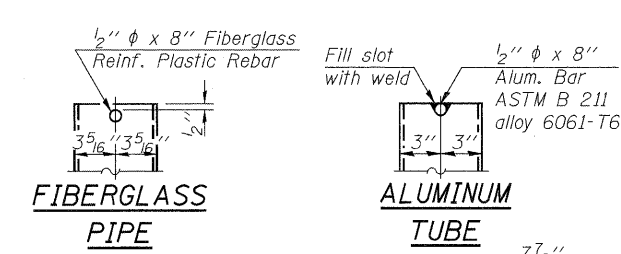


**SECTION C-C**



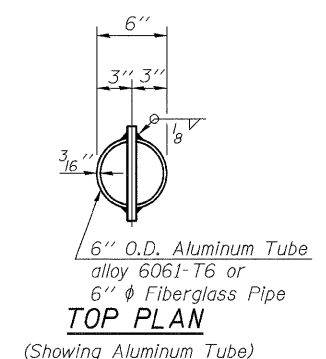
**BAR x(E)**

**SECTION THRU PARAPET**

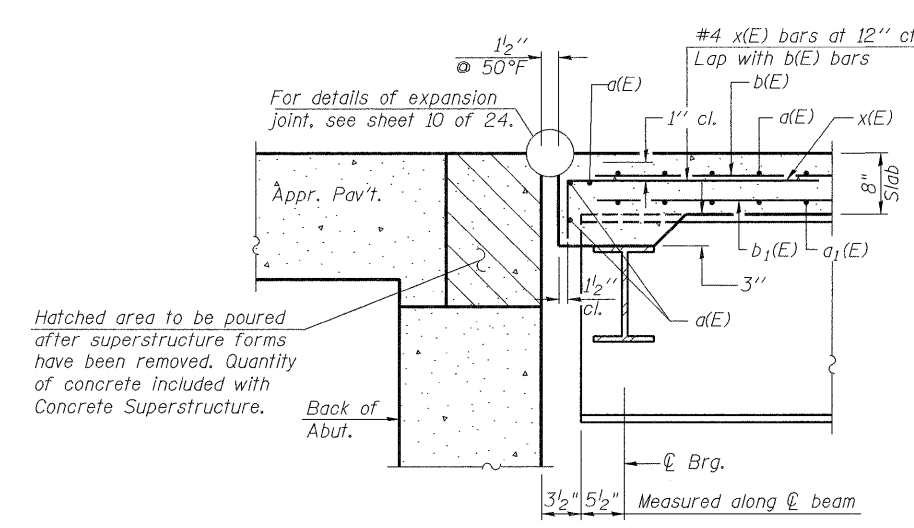


**FIBERGLASS PIPE**

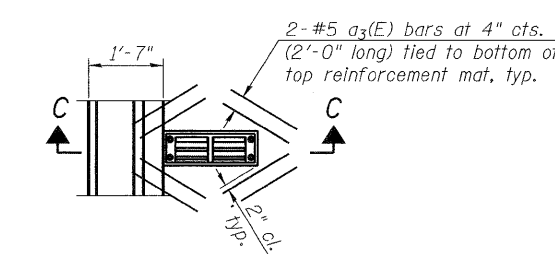
**ALUMINUM TUBE**



**TOP PLAN**  
(Showing Aluminum Tube)



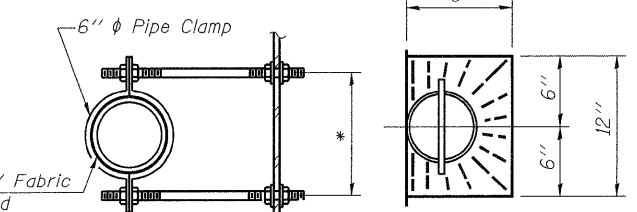
**SECTION A-A**



**PLAN**

Note:  
Cut longitudinal reinforcement to clear drainage scuppers.

**SUPERSTRUCTURE DETAILS  
SOUTHBOUND ILLINOIS ROUTE 394 OVER PLUM CREEK  
STATION 20+07.55**



**SECTION B-B**

**TOP PLAN**

DESIGNED	SK/GMK/LCM
CHECKED	GBC/GMK/SMK
DRAWN	RR/LCM/SK
CHECKED	GBC/GMK/SMK

SHEET NO. 9	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	332	2002-113R	WILL	242	180
23 SHEETS	SN-099-0183		CONTRACT NO. 62542		
	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

