

FOR INDEX OF SHEETS, SEE SHEET NO.2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID PROJECT
LAPP RESURFACING
BIG TIMBER ROAD (FAU 1302)
FROM RANDALL ROAD (FAU 2505)
TO MCLEAN BOULEVARD
PROJECT NO. M-9003(314)
SECTION NO. 09-00177-00-RS
KANE COUNTY
JOB NO. C-91-621-09**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT - LAPP		
CONTRACT NO. 63301				



PROJECT LOCATED IN THE CITY OF ELGIN

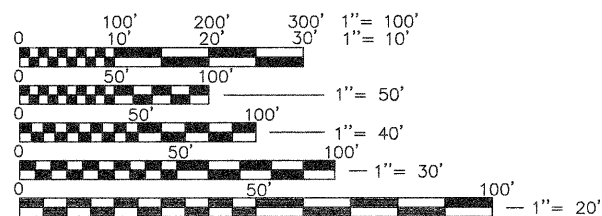
LOCATION OF SECTION INDICATED THIS: — ■ —

DESIGN DESIGNATION

BIG TIMBER ROAD: MINOR ARTERIAL
DESIGN SPEED = 45 M.P.H.
POSTED SPEED = 45 M.P.H.
ADT(2006) = 16,900 VPD (AT MCLEAN BLVD.)

FIELD ENGINEER: MARILIN SOLOMON (847) 705-4407

CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.
CONTACT: TIMOTHY V. WEIDNER (630)-466-6700



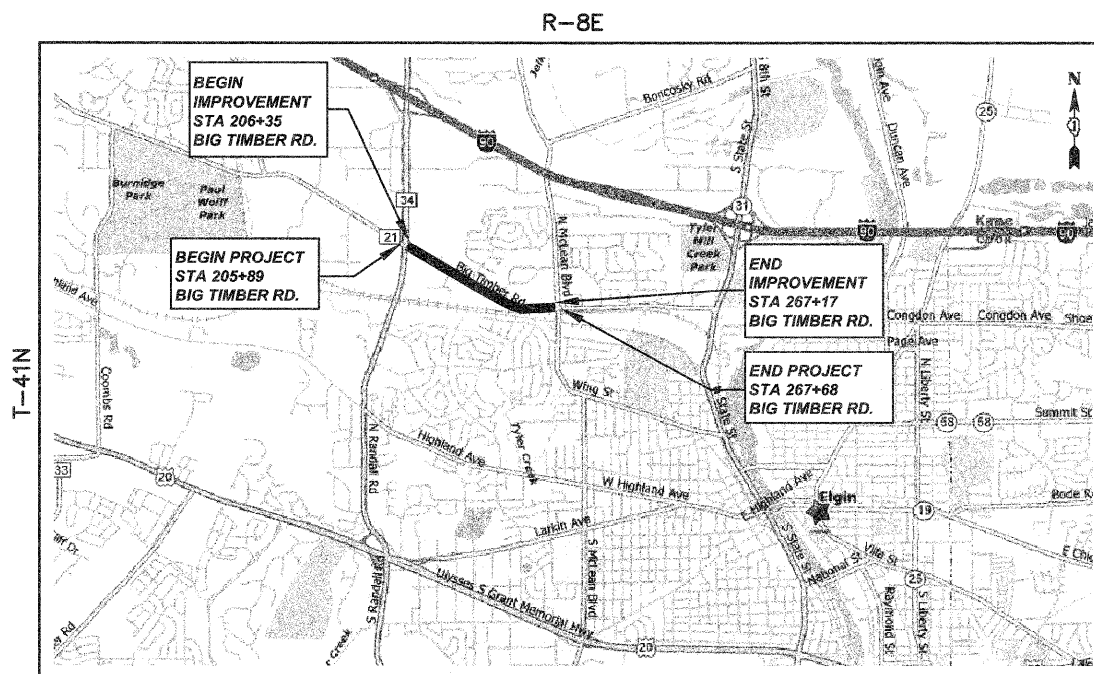
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULIE
JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811



Know what's below.
Call before you dig.

CONTRACT 63301



E 1/2 SW 1/4 AND SE 1/4 SECTION 4, SW 1/4 SECTION 3, NW 1/4 SECTION 10, 3RD P.M.

LOCATION MAP
N.T.S.

NET LENGTH OF PROJECT = 6,082 FEET (1.15 MILES)
GROSS LENGTH OF PROJECT = 6,179 FEET (1.17 MILES)

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	August 13, 2009 <i>Joseph J. ...</i> CITY OF ELGIN, CITY ENGINEER
PASSED	SEPTEMBER 1, 2009 <i>Chris ...</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	SEPTEMBER 1, 2009 <i>Diane M. O'Keefe</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 8/14/2009
BY: *Scot H. Prindville*
SCOT H. PRINDVILLE
LICENSE EXPIRES: NOVEMBER 30, 2009



SEAL

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	2
CONTRACT NO. 63301				

GENERAL NOTES AND CONSTRUCTION SPECIFICATIONS

- THE CONTRACTOR SHALL OBTAIN, ERECT, MAINTAIN AND REMOVE ALL SIGNS, BARRICADES, FLAGGERS AND OTHER TRAFFIC CONTROL DEVICES AS MAY BE NECESSARY FOR THE PURPOSE OF REGULATING, WARNING OR GUIDING TRAFFIC. PLACEMENT AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PARTS OF ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS AND THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
- LOCATION OF UTILITIES SHOWN ON PLANS ARE APPROXIMATE ONLY, AND ARE NOT NECESSARILY COMPLETE. CONTRACTOR SHALL MAKE HIS OWN INVESTIGATIONS AS TO LOCATION OF ALL EXISTING UNDERGROUND STRUCTURES, CABLES AND PIPE LINES.
- ALL ELEVATIONS ARE BASED ON U.S.G.S. DATUM.
- ALL ROAD SIGNS, STREET SIGNS AND TRAFFIC SIGNS WHICH NEED TO BE RELOCATED OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR EXCEPT THOSE THAT ARE NECESSARY FOR PROPER TRAFFIC CONTROL WHICH SHALL BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESET ALL SAID SIGNS. THE WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL PAY ITEMS, AND SHALL NOT BE PAID FOR SEPARATELY.
- SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.08.
- ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE UNIT PRICE FOR ALL REMOVAL PAY ITEMS SHALL INCLUDE ALL REQUIRED SAW CUTS.
- ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.
- CONTRACTOR SHALL PROVIDE AND INSTALL TWO WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL) THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS: AN ESTIMATED QUANTITY BASED ON FIELD OBSERVATION HAS BEEN INCLUDED IN THE CONTRACT. THE LOCATION AND LIMITS OF ALL JOINT OR CRACK FILLING SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. IF THE MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS IS NOT REQUIRED, THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- BEFORE STARTING ALL EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 OR 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF ELGIN.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

- WHENEVER, DURING CONSTRUCTION OPERATIONS LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF CONTRACT PAY ITEMS.
- EXISTING PUBLIC AND PRIVATE UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION OBTAINED FROM UTILITY COMPANIES, MUNICIPALITIES, AND SURVEYS. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE LOCATION OF ALL UTILITIES AND STRUCTURES THAT MAY BE FOUND IN THE VICINITY OF THE CONSTRUCTION. HE WILL ALSO ASSUME RESPONSIBILITY FOR ALL UTILITIES WHETHER SHOWN OR NOT, AND MUST REALIZE THAT THE ACTUAL LOCATIONS AND/OR ELEVATIONS OF THE UTILITIES MAY BE DIFFERENT THAN INDICATED.

SHOULD ANY DAMAGES OCCUR DUE TO THE CONTRACTOR'S NEGLIGENCE, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL MAKE REPAIRS IN A MANNER ACCEPTABLE TO THE ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES OF HIS CONSTRUCTION SCHEDULE AND COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY COMPANIES SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER.
- DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR TO REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF TO BE MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
- BIG TIMBER ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. WHEN NECESSARY TO CLOSE ONE LANE OF BIG TIMBER ROAD DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF SIGNS AND FLAGMEN AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL OTHER TIMES WHEN NON-CROSSING WORK OR NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON. ACCESS TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. ANY TEMPORARY ACCESS CLOSURES SHALL BE COORDINATED WITH THE AFFECTED PROPERTY OWNER.

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10.	PLAN, CONSTRUCTION NOTES, AND DETAILS
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16.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS
17.	(TS-07) DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

STATE STANDARDS

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR SLOW MOVING OPERATION, FOR SPEEDS >45 MPH
701601-06	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

LEGEND

EXISTING	DESCRIPTION	PROPOSED
	SANITARY SEWER	
	STORM SEWER	
	END SECTION	
	WATER MAIN & SIZE	
	WATER SERVICE & BOX	
	CONTOUR	
	GAS MAIN	
	TELEPHONE CABLE	
	MANHOLE	
	CATCH BASIN	
	INLET	
	HYDRANT	
	VALVE VAULT	
	TREE	
	ELEVATION	
	STREET LIGHT	
	SIGNS	
	FOUND IRON PIPE	
	GUY WIRE	
	FLAG POLE	
	UTILITY POLE	
	UTILITY PEDESTAL	
	HANDHOLE	
	SIGNAL POST OR MAST ARM	
	CONTROLLER	
	HANDHOLE	

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CITY OF ELGIN

NO.	DATE	REVISIONS

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**GENERAL NOTES, STATE
STANDARDS, AND INDEX
OF SHEETS**

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-CVR
SHEET	2 OF 17

Path: \\SISKIYOU\EG0901\DWG\JWG_FINAL_ENG\EG0901-CVR

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	CONST. CODE
			1000
			TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	7,630
40600300	AGGREGATE (PRIME COAT)	TON	80
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	50
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2,140
40600895	CONSTRUCTING TEST STRIP	EACH	1
Δ 40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	220
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4,280
Δ 42400800	DETECTABLE WARNINGS	SQ FT	176
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	22,020
Δ 44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	16,120
Δ 44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	570
Δ 44004610	SIDEWALK REMOVAL AND REPLACEMENT (SPECIAL)	SQ FT	2,525
Δ 44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	35
Δ 44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	50
Δ 44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	260
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	38,140
60255500	MANHOLES TO BE ADJUSTED	EACH	9
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	16
67100100	MOBILIZATION	L SUM	1
Δ 70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	3,630
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25,230
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,210
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	500
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	23,500
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,730
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	505
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	365
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	410
Δ * 81400115	HANDHOLE TO BE ADJUSTED	EACH	5
Δ * 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1,540
Δ X0321556	SANITARY MANHOLES TO BE ADJUSTED	EACH	8
Δ XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	7
Δ XX006425	RESTORATION	SQ YD	170
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1

Δ SEE SPECIAL PROVISIONS
* SPECIALTY ITEMS

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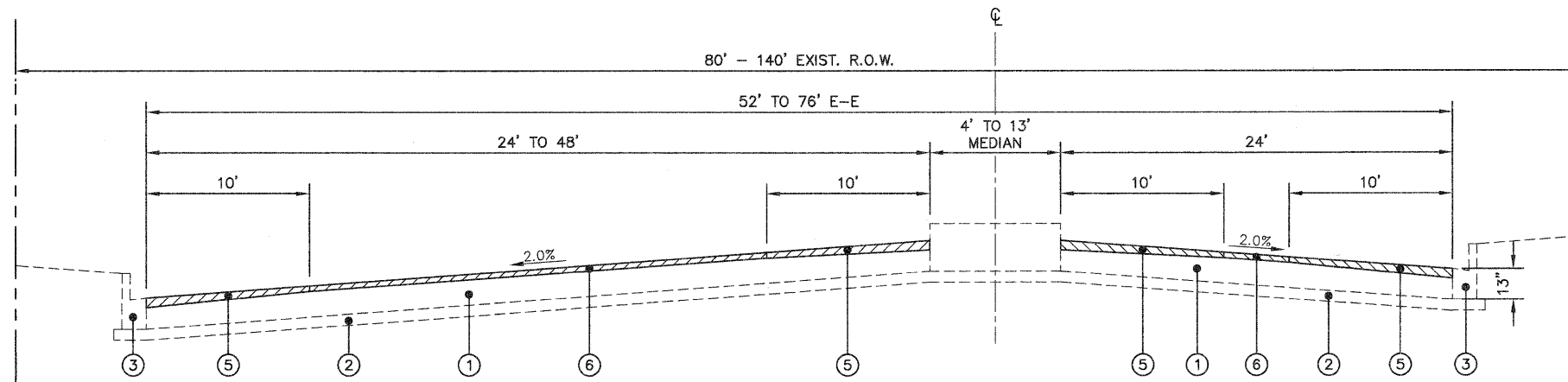
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**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

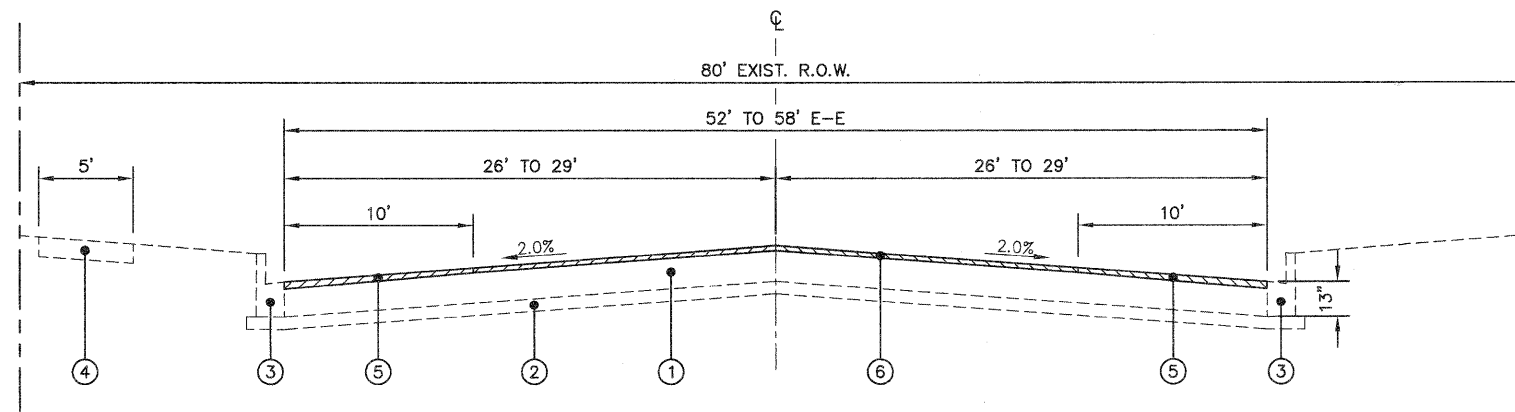
SUMMARY OF QUANTITIES

DATE: AUGUST 2009
PROJECT NO: EG0901
FILE: EG0901-CVR
SHEET **3** OF **17**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-B5	KANE	17	4
CONTRACT NO. 63301				



EXISTING TYPICAL SECTION
STA 206+35 TO STA 218+79, BIG TIMBER ROAD
 N.T.S.



EXISTING TYPICAL SECTION
STA 218+79 TO STA 267+17, BIG TIMBER ROAD
 N.T.S.

LEGEND	
①	EXISTING 13" ASPHALT PAVEMENT
②	EXISTING 4" AGGREGATE BASE COURSE
③	EXISTING B-6.12 COMBINATION CURB AND GUTTER
④	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5"
⑤	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
⑥	HOT-MIX ASPHALT SURFACE REMOVAL, 2.0"
⑦	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
⑧	LEVELING BINDER (MACHINE METHOD), N70, 1.0"
⑨	BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
⑩	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑪	PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)

Plotted: August 26, 2009 @ 10:10 AM By: Kris Pung - Tab: 04 Section - 22x34

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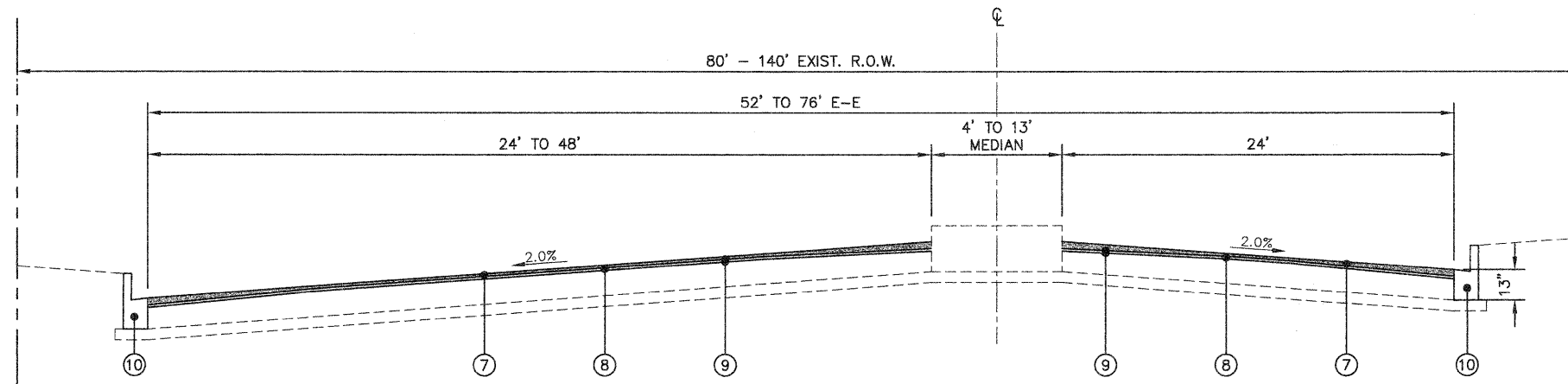
NO.	DATE	REVISIONS

BIG TIMBER ROAD
LAPP IMPROVEMENTS

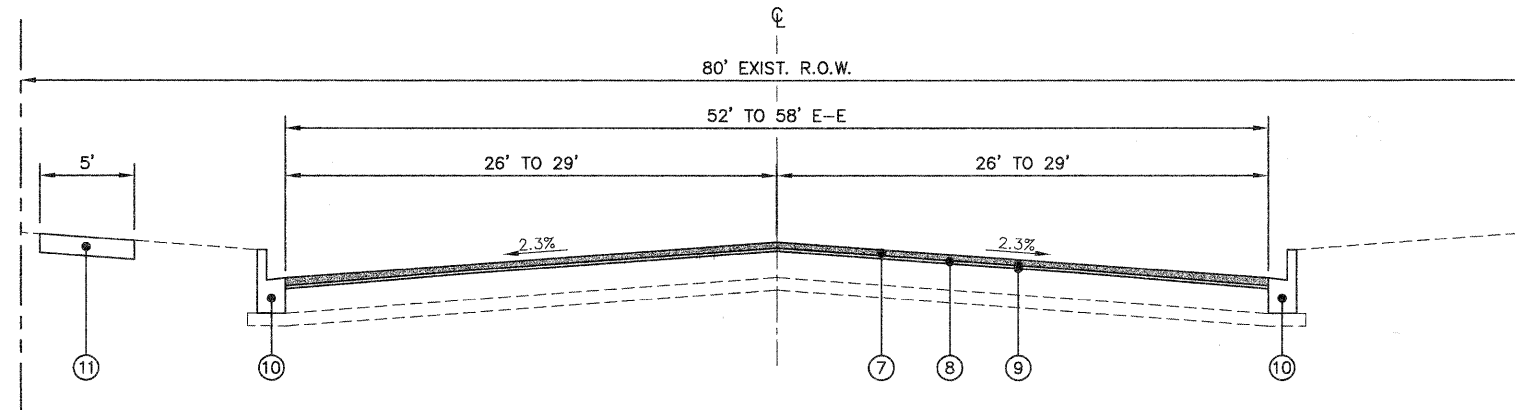
TYPICAL EXISTING
CROSS SECTIONS
AND REMOVAL WORK

DATE:	AUGUST 2009
PROJECT NO:	EG0901
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SHEET	4 OF 17

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PROPOSED TYPICAL SECTION
STA 206+35 TO STA 218+79, BIG TIMBER ROAD
 N.T.S.



PROPOSED TYPICAL SECTION
STA 218+79 TO STA 267+17, BIG TIMBER ROAD
 N.T.S.

LEGEND

- | | |
|---|---|
| ① EXISTING 13" ASPHALT PAVEMENT | ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0" |
| ② EXISTING 4" AGGREGATE BASE COURSE | ⑧ LEVELING BINDER (MACHINE METHOD), N70, 1.0" |
| ③ EXISTING 8-6.12 COMBINATION CURB AND GUTTER | ⑨ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT) |
| ④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5" | ⑩ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) |
| ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | ⑪ PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) |
| ⑥ HOT-MIX ASPHALT SURFACE REMOVAL, 2.0" | |

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5 MM)	4% @ 70 Gyr.
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM)	4% @ 70 Gyr.
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS
 PATCHING SHALL BE PERFORMED AFTER MILLING OPERATIONS

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NO.	DATE	REVISIONS

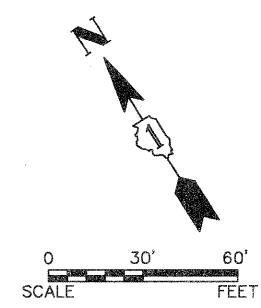
BIG TIMBER ROAD
LAPP IMPROVEMENTS

TYPICAL PROPOSED
CROSS SECTIONS

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-CVR
SHEET	5 OF 17

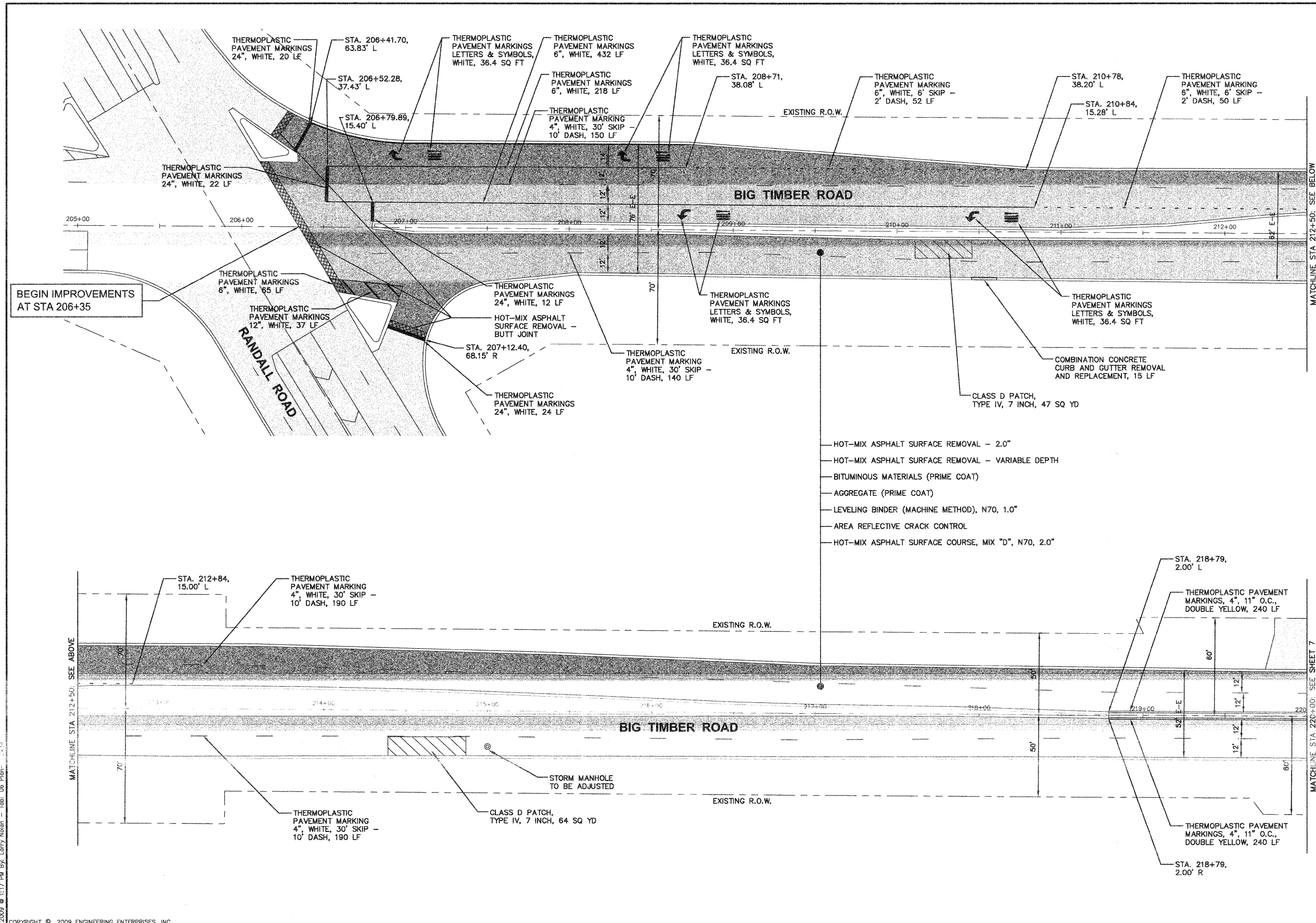
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	6
CONTRACT NO. 63301				



LEGEND

- RESURFACING
- CLASS C & D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT & SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT



- HOT-MIX ASPHALT SURFACE REMOVAL - 2.0"
- HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- BITUMINOUS MATERIALS (PRIME COAT)
- AGGREGATE (PRIME COAT)
- LEVELING BINDER (MACHINE METHOD), N70, 1.0"
- AREA REFLECTIVE CRACK CONTROL
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"

- NOTES:**
1. AREA REFLECTIVE CRACK CONTROL TO BE USED OVER THE ENTIRE AREA AFTER LEVELING BINDER HAS BEEN PLACED.
 2. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER.

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**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

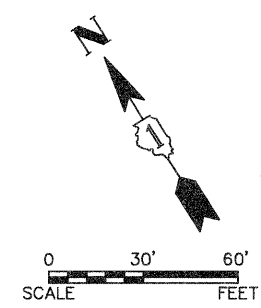
**PLAN, CONSTRUCTION
NOTES, AND DETAILS**

DATE:	AUGUST 2009
PROJECT NO.:	EG0901
FILE:	EG0901-PLAN
SHEET	6 OF 17

Pictured August 13, 2009 @ 1:17 PM By: Larry Nelson - Tab: 06 Plan - 24.5
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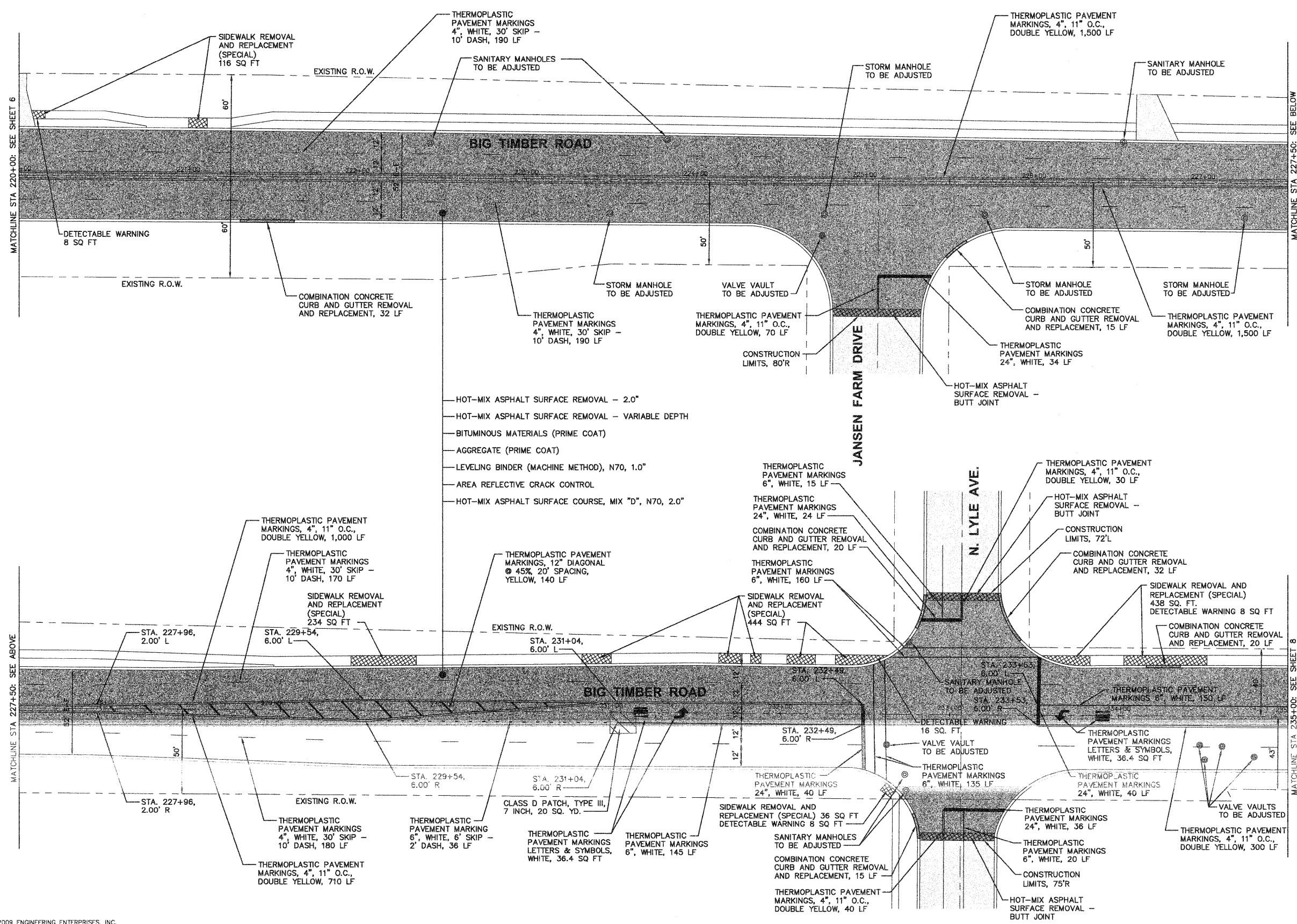
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	7
CONTRACT NO. 63301				



LEGEND

- RESURFACING
- CLASS C & D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT & SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT



- HOT-MIX ASPHALT SURFACE REMOVAL - 2.0"
- HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
- BITUMINOUS MATERIALS (PRIME COAT)
- AGGREGATE (PRIME COAT)
- LEVELING BINDER (MACHINE METHOD), N70, 1.0"
- AREA REFLECTIVE CRACK CONTROL
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"

- NOTES:**
1. AREA REFLECTIVE CRACK CONTROL TO BE USED OVER THE ENTIRE AREA AFTER LEVELING BINDER HAS BEEN PLACED.
 2. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER.

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CITY OF ELGIN	
NO.	DATE

REVISIONS	
NO.	DATE

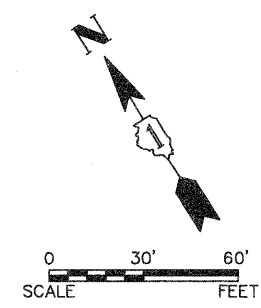
**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION
NOTES, AND DETAILS**

DATE:	AUGUST 2009
PROJECT NO.:	EG0901
FILE:	EG0901-PLAN
SHEET	7 OF 17

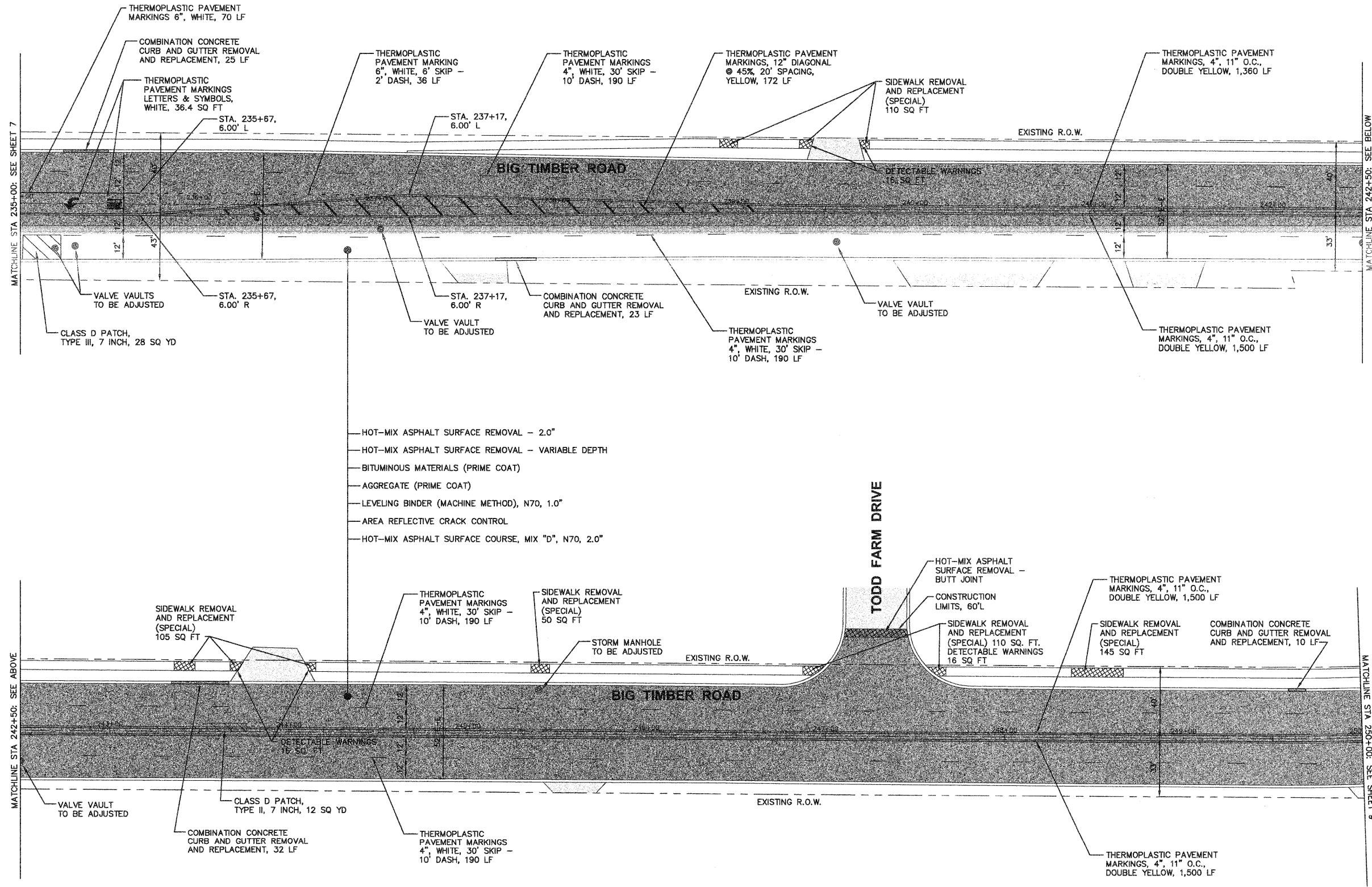
Printed: August 13, 2009 @ 9:19 AM By: Larry Nelson - Tab: C:\p\eg0901.dwg PLOT: PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	8
CONTRACT NO. 63301				



LEGEND

- RESURFACING
- CLASS C & D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT & SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT



- NOTES:**
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NO.	DATE	REVISIONS

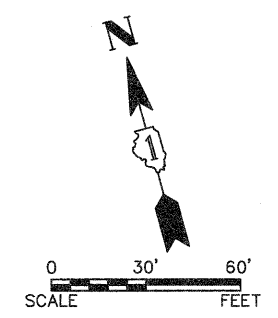
**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION
NOTES, AND DETAILS**

DATE:	AUGUST 2009
PROJECT NO.:	EG0901
FILE:	EG0901-PLAN
SHEET	8 OF 17

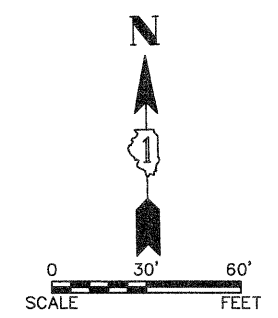
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P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	9
CONTRACT NO. 63301				

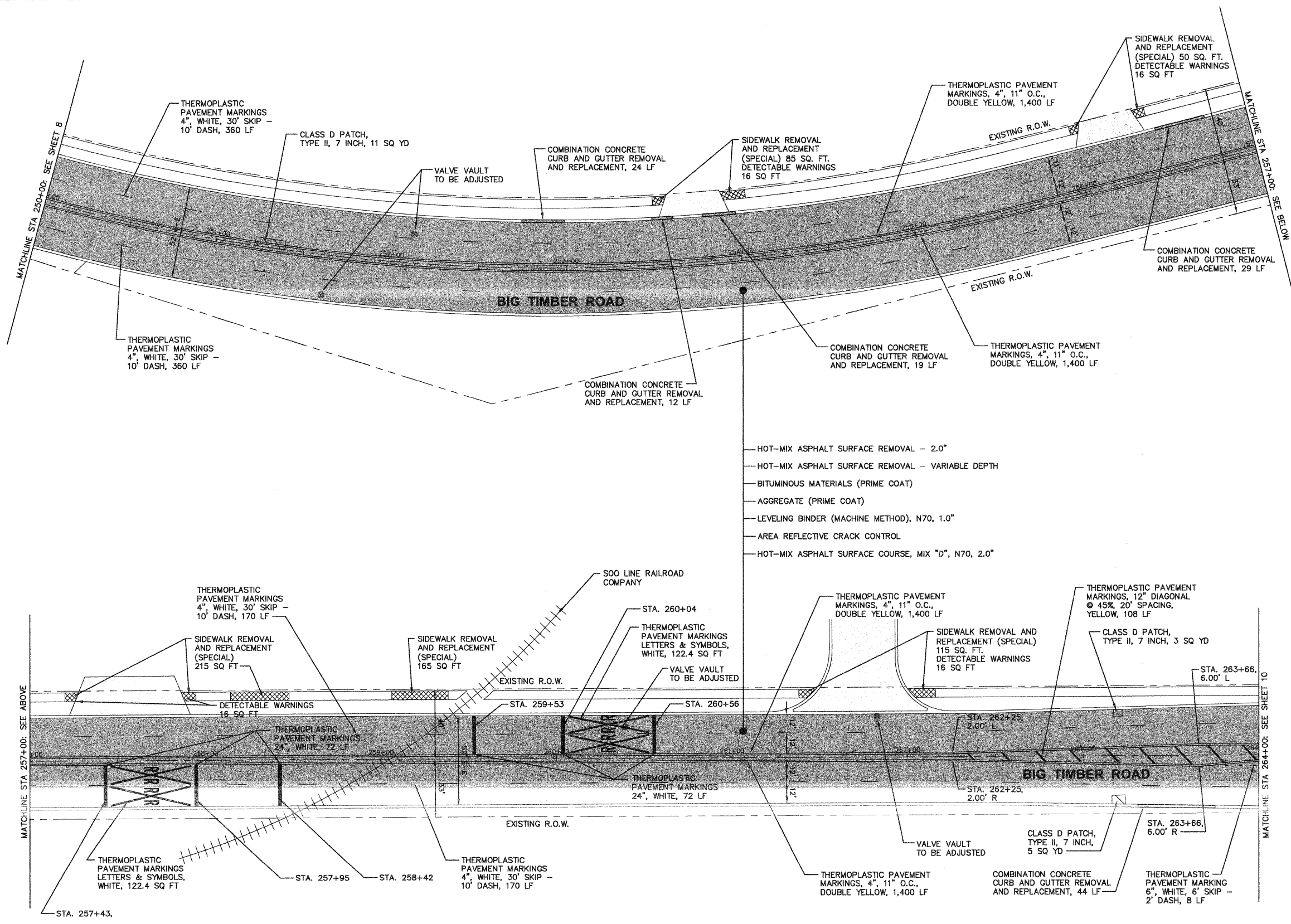


LEGEND

- RESURFACING
- CLASS C & D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT & SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT



- NOTES:**
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HOT-MIX ASPHALT SURFACE REMOVAL - 2.0"
 HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
 BITUMINOUS MATERIALS (PRIME COAT)
 AGGREGATE (PRIME COAT)
 LEVELING BINDER (MACHINE METHOD), N70, 1.0"
 AREA REFLECTIVE CRACK CONTROL
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"

Plotted August 26, 2009 @ 11:35 AM By: Kris Pung - Tab: 09 Plan - 22-34

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NO.	DATE	REVISIONS

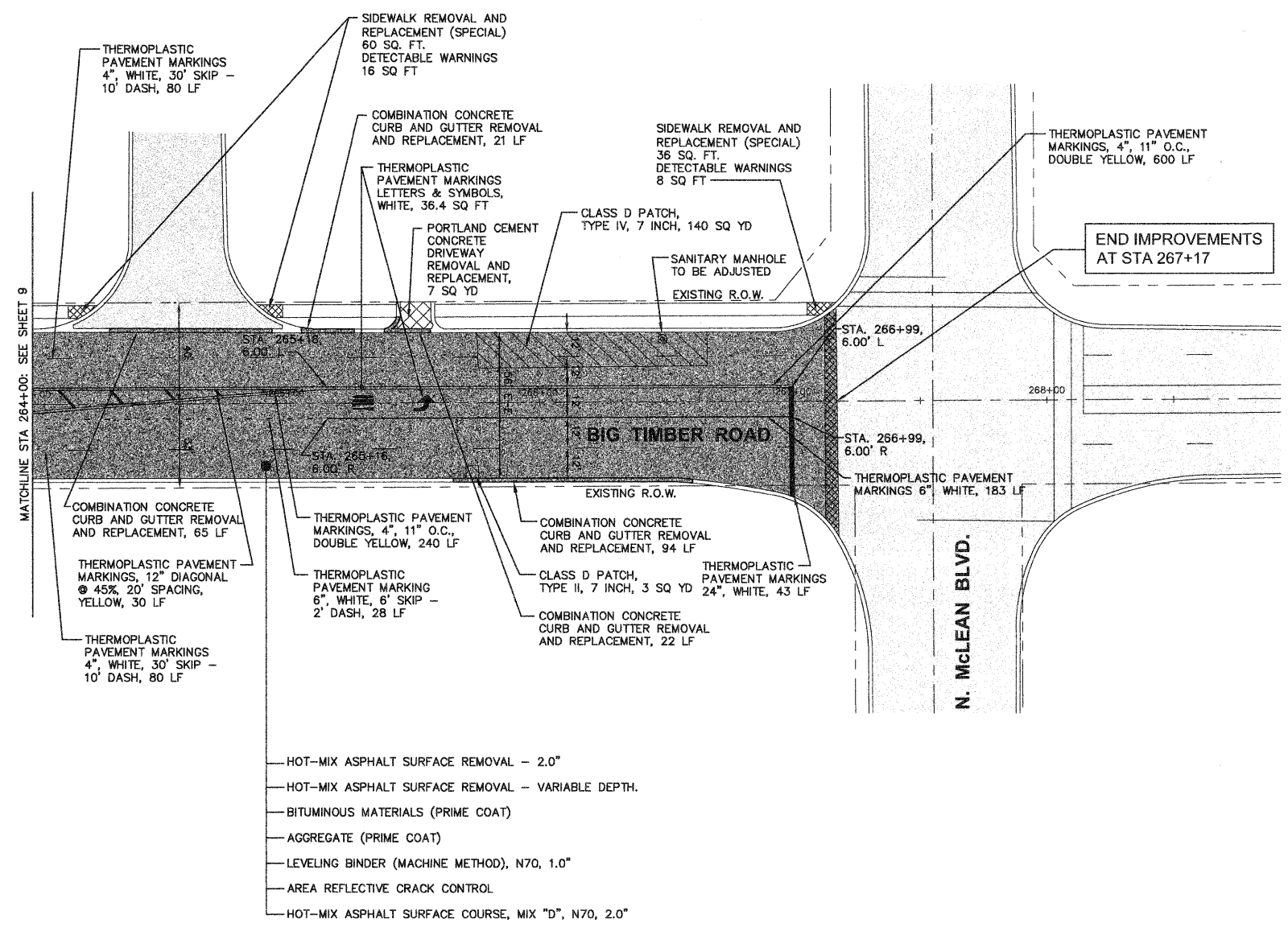
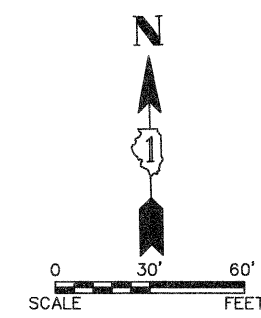
**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION
NOTES, AND DETAILS**

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-PLAN
SHEET	9 OF 17

Path: \\sbsrpg01\eg0901\DWG\TBL\ENG\EG0901-PLAN

P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	10
CONTRACT NO. 03301				



LEGEND

- RESURFACING
- CLASS C & D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT & SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT

NOTES:

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2. BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER.

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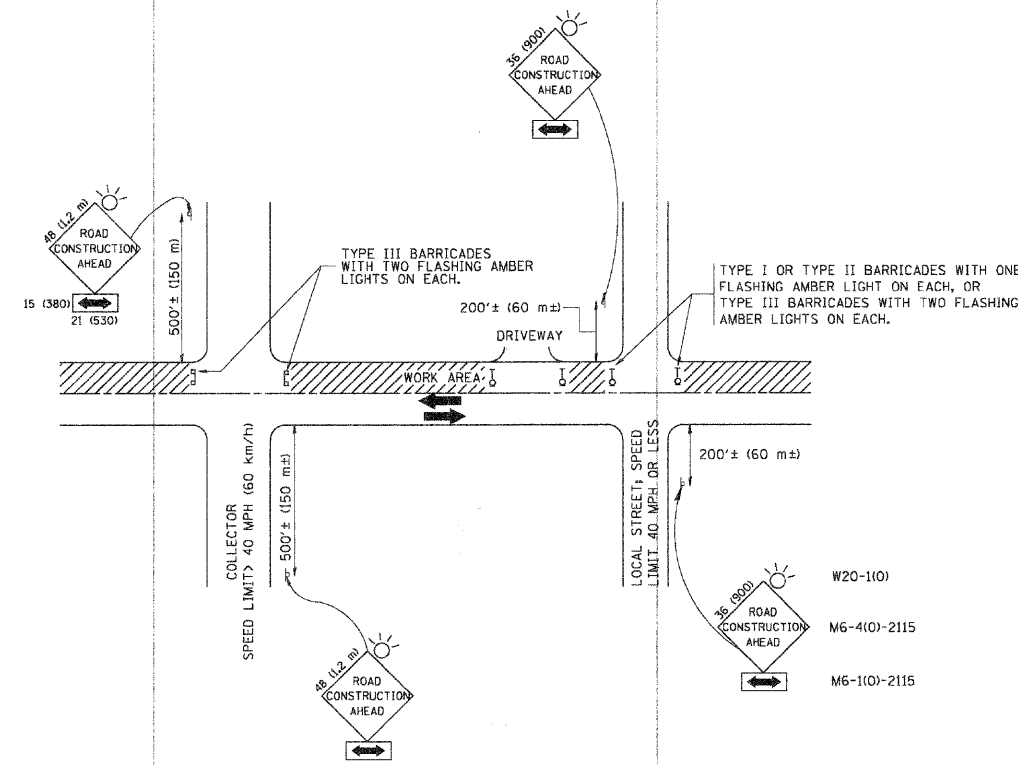
NO.	DATE	REVISIONS

**BIG TIMBER ROAD
 LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION
 NOTES, AND DETAILS**

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-PLAN
SHEET	10 OF 17

Path: \\S:\SKETCH\EG0901\DWG\DWG_FINAL_ENG\EG0901-PLAN



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAYS:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.8000" / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-09	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-10			
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

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CITY OF ELGIN

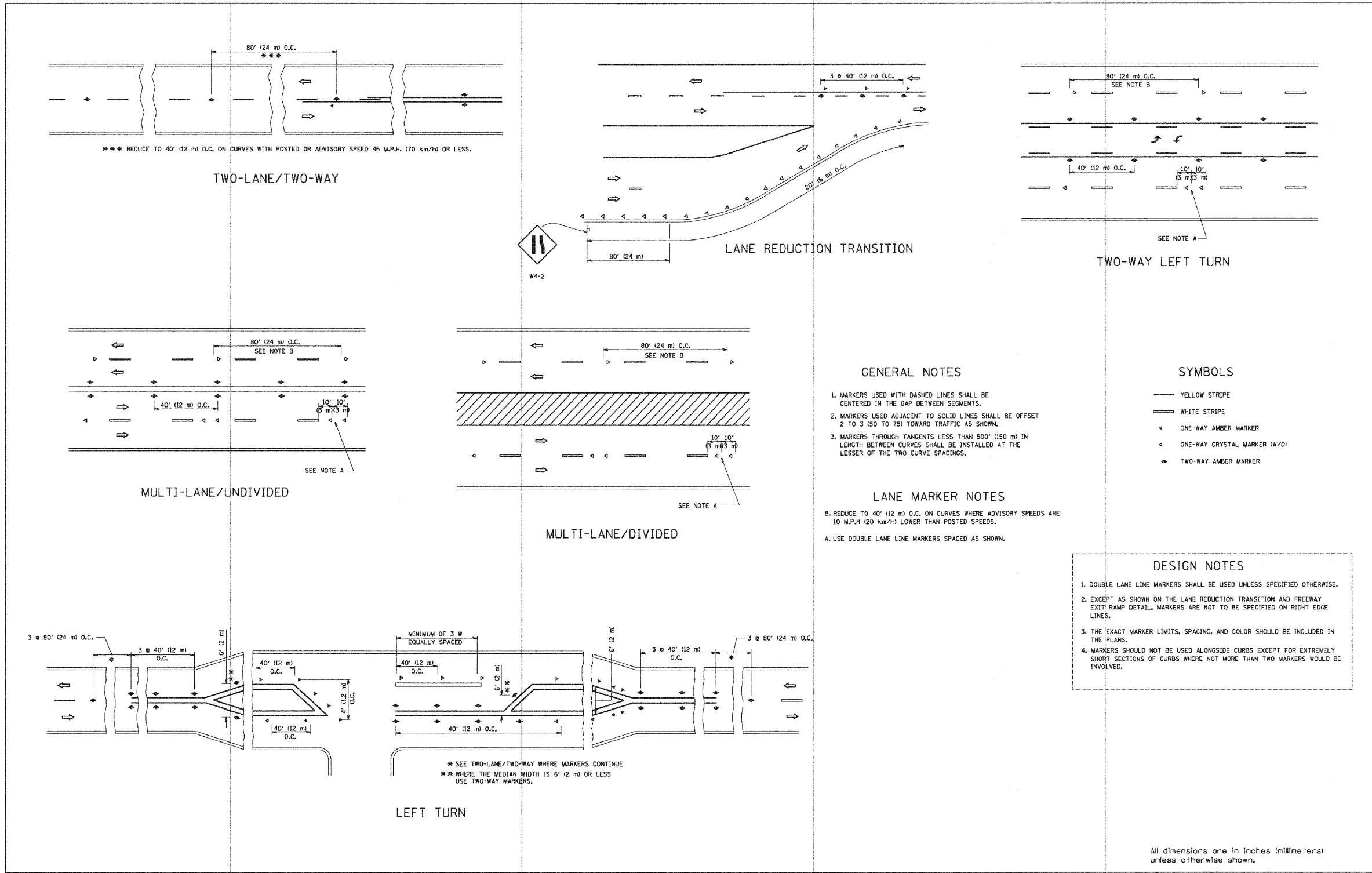
NO.	DATE	REVISIONS

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**TRAFFIC CONTROL AND PROTECTION
FOR SIDE ROADS, INTERSECTIONS,
AND DRIVEWAYS**

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-CVR
SHEET	11 OF 17

Path: \\S:\PROJ\EG0901\DWG\DWG_FINAL\ENG\EG0901-CVR



GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE TO M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50,000 / IN.	PLOT DATE = 1/4/2009	CHECKED - DATE -	SCALE: NONE		SHEET NO. 1 OF 1	SHEETS	STA. TO STA.	TC-11		CONTRACT NO.	

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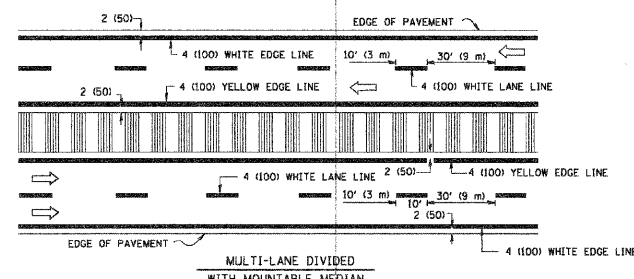
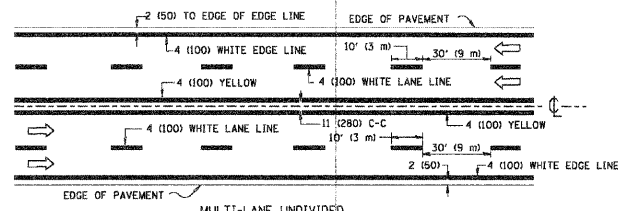
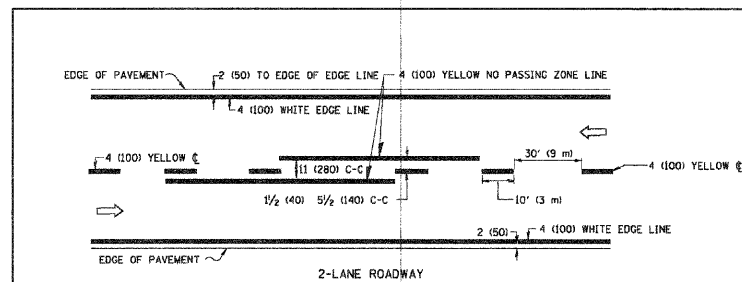
CITY OF ELGIN				
NO.	DATE	REVISIONS		

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

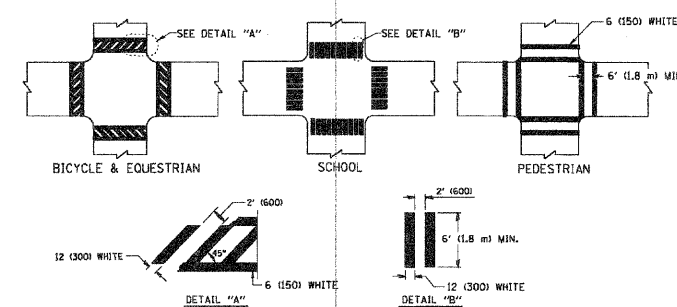
**TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT
MARKERS (SNOW-PLOW RESISTANT)**

DATE:	AUGUST 2009
PROJECT NO.:	EG0901
FILE:	EG0901-CVR
SHEET	12 OF 17

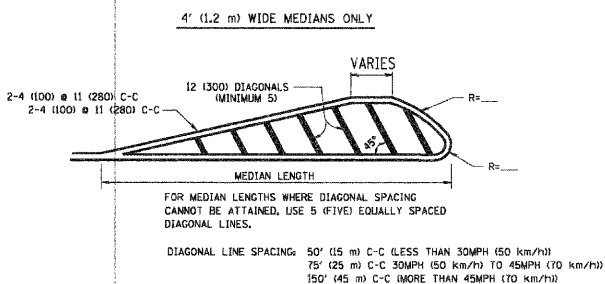
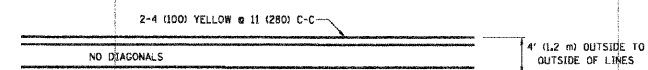
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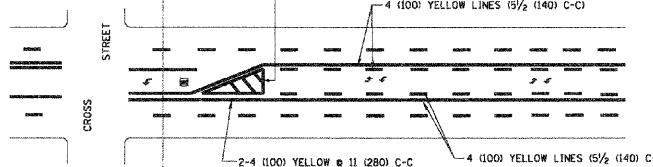
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE.
TYPICAL LANE AND EDGE LINE MARKING



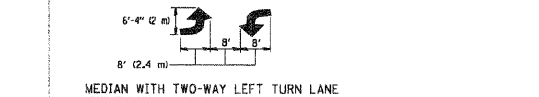
TYPICAL CROSSWALK MARKING



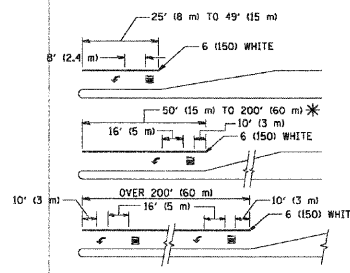
MEDIANS OVER 4' (1.2 m) WIDE



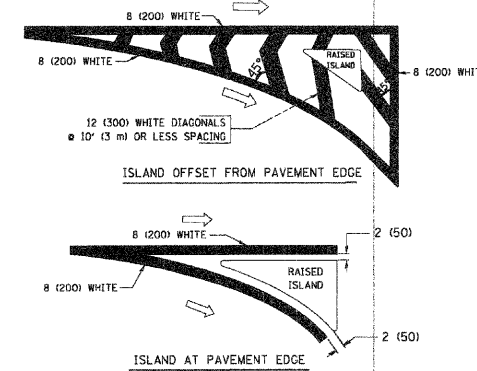
TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW WHITE WHITE	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. ONE WAY TRAFFIC
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "X" IS 3.6 SQ. FT. (0.33 m²) EACH "X" IS 54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 10-09-96
		CHECKED -	REVISED - A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		TC-13		CONTRACT NO.		
SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.		

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CITY OF ELGIN

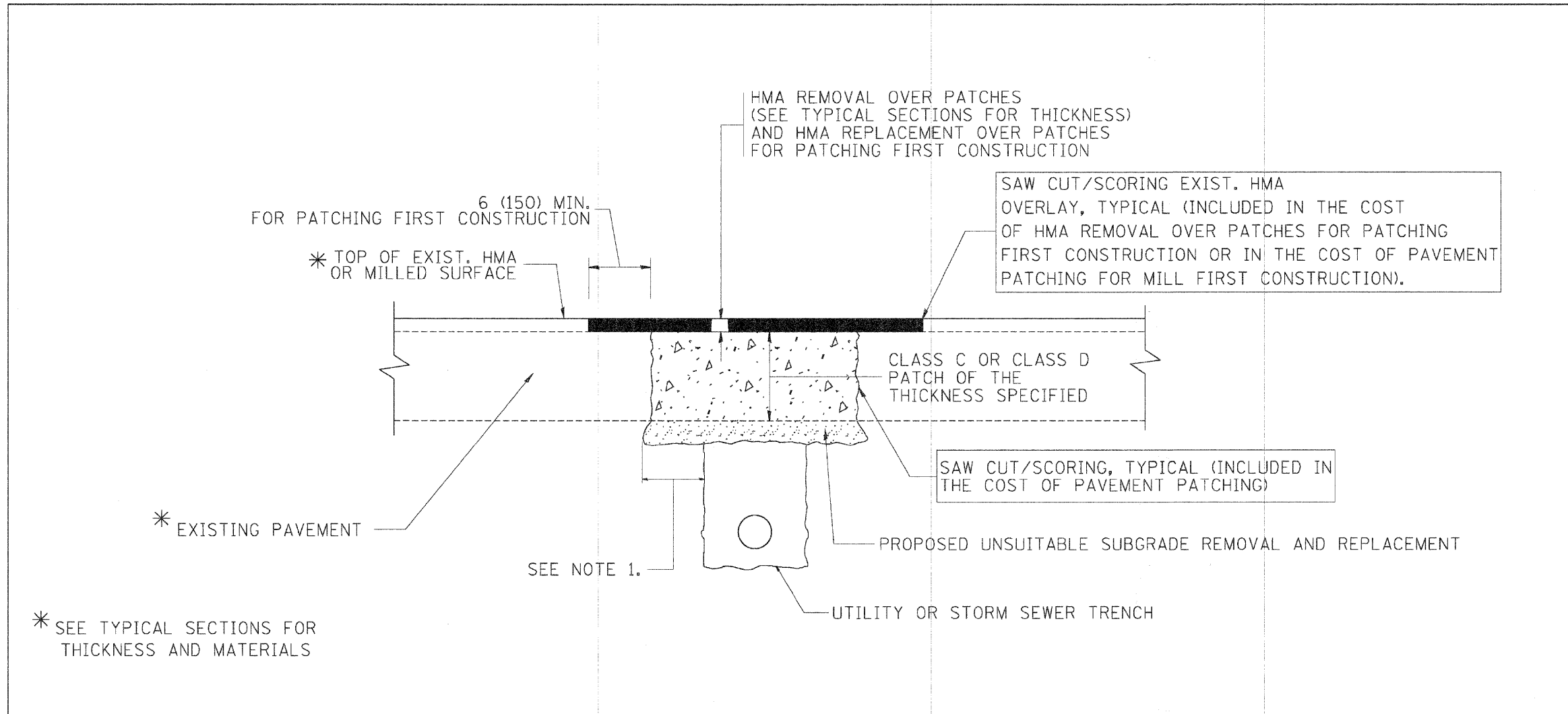
**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**DISTRICT ONE -
TYPICAL PAVEMENT MARKINGS**

DATE: AUGUST 2009
 PROJECT NO: EG0901
 FILE: EG0901-CVR
 SHEET **13** OF **17**

NO.	DATE	REVISIONS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-RS	KANE	17	14
CONTRACT NO. 63301				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\elgin\bd22\34\bd22.dgn	USER NAME = baward	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 09-04-07
		DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		BD400-04 (BD-22)		KANE	17	14
SHEET NO. 1 OF 1 SHEETS		CONTRACT NO.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

Plotted: August 25, 2009 3:16 PM By: kch_pung - Tab: 14 BD-22 22x34

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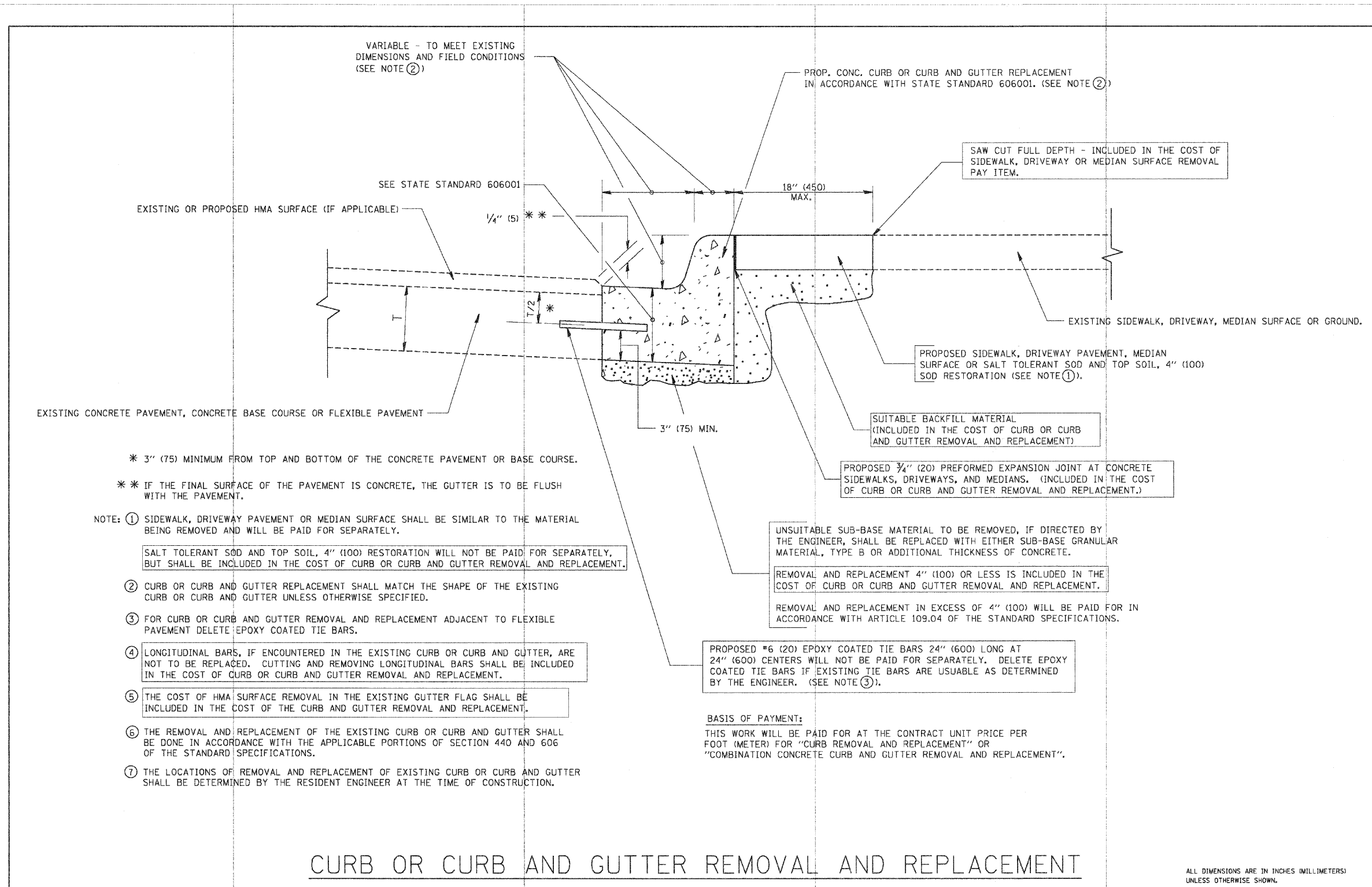
NO.	DATE	REVISIONS

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

DATE:	AUGUST 2009
PROJECT NO.:	EG0901
FILE:	EG0901-CVR
SHEET	14 OF 17

Path: H:\SBS\PROJ\EG0901\WORK\DWG_FINAL\ENGL\EG0901-CVR



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\data\22x34\bd24.dgn	USER NAME = ggg11enobts	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
PLOT SCALE = 5/8" = 1' / IN.	CHECKED - M. GOMEZ 01-22-01	DATE - 03-11-94	REVISED - R. BORO 01-01-01	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	CONTRACT NO. BD600-06 (BD-24)
PLOT DATE = 1/4/2009				STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS / FED. AID PROJECT

Plotted: August 13, 2009 7:40 AM By: Larry Nelson - Tab: 15, BD-24, 22x34

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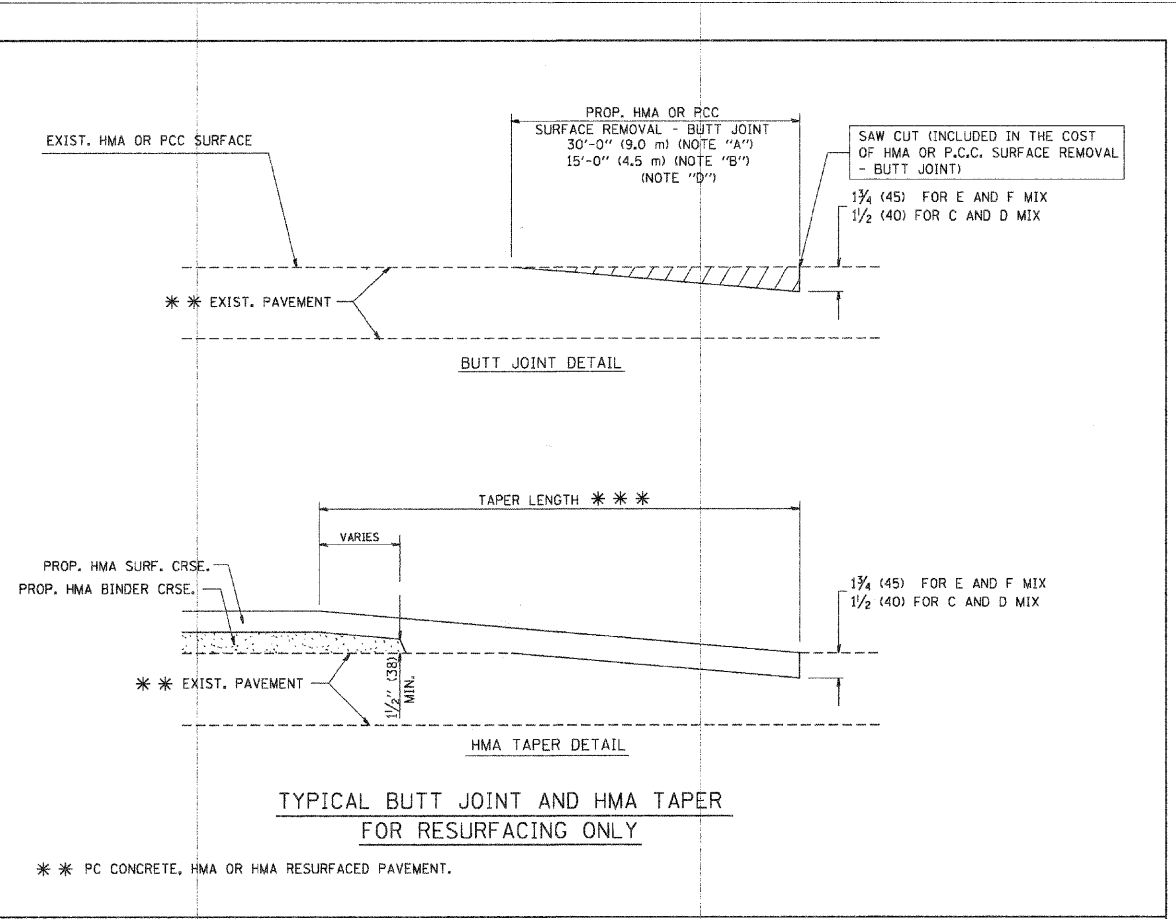
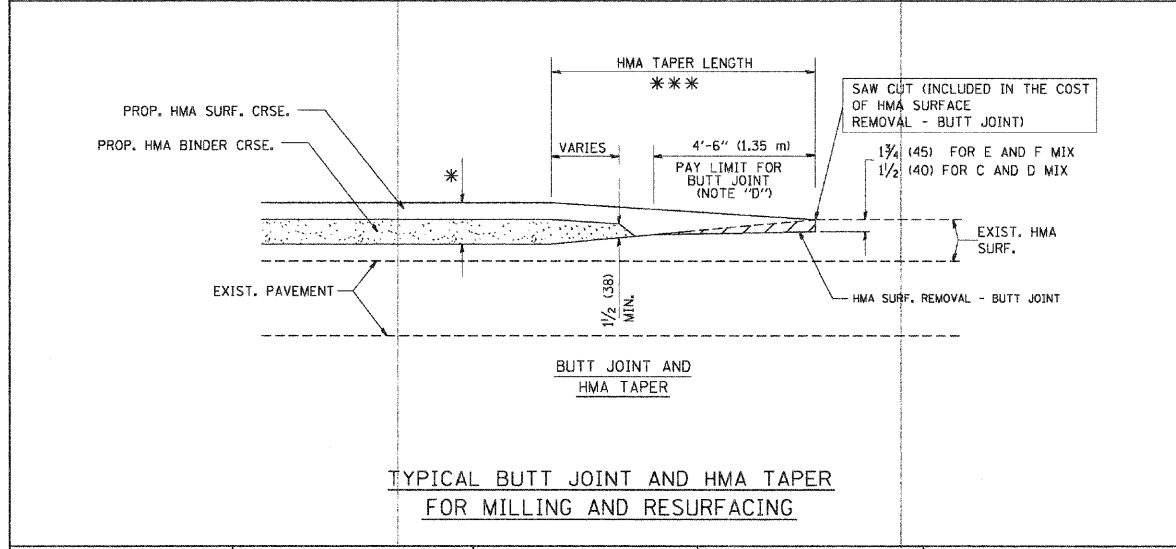
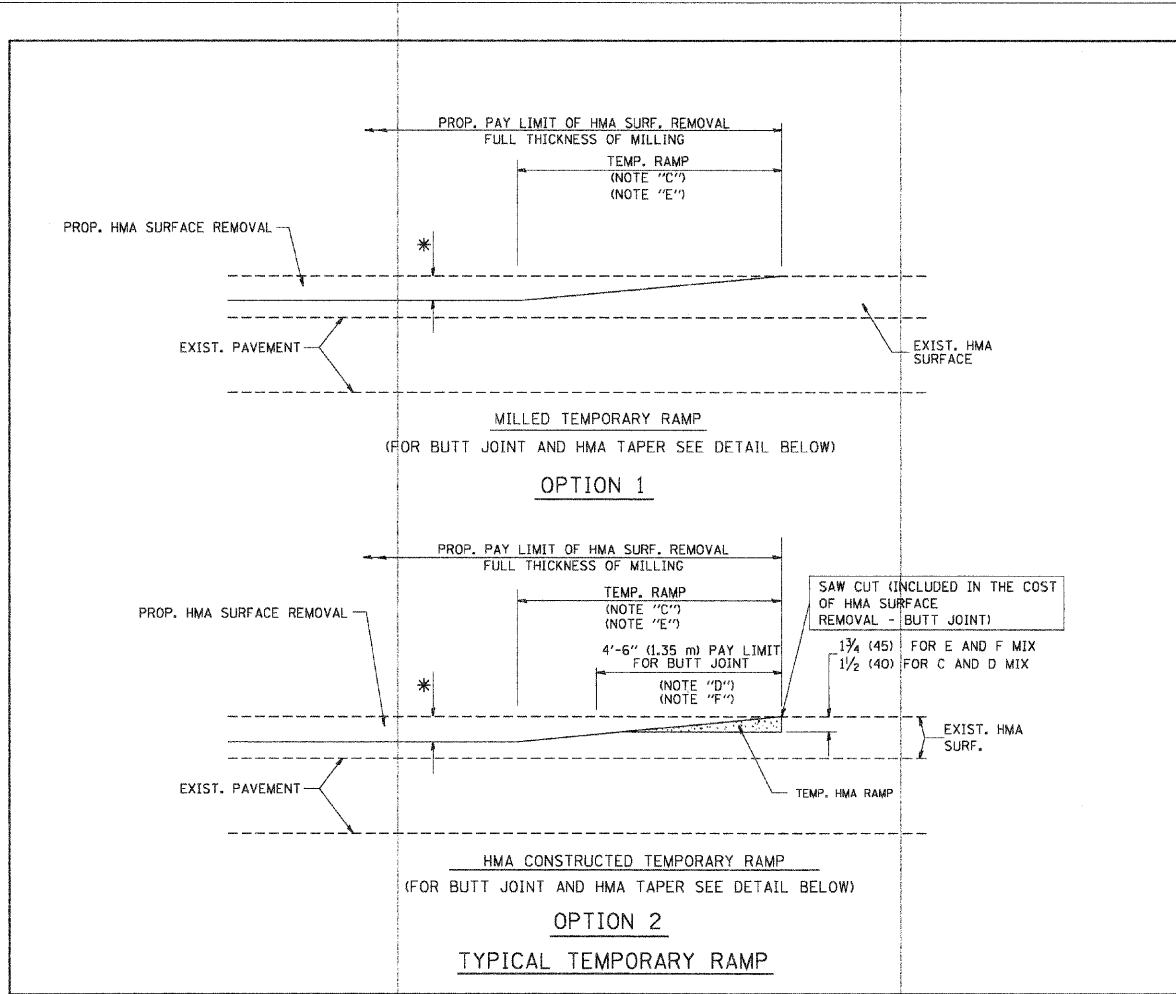
CITY OF ELGIN		
	NO.	DATE
	REVISIONS	

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

DATE: AUGUST 2009
 PROJECT NO: EG0901
 FILE: EG0901-CVR
 SHEET **15** OF **17**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1302	09-00177-00-B3	KANE	17	16
CONTRACT NO. 63301				



NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 B: MINOR SIDE ROADS.
 C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
 G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL - BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:
 THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\data\test\22x34\bd32.dgn	USER NAME = gajlanob1	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 5/8"=1'-0" / IN.	CHECKED -	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/4/2009	DATE - 06-13-90	REVISOR - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07

BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE			BD400-05 BD32			
SHEET NO. 1 OF 1 SHEETS		STA.	CONTRACT NO.		FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT	

Plotted: August 13, 2009 @ 7:40 AM By: Larry Nelson - Tab: 16, BD-32, 22x34

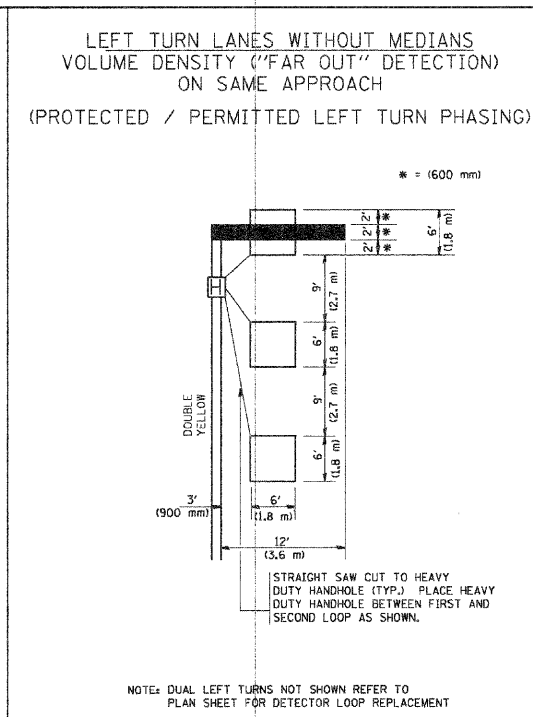
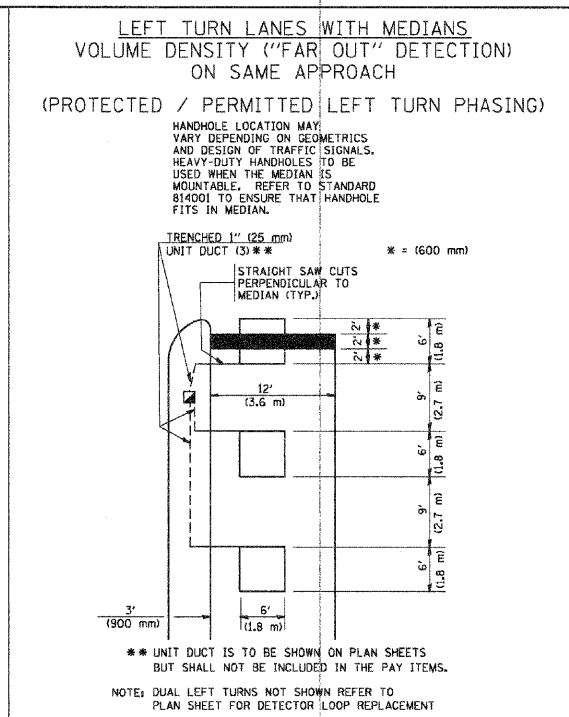
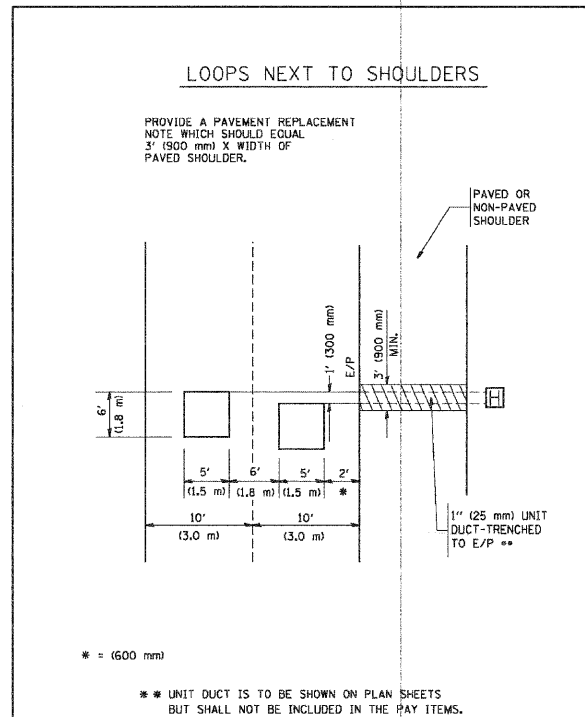
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NO.	DATE	REVISIONS	

**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**BUTT JOINT AND
HMA TAPER DETAILS**

DATE:	AUGUST 2009
PROJECT NO:	EG0901
FILE:	EG0901-CVR
SHEET	16 OF 17



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

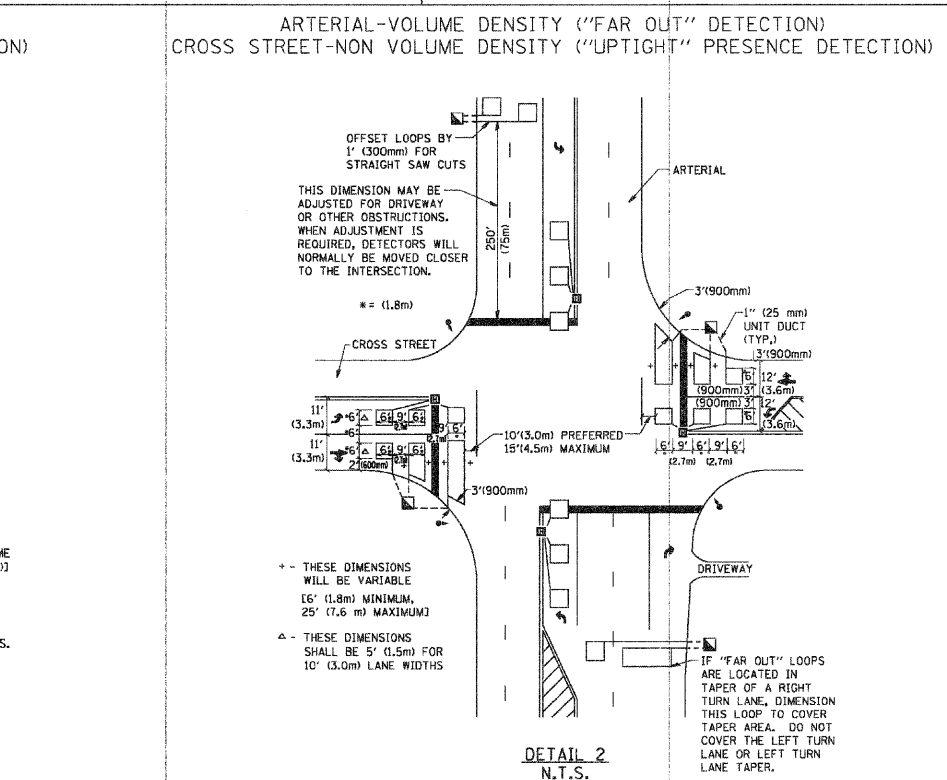
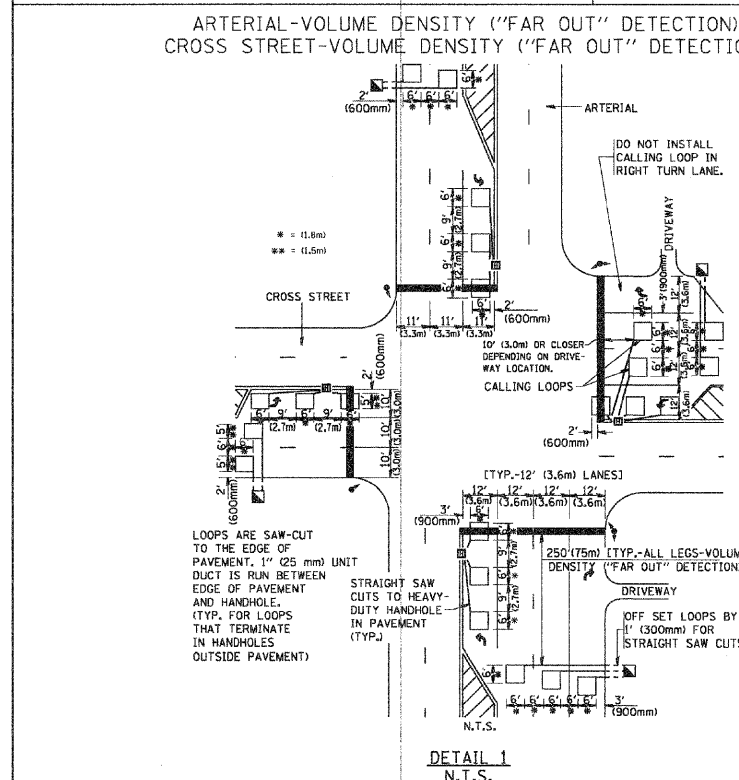
LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.



FILE NAME = W:\dist\ts\22x34\ts07.dgn	USER NAME = gajimmbz	DESIGNED -	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 68.8888" / IN.	CHECKED - R.K.F.	REVISD -	REVISD -			TS-07	CONTRACT NO.			
PLOT DATE = 1/4/2008	DATE	REVISD -	REVISD -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
						DATE: AUGUST 2009	PROJECT NO: EG0901	FILE: EG0901-CVR	SHEET 17 OF 17	

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**BIG TIMBER ROAD
LAPP IMPROVEMENTS**

**DISTRICT ONE - DETECTOR LOOP
INSTALLATION DETAILS FOR
ROADWAY RESURFACING**