

8/20/05 AM

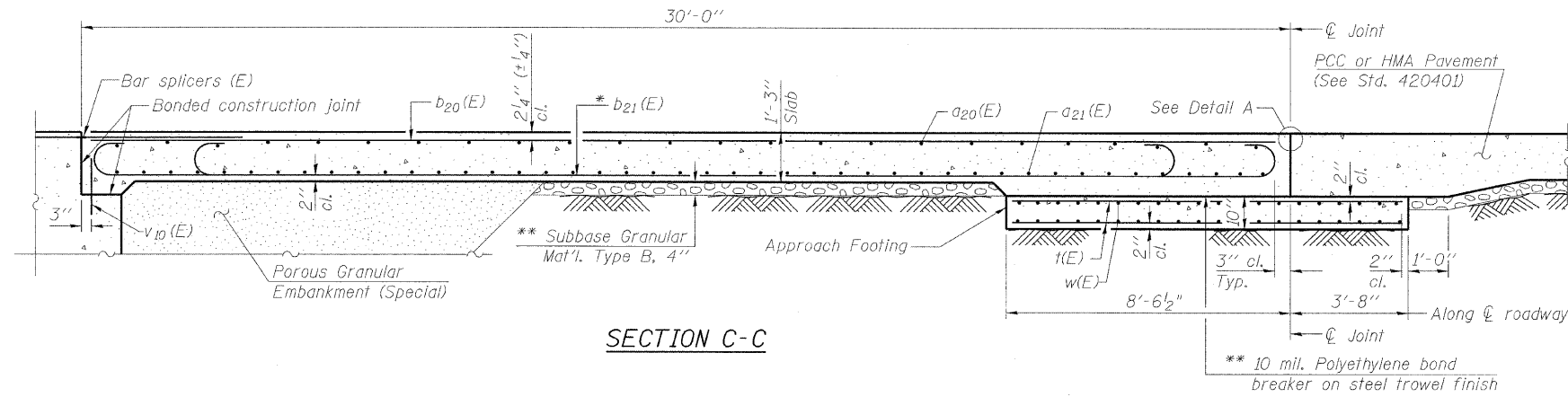
8/6/2009

H:\ProJec\15\2945\Structural\NCP\RRUS20 OVER ICR PRE\FINAL DWG\510100095-64D50-015-BRIDGE APPROACH SLAB DETAIL.SSHT.2.dgn

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	STATION	SHEET NO.	SHEET NO. 15 32 SHEETS
F.A.P. 301	21-VBR 21RS-2	STEPHENSON	112	63	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

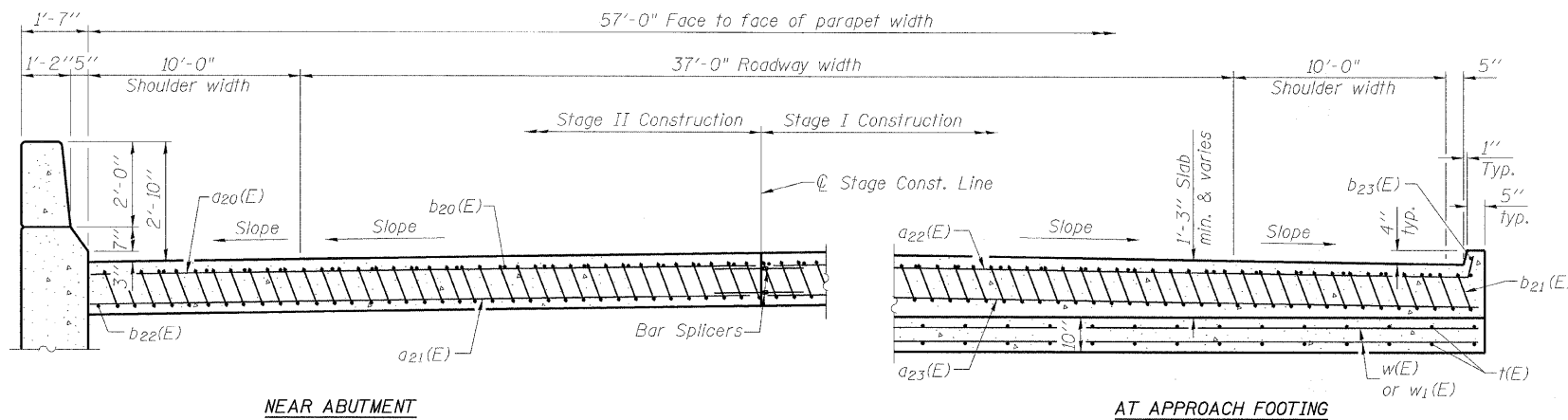
Contract #64D15



SECTION C-C

Note:  
See sheet 14 of 32 for Sections C-C & D-D  
 $a_{20}(E)$ ,  $a_{21}(E)$ , and  $w(E)$  bar spacings measured perpendicular to  $\varnothing$  Rdwy.

Notes:  
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.  
Approach footing concrete shall be paid for as Concrete Structures.  
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
For  $v_{10}(E)$  bar details, see sheet 24 of 32.  
The approach footing maximum applied service bearing pressure ( $Q_{max}$ ) = 2.0 ksf.  
For bar splicer details, see sheet 28 of 32.  
Cost of excavation for approach footing included with Concrete Structures.  
For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 32.



NEAR ABUTMENT

SECTION D-D

(See Plan for dimensions not shown)

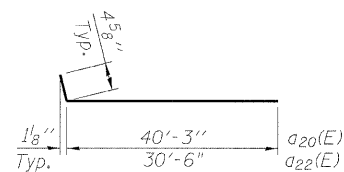
AT APPROACH FOOTING

\* Tilt #9  $b_{21}(E)$  bars as required to maintain clearance.

\*\* Cost included with Concrete Superstructure.

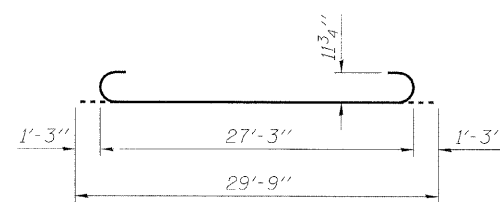
TWO APPROACHES  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
$a_{20}(E)$	50	#4	40'-7"	—
$a_{21}(E)$	92	#5	40'-5"	—
$a_{22}(E)$	50	#4	30'-10"	—
$a_{23}(E)$	92	#5	30'-8"	—
$b_{20}(E)$	116	#4	29'-8"	—
$b_{21}(E)$	276	#9	29'-9"	—
$b_{22}(E)$	4	#4	15'-6"	—
$b_{23}(E)$	4	#4	14'-2"	—
$t(E)$	240	#4	11'-10"	—
$w(E)$	80	#5	40'-5"	—
$w_1(E)$	80	#5	30'-8"	—
Concrete Superstructure		Cu. Yd.	161.0	
Concrete Structures		Cu. Yd.	44.0	
Reinforcement Bars, Epoxy Coated		Pound	47,450	



BAR  $a_{20}(E)$  &  $a_{22}(E)$

Bend at curb only



BAR  $b_{21}(E)$

DESIGNED	MMH
CHECKED	CEN
DRAWN	R.VEJAR
CHECKED	CEN

BRIDGE APPROACH SLAB DETAILS SHT. 2  
US 20 OVER ILLINOIS CENTRAL RAILROAD  
F.A.P. RTE. 301 - SEC. 21-VBR & 21RS-2  
STEPHENSON COUNTY  
STATION 569+56.19  
STRUCTURE NO. 089-0077

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PROJECT NUMBER: 2945