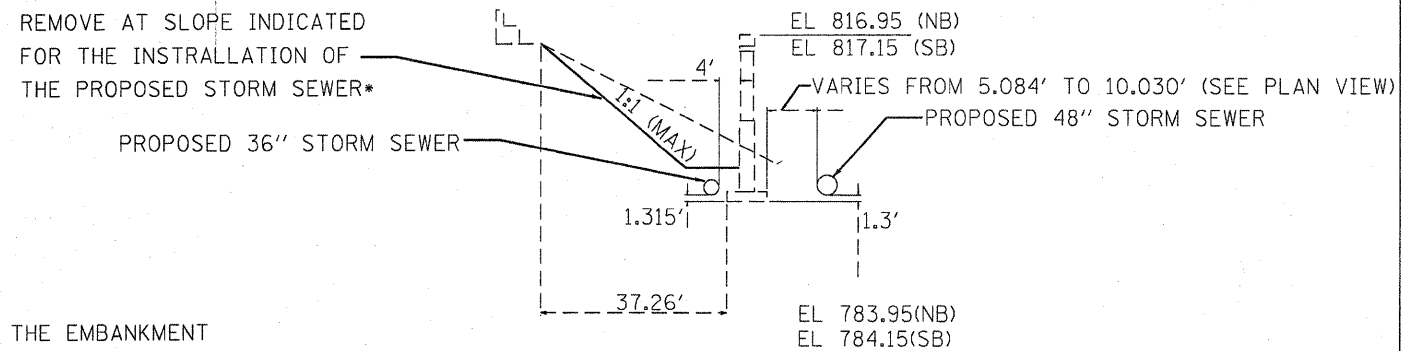


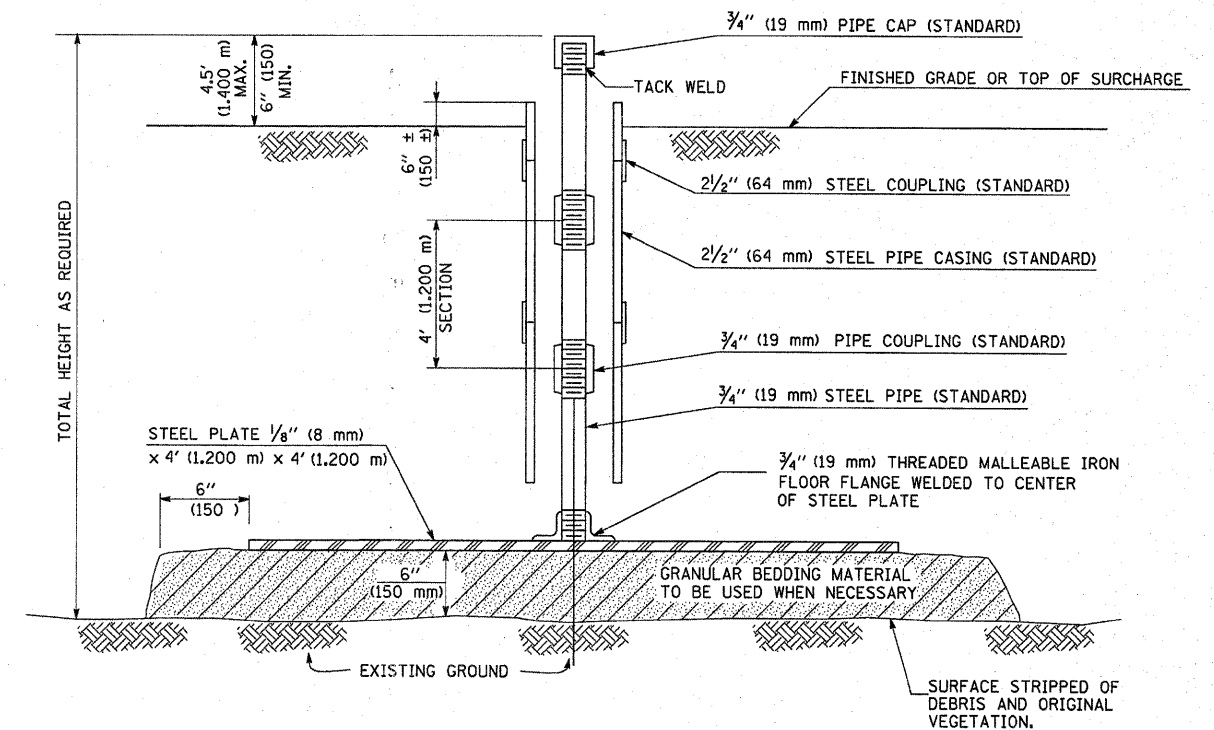
1. The installation of the pipe culverts shall be completed prior to construction for the embankment. Removal of the existing slopewall and embankment to facilitate pipe installation shall be no greater than a 1:1 slope to ensure stability of the existing abutments and piers
2. The Contractor shall take extra precaution not to over excavate and excavate below the existing footing elevation during excavation for pipe installation to ensure the stability of the existing piers.

NORTH PIER



DETAIL FOR THE INSTALLATION OF PROPOSED STORM SEWER

*REMOVAL OF THE EMBANKMENT NECESSARY TO CONSTRUCT AS SHOWN SHALL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.



SETTLEMENT PLATFORM DETAIL

1. SEE SECTION 204.06 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
2. TWO SETTLEMENT PLATFORMS SHALL BE CONSTRUCTED AT THE SITE OF THE ABANDONED RAILROAD STRUCTURES - ONE FOR STAGE I AND ONE FOR STAGE II CONSTRUCTION. THE COST IS INCLUDED IN THE COST OF FURNISHED EXCAVATION

FILE NAME =	USER NAME = braboypa	DESIGNED -	REVISD -
ci:\pw\work\PIWID01\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn		DRAWN -	REVISD -
PLOT SCALE = 5/8" = 1' / IN.		CHECKED -	REVISD -
PLOT DATE = Aug 26, 2009 - 03:06:08 PM		DATE -	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(27-1)VBR & VBR-110M	FORD	28	25
CONTRACT NO. 66862				
ILLINOIS FED. AID PROJECT				