

| F.A. RTE. | SECTION               | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-----------|-----------------------|----------|--------------------|-----------|
| BQ        | [(27-1)VBR & VBR-1JDM | FORD     | 28                 | 1         |
|           |                       | ILLINOIS | CONTRACT NO. 66862 |           |

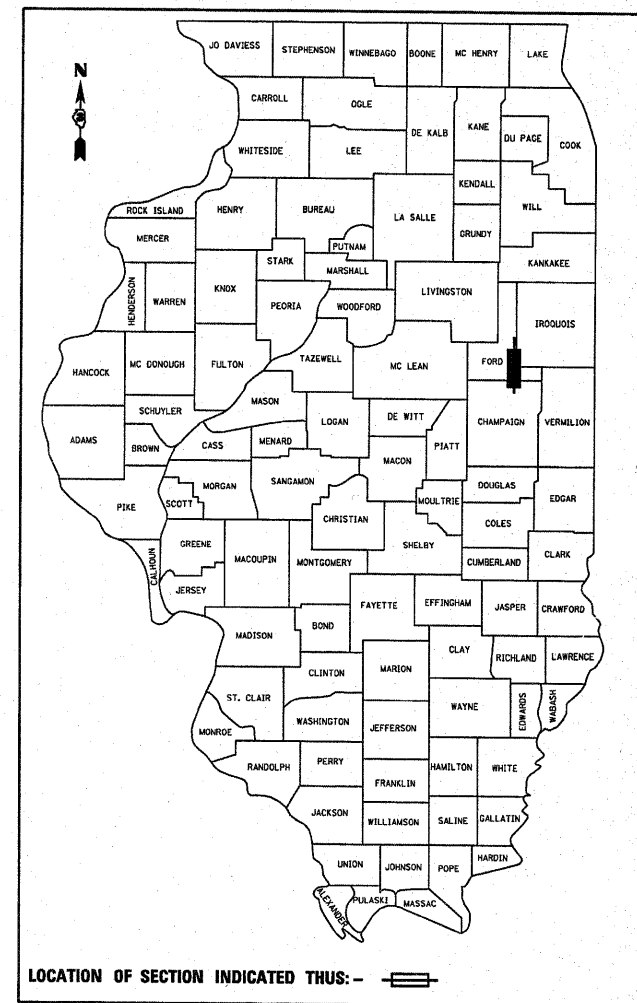
P-93-032-08  
D-93-037-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PROPOSED  
HIGHWAY PLANS**

FAI ROUTE 57 (I-57)  
SECTION [(27-1)VBR & VBR-1JDM  
PROJECT ACIM-057-6(185)262  
FORD COUNTY

C-93-061-09

TYPE of IMPROVEMENT  
REMOVAL OF STRUCTURES CARRYING  
I-57 OVER ABANDONED RAILROAD



FUNCTIONAL CLASSIFICATION-  
INTERSTATE  
2007 ADT = 14,000VPD  
%PV = 64.7% %SU = 4.6% %MU = 30.7%

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED Nov 26 2009  
George Dwyer  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 2, 2009  
Charles J. Ingersoll  
ENGINEER OF DESIGN AND ENVIRONMENT

October 2, 2009  
Christine M. Reed  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

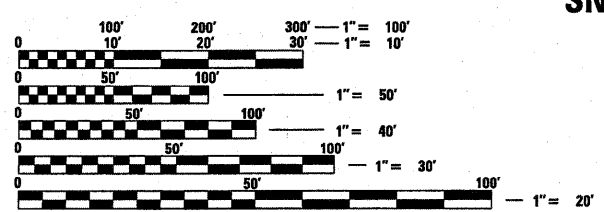
Index of Sheets

|       |                                    |
|-------|------------------------------------|
| 1     | COVERSHEET                         |
| 2     | GENERAL NOTES                      |
| 3     | SUMMARY                            |
| 4     | TYPICAL SECTIONS                   |
| 5-6   | SCHEDULE OF QUANTITIES             |
| 7     | PLAN AND PROFILE SHEET             |
| 8     | STAGE CONSTRUCTION TYPICAL SECTION |
| 9-10  | STAGE CONSTRUCTION PLAN SHEETS     |
| 11    | RIGHT-OF-WAY SHEET                 |
| 12-22 | EXISTING BRIDGE PLANS              |
| 23    | DRAINAGE STRUCTURE DETAIL SHEET    |
| 24-26 | DETAILS                            |
| 27-28 | CROSS SECTIONS                     |

STANDARDS

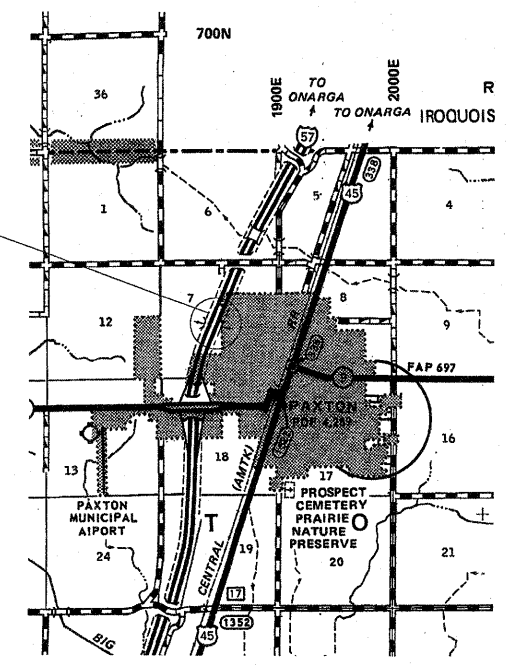
|           |   |
|-----------|---|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS            |
| 001001-02 | AREAS OF REINFORCEMENT BARS                             |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT                        |
| 202001-01 | EARTH MEDIAN DITCH CHECK                                |
| 280001-04 | TEMPORARY EROSION CONTROL SYSTEMS                       |
| 606401-01 | PAVED DITCH   |
| 630001-08 | STEEL PLATE BEAM GUARDRAIL                              |
| 630201-06 | PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL     |
| 635001-01 | DELINEATORS   |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT                 |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS                   |
| 642001-01 | SHOULDER RUMBLE STRIPS                                  |
| 665001-02 | WOVEN WIRE FENCE  |
| 666001-01 | RIGHT-OF-WAY MARKERS                                    |
| 701401-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY                        |
| 701402-07 | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER          |
| 701406-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY   |
| 701901-01 | TRAFFIC CONTROL DEVICES                                 |
| 704001-05 | TEMPORARY CONCRETE BARRIER                              |
| 780001-02 | TYPICAL PAVEMENT MARKINGS                               |
| 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |

LOCATION OF PROJECT  
SN 027-0050  
SN 027-0051



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



GROSS LENGTH = NET LENGTH = 163 FT. = 0.03 MILE

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

PROJECT ENGINEER: JOE KANNEL  
SQAUD LEADER: PAT BRABOY  
TOWNSHIP: PATTON  
CONTRACT NO. 66862

**GENERAL NOTES**

(Revised January 23, 2009)

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.

BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

SEEDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.

THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR INCHES IN AREAS TO BE SEEDED OR SODDED. THE VEGETATION SUSTAINING SOIL REQUIRED WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.

ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

|  |        |                            |
|--|--------|----------------------------|
| GRANULAR MATERIALS                     | 2.05   | TONS / CU YD               |
| POLYMERIZED BIT MATERIALS (PRIME COAT) | 0.10   | GAL / SQ YD                |
| FOR ADDITIONAL HMA LIFTS "FOG COAT"    | 0.08   | GAL / SQ YD                |
| AGGREGATE PRIME COAT                   | 0.002  | TONS / SQ YD               |
| HMA RESURFACING                        | 112    | LBS / SQ YD / IN           |
| SHORT TERM PAVEMENT MARKING            | 10     | FT / 100 FT OF APPLICATION |
| MIX FOR CRACKS, JTS & FLGWYS           | 0.0003 | TONS / SQ YD               |
| LEVEL BINDER (HAND METHOD)             | 0.0005 | TONS / SQ YD               |

ALL DAMAGE TO DEPARTMENT OWNED UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE. THIS SHALL INCLUDE ALL TEMPORARY REPAIRS REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. SPLICING OF ELECTRIC CABLE SHALL NOT BE ALLOWED. ELECTRIC CABLE SHALL BE REPLACED FROM POLE TO POLE OR CONTROLLER.

THE WORK REQUIRED TO CONNECT ANY SEWER TO AN EXISTING DRAINAGE STRUCTURE OR PIPE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE SEWER ITEMS.

THE CONTRACTOR SHALL CONTACT JULIE AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH UTILITIES ARE IN THE AREA.

| MIXTURES TABLE        |                   |                                  |                             |                          |                              |
|-----------------------|-------------------|----------------------------------|-----------------------------|--------------------------|------------------------------|
|                       | HMA SURFACE POLY  | HMA BASE COURSE TOP LIFT, 2 1/4" | HMA BASE COURSE BOTTOM LIFT | HMA SHOULDER (12" THICK) | HMA SHOULDER ( 9" THICK) *** |
| PG GRADE              | SBS PG-70-22      | SBS PG-70-22                     | PG-64-22                    | PG-64-22                 | PG-64-22                     |
| MAX % RAP ALLOWABLE** | 10                | 10                               | 15                          | 15                       | 15                           |
| DESIGN AIR VOIDS      | 4.0% @ N90        | 4.0% @ N90                       | 4.0% @ N70                  | 3.0% @ N50               | 4.0% @ N70                   |
| MIXTURE COMPOSITION   | IL 12.5 OR IL 9.5 | IL 19.0                          | IL 19.0                     | IL 19.0                  | IL 19.0                      |
| FRICITION AGGREGATE   | MIXTURE D         |                                  |                             |                          |                              |
| DENSITY TEST METHOD   | CORES/ NUCLEAR    | CORES/ NUCLEAR                   | CORES/ NUCLEAR              | CORES/ NUCLEAR           | CORES/ NUCLEAR*              |

\* MATERIAL SHALL BE COMPACTED TO 93.0-97.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY, EXCEPT THAT WHEN PLACED AS FIRST LIFT ON AN UNIMPROVED SUBGRADE THE MINIMUM PERCENT COMPACTION SHALL BE 92.0 PERCENT. THE MAXIMUM THEORETICAL DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE AS SPECIFIED IN THE QC/QA SPECIFICATION.

\*\*WHEN MORE THAN 20% RAP IS USED, A SOFTER ASPHALT BINDER (PG 58-22) MAY BE REQUIRED AS DETERMINED BY THE ENGINEER

**COMMITMENTS:**

THE RESIDENT ENGINEER IS TO CONTACT ROGER GUSTAFSON PRIOR TO CONSTRUCTION TO DETERMINED HOW HIS FIELD TILE IS TO BE CONNECTED TO THE PROPOSED DROP BOX ON THE WEST SIDE OF I-57. HIS PHONE NUMBER IS 815-228-3790

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DISTRICT THREE

PREPARED BY: [Signature]  
DISTRICT STUDIES & PLANS ENGINEER

DATE: 8-27-09

EXAMINED BY: [Signature]  
DISTRICT CONSTRUCTION ENGINEER

[Signature]  
DISTRICT MATERIALS ENGINEER

[Signature]  
DISTRICT OPERATIONS ENGINEER

|   |                       |            |                           |   |                      |             |         |        |              |           |
|---|-----------------------|------------|---------------------------|---|----------------------|-------------|---------|--------|--------------|-----------|
| FILE NAME =   | USER NAME = btaboyppa | DESIGNED - | REVISD -                  | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>GENERAL NOTES</b> | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\FWIDOT\BRABOYPC\dms64344\d32345-shr-cover.dgn            | DRAWN -               | REVISD -   | 57                        |   |                      | I-27-DVBR   | Force   | 28     | 2            |           |
| PLOT SCALE = 50.0000' / IN.                                       | CHECKED -             | REVISD -   | CONTRACT NO. 66862        |   |                      |             |         |        |              |           |
| PLOT DATE = Aug 27, 2009 - 09:38:46 AM                            | DATE -                | REVISD -   | ILLINOIS FED. AID PROJECT |   |                      |             |         |        |              |           |
| SCALE: _____ SHEET NO. ___ OF ___ SHEETS STA. _____ TO STA. _____ |                       |            |                           |   |                      |             |         |        |              |           |

SUMMARY OF QUANTITIES

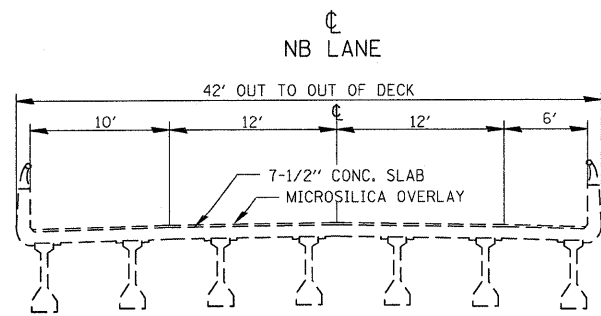
| CODE NO             | ITEM   | UNIT   | TOTAL QTY | ROADWAY 1000 90% FED. 10% STATE |
|---------------------|--|--------|-----------|---------------------------------|
| 20100110            | TREE REMOVAL (6 TO 15 UNITS DIAMETER)                    | UNIT   | 8         | 8                               |
| 20400800            | FURNISHED EXCAVATION                                     | CU YD  | 28984     | 28984                           |
| 21400100            | GRADING AND SHAPING DITCHES                              | FOOT   | 600       | 600                             |
| * 25000300          | SEEDING, CLASS 3   | ACRE   | 0.81      | 0.81                            |
| * 25000400          | NITROGEN FERTILIZER NUTRIENT                             | POUND  | 72.9      | 72.9                            |
| * 25000500          | PHOSPHORUS FERTILIZER NUTRIENT                           | POUND  | 72.9      | 72.9                            |
| * 25000600          | POTASSIUM FERTILIZER NUTRIENT                            | POUND  | 72.9      | 72.9                            |
| * 25101005          | HEAVY DUTY EXCELSIOR BLANKET                             | SQ YD  | 3214      | 3214                            |
| * 25100115          | MULCH, METHOD 2  | ACRE   | 0.15      | 0.15                            |
| 28000250            | TEMPORARY EROSION CONTROL SEEDING                        | POUND  | 243       | 243                             |
| 28000300            | TEMPORARY DITCH CHECKS                                   | EACH   | 2         | 2                               |
| 28000400            | PERIMETER EROSION BARRIER                                | FOOT   | 681       | 681                             |
| 28000500            | INLET AND PIPE PROTECTION                                | EACH   | 2         | 2                               |
| 28100107            | STONE RIPRAP, CLASS A4                                   | SQ YD  | 5         | 5                               |
| 28200200            | FILTER FABRIC  | SQ YD  | 5         | 5                               |
| 31101200            | SUB-BASE GRANULAR MATERIAL, TYPE B 4"                    | SQ YD  | 12        | 12                              |
| 35501320            | HOT-MIX ASPHALT BASE COURSE, 9"                          | SQ YD  | 1904      | 1904                            |
| 40600115            | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT)            | GALLON | 224       | 224                             |
| 40600982            | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT             | SQ YD  | 800       | 800                             |
| 40603545            | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 | TON    | 190       | 190                             |
| 44000300            | CURB REMOVAL   | FOOT   | 240       | 240                             |
| <del>44000186</del> | HOT-MIX ASPHALT SURFACE REMOVAL, 9"                      | SQ YD  | 1626      | 1626                            |
| 48203045            | HOT-MIX ASPHALT SHOULDERS, 12"                           | SQ YD  | 696       | 696                             |
| 48101200            | AGGREGATE SHOULDERS, TYPE B                              | TON    | 60        | 60                              |
| 50100100            | REMOVAL OF EXISTING STRUCTURES                           | EACH   | 2         | 2                               |
| 50300225            | CONCRETE STRUCTURES                                      | CU YD  | 2.85      | 2.85                            |
| <del>35501351</del> | HOT-MIX ASPHALT BASE COURSE, 17"                         | SQ YD  | 880       | 880                             |

SUMMARY OF QUANTITIES

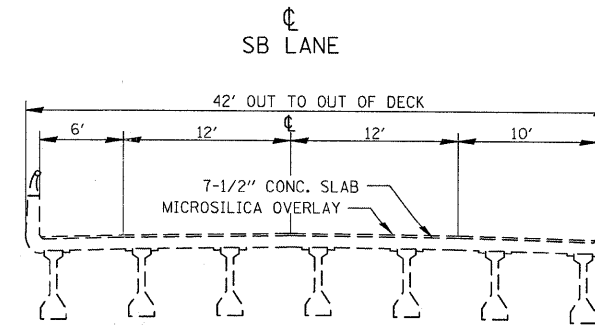
| CODE NO             | ITEM   | UNIT   | TOTAL QTY | ROADWAY 1000 90% FED. 10% STATE |
|---------------------|--|--------|-----------|---------------------------------|
| 50800105            | REINFORCEMENT BARS   | POUND  | 1548      | 1548                            |
| 54213681            | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"            | EACH   | 2         | 2                               |
| 54213693            | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 48"            | EACH   | 1         | 1                               |
| 550A1920            | STORM SEWERS, CLASS A, TYPE 7 36"                              | FOOT   | 293       | 293                             |
| 550A1950            | STORM SEWERS, CLASS A, TYPE 7 48"                              | FOOT   | 315       | 315                             |
| <del>X0326694</del> | PLUG EXISTING STORM SEWERS                                     | CU YD  | 96.3      | 96.3                            |
| 55101600            | STORM SEWER REMOVAL 36"  | FOOT   | 240       | 240                             |
| 60615910            | PAVED DITCH, TYPE A-22   | FOOT   | 233       | 233                             |
| * 63000001          | STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS              | FOOT   | 432       | 432                             |
| 63200305            | STEEL PLATE BEAM GUARD RAIL REMOVAL                            | FOOT   | 930       | 930                             |
| 63500105            | DELINEATORS  | EACH   | 2         | 2                               |
| 64200105            | SHOULDER RUMBLE STRIP  | FOOT   | 2620      | 2620                            |
| 66500105            | WOVEN WIRE FENCE, 4'   | FOOT   | 145       | 145                             |
| <del>66900200</del> | NON-SPECIAL WASTE DISPOSAL                                     | CU YD  | 270       | 270                             |
| <del>66502300</del> | WOVEN WIRE FENCE REMOVAL                                       | FOOT   | 387       | 387                             |
| <del>66900450</del> | SPECIAL WASTE PLANS AND REPORTS                                | L SUM  | 1         | 1                               |
| 67000400            | ENGINEER'S FIELD OFFICE, TYPE A                                | CAL MO | 9         | 9                               |
| <del>66900530</del> | SOIL DISPOSAL ANALYSIS   | EACH   | 1         | 1                               |
| 67100100            | MOBILIZATION   | L SUM  | 1         | 1                               |
| 70101005            | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL)      | EACH   | 1         | 1                               |
| 70300100            | SHORT-TERM PAVEMENT MARKING                                    | FOOT   | 314       | 314                             |
| 70300220            | TEMPORARY PAVEMENT MARKING - LINE 4"                           | FOOT   | 3140      | 3140                            |
| 70300240            | TEMPORARY PAVEMENT MARKING - LINE 6"                           | FOOT   | 394       | 394                             |
| 70301000            | WORK ZONE PAVEMENT MARKING REMOVAL                             | SQ FT  | 2971      | 2971                            |
| 70400100            | TEMPORARY CONCRETE BARRIER                                     | FOOT   | 860       | 860                             |
| 70400200            | RELOCATE TEMPORARY CONCRETE BARRIER                            | FOOT   | 764       | 764                             |
| * 78000200          | THERMOPLASTIC PAVEMENT MARKING - LINE 4"                       | FOOT   | 3140      | 3140                            |
| * 78000400          | THERMOPLASTIC PAVEMENT MARKING - LINE 6"                       | FOOT   | 394       | 394                             |
| * 78100100          | RAISED REFLECTIVE PAVEMENT MARKER                              | EACH   | 40        | 40                              |
| * 78200405          | GUARDRAIL MARKERS  | EACH   | 6         | 6                               |
| 78300105            | PAVEMENT MARKING REMOVAL                                       | FOOT   | 4774      | 4774                            |
| 78300200            | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL                      | EACH   | 40        | 40                              |
| X0325969            | PORTABLE VEHICLE MOUNTED, CHANGEABLE MESSAGE BOARD             | CAL DA | 60        | 60                              |
| X7030100            | WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III                  | FOOT   | 11240     | 11240                           |
| X7010820            | TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 (SPECIAL)      | L SUM  | 1         | 1                               |
| Z0001050            | AGGREGATE SUBGRADE 12"   | SQ YD  | 953       | 953                             |
| Z0030250            | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3 | EACH   | 2         | 2                               |
| Z0030350            | IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3  | EACH   | 2         | 2                               |

\*SPECIALTY ITEMS

|  |                      |            |          |   |                              |                           |                       |        |              |           |  |
|--|----------------------|------------|----------|---|------------------------------|---------------------------|-----------------------|--------|--------------|-----------|--|
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| PLLOT SCALE = 50.0000' / IN.                                       | CHECKED -            | REVISD -   | REVISD - |   |                              | 57                        | [(27-1)VBR & VBR-1]DM | FORD   | 28           | 3         |  |
| PLLOT DATE = Aug 27, 2009 - 09:54:32 AM                            | DATE -               | REVISD -   | REVISD - |   |                              | CONTRACT NO. 66862        |                       |        |              |           |  |
|  |                      |            |          |   |                              | ILLINOIS FED. AID PROJECT |                       |        |              |           |  |

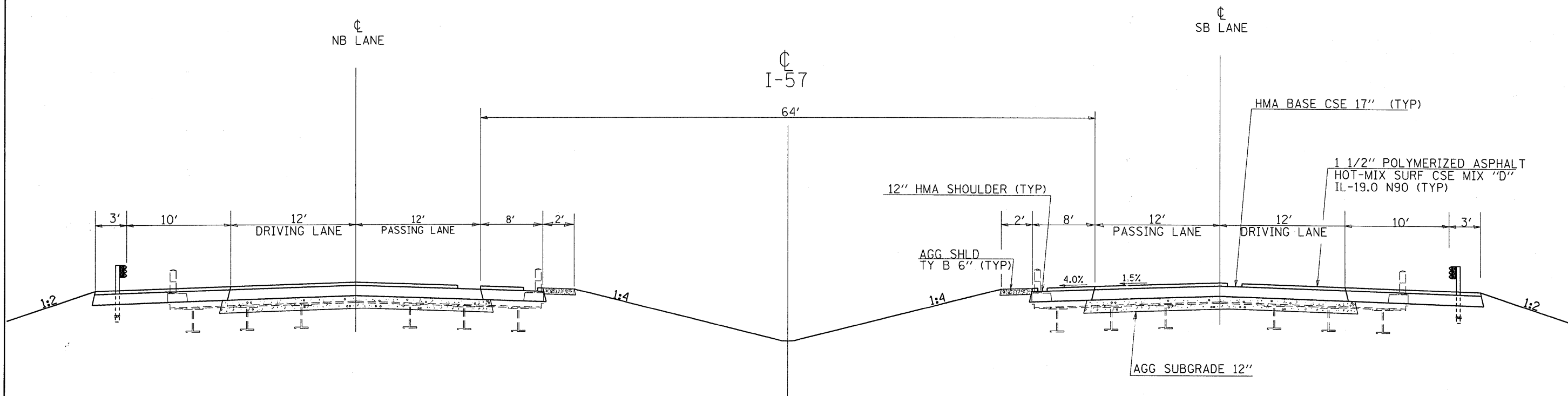


I-57



**EXISTING TYPICAL SECTION**

MEDIAN STA 289+79



**PROPOSED TYPICAL SECTION**

MEDIAN STA 289+79

FOR SN 027-0050 AND 027-0051 TBR

|   |                      |            |           |
|---|----------------------|------------|-----------|
| FILE NAME =   | USER NAME = braboygo | DESIGNED - | REVISED - |
| ca\pw_work\pwsdot\braboygo\dms64344\038662-SHT-PLNPRF.dgn |                      | DRAWN -    | REVISED - |
| PLOT SCALE = 50.0000' / IN.                               | CHECKED -            | REVISED -  | REVISED - |
| PLOT DATE = Aug 27, 2009 - 09:49:26 AM                    | DATE -               | REVISED -  | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

|                    |                       |        |                           |           |
|--------------------|-----------------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION               | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | (027-1)VBR & VBR-1JDM | FORD   | 28                        | 4         |
| CONTRACT NO. 66862 |                       |        | ILLINOIS FED. AID PROJECT |           |

| MAINLINE SCHEDULE                  |        |                             |                  |              |                  |                       |                |                             |              |
|------------------------------------|--------|-----------------------------|------------------|--------------|------------------|-----------------------|----------------|-----------------------------|--------------|
| LOCATION                           | LENGTH | 1 1/2" POLY ASPH SURF MIX D | HMA BASE CSE 17" | HMA SHLD 12" | AGG SUBGRADE 12" | POLY. BIT MAT'L PR CT | AGG SHLD TY B* | HMA SURF REMOVAL BUTT JOINT | CURB REMOVAL |
|                                    | FT     | TON                         | SQ YD            | SQ YD        | SQ YD            | GAL                   | TON            | SQ YD                       | FOOT         |
| NBL STA 288+80 TO STA 290+291+33   | 253    | 95                          |                  |              |                  | 112                   |                | 400                         |              |
| NBL STA 289+25.77 TO STA 290+88.79 | 163    |                             | 440              | 348          | 477              |                       | 30             |                             | 120          |
| SUB TOTAL                          |        | 95                          | 440              | 348          | 477              |                       | 30             |                             | 120          |
| SBL STA 289+88 TO STA 289+14       | 253    | 95                          |                  |              |                  | 112                   |                | 400                         |              |
| SBL STA 290+33.05 TO STA 288+69.74 | 163    |                             | 440              | 348          | 477              |                       | 30             |                             | 12           |
| SUB TOTAL                          |        | 95                          | 440              | 348          | 477              |                       | 30             |                             | 120          |
| <b>TOTAL</b>                       |        | <b>190</b>                  | <b>880</b>       | <b>696</b>   | <b>953</b>       | <b>224</b>            | <b>60</b>      | <b>800</b>                  | <b>240</b>   |

| FURNISHED EXCAVATION           |          |                 |
|--------------------------------|----------|-----------------|
| STA TO STA                     | NB<br>SB | FURNISHED EX    |
|                                |          | CU YD           |
| <b>FORD CO</b>                 |          |                 |
| STA 289+25.77 TO STA 290+88.79 | NB       | 14750.86        |
| STA 290+33.05 TO STA 288+69.74 | SB       | 14233.81        |
| <b>TOTALS</b>                  |          | <b>28984.67</b> |

\*6" THICKNESS THROUGH BRIDGE REMOVAL AREA. PLACE WEDGE NEXT TO NEW 8' MEDIAN SHOULDER AREA FOR THE REST

| WOVEN WIRE FENCE                            |          |                          |                     |
|---|----------|--------------------------|---------------------|
| LOCATION                                    | NB<br>SB | WOVEN WIRE FENCE REMOVAL | WOVEN WIRE FENCE 4' |
| STA TO STA                                  |          |                          | FT                  |
| STA 288+71(150' LT) TO STA 289+45 (145' LT) | NB       |                          | 75                  |
| 290+01(150' RT) TO 291+70 (145' RT)         | SB       |                          | 70                  |
| NW QUADRANT                                 | SB       | 109                      |                     |
| NE QUADRANT                                 | SB       | 80                       |                     |
| SW QUADRANT                                 | NB       | 87                       |                     |
| SE QUADRANT                                 | NB       | 111                      |                     |
| <b>TOTAL</b>                                |          | <b>387</b>               | <b>145</b>          |

| DRAINAGE SCHEDULE                  |                       |                  |                         |              |                   |                   |               |               |                          |             |
|------------------------------------|-----------------------|------------------|-------------------------|--------------|-------------------|-------------------|---------------|---------------|--------------------------|-------------|
| LOC                                | SIDE (RT, MEDIAN, LT) | GR & SHAP DITCH. | PLUG EXIST. STORM SEWER | RIPRAP CL A4 | PRC FLARED ES 36" | PRC FLARED ES 48" | SS CLA T7 36" | SS CLA T7 48" | STORM SEWER REMOVAL 36"* | PAVED DITCH |
| STA                                |                       | FOOT             | CU YD                   | SQ YD        | EACH              | EACH              | FOOT          | FOOT          | FOOT                     | FOOT        |
| 280+70 TO 286 + 70 140' LT         | NB                    | 600              |                         |              |                   |                   |               |               |                          |             |
| 290 + 06.03 TO 290 + 27.46 138' RT | SB                    |                  |                         |              |                   |                   |               |               |                          | 21          |
| 288 + 66.18 TO 288 + 78.29 146' LT | NB                    |                  |                         |              |                   |                   |               |               |                          | 12          |
| 286 + 69.41 TO 288 + 78.23 138' LT | NB                    |                  |                         |              |                   |                   |               |               |                          | 200         |
| 290 + 00                           | MED                   |                  |                         |              |                   |                   |               |               |                          |             |
| 289 + 42.05                        | MED                   |                  |                         |              | 2                 |                   | 293           |               | 240                      |             |
| 289 + 42.89                        | MED                   |                  | 96.3                    |              |                   |                   |               |               |                          |             |
| 289 + 74.46                        | MED                   |                  |                         |              |                   | 1                 |               | 315           |                          |             |
| 290 + 62.38 133' RT                | SB                    |                  |                         | 5            |                   |                   |               |               |                          |             |
| <b>TOTAL</b>                       |                       | <b>600</b>       | <b>96.3</b>             | <b>5</b>     | <b>2</b>          | <b>1</b>          | <b>293</b>    | <b>315</b>    | <b>240</b>               | <b>233</b>  |

\*INCLUDES THE REMOVAL OF END SECTIONS FOR THIS CULVERT AND THE CULVERT TO BE PLUGGED AT STA 290+00

| EROSION CONTROL SCHEDULE         |                 |                           |                           |                   |
|----------------------------------|-----------------|---------------------------|---------------------------|-------------------|
| LOCATION                         | NB<br>MED<br>SB | PERIMETER EROSION BARRIER | INLET AND PIPE PROTECTION | TEMP DITCH CHECKS |
| STA TO STA                       |                 | FT                        | EACH                      | EACH              |
| STA 286+68 TO STA 291+16,138' LT | NB              | 448                       |                           |                   |
| STA 289+00                       | MED             |                           |                           | 1                 |
| STA 291+00                       | MED             |                           |                           | 1                 |
| STA 289+23 TO STA 291+56         | SB              | 233                       |                           |                   |
| 289+80, 130' RT                  | SB              |                           | 1                         |                   |
| 290+20 130' RT                   | SB              |                           | 1                         |                   |
| <b>TOTAL</b>                     |                 | <b>681</b>                | <b>2</b>                  | <b>2</b>          |

| GUARDRAIL              |          |            |            |          |
|------------------------|----------|------------|------------|----------|
|                        | NB<br>SB | SPBGR TY A | GR REMOVAL | GR MKR   |
| STA TO STA             |          | FT         | EA         | EA       |
| <b>FORD CO</b>         |          |            |            |          |
| 288+45.65 TO 290+75.94 | NB       | 216        | 60*        | 3        |
| 285+02 TO 294+58.6     | MED      |            | 810        |          |
| 288+84.01 TO 291+11.50 | SB       | 216        | 60*        | 3        |
| <b>SUBTOTAL</b>        |          | <b>432</b> | <b>930</b> | <b>6</b> |

\* FOR THE REMOVAL OF TBT, T 6 IN THE DRIVING LANES AT THE BRIDGES

| SEEDING SCHEDULE       |                 |                 |                              |                               |                                 |                                  |                              |                              |
|------------------------|-----------------|-----------------|------------------------------|-------------------------------|---------------------------------|----------------------------------|------------------------------|------------------------------|
| LOCATION               | NB<br>MED<br>SB | SEEDING CLASS 3 | NITROGEN FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT | PHOSPHOROUS FERTILIZER NUTRIENT | TREE REMOVAL 6-15 UNITS DIAMETER | HEAVY DUTY EXCELSIOR BLANKET | TEMP EROSION CONTROL SEEDING |
| STA TO STA             |                 | ACRE            | POUND                        | POUND                         | POUND                           | UNITS                            | SQ YD                        | POUND                        |
| 288+75.65 TO 290+45.94 | NB              | 0.32            | 28.8                         | 28.8                          | 28.8                            |                                  | 1556                         | 96                           |
| 290+40, 134' RT        | SB              |                 |                              |                               |                                 | 8                                |                              |                              |
| 289+00 TO 290+56       | MED             | 0.15            | 13.5                         | 13.5                          | 13.5                            |                                  | 0                            | 45                           |
| 289+14.01 TO 290+26.97 | SB              | 0.34            | 30.6                         | 30.6                          | 30.6                            |                                  | 1658                         | 102                          |
| <b>TOTAL</b>           |                 | <b>0.81</b>     | <b>72.9</b>                  | <b>72.9</b>                   | <b>72.9</b>                     | <b>8</b>                         | <b>3214</b>                  | <b>243</b>                   |

USE MULCH METHOD 2 IN MEDIANS AND HEAVY DUTY EXCELSIOR BLANKET ON THE REST

| LOCATION            |        | LENGTH | PAVEMENT MARKINGS |        |       |       |      |      |        |        |       |       |
|---------------------|--------|--------|-------------------|--------|-------|-------|------|------|--------|--------|-------|-------|
|                     |        |        | THPL              | THPL   | THPL  | TEMP  | TEMP | TEMP | RAISED | RAISED | SHORT | WORK  |
| PVT                 | PVT    |        | PVT               | PVT    | PVT   | PVT   | REF  | REF  | TERM   | ZONE   |       |       |
| MK LN               | MK LN  |        | MK LN             | MK LN  | MK LN | MK LN | PVT  | PVT  | PVT    | PVT    |       |       |
| 4"                  | 4"     |        | 6"                | 4"     | 4"    | 6"    | MKR  | MKR  | MKR    | MKR    |       |       |
| WHITE               | YELLOW | WHITE  | WHITE             | YELLOW | WHITE | REM   |      |      | REM    |        |       |       |
| FORD COUNTY         |        | FT     | FT                | FT     | FT    | FT    | FT   | FT   | EACH   | EACH   | FT    | SQ FT |
| 286 + 00 TO 293+85  | NB     | 785    | 785               | 785    | 197   | 785   | 785  | 197  | 20     | 20     | 157   | 52    |
| 285+75.15 TO 293+60 | SB     | 785    | 785               | 785    | 197   | 785   | 785  | 197  | 20     | 20     | 157   | 52    |
| <b>SUBTOTAL</b>     |        |        | 1570              | 1570   | 394   | 1570  | 1570 | 394  | 40     | 40     | 314   | 104   |
| <b>GRAND TOTAL</b>  |        |        | 3140              | 3140   | 394   | 3140  | 3140 | 394  | 40     | 40     | 314   | 104   |

| STAGE CONSTRUCTION ITEMS                       |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
|--|------------------|--------|---------------------------------|---------------------------------|---------------------------------|--|--------------------------|----------------------------|--------------------------------|----------------------|-----------------------------------|----------------------------------|-----------------------------------|---------------|
| LOCATION                                       | STA              | LENGTH | TEMPORARY CONCRETE BARRIER FOOT | RELOCATE TEMP CONC BARRIER FOOT | IMPACT ATTENUATORS NRD,TL3 EACH | RELOCATE IMPACT ATTENUATORS NRD,TL3 EACH | HMA(1) BASE CSE 9" SQ YD | HMA SURF REMOVAL, 9" SQ YD | SHOULDER RUMBLE STRIP (4) FOOT | DELINEATORS (4) EACH | WORK ZONE PVT MK REMOVAL(2) SQ FT | PAVEMENT MARKING REMOVAL(3) FOOT | WET TEMP PVT MK TAPE, TYPE III-4" |               |
|  |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  | WHITE FOOT                        | YELLOW FOOT   |
| <b>STAGE I</b>                                 |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| NBL  | 287+30 TO 307+55 | 2025   |                                 |                                 |                                 |  |                          |                            |                                |                      | 668                               | 506.0                            |                                   | 2025.0        |
| SBL  | 287+30 TO 292+55 | 525    |                                 |                                 |                                 |  |                          |                            |                                |                      | 173                               | 525.0                            | 525.0                             |               |
| NBL  | 272+05 TO 292+30 | 2025   |                                 |                                 |                                 |  |                          |                            |                                |                      | 668                               | 506.0                            |                                   | 2025.0        |
| SBL  | 287+05 TO 292+30 | 525    |                                 |                                 |                                 |  |                          |                            |                                |                      | 173                               | 525.0                            | 525.0                             |               |
| <b>STAGE II</b>                                |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| NBL  | 286+00 TO 308+85 | 2285   |                                 |                                 |                                 |  |                          |                            |                                |                      | 754                               | 571.0                            | 2285.0                            |               |
| SBL  | 286+00 TO 293+85 | 785    |                                 |                                 |                                 |  |                          |                            |                                |                      | 259                               | 785.0                            |                                   | 785.0         |
| NBL  | 270+75 TO 293+60 | 2285   |                                 |                                 |                                 |  |                          |                            |                                |                      | 754                               | 571.0                            | 2285.0                            |               |
| SBL  | 285+75 TO 293+60 | 785    |                                 |                                 |                                 |  |                          |                            |                                |                      | 259                               | 785.0                            |                                   | 785.0         |
| <b>STAGE I</b>                                 |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| NBL  | 288+85 TO 292+67 | 382    | 382.0                           |                                 | 1                               |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| SBL  | 286+93 TO 290+75 | 382    | 382.0                           |                                 | 1                               |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| <b>STAGE II</b>                                |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| NBL  | 288+85 TO 293+15 | 430    | 48.0                            | 382                             |                                 | 1  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| SBL  | 286+45 TO 290+75 | 430    | 48.0                            | 382                             |                                 | 1  |                          |                            |                                |                      |                                   |                                  |                                   |               |
| <b>NBL ON 10' SHLD (FOR STAGE I TRAFFIC)**</b> |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
|  | 287+30 TO 292+55 | 525    |                                 |                                 |                                 | 400                                      | 400                      | 525                        | 1                              |                      |                                   |                                  |                                   |               |
| <b>SBL ON 10' SHLD (FOR STAGE I TRAFFIC)**</b> |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
|  | 287+05 TO 292+30 | 525    |                                 |                                 |                                 | 400                                      | 400                      | 525                        | 1                              |                      |                                   |                                  |                                   |               |
| <b>NBL ON 6' SHLD (FOR STAGE II TRAFFIC)**</b> |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
|  | 286+00 TO 293+85 | 785    |                                 |                                 |                                 | 552                                      | 413                      | 785                        |                                |                      |                                   |                                  |                                   |               |
| <b>SBL ON 6' SHLD (FOR STAGE II TRAFFIC)**</b> |                  |        |                                 |                                 |                                 |  |                          |                            |                                |                      |                                   |                                  |                                   |               |
|  | 285+75 TO 293+60 | 785    |                                 |                                 |                                 | 552                                      | 413                      | 785                        |                                |                      |                                   |                                  |                                   |               |
| <b>TOTALS</b>                                  |                  |        | <b>860.0</b>                    | <b>764.0</b>                    | <b>2.0</b>                      | <b>2.0</b>                               | <b>1904.0</b>            | <b>1626.0</b>              | <b>2620.0</b>                  | <b>2.0</b>           | <b>2867.0</b>                     | <b>4774.0</b>                    | <b>5620.0</b>                     | <b>5620.0</b> |

\*DEDUCT 165' FOR BRIDGE  
 \*\*DEDUCT 165' FOR NEW SHOULDER PLACED DURING STAGE I CONSTRUCTION. EXISTING MEDIAN SHOULDER IS 6'. PROPOSED IS 8'.  
 (1) TO BE PERFORMED PRIOR TO STAGE I CONSTRUCTION-EXISTING SHOULDER IS 9"  
 (2) WK ZONE PVT MK REMOVAL IS TO REMOVE THE WET TEMP PVT MK TAPE  
 (3) PAVEMENT MARKING REMOVAL IS TO REMOVE EXISTING PAVEMENT MARKINGS PRIOR TO STAGE I AND/OR STAGE II CONSTRUCTION  
 (4) SHOULDER RUMBLE STRIP AND DELINATORS SHOWN HERE FOR CLARITY. THEY ARE TO BE PLACED AFTER FINAL HMA SURFACE COURSE AT END OF PROJECT

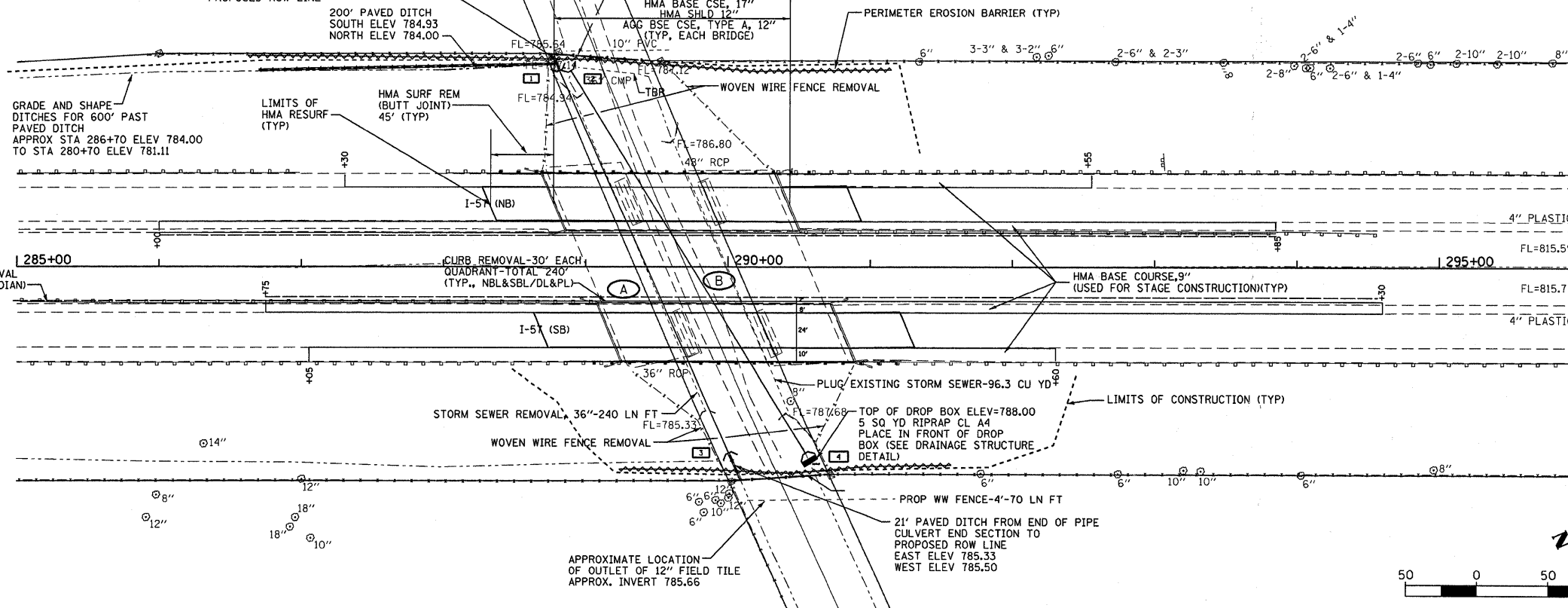
BENCHMARK:  
CHISELED " " SET IN NE CORNER OF BRIDGE  
IN WINGWALL AT THE NB LANE  
EL. = 822.71

12' PAVED DITCH FROM END OF PIPE  
CULVERT END SECTION TO  
PROPOSED ROW LINE

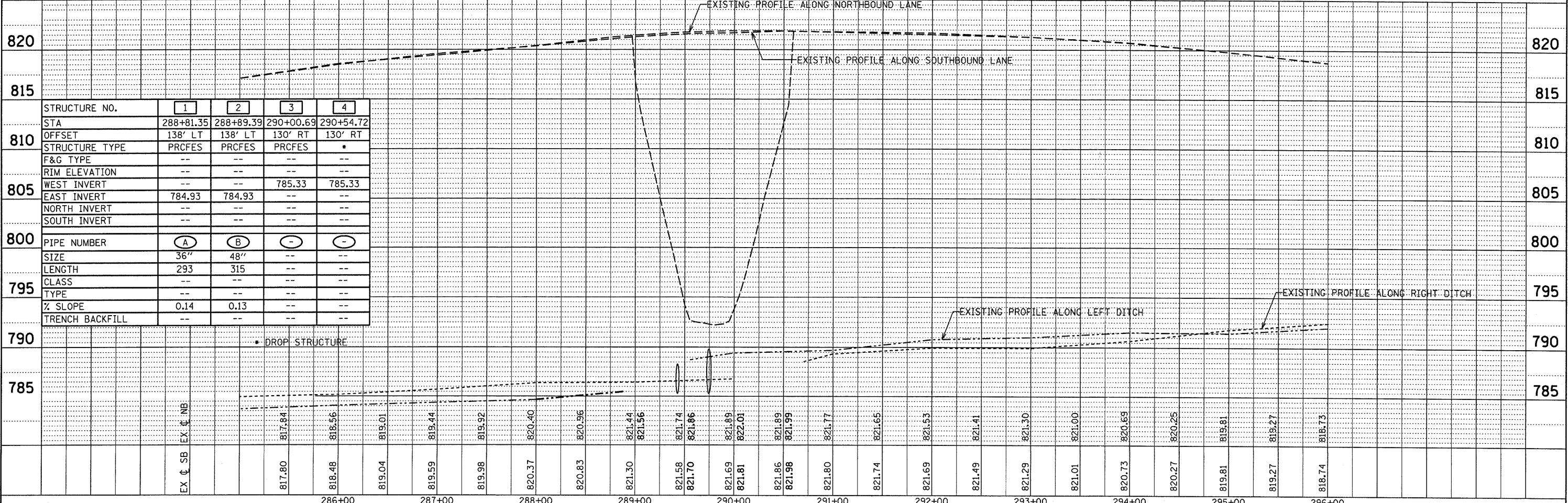
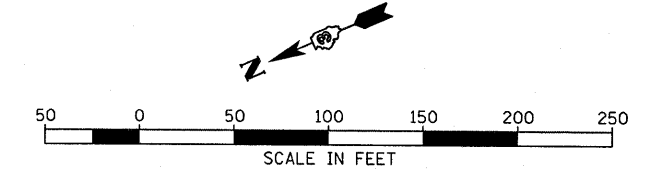
PROP WW FENCE-4'-75 LN FT

BENCHMARK:  
BRASS PLUG AT @ I-57 AT STA. 291+00.36  
EL. = 819.82

BENCHMARK:  
CHISELED " " SET IN SW CORNER OF BRIDGE  
IN WINGWALL AT THE SB LANE  
EL. = 823.52



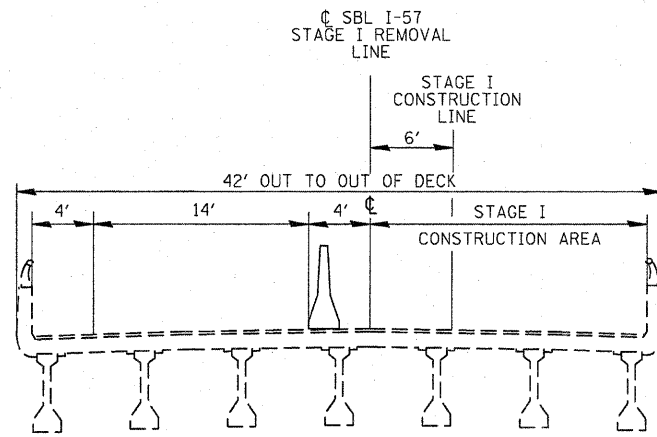
- NOTES:
- PIERS SHALL BE REMOVED TO A DEPTH OF 10' BELOW THE BOTTOM OF THE GIRDER
  - ABUTMENTS SHALL BE COMPLETELY REMOVED



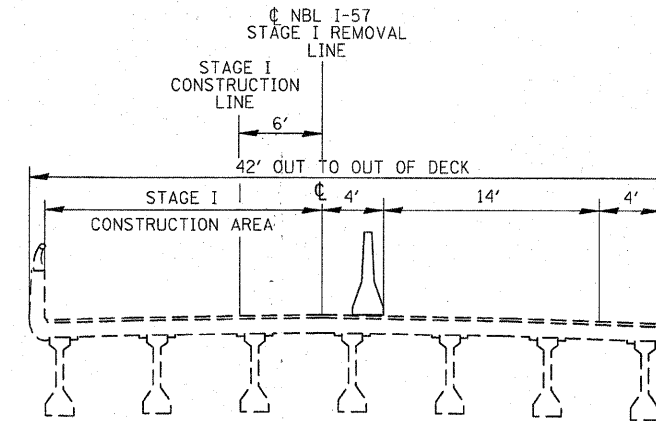
| STRUCTURE NO.   | 1         | 2         | 3         | 4         |
|-----------------|-----------|-----------|-----------|-----------|
| STA             | 288+81.35 | 288+89.39 | 290+00.69 | 290+54.72 |
| OFFSET          | 138' LT   | 138' LT   | 130' RT   | 130' RT   |
| STRUCTURE TYPE  | PRCFES    | PRCFES    | PRCFES    | *         |
| F&G TYPE        | --        | --        | --        | --        |
| RIM ELEVATION   | --        | --        | --        | --        |
| WEST INVERT     | --        | --        | 785.33    | 785.33    |
| EAST INVERT     | 784.93    | 784.93    | --        | --        |
| NORTH INVERT    | --        | --        | --        | --        |
| SOUTH INVERT    | --        | --        | --        | --        |
| PIPE NUMBER     | A         | B         | -         | -         |
| SIZE            | 36"       | 48"       | --        | --        |
| LENGTH          | 293       | 315       | --        | --        |
| CLASS           | --        | --        | --        | --        |
| TYPE            | --        | --        | --        | --        |
| % SLOPE         | 0.14      | 0.13      | --        | --        |
| TRENCH BACKFILL | --        | --        | --        | --        |

DATE: 08/08  
BY: CHAMIN  
SURVEYED: [ ]  
PLOTTED: [ ]  
CHECKED: [ ]  
NOTE BOOK NO.: [ ]  
DATE: 08/08  
BY: CHAMIN  
SURVEYED: [ ]  
PLOTTED: [ ]  
CHECKED: [ ]  
NOTE BOOK NO.: [ ]

DATE: 08/08  
BY: CHAMIN  
SURVEYED: [ ]  
PLOTTED: [ ]  
CHECKED: [ ]  
NOTE BOOK NO.: [ ]  
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SURVEYED: [ ]  
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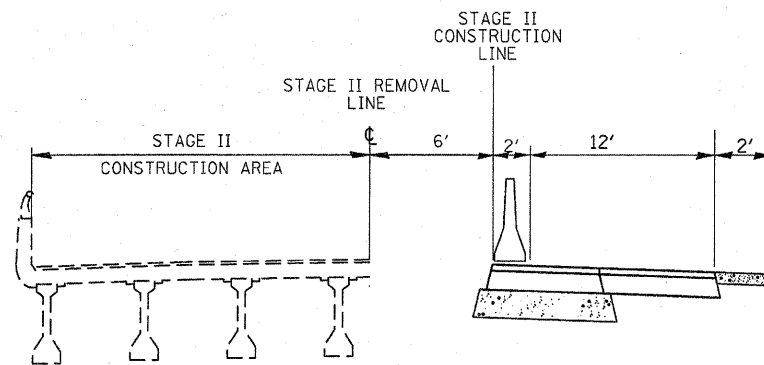


NBL SN 027-0051

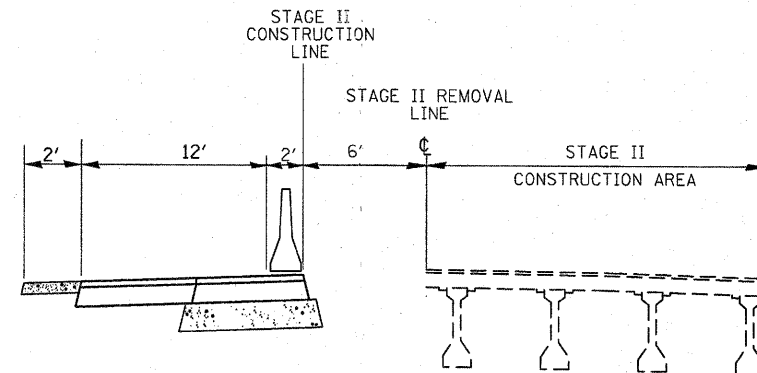


SBL SN 027-0050

STAGE I



NBL SN 027-0051



SBL SN 027-0050

STAGE II

STAGE CONSTRUCTION DIAGRAM

|  |                      |            |           |
|--|----------------------|------------|-----------|
| FILE NAME =  | USER NAME = braboypp | DESIGNED - | REVISED - |
| c:\pwwork\pwidot\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn |                      | DRAWN -    | REVISED - |
| PLOT SCALE = 5/8" = 1' IN.                               |                      | CHECKED -  | REVISED - |
| PLOT DATE = Aug 26, 2009 - 03:14:01 PM                   |                      | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

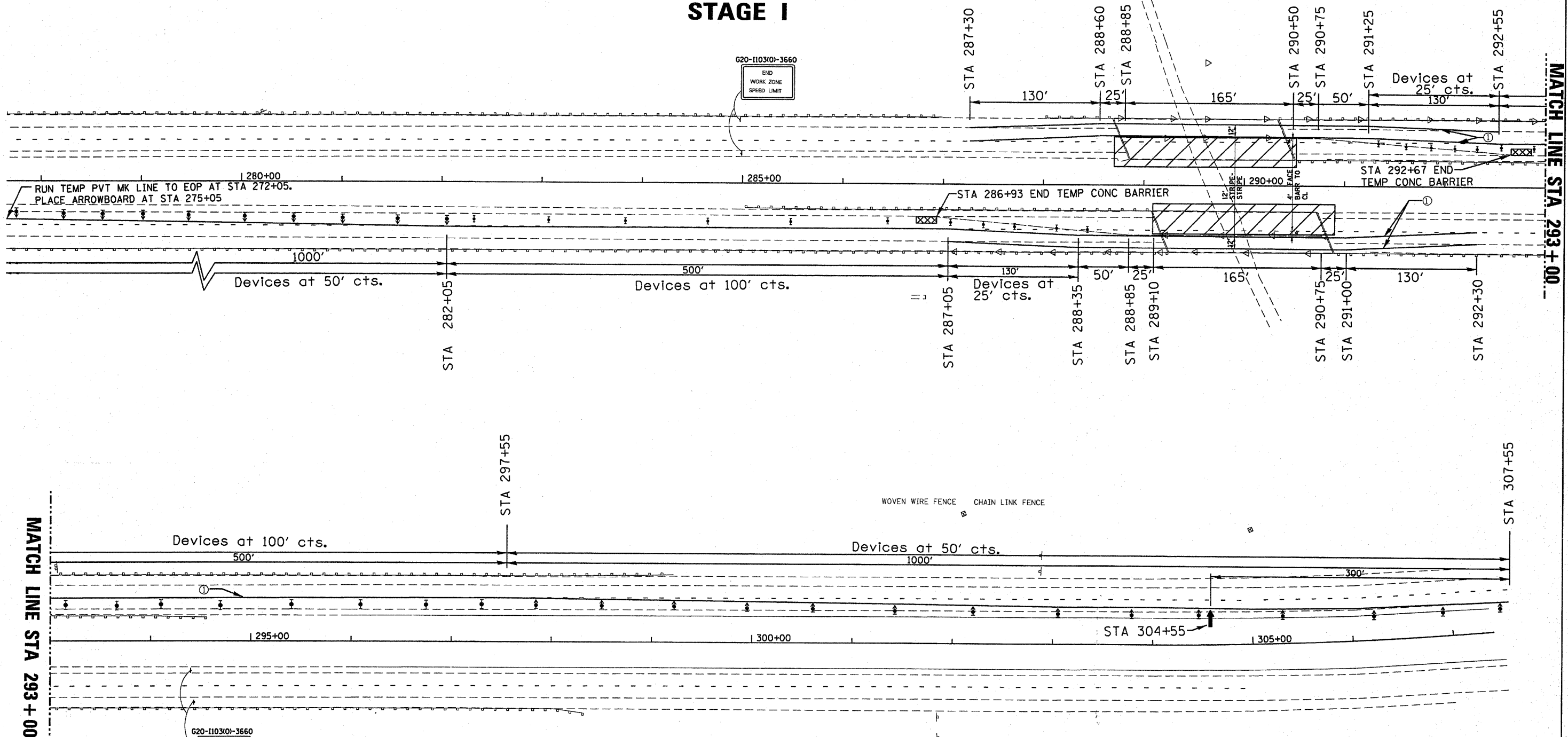
STAGE CONSTRUCTION DIAGRAM

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                     |        |                           |           |
|--------------------|---------------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION             | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | 027-1YBR & YBR-1JDM | FORD   | 28                        | 8         |
| CONTRACT NO. 66862 |                     |        | ILLINOIS FED. AID PROJECT |           |

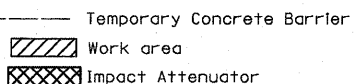


# STAGE I



### SYMBOLS

- ↑ Arrow board
- ⊥ Sign
- ▲ Direction indicator barricade with steady burn monodirectional light
- ◁ Monodirectional Barrier Wall/Guardrail Marker
- ⬇ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- ① Wet temporary pavement marking tape - TY III shall be placed throughout the taper and along-side the work area. The edge lines shall be white and the line near the barrier shall be yellow.



### STAGE CONSTRUCTION NOTES:

#### PRESTAGE I CONSTRUCTION

1. INSTALL STORM SEWER
2. PLACE EMBANKMENT TO WITHIN 10' OF THE BOTTOM FACE OF THE I-BEAM OF THE BRIDGE
3. USING TC&P STD 701401, PLACE HMA BASE COURSE, 9" (FOR STAGE CONSTRUCTION)

#### STAGE I CONSTRUCTION:

1. REMOVE MEDIAN PORTION OF THE EXISTING BRIDGES
2. PLACE THE REMAINING EMBANKMENT IN MEDIAN ARE AND BUILD PAVEMENT AS SHOWN IN PLANS

SEE STANDARD 701402 FOR DETAILS NOT SHOWN

|   |                       |            |           |
|---|-----------------------|------------|-----------|
| FILE NAME =   | USER NAME = braboyppa | DESIGNED - | REVISED - |
| at\pwwork\pwwid\T\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn |                       | DRAWN -    | REVISED - |
|   |                       | CHECKED -  | REVISED - |
|   |                       | DATE -     | REVISED - |
|   |                       |            |           |

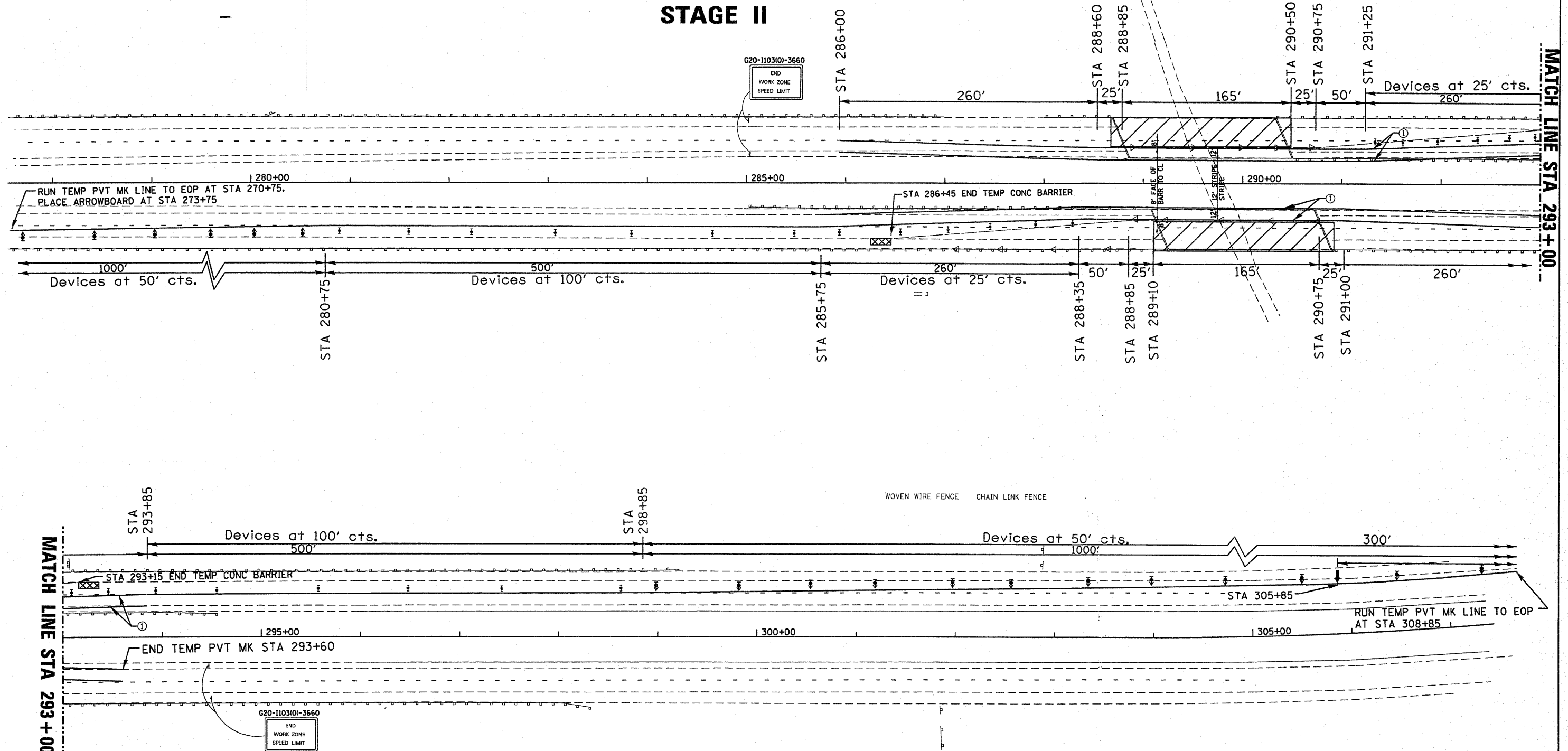
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

## STAGE I CONSTRUCTION

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                       |        |                           |           |
|--------------------|-----------------------|--------|---------------------------|-----------|
| F.A. RTE. -        | SECTION               | COUNTY | TOTAL SHEETS              | SHEET NO. |
| -57-               | [(27-1)VBR & VBR-1]DM | FORD   | 28                        | 9         |
| CONTRACT NO. 66862 |                       |        | ILLINOIS FED. AID PROJECT |           |

# STAGE II



MATCH LINE STA 293 + 00

MATCH LINE STA 293 + 00

### SYMBOLS

- ↑ Arrow board
- ⊥ Sign
- ▲ Direction Indicator barricade with steady burn monodirectional light
- ◁ Monodirectional Barrier Wall/Guardrail Marker
- ⊥ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- ⓪ Wet temporary pavement marking tape - TY III shall be placed throughout the taper and along-side the work area. The edge lines shall be white and the line near the barrier shall be yellow.
- Temporary Concrete Barrier
- ▨ Work area
- ▩ Impact Attenuator

SEE STANDARD 701402 FOR DETAILS NOT SHOWN

STAGE II CONSTRUCTION:  
 USING TC&P STD 701402-  
 1. REMOVE REMAINDER OF THE EXISTING BRIDGES  
 2. PLACE THE REMAINING EMBANKMENT AND BUILD PAVEMENT AS SHOWN IN PLANS  
 POST-STAGE II CONSTRUCTION  
 USING TC&P STD 701401, PLACE SURFACE COURSE AND STRIPE

|  |  |            |           |
|--|--|------------|-----------|
| FILE NAME =<br>c:\pwwork\pwwid\BRABOYPC\dms64344\036682-SHT-PLNPRF.dgn | USER NAME = braboypc                   | DESIGNED - | REVISED - |
|  | PLOT SCALE = 5/8"=2698' / IN.          | DRAWN -    | REVISED - |
|  | PLOT DATE = Aug 26, 2009 - 03:06:18 PM | CHECKED -  | REVISED - |
|  |  | DATE -     | REVISED - |

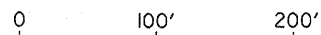
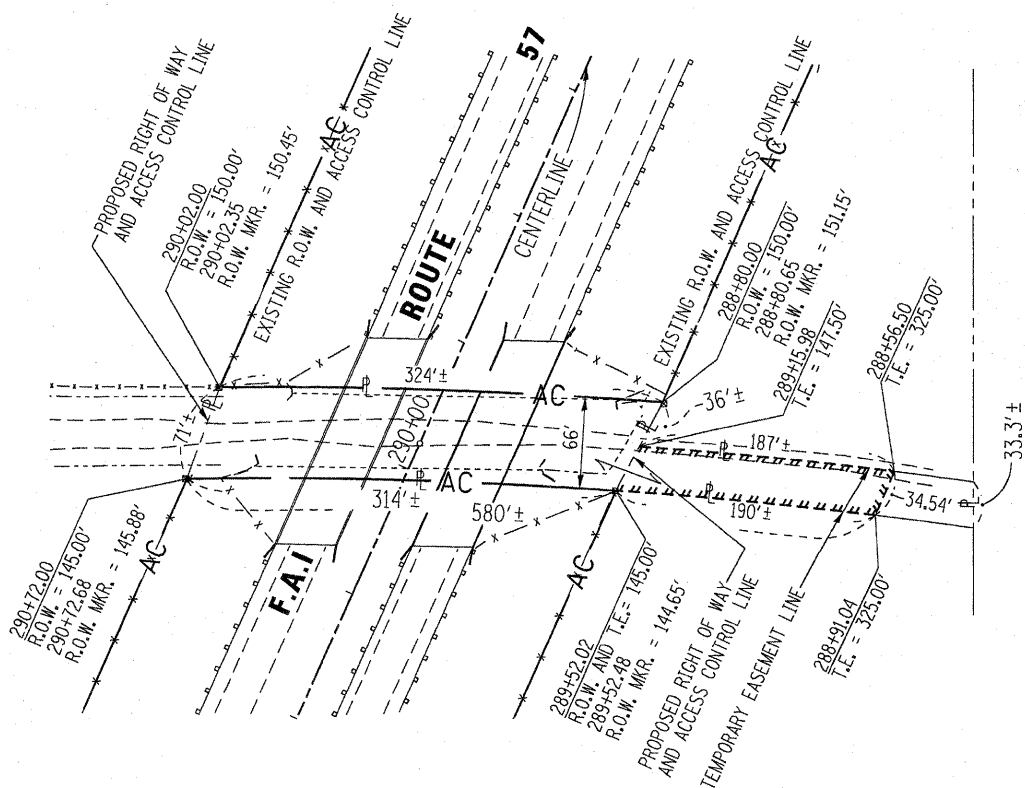
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE II CONSTRUCTION**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                               |             |                 |                           |
|--------------------|-------------------------------|-------------|-----------------|---------------------------|
| F.A. RTE. 57       | SECTION I(27-1)VBR & VBR-1IDM | COUNTY FORD | TOTAL SHEETS 28 | SHEET NO. 10              |
| CONTRACT NO. 66862 |                               |             |                 | ILLINOIS FED. AID PROJECT |

A PART OF THE NW 1/4 OF SEC. 7, T. 23 N., R 10 E., 3RD. P.M.



SCALE: 1" = 100'

**PARCEL 3VM0001**

**ROGER GUSTAFSON**

TOTAL HOLDING = 0.679 AC.±  
 TOTAL RIGHT OF WAY REQUIRED = 0.483 AC.±  
 REMAINDER = 0.196 AC.±  
 TEMPORARY EASEMENT AREA = 0.143 AC.±  
 PURPOSE: DITCH RE-CONSTRUCTION

**PREMISE PLAT**

ROUTE: FAI 57 (INTERSTATE 57)  
 SECTION: ((27-1)VBR & VBR-1)DM  
 COUNTY: FORD  
 JOB NO.: R-93-007-09

|   |  |            |          |
|---|--|------------|----------|
| FILE NAME =   | USER NAME = braboypa                   | DESIGNED - | REVISD - |
| ca:\pwwork\pwidot\BRABOYPC\dms6434\1\36862-SHT-PLNPRF.dgn |  | DRAWN -    | REVISD - |
|   | PLOT SCALE = 50.2690' / IN.            | CHECKED -  | REVISD - |
|   | PLOT DATE = Aug 26, 2009 - 03:06:35 PM | DATE -     | REVISD - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

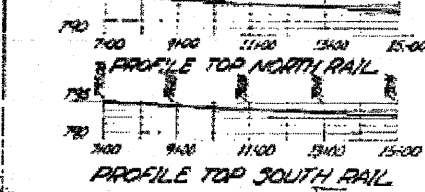
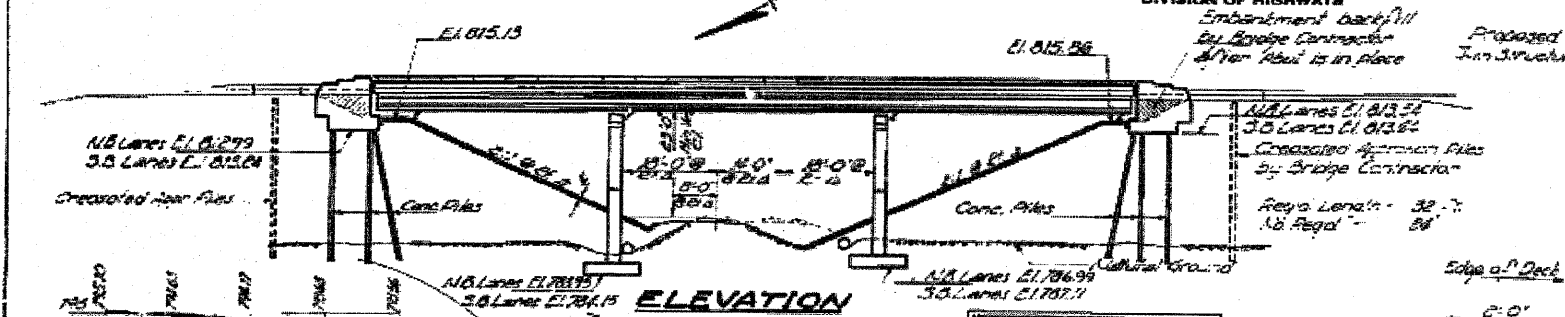
**RIGHT-OF-WAY SHEET**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                       |        |                           |           |
|--------------------|-----------------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION               | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | ((27-1)VBR & VBR-1)DM | FORD   | 28                        | 11        |
| CONTRACT NO. 66862 |                       |        | ILLINOIS FED. AID PROJECT |           |

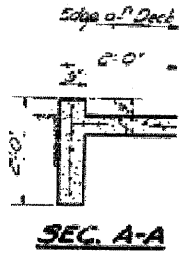
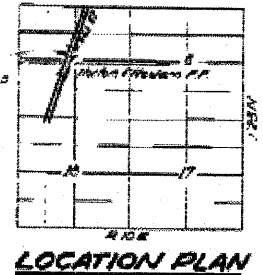
Sta. 289+00 on South side of Norfolk & Western RR  
 Elev. 789.00

STATE OF ILLINOIS  
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
 DIVISION OF HIGHWAYS



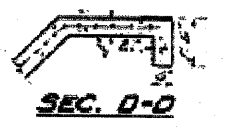
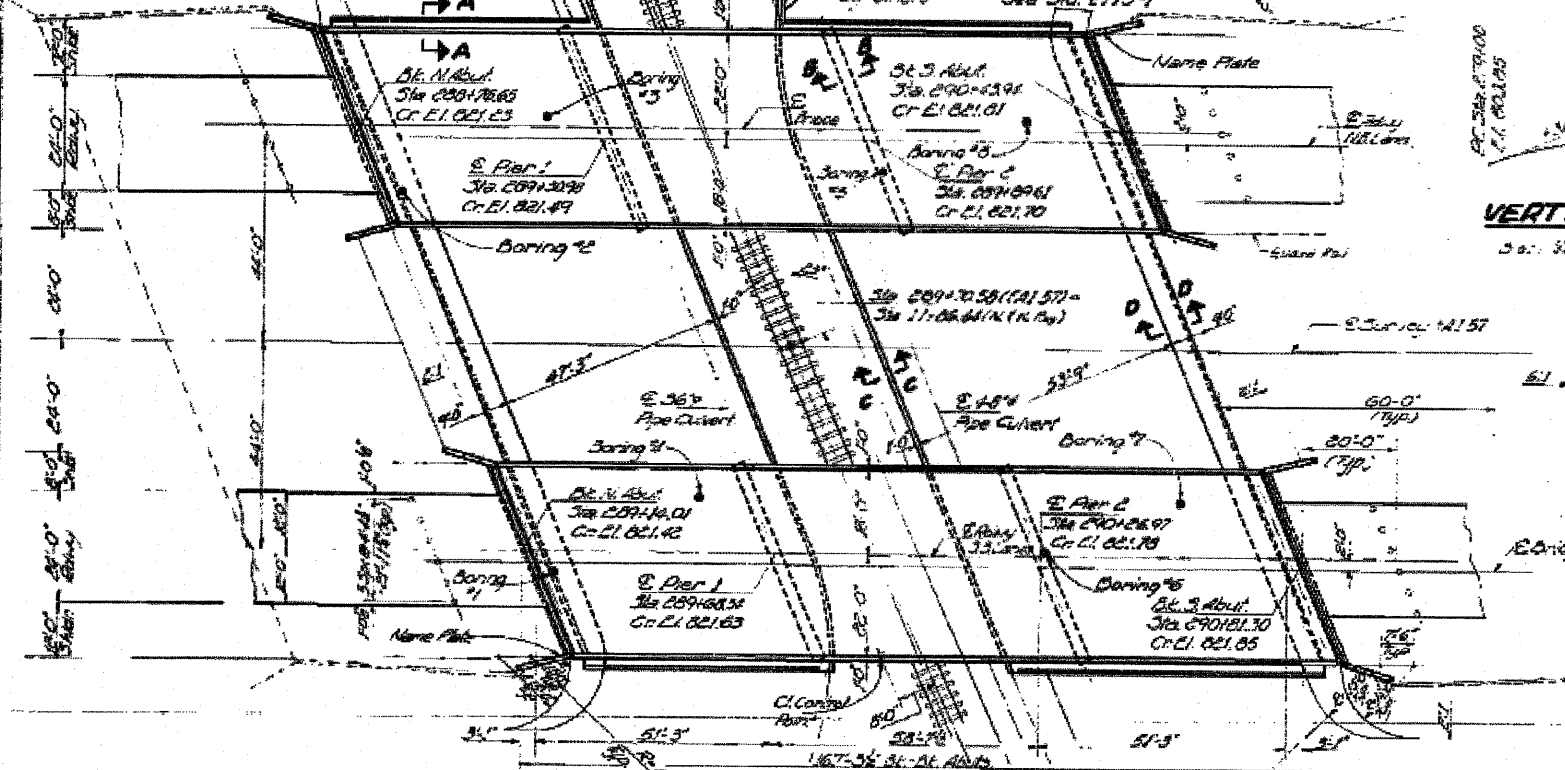
NORFOLK & WESTERN R.R.  
 BUILT 1908 BY  
 STATE OF ILLINOIS  
 P.A. 1757 SEC. 27-119  
 P.A. 1901 1-16-37-119  
 STA. 289+00.50  
 LOADING 10500 & ALT.

NAME PLATE  
 See Sta. 2113-1



VERTICAL CURVE DATA

See Sta. 2113-1



DESIGN STRESSES

$f_c = 1400$  psi (Concrete)  
 $f_s = 20000$  psi (Steel)  
 $v_e = 75$  psi (Figs.)  
 $n = 10$

PRECAST PRESTRESSED UNITS

$f_c = 5000$  psi  
 $f_s = 4000$  psi  
 $f_s = 20000$  psi (Tension)  
 $f_{st} = 17000$  psi

GENERAL NOTES

Concrete shall be used throughout. Concrete aggregates to be used in bridge shall be hard, must be sound, free of organic matter, and of 50-75 maximum. The concrete shall be of the type of concrete specified in the contract documents. The concrete shall be reinforced with steel reinforcement bars of the type and size specified in the contract documents. The reinforcement bars shall be placed in the concrete in accordance with the contract documents. The contractor shall be responsible for the proper placement and curing of the concrete. The contractor shall be responsible for the proper placement and curing of the concrete. The contractor shall be responsible for the proper placement and curing of the concrete.

TOTAL BILL OF MATERIAL

| ITEM                                   | QUANTITY | UNIT  | PRICE | TOTAL   |
|--|----------|-------|-------|---------|
| Class 4 Exc. - 2" - 4" - 6" - 8" - 10" | 1.97     | cu yd | 1.97  | 3.89    |
| Structural Steel                       | 100.00   | lb    | 0.60  | 60.00   |
| Class 1 Concrete                       | 100.00   | cu yd | 55.00 | 5500.00 |
| Reinforcing Steel                      | 100.00   | lb    | 0.50  | 50.00   |
| Cast-in-place Concrete                 | 100.00   | cu yd | 55.00 | 5500.00 |
| Concrete Piles                         | 100.00   | cu yd | 55.00 | 5500.00 |
| Cast-in-place Concrete                 | 100.00   | cu yd | 55.00 | 5500.00 |
| Name Plate                             | 1        | ea    | 8.00  | 8.00    |
| Slope Wall                             | 100.00   | sq ft | 2.00  | 200.00  |
| Furnishing & Erecting                  | 100.00   | sq ft | 2.00  | 200.00  |
| Concrete                               | 100.00   | cu yd | 55.00 | 5500.00 |
| Protective Coat                        | 100.00   | sq ft | 0.50  | 50.00   |
| Slope Steel                            | 100.00   | sq ft | 0.50  | 50.00   |

GENERAL PLAN ELEVATION

EAL 57 OVER NORFOLK & WESTERN R.R.  
 EAL 57 SEC. 27-119  
 FORD COUNTY  
 CONTRACT NO. 66862

DESIGNED: *Bobby Farkis*  
 CHECKED: *[Signature]*  
 DRAWN: *[Signature]*  
 CHECKED: *[Signature]*

EXAMINED: *[Signature]*  
 PASSED  
 APPROVED: \_\_\_\_\_

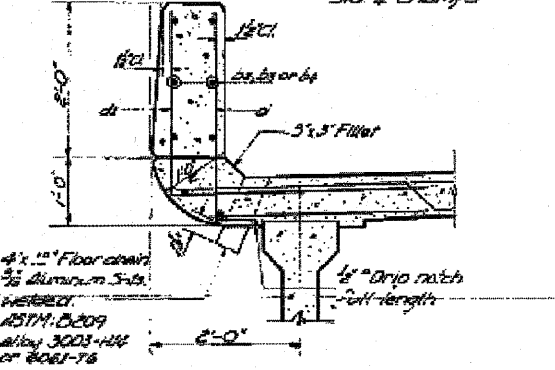
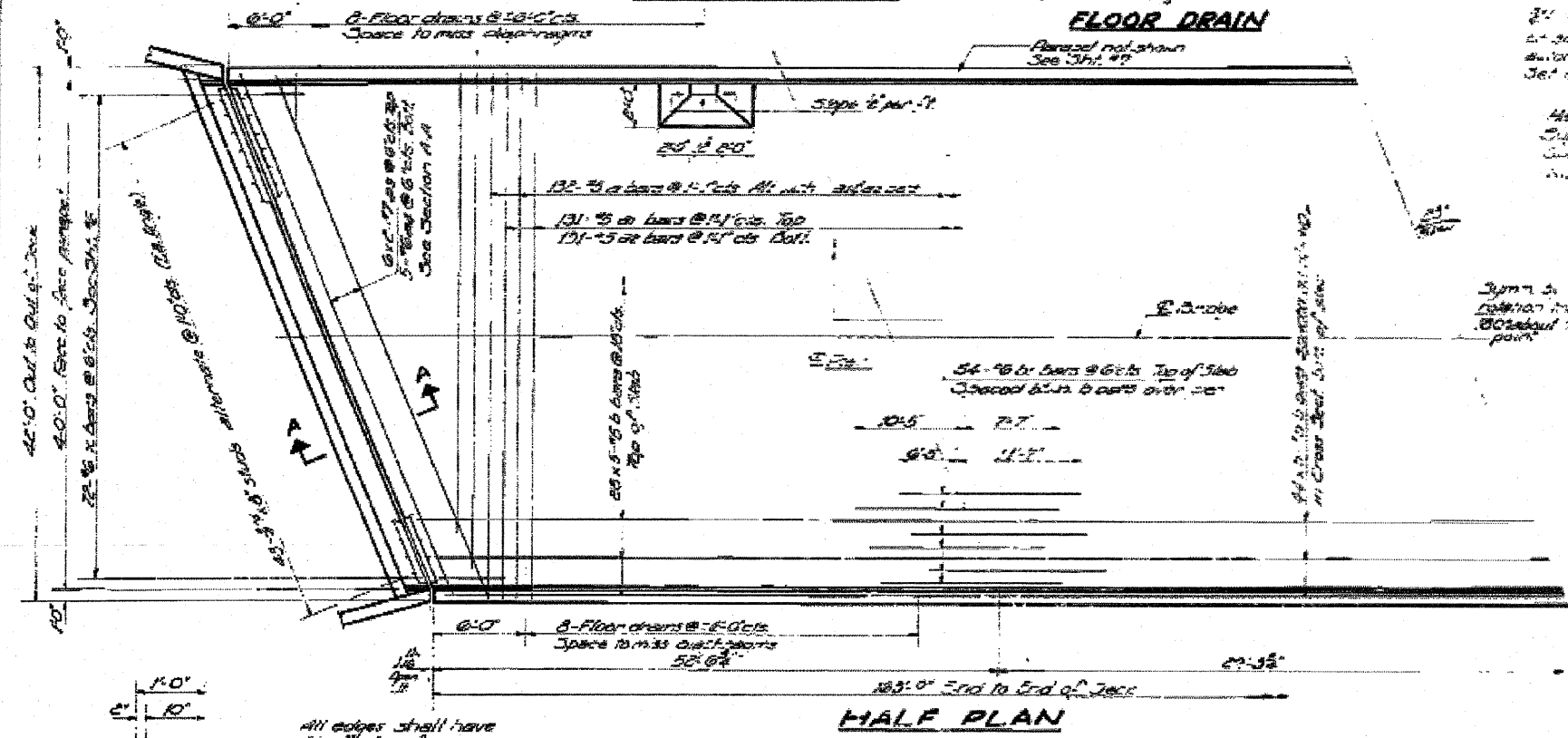
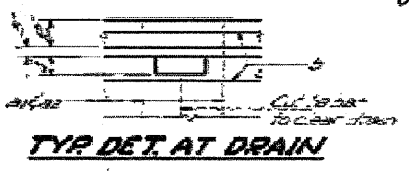
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

1/8" holes @ 12" on center in 1/2" bars across the slope line at 10' intervals shall be drilled, grouted or caulked with 1/2" diameter grout. Grout shall be removed.

|          |     |     |
|----------|-----|-----|
| DATE     | BY  | NO. |
| 10/20/09 | PLM | 28  |

1. The drainage shall be 20" x 15" - 6" deep  
The slope shall be 1/8" per foot  
Minimum bar length - 20' dia

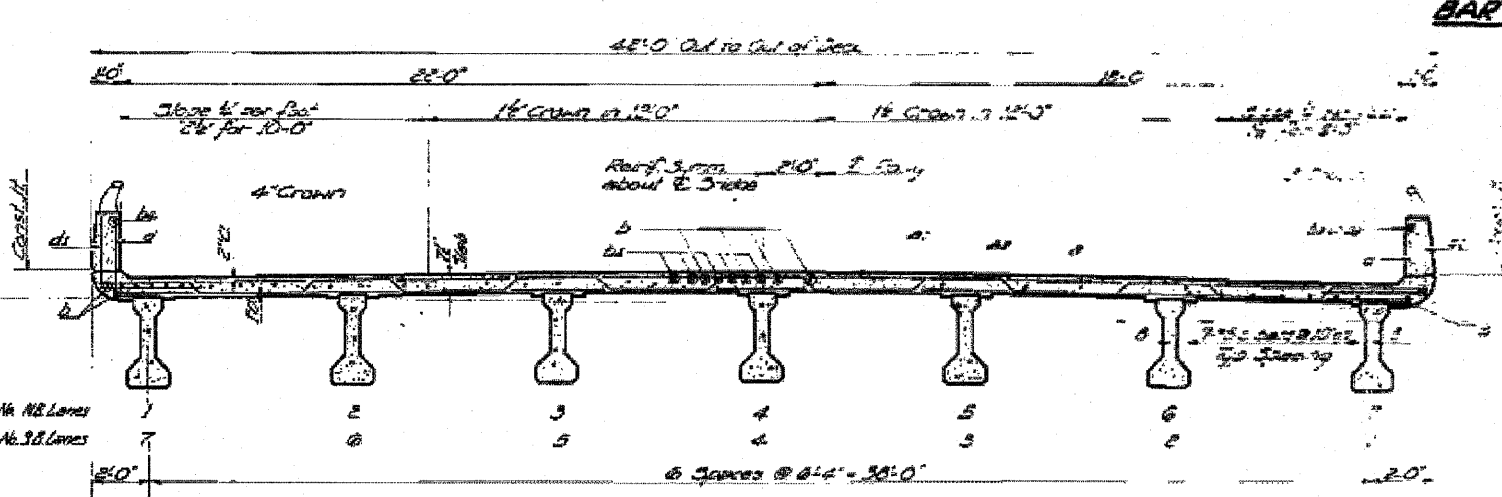
\* Order of bars shall be as shown  
2. If the drain is to be installed in the  
remainder of the deck, it shall be installed  
as shown.



4x12" Floor drain  
3/4" diameter hole  
3x3" Filler  
1/2" Drip notch  
1/2" length

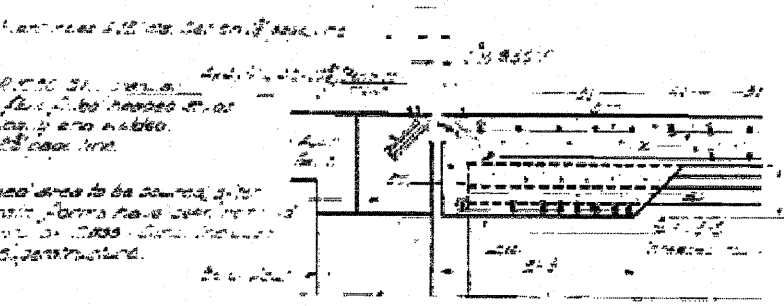
DESIGNED *Philip Jorchi*  
CHECKED *PLM*  
DRAWN *A.A.*  
CHECKED *PLM*

EXAMINED *Eric Hummer*  
PASSED  
APPROVED

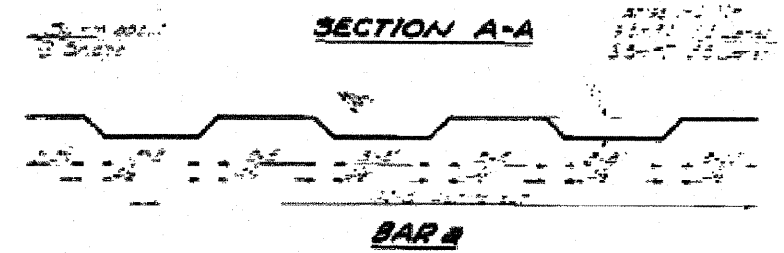


NEAR PIERS NEAR MIDSPAN

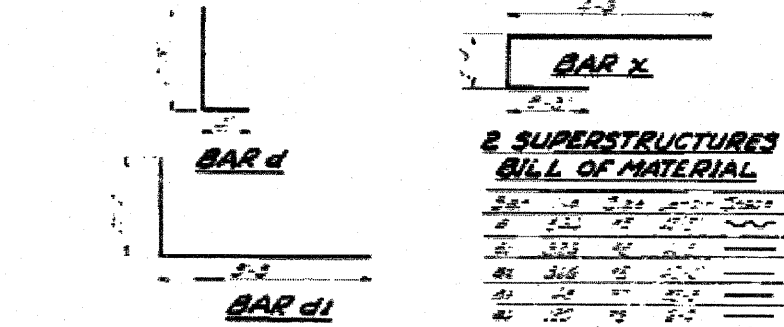
GROSS SECTION  
Looking Ahead - N.B. Lanes  
Looking Back - S.B. Lanes



SECTION A-A



BAR B



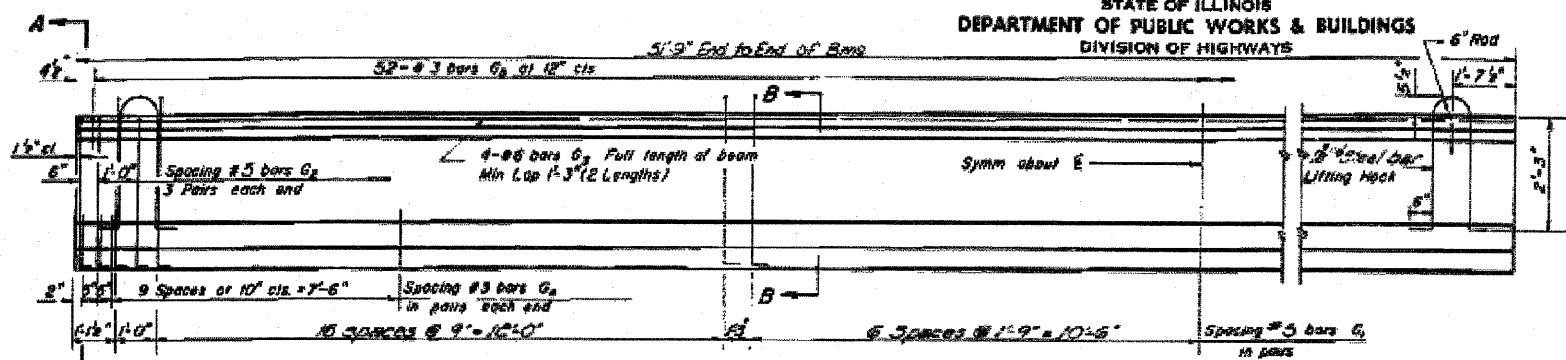
2 SUPERSTRUCTURES  
BILL OF MATERIAL

| NO. | DESCRIPTION               | QTY | UNIT  | PRICE  | TOTAL  |
|-----|---------------------------|-----|-------|--------|--------|
| 1   | 20' x 15' Aluminum Bar    | 1   | LB    | 1.50   | 1.50   |
| 2   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 3   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 4   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 5   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 6   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 7   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 8   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 9   | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 10  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 11  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 12  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 13  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 14  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 15  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 16  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 17  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 18  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 19  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 20  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 21  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 22  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 23  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 24  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 25  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 26  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 27  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 28  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 29  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |
| 30  | 1/2' x 18' x 18' Concrete | 1   | CU YD | 120.00 | 120.00 |

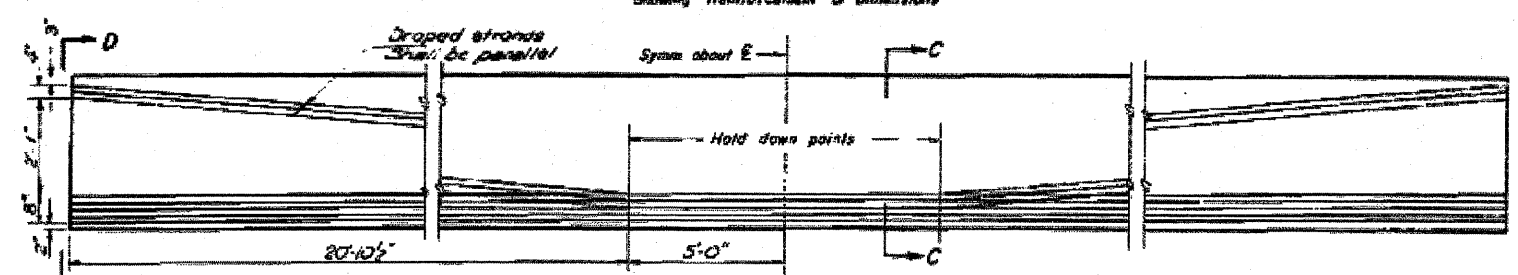
SUPERSTRUCTURE  
F.A. RT. 57 SEC. 27-1VB  
FORD COUNTY  
STA. 289+70.58

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

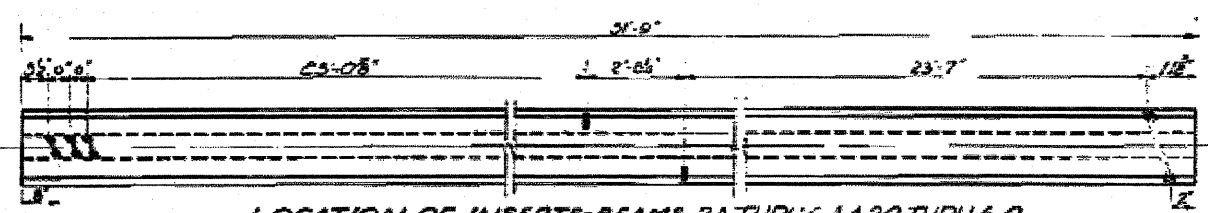
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|----------|-----|---------|--------------|-----------|
| DATE     | BY  | CHECKED | SCALE        | SHEET NO. |
| 11-27-57 | JTB | JTB     | 1/8" = 1'-0" | 3         |



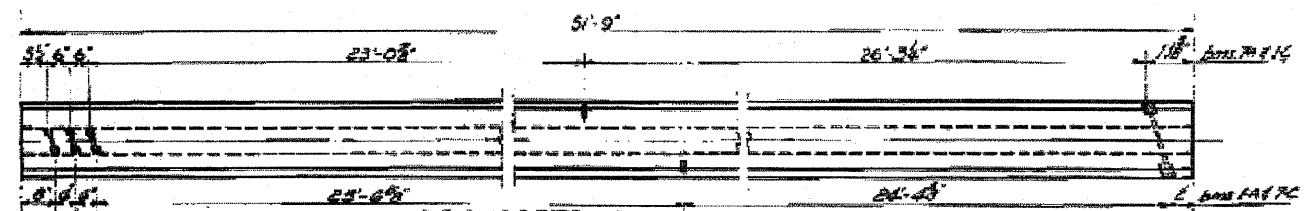
**ELEVATION OF BEAMS - SPANS 163**  
Showing Reinforcement & Dimensions



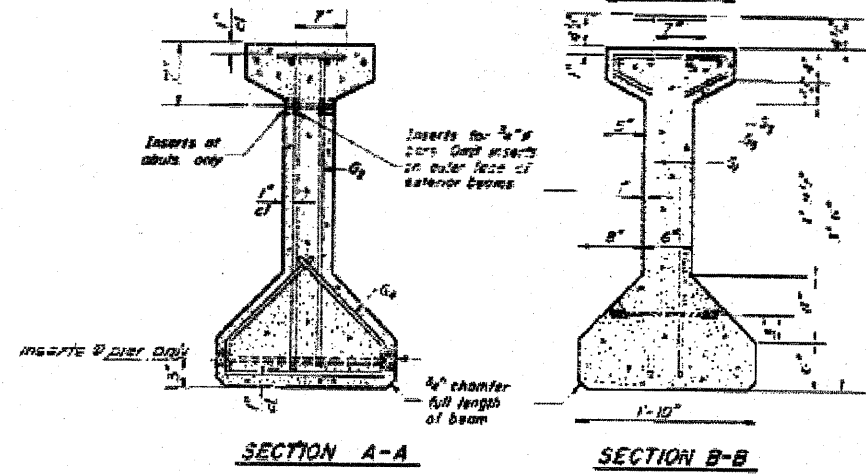
**ELEVATION OF BEAMS - SPANS 143**  
Showing Prestressing Steel



**LOCATION OF INSERTS - BEAMS 2A THRU 6A & 2C THRU 6C**

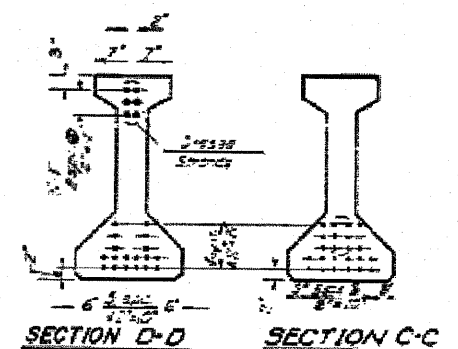


**LOCATION OF INSERTS - BEAMS 1A, 7A, 1C, 7C**



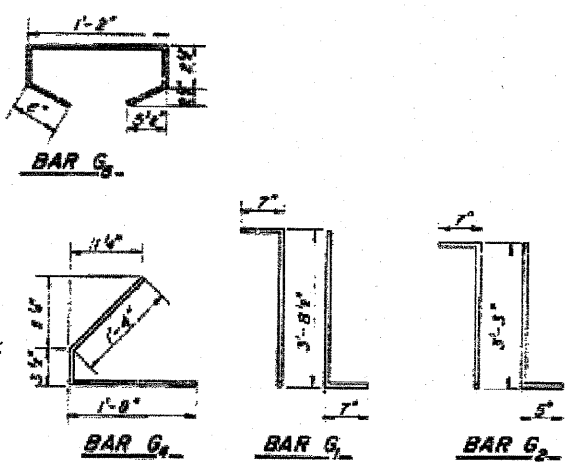
**SECTION A-A**

**SECTION B-B**



**SECTION D-D**

**SECTION C-C**



**BAR SCHEDULE**

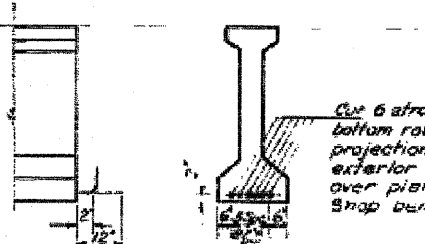
| ID | Qty | Size | Length | Notes |
|----|-----|------|--------|-------|
| G1 | 272 | #5   | 6'-0"  | TL    |
| G2 | 336 | #5   | 6'-5"  | TL    |
| G3 | 228 | #5   | 26'-5" | TL    |
| G4 | 132 | #3   | 2'-3"  | L     |
| G5 | 122 | #3   | 2'-7"  | L     |

**BILL OF MATERIAL**

| Item  | UNIT   | Qty  |
|---|--------|------|
| FURNISHING & Erecting Precast Prestressed Concrete I-Beams, 42" | Lin Ft | 1442 |

**NOTES**

All inserts and threaded rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete I-Beams shall be included in the contract unit price per linear foot of "Furnishing And Erecting Precast Prestressed Concrete I-Beams, 42 In." See Supplemental Specs for additional information regarding materials, Prestressing equipment, construction and handling methods and other requirements for Precast Prestressed Concrete I-Beams. Prestressing Steel shall have a nominal diameter of 1/8". Inserts for 3/4" threaded rods are to be two strut, coil type for interior I-Beams and single coil, flared loop type for exterior I-Beams. Steel for lifting hooks shall be non-deformed bars of structural or intermediate grade bitter steel. Ends of beams to be encased with cast in place concrete shall not be coated with asphalt paint.



**DETAIL AT END OF EXTERIOR BEAMS OVER PIERS**

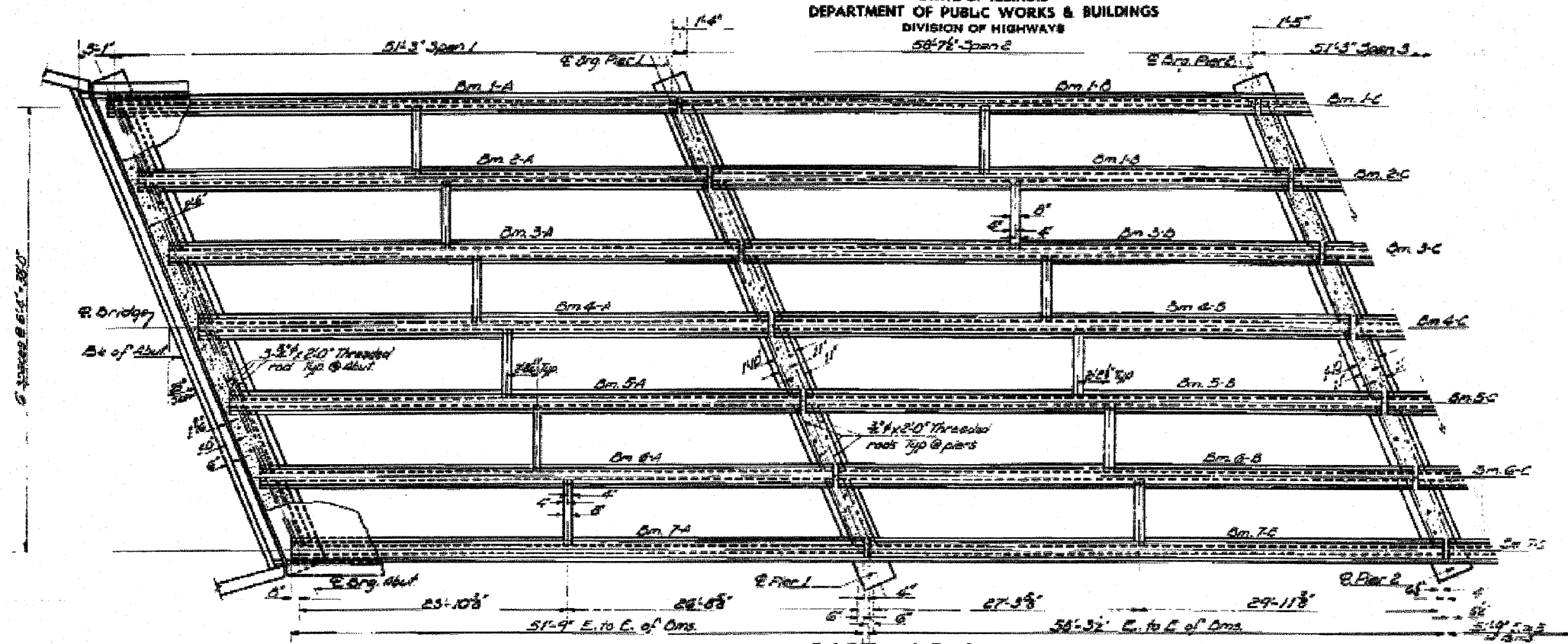
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|---------------------------------|-----------------------------|
| DESIGNED <i>Robert J. Smith</i> | DATE <i>May 5, 1965</i>     |
| CHECKED <i>[Signature]</i>      | APPROVED <i>[Signature]</i> |
| REVISIONS                       |                             |

PI-1-42 S 8-1-63

**BEAM DETAILS SPANS 163  
FA, RT 57 SEC. 27-1WB  
FORD COUNTY  
STA. 289+70.58**

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

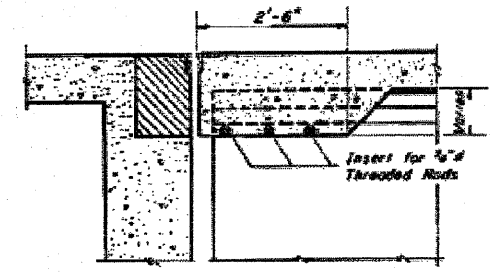
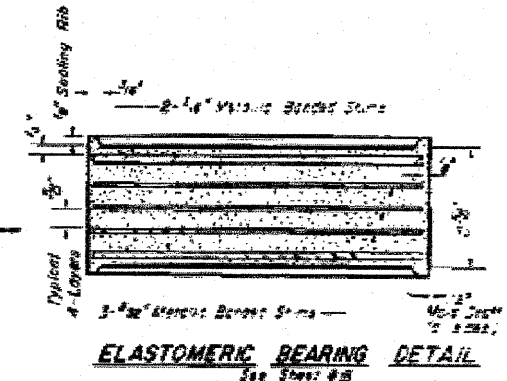
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|-------------|------|--------|---------|
| PROJECT NO. | ROAD | 150 29 | 16-1073 |
| SHEET NO.   |      |        | 5       |



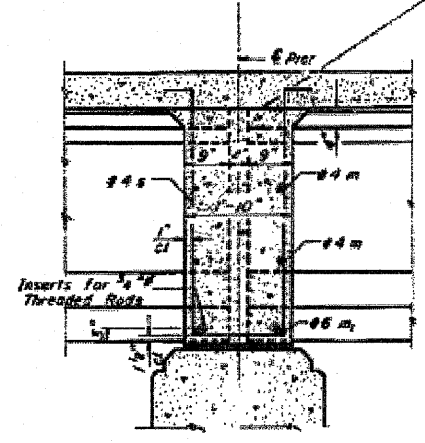
Note: For location of inserts See Sht. #34

**PART OF PLAN**

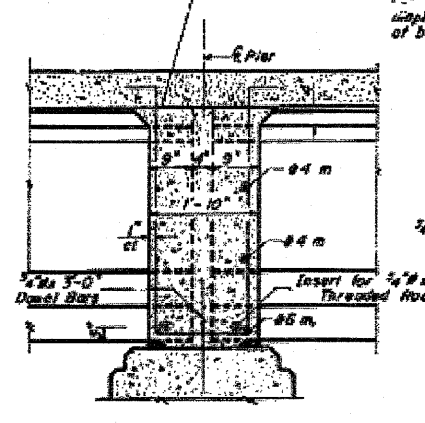
Note: Concrete in slab above the east abutment shall be placed not less than 45 minutes not more than 90 minutes after diaphragm has been poured. Pour diaphragm flush with top of beam.



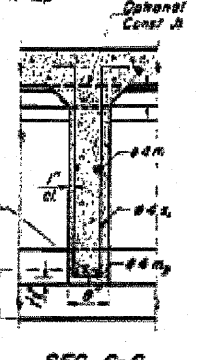
**SECTION AT ABUTMENT**



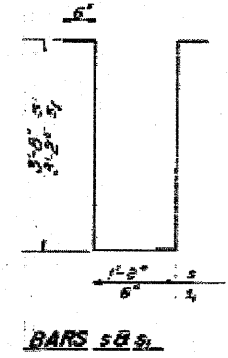
**PIER 2**



**PIER 1**



**SEC. C-C**  
(See Sht. #4)



**BARS 5/8"**

Note: Bars m, n, m<sub>1</sub>, s & s<sub>1</sub> are shown in Bill of Material Sheet #2.

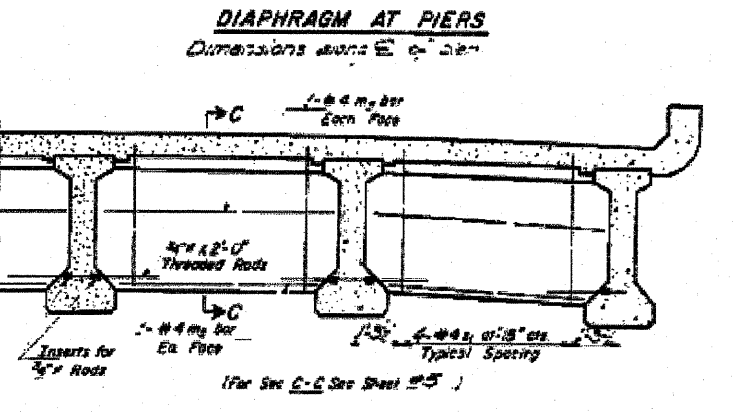
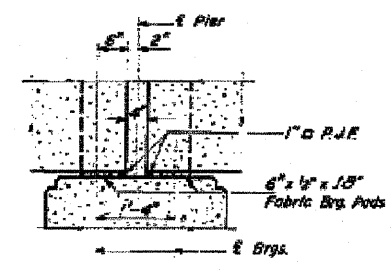
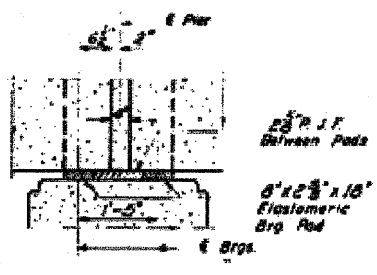
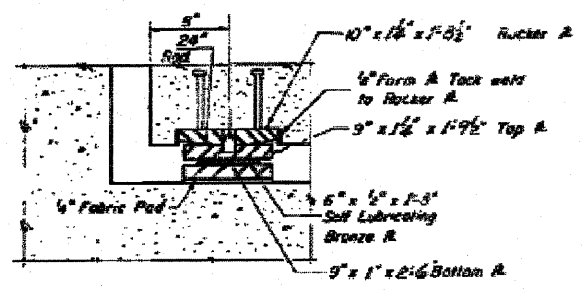
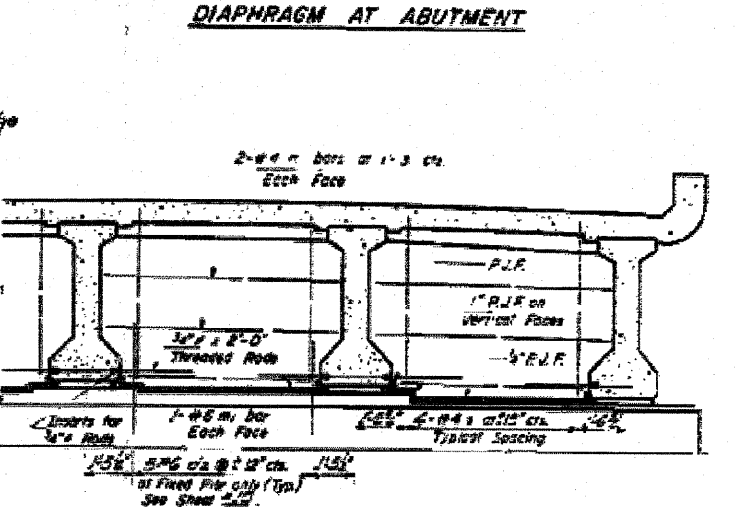
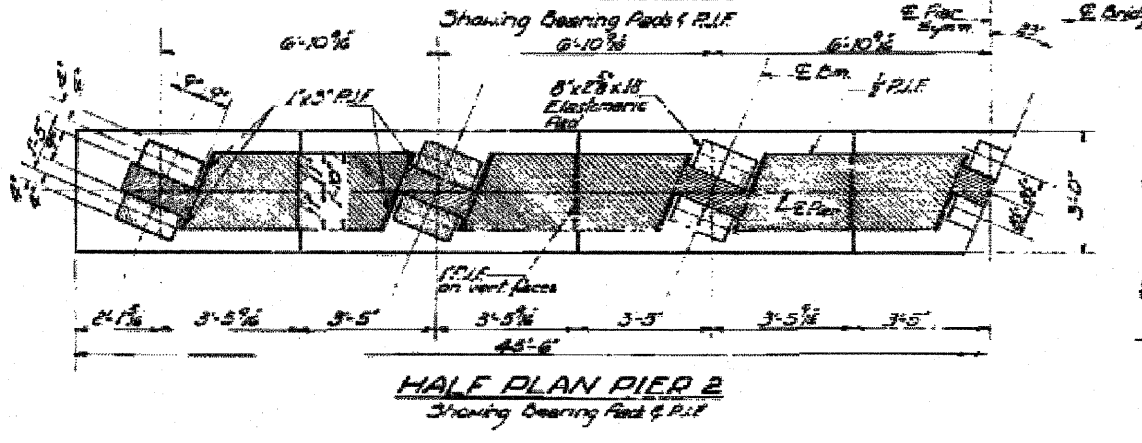
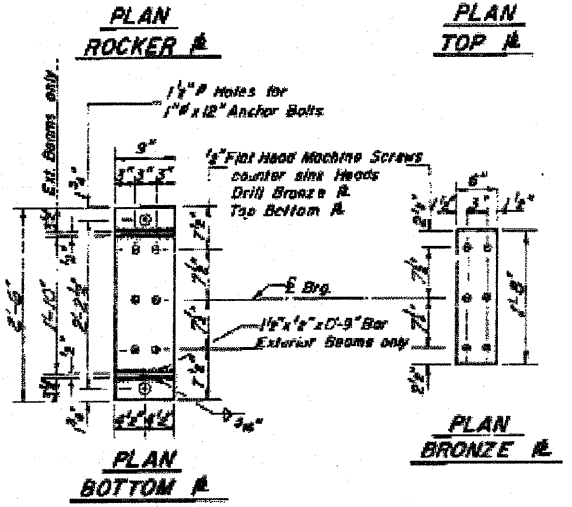
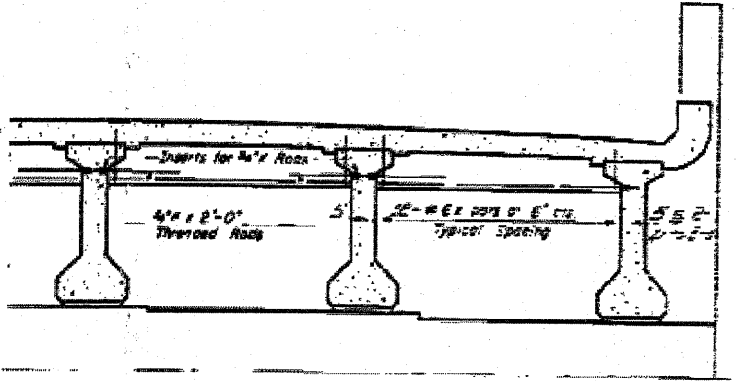
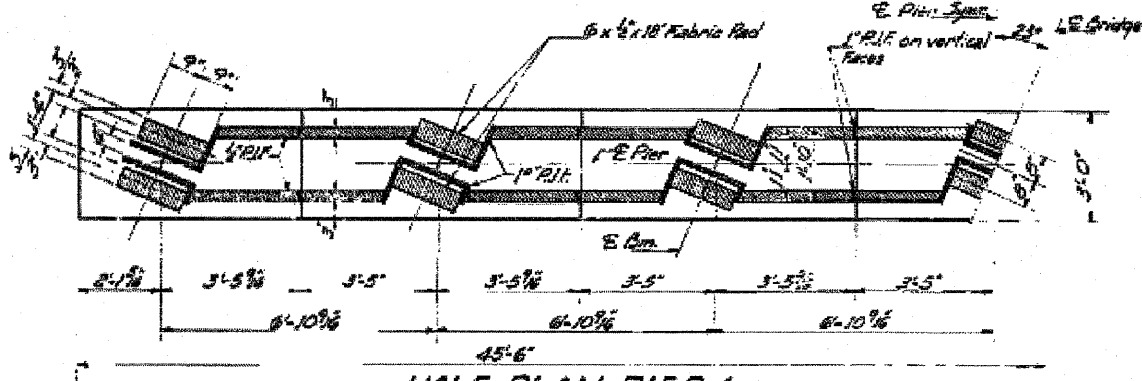
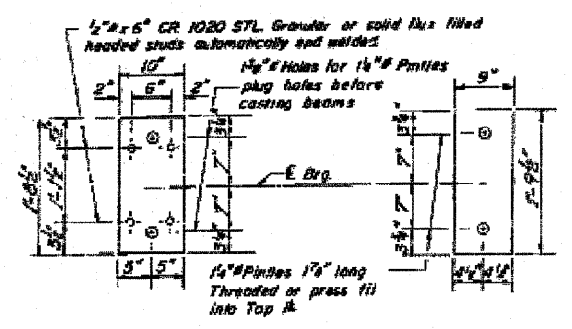
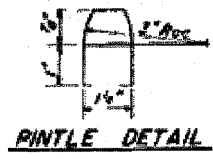
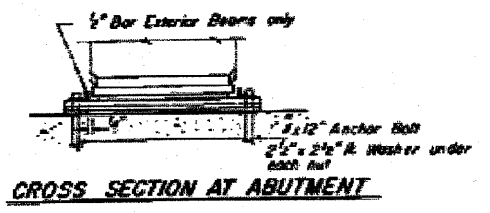
|                                |                             |
|--------------------------------|-----------------------------|
| DESIGNED <i>Robert J. Cook</i> | MAY 5 1965                  |
| CHECKED <i>[Signature]</i>     | EXAMINED <i>[Signature]</i> |
| DRAWN <i>Thomas B. Fuller</i>  | PASSED                      |
| CHECKED <i>[Signature]</i>     | APPROVED                    |

PI-2-C 8-1-63

**FRAMING PLAN**  
CAL. RT. 57 SEC. 27-JVB  
FORD COUNTY  
STA. 289+70.58

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |      |      |      |              |
|-------------|------|------|------|--------------|
| DATE        | BY   | CHKD | APPD | SHEET NO.    |
| 11-27-63    | FORD | JCB  | CC   | 16           |
| PROJECT NO. |      |      |      | TOTAL SHEETS |
| 11-27-63    |      |      |      | 16           |



|          |    |      |
|----------|----|------|
| DESIGNED | BY | DATE |
| Checked  | By |      |
| DRAWN    | BY |      |
| CHECKED  | BY |      |

PI-3-B 8-1-63

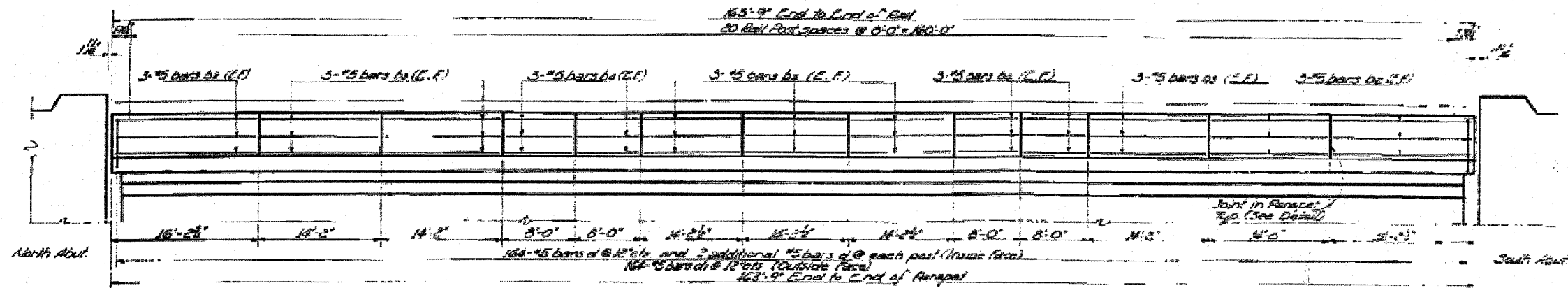
Note:  
Elastic Bearing Pads shall be grade 50.  
Cost of Elastic and Fabric Bearing Pads shall be incidental to "Furnishing and Erecting Precast Prestressed Concrete I-Beams." See Special Provisions.  
Cost of Rocker A - cast 1/2 beam is included in the cost of "Furnishing and Erecting Precast Prestressed Concrete I-Beams."

**BEARINGS & DIAPHRAGMS**  
**EAL RT. 57 SEC. 27-1VB**  
**FORD COUNTY**  
**STA. 289+70.58**



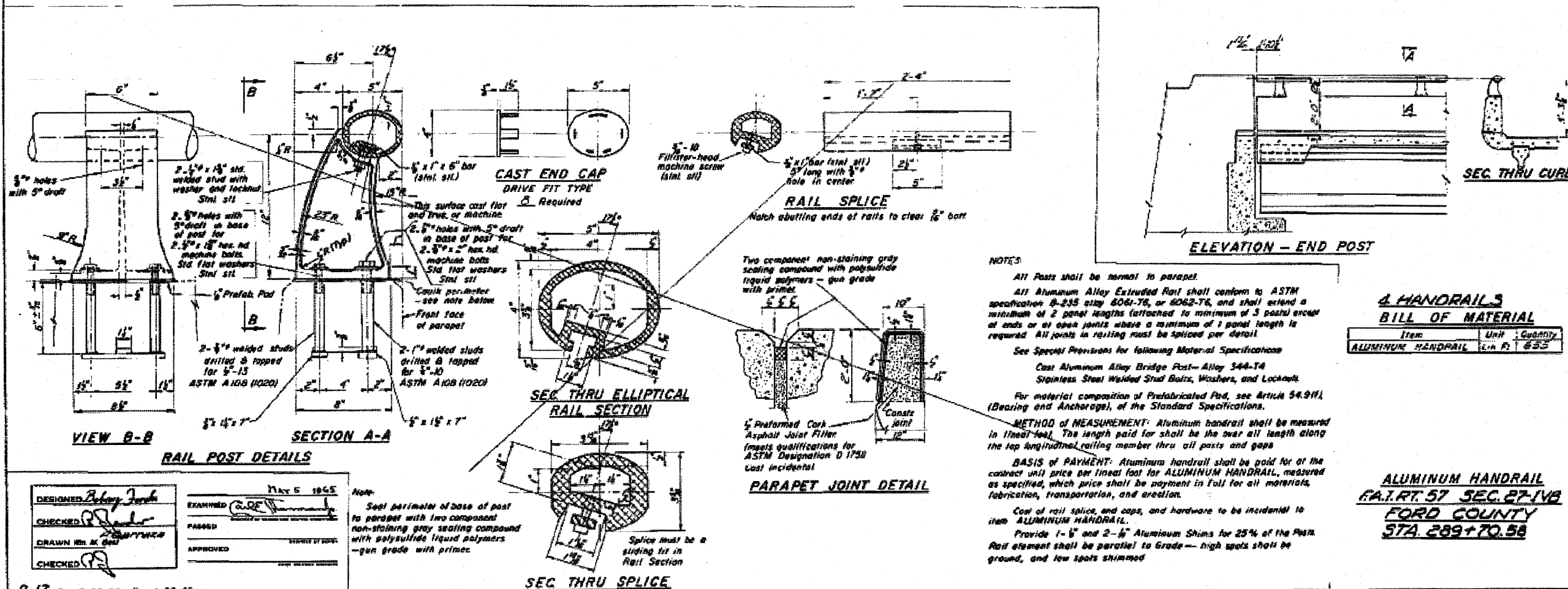
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|          |      |       |     |      |           |
|----------|------|-------|-----|------|-----------|
| DATE     | BY   | SCALE | NO. | REV. | SHEET NO. |
| 10/27/10 | FOAD | 1/8"  | 41  |      | 16 SHEETS |



**ELEVATION**

For Rail Detail See Sheet 74



**NOTES:**

All Posts shall be normal to parapet.

All Aluminum Alloy Extruded Rail shall conform to ASTM specification B-235 alloy 6061-T6, or 6062-T6, and shall extend a minimum of 2 panel lengths (attached to minimum of 3 posts) except at ends or at open joints above a minimum of 1 panel length is required. All joints in railing must be spliced per detail.

See Special Provisions for following Material Specifications:  
Cast Aluminum Alloy Bridge Post—Alloy 344-T4  
Stainless Steel Welded Stud Bolts, Washers, and Locknuts

For material composition of Prefabricated Pad, see Article 54.9(1), (Bearing and Anchorage), of the Standard Specifications.

**METHOD OF MEASUREMENT:** Aluminum handrail shall be measured in linear feet. The length paid for shall be the over all length along the top longitudinal railing member thru all posts and gaps.

**BASIS OF PAYMENT:** Aluminum handrail shall be paid for at the contract unit price per linear foot for ALUMINUM HANDRAIL, measured as specified, which price shall be payment in full for all materials, fabrication, transportation, and erection.

Cost of rail splice, and caps, and hardware to be incidental to item ALUMINUM HANDRAIL.

Provide 1-1/2" and 2-1/2" Aluminum Shims for 25% of the Panel. Rail element shall be parallel to Grade— high spots shall be ground, and low spots shimmed.

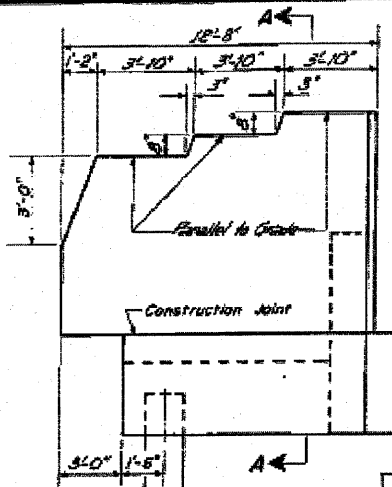
**4 HANDRAILS  
BILL OF MATERIAL**

| Item              | Unit     | Quantity |
|-------------------|----------|----------|
| ALUMINUM HANDRAIL | LIN. FT. | 653      |

**ALUMINUM HANDRAIL  
F.A.I.R.T. 57 SEC. 27-1VA  
FORD COUNTY  
STA. 289+70.58**

|                                 |                              |
|---------------------------------|------------------------------|
| DESIGNED: <i>Robert J. Ford</i> | DATE: MAY 5 1965             |
| CHECKED: <i>[Signature]</i>     | EXAMINED: <i>[Signature]</i> |
| DRAWN: <i>[Signature]</i>       | APPROVED: _____              |
| CHECKED: <i>[Signature]</i>     |                              |

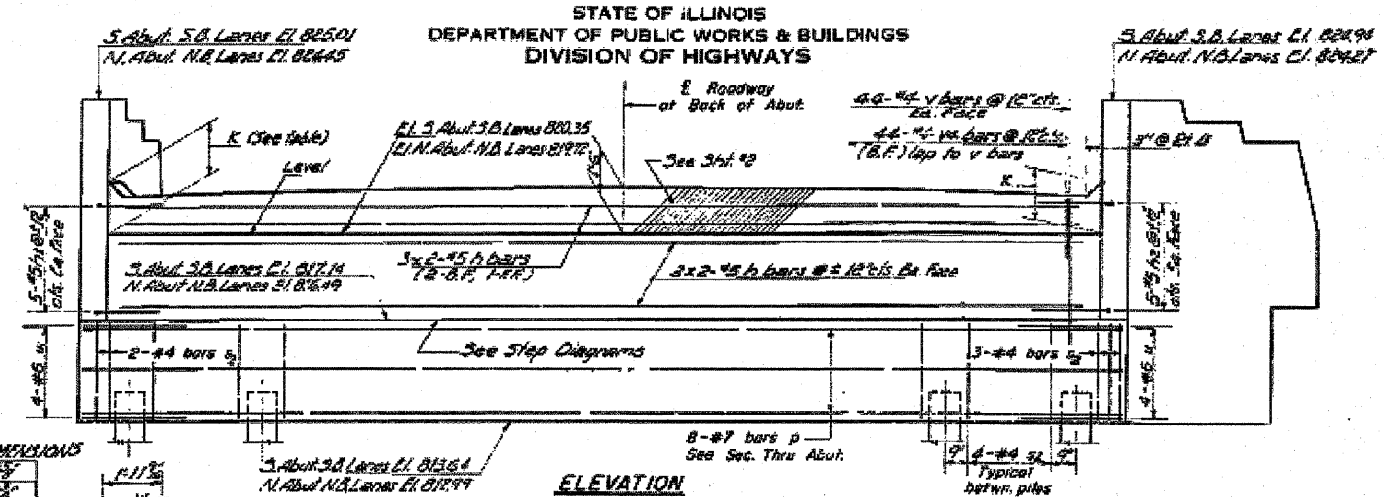
R-17 DWG. 5-28-65 REV. 1-20-65  
REV. 1-25-65



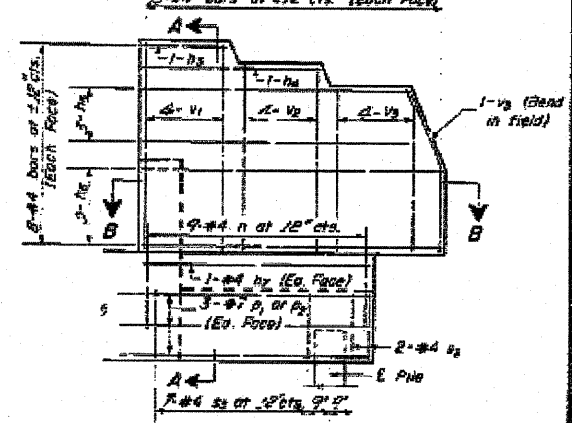
WING WALL ELEVATION  
Dimensions

TABLE OF DIMENSIONS

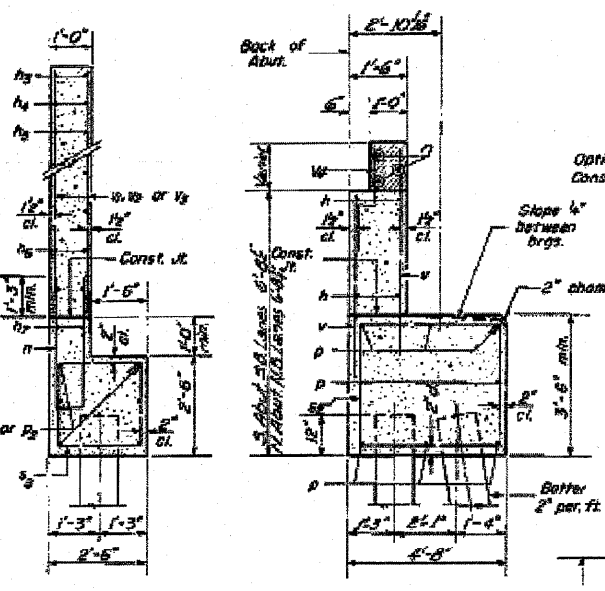
|             |    |       |
|-------------|----|-------|
| N. Abut.    | E. | 1'-0" |
| N. Lanes    | A1 | 1'-6" |
| S. Abut.    | E. | 1'-6" |
| S. B. Lanes | A1 | 1'-5" |



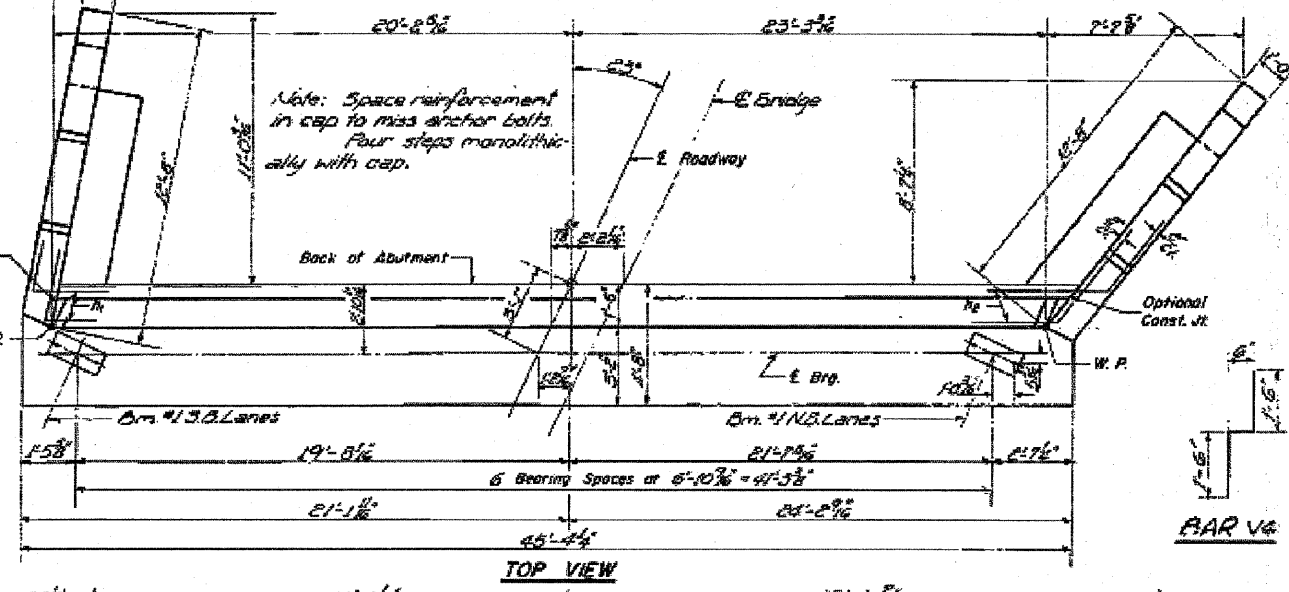
ELEVATION



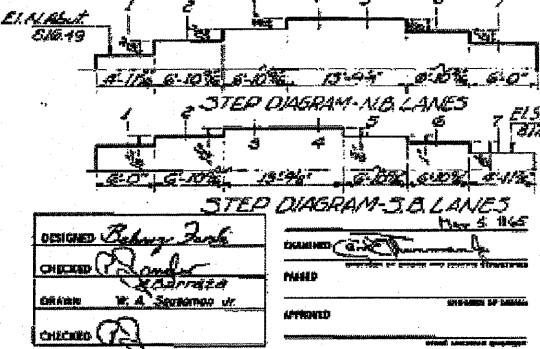
WING WALL ELEVATION  
Reinforcement



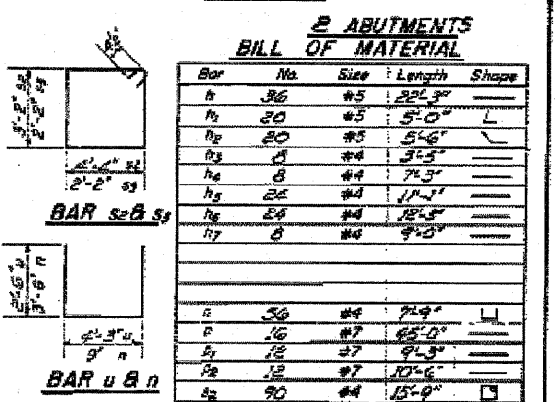
SEC. A-A  
SEC. THRU ABUT.



TOP VIEW



A-B-R (15°-35°) 7-19-62 Rev. 11-27-62  
Rev. N.H. 1-28-58



SEC. B-B  
2 ABUTMENTS  
BILL OF MATERIAL

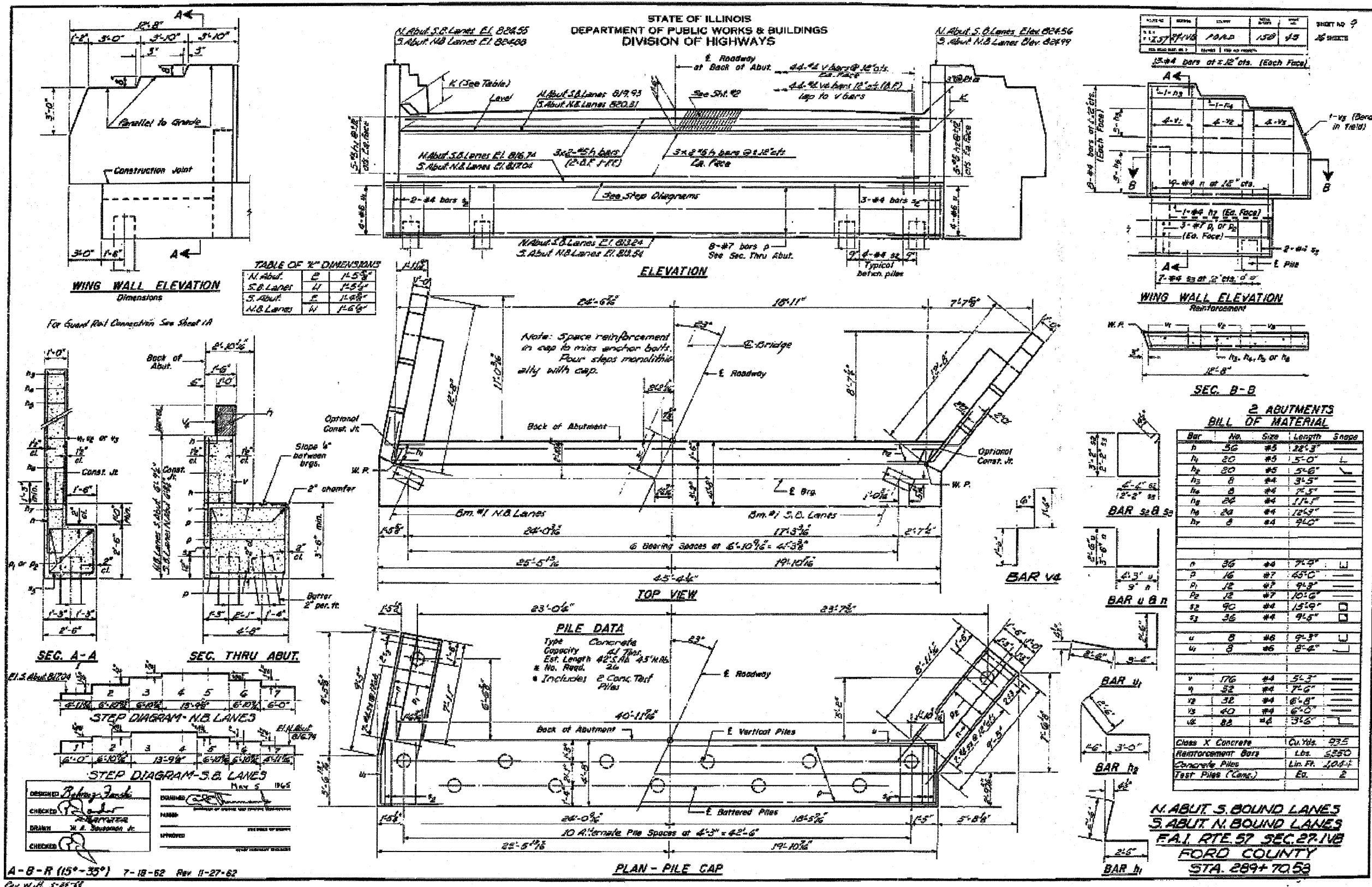
| Bar | No. | Size | Length | Shape |
|-----|-----|------|--------|-------|
| B   | 36  | #5   | 22'-3" | L     |
| C   | 20  | #5   | 5'-0"  | L     |
| D   | 20  | #5   | 5'-6"  | L     |
| E   | 8   | #4   | 3'-5"  | —     |
| F   | 8   | #4   | 7'-3"  | —     |
| G   | 24  | #4   | 17'-3" | —     |
| H   | 24  | #4   | 12'-5" | —     |
| I   | 8   | #4   | 9'-3"  | —     |
| J   | 36  | #4   | 7'-9"  | L     |
| K   | 16  | #7   | 65'-0" | —     |
| L   | 12  | #7   | 9'-3"  | —     |
| M   | 12  | #7   | 10'-6" | —     |
| N   | 20  | #4   | 15'-0" | □     |
| O   | 36  | #4   | 9'-5"  | □     |
| P   | 8   | #6   | 7'-3"  | L     |
| Q   | 8   | #6   | 8'-7"  | L     |
| R   | 176 | #4   | 5'-9"  | —     |
| S   | 32  | #4   | 7'-6"  | —     |
| T   | 32  | #4   | 6'-8"  | —     |
| U   | 40  | #4   | 8'-0"  | —     |
| V   | 63  | #4   | 5'-8"  | —     |

PILE DATA  
Type Concrete  
Capacity 27 Tons  
Est. Length 40' S. Ab., 45' N. Ab.  
± No. Reqd. 26  
\* Includes 2 Conc. Test Piles

S. ABUT-S. BOUND LANES  
N. ABUT-N. BOUND LANES  
F.A. RTE. 57 SEC. 22 1/8  
FORD COUNTY  
STA. 289+70.58

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

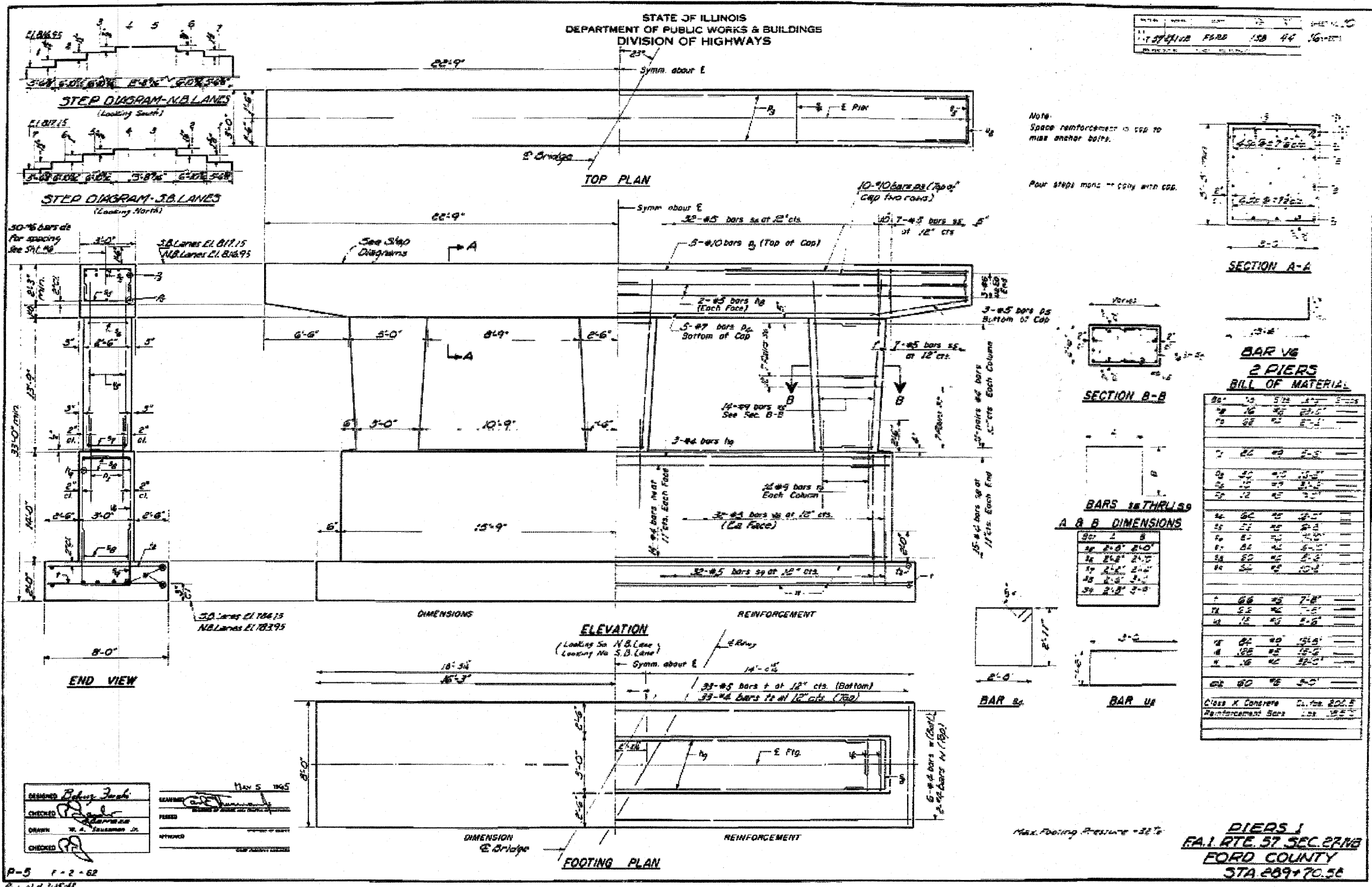
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|----------|--------|---------|--------------|-----------|
| DATE     | BY     | CHECKED | SCALE        | SHEET NO. |
| 12-27-57 | J.P.M. | J.S.D.  | 1/8" = 1'-0" | 28        |



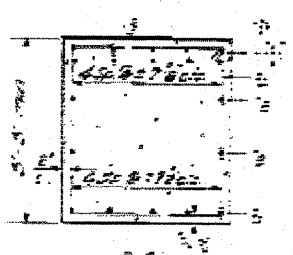
A-B-R (15'-33") 7-18-52 Rev 11-27-52  
Rev. H.H. 3-28-53

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

DATE: 12-1-62  
PROJECT: I-57 SEC. 27 NB  
SHEET NO. 20



NOTE:  
Space reinforcement is cap to  
main anchor bolts.  
Pour steps main cap with cap.



**SECTION A-A**

**SECTION B-B**

**BAR V6**

**2 PIERS**

**BILL OF MATERIALS**

| NO. | QTY | SIZE | LENGTH | WEIGHT |
|-----|-----|------|--------|--------|
| 1   | 16  | #5   | 21'-0" | 11.5   |
| 2   | 16  | #5   | 21'-0" | 11.5   |
| 3   | 16  | #5   | 21'-0" | 11.5   |
| 4   | 16  | #5   | 21'-0" | 11.5   |
| 5   | 16  | #5   | 21'-0" | 11.5   |
| 6   | 16  | #5   | 21'-0" | 11.5   |
| 7   | 16  | #5   | 21'-0" | 11.5   |
| 8   | 16  | #5   | 21'-0" | 11.5   |
| 9   | 16  | #5   | 21'-0" | 11.5   |
| 10  | 16  | #5   | 21'-0" | 11.5   |
| 11  | 16  | #5   | 21'-0" | 11.5   |
| 12  | 16  | #5   | 21'-0" | 11.5   |
| 13  | 16  | #5   | 21'-0" | 11.5   |
| 14  | 16  | #5   | 21'-0" | 11.5   |
| 15  | 16  | #5   | 21'-0" | 11.5   |
| 16  | 16  | #5   | 21'-0" | 11.5   |
| 17  | 16  | #5   | 21'-0" | 11.5   |
| 18  | 16  | #5   | 21'-0" | 11.5   |
| 19  | 16  | #5   | 21'-0" | 11.5   |
| 20  | 16  | #5   | 21'-0" | 11.5   |
| 21  | 16  | #5   | 21'-0" | 11.5   |
| 22  | 16  | #5   | 21'-0" | 11.5   |
| 23  | 16  | #5   | 21'-0" | 11.5   |
| 24  | 16  | #5   | 21'-0" | 11.5   |
| 25  | 16  | #5   | 21'-0" | 11.5   |
| 26  | 16  | #5   | 21'-0" | 11.5   |
| 27  | 16  | #5   | 21'-0" | 11.5   |
| 28  | 16  | #5   | 21'-0" | 11.5   |
| 29  | 16  | #5   | 21'-0" | 11.5   |
| 30  | 16  | #5   | 21'-0" | 11.5   |
| 31  | 16  | #5   | 21'-0" | 11.5   |
| 32  | 16  | #5   | 21'-0" | 11.5   |
| 33  | 16  | #5   | 21'-0" | 11.5   |
| 34  | 16  | #5   | 21'-0" | 11.5   |
| 35  | 16  | #5   | 21'-0" | 11.5   |
| 36  | 16  | #5   | 21'-0" | 11.5   |
| 37  | 16  | #5   | 21'-0" | 11.5   |
| 38  | 16  | #5   | 21'-0" | 11.5   |
| 39  | 16  | #5   | 21'-0" | 11.5   |
| 40  | 16  | #5   | 21'-0" | 11.5   |
| 41  | 16  | #5   | 21'-0" | 11.5   |
| 42  | 16  | #5   | 21'-0" | 11.5   |
| 43  | 16  | #5   | 21'-0" | 11.5   |
| 44  | 16  | #5   | 21'-0" | 11.5   |
| 45  | 16  | #5   | 21'-0" | 11.5   |
| 46  | 16  | #5   | 21'-0" | 11.5   |
| 47  | 16  | #5   | 21'-0" | 11.5   |
| 48  | 16  | #5   | 21'-0" | 11.5   |
| 49  | 16  | #5   | 21'-0" | 11.5   |
| 50  | 16  | #5   | 21'-0" | 11.5   |
| 51  | 16  | #5   | 21'-0" | 11.5   |
| 52  | 16  | #5   | 21'-0" | 11.5   |
| 53  | 16  | #5   | 21'-0" | 11.5   |
| 54  | 16  | #5   | 21'-0" | 11.5   |
| 55  | 16  | #5   | 21'-0" | 11.5   |
| 56  | 16  | #5   | 21'-0" | 11.5   |
| 57  | 16  | #5   | 21'-0" | 11.5   |
| 58  | 16  | #5   | 21'-0" | 11.5   |
| 59  | 16  | #5   | 21'-0" | 11.5   |
| 60  | 16  | #5   | 21'-0" | 11.5   |
| 61  | 16  | #5   | 21'-0" | 11.5   |
| 62  | 16  | #5   | 21'-0" | 11.5   |
| 63  | 16  | #5   | 21'-0" | 11.5   |
| 64  | 16  | #5   | 21'-0" | 11.5   |
| 65  | 16  | #5   | 21'-0" | 11.5   |
| 66  | 16  | #5   | 21'-0" | 11.5   |
| 67  | 16  | #5   | 21'-0" | 11.5   |
| 68  | 16  | #5   | 21'-0" | 11.5   |
| 69  | 16  | #5   | 21'-0" | 11.5   |
| 70  | 16  | #5   | 21'-0" | 11.5   |
| 71  | 16  | #5   | 21'-0" | 11.5   |
| 72  | 16  | #5   | 21'-0" | 11.5   |
| 73  | 16  | #5   | 21'-0" | 11.5   |
| 74  | 16  | #5   | 21'-0" | 11.5   |
| 75  | 16  | #5   | 21'-0" | 11.5   |
| 76  | 16  | #5   | 21'-0" | 11.5   |
| 77  | 16  | #5   | 21'-0" | 11.5   |
| 78  | 16  | #5   | 21'-0" | 11.5   |
| 79  | 16  | #5   | 21'-0" | 11.5   |
| 80  | 16  | #5   | 21'-0" | 11.5   |
| 81  | 16  | #5   | 21'-0" | 11.5   |
| 82  | 16  | #5   | 21'-0" | 11.5   |
| 83  | 16  | #5   | 21'-0" | 11.5   |
| 84  | 16  | #5   | 21'-0" | 11.5   |
| 85  | 16  | #5   | 21'-0" | 11.5   |
| 86  | 16  | #5   | 21'-0" | 11.5   |
| 87  | 16  | #5   | 21'-0" | 11.5   |
| 88  | 16  | #5   | 21'-0" | 11.5   |
| 89  | 16  | #5   | 21'-0" | 11.5   |
| 90  | 16  | #5   | 21'-0" | 11.5   |
| 91  | 16  | #5   | 21'-0" | 11.5   |
| 92  | 16  | #5   | 21'-0" | 11.5   |
| 93  | 16  | #5   | 21'-0" | 11.5   |
| 94  | 16  | #5   | 21'-0" | 11.5   |
| 95  | 16  | #5   | 21'-0" | 11.5   |
| 96  | 16  | #5   | 21'-0" | 11.5   |
| 97  | 16  | #5   | 21'-0" | 11.5   |
| 98  | 16  | #5   | 21'-0" | 11.5   |
| 99  | 16  | #5   | 21'-0" | 11.5   |
| 100 | 16  | #5   | 21'-0" | 11.5   |

**BARS IN THRU LUG**

**A & B DIMENSIONS**

| NO. | A     | B     |
|-----|-------|-------|
| 1   | 2'-0" | 2'-0" |
| 2   | 2'-0" | 2'-0" |
| 3   | 2'-0" | 2'-0" |
| 4   | 2'-0" | 2'-0" |
| 5   | 2'-0" | 2'-0" |
| 6   | 2'-0" | 2'-0" |
| 7   | 2'-0" | 2'-0" |
| 8   | 2'-0" | 2'-0" |
| 9   | 2'-0" | 2'-0" |
| 10  | 2'-0" | 2'-0" |
| 11  | 2'-0" | 2'-0" |
| 12  | 2'-0" | 2'-0" |
| 13  | 2'-0" | 2'-0" |
| 14  | 2'-0" | 2'-0" |
| 15  | 2'-0" | 2'-0" |
| 16  | 2'-0" | 2'-0" |
| 17  | 2'-0" | 2'-0" |
| 18  | 2'-0" | 2'-0" |
| 19  | 2'-0" | 2'-0" |
| 20  | 2'-0" | 2'-0" |
| 21  | 2'-0" | 2'-0" |
| 22  | 2'-0" | 2'-0" |
| 23  | 2'-0" | 2'-0" |
| 24  | 2'-0" | 2'-0" |
| 25  | 2'-0" | 2'-0" |
| 26  | 2'-0" | 2'-0" |
| 27  | 2'-0" | 2'-0" |
| 28  | 2'-0" | 2'-0" |
| 29  | 2'-0" | 2'-0" |
| 30  | 2'-0" | 2'-0" |
| 31  | 2'-0" | 2'-0" |
| 32  | 2'-0" | 2'-0" |
| 33  | 2'-0" | 2'-0" |
| 34  | 2'-0" | 2'-0" |
| 35  | 2'-0" | 2'-0" |
| 36  | 2'-0" | 2'-0" |
| 37  | 2'-0" | 2'-0" |
| 38  | 2'-0" | 2'-0" |
| 39  | 2'-0" | 2'-0" |
| 40  | 2'-0" | 2'-0" |
| 41  | 2'-0" | 2'-0" |
| 42  | 2'-0" | 2'-0" |
| 43  | 2'-0" | 2'-0" |
| 44  | 2'-0" | 2'-0" |
| 45  | 2'-0" | 2'-0" |
| 46  | 2'-0" | 2'-0" |
| 47  | 2'-0" | 2'-0" |
| 48  | 2'-0" | 2'-0" |
| 49  | 2'-0" | 2'-0" |
| 50  | 2'-0" | 2'-0" |
| 51  | 2'-0" | 2'-0" |
| 52  | 2'-0" | 2'-0" |
| 53  | 2'-0" | 2'-0" |
| 54  | 2'-0" | 2'-0" |
| 55  | 2'-0" | 2'-0" |
| 56  | 2'-0" | 2'-0" |
| 57  | 2'-0" | 2'-0" |
| 58  | 2'-0" | 2'-0" |
| 59  | 2'-0" | 2'-0" |
| 60  | 2'-0" | 2'-0" |
| 61  | 2'-0" | 2'-0" |
| 62  | 2'-0" | 2'-0" |
| 63  | 2'-0" | 2'-0" |
| 64  | 2'-0" | 2'-0" |
| 65  | 2'-0" | 2'-0" |
| 66  | 2'-0" | 2'-0" |
| 67  | 2'-0" | 2'-0" |
| 68  | 2'-0" | 2'-0" |
| 69  | 2'-0" | 2'-0" |
| 70  | 2'-0" | 2'-0" |
| 71  | 2'-0" | 2'-0" |
| 72  | 2'-0" | 2'-0" |
| 73  | 2'-0" | 2'-0" |
| 74  | 2'-0" | 2'-0" |
| 75  | 2'-0" | 2'-0" |
| 76  | 2'-0" | 2'-0" |
| 77  | 2'-0" | 2'-0" |
| 78  | 2'-0" | 2'-0" |
| 79  | 2'-0" | 2'-0" |
| 80  | 2'-0" | 2'-0" |
| 81  | 2'-0" | 2'-0" |
| 82  | 2'-0" | 2'-0" |
| 83  | 2'-0" | 2'-0" |
| 84  | 2'-0" | 2'-0" |
| 85  | 2'-0" | 2'-0" |
| 86  | 2'-0" | 2'-0" |
| 87  | 2'-0" | 2'-0" |
| 88  | 2'-0" | 2'-0" |
| 89  | 2'-0" | 2'-0" |
| 90  | 2'-0" | 2'-0" |
| 91  | 2'-0" | 2'-0" |
| 92  | 2'-0" | 2'-0" |
| 93  | 2'-0" | 2'-0" |
| 94  | 2'-0" | 2'-0" |
| 95  | 2'-0" | 2'-0" |
| 96  | 2'-0" | 2'-0" |
| 97  | 2'-0" | 2'-0" |
| 98  | 2'-0" | 2'-0" |
| 99  | 2'-0" | 2'-0" |
| 100 | 2'-0" | 2'-0" |

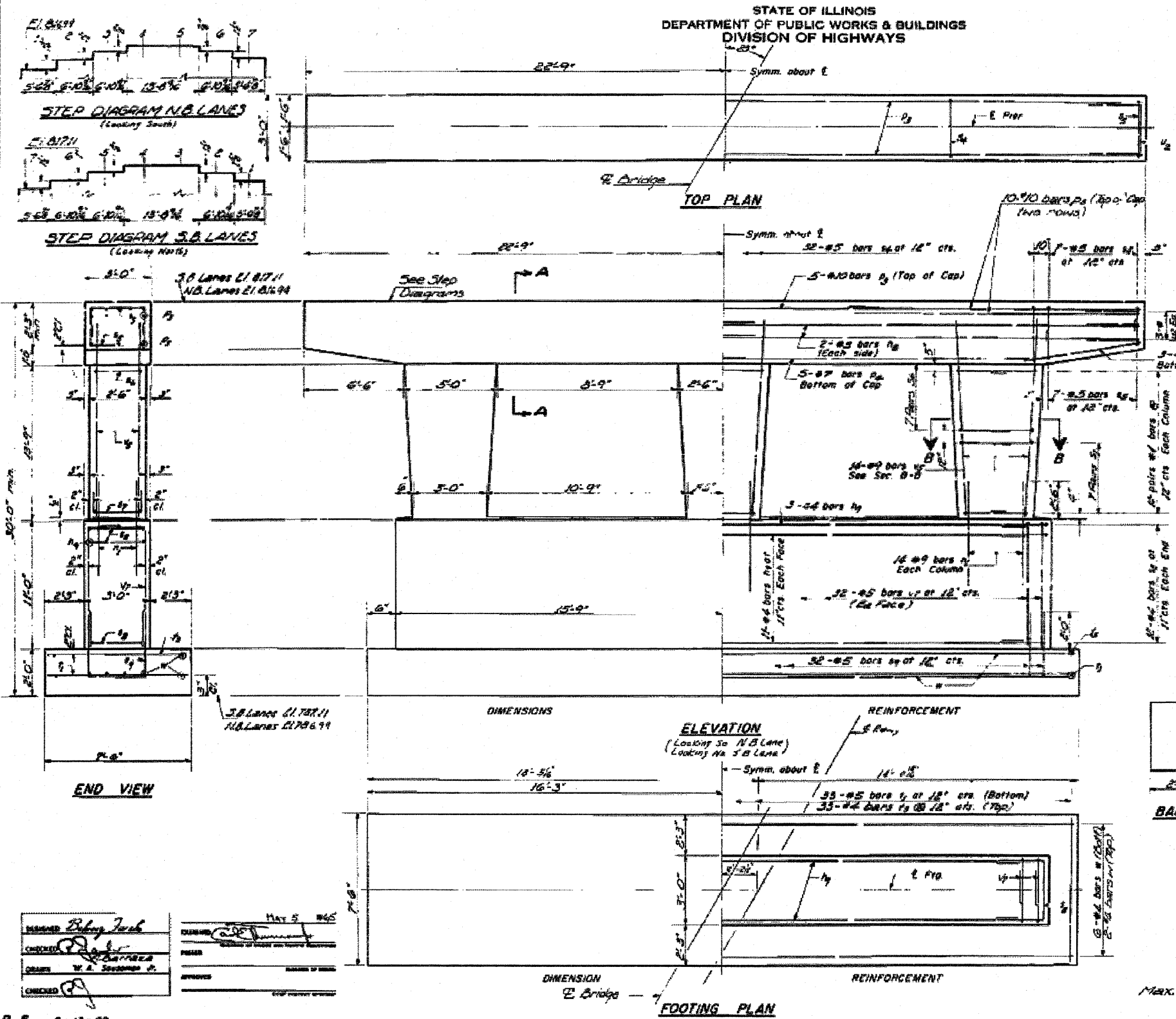
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**FA.I. RTE. 57 SEC. 27 NB**  
**FORD COUNTY**  
**3TA 289+70.56**

DESIGNED: *Robert J. ...*  
CHECKED: *...*  
DATE: May 5, 1965

P-5 1-2-62  
REV. N.H. 1-28-62

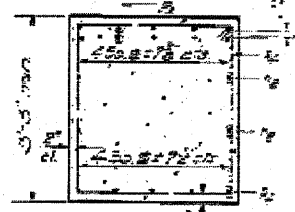
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

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|----------|---------|------|--------|
| DATE     | REVISED | BY   | REASON |
| 11-17-62 |         | W.A. |        |

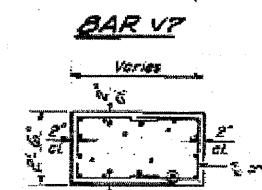


Note  
Space reinforcement in cap to  
miss anchor bolts.

Four steps monolithically with  
Cap.



SECTION A-A



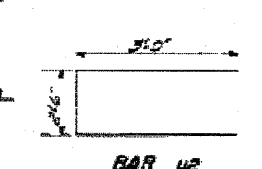
SECTION B-B

2 PIERS  
BILL OF MATERIAL

| Bar No. | Size | Length | Shape |
|---------|------|--------|-------|
| #1      | 10   | 45     | 21-2" |
| #2      | 50   | 46     | 27-6" |
| #3      | 60   | 49     | 5-6"  |
| #4      | 30   | 40     | 16-5" |
| #5      | 10   | 49     | 31-5" |
| #6      | 12   | 45     | 7-0"  |
| #7      | 64   | 45     | 18-0" |
| #8      | 56   | 48     | 6-8"  |
| #9      | 62   | 46     | 7-10" |
| #10     | 64   | 46     | 6-10" |
| #11     | 68   | 46     | 8-0"  |
| #12     | 64   | 45     | 10-8" |
| #13     | 66   | 48     | 7-2"  |
| #14     | 66   | 46     | 7-2"  |
| #15     | 12   | 46     | 8-6"  |
| #16     | 86   | 40     | 15-8" |
| #17     | 100  | 45     | 18-6" |
| #18     | 16   | 46     | 32-0" |

BARS & THURLS

| Bar | A     | B      |
|-----|-------|--------|
| #1  | 2'-0" | 2'-0"  |
| #2  | 2'-4" | 2'-10" |
| #3  | 2'-0" | 2'-10" |
| #4  | 2'-4" | 2'-10" |
| #5  | 2'-0" | 3'-0"  |
| #6  | 2'-0" | 3'-0"  |



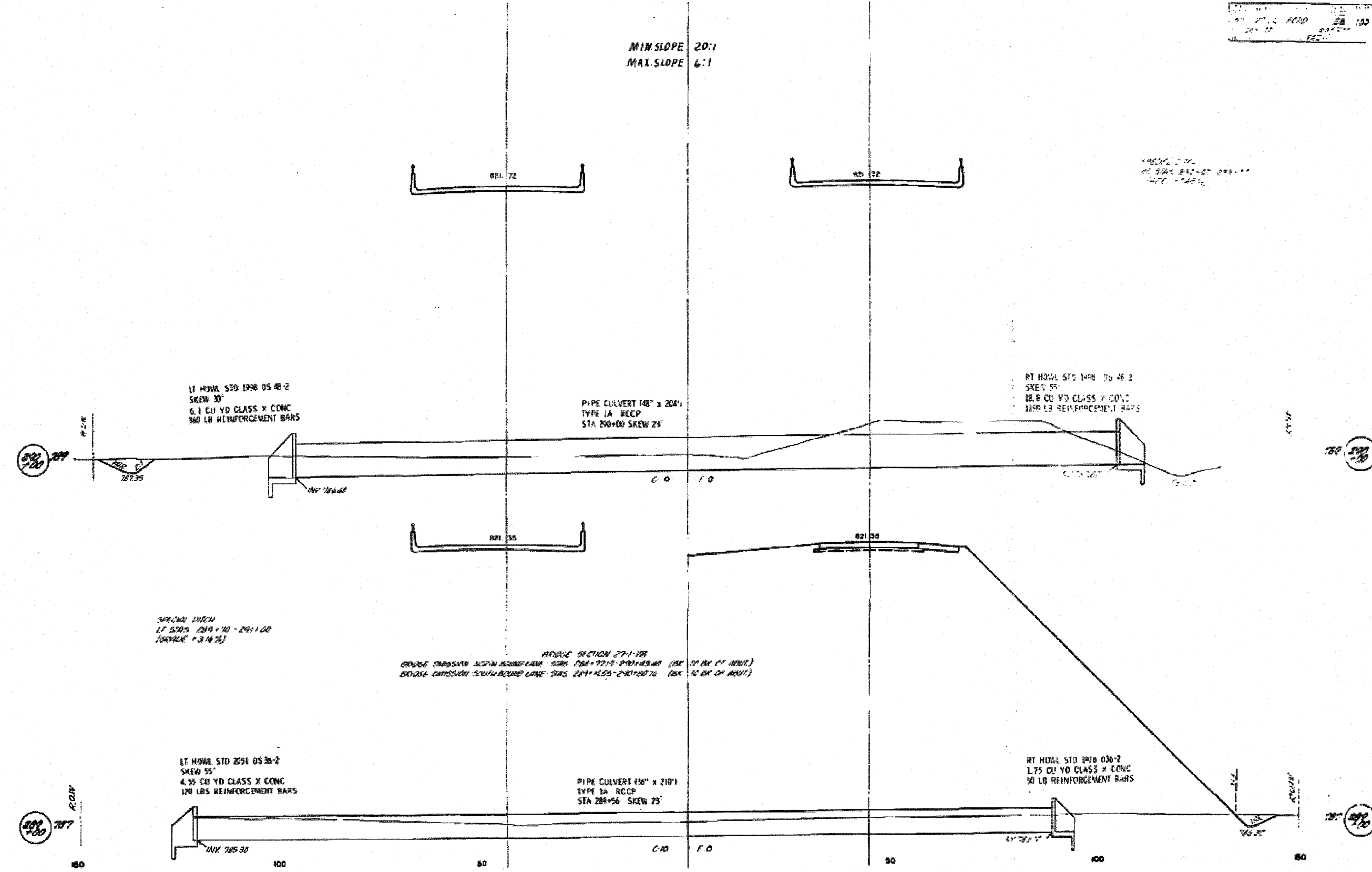
DESIGNED *Byron Jank*  
CHECKED *[Signature]*  
DRAWN *[Signature]*  
DATE *MAY 5 1962*

P-5 2-13-62  
Rev. H. H. J. 1-11-62

MAX. Footing Pressure = 3.2 Ts

PIER 2  
EAL RT 57 SEC. 27-1V8  
FORD COUNTY  
STA. 289+70.55

DATE: 08/26/09  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]



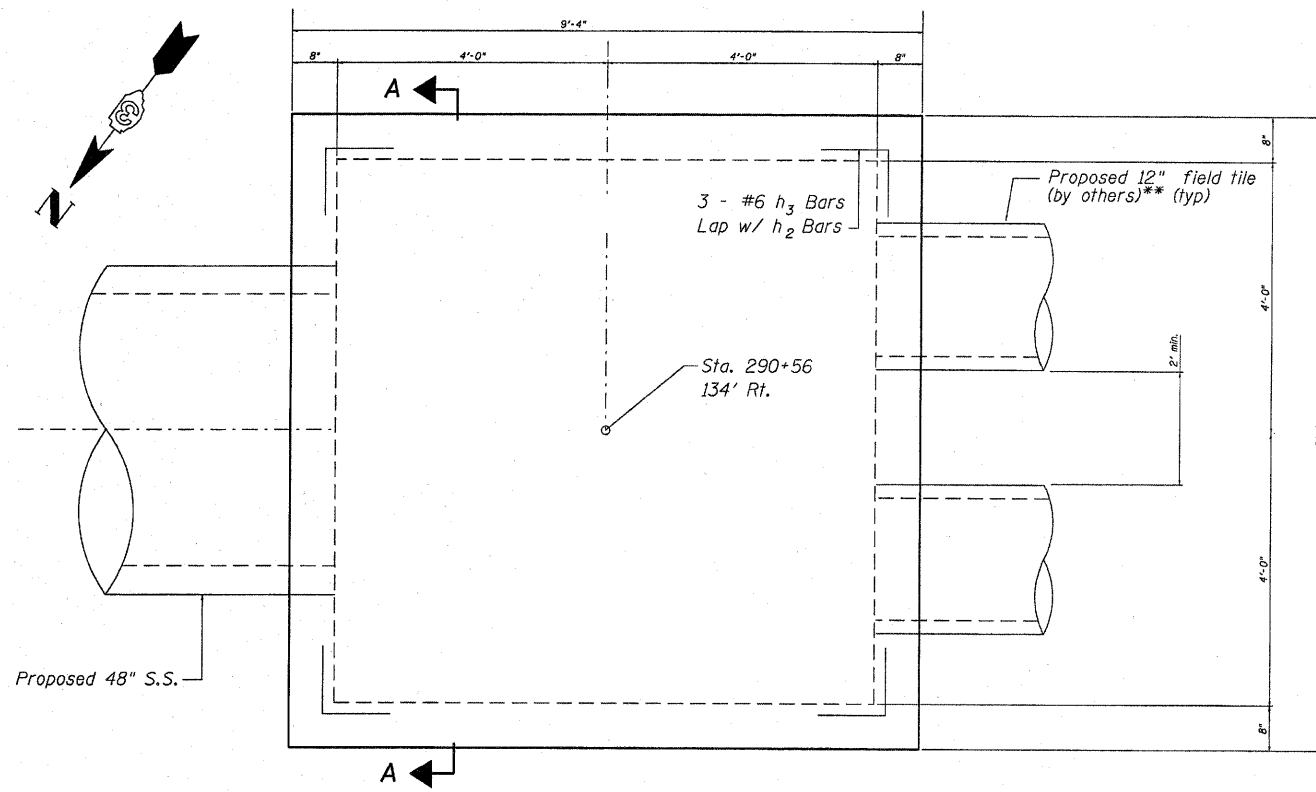
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| ca:\pwork\VP\DOT\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn |                      | DRAWN -    | REVISED - |
| PLOT SCALE = 50.2690' / IN.                              |                      | CHECKED -  | REVISED - |
| PLOT DATE = Aug 26, 2009 - 03:05:04 PM                   |                      | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING BRIDGE PLANS  
 FOR INFORMATION ONLY**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                      |        |                           |           |
|--------------------|----------------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION              | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 57                 | I27-DIVBR & VBR-11DM | FORD   | 28                        | 22        |
| CONTRACT NO. 66862 |                      |        | ILLINOIS FED. AID PROJECT |           |



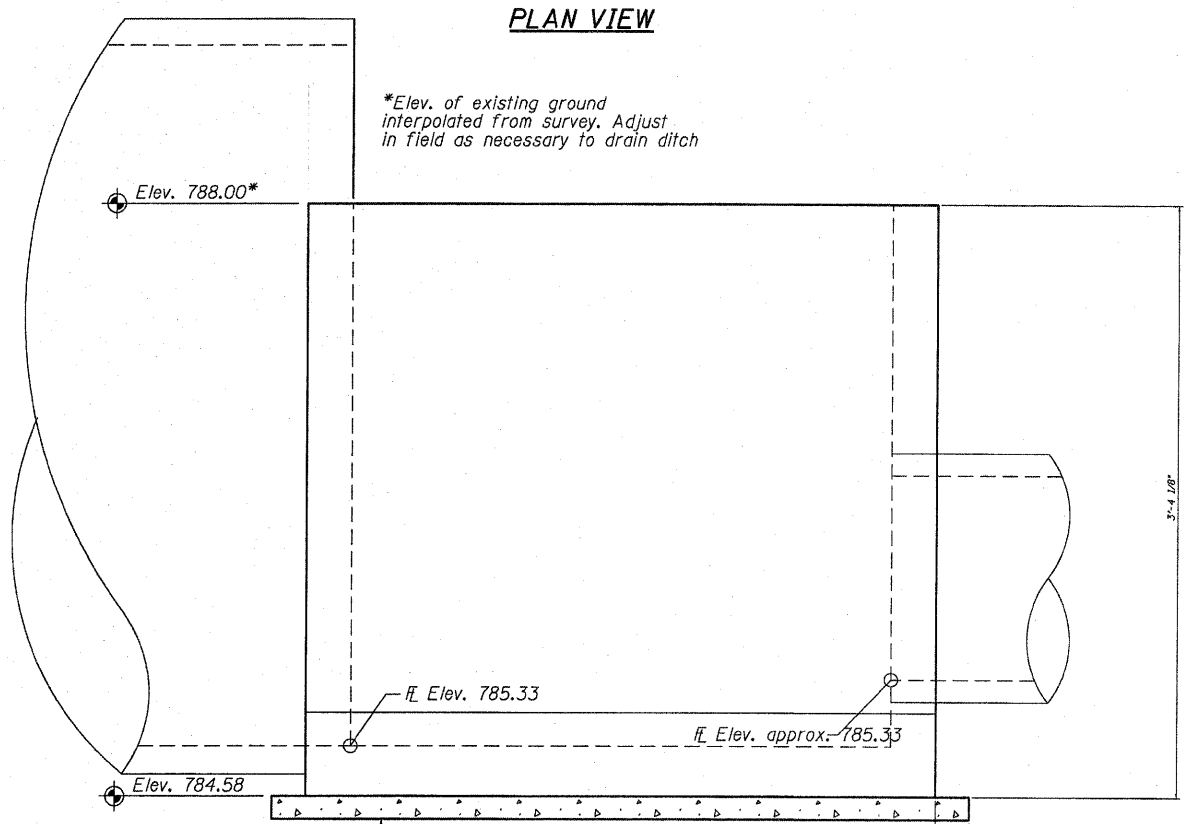
\*\*\*Field tiles to be placed by adjacent property owner during construction. Permit being applied for by land owner during design phase. The permit is anticipated to be approved prior to construction. See plan and profile sheet for approximate location.

Prop field tile location is shown here entering from the west. Property owner has expressed an interest to enter from the south side of the drainage structure. Contact owner prior to construction.

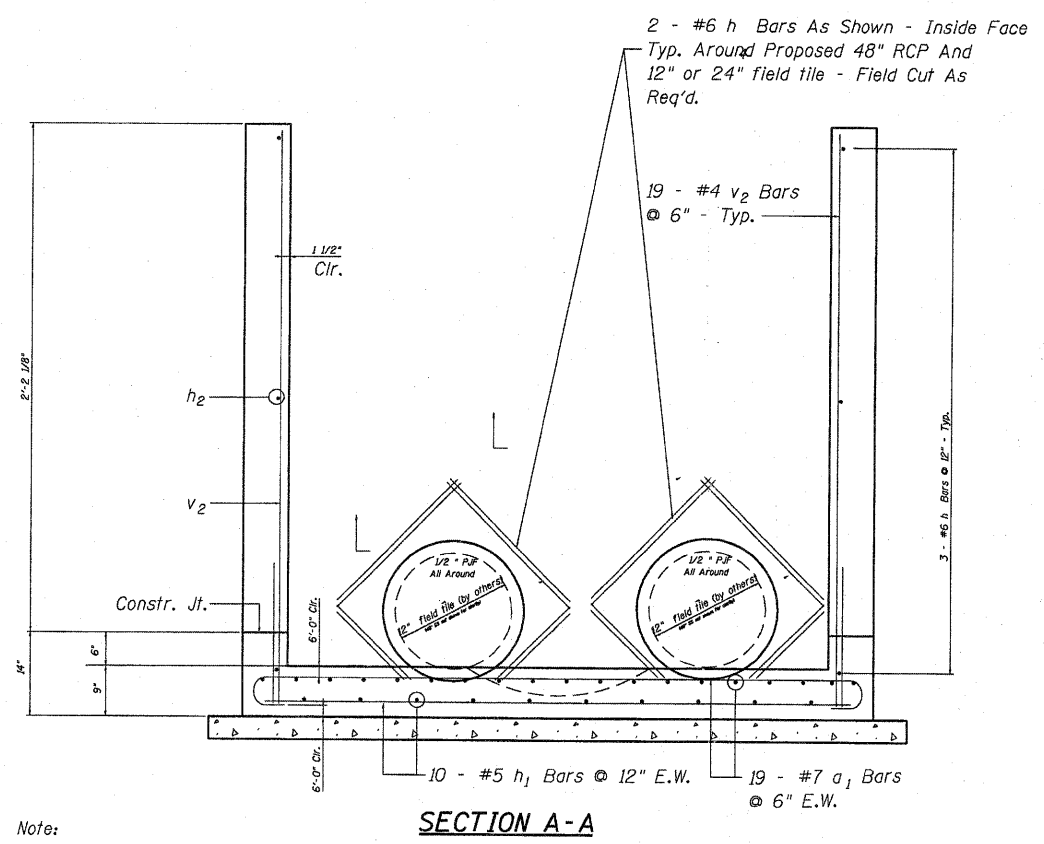
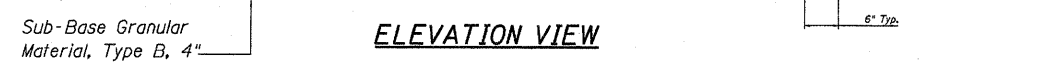
**BILL OF MATERIAL**

| Bar                                   | No. | Size | Length  | Shape |
|---------------------------------------|-----|------|---------|-------|
| a <sub>1</sub>                        | 38  | #7   | 10'-8"  |       |
| h <sub>1</sub>                        | 20  | #5   | 9'-1"   |       |
| h <sub>2</sub>                        | 12  | #6   | 9'-1"   |       |
| h <sub>3</sub>                        | 12  | #6   | 5'-0"   |       |
| h <sub>4</sub>                        | 16  | #6   | 5'-0"   |       |
| v <sub>2</sub>                        | 76  | #4   | 3'-1"   |       |
| Concrete Structures                   |     |      | Cu. Yd. | 2.85  |
| Reinforcement Bars                    |     |      | Pound   | 1548  |
| Sub-Base Granular Material, Type B 4" |     |      | Sq. Yd. | 11.9  |

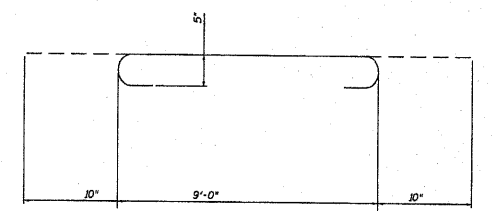
**PLAN VIEW**



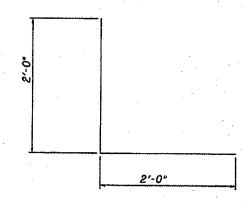
**ELEVATION VIEW**



Note: Field Cut h<sub>2</sub> Bars And v Bars As Req'd.



**BAR a<sub>1</sub>**



**BAR h<sub>3</sub>**

NOT TO SCALE

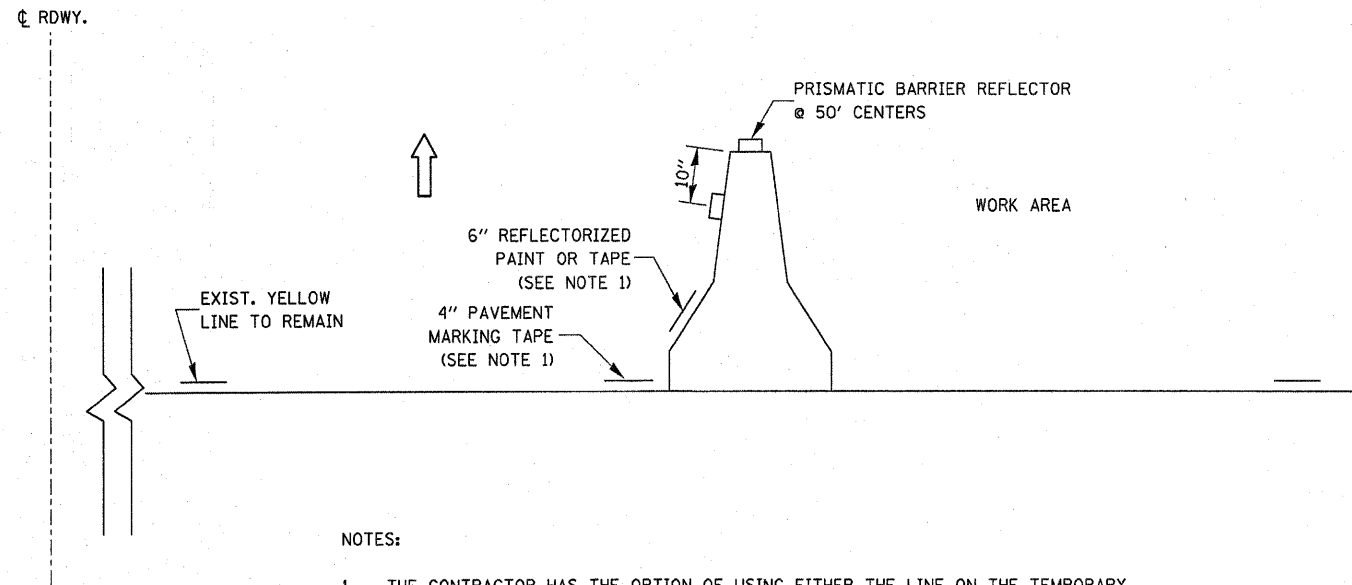
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| FILE NAME = 3/1-11   | USER NAME = braboygc | DESIGNED - | REVISED - |
| cs:\pwork\p\WIDOT\BRABOYPC\dms64344\0366862-SHT-PLNPRF.dgn | DRAWN -              | REVISED -  | REVISED - |
| PLOT SCALE = 50.2690' / IN.                                | CHECKED -            | REVISED -  | REVISED - |
| PLOT DATE = Aug 26, 2009 - 03:05:21 PM                     | DATE -               | REVISED -  | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE STRUCTURE DETAIL

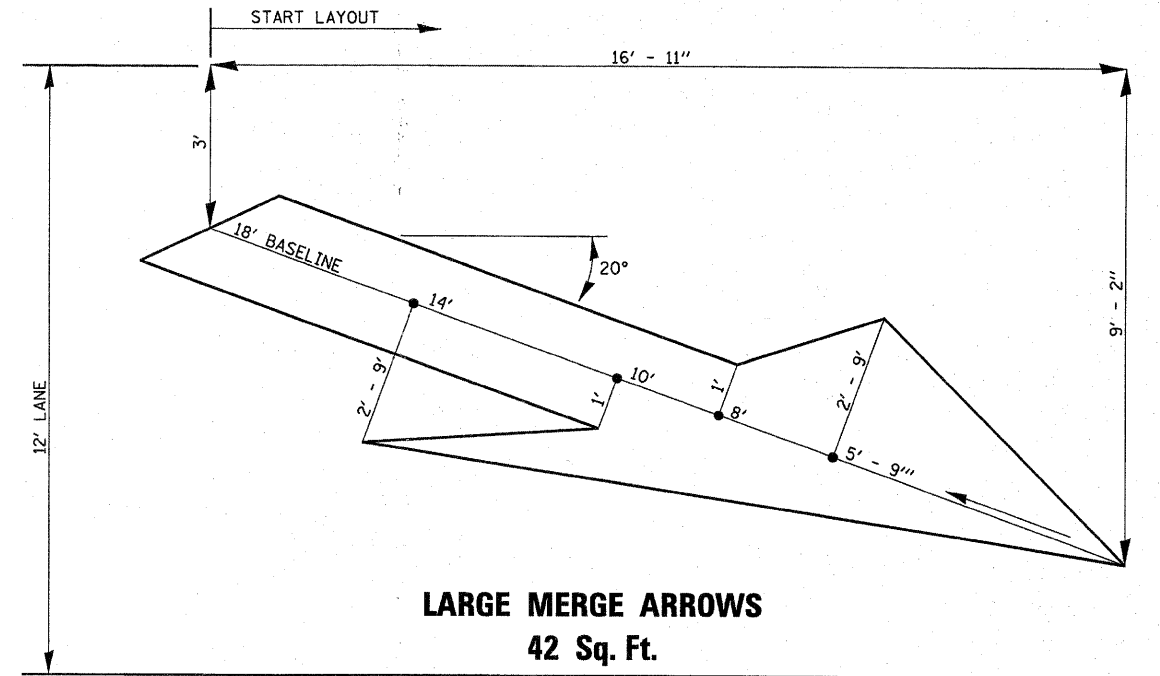
SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                    |                               |             |                 |                           |
|--------------------|-------------------------------|-------------|-----------------|---------------------------|
| F.A. RTE. 57       | SECTION [(27-1)VBR & VBR-1]DM | COUNTY FORD | TOTAL SHEETS 28 | SHEET NO. 23              |
| CONTRACT NO. 66862 |                               |             |                 | ILLINOIS FED. AID PROJECT |

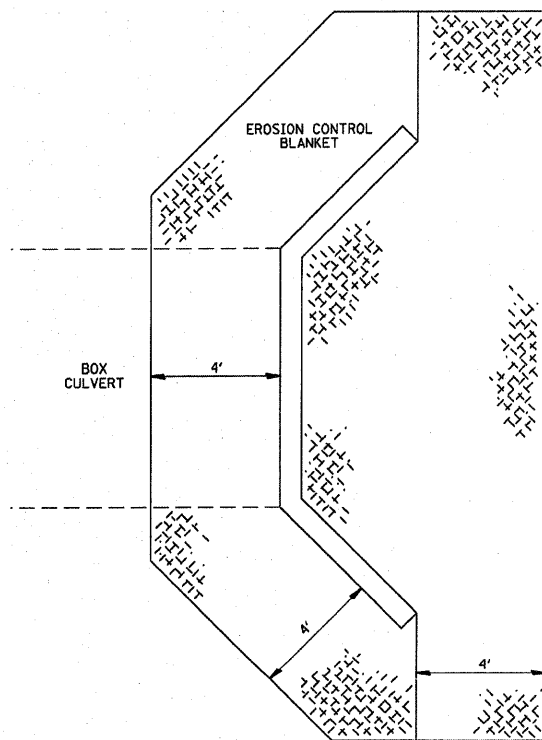


**NOTES:**

1. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE LINE ON THE TEMPORARY CONCRETE BARRIER OR ON THE PAVEMENT.
2. THE COLOR OF THE REFLECTORS AND PAVEMENT/BARRIER MARKING LINE WILL VARY WITH STAGING AND SHALL MATCH THE EXISTING LINE IN THE WORK AREA.
3. THE COST OF THE REFLECTORS AND THE PAVEMENT/BARRIER MARKING LINE IS INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER.

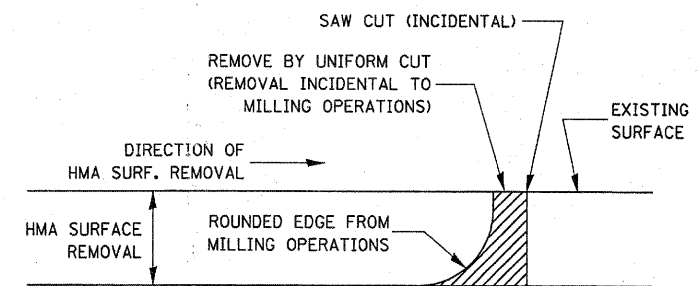


780-17



**EROSION CONTROL BLANKET  
AT BOX CULVERT END SECTIONS**

251-3



**NOTE:**

WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

**HMA DETAIL AT BUTT JOINTS**

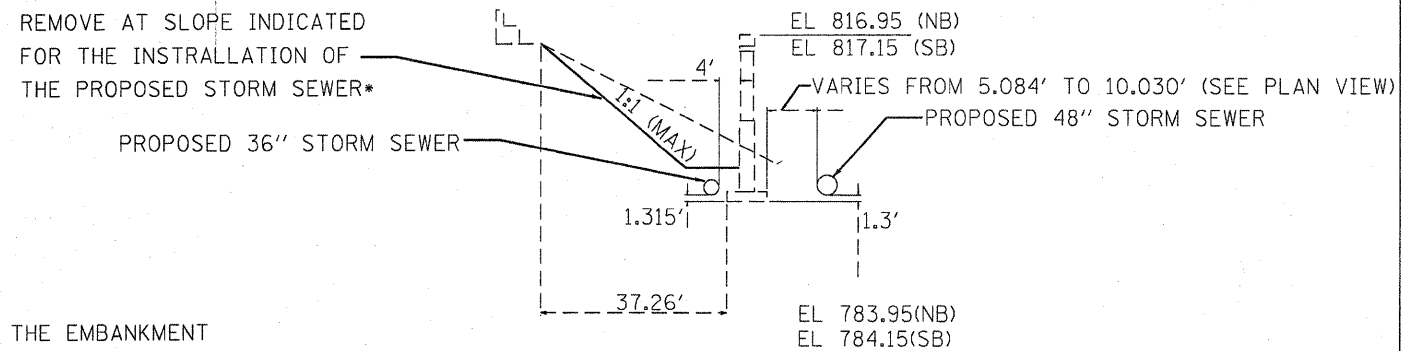
406-8

|  |                             |            |           |   |                |                           |                     |        |              |           |  |
|--|-----------------------------|------------|-----------|---|----------------|---------------------------|---------------------|--------|--------------|-----------|--|
| FILE NAME =  | USER NAME = braboypc        | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETAILS</b> | F.A. RTE.                 | SECTION             | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| cr:\pwork\pwidot\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn | PLOT SCALE = 50.2690' / IN. | DRAWN -    | REVISED - |   |                | 57                        | (27-1)VBR & VBR-1DM | FORD   | 28           | 24        |  |
| PLOT DATE = Aug 26, 2009 - 03:05:59 PM                   | DATE -                      | CHECKED -  | REVISED - |   |                | CONTRACT NO. 66862        |                     |        |              |           |  |
|  |                             |            |           |   |                | ILLINOIS FED. AID PROJECT |                     |        |              |           |  |



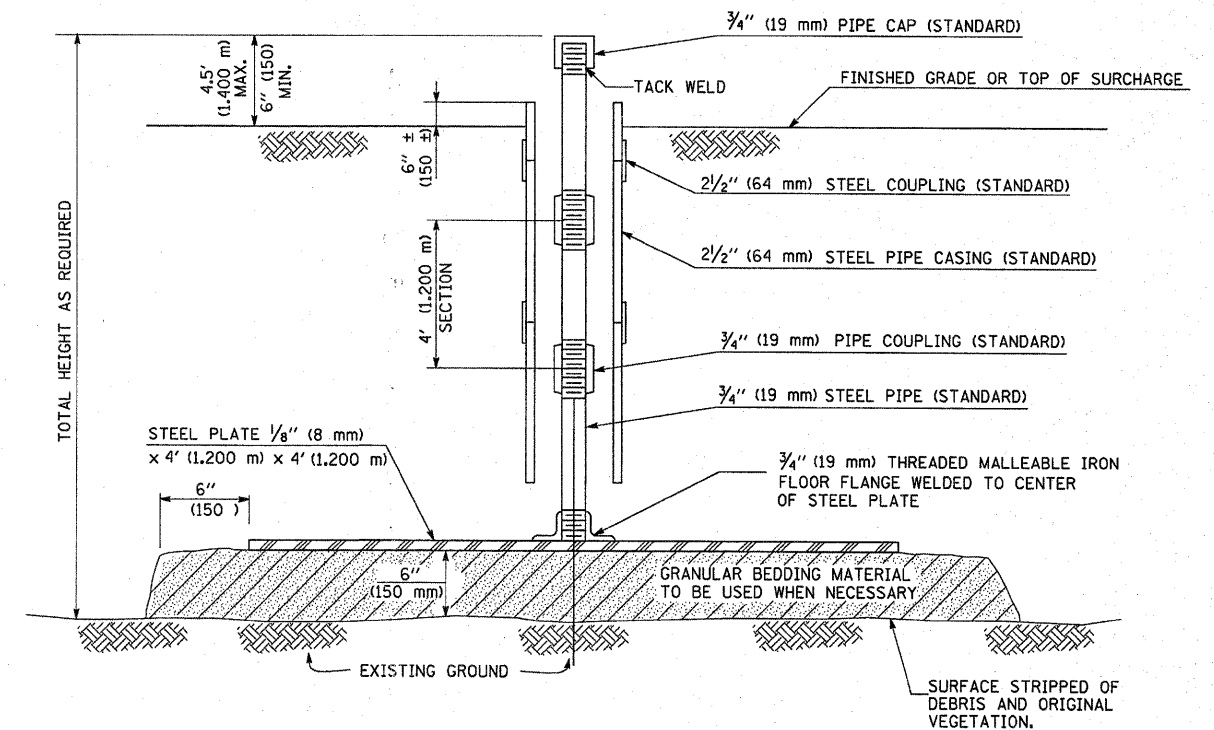
1. The installation of the pipe culverts shall be completed prior to construction for the embankment. Removal of the existing slopewall and embankment to facilitate pipe installation shall be no greater than a 1:1 slope to ensure stability of the existing abutments and piers
2. The Contractor shall take extra precaution not to over excavate and excavate below the existing footing elevation during excavation for pipe installation to ensure the stability of the existing piers.

### NORTH PIER



### DETAIL FOR THE INSTALLATION OF PROPOSED STORM SEWER

\*REMOVAL OF THE EMBANKMENT NECESSARY TO CONSTRUCT AS SHOWN SHALL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.



### SETTLEMENT PLATFORM DETAIL

1. SEE SECTION 204.06 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
2. TWO SETTLEMENT PLATFORMS SHALL BE CONSTRUCTED AT THE SITE OF THE ABANDONED RAILROAD STRUCTURES - ONE FOR STAGE I AND ONE FOR STAGE II CONSTRUCTION. THE COST IS INCLUDED IN THE COST OF FURNISHED EXCAVATION

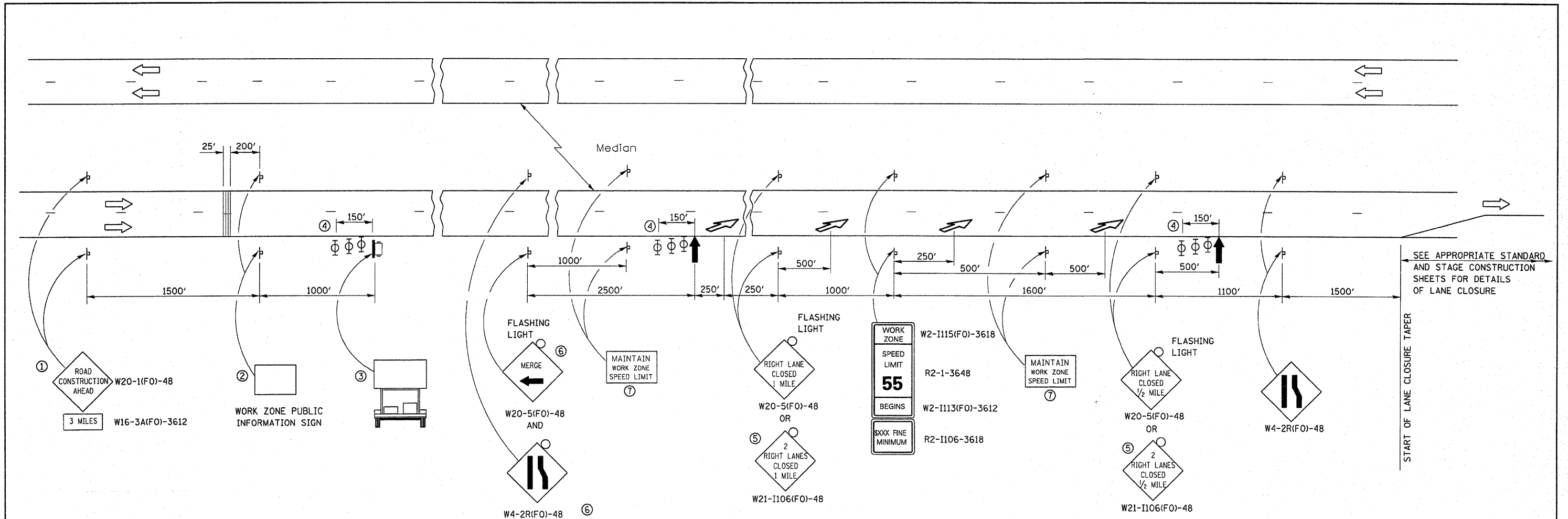
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| FILE NAME =   | USER NAME = braboypa | DESIGNED - | REVISIONS |
| ci:\pw\work\PIWID01\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn |                      | DRAWN -    | REVISIONS |
| PLOT SCALE = 58.2690' / IN.                                 |                      | CHECKED -  | REVISIONS |
| PLOT DATE = Aug 26, 2009 - 03:06:08 PM                      |                      | DATE -     | REVISIONS |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

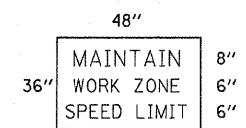
DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

|                           |                      |        |              |           |
|---------------------------|----------------------|--------|--------------|-----------|
| F.A. RTE.                 | SECTION              | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57                        | (27-1)VBR & VBR-110M | FORD   | 28           | 25        |
| CONTRACT NO. 66862        |                      |        |              |           |
| ILLINOIS FED. AID PROJECT |                      |        |              |           |



SEE APPROPRIATE STANDARD AND STAGE CONSTRUCTION SHEETS FOR DETAILS OF LANE CLOSURE



- ⑦ 48"x36" FLUORESCENT ORANGE SIGN WITH BLACK LETTERS.
- ↑ ARROW BOARD
- ☐ PORTABLE CHANGEABLE MESSAGE SIGN
- ⊥ SIGN
- ⊕ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH MONODIRECTIONAL FLASHING LIGHT
- ↘ LANE DROP ARROW - SEE STANDARD 780001
- ▨ TEMPORARY THERMOPLASTIC RUMBLE STRIPS

- ① THE ROAD CONSTRUCTION AHEAD SIGN SHALL BE LOCATED 3 MILES IN ADVANCE OF THE PROJECT LIMITS.
- ② THE MESSAGE AND SIZE OF THE WORK ZONE PUBLIC INFORMATION SIGN SHALL BE AS SPECIFIED BY THE DEPARTMENT.
- ③ TO BE PLACED IN THE MEDIAN WHEN FEASIBLE. THE MESSAGE BOARD SHALL BE USED TO DISPLAY STATUS OF LANES WITHIN THE PROJECT. THE PRIMARY MESSAGES SHALL BE:  
"RIGHT LANE CLOSED" / " x MILES AHEAD"  
"LEFT LANE CLOSED" / " x MILES AHEAD"  
"ALL LANES OPEN"
- ④ THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 50' CENTERS.
- ⑤ THIS SIGN SHALL BE USED WHEN 2 LANES ARE CLOSED.
- ⑥ WHEN THE LEFT LANE IS CLOSED, SWITCH THESE TWO SIGNS AND THE DIRECTION OF THE MERGE ARROW.

**GENERAL NOTE:**

THIS STANDARD IS USED WHERE AT ANY TIME A LANE IS CLOSED ON A FREEWAY/EXPRESSWAY.

WHEN THE LEFT LANE IS CLOSED, LEFT LANE CLOSED SIGNS SHALL BE SUBSTITUTED FOR THE RIGHT LANE CLOSED SIGNS.

THE FIRST TWO SIGNS AND THE MESSAGE BOARD ARE STATIONARY. THE OTHER SIGNS AND ARROWBOARDS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED DISTANCE FROM THE START OF THE LANE CLOSURE TAPER(S).

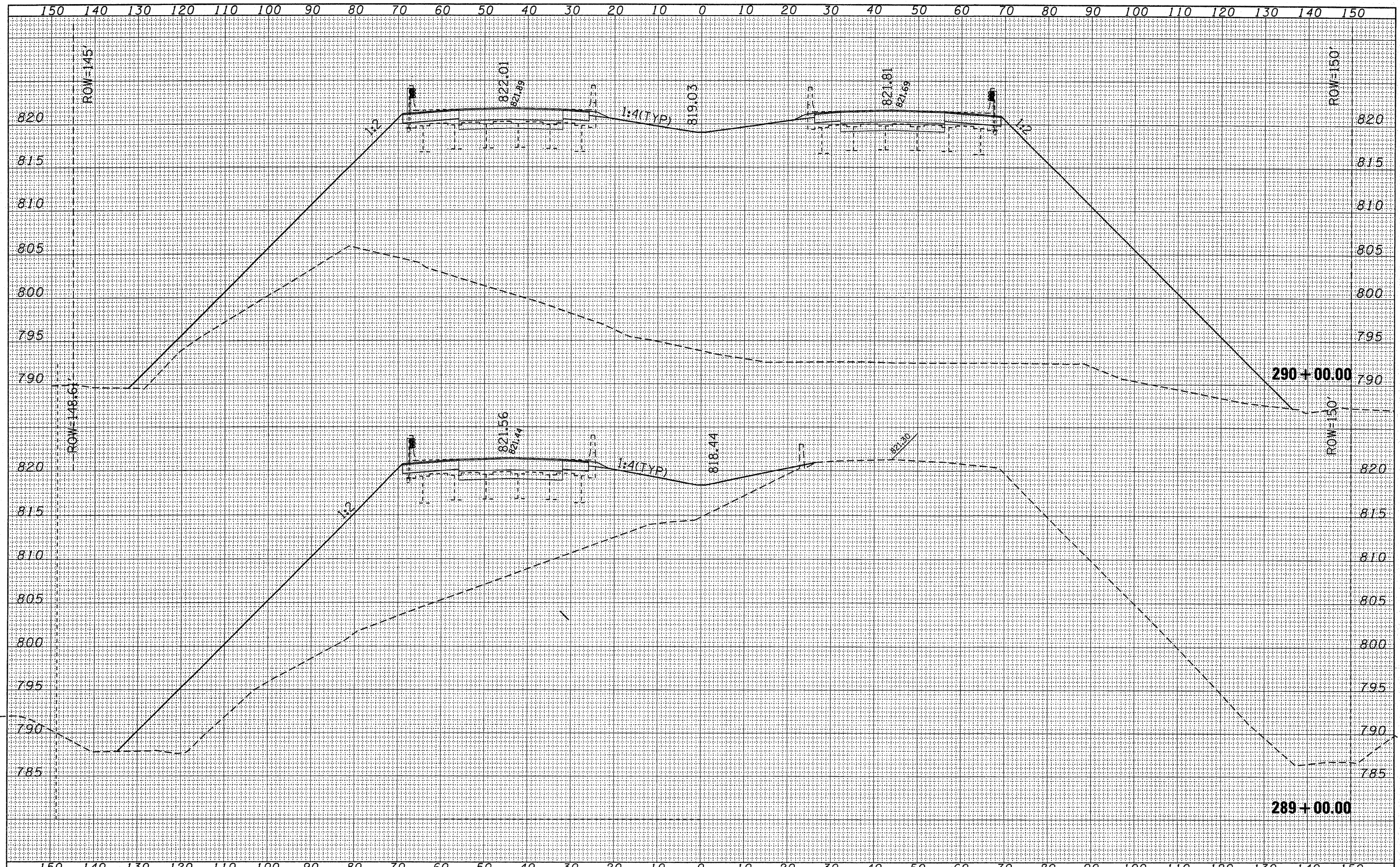
SEE SPECIAL PROVISIONS.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.

|  |                      |            |                           |   |                                  |                      |         |        |              |           |
|--|----------------------|------------|---------------------------|---|----------------------------------|----------------------|---------|--------|--------------|-----------|
| FILE NAME =  | USER NAME = braboypc | DESIGNED - | REVISED -                 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>STANDARD 701400 (SPECIAL)</b> | F.A. RTE.            | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| or:\pwork\PMIDOT\BRABOYPC\dms64344\036862-SHT-PLNPRF.dgn | DESIGNED -           | REVISED -  | 57                        |   |                                  | (27-1)VBR & VBR-1)DM | FORD    | 28     | 26           |           |
| PLOT SCALE = 50.26' = 1" / IN.                           | CHECKED -            | REVISED -  | CONTRACT NO. 66862        |   |                                  |                      |         |        |              |           |
| PLOT DATE = Aug 26, 2009 - 03:06:25 PM                   | DATE -               | REVISED -  | ILLINOIS FED. AID PROJECT |   |                                  |                      |         |        |              |           |

DATE \_\_\_\_\_  
 BY \_\_\_\_\_  
 SURVEYED \_\_\_\_\_  
 PLOTTED \_\_\_\_\_  
 TEMPLATE \_\_\_\_\_  
 AREAS CHECKED \_\_\_\_\_  
 ORIGINAL SURVEY NO. \_\_\_\_\_

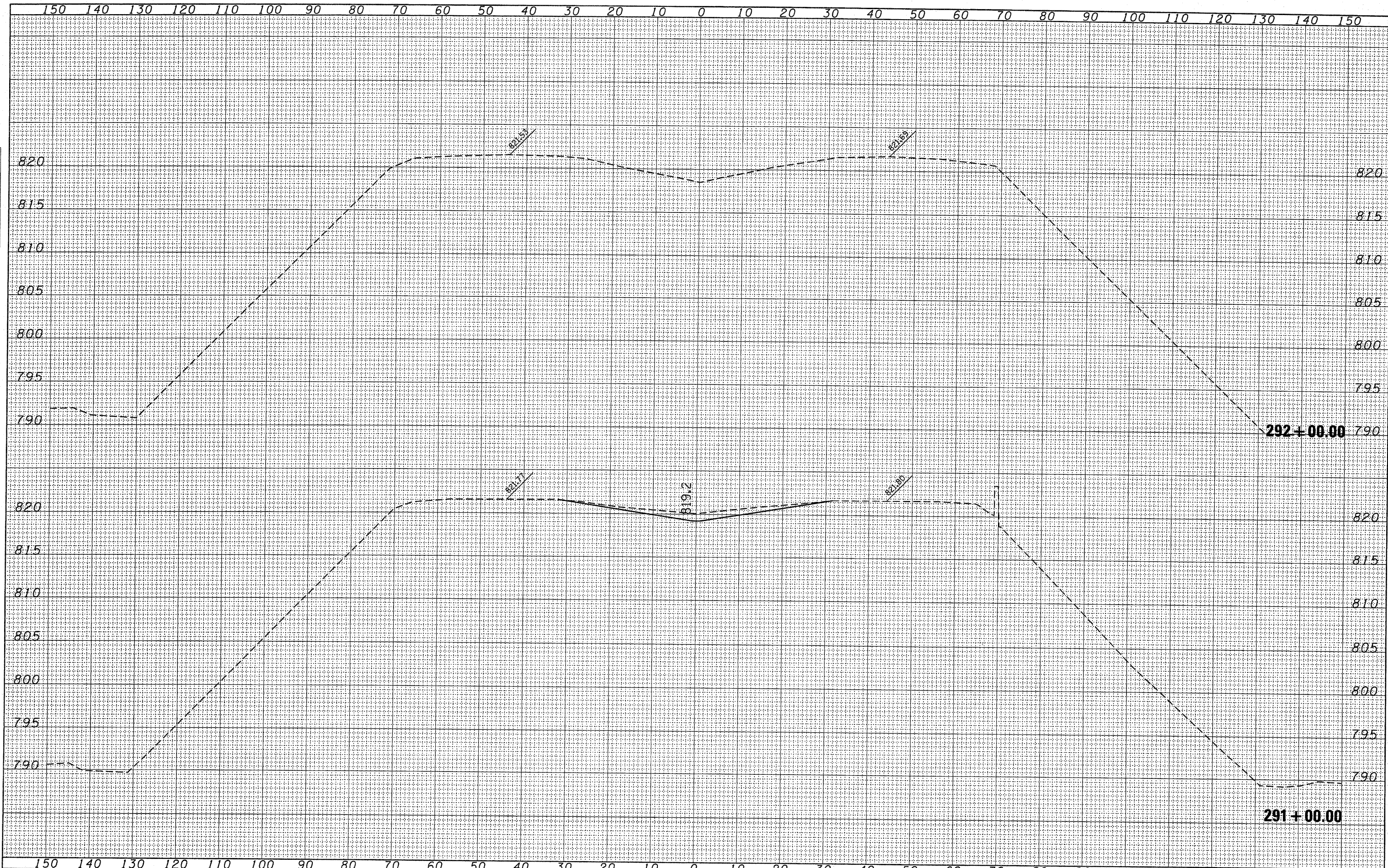
DATE \_\_\_\_\_  
 BY \_\_\_\_\_  
 SURVEYED \_\_\_\_\_  
 PLOTTED \_\_\_\_\_  
 TEMPLATE \_\_\_\_\_  
 AREAS CHECKED \_\_\_\_\_  
 ORIGINAL SURVEY NO. \_\_\_\_\_



|                                      |                 |                |            |   |   |                     |                                  |                    |                              |             |                 |              |
|--------------------------------------|-----------------|----------------|------------|---|---|---------------------|----------------------------------|--------------------|------------------------------|-------------|-----------------|--------------|
| FILE NAME =<br>D366862-SHT-XSSHT.DGN | USER NAME = --- | DESIGNED - JKC | REVISED -  | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CROSS SECTIONS</b>                           |                     |                                  | F.A.I. RTE. 57     | SECTION [27-1]VBR & VBR-13DM | COUNTY FORD | TOTAL SHEETS 28 | SHEET NO. 27 |
| PLOT SCALE = 1"=10'                  | CHECKED - JKC   | REVISIED -     | REVISIED - |   | SCALE: 1"=10'                                   | SHEET NO. OF SHEETS | STA. 289+00.00 TO STA. 290+00.00 | CONTRACT NO. 66862 |                              |             |                 |              |
| PLOT DATE = 11/20/08                 | DATE - 11/08    | REVISIED -     | REVISIED - |   | FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                     |                                  |                    |                              |             |                 |              |
|                                      |                 |                |            |   |   |                     |                                  |                    |                              |             |                 |              |

|                            |  |
|----------------------------|--|
| DATE                       |  |
| BY                         |  |
| SURVEYED                   |  |
| PLOTTED                    |  |
| TEMPLATE                   |  |
| AREAS CHECKED              |  |
| FINAL SURVEY NOTE BOOK NO. |  |

|                            |  |
|----------------------------|--|
| DATE                       |  |
| BY                         |  |
| SURVEYED                   |  |
| PLOTTED                    |  |
| TEMPLATE                   |  |
| AREAS CHECKED              |  |
| FINAL SURVEY NOTE BOOK NO. |  |



FILE NAME = D366862-SHT-XSSHT.DGN

USER NAME = ---  
 DESIGNED - JKC  
 DRAWN - LAG  
 CHECKED - JKC  
 DATE - 11/08

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

SCALE: 1"=10'    SHEET NO.    OF    SHEETS    STA. 291+00.00 TO STA. 292+00.00

| F.A.I. RTE.  | SECTION               | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|-----------------------|--------|--------------|-----------|
| 57   | [(27-1)VBR & VBR-1]DM | FORD   | 28           | 28        |
| CONTRACT NO. 66862                                 |                       |        |              |           |
| FED. ROAD DIST. NO.    [ILLINOIS] FED. AID PROJECT |                       |        |              |           |