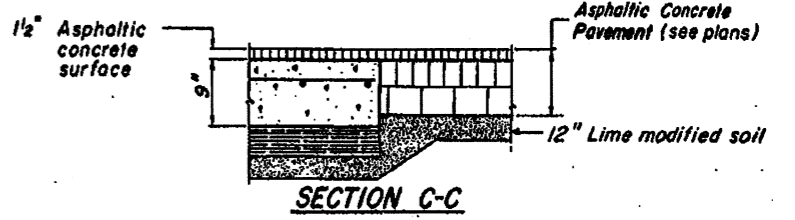
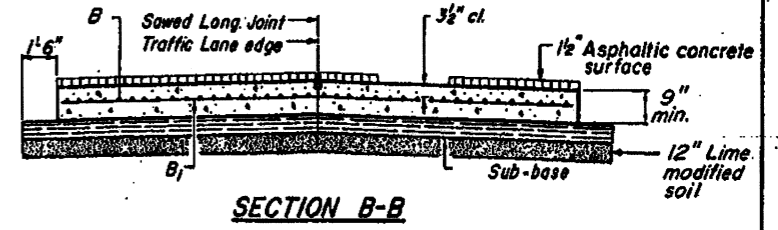


Note:
When the road plans show curb and gutter or bridge approach shoulder pavement adjacent to approach slabs, the tie bars shall be as specified on the appropriate standard. The cost of these tie bars will be included in the contract unit price for the adjacent item.
Transitions for curb and gutter shall be as shown on the plans.



GENERAL NOTES

- With the approval of the Engineer the contractor will be permitted to reduce the paving widths by substituting a Keyed Longitudinal Construction Joint with tie bars in lieu of the Specified Sawed Longitudinal Joint.
- The Contractor at his option may place the sub-base monolithic with the bridge approach pavement. When this option is used, the sub-base may be constructed to the same width as the pavement, and the reinforcement shall be in accordance to the total pavement and sub-base thickness.
- The sub-base shall be of the same material and thickness as under adjacent pavement. When sub-base is not required under adjacent pavement, the sub-base shall be either 6" granular or 4" stabilized material.
- The asphaltic surface shall be omitted from the bridge approach pavement when the adjacent structure has no asphaltic surface.
- Pavement joints shall be as detailed on Standard 2323.
- The cost of tie bars, sub-base and bituminous prime shall be included in the cost of Bridge Approach Pavement.

Illinois Department of Transportation

APPROVED Sept. 14, 1989
John J. ...
 Engineer of Bridges and Structures

APPROVED Sept. 14, 1989
...
 Engineer of Design

ISSUED 1-2-75

BRIDGE APPROACH PAVEMENT
 FOR USE WITH ASPHALTIC CONCRETE PAVEMENT

(Sheet 1 of 2)
 STANDARD 2360-4

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