

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A. REG.	WINNEBAGO	X	1
ILLINOIS PROJECT			

85488

WEST RIVERSIDE BOULEVARD (F.A.P.-525)
(N. MAIN ST/IL RT 2 TO ROCK RIVER)
CITY OF ROCKFORD
WINNEBAGO COUNTY
SECTION 09-00560-00-RS
JOB C-92-173-09
PROJECT ARA-5099 (087)
CONTRACT NO. 85488

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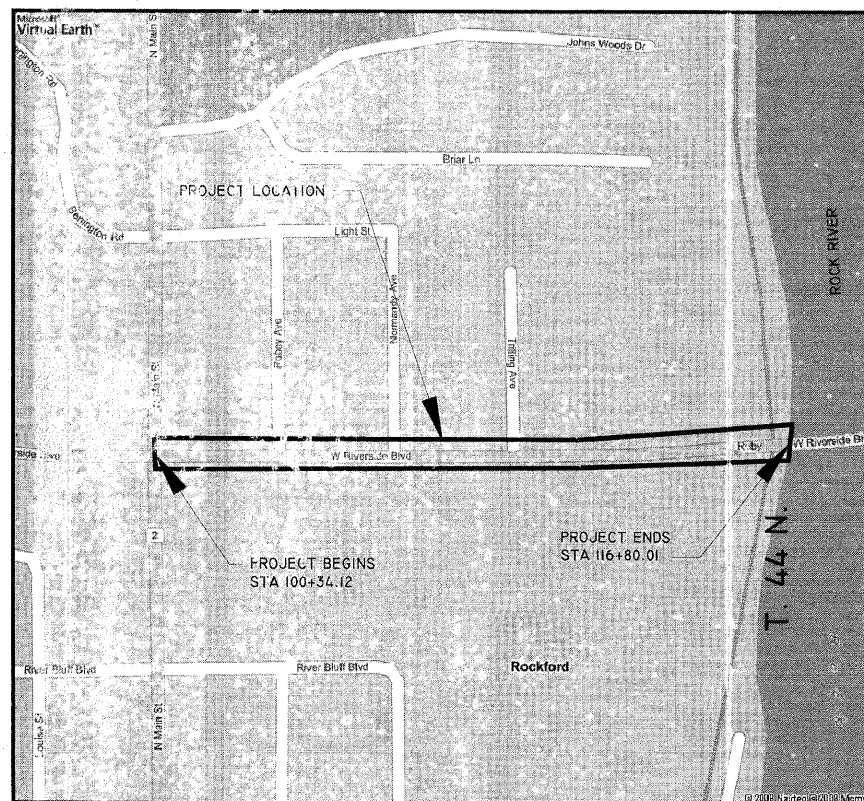
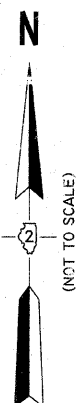
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DTL-DRV	DRIVEWAY DETAIL
DTL-DRV5	DRIVEWAY DETAIL (5.0' FLARES)
DTL-ADARAMP09	ADA CURB RAMP DETAIL FOR INTERSECTIONS
	EROSION CONTROL DETAIL FOR INLETS

SCALES:

SWWP PLAN:	1" = 30'
ROADWAY PLAN:	1" = 20'
STRIPING PLAN:	1" = 20'

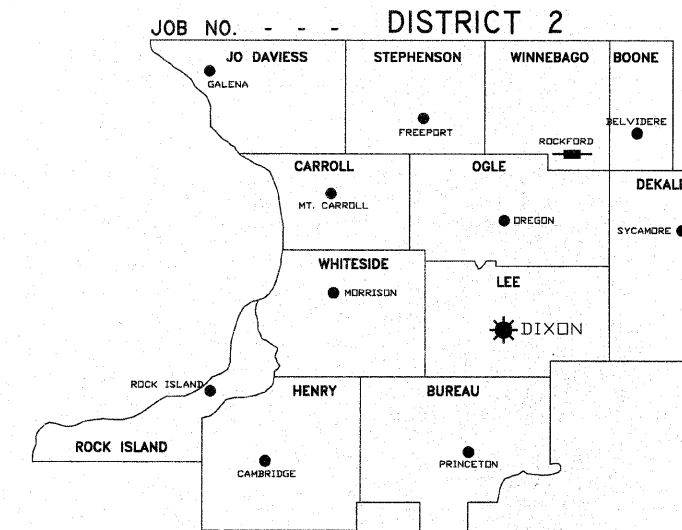


R. I E. 3RD P.M.

ROCKFORD TOWNSHIP, SECTION 1
WINNEBAGO COUNTY

TOTAL PROJECT LENGTH = 1645.89 FEET = 0.31 MILES

ADT = 26,800 (ADT) 5% TRUCKS
PRINCIPAL ARTERIAL
LAPP POLICY



★ DISTRICT HEADQUARTERS
■ PROJECT LOCATION

AGENCY RESPONSIBLE FOR LETTING	
APPROVED	<i>Jon Hollander, P.E.</i> CITY ENGINEER Local Agency, Position
PASSED	<i>Sept. 14, 2009</i> <i>Jim L. Johnson</i> District 2 Engineer of Local Roads & Streets
RELEASING FOR BID BASED ON LIMITED REVIEW	<i>Sept. 14, 2009</i> <i>George F. Ryan</i> Deputy Director of Highways, Region 2 Engineer



I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Illinois.

Signature: *Patrick D. Stewart*
PATRICK D. STEWART, P.E.

Date: *9-03-09* Reg No.: 062-046612
(Month, Day, Year)

My registration renewal date is November 30, 2009.

"PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS"

SENIOR SQUAD LEADER
PROJECT ENGINEER BRADLEY S. LANIGE (815) 398-2332
CONSULTANT: MCCLURE ENGINEERING ASSOCIATES, INC.

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE:	N/A
DRAWN BY:	TWH
CHECKED BY:	POS
DATE:	08/28/2009

McClure
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(815) 398-3332 FAX (815) 398-2495
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COVER	
RIVERSIDE ARRA	SECTION 09-00560-00-RS
WEST RIVERSIDE BOULEVARD	ROCKFORD, ILLINOIS
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 COVER.DWG	JOB: 04-28-09-029

SHEET NO.	1
OF	21

GENERAL

IN THE FOLLOWING, THE ILLINOIS DEPARTMENT OF TRANSPORTATION WILL BE REFERRED TO AS IDOT.

THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PREPARED BY IDOT, ADOPTED JANUARY 1, 2007, AND THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" PREPARED BY IDOT, ADOPTED JANUARY 1, 2009. ALL SEWER AND WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", FIFTH EDITION. SIGN CONSTRUCTION AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.

GENERAL SAFETY PROVISION: TO PROVIDE DRIVERS WITH SAFE TRAVEL CONDITIONS DURING THE CONSTRUCTION PROJECT, AND TO PROVIDE SAFE WORKING CONDITIONS FOR ALL EMPLOYEES, THE RULES, REGULATIONS, AND CONDITIONS STATED BELOW WILL PREVAIL FOR THE DURATION OF THIS CONTRACT. ANY EMPLOYEE OF THE CONTRACTOR OR HIS SUBCONTRACTORS WHO REFUSES TO COMPLY WITH THESE GENERAL SAFETY PROVISIONS SHALL BE REMOVED FROM THE JOB SITE IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS. THE CONTRACTOR AND ANY SUBCONTRACTORS RETAINED BY HIM SHALL COMPLY WITH THE STATE AND FEDERAL REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 (OSHA), AS IT RELATES TO HIS OPERATIONS, REVISED AS OF JULY 1, 1987.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. HE WILL NOT BE ALLOWED TO BUILD FIRES ON THE SITE.

WHEN ARTIFICIAL LIGHTING IS UTILIZED DURING NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC, AS WELL AS ADJOINING RESIDENTIAL AREAS.

THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT REDUCED SIZE PLANS. DO NOT SCALE REDUCED SIZE PLANS FOR CONSTRUCTION DIMENSIONS.

THE IDOT STANDARD DRAWINGS LISTED ON THE COVER SHEET ARE INTENDED TO BE THE LATEST REVISIONS AND SHALL TAKE PRECEDENCE OVER EARLIER DRAWINGS THAT MAY BE REFERRED TO ELSEWHERE IN THE PLANS OR SPECIAL PROVISIONS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES FOR THE ITEMS BEING CONSTRUCTED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR IS RESPONSIBLE TO PROTECT ALL INLETS AND MANHOLES FROM ADJACENT WORK, AND SHALL RESTORE ALL DAMAGED STRUCTURES TO ACCEPTABLE CONDITIONS.

THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING HIS CONSTRUCTION OPERATIONS WITH OTHER CONSTRUCTION, UTILITY, AND/OR MAINTENANCE OPERATIONS WITHIN OR ADJACENT TO THE PROJECT.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR DISTURBED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS, MONUMENTS AND RIGHT-OF-WAY PINS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DISTURBED OR DESTROYED BY HIS OPERATIONS. REPLACEMENT OF MONUMENTS WILL BE DETERMINED BY THE ENGINEER. PROPERTY MARKERS DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR AT NO ADDITIONAL EXPENSE TO THE OWNER.

THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING KNOWN PROPERTY LINES, OR SURVEY MARKERS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL PLAN QUANTITIES.

THE CONTRACTOR SHALL REMOVE, STORE, AND RELOCATE TO THE SATISFACTION OF THE ENGINEER ALL EXISTING STREET NAME SIGNS WHICH ARE TO BE RELOCATED IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS. THE PLACEMENT OF ADDITIONAL SIGNING SHALL BE THE RESPONSIBILITY OF THE CITY OF ROCKFORD, OR THEIR APPROVED REPRESENTATIVES.

ANY PRIVATE OR COMMERCIAL SIGNS WHICH NEED TO BE REMOVED FOR CONSTRUCTION PURPOSES SHALL BE HANDLED AS FOLLOWS:

WITHIN THE EXISTING RIGHT-OF-WAY: SIGNS SHALL BE REMOVED OR REMOVED AND REPLACED AS DIRECTED BY THE PLANS. PAYMENT FOR ALL MATERIALS AND LABOR REQUIRED FOR THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

OUTSIDE THE EXISTING RIGHT-OF-WAY: THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATION NEAR ANY AND ALL EXISTING SIGNS OUTSIDE THE RIGHT-OF-WAY. ANY DAMAGE DONE TO EXISTING SIGNS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL EXPENSE TO THE OWNER.

ALL ITEMS SHALL INCLUDE ALL THE NECESSARY MATERIALS AND LABOR TO COMPLETE THE ITEM IN PLACE; MATERIALS AND LABOR NOT SPECIFICALLY IDENTIFIED SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF THE ITEM BEING CONSTRUCTED.

ALL MATERIALS SHALL MEET OR EXCEED IDOT STANDARDS.

THE CONTRACTOR SHALL NOTIFY THE CITY OF ROCKFORD, PROPERTY OWNERS, THE ENGINEER, AND ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION OR CHANGE IN CLOSURES OR DETOURS.

ALL RADII AND DIMENSIONS ARE REFERENCED TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

NO WORK SHALL BE PERFORMED BEYOND THE PROJECT LIMITS WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE PROPERTY OWNER AND ENGINEER.

CONSTRUCTION STAKING AND INSPECTION SHALL BE THE RESPONSIBILITY OF THE OWNER. CONSTRUCTION STAKES SHALL BE REQUESTED A MINIMUM OF 48 HOURS IN ADVANCE. ANY RESTAKING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC CONTROL

ALL CONSTRUCTION WORK ON WEST RIVERSIDE BOULEVARD SHALL BE COMPLETED UNDER TRAFFIC WITH A MINIMUM OF ONE LANE AVAILABLE FOR EACH DIRECTION OF FLOW. TAPERING AND MERGING OF TRAFFIC ON THE BRIDGE WILL NOT BE ALLOWED.

THE CONTRACTOR SHALL INSTITUTE TRAFFIC CONTROL MEASURES AT ALL TIMES, IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNER FOR SHORT-TERM CLOSURES. A COPY OF SAID ARRANGEMENT MUST BE GIVEN TO THE ENGINEER.

THE CONTRACTOR IS ADVISED THAT IN THE EVENT OF SNOW, HE WILL BE HELD RESPONSIBLE FOR THE IMMEDIATE REMOVAL OF ANY TRAFFIC CONTROL AND PROTECTION/MAINTENANCE OF TRAFFIC DEVICES REQUIRED FOR HIS OPERATIONS THAT WOULD INTERFERE WITH SNOW REMOVAL OPERATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR THE TRAFFIC AS DIRECTED BY THE ENGINEER. ANY DROP-OFF GREATER THAN 3 INCHES ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH BARRICADES, AND SHALL BE INCLUDED IN THE UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION.

ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT. ALL SIGNS SHALL BE FURNISHED, INSTALLED AND MAINTAINED BY THE CONTRACTOR. PAYMENT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION.

TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN CONDITIONS MAY REQUIRE THE ENGINEER TO MODIFY THE LOCATION OF THE TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL SUPPLY THE ENGINEER WITH A PRIMARY AND ALTERNATE CONTACT PERSON WITH 24-HOUR, 7-DAY ACCESS WHO CAN REMEDY DEFICIENT OR EMERGENCY TRAFFIC CONTROL MEASURES. THE CONTRACTOR SHALL INSPECT THE TRAFFIC CONTROL DEVICES NOT LESS THAN TWICE DAILY TO ENSURE THAT ALL BARRELS, BARRICADES, AND WARNING DEVICES ARE IN THEIR PROPER PLACE AND OPERATIONAL.

DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT ADJACENT TRAFFIC LANES OPEN TO TRAFFIC FROM DEBRIS BEING BLOWN OR OTHERWISE REMOVED FROM THE CONSTRUCTION AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR KEEPING DEBRIS OFF THE ADJACENT TRAVELED LANE SURFACE.

REMOVAL

EXISTING STRUCTURES (INCLUDING FOUNDATIONS, WALLS, CISTERNS, OR OTHER UNDERGROUND STRUCTURES) WITHIN THE RIGHT OF WAY SHALL BE REMOVED IN ACCORDANCE WITH SECTION 501 OF THE STANDARD SPECIFICATIONS. WITHOUT ADDITIONAL COMPENSATION, UNLESS OTHERWISE NOTED IN THE PLANS OR SPECIAL PROVISIONS.

UNDERGROUND TANKS OR SEPTIC TANKS WITHIN THE RIGHT-OF-WAY WHICH HAVE NOT BEEN REMOVED AND WILL NOT INTERFERE WITH CONSTRUCTION SHALL BE FILLED WITH FREE-FLOWING SAND AT THE DIRECTION OF THE ENGINEER. THE COST OF THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 109 OF THE STANDARD SPECIFICATIONS.

ALL ROADWAY REMOVAL ITEMS SHALL CONFORM TO SECTION 440 OF THE IDOT STANDARD SPECIFICATIONS. ALL JOINTS BETWEEN THE PORTION REMOVED AND THAT LEFT IN PLACE SHALL BE SAWCUT TO SUCH A DEPTH THAT A CLEAN, NEAT EDGE WILL RESULT WITH NO SPALLING TO THE REMAINING PORTION. THE COST OF SAWCUTTING SHALL BE INCLUDED IN THE UNIT PRICE OF THE ITEM BEING REMOVED. ADDITIONAL SAWCUTTING MAY BE REQUIRED AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST TO THE OWNER.

EXCESS EXCAVATED MATERIAL, PAVEMENT REMOVAL, AND ALL DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE PROPERLY DISPOSED OF OFF-SITE.

EXCAVATION/EARTHWORK

THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH ARE NOT INDICATED TO BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.

CLEARING SHALL BE DONE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS AND SHALL NOT BE MEASURED FOR PAYMENT.

TOPSOIL/SEEDING

DISTURBED AREAS ARE LOCATIONS WHERE THE CONTRACTOR'S OPERATIONS HAVE DAMAGED EXISTING GROUND COVER AND/OR TOPSOIL OUTSIDE OF THE LIMITS OF CONSTRUCTION AS SHOWN IN THE PLANS. SEEDING OF THESE DISTURBED AREAS IS INCLUDED IN ALL THE OTHER PROJECT PAY ITEMS AND NO ADDITIONAL COMPENSATION IS ALLOWED.

UTILITIES

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION OR HAVE THE POTENTIAL FOR CREATING FUTURE PROBLEMS SHALL BE REMOVED AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY AT AN APPROVED LOCATION OBTAINED BY THE CONTRACTOR, ACCORDING TO ARTICLE 202.03 OF THE IDOT STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT BID PRICE FOR EARTH EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

UTILITIES SHOWN ON THE PLANS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND NO GUARANTEE OF THEIR ACCURACY IS MADE OR INFERRED. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ON THE DRAWINGS REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL-INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATION INTO THE LOCATION, SIZE, DEPTH, NUMBER AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER. THIS WORK SHALL BE AT THE CONTRACTORS EXPENSE.

THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. UTILITIES WHICH ARE NOT MEMBERS OF J.U.L.I.E. SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR.

ALL EXISTING CONDUITS WHICH WILL BE ABANDONED AND LEFT IN PLACE SHALL BE PLUGGED AT ALL OPEN ENDS WITH CONCRETE OR OTHER APPROVED MEANS. THE COST WILL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT BID PRICE FOR THE ITEM BEING REMOVED OR ABANDONED.

IF DURING MANHOLE ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME OR LID, HE SHALL FURNISH A NEW FRAME AND LID, NEENAH R-1975 OR EQUAL, AT NO ADDITIONAL COST. ALL NEW MANHOLES AND INLETS AND THOSE TO BE RECONSTRUCTED SHALL BE CONSTRUCTED WITH NEW FRAMES AND GRATES.

IF DURING INLET ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME, CURB BOX OR GRATE, HE SHALL FURNISH A NEW FRAME, GRATE AND CURB BOX, NEENAH R-3246 OR EQUAL, AT NO ADDITIONAL COST.

THE CONTRACTOR SHALL ADVISE THE DISTRICT WHEN MANHOLE ADJUSTMENTS ARE TO BE MADE. SO THE DISTRICT MAY WITNESS THEM. THE METHOD AND AMOUNT OF ADJUSTMENT SHOULD BE NOTED ON THE PLANS. DISTRICT SPECIFICATIONS REQUIRE:

- a. A MINIMUM OF 4" OF ADJUSTING RINGS (4" ADJUSTMENT RING NOT REQUIRED IN TURF AREA OR CURB AND GUTTER ROADWAYS)
- b. A MAXIMUM OF 12" ADJUSTING RINGS.
- c. NO MORE THAN ONE (1) 2" ADJUSTING RING PER MANHOLE.
- d. A MAXIMUM OF 30" FROM THE TOP OF CASTING TO THE FIRST STEP.

SANITARY MANHOLE ADJUSTMENT BY GROUTING IS NOT PERMITTED.

IF THE SANITARY MANHOLE CASTING IS NOT A STANDARD NEENAH R-1670 OR EAST JORDAN E-117 THE DISTRICT SUPPORTING SERVICES DEPARTMENT SHOULD BE CONTACTED PRIOR TO ADJUSTING THE MANHOLE FOR THE REPLACEMENT CASTING.

STATIONS AND OFFSETS FOR DRAINAGE STRUCTURES ARE TO THE CENTER OF THE STRUCTURE'S FRAME.

NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORDS "STORM" OR "SANITARY" ON THE LID. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.

ROADWAY ITEMS

PROTECTIVE COAT SHALL BE APPLIED TO ALL CONCRETE SURFACES AND APPURTENANCES PER ARTICLE 420.18 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". THE COST OF THE PROTECTIVE COAT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE TO WHICH IT IS APPLIED.

PREFORMED FIBER EXPANSION JOINTS WHERE REQUIRED SHALL BE INCLUDED IN THE CONTRACT UNIT BID PRICE FOR WHICH IT IS APPLIED (CURB & GUTTER, DRIVEWAYS, ETC.)

PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.

AGGREGATE SURFACE COURSE TYPE B, SHALL BE USED FOR TEMPORARY ACCESS WHILE THE EXISTING ENTRANCES ARE UNDER CONSTRUCTION, AND SHALL BE USED AS DIRECTED BY THE ENGINEER. THIS AGGREGATE SHALL NOT BE REUSED WITH THE PCC PAVEMENT AND IS INCIDENTAL TO THE P.C.C. DRIVEWAY PAVEMENT.

WHERE CURB AND GUTTER IS ADJACENT TO PCC PAVEMENT, THE CONTRACTOR SHALL PLACE A CONTRACTION JOINT IN PROLONGATION WITH THE JOINTS IN THE EXISTING PAVEMENT. THE JOINT SHALL BE A SAWED CONTRACTION JOINT WITH THE BAR ASSEMBLY AS SHOWN ON THE LATEST REVISION OF HIGHWAY STANDARD 420001. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT BID PRICE FOR CURB AND GUTTER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE CONSTRUCTION JOINTS FOR THE NEW CURB AND GUTTER AFTER INSPECTION OF THE EXISTING CONCRETE PAVEMENT.

EROSION CONTROL

THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES IN A FULLY FUNCTIONAL CONDITION THROUGHOUT THE COURSE OF THE ENTIRE PROJECT AND REMOVE THE MEASURES UPON FINAL STABILIZATION OF THE PROJECT. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY THE CONTRACTOR AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:

- A. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING OR DETERIORATION.
- B. SILT FENCES AND DITCH CHECKS SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM THE SILT FENCES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE SILT FENCE.
- C. EROSION CONTROL BLANKET SHALL BE REPAIRED OR REPLACED WHEN IT BECOMES DISLODGED.

PRIOR TO THE START OF CONSTRUCTION, PLACE SILT FENCE AT THE BOTTOM OF ALL UNSEEDED SLOPES AS SHOWN ON THE PLANS, AND ELSEWHERE AS NECESSARY DURING CONSTRUCTION.

ALL UNSURFACED AREAS ARE TO RECEIVE FOUR (4) INCHES OF TOPSOIL AND BE SEEDED, BLANKETED, AND WATERED AS SPECIFIED WITHIN SEVEN (7) DAYS OF FINAL TOPSOIL RESPREADING AND UNTIL A HEALTHY STAND OF GRASS IS OBTAINED, AS DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL NOT TRACK OR SPILL DIRT CLUMPS ONTO ADJACENT PUBLIC ROADWAYS. IF DIRT IS DEPOSITED ONTO PUBLIC PAVEMENTS, THE CONTRACTOR SHALL REMOVE THE DIRT IMMEDIATELY. ALL PAVEMENT SHALL BE THOROUGHLY CLEANED AT THE END OF EACH DAY.

ALL DISTURBED AREAS THAT WILL BE LEFT IDLE FOR MORE THAN THIRTY (30) DAYS SHALL BE TEMPORARILY SEEDED WITHIN SEVEN (7) DAYS.


INLET PROTECTION MUST BE PROVIDED AND MAINTAINED FOR ALL PROPOSED STORM SEWER INLETS, EXISTING DOWNSTREAM INLETS, CULVERT INLETS, AND OPEN GRATED MANHOLES UNTIL FINAL STABILIZATION OF THE PROJECT.

UPON FINAL STABILIZATION OF THE PROJECT, THE CONTRACTOR SHALL CLEAN THE STORM SEWER SYSTEM OF DEBRIS AND SILT WHICH SHALL BE DISPOSED OF PROPERLY AT THE DIRECTION OF THE ENGINEER.

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE:	N/A
DRAWN BY:	TWH
CHECKED BY:	PDS
DATE:	08/28/2009

	
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GENERAL NOTES	
RIVERSIDE ARRA (SECTION 09-00560-00-RS)	
WEST RIVERSIDE BOULEVARD	ROCKFORD, ILLINOIS
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 GENERAL NOTES.DWG	JOB: 04-28-09-029

SHEET NO.	2 OF 21
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SUMMARY OF QUANTITIES

CONSTRUCTION TYPE CODE: I000

Table with columns: SPEC PROV, IDOT CODE NUMBER, CODE ITEM, UNIT, QTY. Lists various construction items like INLET AND PIPE PROTECTION, PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10", BITUMINOUS MATERIALS (PRIME COAT), etc.

△ SPECIALTY ITEMS

SCHEDULE OF QUANTITIES

28000500 - INLET AND PIPE PROTECTION

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for inlet and pipe protection.

TOTAL = 16 EACH

35400500 - PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10"

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for concrete base course widening.

TOTAL = 68 SQ YD

40600100 - BITUMINOUS MATERIALS (PRIME COAT)

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for bituminous materials.

TOTAL = 9578 GALLON

40600300 - AGGREGATE (PRIME COAT)

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for aggregate.

TOTAL = 41 TON

40600400 - MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for mixture for cracks.

TOTAL = 1 TON

40600635 - LEVELING BINDER (MACHINE METHOD), N70

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for leveling binder.

TOTAL = 794 TON

40600990 - TEMPORARY RAMP

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for temporary ramp with detailed descriptions.

TOTAL = 903 SQ YD

40603365 - HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for hot-mix asphalt surface course.

TOTAL = 876 TON

42400100 - PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for concrete sidewalk.

TOTAL = 453 SQ FT

42400800 - DETECTABLE WARNINGS

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for detectable warnings.

TOTAL = 60 SQ FT

44000154 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for asphalt surface removal.

TOTAL = 1630 SQ YD

44000160 - HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for asphalt surface removal.

TOTAL = 3495 SQ YD

44000161 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"

Table with columns: STA, TO, STA, QTY. Lists stationing and quantities for asphalt surface removal.

TOTAL = 5876 SQ YD

44000600 - SIDEWALK REMOVAL

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for sidewalk removal.

TOTAL = 453 SQ FT

44001700 - COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for curb and gutter removal.

TOTAL = 404 FOOT

44001800 - COMBINATION CURB AND GUTTER REMOVAL (PARTIAL)

Table with columns: STA, OFF, TO, QTY. Lists stationing and quantities for partial curb and gutter removal.

TOTAL = 104 FOOT

Table for SHEET REVIEW with columns: AGENCY, DATE.

Table for REVISIONS with columns: NO., ITEM, DATE.

Table for project details including SCALE, DRAWN BY, CHECKED BY, DATE.

McClure Engineering Associates, Inc. logo and contact information.

Table for SUMMARY AND SCHEDULE OF QUANTITIES with project title and drawing information.

SCHEDULE OF QUANTITIES

44003100 - MEDIAN REMOVAL

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+59.5 to 103+13.6 and a total of 1237 SQ FT.

44200970 - CLASS B PATCHES, TYPE II, 10 INCH

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.0 to 102+87.2 and a total of 99 SQ YD.

44201705 - CLASS D PATCHES, TYPE II, 5 INCH

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 106+73.3 to 110+05.9 and a total of 292 SQ YD.

44201711 - CLASS D PATCHES, TYPE IV, 5 INCH

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 110+99.3 to 113+57.9 and a total of 235 SQ YD.

44212900 - PAVEMENT PATCHING (PARTIAL DEPTH)

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 116+79.8 to 116+80.19 and a total of 6 SQ YD.

44300200 - STRIP REFLECTIVE CRACK CONTROL TREATMENT

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+59.5 to 102+48.0 and a total of 188 FOOT.

50102400 - CONCRETE REMOVAL

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.7 to 102+50.5 and a total of 11 CU YD.

60255500 - MANHOLES TO BE ADJUSTED

Table with columns: STA, OFF, QTY. Includes data for 101+16.2 to 112+39.1 and a total of 6 EACH.

60262700 - INLETS TO BE RECONSTRUCTED

Table with columns: STA, OFF, QTY. Includes data for 103+35.4 to 116+69.6 and a total of 9 EACH.

60265108 - INLETS TO BE RECONSTRUCTED WITH NEW FRAME AND GRATE, SPECIAL

Table with columns: STA, OFF, QTY. Includes data for 101+16.7 and a total of 1 EACH.

60622353 - CONCRETE MEDIAN, TYPE SM-6

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.7 to 102+50.5 and a total of 1037 SQ FT.

67100100 - MOBILIZATION

Table with columns: STA, OFF, QTY. Includes data for 101+16.7 and a total of 1 L SUM.

70101700 - TRAFFIC CONTROL AND PROTECTION

Table with columns: STA, OFF, QTY. Includes data for 100+59.5 to 103+13.6 and a total of 1 L SUM.

70106800 - CHANGEABLE MESSAGE SIGN

Table with columns: STA, OFF, QTY. Includes data for (2 SIGNS X 3 MONTHS) and a total of 6 CAL MO.

70300100 - SHORT-TERM PAVEMENT MARKING

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for (LENGTH OF PROJECT) = 1646' X 5 LANES X 4' X 3 APPS = 2520 and a total of 2520 FOOT.

70300210 - TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS

Table with columns: STA, OFF, QTY. Includes data for 100+64.6 to 114+44.1 and a total of 312 SQ FT.

70300220 - TEMPORARY PAVEMENT MARKING - LINE 4"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+47.7 to 104+77.7 and a total of 6940 FOOT.

70300240 - TEMPORARY PAVEMENT MARKING - LINE 6"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 110+87.5 to 113+55.5 and a total of 248 FOOT.

70300250 - TEMPORARY PAVEMENT MARKING - LINE 8"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+33.7 to 113+64.1 and a total of 1770 FOOT.

70300260 - TEMPORARY PAVEMENT MARKING - LINE 12"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+47.7 to 112+05.7 and a total of 204 FOOT.

70300280 - TEMPORARY PAVEMENT MARKING - LINE 24"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.2 to 113+24.3 and a total of 247 FOOT.

70301000 - WORK ZONE PAVEMENT MARKING REMOVAL

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for (4" TEMPORARY PAVEMENT MARKING) = 6,940 L.F. and a total of 4315 SQ FT.

78000100 - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

Table with columns: STA, OFF, QTY. Includes data for 100+64.6 to 114+44.1 and a total of 312 SQ FT.

78000200 - THERMOPLASTIC PAVEMENT MARKINGS - LINE 4"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+47.7 to 104+77.7 and a total of 7565 FOOT.

78000400 - THERMOPLASTIC PAVEMENT MARKINGS - LINE 6"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 110+87.5 to 113+55.5 and a total of 248 FOOT.

78000500 - THERMOPLASTIC PAVEMENT MARKINGS - LINE 8"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+33.7 to 113+64.1 and a total of 1770 FOOT.

78000600 - THERMOPLASTIC PAVEMENT MARKINGS - LINE 12"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+47.7 to 112+05.7 and a total of 204 FOOT.

78000650 - THERMOPLASTIC PAVEMENT MARKINGS - LINE 24"

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.2 to 113+24.3 and a total of 247 FOOT.

81400215 - HEAVY-DUTY HANDHOLE TO BE ADJUSTED

Table with columns: STA, OFF, QTY. Includes data for 100+44.7 to 100+44.7 and a total of 1 EACH.

88600600 - DETECTOR LOOP REPLACEMENT

Table with columns: STA, OFF, QTY. Includes data for 100+43.7 to 113+30.1 and a total of 333 FOOT.

X0321556 - SANITARY MANHOLES TO BE ADJUSTED

Table with columns: STA, OFF, QTY. Includes data for 103+29.5 to 103+29.5 and a total of 1 EACH.

X0325702 - NIGHTTIME WORK ZONE LIGHTING

Table with columns: STA, OFF, QTY. Includes data for 101+16.7 and a total of 1 L SUM.

Z0017202 - DOWEL BARS, 1 1/2"

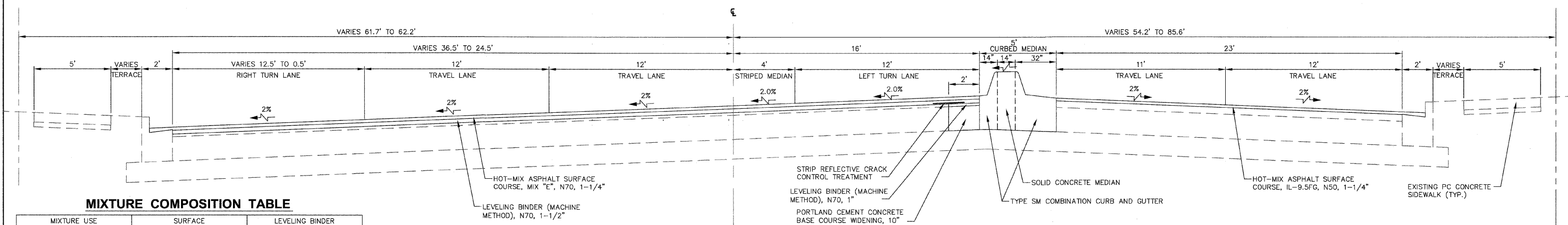
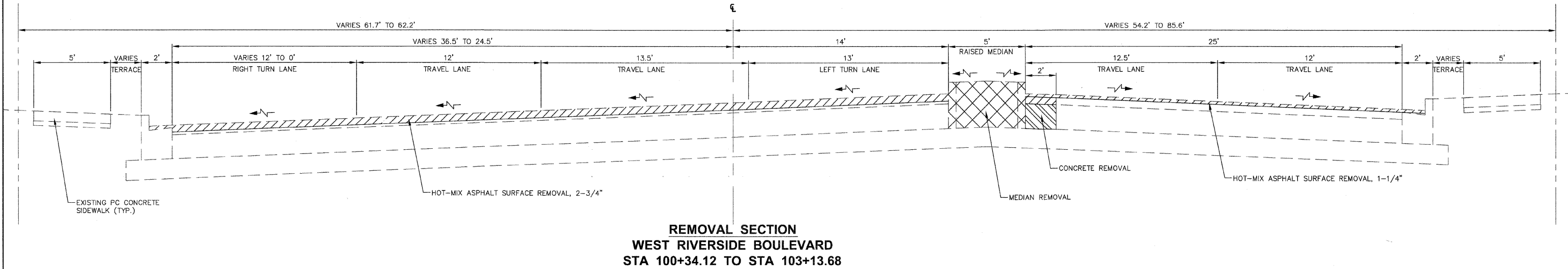
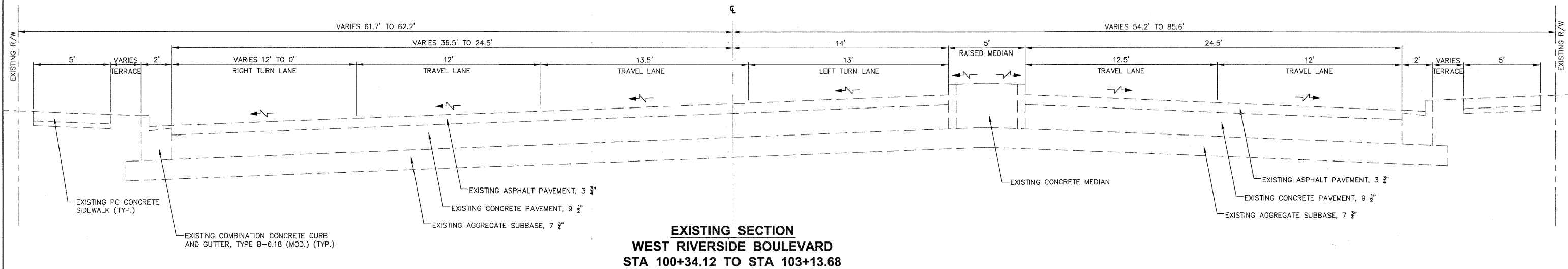
Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 100+41.0 to 102+87.2 and a total of 346 EACH.

Z0028415 - GEOTECHNICAL REINFORCEMENT

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 106+58.1 to 110+99.3 and a total of 449 SQ YD.

Z0028700 - GRANULAR SUBGRADE REPLACEMENT

Table with columns: STA, OFF, TO, STA, OFF, QTY. Includes data for 106+58.1 to 110+99.3 and a total of 75 CU YD.



MIXTURE COMPOSITION TABLE

MIXTURE USE	SURFACE	LEVELING BINDER
PG	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL-9.5	IL-9.5
FRICTION COEFFICIENT	E	N/A
20-YR ESAL	5.7	5.7

**PROPOSED SECTION
WEST RIVERSIDE BOULEVARD
STA 100+34.12 TO STA 103+13.68**

SHEET REVIEW	
AGENCY	DATE

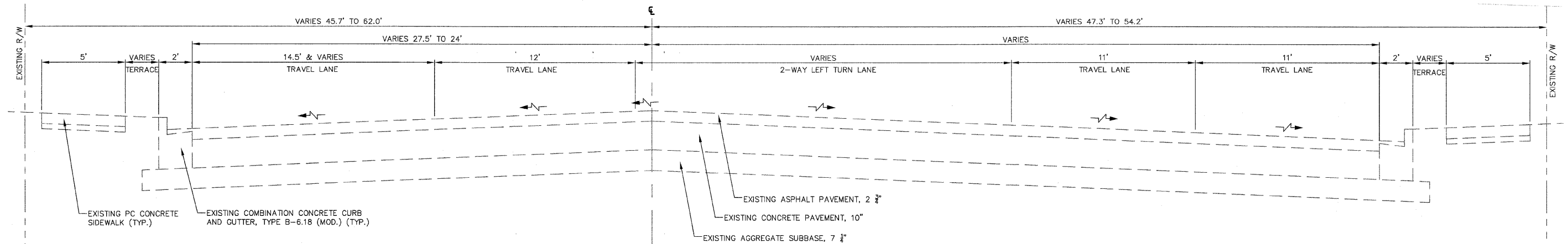
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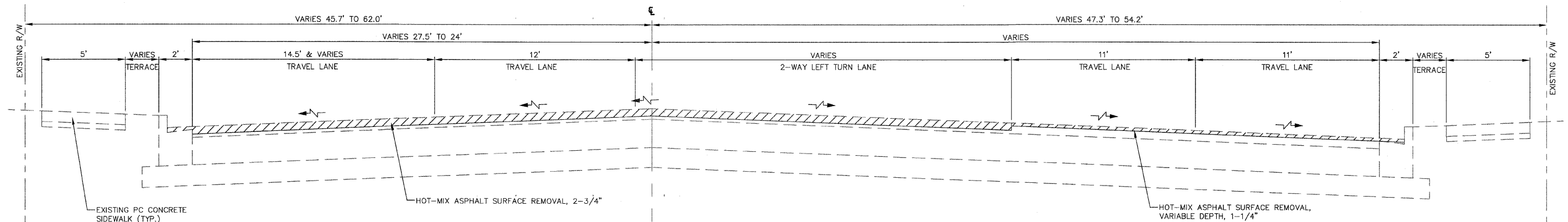
McClure
Engineering Associates, Inc.
7282 Argus Drive
Rockford, Illinois 61107-5837
(815) 398-2332 FAX (815) 398-2496
Design Firm License: Illinois 184-000816

TYPICAL SECTIONS
RIVERSIDE ARRA (SECTION 09-00560-00-RS)
WEST RIVERSIDE BOULEVARD
ROCKFORD, ILLINOIS
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 TYPICAL SECTIONS.DWG
JOB: 04-28-09-029

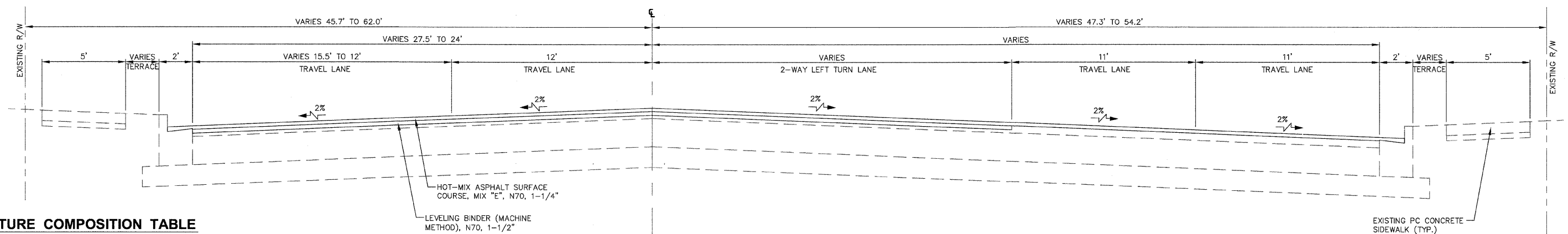
SHEET NO.
5
OF
21



**EXISTING SECTION
WEST RIVERSIDE BOULEVARD
STA. 103+13.68 TO STA. 106+73.72**



**REMOVAL SECTION
WEST RIVERSIDE BOULEVARD
STA. 103+13.68 TO STA. 106+73.72**



**PROPOSED SECTION
WEST RIVERSIDE BOULEVARD
STA. 103+13.68 TO STA. 106+73.72**

MIXTURE COMPOSITION TABLE

MIXTURE USE	SURFACE	LEVELING BINDER
PG	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL-9.5	IL-9.5
FRICTION COEFFICIENT	E	N/A
20-YR ESAL	5.7	5.7

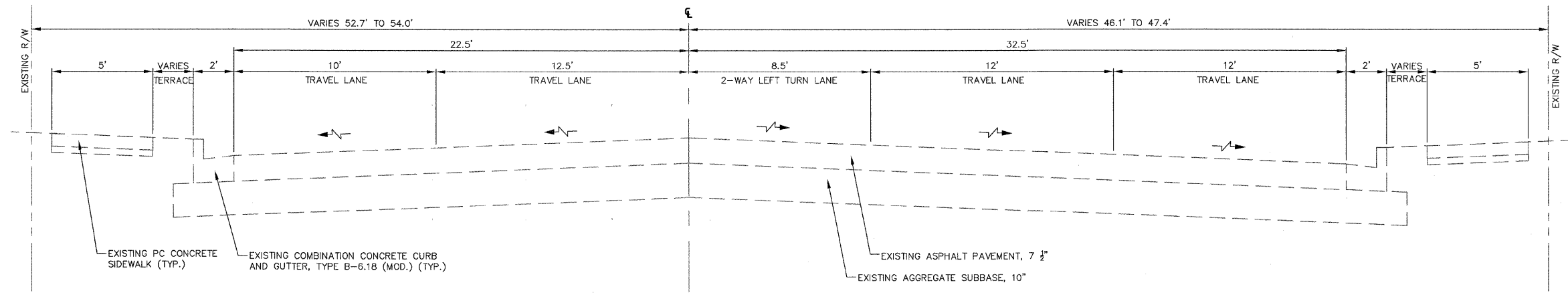
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AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

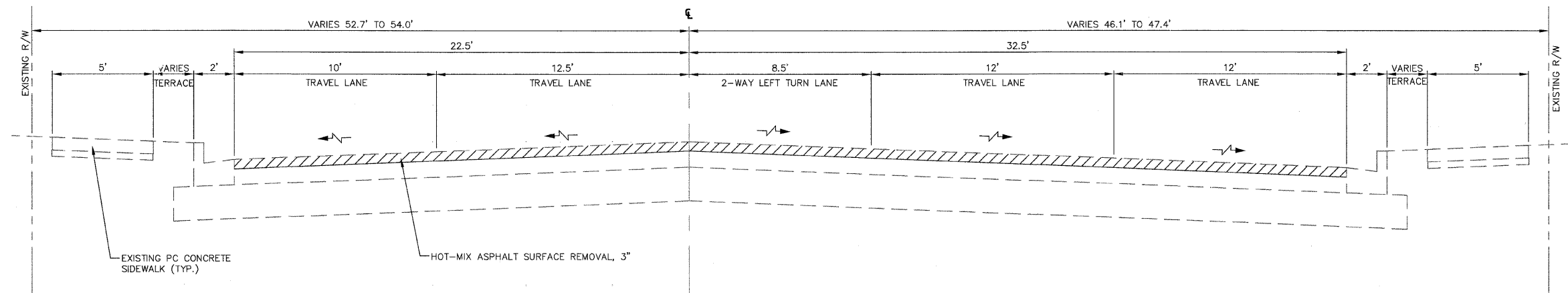
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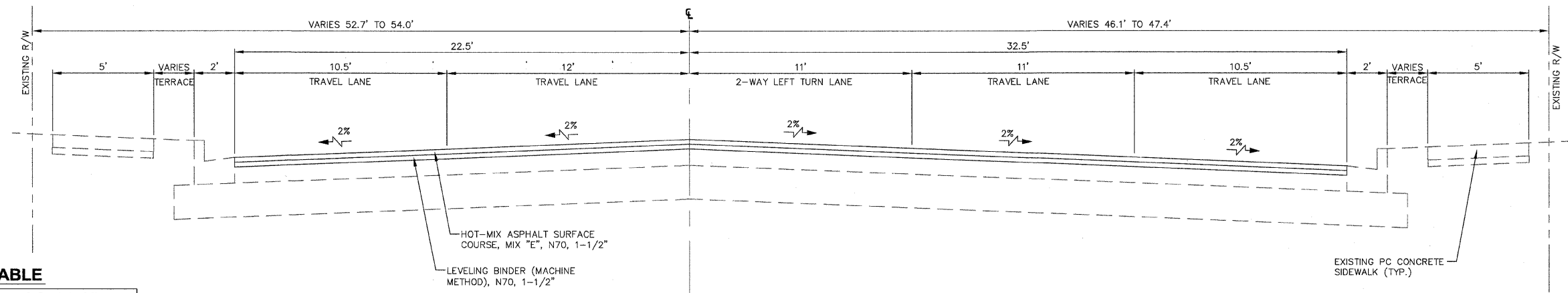
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WEST RIVERSIDE BOULEVARD
ROCKFORD, ILLINOIS
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**EXISTING SECTION
WEST RIVERSIDE BOULEVARD
STA. 106+73.72 TO STA 111+00.00**



**REMOVAL SECTION
WEST RIVERSIDE BOULEVARD
STA. 106+73.72 TO STA 111+00.00**



**PROPOSED SECTION
WEST RIVERSIDE BOULEVARD
STA. 106+73.72 TO STA. 111+00.00**

MIXTURE COMPOSITION TABLE

MIXTURE USE	SURFACE	LEVELING BINDER
PG	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL-9.5 OR IL-12.5	IL-9.5 OR IL-12.5
FRICTION COEFFICIENT	E	N/A
20-YR ESAL	4.4	4.4

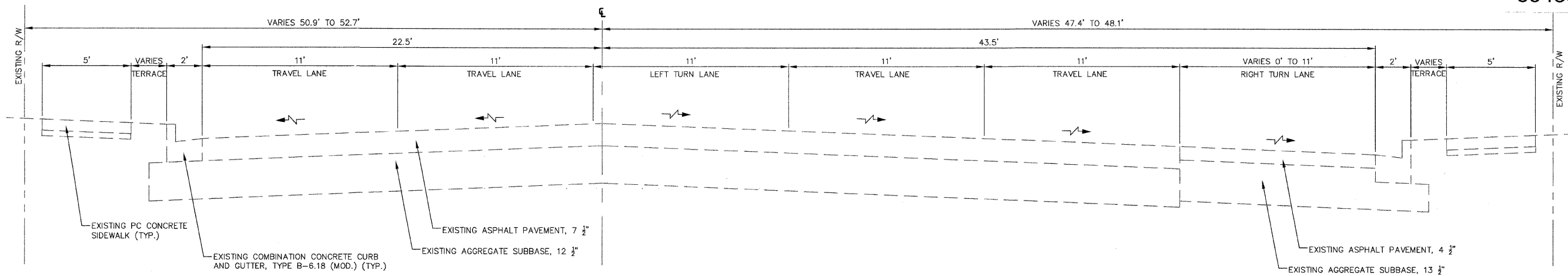
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AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

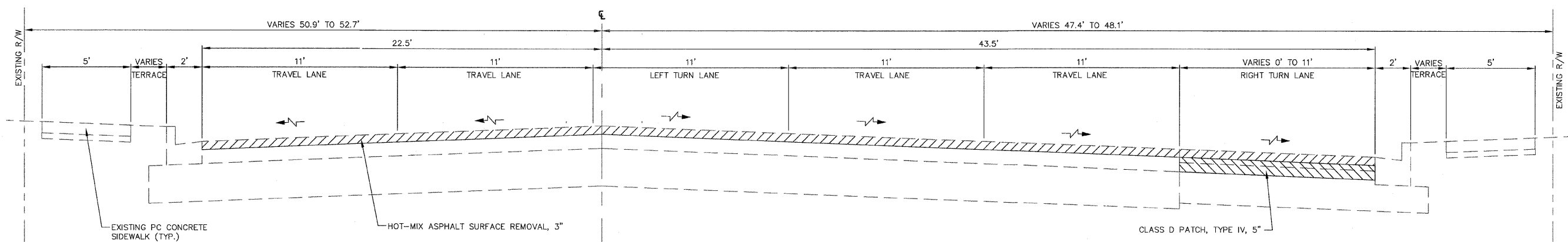
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DRAWN BY:	TWH
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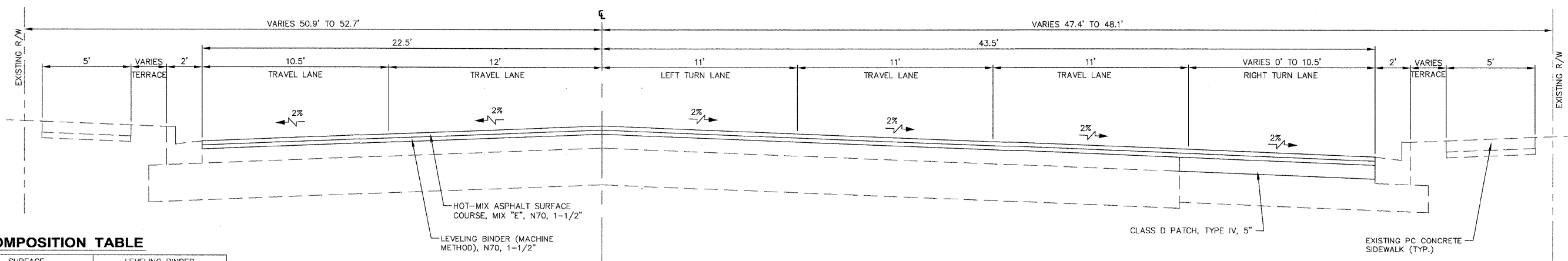
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RIVERSIDE ARRA (SECTION 09-00560-00-RS)
WEST RIVERSIDE BOULEVARD
ROCKFORD, ILLINOIS
FILE: H:\109-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\109-029 TYPICAL SECTIONS.DWG
JOB: 04-28-09-029



**EXISTING SECTION
WEST RIVERSIDE BOULEVARD
STA. 111+00.00 TO STA. 113+08.86**



**REMOVAL SECTION
WEST RIVERSIDE BOULEVARD
STA. 111+00.00 TO STA. 113+08.86**



**PROPOSED SECTION
WEST RIVERSIDE BOULEVARD
STA. 111+00.00 TO STA. 113+08.86**

MIXTURE COMPOSITION TABLE

MIXTURE USE	SURFACE	LEVELING BINDER
PG	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL-9.5 OR IL-12.5	IL-9.5 OR IL-12.5
FRICTION COEFFICIENT	E	N/A
20-YR ESAL	4.4	4.4

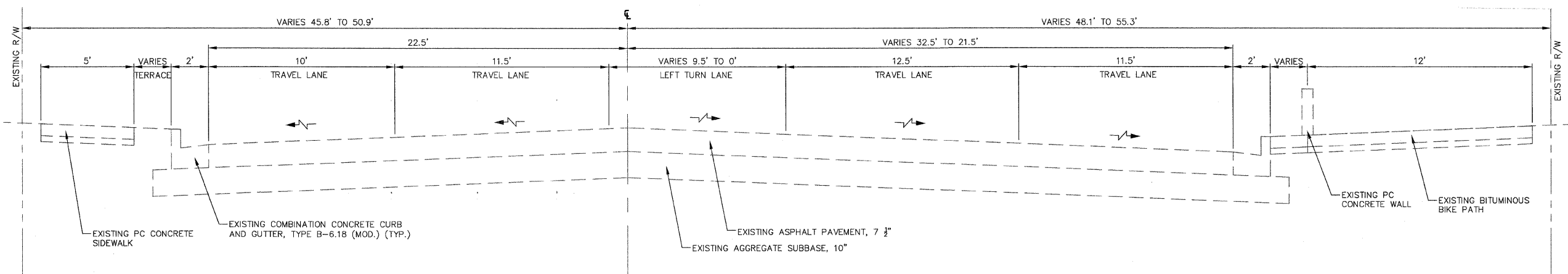
SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

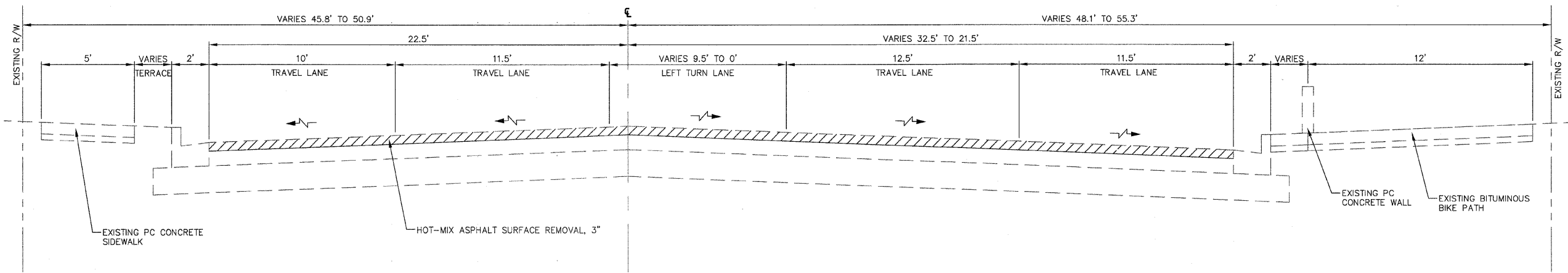
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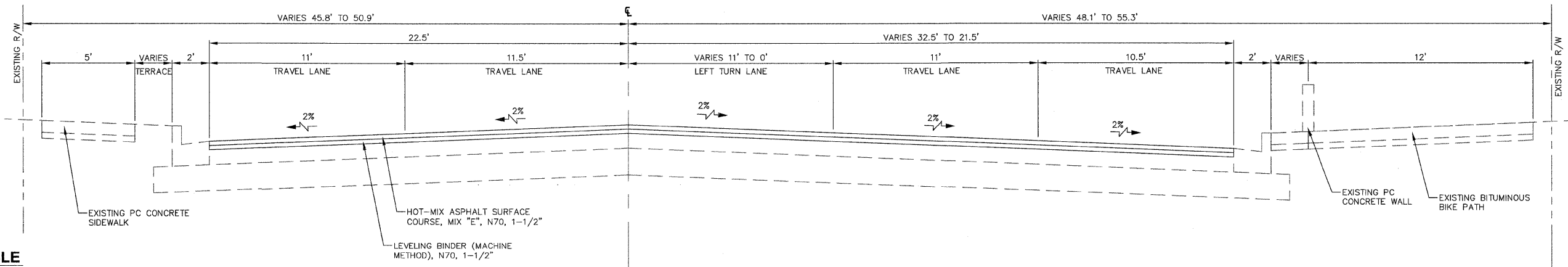
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RIVERSIDE ARRA (SECTION 09-00560-00-RS)
WEST RIVERSIDE BOULEVARD ROCKFORD, ILLINOIS
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 TYPICAL SECTIONS.DWG JOB: 04-28-09-029



**EXISTING SECTION
WEST RIVERSIDE BOULEVARD
STA. 113+08.86 TO STA. 116+80.01**



**REMOVAL SECTION
WEST RIVERSIDE BOULEVARD
STA. 113+08.86 TO STA. 116+80.01**



**PROPOSED SECTION
WEST RIVERSIDE BOULEVARD
STA. 113+08.86 TO STA. 116+80.01**

MIXTURE COMPOSITION TABLE

MIXTURE USE	SURFACE	LEVELING BINDER
PG	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL-9.5 OR IL-12.5	IL-9.5 OR IL-12.5
FRICTION COEFFICIENT	E	N/A
20-YR ESAL	4.4	4.4

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

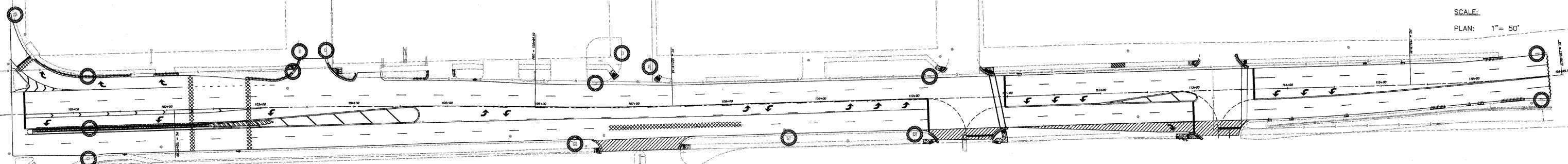
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TYPICAL SECTIONS
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WEST RIVERSIDE BOULEVARD ROCKFORD, ILLINOIS
FILE: H:109-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\109-029 TYPICAL SECTIONS.DWG JOB: 04-28-09-029



SCALE:
PLAN: 1" = 50'



STORMWATER POLLUTION PREVENTION

1. THE STORMWATER POLLUTION PREVENTION PLAN IS COMPRISED OF THE DRAWING MAPS, SPECIFICATIONS, AND THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS.
2. ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN, ILLINOIS EPA NPDES GENERAL PERMIT, CITY OF ROCKFORD NPDES PERMIT, AND BECOME FAMILIAR WITH THEIR CONTENTS.
3. CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES AS REQUIRED BY THE SWPPP. ADDITIONAL BEST MANAGEMENT PRACTICES SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST TO OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.
4. BEST MANAGEMENT PRACTICES (BMP'S) AND CONTROLS SHALL CONFORM TO FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AS APPLICABLE. CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY PERMITTING AGENCY OR OWNER.
5. GENERAL CONTRACTOR SHALL DENOTE ON EXHIBITS THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA.
6. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED.
7. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
8. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES ONTO ROADWAY OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY.
9. CONTRACTORS OR SUBCONTRACTORS WILL BE RESPONSIBLE FOR REMOVING SEDIMENT THAT MAY HAVE COLLECTED IN THE STORM SEWER DRAINAGE SYSTEMS IN CONJUNCTION WITH THE STABILIZATION OF THE SITE.
10. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY. THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.
11. INLET PROTECTION MUST BE PROVIDED AND MAINTAINED FOR ALL PROPOSED INLETS UNTIL FINAL GRADES HAVE BEEN ESTABLISHED.
12. THE EROSION CONTROL MEASURES INDICATED ON THE DRAWING MAPS ARE THE MINIMUM REQUIREMENTS.

MAINTENANCE NOTES

ALL BMP MEASURES STATED ON THIS STORM WATER POLLUTION PREVENTION MAP AND PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE ROADWAY PROJECT. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED PERSON AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:

1. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING, OR DETERIORATION.
2. ALL SEEDED AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED, AND RESEEDED AS NEEDED.
3. SILT FENCES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM THE SILT FENCES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE SILT FENCE.
4. THE CONSTRUCTION AREA ACCESSES SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO ADJACENT PUBLIC ROADWAYS AND RIGHTS-OF-WAY OUTSIDE WORK AREA.
5. SILT BAGS IN REQUIRED INLET STRUCTURES SHALL BE MAINTAINED IN OPERATIONAL CONDITIONS AT ALL TIMES. SEDIMENT FROM SILT BAGS SHALL BE REMOVED FROM BAGS AND QUANTIFIED AND REPORTED WEEKLY.

GENERAL NOTE:

1. SEE STANDARDS AND DETAILS FOR BMPS.

KEY LEGEND

INLET PROTECTION

SHEET REVIEW	
AGENCY	DATE

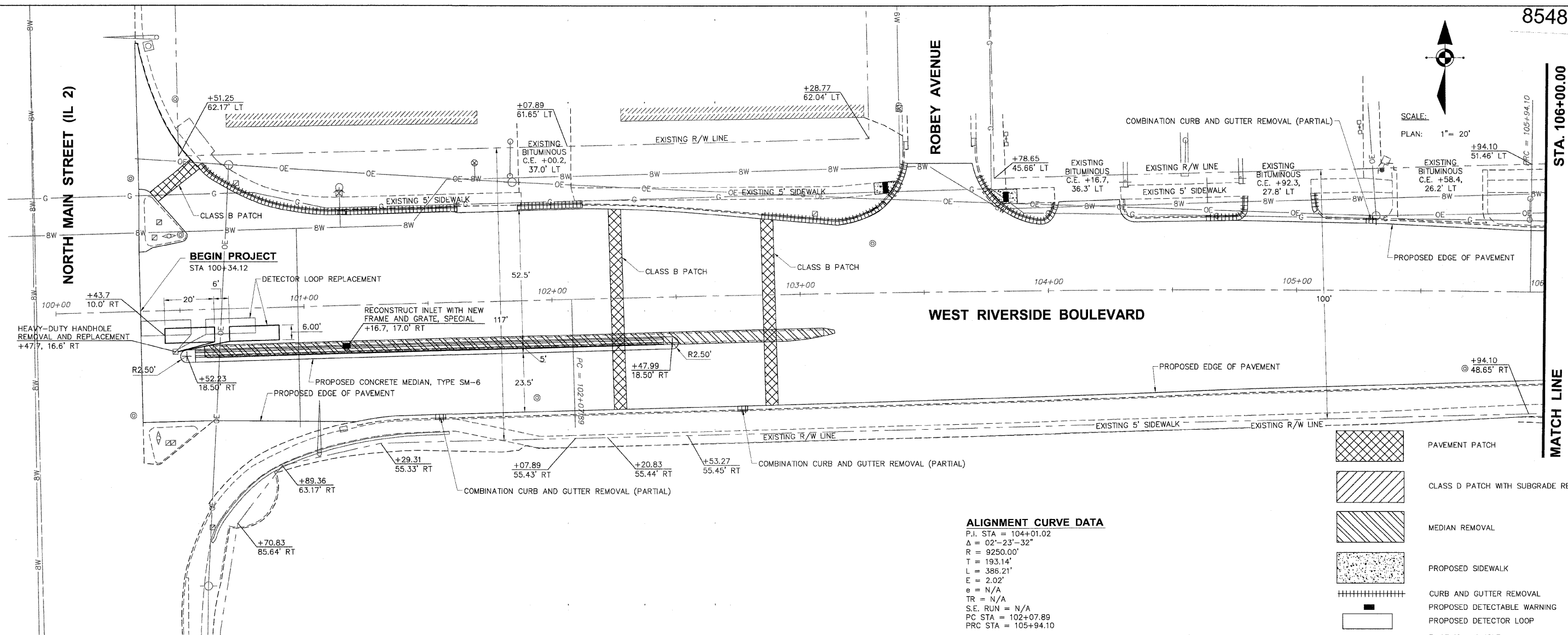
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NO.	DATE

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DATE:	08/28/2009


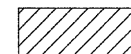
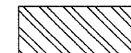

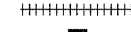

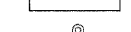
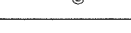
McClure
Engineering Associates, Inc.
7282 Argus Drive
(815) 398-2332
Rockford, Illinois 61107-5837
FAX (815) 398-2496
Design Firm License: Illinois 184-000816

STORM WATER POLLUTION PREVENTION PLAN
RIVERSIDE ARRA (SECTION 09-00560-00-RS)
WEST RIVERSIDE BOULEVARD
ROCKFORD, ILLINOIS
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 SWPP.DWG
JOB: 04-28-09-029

SHEET NO.	10
OF	21



SCALE:
PLAN: 1" = 20'

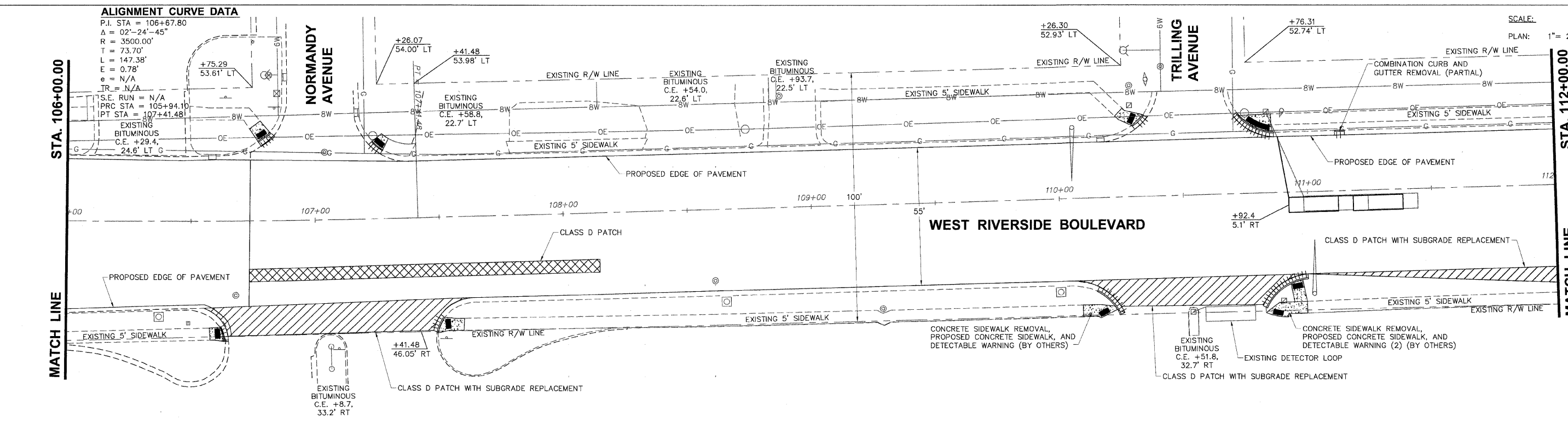
-  PAVEMENT PATCH
-  CLASS D PATCH WITH SUBGRADE REPLACEMENT
-  MEDIAN REMOVAL
-  PROPOSED SIDEWALK
-  CURB AND GUTTER REMOVAL
-  PROPOSED DETECTABLE WARNING
-  PROPOSED DETECTOR LOOP
-  EXISTING MANHOLE

ALIGNMENT CURVE DATA

P.I. STA = 104+01.02
 $\Delta = 02^\circ-23'-32''$
 R = 9250.00'
 T = 193.14'
 L = 386.21'
 E = 2.02'
 e = N/A
 TR = N/A
 S.E. RUN = N/A
 PC STA = 102+07.89
 PRC STA = 105+94.10

ALIGNMENT CURVE DATA

P.I. STA = 106+67.80
 $\Delta = 02^\circ-24'-45''$
 R = 3500.00'
 T = 73.70'
 L = 147.38'
 E = 0.78'
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 IPT STA = 107+41.48

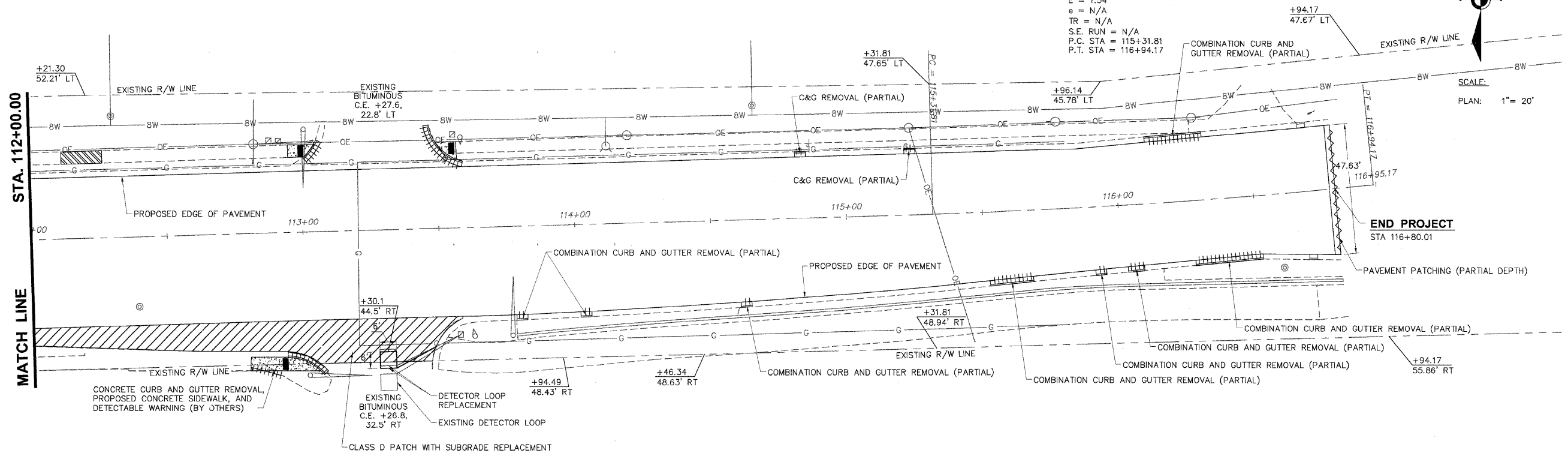


SCALE:
PLAN: 1" = 20'

<p>SHEET REVIEW</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>AGENCY</th> <th>DATE</th> </tr> </thead> <tbody> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </tbody> </table>	AGENCY	DATE							<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>ITEM</th> <th>DATE</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	ITEM	DATE										<p>SCALE: 1" = 20'</p> <p>DRAWN BY: TWH</p> <p>CHECKED BY: PDS</p> <p>DATE: 08/21/2009</p>	<p>McClure Engineering Associates, Inc.</p> <p>7282 Argus Drive Rockford, Illinois 61107-5837 (815) 398-2332 FAX (815) 398-2496 Design Firm License: Illinois 184-000816</p>	<p>ROADWAY PLAN - STA 100+00 TO 112+00</p> <p>RIVERSIDE ARRA (SECTION 09-00560-00-RS)</p> <p>WEST RIVERSIDE BOULEVARD ROCKFORD, ILLINOIS</p> <p>FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 P & P.DWG JOB: 04-28-09-029</p>	<p>SHEET NO.</p> <p>11 OF 21</p>
AGENCY	DATE																								
NO.	ITEM	DATE																							


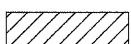
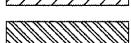
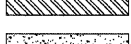
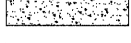
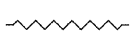



ALIGNMENT CURVE DATA

P.I. STA = 116+13.03
 Δ = 04°-20'-12"
 R = 2145.00'
 T = 81.22'
 L = 162.36'
 E = 1.54'
 e = N/A
 TR = N/A
 S.E. RUN = N/A
 P.C. STA = 115+31.81
 P.T. STA = 116+94.17



SCALE: 1" = 20'
 PLAN: 1" = 20'

END PROJECT
 STA 116+80.01

-  CLASS B PATCH
-  CLASS D PATCH WITH SUBGRADE REPLACEMENT
-  SIDEWALK REMOVAL AND REPLACEMENT
-  PROPOSED SIDEWALK
-  CURB AND GUTTER REMOVAL
-  CURB AND GUTTER REMOVAL
-  PROPOSED DETECTABLE WARNING
-  PROPOSED DETECTOR LOOP
-  EXISTING MANHOLE

SHEET REVIEW	
AGENCY	DATE

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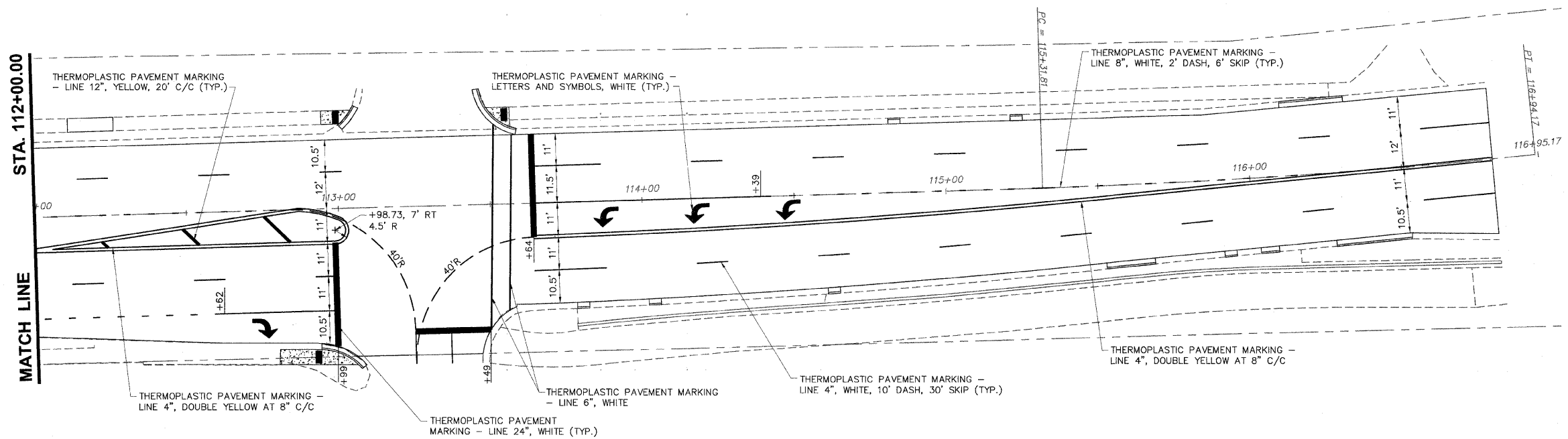
McClure
 Engineering Associates, Inc.
 7282 Argus Drive
 Rockford, Illinois 61107-5837
 (815) 398-2332 FAX (815) 398-2498
 Design Firm License: Illinois 184-000816

ROADWAY PLAN - STA 112+00 TO 116+95
 RIVERSIDE ARRA (SECTION 09-00560-00-RS)
 WEST RIVERSIDE BOULEVARD
 ROCKFORD, ILLINOIS
 FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 P & P.DWG
 JOB: 04-28-09-029

SHEET NO.	12 OF 21
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SCALE:
PLAN: 1" = 20'



SHEET REVIEW	
AGENCY	DATE

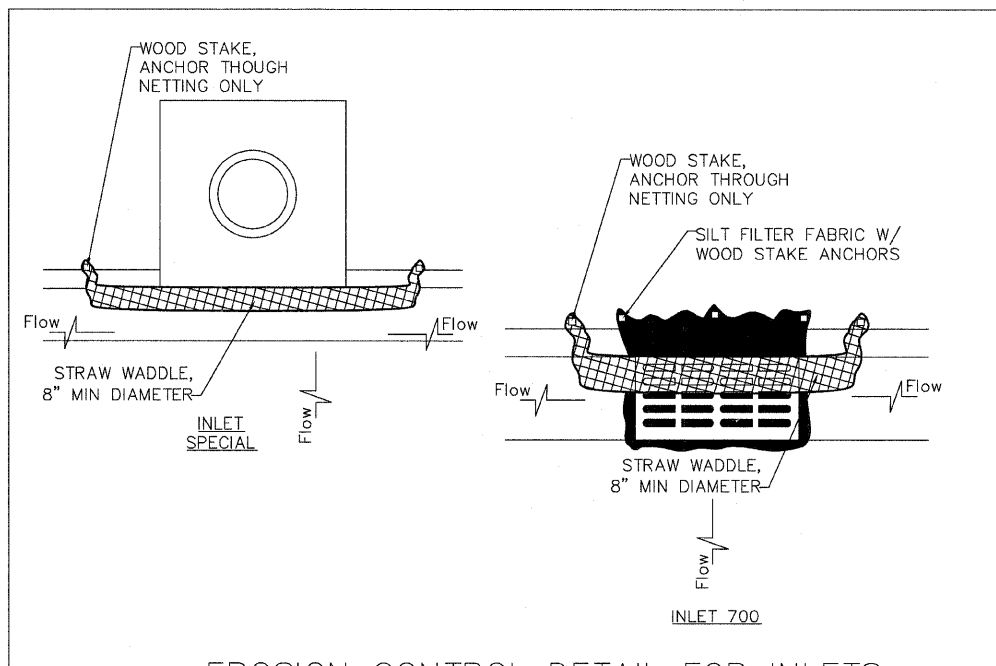
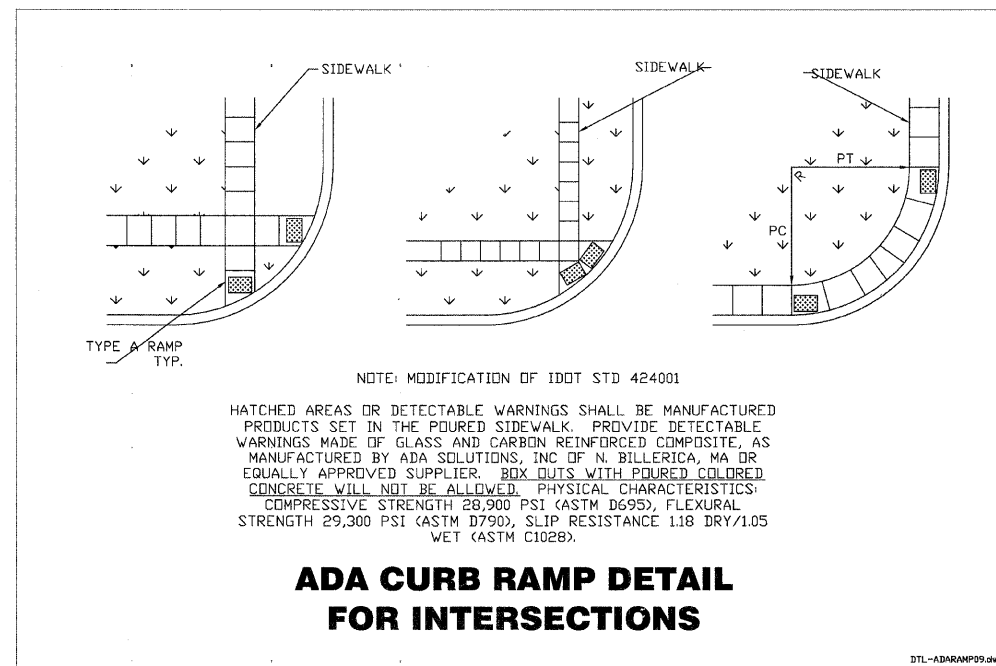
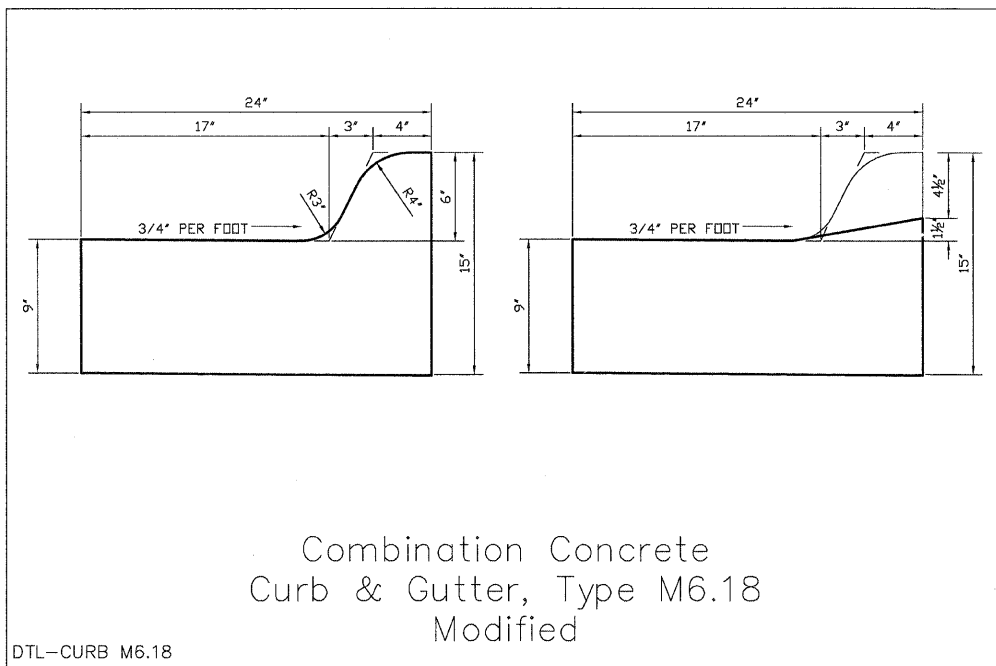
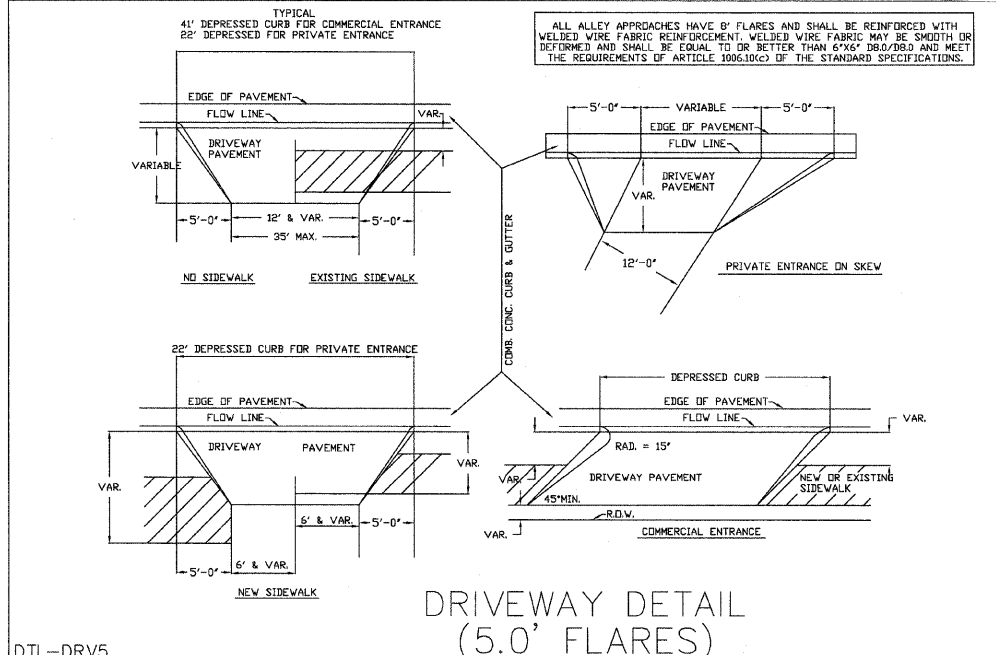
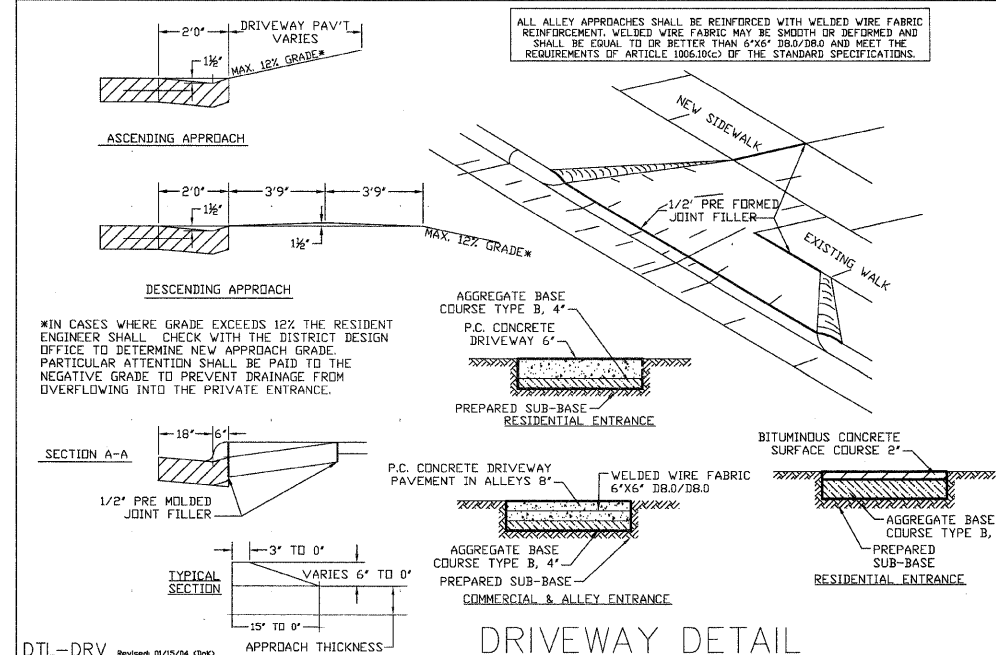
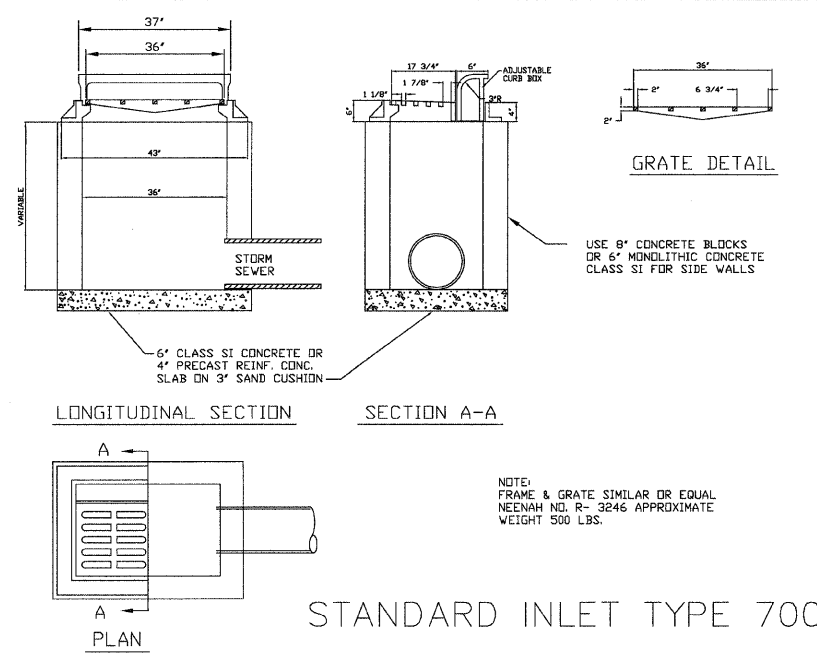
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7282 Argus Drive
Rockford, Illinois 61107-5837
(815) 398-2332 FAX (815) 398-2496
Design Firm License: Illinois 184-000816

STRIPING PLAN - STA 112+00 TO 116+95
RIVERSIDE ARRA (SECTION 09-00560-00-RS)
WEST RIVERSIDE BOULEVARD
ROCKFORD, ILLINOIS

FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 STRIPE.DWG
JOB: 04-28-09-029



GENERAL NOTES

DEPRESSED CURBS - The top of curbs shall be depressed where the curb and gutter is constructed across alleys and for private drives or where directed by Engineer.

THICKNESS - "t" = Thickness of pavement.

DRAINAGE OPENINGS - At all location where castings are to be incorporated in the curb and gutter, a 1" expansion joint shall be installed in the curb and gutter a distance of 5 ft. from each side of the casting. At least 2 tie bar shall be placed in the 5 ft. portion and the bars shall be at least 9" from the joint or casting. Any extra bars required shall be considered incidental. When the width of the casting is less than the width of the curb and gutter 2-No. 4 re-bar (L = 12" + casting length + 12") shall be incorporated in the continuous portion of concrete gutter in front of the casting.

TIE BARS - Tie bars shall be in accordance with details for Bulkhead Longitudinal Construction joint shown of Standard 420001-03. When curb and gutter is constructed adjacent to flexible pavement tie bar shall be omitted.

JOINTS - In addition to the requirements of Section 606 of the Standard Specification joints shall be constructed as follows: Contraction joint and expansion joints shall be installed in the curb and gutter in prolongation with joints in adjacent PCC pavement or base course except that dowel bars will not be required in contraction joints. Contraction joints when sawed or template formed shall be sealed in accordance with Article 420.12 of the Standard Specifications.

When curb and gutter is constructed adjacent to flexible pavement, a 1" expansion joint shall be installed at points of curvature for short-radius curves and at construction joints. Contraction joints shall be placed between expansion joints at distances not to exceed 25 ft

All expansion joints shall be constructed with a minimum 1" thick preformed expansion joint filler conforming to the cross section of the curb and gutter and shall be provided with a 1-1/4" diameter x 18" coated smooth dowel bar conforming to Article 1006.11(b) of the Standard Specifications. The greased end of the dowel bar shall be fitted with a cap having a pinched stop that will provide a minimum 1" of expansion.

Construction joints constructed in curb and gutter adjacent to PCC pavement and PCC base course shall be provided with No. 4 deformed steel tie bars 30" long. Tie bars shall be placed on 9" + centers (minimum 2 per joint).

GENERAL NOTES (CONT.)

TRANSITIONS - The transition from full height curb to depressed curb shall be made at the rate of 3" per foot of length or flatter.

ADJACENT PCC PAVEMENT - Adjacent PCC pavement or base course less than 1" in width shall be constructed monolithic with the curb and gutter. This area of pavement will be included in the measured area of the adjacent pavement.

MONOLITHIC CONSTRUCTION - Curb and combination curb and gutter may be constructed monolithically with PCC pavement at the option of the Contractor. Tie bar will not be required. Pavement reinforcement shall be extended laterally to within 3" to 5" from the back of the curb.

Curbs and combination curb and gutter may be constructed monolithically with PCC base course. Tie bar will be required. Tie bars shall be held in the proper location by support pins or approved mechanical means.

Longitudinal joints between the PCC pavement or PCC base course and curb or combination curb and gutter will not be required.

SLIPFORM CONSTRUCTION - Vertical faces may be battered at the rate of 3/4" per foot of height to aid in slipform operations.

SHEET REVIEW	
AGENCY	DATE

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SCALE:	N/A
DRAWN BY:	TWH
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DATE:	08/28/2009

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Engineering Associates, Inc.

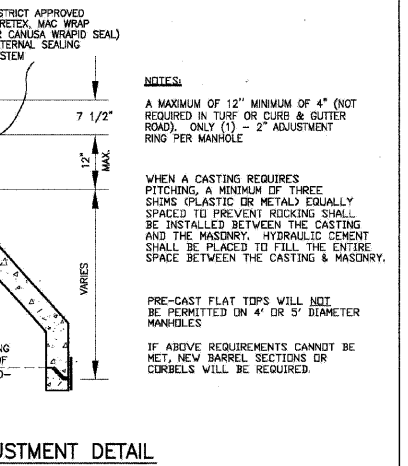
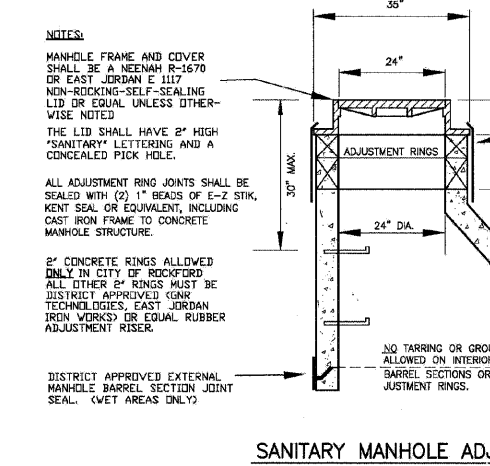
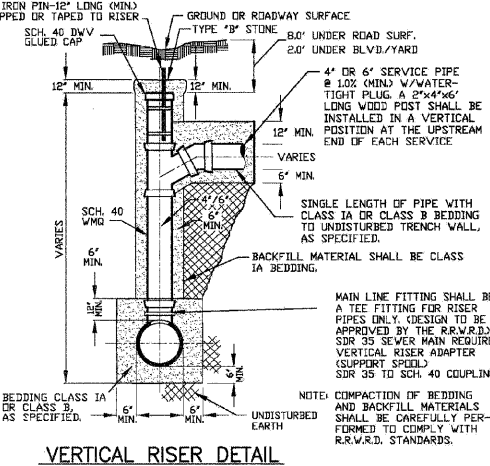
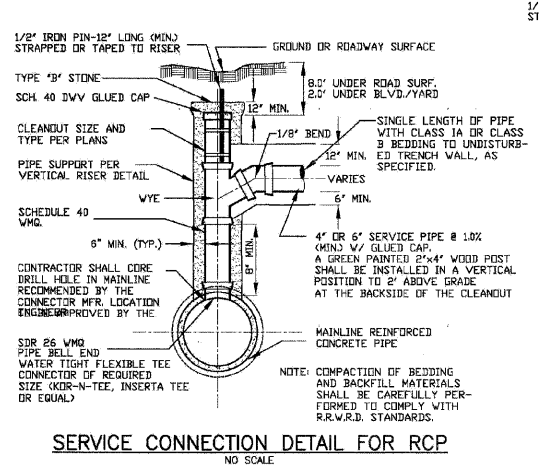
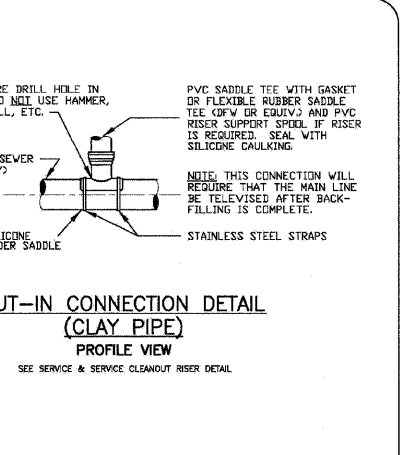
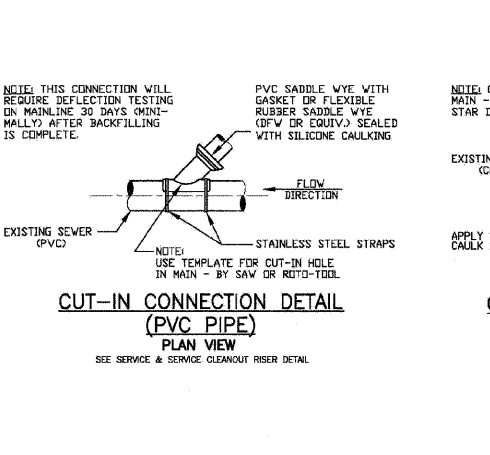
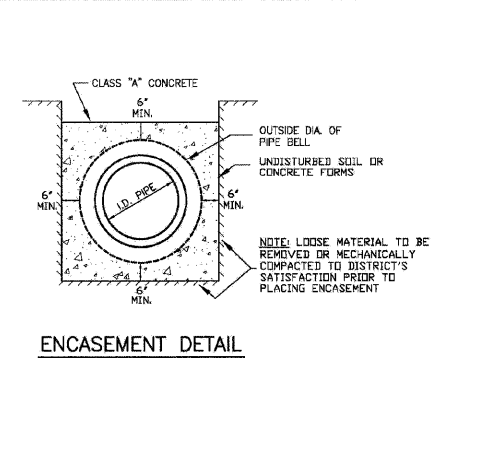
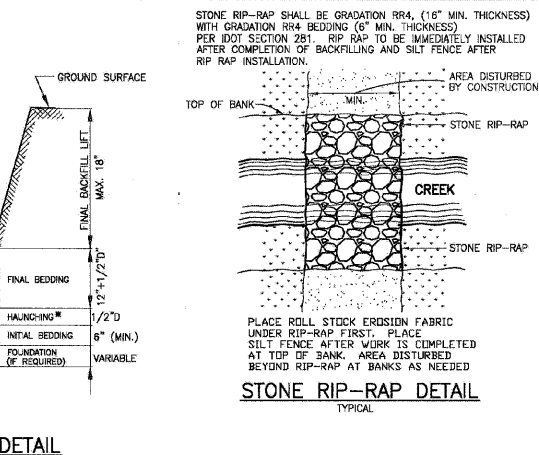
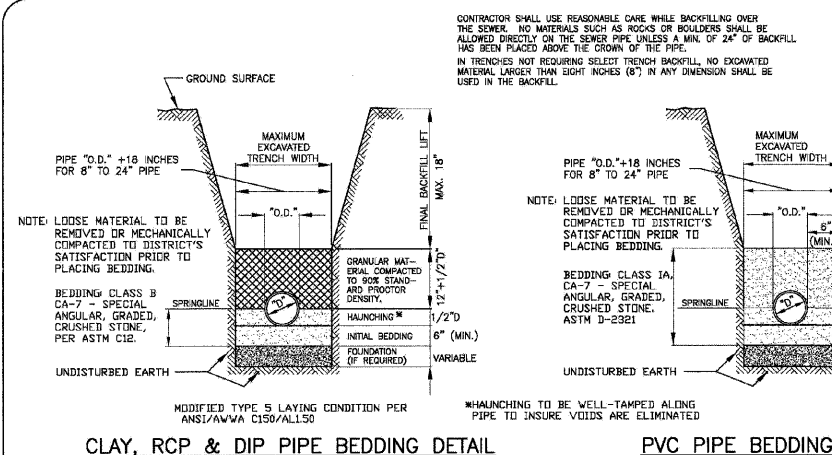
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Rockford, Illinois 61107-5837
(815) 398-2332 FAX (815) 398-2496
Design Firm License: Illinois 184-000616

CITY OF ROCKFORD & MISC. DETAILS

RIVERSIDE ARRA (SECTION 09-00560-00-RS)

WEST RIVERSIDE BOULEVARD ROCKFORD, ILLINOIS

FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 DETAILS.DWG JOB: 04-28-09-029



MINIMUM MANHOLE DIAMETERS

PIPE SIZE	MIN. DIAMETER	MIN. WALL THICKNESS
8" - 16"	4"	5"
18" - 24"	6"	5"
26" - 36"	6"	5"

(OR AS DIRECTED BY THE DISTRICT)

NOTE: THE MAXIMUM DROP FROM THE INVERT OF ANY PIPE IN A MANHOLE TO THE Poured CHANNEL UNDER THAT PIPE SHALL BE 6".

NOTE: MANHOLES LOCATED OUTSIDE PUBLIC RIGHT-OF-WAY MUST BE MARKED WITH A METAL FENCE POST.

ALL NEW MANHOLES SHALL BE VACUUM TESTED PER ASTM C 1244 PRIOR TO PLACING INTO SERVICE. EXISTING MHS WILL NOT REQUIRE VACUUM TESTING.

12" MAX., ONLY (1) - 2" ADJUSTMENT RING PER MANHOLE.

NOTE: 4" ADJUSTMENT RING NOT REQUIRED IN TURF AREA OR CURB AND GUTTER ROADWAYS.

NOTE: ALL ADJUSTMENT RING JOINTS SHALL BE SEALED WITH (2) 1" BEADS OF E-Z STIK, KENT SEAL OR EQUIVALENT ON THE SHIP LAP.

IN WET AREAS AN ADDITIONAL 1" BEAD OF E-Z STIK, KENT SEAL OR EQUIVALENT WILL BE REQUIRED ON THE LOWER HORIZONTAL PORTION OF THE SHIP LAP.

MANHOLE STRUCTURE TO BE CONSTRUCTED OF PRE-CAST REINFORCED CONCRETE MANHOLE RISER PIPE. MANHOLE CONSTRUCTION SHALL CONFORM TO A.S.T.M. C478-90 OR LATEST DESIGNATION.

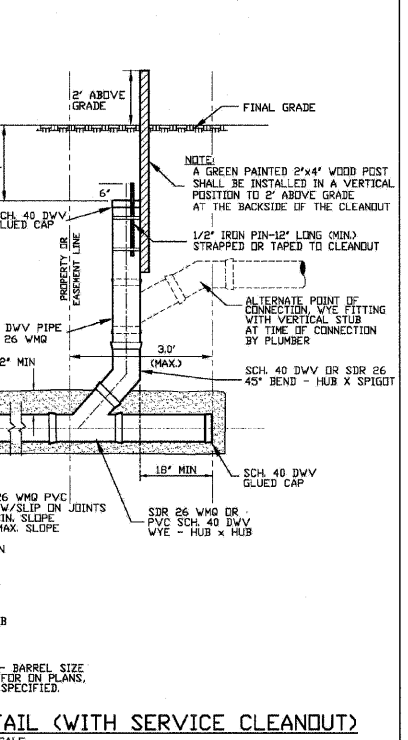
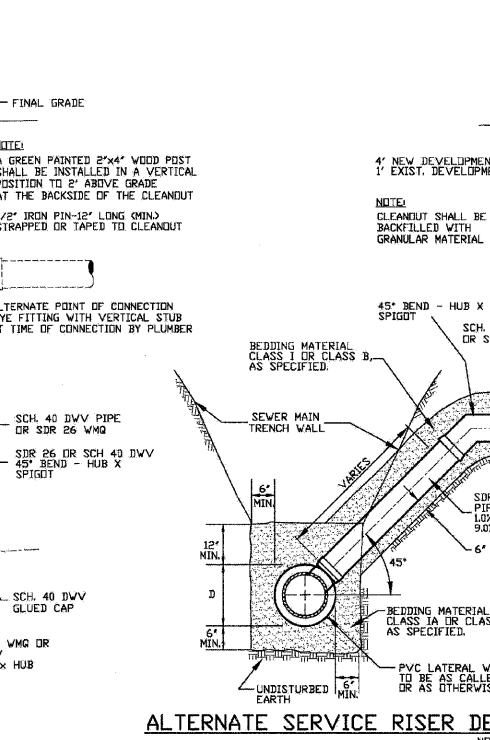
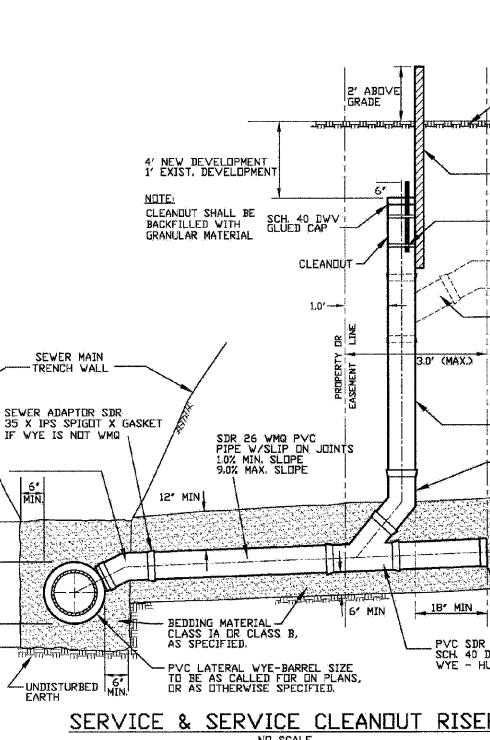
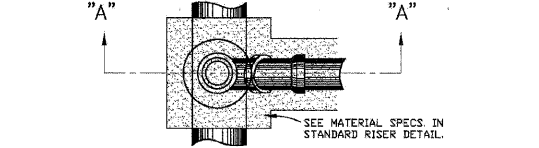
WHEN A CASTING REQUIRES PITCHING, A MINIMUM OF THREE SHIMS (PLASTIC OR METAL) EQUALLY SPACED TO PREVENT ROCKING SHALL BE INSTALLED BETWEEN THE CASTING AND THE MASONRY. HYDRAULIC CEMENT SHALL BE PLACED TO FILL THE SPACE BETWEEN THE CASTING & MASONRY.

PRE-CAST FLAT TOPS WILL NOT BE PERMITTED ON DISTRICT MANHOLES.

PIPE CONNECTION TO PROPOSED MANHOLE SHALL BE MADE WATER-TIGHT BY MEANS OF RUBBER GASKET SEAL, G-LID, OR EQUAL, CONFORMING TO A.S.T.M. F-477, CAST INTEGRALLY IN MANHOLE WALL OR RUBBER GASKET SEAL AND STAINLESS STEEL CLAMP CONFORMING TO A.S.T.M. C-923.

PIPE CONNECTION TO EXISTING MANHOLE SHALL BE MADE WATER-TIGHT BY MEANS OF RUBBER GASKET SEAL AND STAINLESS STEEL CLAMP CONFORMING TO A.S.T.M. C-923. PSX SERIES SIX OR EQUAL PIPE ACCESS SHALL BE MADE BY CORE DRILLING MANHOLE WALL.

PSX SERIES GASKETS ARE NOT PERMITTED ON MH OPENINGS OVER 20" DEEP. MH OPENINGS OVER 20" DEEP SHALL REQUIRE A-LOCK TO MANHOLE GASKET.



ROCK RIVER WATER RECLAMATION DISTRICT

3333 KISHWAUKEE ST., P.O. BOX 7480, ROCKFORD, ILLINOIS 61126-7480 - PH. (815) 387-7660/FAX (815) 387-7665

FILE: RRWR.DWG

STANDARD DETAIL SHEET

REVISIONS:

REV: 1/1/97 PAD	REV: 3/5/00 PAD	REV: 5/14/03 PAD
REV: 2/7/97 PAD	REV: 1/10/01 PAD	REV: 2/2/04 PAD
REV: 3/19/99 PAD	REV: 2/5/02 PAD	REV: 1/29/09 PAD

SCALE: N/A

DRAWN BY: TWH

CHECKED BY: PDS

DATE: 08/28/2009

McClure Engineering Associates, Inc.

7282 Argus Drive, Rockford, Illinois 61107-5837 (815) 398-2332 FAX (815) 398-2496 Design Firm License: Illinois 184-000816

RRWR DETAILS

RIVERSIDE ARRA (MFT SECTION 09-00560-00-RS)

WEST RIVERSIDE BOULEVARD, ROCKFORD, ILLINOIS

FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 DETAILS.DWG

JOB: 04-28-09-029

SHEET OF

SHEET REVIEW

AGENCY	DATE

REVISIONS

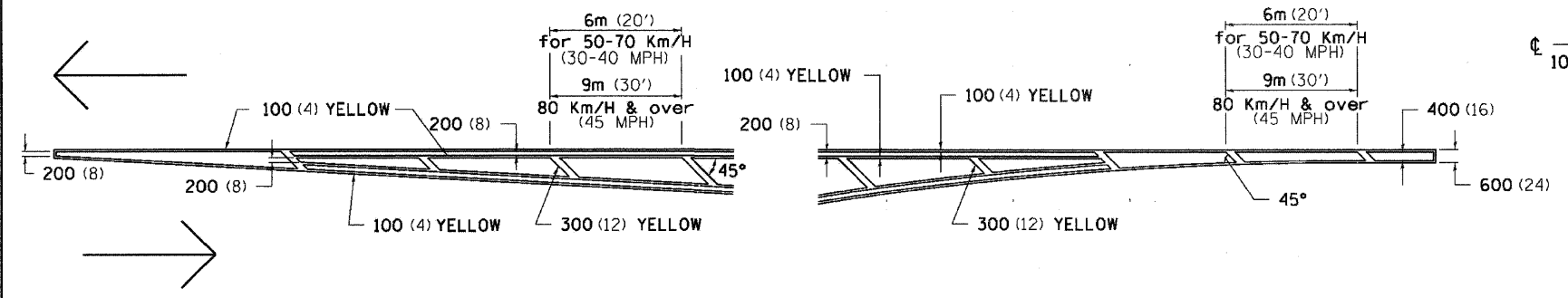
NO.	ITEM	DATE

SHEET NO.

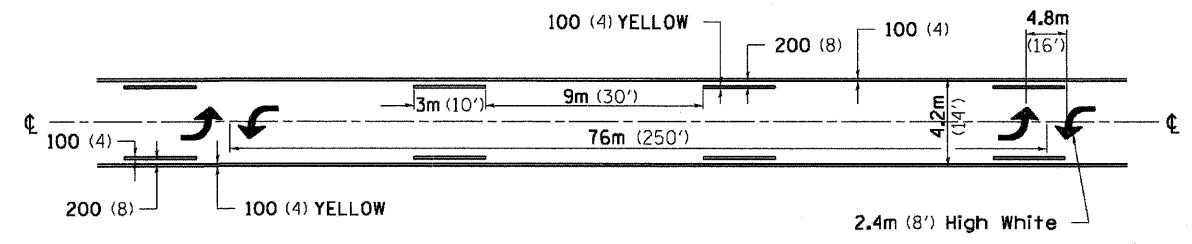
16 OF 21

TYPICAL PAVEMENT MARKINGS

TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE

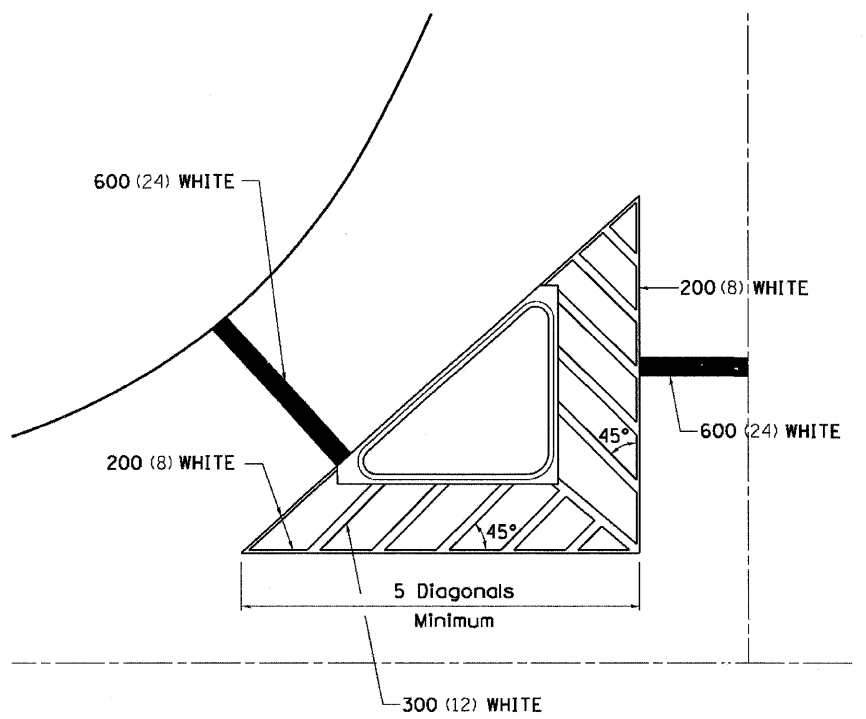


MEDIAN PAVEMENT MARKING

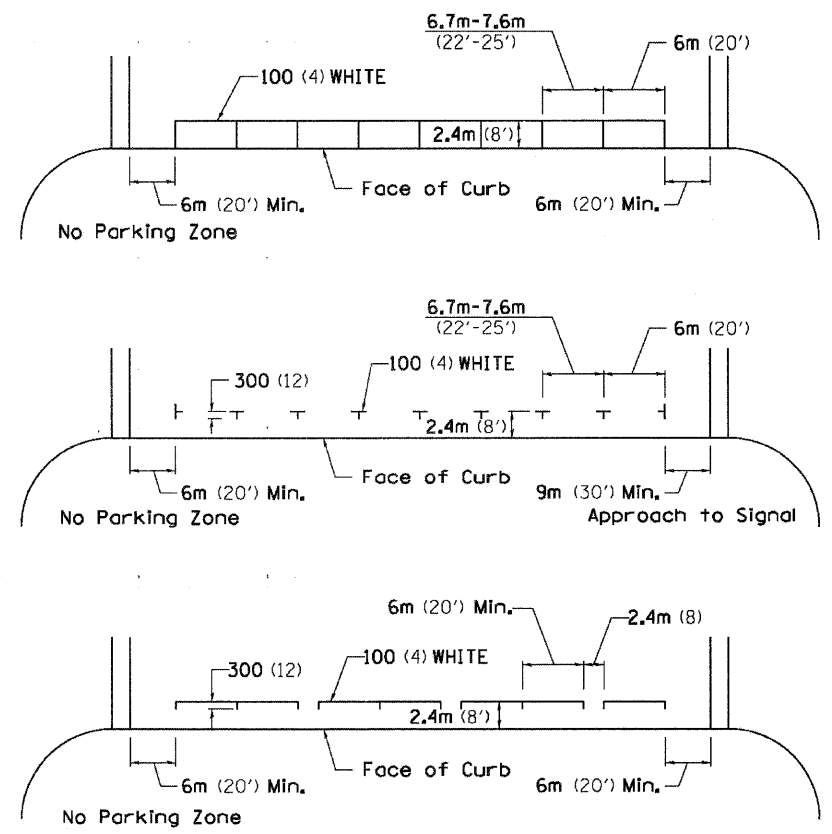


•• ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

TYPICAL ISLAND OFFSET SHOULDER WIDTH

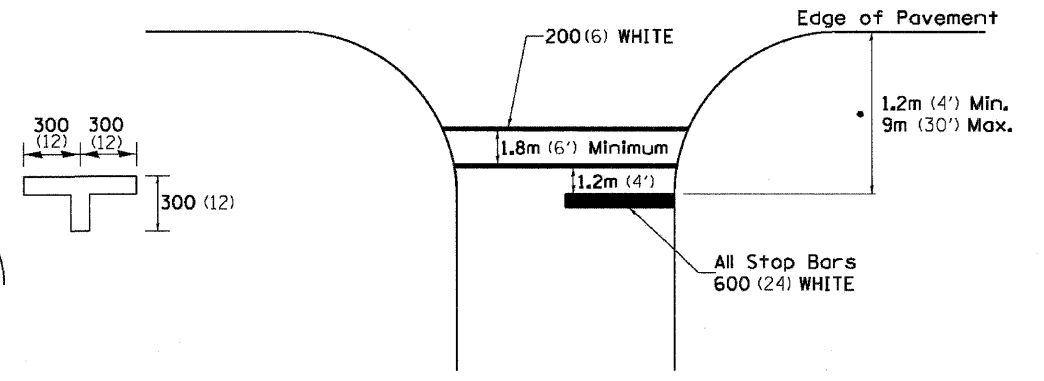


TYPICAL PARKING SPACING



STANDARD CROSSWALK MARKING

See Schedules for Locations



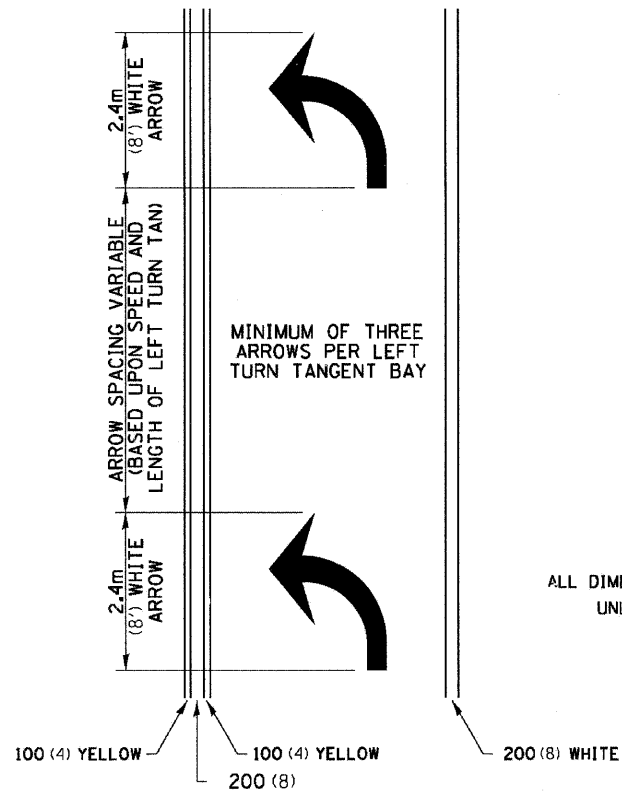
• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

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PLOT SCALE = 1:2000' / IN.		CHECKED -	REVISED -			FED. ROAD DIST. NO. _____ (ILLINOIS) FED. AID PROJECT						
PLOT DATE = Tue Oct 21 14:35:35 2008		DATE -	REVISED -									

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AGENCY	DATE															
NO.	ITEM	DATE														

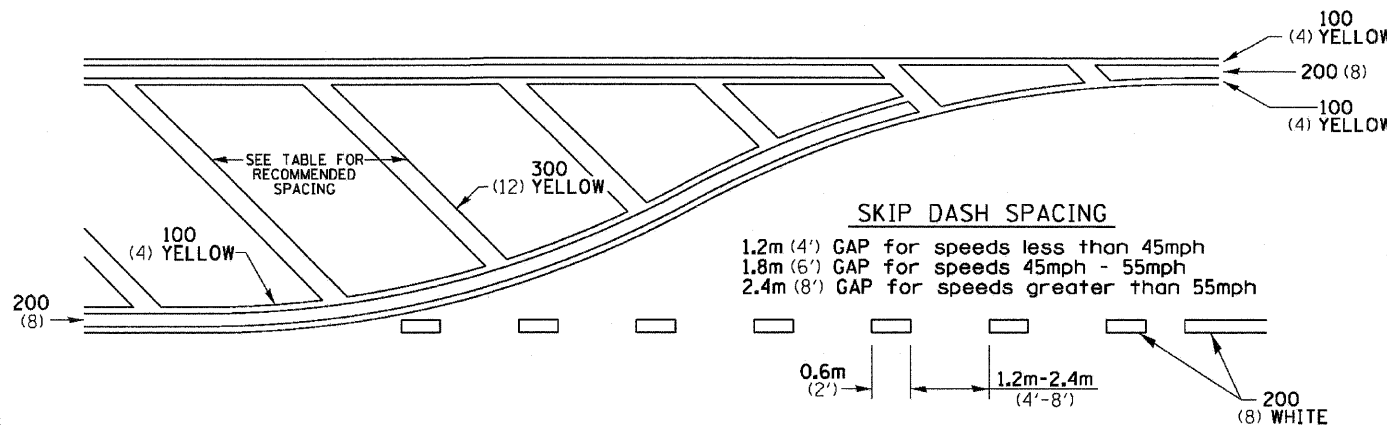
TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT



ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

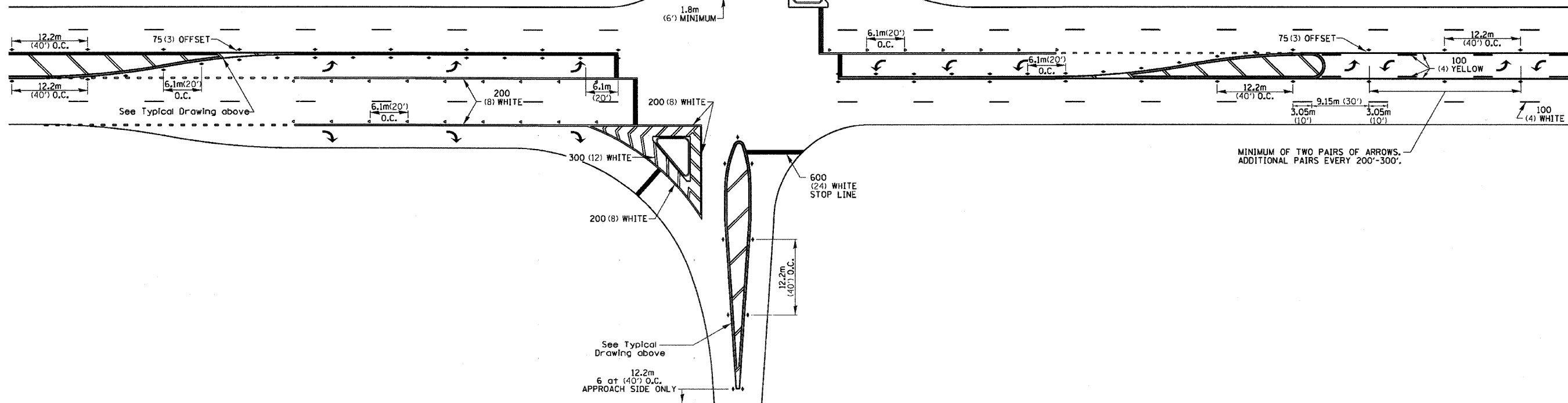
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 50Km/H (30MPH)	15.3m (50')	4.53m (15')	3.05m (10')
50-60Km/H (30-40MPH)	22.9m (75')	6.1m (20')	4.53m (15')
70Km/H (45MPH) & over	22.9m (75')	9.05m (30')	6.1m (20')

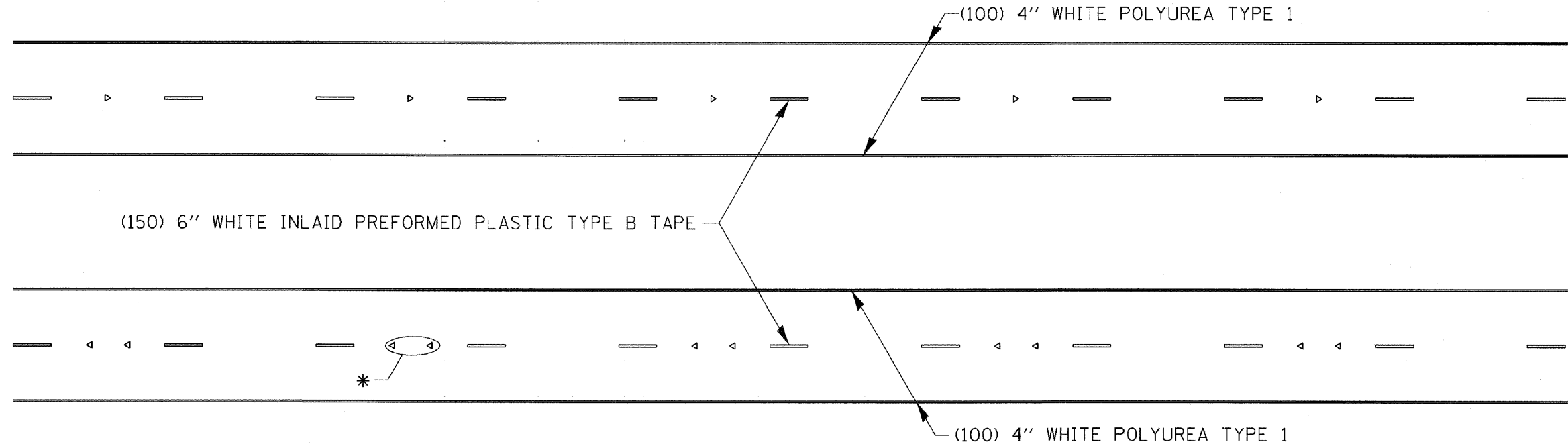
NOTE: If the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED - 1Q-21-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
C:\Documents and Settings\meg11\Desktop\ID2standards.cel		DRAWN -	REVISED -			SCALE: _____	SHEET NO. _____ OF _____ SHEETS	STA. _____ TO STA. _____	CONTRACT NO. _____			
PLOT SCALE = 1:8000 / IN.		CHECKED -	REVISED -			FED. ROAD DIST. NO. _____ [ILLINOIS] FED. AID PROJECT						
PLOT DATE = Tue Oct 21 14:36:48 2008		DATE -	REVISED -									

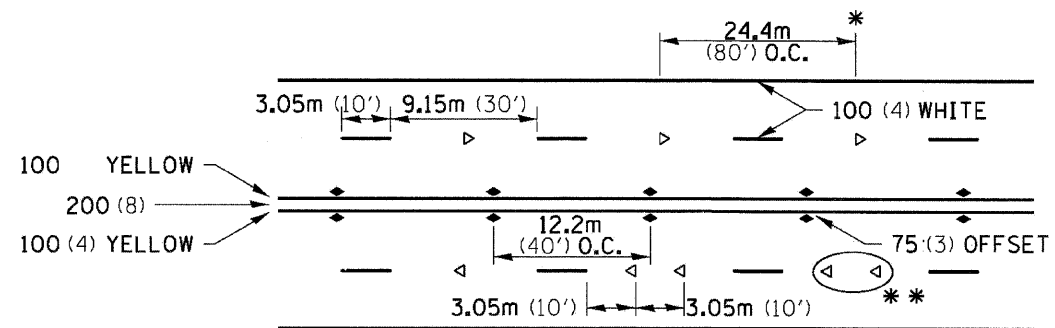
<p>SHEET REVIEW</p> <table border="1"> <tr><th>AGENCY</th><th>DATE</th></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>	AGENCY	DATE					<p>REVISIONS</p> <table border="1"> <tr><th>NO.</th><th>ITEM</th><th>DATE</th></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> </table>	NO.	ITEM	DATE							<p>SCALE: NA</p> <p>DRAWN BY: TH</p> <p>CHECKED BY: PDS</p> <p>DATE: 8/28/2009</p>	<p>McClure Engineering Associates, Inc.</p> <p>7282 Argus Drive Rockford, Illinois 61107-5837 (815) 398-2332 FAX (815) 398-2496 Design Firm License: Illinois 184-000816</p>	<p>DETAILS - IDOT-DIST2</p> <p>WEST RIVERSIDE BOULEVARD ROCKFORD, ILLINOIS</p> <p>FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 DETAILS.DWG JOB: 04-28-09-029</p>	<p>SHEET NO.</p> <p>18 OF 21</p>
AGENCY	DATE																			
NO.	ITEM	DATE																		

TYPICAL PAVEMENT MARKINGS



* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.
USE DOUBLE MARKERS WHEN ADT \geq 25,000.

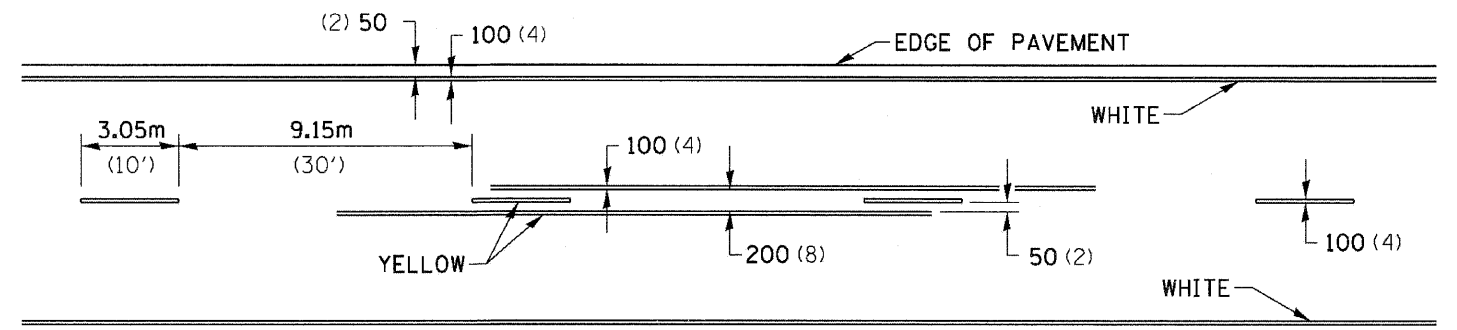
MULTI-LANE / DIVIDED



- REDUCE TO 12.2m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 15Km/H (10MPH) LOWER THAN POSTED SPEEDS.
- USE DOUBLE MARKERS WHEN ADT \geq 25,000

MULTI-LANE / UNDIVIDED

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES



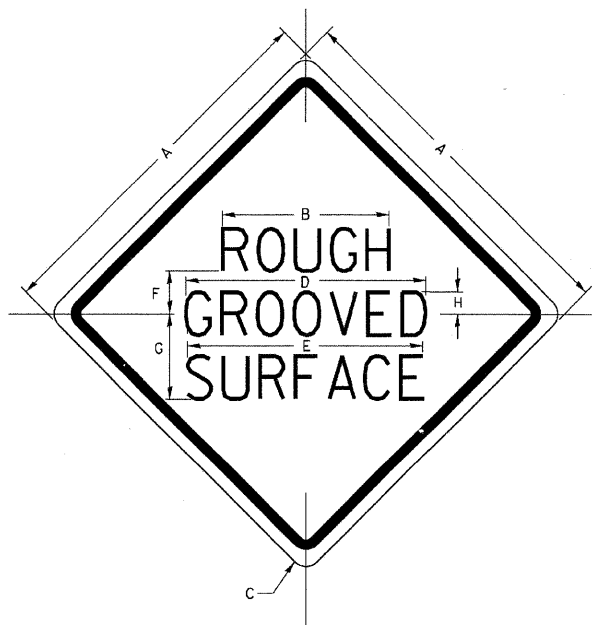
SYMBOLS

FILE NAME =	USER NAME = #USER#	DESIGNED - ---	REVISED - 10-21-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
C:\Documents and Settings\meg11\Desktop\02Standards.oel	DRAWN - ---	REVISED - ---	REVISED - ---			SCALE: _____	SHEET NO. ____ OF ____ SHEETS	STA. _____ TO STA. _____	CONTRACT NO. _____			
PLOT SCALE = 1:2000' / IN.	CHECKED - ---	REVISED - ---	REVISED - ---			FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT						
PLOT DATE = Tue Oct 21 14:37:09 2008	DATE - -----	REVISED - ---	REVISED - ---									

SHEET REVIEW	AGENCY		DATE		REVISIONS	NO.	ITEM	DATE	SCALE: NA		DETAILS - IDOT-DIST2		SHEET NO. 19 OF 21
	DRAWN BY: TH	WEST RIVERSIDE BOULEVARD		ROCKFORD, ILLINOIS									
CHECKED BY: PDS	FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 DETAILS.DWG		JOB: 04-28-09-029		DATE: 8/28/2009	7282 Argus Drive Rockford, Illinois 61107-5837 (815) 398-2332 FAX (815) 398-2496 Design Firm License: Illinois 184-000816							

ROUGH GROOVED SURFACE SIGN

ILLINOIS STANDARD W8-I107
SIGN PANEL TYPE 1



COLOR: LEGEND AND BORDER - BLACK NON-REFLECTIVE
BACKGROUND - ORANGE REFLECTORIZED

SIGN SIZE	DIMENSIONS							
	A	B	C	D	E	F	G	H
1200x1200 (48x48)	1200 (48.0)	600 (24.1)	75 (3.0)	850 (34.0)	825 (33.0)	150 (6.0)	325 (13.0)	88 (3.5)

SIGN SIZE	SERIES LINES			MARGIN	BORDER	BLANK STD.
	1	2	3			
1200x1200 (48x48)	7C	7C	7C	20 (0.8)	30 (1.2)	B4-48D

ALL DIMENSIONS IN INCHES.

GENERAL NOTES

SIGN PANELS AND FACE MATERIALS SHALL BE ACCORDING TO SECTION 720 OF THE STANDARD SPECIFICATIONS
METAL POSTS SHALL BE IN ACCORDANCE WITH STD. 720011.

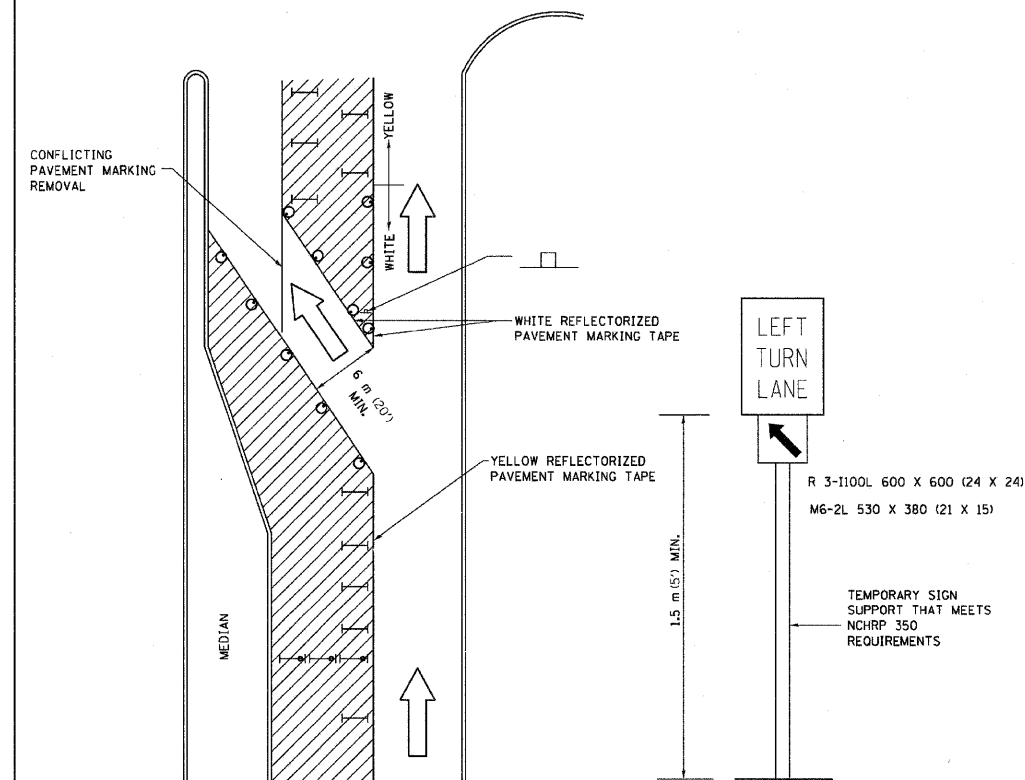
ALL MOUNTING HARDWARE SHALL BE ALUMINUM, STAINLESS STEEL, ZINC OR CADMIUM PLATED STEEL AND SHALL BE INCLUDED TO THE COST OF THE INSTALLATION.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 1-09-08	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	REGION 2 / DISTRICT 2 STANDARD				
REVISED -	CONTRACT NO.				
REVISED -	SCALE: 1/8" = 1'-0"	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

ROUGH GROOVED SURFACE SIGN 91.2

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)



LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE OR DRUM WITH FLASHING BURNING LIGHT
- DRUM OR BARRICADE WITH STEADY BURN LIGHT
- SIGN (SEE DETAIL)
- TYPE I OR II CHECK BARRICADE WITH STEADY LIGHT BURN

GENERAL NOTES

CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT.
STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS WILL BE MONODIRECTIONAL.
REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 10-15-04	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	REGION 2 / DISTRICT 2 STANDARD				
REVISED -	CONTRACT NO.				
REVISED -	SCALE: 1/8" = 1'-0"	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) 94.2

SHEET REVIEW

AGENCY	DATE

REVISIONS

NO.	ITEM	DATE

SCALE: NA
DRAWN BY: TH
CHECKED BY: PDS
DATE: 8/28/2009

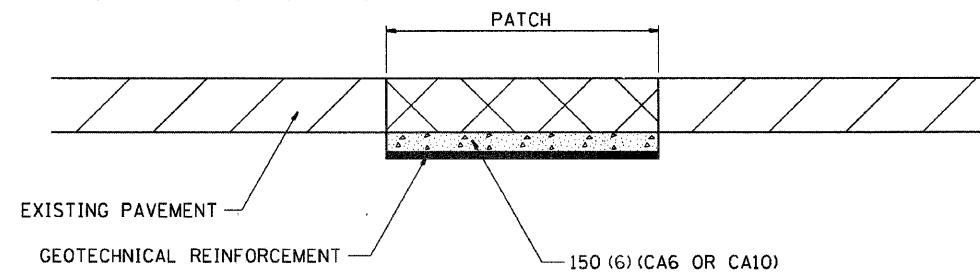
McClure
Engineering Associates, Inc.
7282 Argus Drive Rockford, Illinois 61107-5837
(815) 398-2332 FAX (815) 398-2496
Design Firm License: Illinois 184-000816

DETAILS - IDOT-DIST2

RIVERSIDE ARRA (MFT SECTION 09-00560-00-RS) ROCKFORD, ILLINOIS
WEST RIVERSIDE BOULEVARD
FILE: H:\09-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\09-029 DETAILS.DWG
JOB: 04-28-09-029

SHEET NO.
20
OF
21

SUBGRADE REPLACEMENT



NOTES:

THE CA 6 OR CA 10 SHALL BE COMPACTED IN A MANNER APPROVED BY THE ENGINEER. IF THE MOISTURE CONTENT OF THE MATERIAL IS SUCH THAT COMPACTION SATISFACTORY TO THE ENGINEER CANNOT BE OBTAINED, SUFFICIENT WATER SHALL BE ADDED SO THAT SATISFACTORY COMPACTION CAN BE OBTAINED.

THE CA 6 OR CA 10 WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CU YD FOR GRANULAR SUBGRADE REPLACEMENT

THE GEOTECHNICAL REINFORCEMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ YD FOR GEOTECHNICAL REINFORCEMENT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 1-09-08	REGION 2 / DISTRICT 2 STANDARD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
REVISED - ---		-----	-----	-----	-----	-----	
REVISED - ---		CONTRACT NO. -----					
REVISED - ---		SCALE: 1:8450 (1/4" = 1')	SHEET NO. ___ OF ___ SHEETS	STA. _____ TO STA. _____	FED. ROAD DIST. NO. _____	ILLINOIS FED. AID PROJECT	

PLOT DATE = Tue Jan 15 14:26:11 2008

SUBGRADE REPLACEMENT 97.4

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE: NA
DRAWN BY: TH
CHECKED BY: PDS
DATE: 8/28/2009

McClure
Engineering Associates, Inc.

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(815) 398-2332

Rockford, Illinois 61107-5837
FAX (815) 398-2496

Design Firm License: Illinois 184-000816

DETAILS - IDOT-DIST2	
RIVERSIDE ARRA (MFT SECTION 09-00560-00-RS)	ROCKFORD, ILLINOIS
WEST RIVERSIDE BOULEVARD	FILE: H:\109-029 RIVERSIDE ARRA\DESIGN\DRAWINGS\109-029 DETAILS.DWG
	JOB: 04-28-09-029

SHEET NO.
21
OF
21