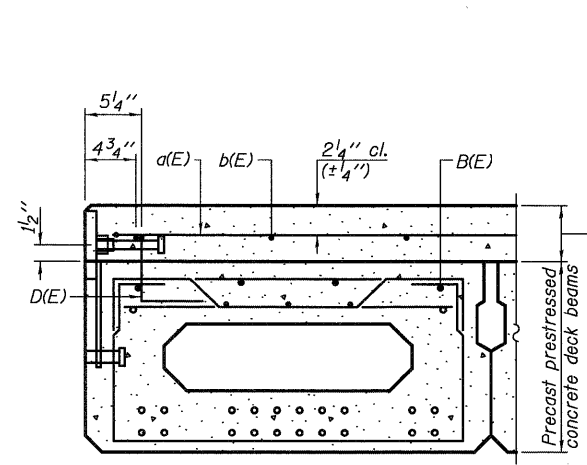
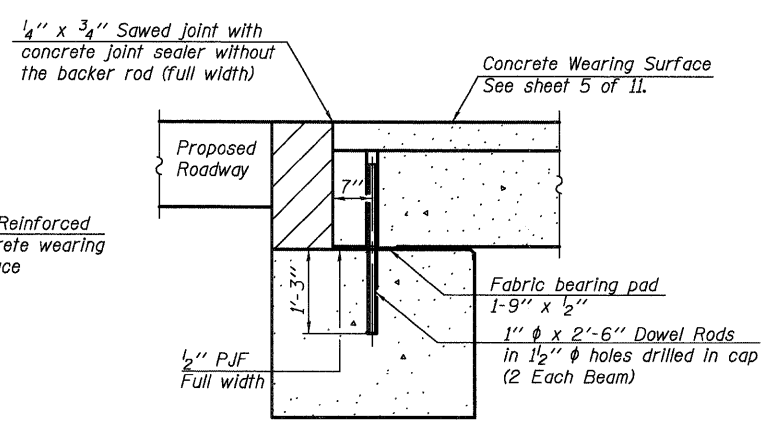


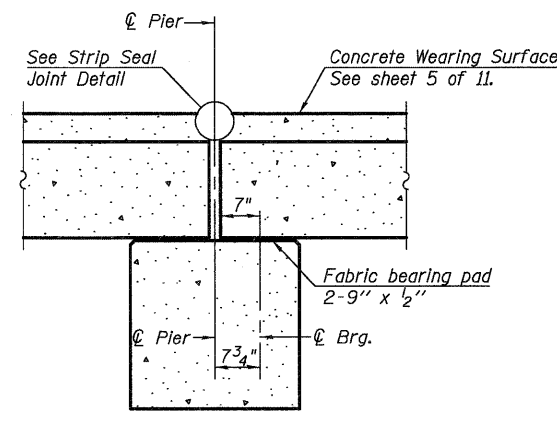
I:\jobs\devitt county\7211 ch 10 over n fork salt creek\cadd\CADD Sheets\0203900-000000.dgn



CROSS SECTION

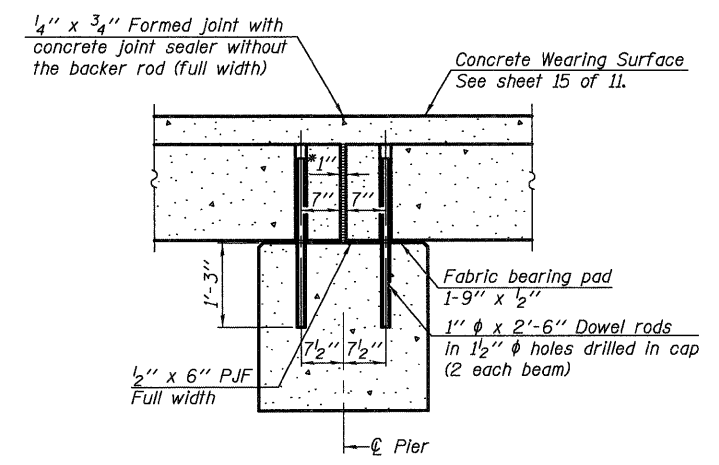


SECTION THRU ABUTMENT



SECTION THRU PIERS 1 & 3

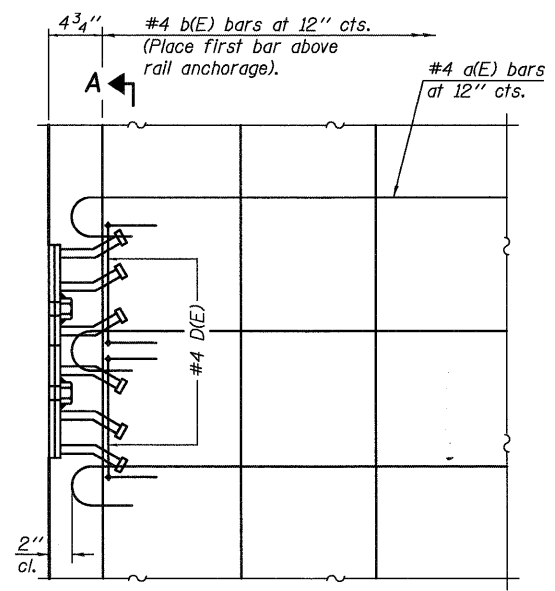
Concrete sealer shall be applied to all surfaces of Piers 1 and 3.



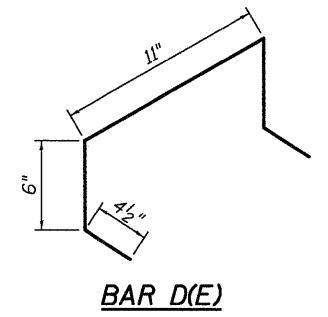
SECTION THRU PIER 2

*1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beam lengths.

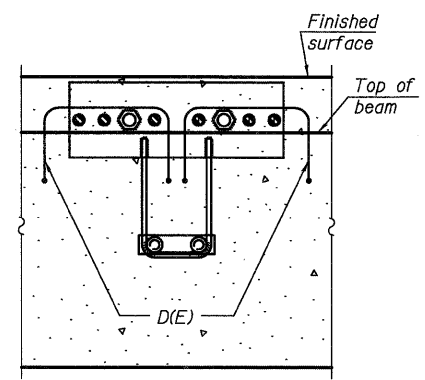
Notes:
 After beams have been erected, holes shall be drilled into substructure and anchor dowels placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.
 All horizontal dimensions are at right angles to beam ends. Hatched area to be poured after beams are in place. See sheet 3 of 11 for bearing pad details.



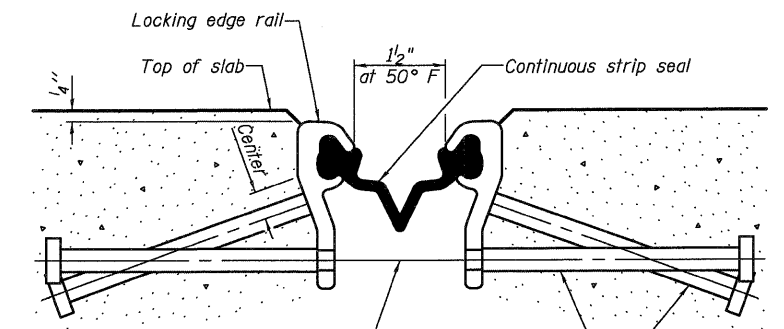
PLAN



BAR D(E)



SECTION A-A

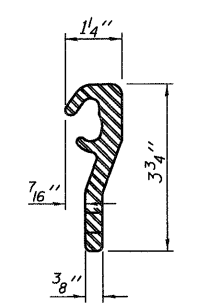


SECTION THRU STRIP SEAL JOINT FOR OVERLAY OVER DECK BEAMS

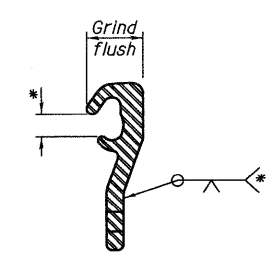
7/16" dia holes at 4'-0" cts. for 3/8" dia bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.
 Place 1/2" dia x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

Notes:
 The rail anchorage and bar D(E) shall be cast with the beam and the wearing surface shall be cast in the field. Cost of D(E) bar and rail anchorages are included with Precast Prestressed Concrete Deck Beams (21" Depth).
 Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam.
 Drilling into the beam will not be permitted.

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.
 The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.
 The inside of the Locking Edge Rail groove shall be free of weld residue. The manufacturer's recommended installation methods shall be followed.
 The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



LOCKING EDGE RAIL



LOCKING EDGE RAIL SPLICE

*Omit weld at seal opening.

USER NAME = dheber-ling	DESIGNED - FLL	REVISED
PLOT SCALE = @ 1/16" = 1' IN.	DRAWN - DLH	REVISED
PLOT DATE = 8/18/2009	CHECKED - CWC	REVISED
	DATE -	REVISED

WHKS & CO.
 ENGINEERING
 7018 KINGSMILL CT.,
 SPRINGFIELD, IL
 (217) 483-9457
 DESIGN FIRM #184001036

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
CH 10 OVER NORTH FORK SALT CREEK
 SCALE: SHEET NO. 4 OF 11 SHEETS

F.A.S. RTE. 543	SECTION 09-00025-01-BR	COUNTY DEWITT	TOTAL SHEETS 17	SHEET NO. 10
S.N. 020-3900		CONTRACT NO. 91423		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		