

SUMMARY OF SHEETS

| SHEET NO | CONTENT |
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| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES, TYPICAL SECTIONS, AND PAVEMENT DESIGN |
| 3 | PAVEMENT MARKING AND CROSSROAD CULVERT SCHEDULES |
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| 6 | CROSSROAD CULVERT CROSS-SECTIONS |

HIGHWAY STANDARDS

BLR-24-2, 442201-03 701036-03, 701011-02,
701201-03, 701301-03, 701306-02, 701311-03,
701901-01, 780001-02, 781001-03

UTILITIES

EGYPTIAN ELECTRIC COOP 1005 W. BROADWAY STEELEVILLE, IL 62288 PH. 618-965-3437
EGYPTIAN TELEPHONE COOP 1010 W. BROADWAY STEELEVILLE, IL 62288 PH. 618-965-3481

AMEREN IP PH. 618-443-9239
EGYPTIAN WATER 1005 W. BROADWAY STEELEVILLE, IL 62288 PH. 618-965-3434

VERIZON SERVICE 1-800-483-5000 REPAIR

J.U.L.I.E. 1-800-892-0123

DESIGN INFORMATION

RURAL COLLECTOR
CLASS III ROADWAY
DESIGN SPEED 50 MPH
ADT(2009) - 1200
(2024) - 825

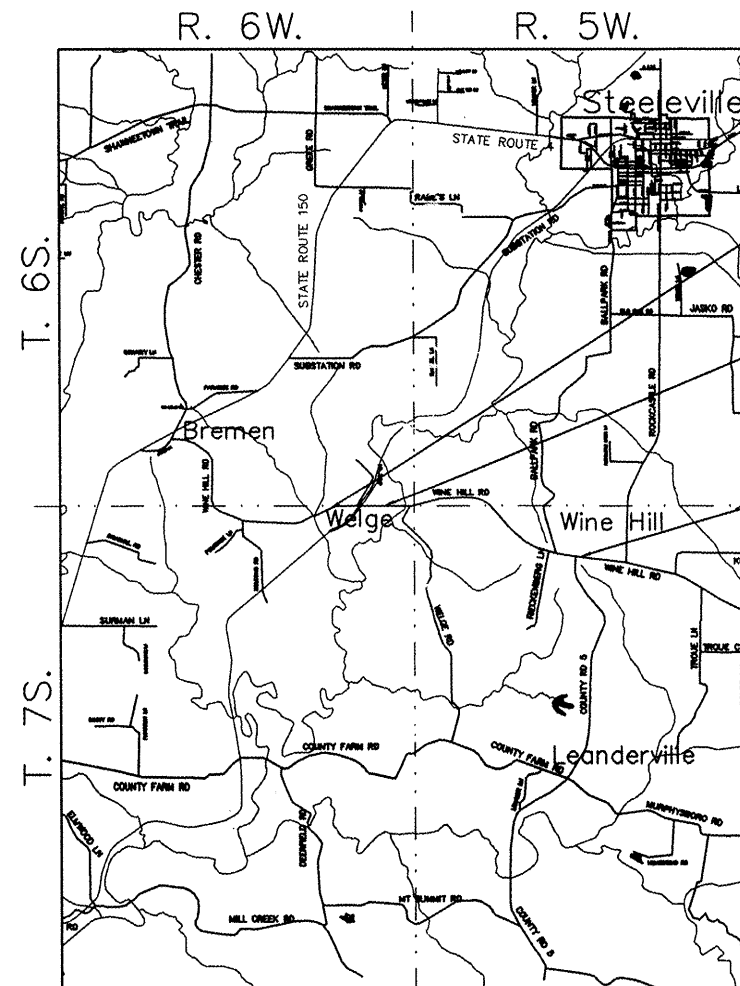
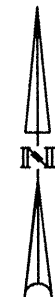
PAVEMENT DESIGN INFORMATION

STRUCTURAL DESIGN TRAFFIC: 7880 (YEAR 2017) PV 693
SU 55
MU 40

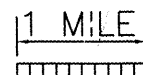
MINIMUM SOIL SUPPORT IFR=3
PAVEMENT STRUCTURAL MATERIALS: HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PLANS FOR
PROPOSED LOCAL AGENCY IMPROVEMENT
RANDOLPH COUNTY
FAS ROUTE 1870
SECTION 09-00035-07-RS
SECTION 09-00033-03-RS
PROJECT NO. ARA-1870(108)
JOB NO. C-98-387-09
ARR FUNDING

| ROUTE | COUNTY | SECTION | SHEET/OF |
|---------------------|----------|----------------|----------|
| FAS 1870 | RANDOLPH | 09-00035-07-RS | 1/6 |
| CONTRACT NO. 97396 | | | |
| FEDERAL AID PROJECT | | | |



3RD P.M.



NET LENGTH OF IMPROVEMENT
15,100 FEET = 2.86 MILES

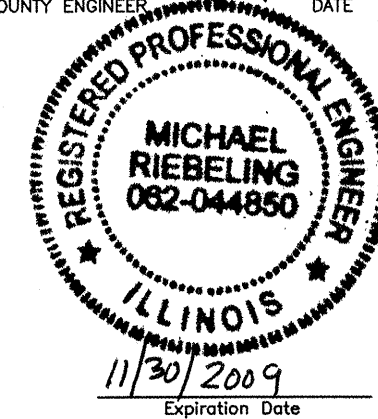
SITE OF PROPOSED IMPROVEMENT

SECTION 09-0003-03-RS
PROPOSED IMPROVEMENT BEGINS
STATION 116+00

SECTION 09-0003-03-RS ENDS
STATION 151+00
SECTION 09-00035-07-RS BEGINS
STATION 0+00

SECTION 09-00035-07-RS &
PROJECT ENDS
STATION 116+00

THESE PLANS WERE PREPARED BY ME OR A MEMBER OF MY STAFF WORKING UNDER MY PERSONAL SUPERVISION.
Michael Riebeling 8/27/2009
COUNTY ENGINEER DATE



| ILLINOIS DEPARTMENT OF TRANSPORTATION | |
|---|---|
| Approved | August 27 2009 <i>Michael Riebeling</i> Randolph County, County Engineer |
| Passed | 9/15 2009 <i>[Signature]</i> District 8 Engineer of Local Roads & Streets |
| Releasing for Bid Based on Limited Review | 9/15 2009 <i>[Signature]</i> Deputy Director of Highways, Region 5 Engineer |

SUMMARY OF QUANTITIES

| | | | |
|--------------------|----------|----------------|----------|
| ROUTE | COUNTY | SECTION | SHEET/OF |
| FAS 1870 | RANDOLPH | 09-00035-07-RS | 2/6 |
| CONTRACT NO. 97396 | | | |

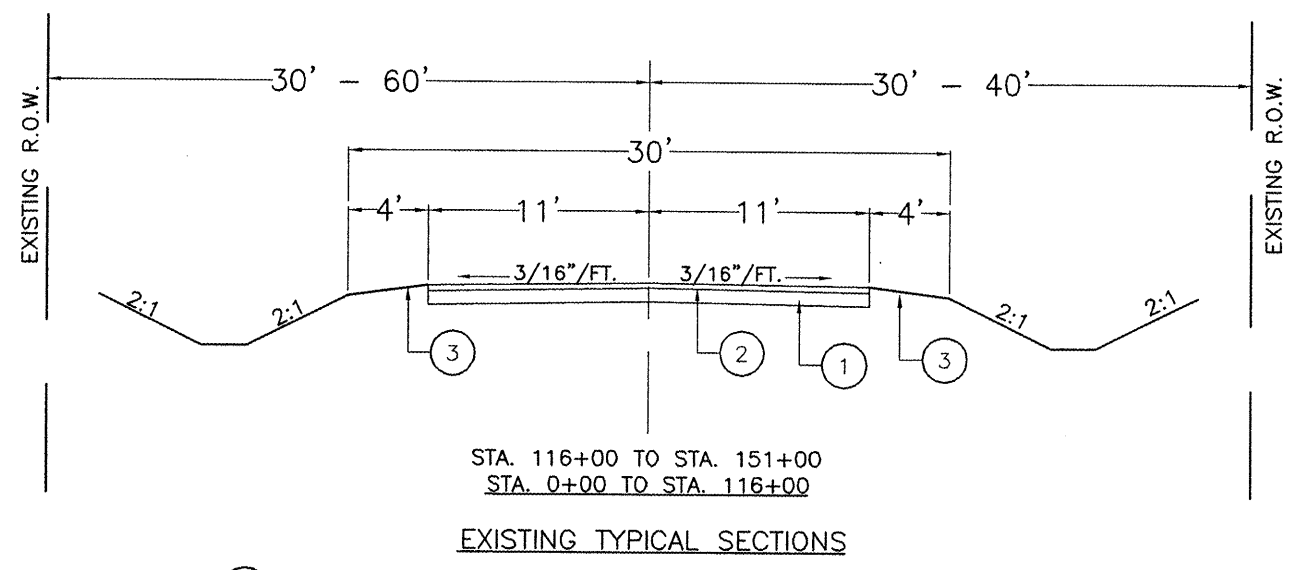
| PAY ITEM | ITEM | UNIT | QUANTITY |
|-----------|--|--------|----------|
| 28100807 | STONE DUMPED RIP RAP, CLASS A4 | TON | 33 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 2,768 |
| 40600635 | LEVELING BINDER (MACHINE METHOD), N70 | TON | 2,512 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 196 |
| 40603315 | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70 | TON | 2,700 |
| 44000153 | HOT-MIX ASPHALT SURFACE REMOVAL, 1" | SQ YD | 971 |
| 44201713 | CLASS D PATCHES, TYPE I, 6 INCH | SQ YD | 13.30 |
| 44201717 | CLASS D PATCHES, TYPE II, 6 INCH | SQ YD | 75.10 |
| 44201721 | CLASS D PATCHES, TYPE III, 6 INCH | SQ YD | 34.20 |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 950 |
| 54010504 | PRECAST CONCRETE BOX CULVERT 5'X4' | FOOT | 66 |
| 54200439 | PIPE CULVERTS, TYPE 1 RCCP 24" | FOOT | 60 |
| 54201273 | PIPE CULVERTS, TYPE 2 RCCP 18" | FOOT | 66 |
| 54201285 | PIPE CULVERTS, TYPE 2 RCCP 30" | FOOT | 82 |
| 54201291 | PIPE CULVERTS, TYPE 2 RCCP 36" | FOOT | 67 |
| 54201303 | PIPE CULVERTS, TYPE 2 RCCP 48" | FOOT | 77 |
| 54202119 | PIPE CULVERTS, TYPE 3 RCCP 24" | FOOT | 88 |
| 54202953 | PIPE CULVERTS, TYPE 4 RCCP 18" | FOOT | 47 |
| 67100100 | MOBILIZATION | L SUM | 1 |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 3,018 |
| 70300200 | TEMPORARY PAVEMENT MARKING | FOOT | 55,396 |
| *78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 122 |
| *78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 66 |
| *78001110 | PAINT PAVEMENT MARKING - LINE 4" | FOOT | 55,396 |
| *78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 206 |
| *78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 3 |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1 |

* DENOTES SPECIALTY ITEM.

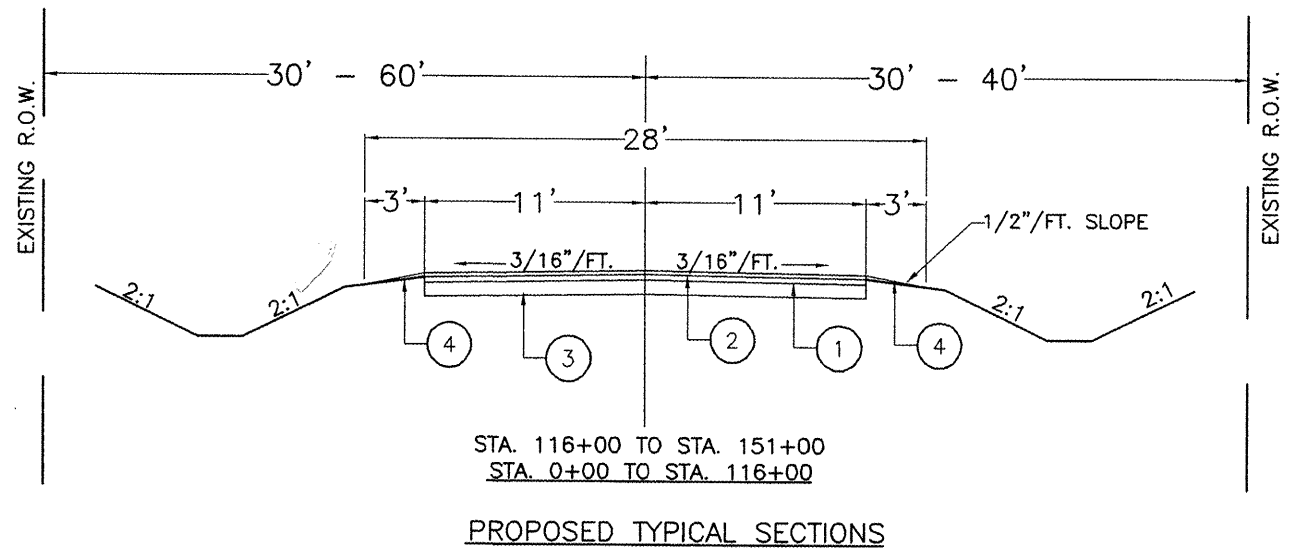
GENERAL NOTES:

1. BITUMINOUS MATERIALS (PRIME COAT) SHALL BE APPLIED AT THE RATE OF 0.075 GALLONS PER SQUARE YARD. EMULSIFIED ASPHALT SHALL BE USED.
2. SHORT-TERM PAVEMENT MARKING APPLIED TO LEVELING BINDER AND FINAL SURFACE
3. PROPOSED PAVEMENT MARKINGS SHALL MATCH EXISTING PAVEMENT MARKINGS.

| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | | |
|--------------------------------------|----------------|-----------------|
| MIXTURE USE | SURFACE | LEVELING BINDER |
| PG | PG 64-22 | PG 64-22 |
| RAP %(MAX) | 10 % | 15 % |
| DESIGN AIR VOIDS | 4.0% @ Ndes=70 | 4.0 % @ Ndes=70 |
| MIX COMPOSITION (GRADATION MIXTURE) | IL 9.5 | IL 9.5 |
| FRICTION AGG | MIXTURE C | |
| MIXTURE WEIGHT | 112 LB/SY/IN | 112 LB/SY/IN |



- ① EXISTING AGGREGATE BASE - 8"
- ② EXISTING BITUMINOUS MIXTURE COMPLETE - 2"
- ③ EXISTING TURF SHOULDER



- ① PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 1 1/4"
- ② PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70 - 1 1/4"
- ③ EXISTING AGGREGATE BASE COURSE - 8"
- ④ PROPOSED AGGREGATE SHOULDERS, TYPE B

PAINT PAVEMENT MARKING SCHEDULE

| LOCATION | LIMITS | LENGTH |
|---|----------------------------|---------------------------------|
| <u>NO PASSING ZONES</u> | | |
| LEFT | STA. 116+00 TO STA. 140+32 | 2432 FT |
| RIGHT | STA. 116+00 TO STA. 125+30 | 930 FT |
| RIGHT | STA. 133+40 TO STA. 136+82 | 342 FT |
| RIGHT | STA. 141+90 TO STA. 151+00 | 910 FT |
| RIGHT | STA. 0+00 TO STA. 33+37 | 3337 FT |
| LEFT | STA. 1+70 TO STA. 44+87 | 4317 FT |
| RIGHT | STA. 38+50 TO STA. 73+67 | 3517 FT |
| LEFT | STA. 49+75 TO STA. 86+20 | 3645 FT |
| RIGHT | STA. 83+00 TO STA. 108+95 | 2595 FT |
| LEFT | STA. 94+05 TO STA. 116+00 | <u>2195 FT</u> |
| TOTAL NO PASSING ZONES | | 24,220 FT |
| <u>SKIP DASH</u> | | |
| | STA. 125+30 TO STA. 133+40 | 810 FT. |
| | STA. 136+82 TO STA. 151+00 | 1418 FT. |
| | STA. 0+00 TO STA. 1+70 | 170 FT. |
| | STA. 33+37 TO STA. 38+50 | 513 FT. |
| | STA. 44+87 TO STA. 49+75 | 488 FT. |
| | STA. 73+67 TO STA. 83+00 | 933 FT. |
| | STA. 86+20 TO STA. 94+05 | 785 FT. |
| | STA. 108+95 TO STA. 116+00 | <u>705 FT.</u> |
| | | 5,822 FT @ 25%= <u>1,456 FT</u> |
| <u>TOTAL YELLOW LINE</u> | | 25,676 FT |
| <u>EDGE LINE</u> | | |
| LT. & RT. | STA. 116+00 TO STA. 151+00 | 6,880 FT |
| LT. & RT. | STA. 0+00 TO STA. 116+00 | 22,840 FT |
| (INCLUDES 60 FT GAP AT EACH OF 4 SIDEROADS = -240') | | |
| <u>TOTAL PAINT PAVEMENT MARKING LINE 4"</u> | | <u>55,396 FT</u> |

| | | | |
|--------------------|----------|----------------|----------|
| ROUTE | COUNTY | SECTION | SHEET/OF |
| FAS 1870 | RANDOLPH | 09-00035-07-RS | 3/6 |
| CONTRACT NO. 97396 | | | |

| THERMOPLASTIC PAVEMENT MARKING SCHEDULE | | | | |
|---|-----------|-----------------------|--------|--------------|
| LOCATION | LIMITS | SQUARE FEET | LENGTH | |
| | | <u>LTRS & SYM</u> | | <u>LINE</u> |
| RIGHT | STA 32+60 | | | 11 FT |
| RIGHT | STA 32+85 | LETTERS AND SYMBOLS | 61 SF | |
| RIGHT | STA 33+10 | | | 11 FT |
| RIGHT | STA 36+10 | | | 11 FT |
| LEFT | STA 37+00 | | | 11 FT |
| LEFT | STA 40+00 | | | 11 FT |
| LEFT | STA 40+25 | LETTERS AND SYMBOLS | 61 SF | |
| LEFT | STA 40+50 | | | 11 FT |
| <u>TOTAL=</u> | | <u>122 SF</u> | | <u>66 FT</u> |

CROSSROAD PIPE SCHEDULE

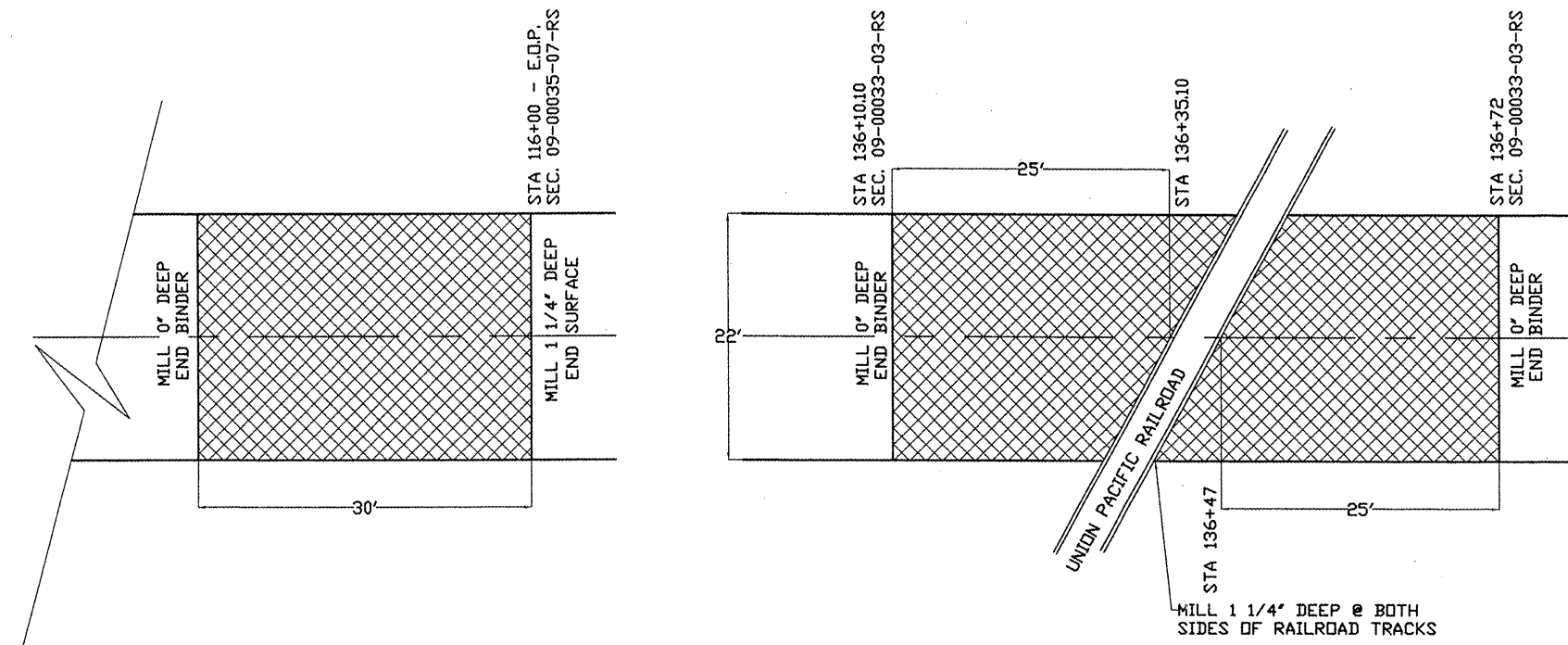
| STATION NO. | LENGTH LT OF C | C TO E LT. | LENGTH RT OF C | C TO E RT. | SKEW ANGLE | PROPOSED CULVERT SIZE | TYPE | TOTAL PIPE LENGTH | EXISTING PIPE | CLASS D PATCH (SQ. YD.) | | | | TRENCH WIDTH |
|------------------------|----------------|------------|----------------|------------|------------|-----------------------|------|-------------------|----------------|-------------------------|------|------------|------|--------------|
| | | | | | | | | | | AREA (LT.) | TYPE | AREA (RT.) | TYPE | |
| SECTION 09-00033-03-RS | | | | | | | | | | | | | | |
| 135+94 | 35' | 4.32 | 25' | 4.14 | 25° LA | 24" RCCP | I | 60' | 24" EQUIV. CMP | 5.4 | II | 5.4 | II | 4' |
| 144+86 | 35' | 6.99 | 31' | 6.27 | 50° LA | 5'X4' BOX CULVERT | | 66' | 36" CMP | 17.1 | III | 17.1 | III | 9' |
| SECTION 09-00035-07-RS | | | | | | | | | | | | | | |
| 39+45 | 22' | 3.39 | 25' | 4.72 | 18° LA | 18" RCCP | IV | 47' | 18" CMP | 4.5 | I | 4.5 | I | 3.5' |
| 55+35 | 33' | 9.02 | 44' | 16.17 | 0° | 48" RCCP | II | 77' | 48" CMP | 8.0 | II | 8.0 | II | 6.5' |
| 62+50 | 28' | 6.92 | 39' | 11.12 | 0° | 36" RCCP | II | 67' | 36" CMP | 6.7 | II | 6.7 | II | 5.5' |
| 96+27 | 47' | 15.62 | 41' | 9.18 | 28° RA | 24" RCCP | III | 88' | 24" CMP | 5.5 | II | 5.5 | II | 4' |
| 102+80 | 50' | 17.05 | 32' | 7.61 | 0° | 30" RCCP | II | 82' | 30" CMP | 6.1 | II | 6.1 | II | 5' |
| 107+26 | 36' | 9.55 | 30' | 8.03 | 0° | 18" RCCP | II | 66' | 18" CMP | 4.3 | I | 11.7 | II | 3.5' |

SUPER ELEVATION DATA

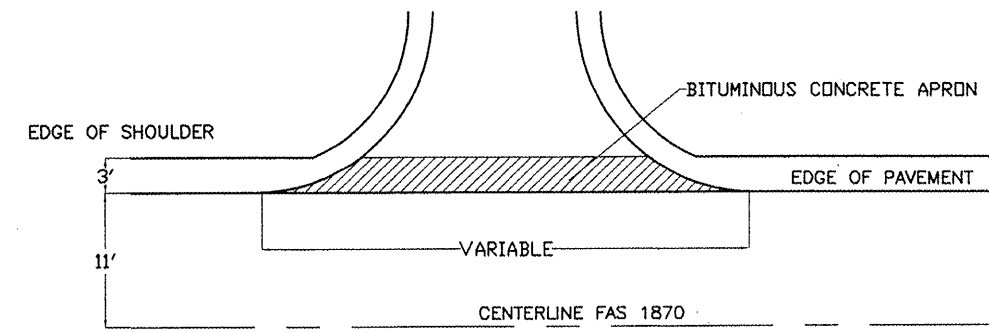
| | | | | | | | |
|---|--|--|--|---|--|--|--|
| <p>CURVE 1 (RIGHT)</p> <p>D= 4'30' I= 29'34" L= 657.03 T= 336.02 R= 1273.57 PC 119+11.60 PI 122+47.62 PT 125+68.63 S.E.(EXIST.) = 0.048 FT/FT S.E.(PROP.) = 0.07 FT/FT</p> | <p>TRANSITION IN STA. 117+71.35 TO 119+58.35</p> <p>TRANSITION OUT STA. 125+21.88 TO 127+08.88</p> | <p>LEFT IN</p> <p>STA 117+71.35 -0.015 STA 118+04.35 0.0 STA 118+37.35 +0.015 STA 118+97.85 +0.0425 STA 119+58.35 +0.07</p> <p>LEFT OUT</p> <p>STA 125+21.88 +0.07 STA 125+82.38 +0.0425 STA 126+42.88 +0.015 STA 126+75.88 0.0 STA 127+08.88 -0.015</p> | <p>RIGHT IN</p> <p>STA 117+71.35 +0.015 STA 118+04.35 +0.015 STA 118+37.35 +0.015 STA 118+97.85 +0.0425 STA 119+58.35 +0.07</p> <p>RIGHT OUT</p> <p>STA 125+21.88 +0.07 STA 125+82.38 +0.0425 STA 126+42.88 +0.015 STA 126+75.88 +0.015 STA 127+08.88 +0.015</p> | <p>CURVE 6 (LEFT)</p> <p>D= 1'30' I= 7'24" L= 493.38 T= 247.01 R= 3819.83 PC 59+90.60 PI 62+37.61 PT 64+83.93 S.E.(EXIST.) = 0.0 FT/FT S.E.(PROP.) = 0.025 FT/FT (40 MPH)</p> | <p>TRANSITION IN STA. 59+43.60 TO 60+18.60</p> <p>TRANSITION OUT STA. 64+65.18 TO 65+40.18</p> | <p>LEFT IN</p> <p>STA 59+43.60 0.0 STA 59+81.10 +0.013 STA 60+18.60 +0.025</p> <p>LEFT OUT</p> <p>STA 64+65.18 +0.025 STA 64+84.18 +0.015 STA 65+12.18 +0.015 STA 65+40.18 +0.015</p> | <p>RIGHT IN</p> <p>STA 59+43.60 0.0 STA 59+81.10 +0.013 STA 60+18.60 +0.025</p> <p>RIGHT OUT</p> <p>STA 64+65.18 +0.025 STA 64+84.18 +0.015 STA 65+12.18 0.0 STA 65+40.18 -0.015</p> |
| SECTION 09-00033-03-RS | | | | | | | |
| <p>CURVE 2 (LEFT)</p> <p>D= 5'0" I= 21'0" L= 420 T= 212 R= 1146.28 PC 11+53.80 PI 13+66.20 PT 15+73.80 S.E.(EXIST.) = 0.05 FT/FT S.E.(PROP.) = 0.075 FT/FT</p> | <p>TRANSITION IN STA. 10+05.30 TO 12+03.30</p> <p>TRANSITION OUT STA. 15+24.30 TO 17+22.30</p> | <p>LEFT IN</p> <p>STA 10+05.30 +0.015 STA 10+38.30 +0.015 STA 10+71.30 +0.015 STA 11+37.30 +0.045 STA 12+03.30 +0.075</p> <p>LEFT OUT</p> <p>STA 15+24.30 +0.075 STA 15+90.30 +0.045 STA 16+56.30 +0.015 STA 16+89.30 +0.015 STA 17+22.30 +0.015</p> | <p>RIGHT IN</p> <p>STA 10+05.30 -0.015 STA 10+38.30 0.0 STA 10+71.30 +0.015 STA 11+37.30 +0.045 STA 12+03.30 +0.075</p> <p>RIGHT OUT</p> <p>STA 15+24.30 +0.075 STA 15+90.30 +0.045 STA 16+56.30 +0.015 STA 16+89.30 0.0 STA 17+22.30 -0.015</p> | <p>CURVE 7 (LEFT)</p> <p>D= 2'0" I= 1'38" L= 731.66 T= 367.87 R= 2864.93 PC 73+74.93 PI 77+74.93 PT 81+06.59 S.E.(EXIST.) = 0.025 FT/FT S.E.(PROP.) = 0.04 FT/FT</p> | <p>TRANSITION IN STA. 72+84.18 TO 74+05.18</p> <p>TRANSITION OUT STA. 80+76.34 TO 81+97.34</p> | <p>LEFT IN</p> <p>STA 72+84.18 +0.015 STA 73+17.18 +0.015 STA 73+50.18 +0.015 STA 73+77.68 +0.0275 STA 74+05.18 +0.04</p> <p>LEFT OUT</p> <p>STA 80+76.34 +0.04 STA 81+03.84 +0.0275 STA 81+31.34 +0.015 STA 81+64.34 +0.015 STA 81+97.34 +0.015</p> | <p>RIGHT IN</p> <p>STA 72+84.18 -0.015 STA 73+17.18 0.0 STA 73+50.18 +0.015 STA 73+77.68 +0.0275 STA 74+05.18 +0.04</p> <p>RIGHT OUT</p> <p>STA 80+76.34 +0.04 STA 81+03.84 +0.0275 STA 81+31.34 +0.015 STA 81+64.34 0.0 STA 81+97.34 -0.015</p> |
| SECTION 09-00035-07-RS | | | | | | | |
| <p>CURVE 3 (RIGHT)</p> <p>D= 3'0" I= 17'22" L= 578.90 T= 291.70 R= 1910.08 PC 24+24.40 PI 27+16.10 PT 30+03.30 S.E.(EXIST.) = 0.03 FT/FT S.E.(PROP.) = 0.055 FT/FT</p> | <p>TRANSITION IN STA. 23+08.90 TO 24+62.90</p> <p>TRANSITION OUT STA. 29+64.80 TO 31+18.80</p> | <p>LEFT IN</p> <p>STA 23+08.90 -0.015 STA 23+41.90 0.0 STA 23+74.90 +0.015 STA 24+18.90 +0.035 STA 24+62.90 +0.055</p> <p>LEFT OUT</p> <p>STA 29+64.80 +0.055 STA 30+08.80 +0.035 STA 30+52.80 +0.015 STA 30+85.80 0.0 STA 31+18.80 -0.015</p> | <p>RIGHT IN</p> <p>STA 23+08.90 +0.015 STA 23+41.90 +0.015 STA 23+74.90 +0.015 STA 24+18.90 +0.035 STA 24+62.90 +0.055</p> <p>RIGHT OUT</p> <p>STA 29+64.80 +0.055 STA 30+08.80 +0.035 STA 30+52.80 +0.015 STA 30+85.80 +0.015 STA 31+18.80 +0.015</p> | <p>CURVE 8 (LEFT)</p> <p>D= 3'0" I= 22'36" L= 753.33 T= 381.63 R= 1910.08 PC 89+84.30 PI 93+65.93 PT 97+37.63 S.E.(EXIST.) = 0.03 FT/FT S.E.(PROP.) = 0.055 FT/FT</p> | <p>TRANSITION IN STA. 88+68.80 TO 90+22.80</p> <p>TRANSITION OUT STA. 96+99.13 TO 98+53.13</p> | <p>LEFT IN</p> <p>STA 88+68.80 +0.015 STA 89+01.80 +0.015 STA 89+34.80 +0.015 STA 89+78.80 +0.035 STA 90+22.80 +0.055</p> <p>LEFT OUT</p> <p>STA 96+99.13 +0.055 STA 97+43.13 +0.035 STA 97+87.13 +0.015 STA 98+20.13 +0.015 STA 98+53.13 +0.015</p> | <p>RIGHT IN</p> <p>STA 88+68.80 -0.015 STA 89+01.80 0.0 STA 89+34.80 +0.015 STA 89+78.80 +0.035 STA 90+22.80 +0.055</p> <p>RIGHT OUT</p> <p>STA 96+99.13 +0.055 STA 97+43.13 +0.035 STA 97+87.13 +0.015 STA 98+20.13 0.0 STA 98+53.13 -0.015</p> |
| SECTION 09-00035-07-RS | | | | | | | |
| <p>CURVE 4 (RIGHT)</p> <p>D= 0'50" I= 3'46" L= 452.06 T= 226.08 R= 6875.55 PC 35+83.72 PI 38+09.80 PT 40+35.78 S.E.(EXIST.) = 0.0 FT/FT S.E.(PROP.) = RC</p> | <p>TRANSITION IN STA. 35+34.22 TO 36+00.22</p> <p>TRANSITION OUT STA. 40+19.28 TO 40+85.28</p> | <p>LEFT IN</p> <p>STA 35+34.22 -0.015 STA 35+67.22 0.0 STA 36+00.22 +0.015</p> <p>LEFT OUT</p> <p>STA 40+19.28 +0.015 STA 40+52.28 0.0 STA 40+85.28 -0.015</p> | <p>RIGHT IN</p> <p>STA 35+34.22 +0.015 STA 35+67.22 +0.015 STA 36+00.22 +0.015</p> <p>RIGHT OUT</p> <p>STA 40+19.28 +0.015 STA 40+52.28 +0.015 STA 40+85.28 +0.015</p> | <p>CURVE 9 (LEFT)</p> <p>D= 2'0" I= 7'06" L= 355 T= 177.73 R= 2864.93 PC 105+13.95 PI 106+91.68 PT 108+68.95 S.E.(EXIST.) = 0.025 FT/FT S.E.(PROP.) = 0.04 FT/FT</p> | <p>TRANSITION IN STA. 104+23.20 TO 105+44.20</p> <p>TRANSITION OUT STA. 108+38.70 TO 109+59.70</p> | <p>LEFT IN</p> <p>STA 104+23.20 +0.015 STA 104+56.20 +0.015 STA 104+89.20 +0.015 STA 105+16.70 +0.0275 STA 105+44.20 +0.04</p> <p>LEFT OUT</p> <p>STA 108+38.70 +0.04 STA 108+66.20 +0.0275 STA 108+93.70 +0.015 STA 109+26.70 +0.015 STA 109+59.70 +0.015</p> | <p>RIGHT IN</p> <p>STA 104+23.20 -0.015 STA 104+56.20 0.0 STA 104+89.20 +0.015 STA 105+16.70 +0.0275 STA 105+44.20 +0.04</p> <p>RIGHT OUT</p> <p>STA 108+38.70 +0.04 STA 108+66.20 +0.0275 STA 108+93.70 +0.015 STA 109+26.70 0.0 STA 109+59.70 -0.015</p> |
| SECTION 09-00035-07-RS | | | | | | | |
| <p>CURVE 5 (RIGHT)</p> <p>D= 6'0" I= 57'06" L= 951.67 T= 519.60 R= 955.37 PC 48+97.40 PI 54+17.00 PT 58+49.07 S.E.(EXIST.) = 0.065 FT/FT S.E.(PROP.) = 0.065 FT/FT (40 MPH)</p> | <p>TRANSITION IN STA. 47+84.15 TO 49+35.15</p> <p>TRANSITION OUT STA. 57+92.60 TO 59+43.60</p> | <p>LEFT IN</p> <p>STA 47+84.15 -0.015 STA 48+12.15 0.0 STA 48+40.15 +0.015 STA 48+87.65 +0.04 STA 49+35.15 +0.065</p> <p>LEFT OUT</p> <p>STA 57+92.60 +0.065 STA 58+42.93 +0.043 STA 58+93.27 +0.022 STA 59+43.60 0.0</p> | <p>RIGHT IN</p> <p>STA 47+84.15 +0.015 STA 48+12.15 +0.015 STA 48+40.15 +0.015 STA 48+87.65 +0.04 STA 49+35.15 +0.065</p> <p>RIGHT OUT</p> <p>STA 57+92.60 +0.065 STA 58+42.93 +0.043 STA 58+93.27 +0.022 STA 59+43.60 0.0</p> | | | | |
| SECTION 09-00035-07-RS | | | | | | | |

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| ROUTE | COUNTY | SECTION | SHEET/OF |
| FAS 1870 | RANDOLPH | 09-00035-07-RS | 5/6 |
| CONTRACT NO 97396 | | | |

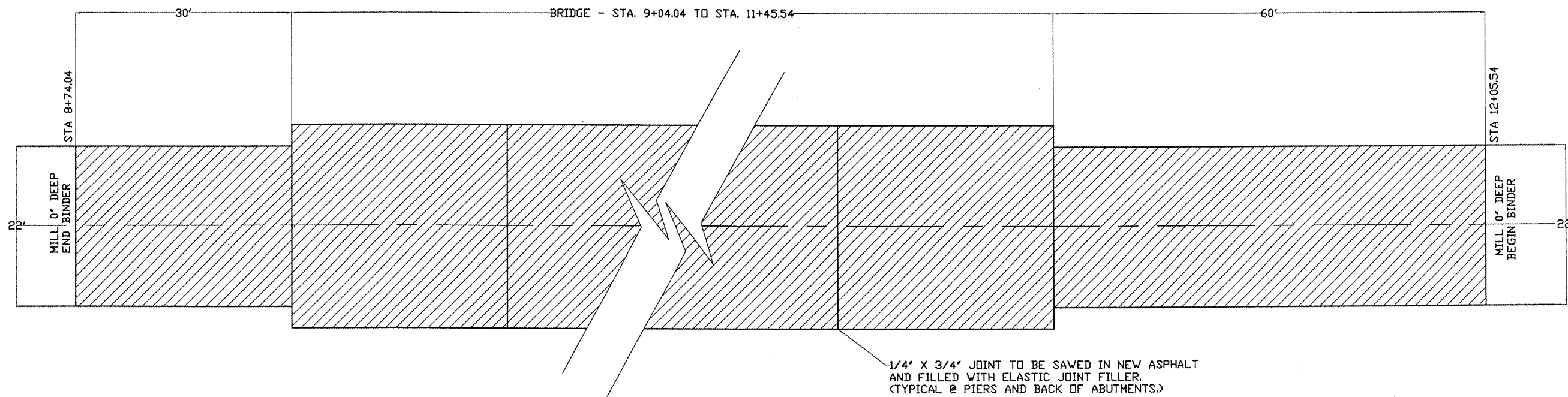
HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT DETAILS



ENTRANCE AND SIDE ROAD DETAIL



HOT-MIX ASPHALT SURFACE REMOVAL, 1"



| ROUTE | COUNTY | SECTION | SHEET/OF |
|--------------------|----------|----------------|----------|
| FAS 1870 | RANDOLPH | 09-00035-07-RS | 6/6 |
| CONTRACT NO. 97396 | | 09-00033-03-RS | |

