

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	1
		ILLINOIS	CONTRACT NO. 62L24	

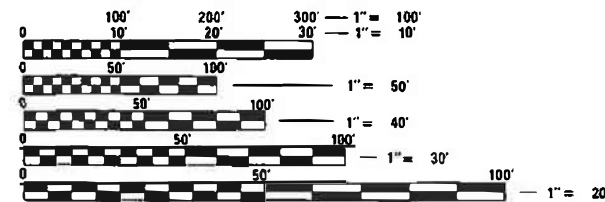
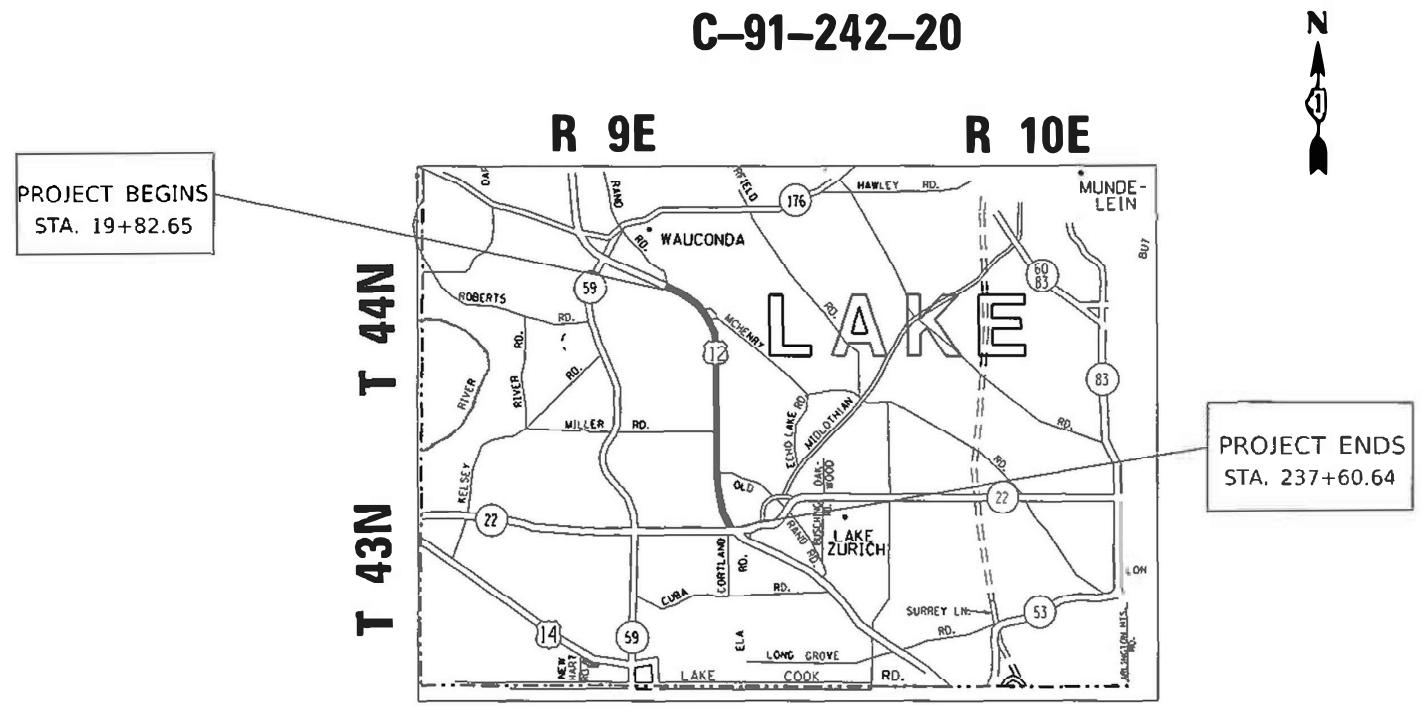
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGE OF LAKE ZURICH AND NORTH BARRINGTON.

TRAFFIC DATA:
2019 ADT: 40100
POSTED SPEED LIMIT: 45 MPH & 55 MPH

PROPOSED HIGHWAY PLANS

F.A.P ROUTE 334: US 12 (RAND ROAD)
SOUTH OF OLD RAND RD TO NORTH OF IL 22
SECTION: 2020-052-RS&SW
SMART OVERLAY, ADA IMPROVEMENTS
LAKE COUNTY



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847)-705-4240
PROJECT MANAGER: FAWAD AQUEEL (847)-705-4247

GROSS LENGTH = 21,778 FT. = 4.12 MILE
NET LENGTH = 21,778 FT. = 4.12 MILE

CONTRACT NO. 62L24

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED July 28 2020

Anthony J. Dringler REGIONAL ENGINEER

October 2, 2020

Scott A. Etk ENGINEER OF DESIGN AND ENVIRONMENT

October 2, 2020

James J. Gu DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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34	ADA RAMPS [PROJECT DETAIL FOR SINGLE PERPENDICULAR RAMPS WITH TURNING SPACE (PD-04)]

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS
442201-03	CLASS C & D PATCHES
606001-07	COMBINATION CURB AND GUTTER
701101-05	OFF ROAD OPERATIONS, MULTILANE, LESS THAN 15 FT TO EDGE OF PAVEMENT
701106-02	OFF ROAD OPERATIONS, MULTILANE, MORE THAN 15 FT AWAY
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS >= 45 MPH TO 55 MPH
701422-10	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
701423-10	LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS >= 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEED >= 45 MPH
701431-13	LANE CLOSURE, MULTILANE, UNDIVIDED WITH CROSSOVER, FOR SPEED >= 45 MPH TO 55 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W, WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

NOTE NO.	DESCRIPTION
1	BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION AT 8-1-1 OR (800) 892-0123 FOR FIELD LOCATION OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
2	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES ALL MUNICIPALITIES WITHIN THE PROJECT LIMITS
3	ALL SIDEWALK RAMP SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER
4	THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5	SAW CUTTING OF PAVEMENTS, SIDEWALK, CURB & GUTTER, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED
6	DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS
7	THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
8	WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL, BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/8 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRAD DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)
9	UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
10	BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE REESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.
11	PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
12	THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV. A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.
13	THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, WALTER CZARNY AT WALTER.CZARNY@ILLINOIS.GOV AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14	BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE THE "BUTT JOINT AND HMA TAPER DETAILS" INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED
15	MATCH EXISTING PAVEMENT AT THE PROJECT LIMITS.
16	ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
17	THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY

GENERAL NOTES (CONTINUED)

NOTE NO.	DESCRIPTION
18	THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
19	THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
20	ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
21	EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
22	LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED IN THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER
23	ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER
24	IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS
25	DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
26	FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT
27	SIDEWALK REMOVAL AND PCC SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER
28	ALL RAISED REFLECTIVE MAKERS SHALL BE INSTALLED ACCORDING TO IDOT DISTRICT ONE DETAIL TC-11
29	ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT DISTRICT ONE DETAIL TC-13
30	ALL SIDEWALK RAMP SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER
31	ALL SIDEWALK RAMP SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER
32	VILLAGE NOTES A. WYNSTONE PROPERTY OWNERS ASSOCIATION (POA) MUST BE CALLED (847-304-3850) TO COORDINATE ALL CONSTRUCTION ACTIVITY SCHEDULES AND IMPACTS TO THE FOLLOWING LOCATIONS 48 HRS. PRIOR TO COMMENCEMENT: I. WYNSTONE SERVICE DRIVE (PUBLIC WORKS) @ APPROXIMATELY 101+46 (R) II. WYNSTONE DRIVE (MAIN PUBLIC ENTRANCE) @ APPROXIMATELY 133+68 (R) III. WYNSTONE OFFICE PARK (NORTH DRIVE) @ APPROXIMATELY 145+29 (R) IV. WYNSTONE OFFICE PARK (SOUTH DRIVE) @ APPROXIMATELY 156+11 B. VILLAGE OF NORTH BARRINGTON MUST BE CALLED (847-381-6000 X10) AND THE VILLAGE OF LAKE ZURICH PUBLIC WORKS (847-540-1696) TO COORDINATE ALL CONSTRUCTION ACTIVITY SCHEDULES AND IMPACTS TO THE FOLLOWING LOCATION 48 HRS. PRIOR TO COMMENCEMENT: I. CLOVER HILL LANE @ APPROXIMATELY 185+10 (R) C. IF TEMPORARY RAMPS ARE NOT CONSTRUCTED AFTER MILLING OPERATIONS AT THE LOCATIONS IDENTIFIED ABOVE, THE WYNSTONE POA AND THE VILLAGES OF NORTH BARRINGTON AND LAKE ZURICH WILL REQUEST THE POSTING OF "BUMP" SIGN, (W8-1) AT THE BUTT JOINT LOCATIONS. D. THE VILLAGE OF NORTH BARRINGTON AND WYNSTONE POA REQUEST TO HAVE THE OPPORTUNITY TO REVIEW/INSPECT THE LOCATIONS

FILE NAME =	USER NAME = a1ramahmm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P 334/US12 (SOUTH OF OLD RAND TO NORTH OF IL-22 INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.P RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLLOT DATE = 8/14/2020	DATE -	REVISED -			CONTRACT NO. 62L24					
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SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005				
20200100	EARTH EXCAVATION	CU YD	40	40					44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	388	388				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	160	160					44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	245	245				
25200110	SODDING, SALT TOLERANT	SO YD	160	160					60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	93705	93705				*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	40	40				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	312	312				*	66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	685	685				*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	3783	3783				*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	16627	16627				*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	10	10				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2200	2200					67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	208233	208233					67100100	MOBILIZATION	L SUM	1	1				
44000600	SIDEWALK REMOVAL	SO FT	2200	2200					70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	2063	2063					70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
	* SPECIALTY ITEMS								70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1				

REV-SEP

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	100% STATE 0005				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	16000	16000				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1365	1365				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	21988	21988					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1200	1200				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	7292	7292					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	2000	2000				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	3516	3516					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1960	1960				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	85340	85340					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1918	1918				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	16000	16000					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4219	4219				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1365	1365					X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1200	1200					X0327611	REMOVE AND REINSTALL BRICK PAVER	SO FT	545	545				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	2000	2000					X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	425	425				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	5497	5497					X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	100	100				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	3500	3500					X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	8	8				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	85340	85340													
	* SPECIALTY ITEMS																

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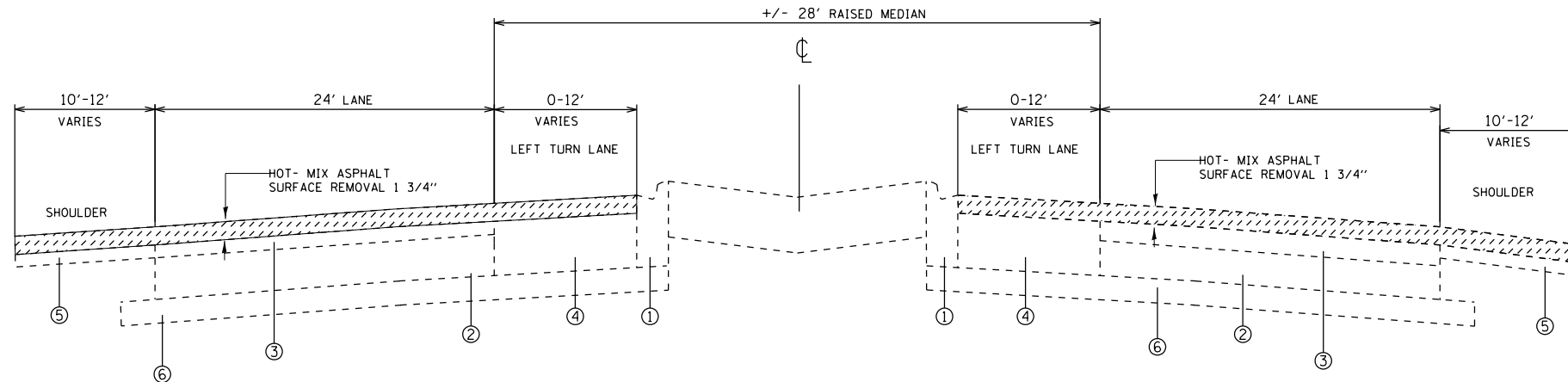
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	4
CONTRACT NO. 62L24				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

REV-SEP

RAND ROAD



EXISTING TYPICAL SECTION RAND ROAD
STA. 19+82.65 TO STA. 83+50

LEGEND

- ① EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ② EXISTING PCC PAVEMENT, +/- 10"
- ③ EXISTING HOT-MIX ASPHALT PAVEMENT, +/- 4" (BEFORE SURFACE REMOVAL)
- ④ EXISTING HOT-MIX ASPHALT, +/-14" (BEFORE SURFACE REMOVAL)
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER, +/- 8" (BEFORE SURFACE REMOVAL)
- ⑥ EXISTING AGGREGATE BASE COURSE
- ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"
- ⑧ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"

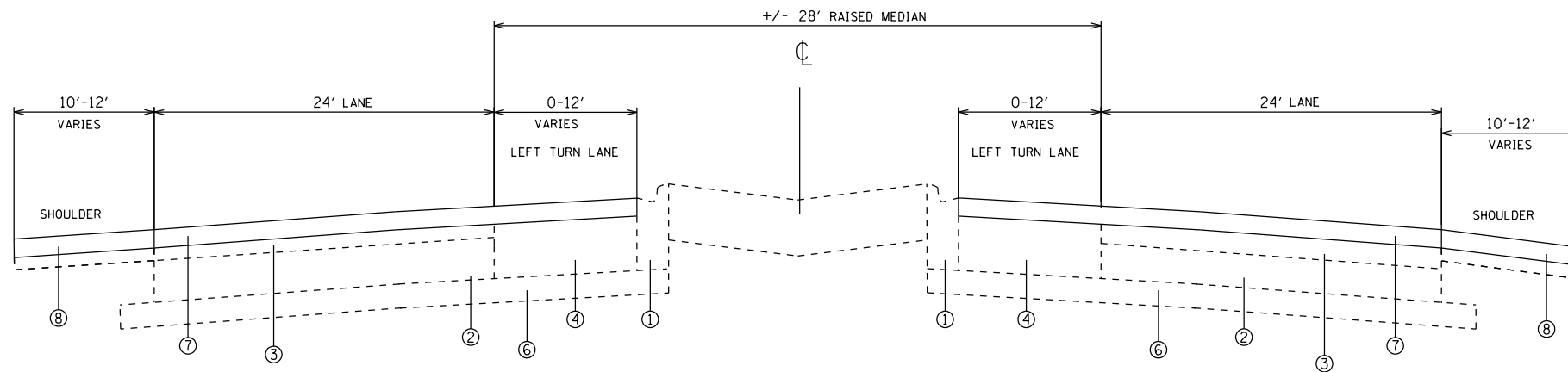
- * CURB AND GUTTER, SIDEWALK REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)
- ** THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILED SURFACE.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT SMART OVERLAY	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"	3.5% @ 80 GYR.	PFPP
PATCHING	CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/OA
HMA SHOULDER SMART OVERLAY	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"	4% @ 70 GYR	QCP

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFPP)

RAND ROAD



PROPOSED TYPICAL SECTION RAND ROAD.
STA. 19+82.65 TO STA. 83+50

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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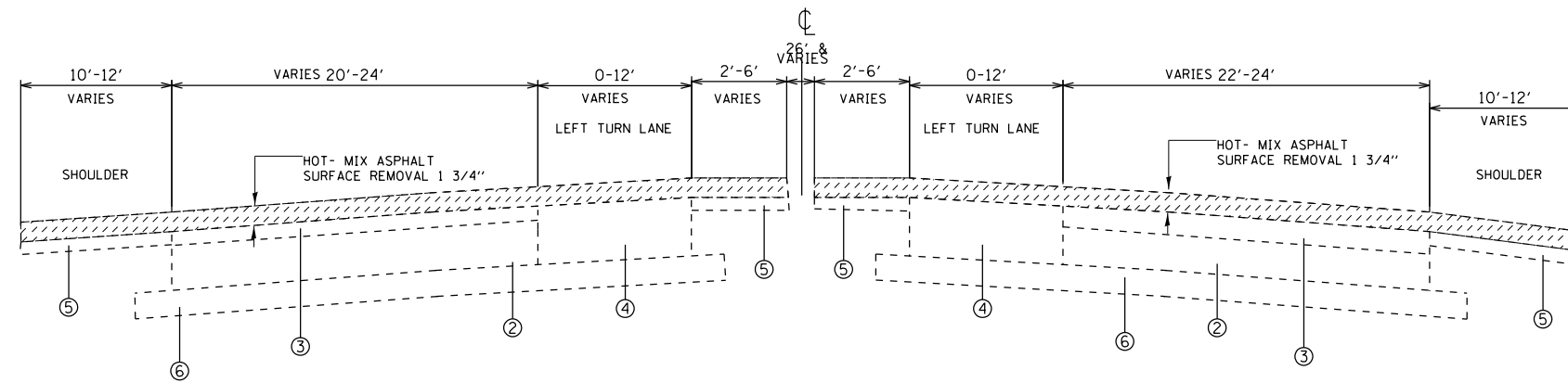
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 12/RAND ROAD (SOUTH OF OLD RAND ROAD TO NORTH OF IL-22)
EXISTING AND PROPOSED TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L24	

RAND ROAD



EXISTING TYPICAL SECTION RAND ROAD
STA. 83+50 TO STA. 237+60.64

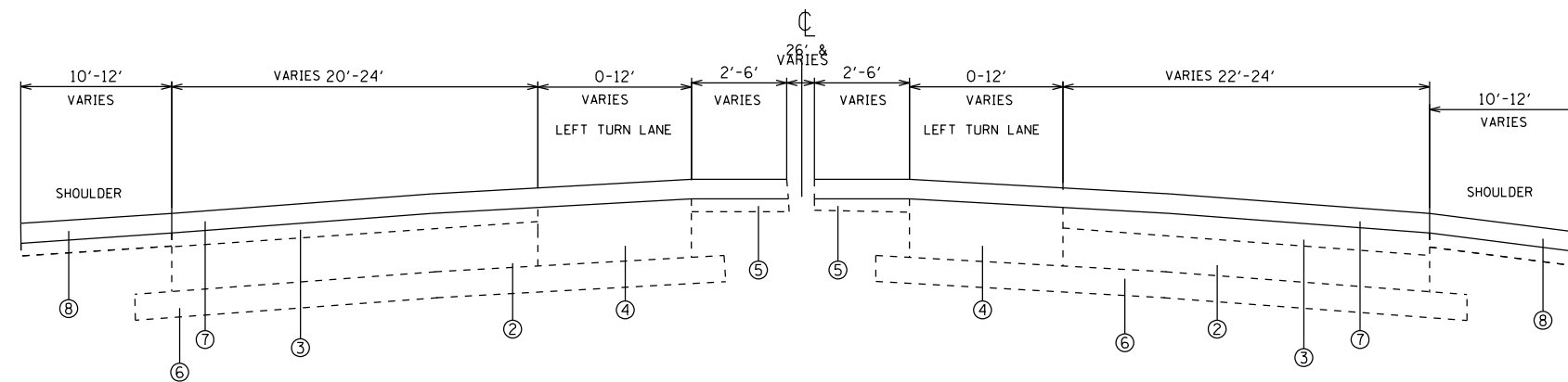
LEGEND

- ① EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ② EXISTING PCC PAVEMENT, +/- 10"
- ③ EXISTING HOT-MIX ASPHALT PAVEMENT, +/- 4" (BEFORE SURFACE REMOVAL)
- ④ EXISTING HOT-MIX ASPHALT, +/-14" (BEFORE SURFACE REMOVAL)
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER, +/- 8" (BEFORE SURFACE REMOVAL)
- ⑥ EXISTING AGGERGATE BASE COURSE
- ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"
- ⑧ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"

* CURB AND GUTTER, SIDEWALK REMOVAL AND REPLACEMENT
(LOCATION AS DIRECTED BY THE ENGINEER)

** THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED
OVER THE MILLED SURFACE.

RAND ROAD



PROPOSED TYPICAL SECTION RAND ROAD
STA. 83+50 TO STA. 237+60.64

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PLOT DATE = 8/14/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 12 /RAND ROAD (SOUTH OF OLD RAND ROAD TO NORTH OF IL-22)
EXISTING AND PROPOSED TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	7
			CONTRACT NO. 62L24	
		ILLINOIS	FED. AID PROJECT	

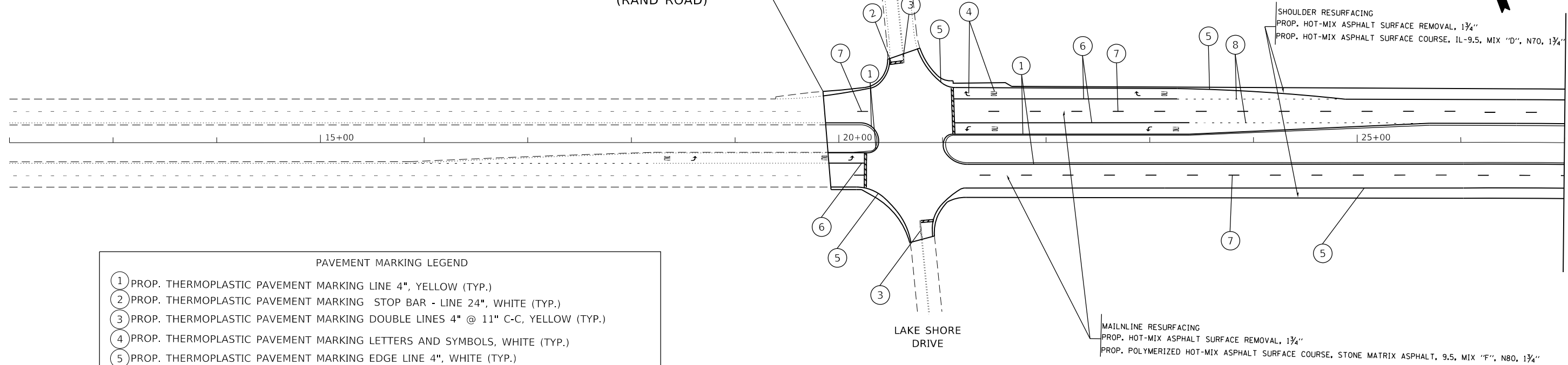
**PROJECT STARTS
STA. 19 + 82.65**

U.S. ROUTE 12
(RAND ROAD)

OLD RAND
ROAD

SHOULDER RESURFACING
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"

MATCHLINE STA. 27+00.00



PAVEMENT MARKING LEGEND

- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
- ③ PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING EDGE LINE 4", WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING TURN LANE - LINE 6", WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 4", 10' DASH - 30' SKIP , WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 6", 2' DASH - 6' SKIP, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
- ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", DIAGONALS, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)

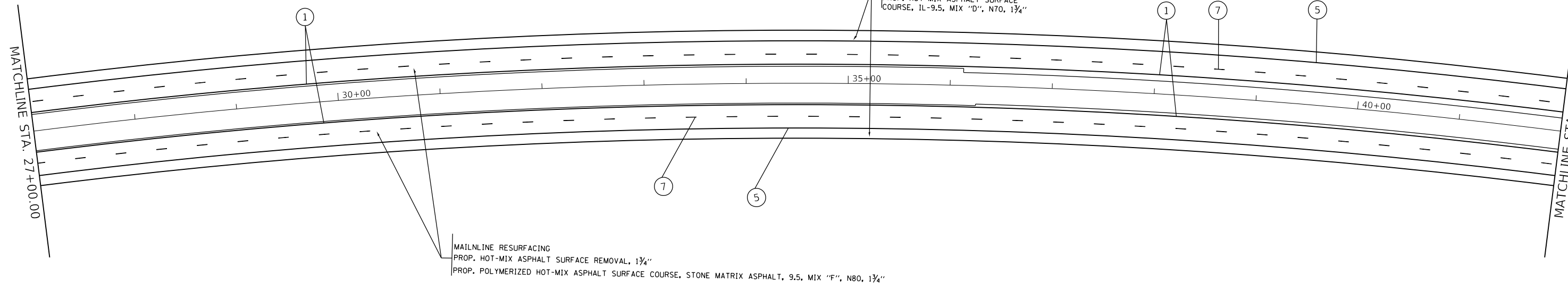
LAKE SHORE
DRIVE

MAILNLINE RESURFACING
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

U.S. ROUTE 12
(RAND ROAD)

SHOULDER RESURFACING
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
PROP. HOT-MIX ASPHALT SURFACE
COURSE, IL-9.5, MIX "D", N70, 1 3/4"

MATCHLINE STA. 42+00.00



MAILNLINE RESURFACING
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

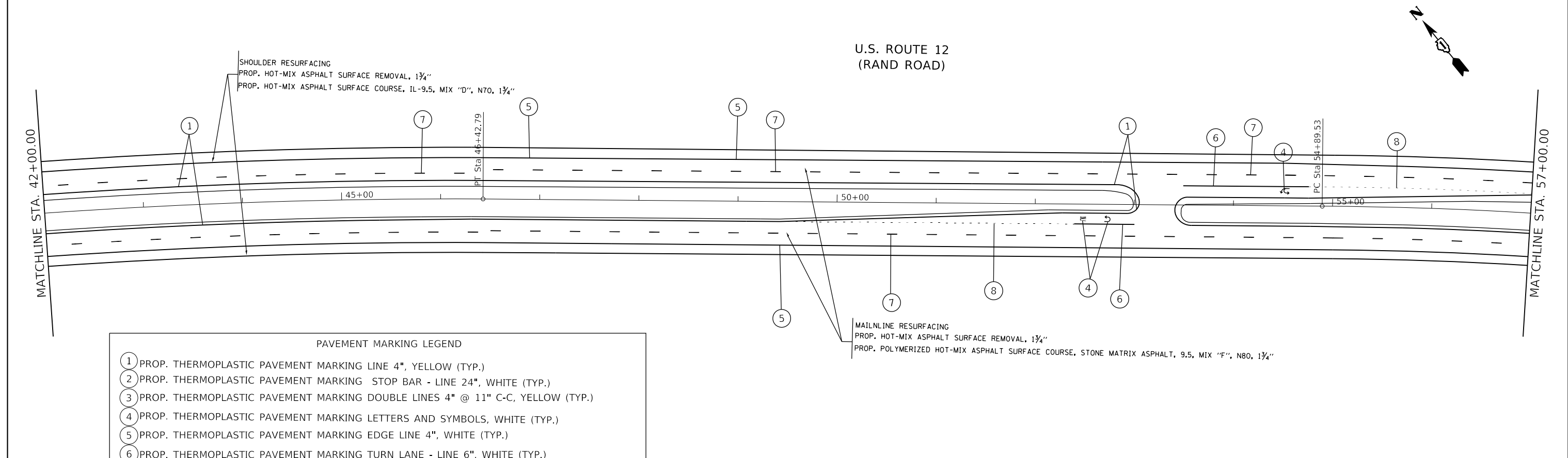
EXISTING AND PROPOSED ROADWAY PLAN
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	8
CONTRACT NO. 62L24				
ILLINOIS		FED. AID PROJECT		

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

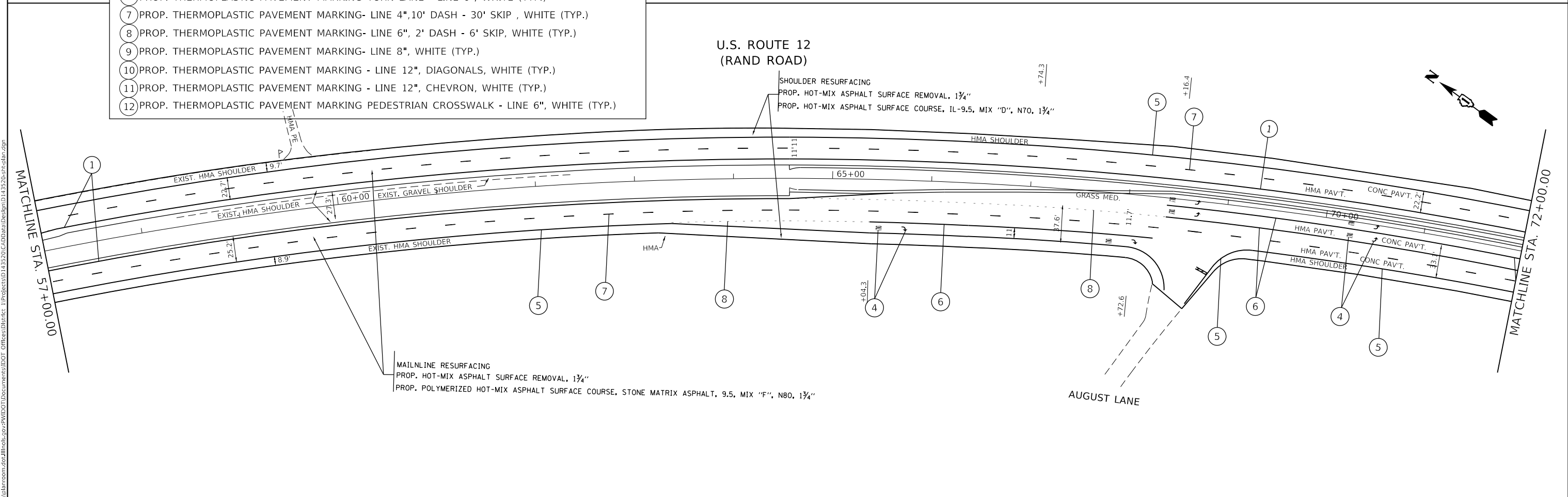
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	DRAWN -	REVISED -
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PLOT DATE = 8/14/2020	DATE -	REVISED -



PAVEMENT MARKING LEGEND

- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
- ③ PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING EDGE LINE 4", WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING TURN LANE - LINE 6", WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 4", 10' DASH - 30' SKIP , WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 6", 2' DASH - 6' SKIP, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
- ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", DIAGONALS, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)



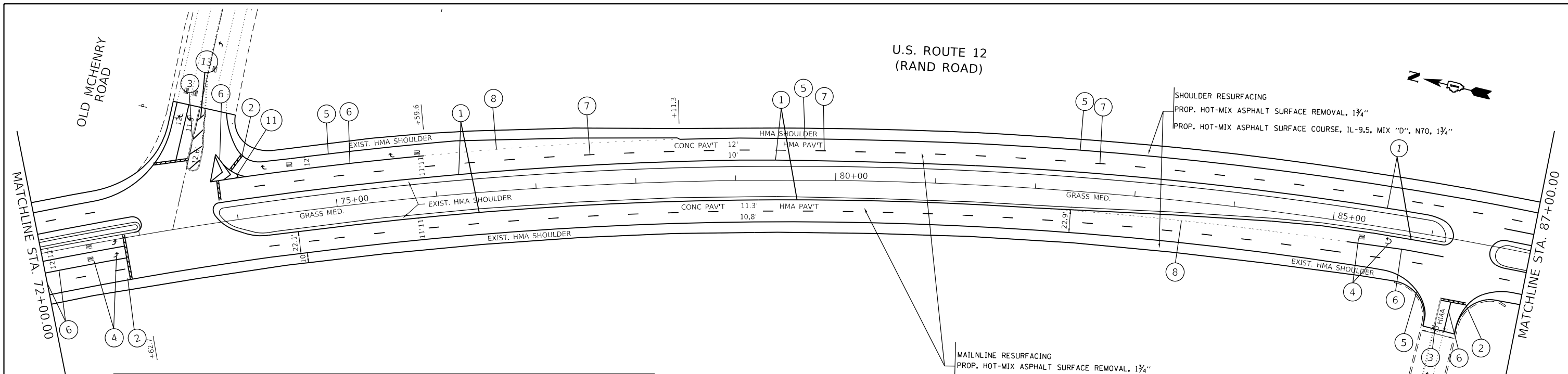
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PLOT DATE = 8/14/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)
 SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

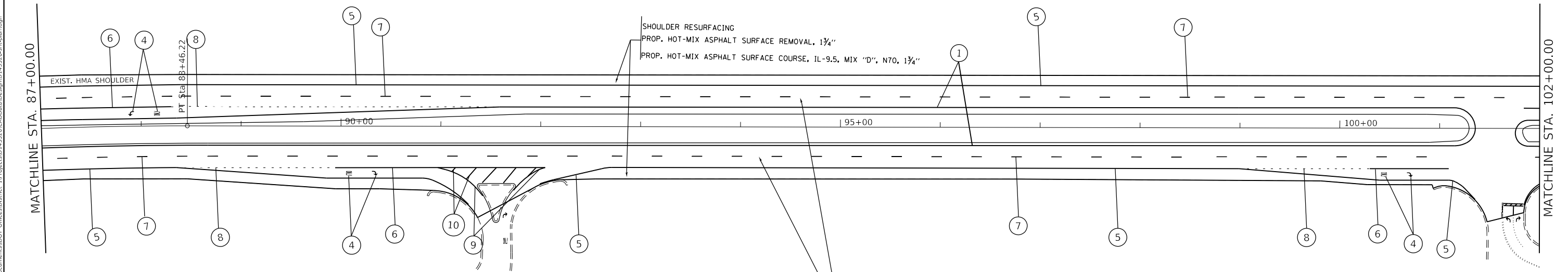
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334	2020-052-R5&SW	LAKE	34	9
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



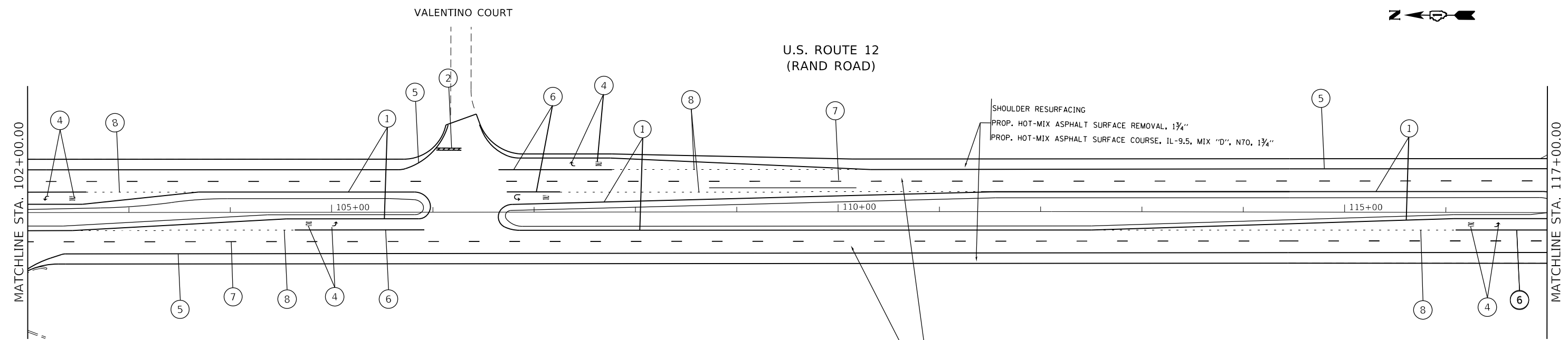
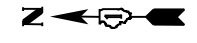
PAVEMENT MARKING LEGEND

- 1) PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
- 2) PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
- 3) PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)
- 4) PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- 5) PROP. THERMOPLASTIC PAVEMENT MARKING EDGE LINE 4", WHITE (TYP.)
- 6) PROP. THERMOPLASTIC PAVEMENT MARKING TURN LANE - LINE 6", WHITE (TYP.)
- 7) PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 4", 10' DASH - 30' SKIP , WHITE (TYP.)
- 8) PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 6", 2' DASH - 6' SKIP, WHITE (TYP.)
- 9) PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
- 10) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", DIAGONALS, WHITE (TYP.)
- 11) PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
- 12) PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)

**U.S. ROUTE 12
(RAND ROAD)**



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SCALE: 1" = 50'				SHEET OF SHEETS STA. TO STA.	

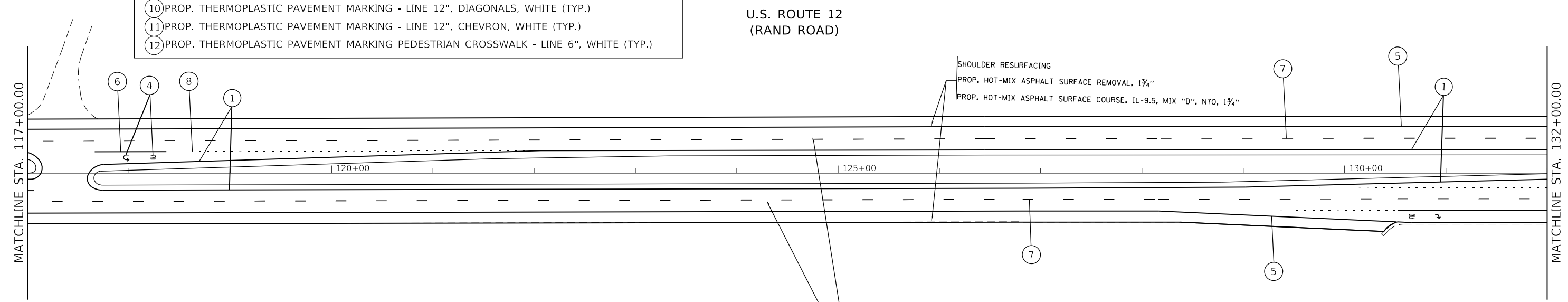


PAVEMENT MARKING LEGEND

- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
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- ④ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING EDGE LINE 4", WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING TURN LANE - LINE 6", WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 4", 10' DASH - 30' SKIP , WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 6", 2' DASH - 6' SKIP, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
- ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", DIAGONALS, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)

SHOULDER RESURFACING
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1¾"

MAILNLINE RESURFACING
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"



SHOULDER RESURFACING
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1¾"

MAILNLINE RESURFACING
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

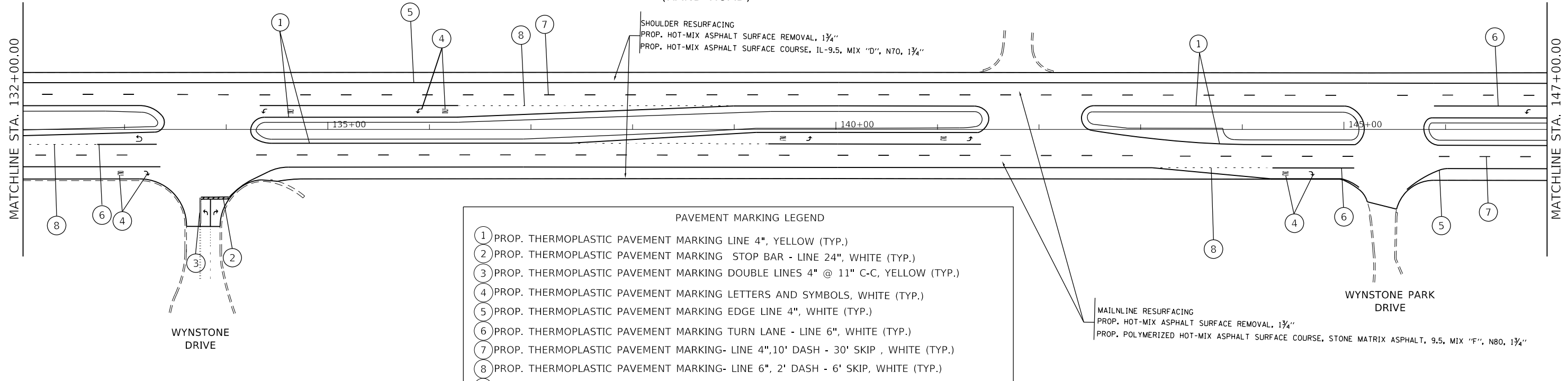
**EXISTING AND PROPOSED ROADWAY PLAN
 U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



U.S. ROUTE 12
(RAND ROAD)

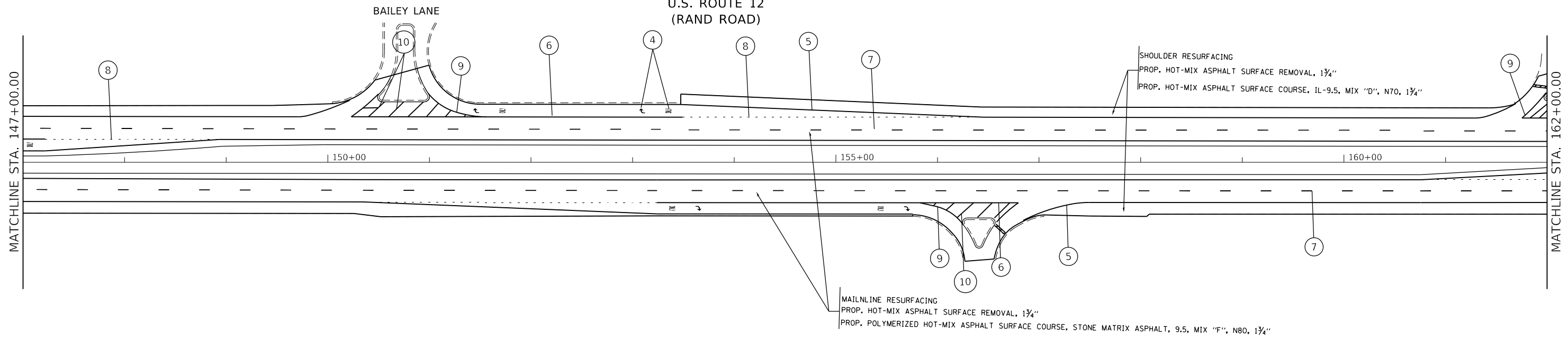


PAVEMENT MARKING LEGEND

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- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)



U.S. ROUTE 12
(RAND ROAD)



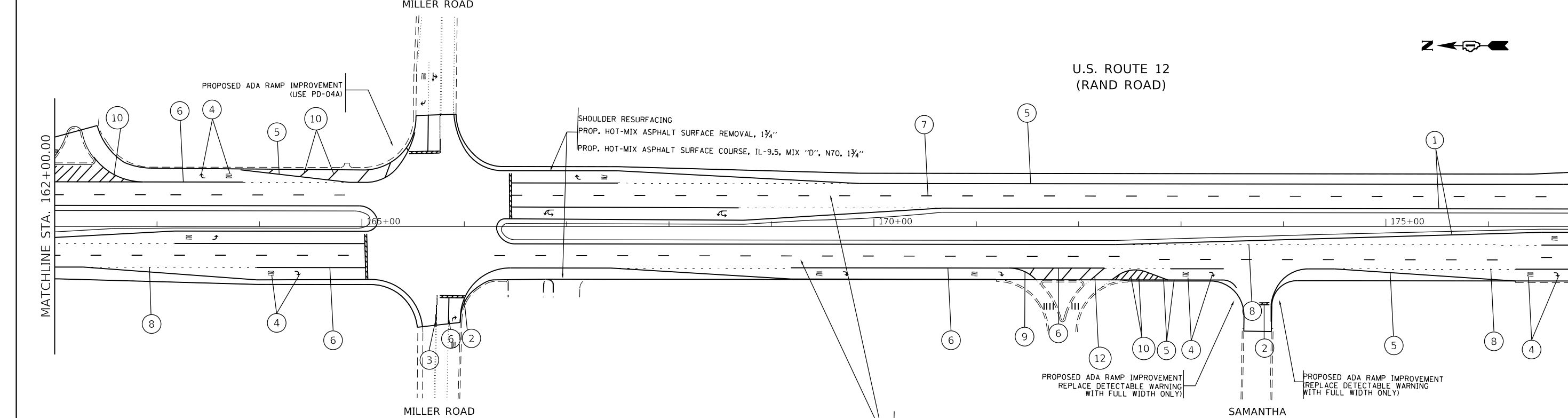
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

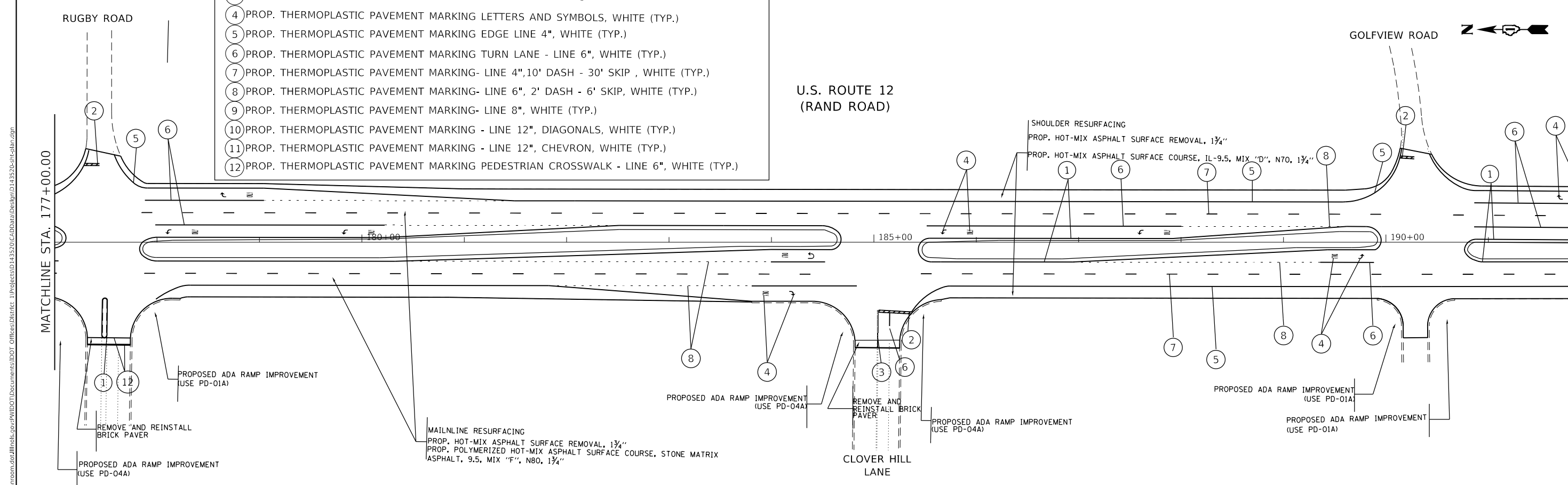
EXISTING AND PROPOSED ROADWAY PLAN			
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)			
SCALE: 1" = 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	12
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
- ③ PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)
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- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING TURN LANE - LINE 6", WHITE (TYP.)
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- ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 6", 2' DASH - 6' SKIP, WHITE (TYP.)
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- ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)



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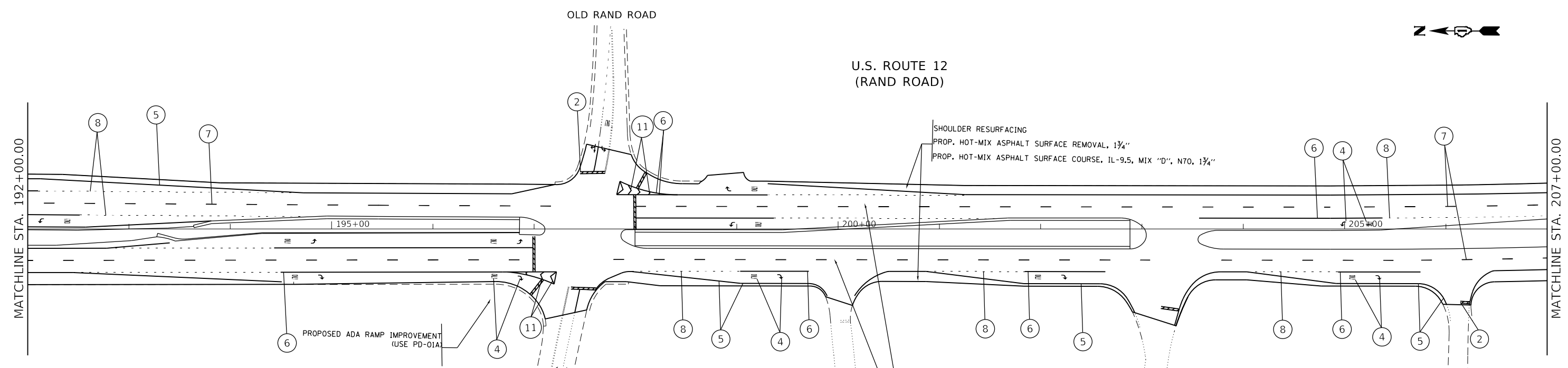
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

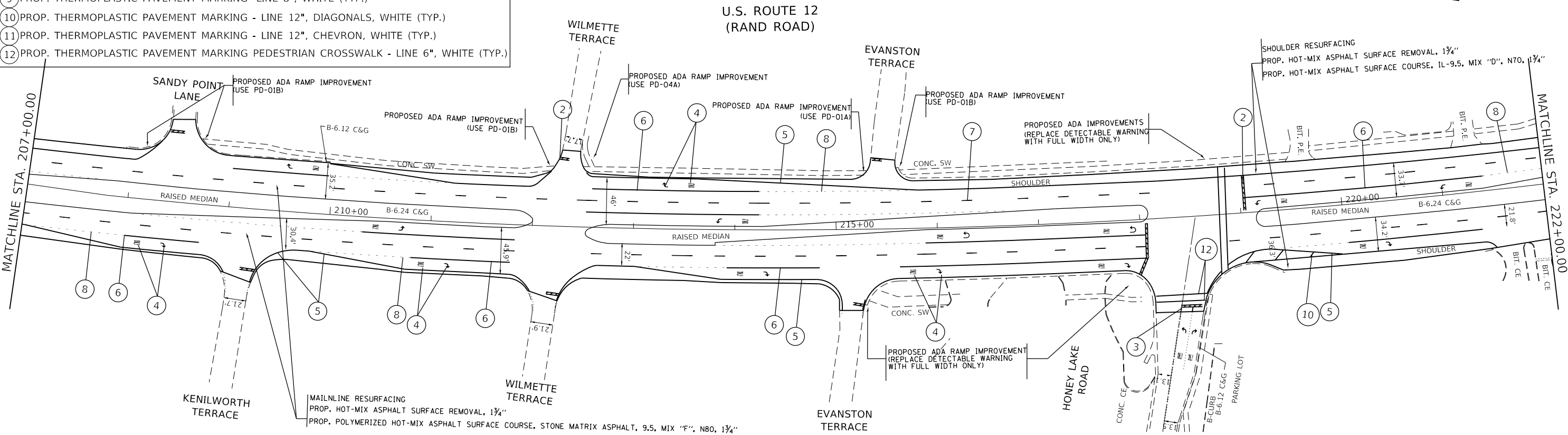
**EXISTING AND PROPOSED ROADWAY PLAN
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS&SW	LAKE	34	13
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



- PAVEMENT MARKING LEGEND**
- 1 PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
 - 2 PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
 - 3 PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)
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 - 9 PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
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 - 11 PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
 - 12 PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)



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 DATE: 8/14/2020

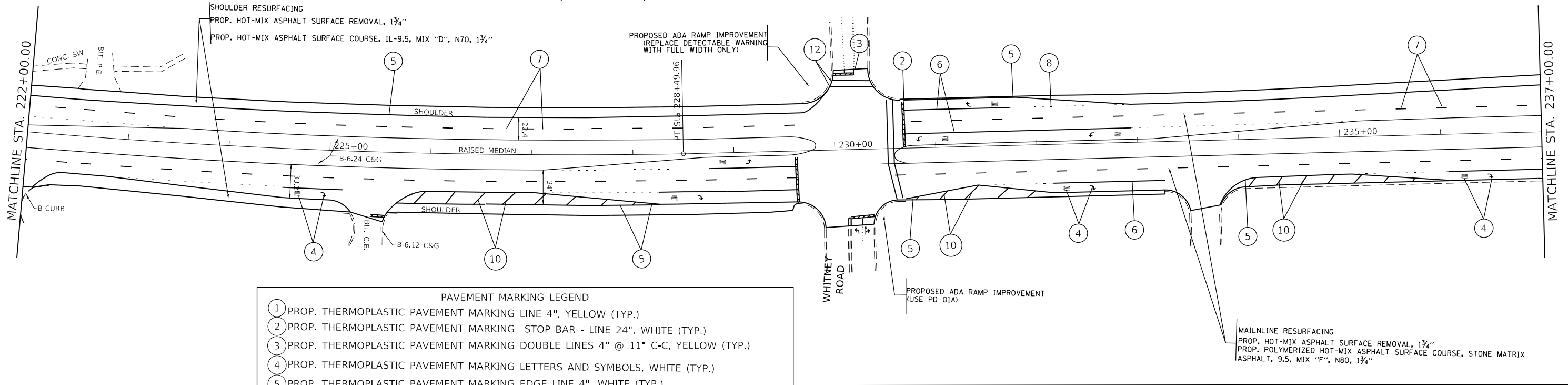
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PLOT DATE = 8/14/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)
 SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	14
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				

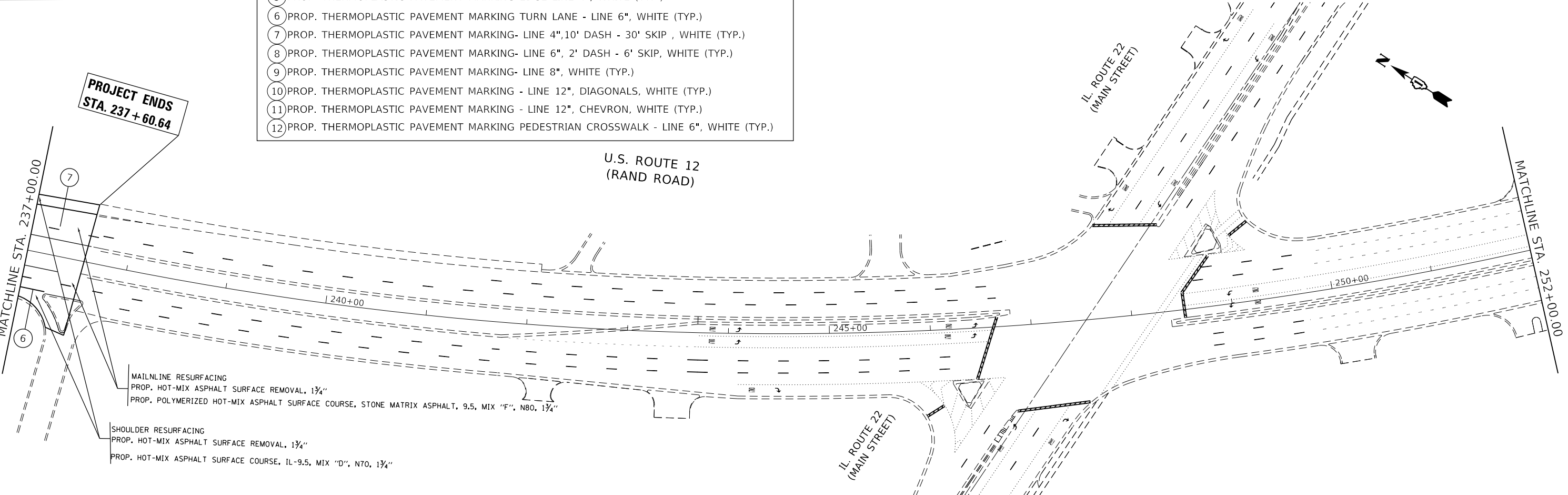
U.S. ROUTE 12
(RAND ROAD)



- PAVEMENT MARKING LEGEND**
- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4", YELLOW (TYP.)
 - ② PROP. THERMOPLASTIC PAVEMENT MARKING STOP BAR - LINE 24", WHITE (TYP.)
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 - ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 4",10' DASH - 30' SKIP , WHITE (TYP.)
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 - ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING- LINE 8", WHITE (TYP.)
 - ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", DIAGONALS, WHITE (TYP.)
 - ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING - LINE 12", CHEVRON, WHITE (TYP.)
 - ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING PEDESTRIAN CROSSWALK - LINE 6", WHITE (TYP.)

**PROJECT ENDS
STA. 237 + 60.64**

U.S. ROUTE 12
(RAND ROAD)



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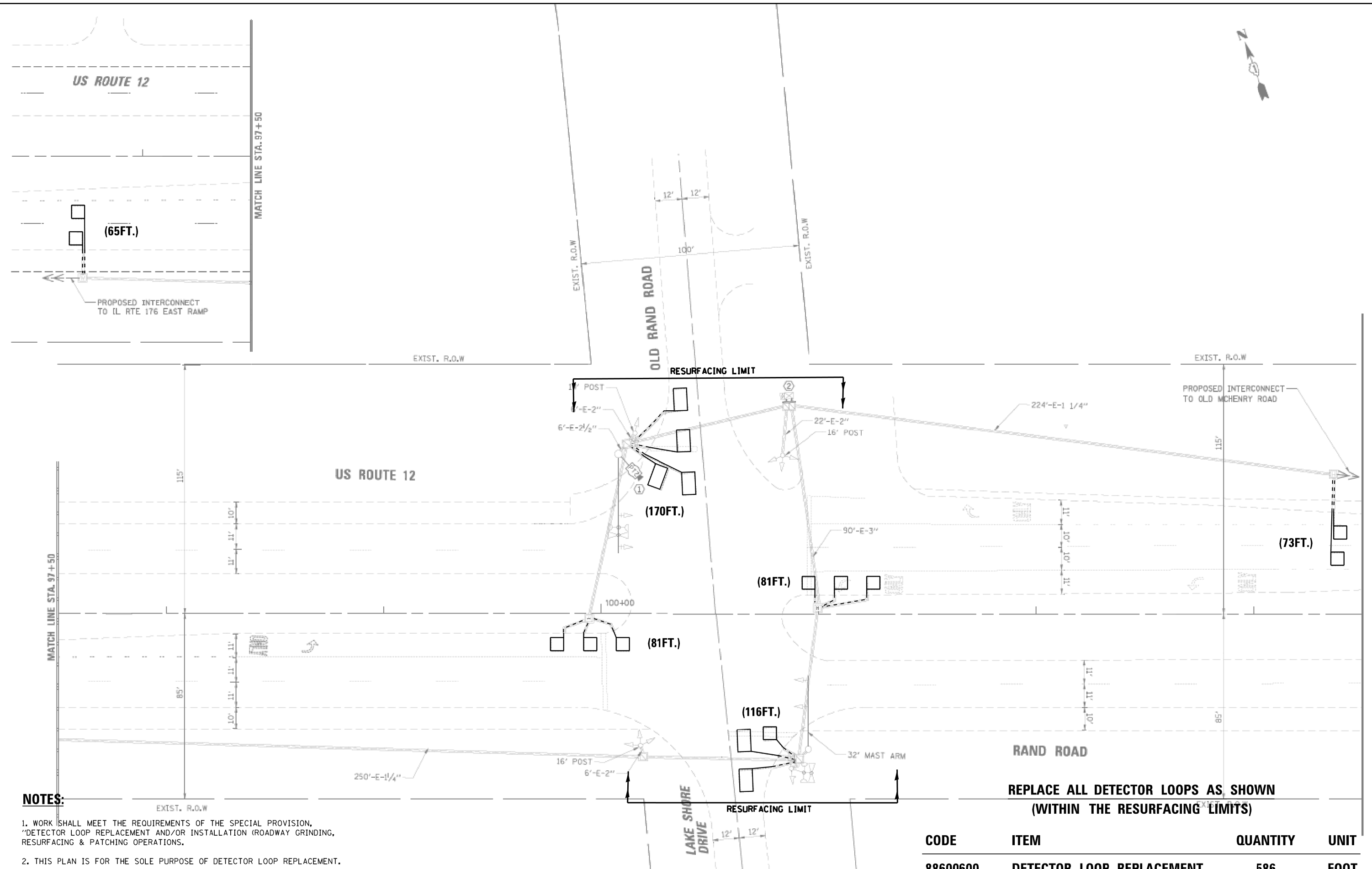
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PLOT DATE = 8/14/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
U.S. ROUTE 12 (SOUTH OF OLD RAND ROAD TO IL. ROUTE 22)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-R5&SW	LAKE	34	15
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	586	FOOT

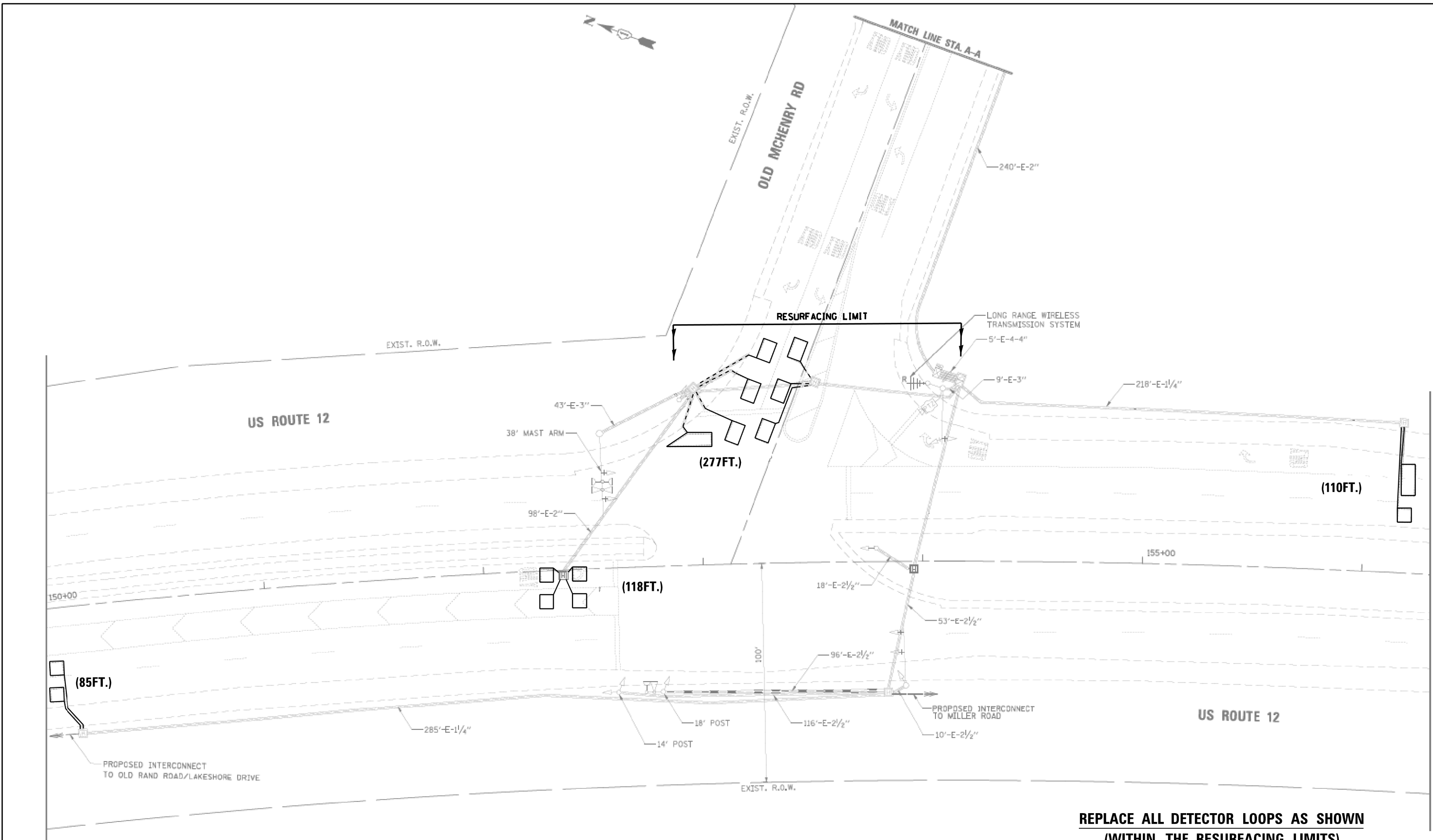
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Default	PLOT DATE = 7/14/2020	DATE - 07/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 12 AT OLD RAND RD. (LAKE SHORE DR.)**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS&SW	LAKE	34	16
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



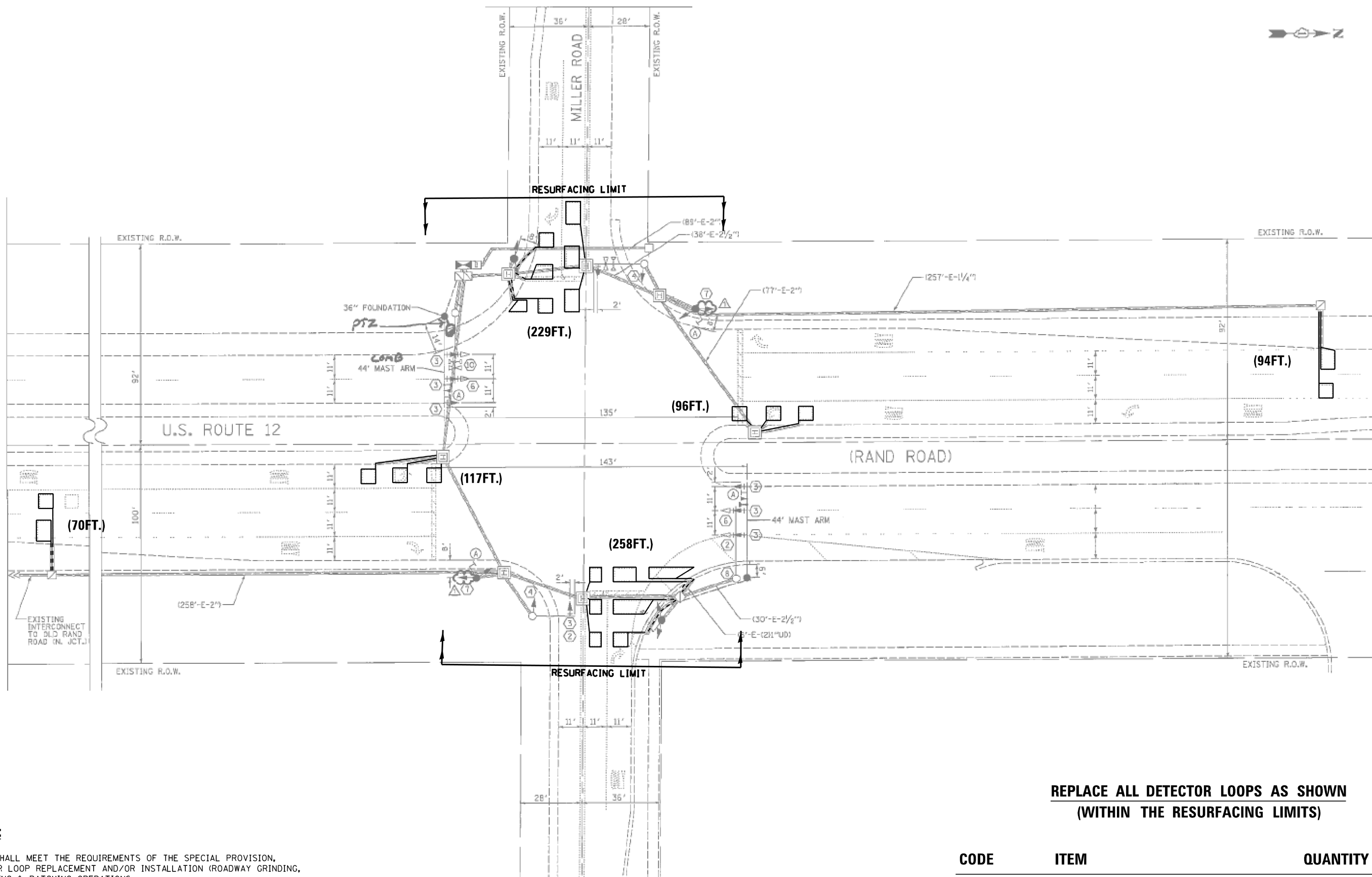
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	590	FOOT

FILE NAME =	USER NAME = mezag	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN US 12 AT OLD MCHENRY RD.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62L24					
	PLOT DATE = 7/14/2020	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	864	FOOT

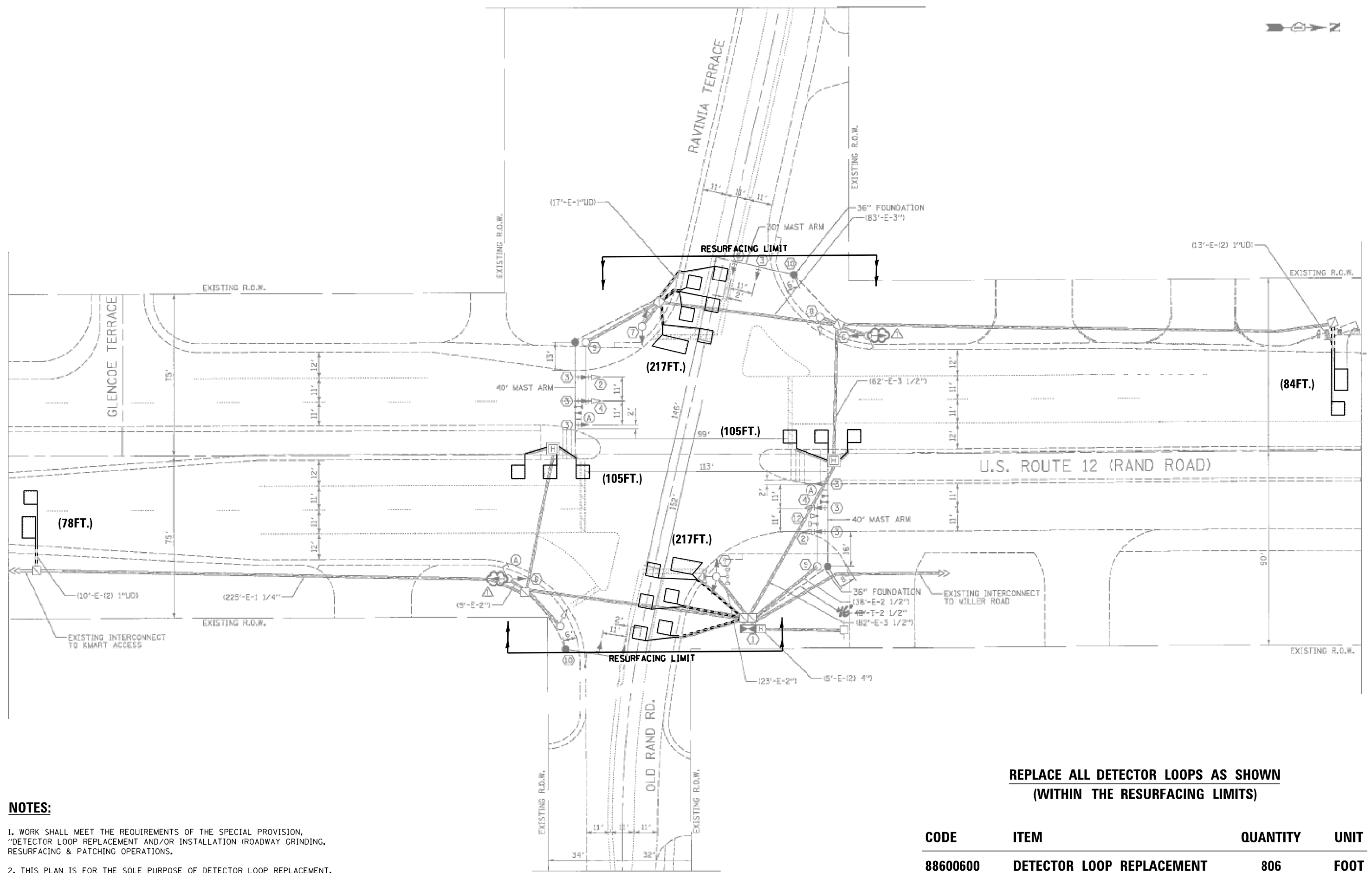
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	PLOT DATE = 7/14/2020	DATE - 07/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 12 AT MILLER RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS&SW	LAKE	84	18
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				

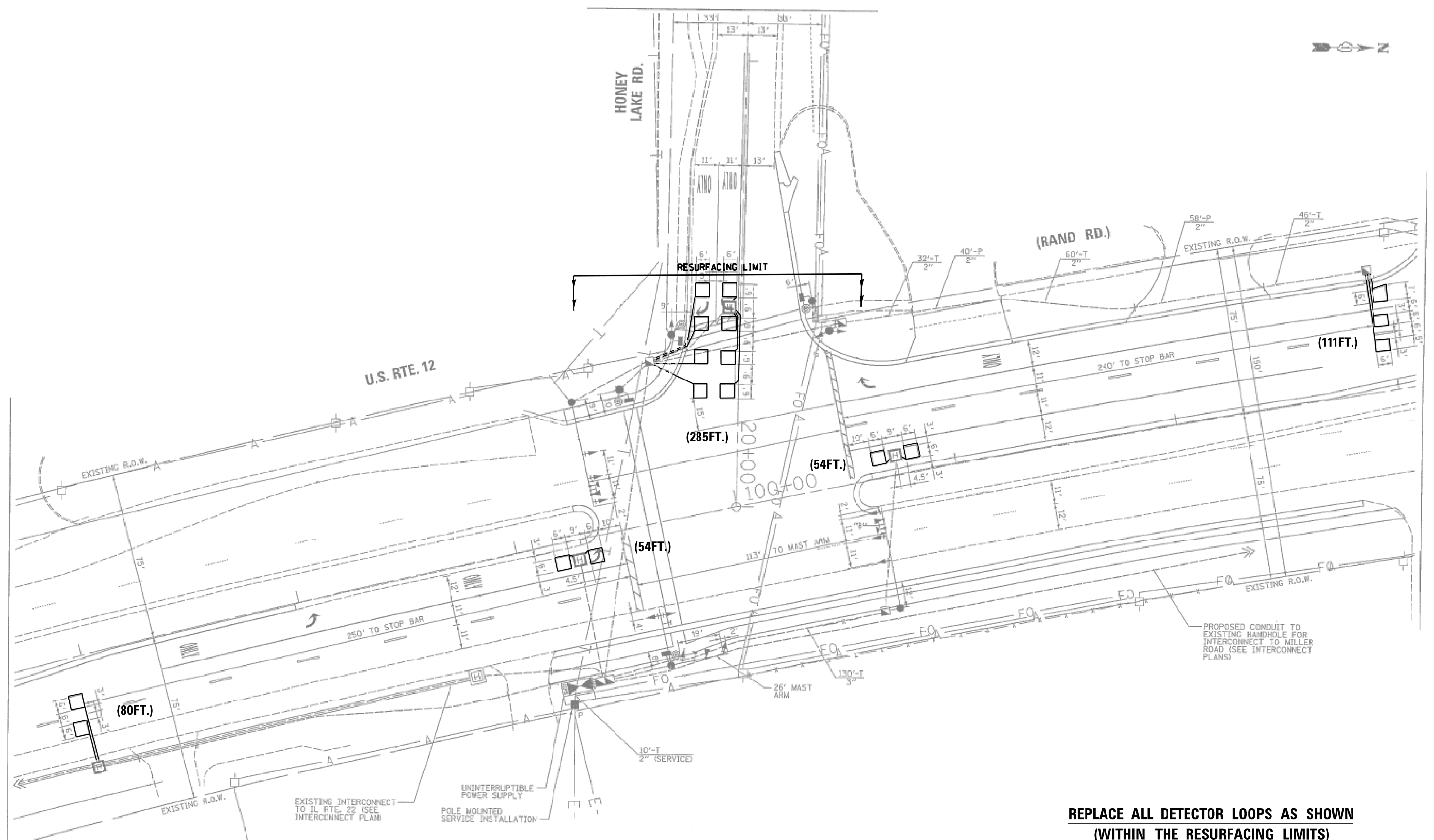


NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	806	FOOT



PROPOSED CONDUIT TO EXISTING HANDHOLE FOR INTERCONNECT TO MILLER ROAD (SEE INTERCONNECT PLANS)

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT
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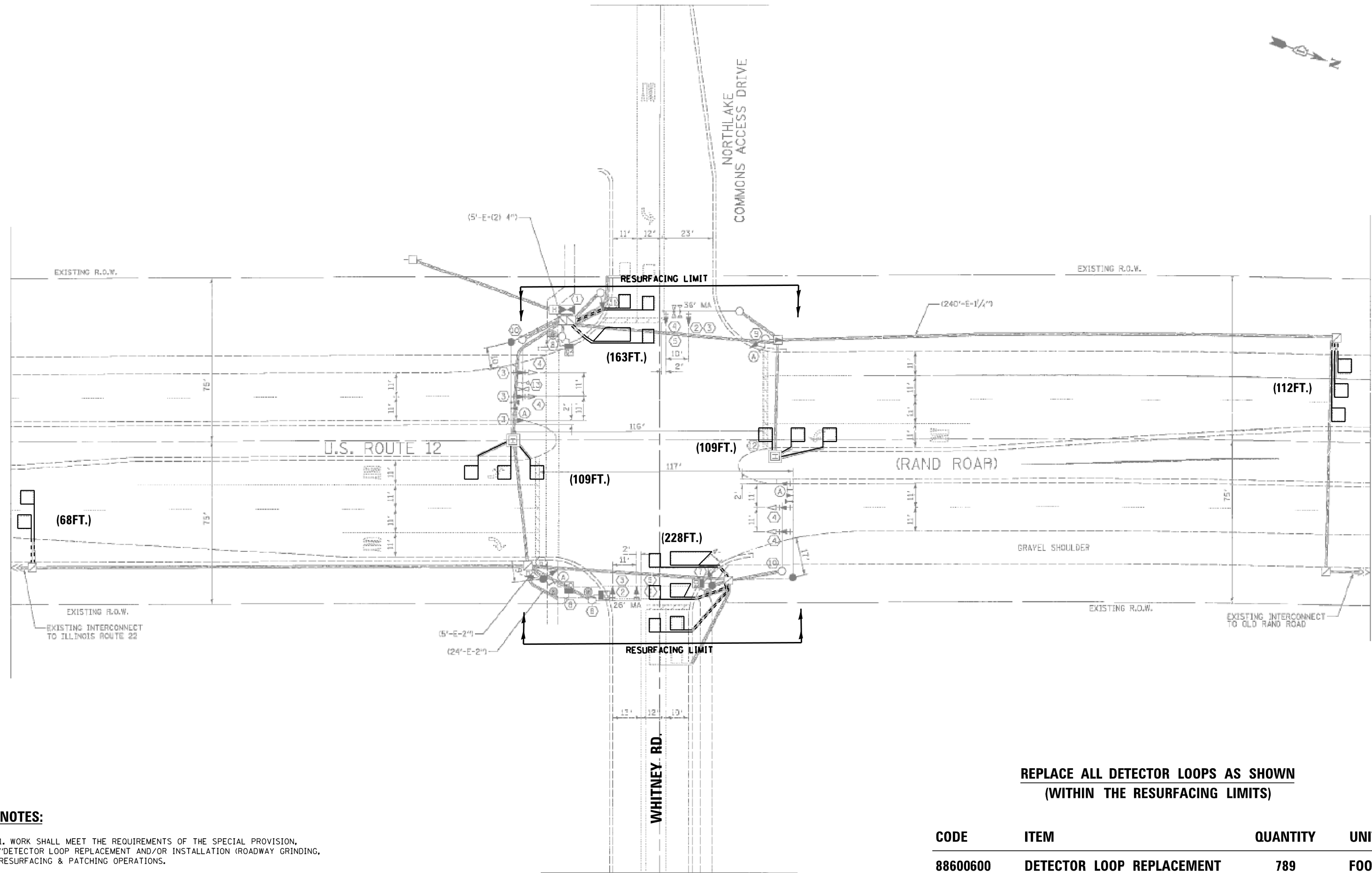
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	PLOT DATE = 7/14/2020	DATE - 07/09/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 12 AT HONEY LAKE RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS&SW	LAKE	84	20
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
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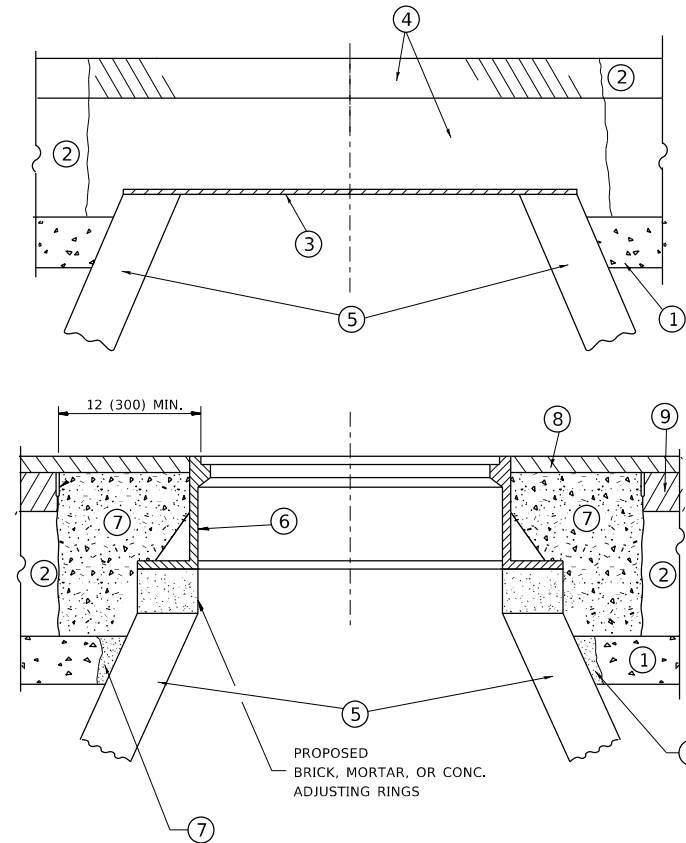
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 12 AT WHITNEY RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS&SW	LAKE	34	21
CONTRACT NO. 62L24				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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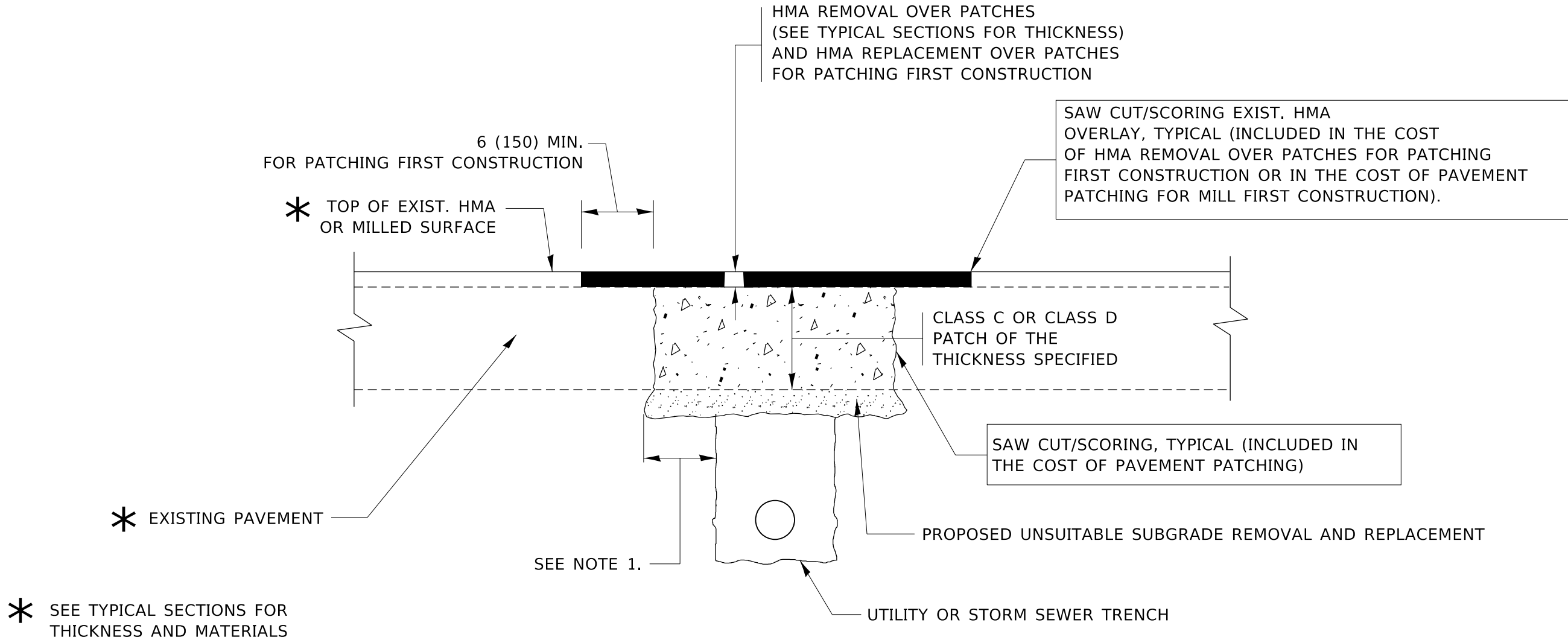
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PLOT DATE = 8/14/2020	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	22
BD600-03 (BD-8)		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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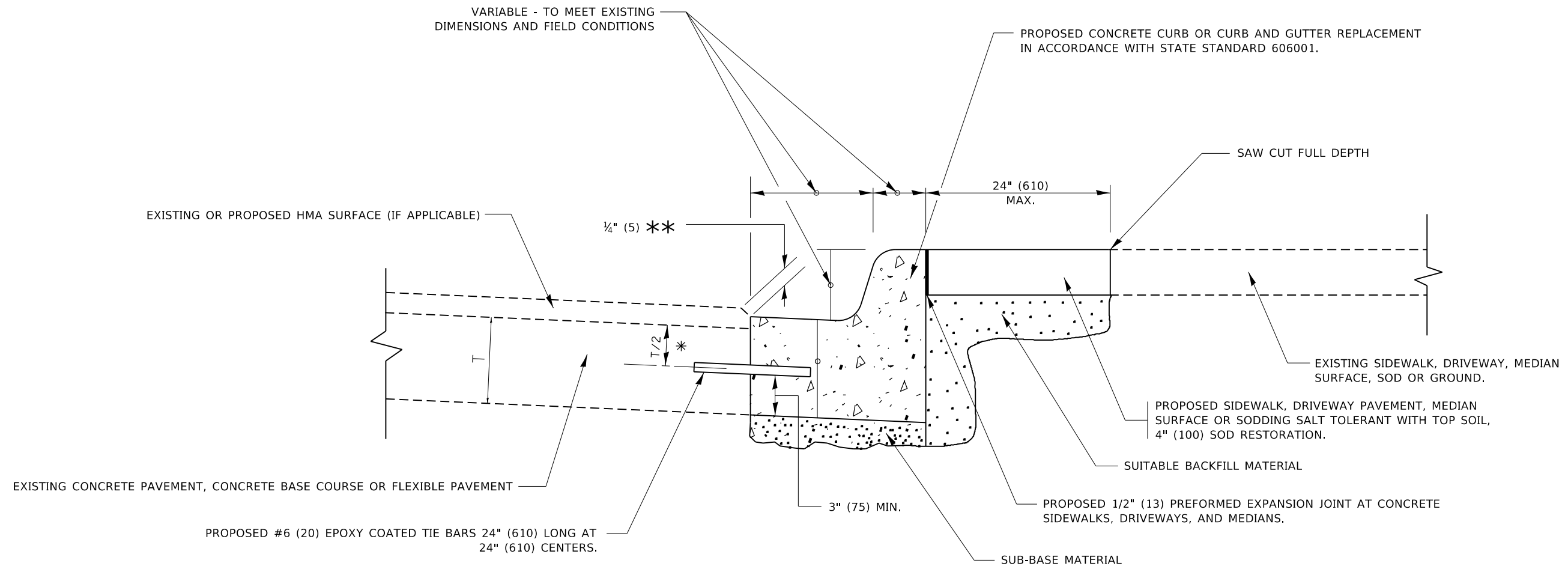
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PLOT DATE = 8/14/2020	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	23
BD400-04 (BD-22)			CONTRACT NO. 62L24	
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

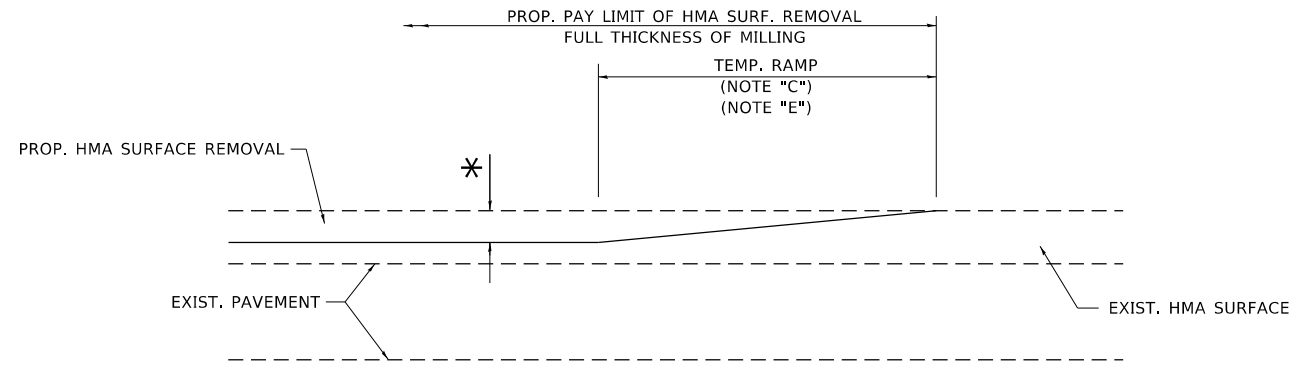
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	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 8/14/2020	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

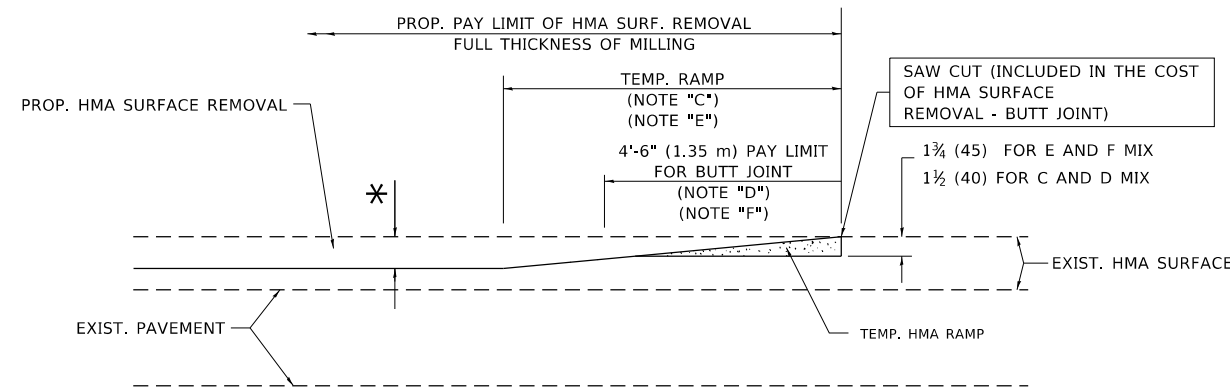
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334	2020-052-RS & SW	LAKE	34	24
BD600-06 (BD-24)		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

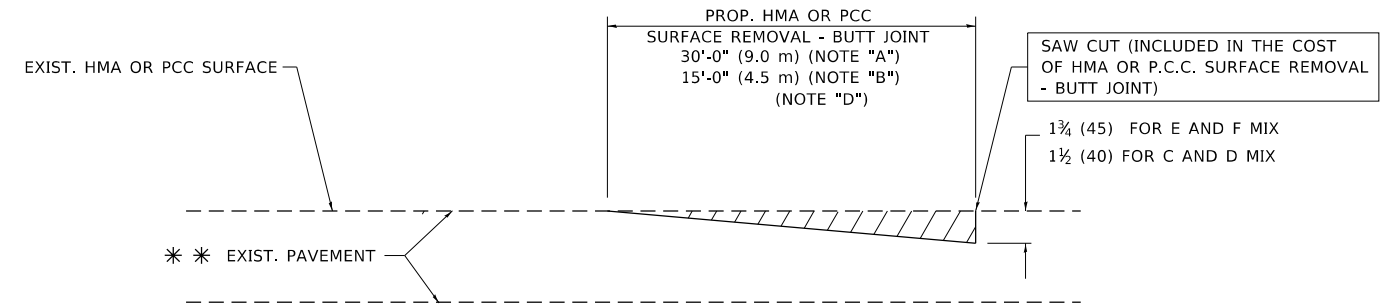


HMA CONSTRUCTED TEMPORARY RAMP

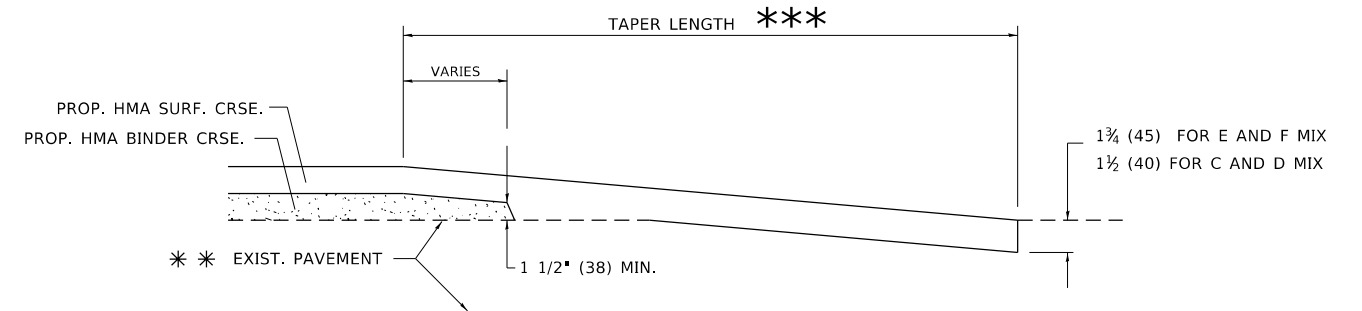
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

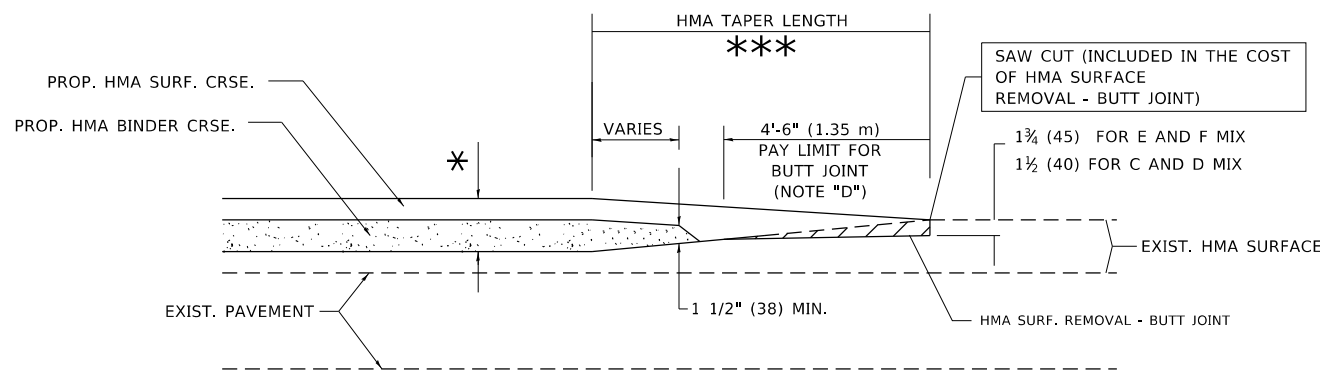
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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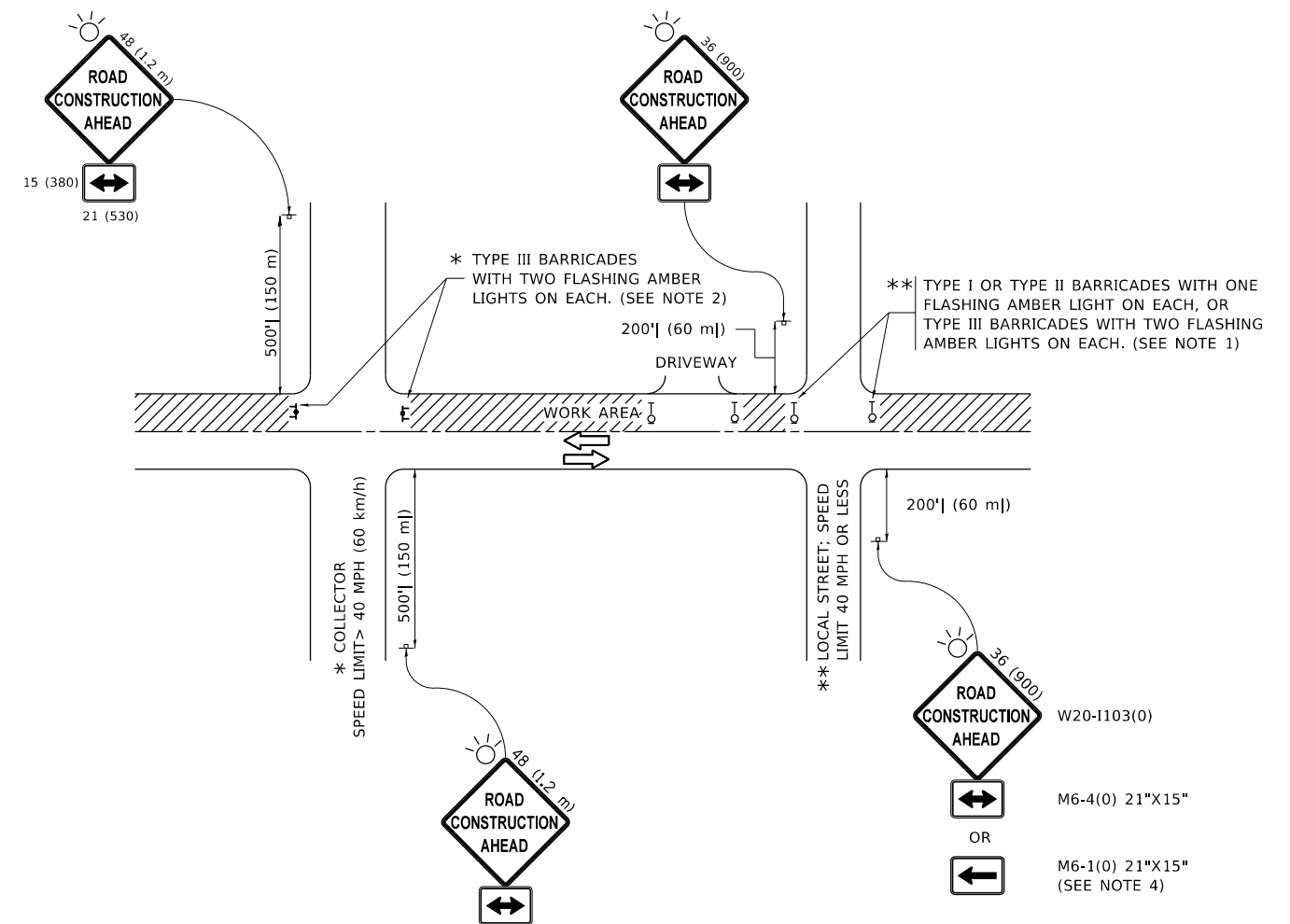
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PLOT DATE = 8/14/2020	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	25
BD400-05 BD32		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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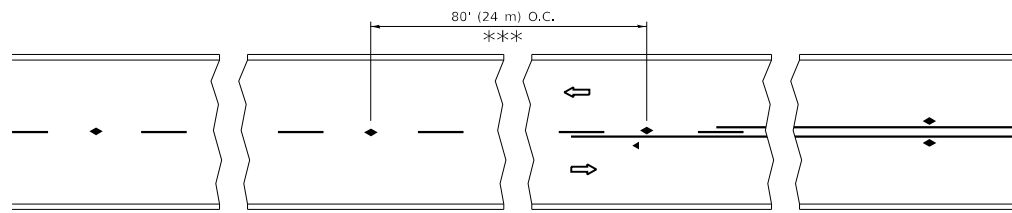
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PLOT DATE = 8/14/2020	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

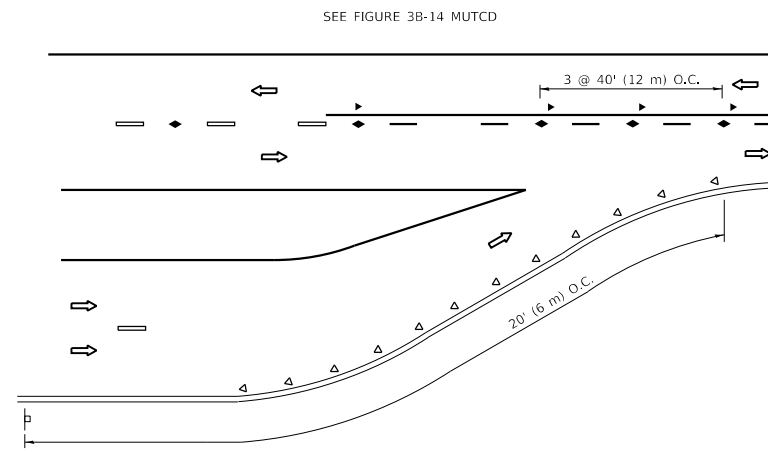
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62L24	
ILLINOIS FED. AID PROJECT				

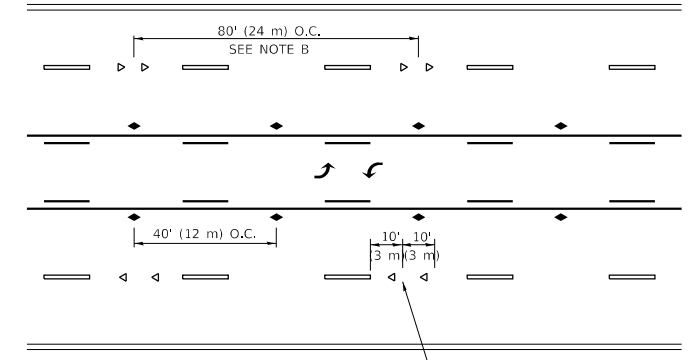


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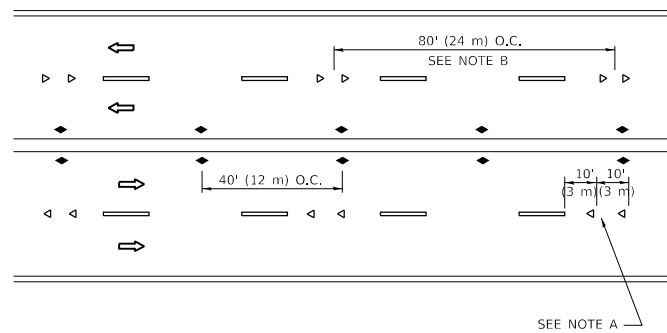
TWO-LANE/TWO-WAY



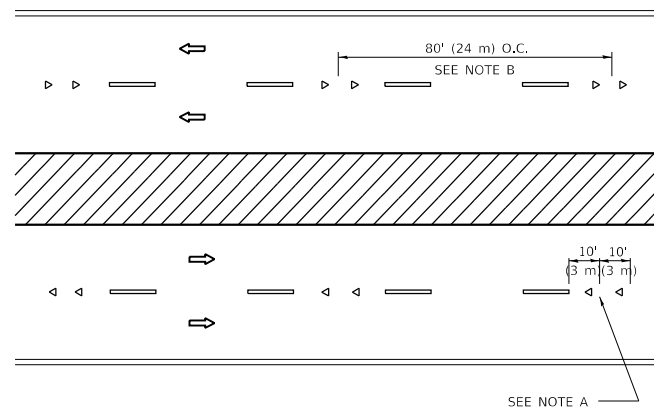
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

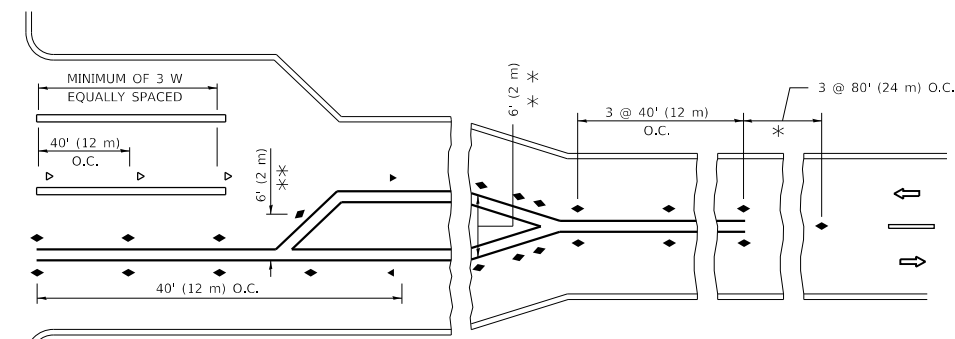
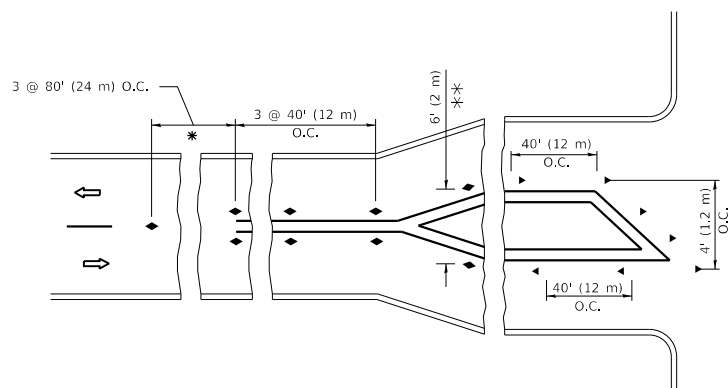
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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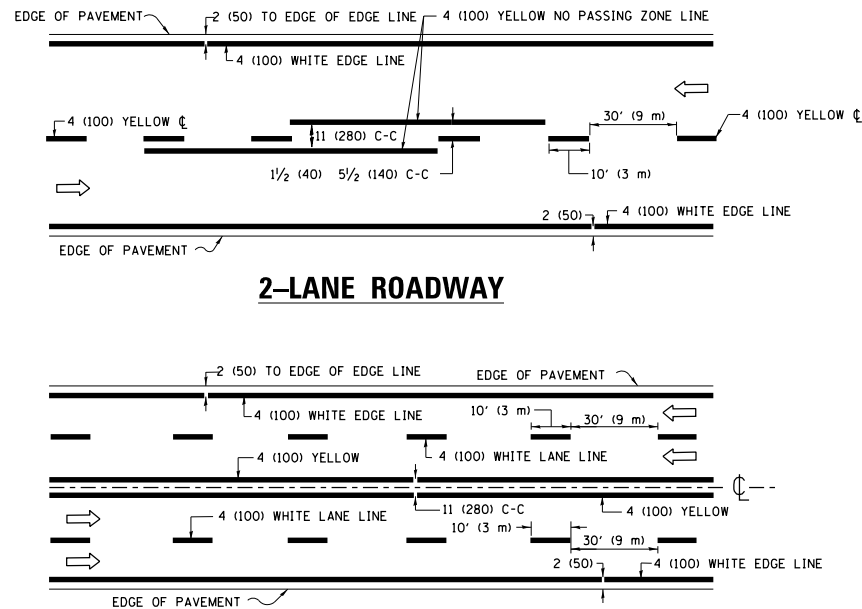
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PLOT DATE = 8/14/2020	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

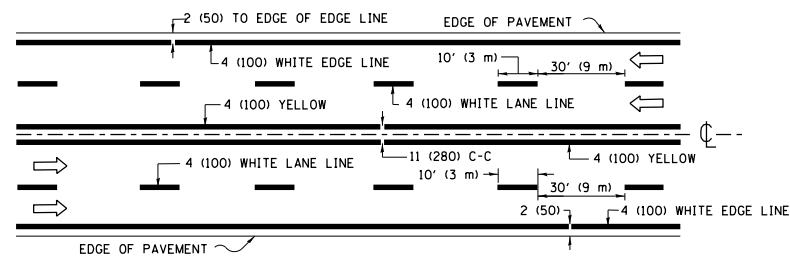
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

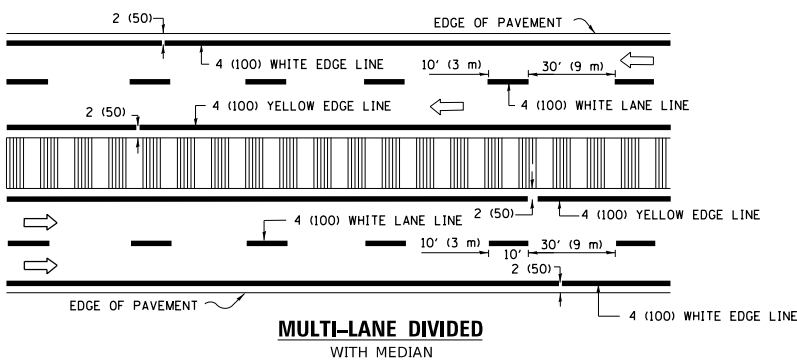
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	27
TC-11			CONTRACT NO. 62L24	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

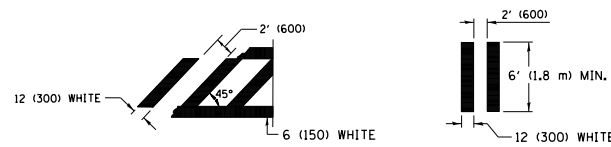
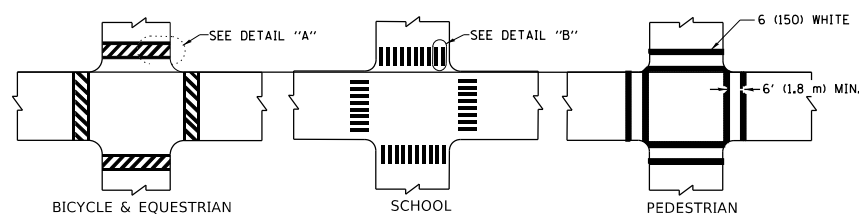


MULTI-LANE UNDIVIDED



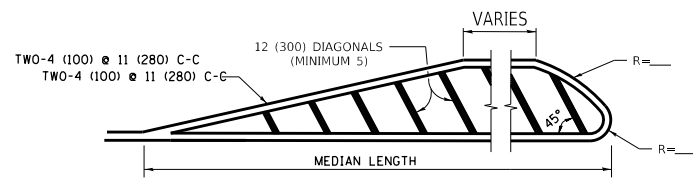
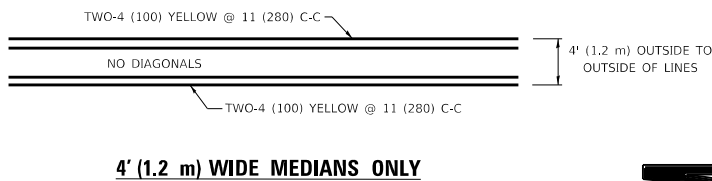
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING



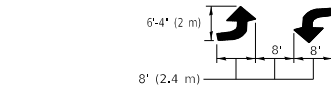
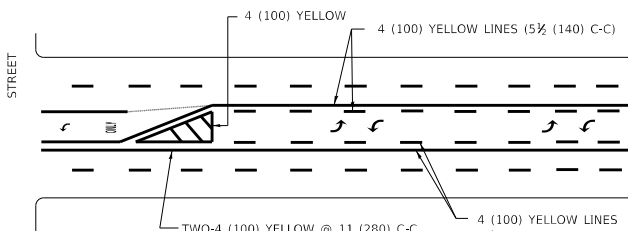
TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



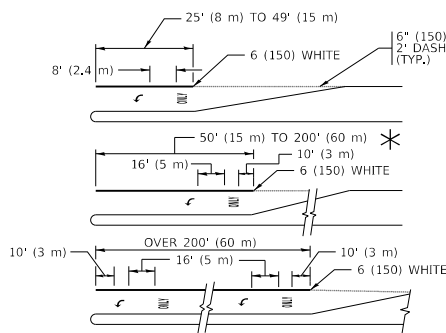
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

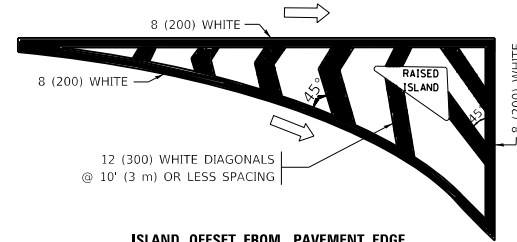
TYPICAL PAINTED MEDIAN MARKING



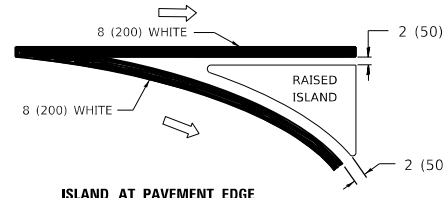
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

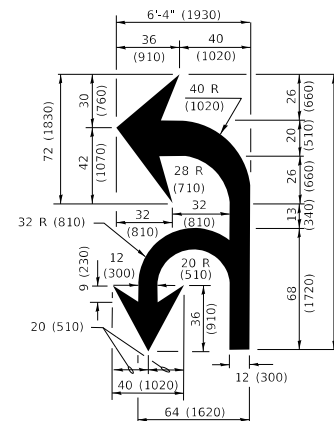
TYPICAL TURN LANE MARKING



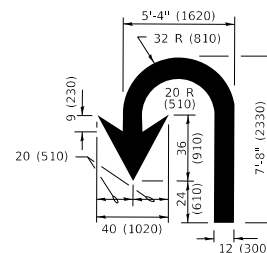
ISLAND OFFSET FROM PAVEMENT EDGE



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

LANE REDUCTION TRANSITION

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 8/14/2020	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	28
TC-13		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

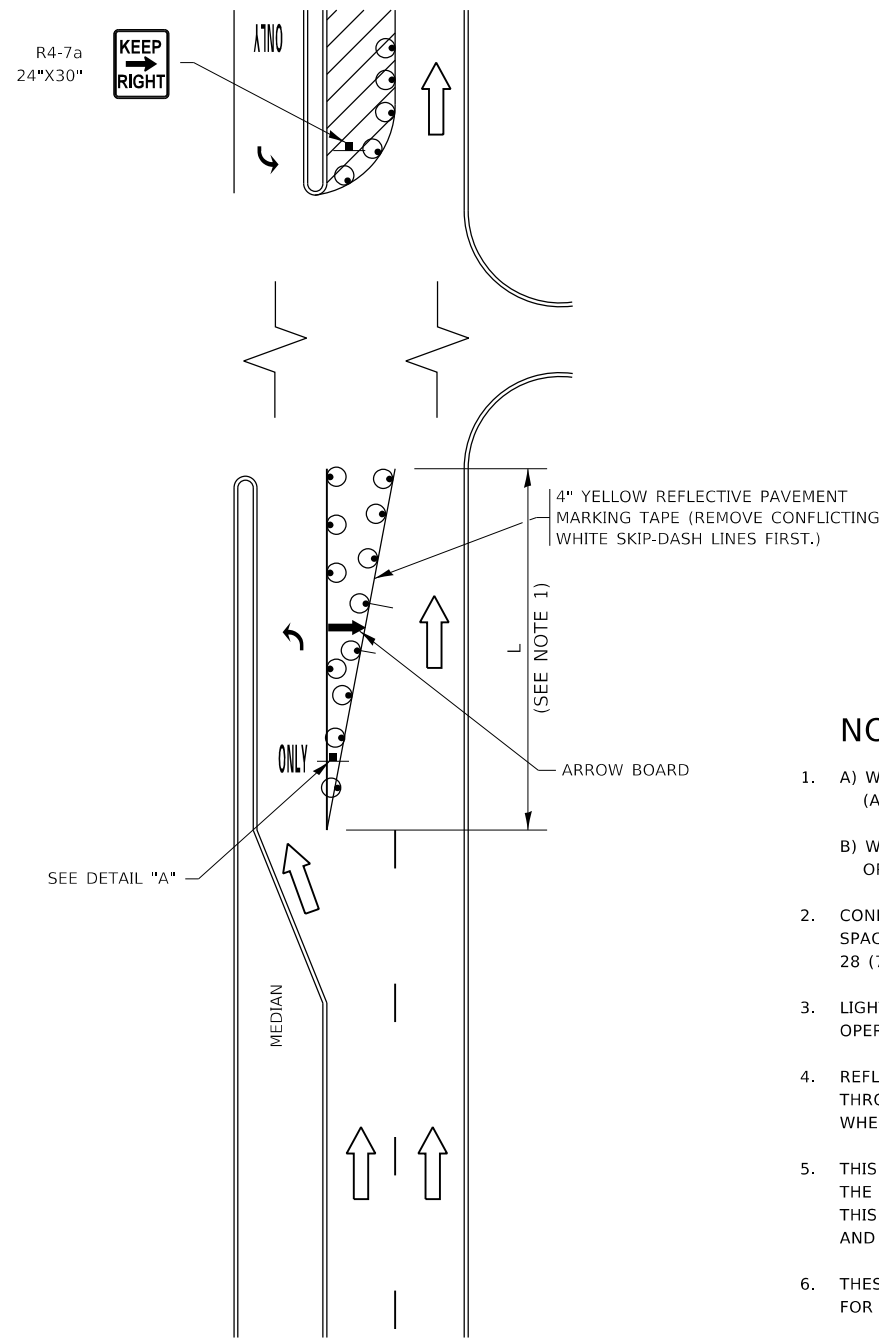


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

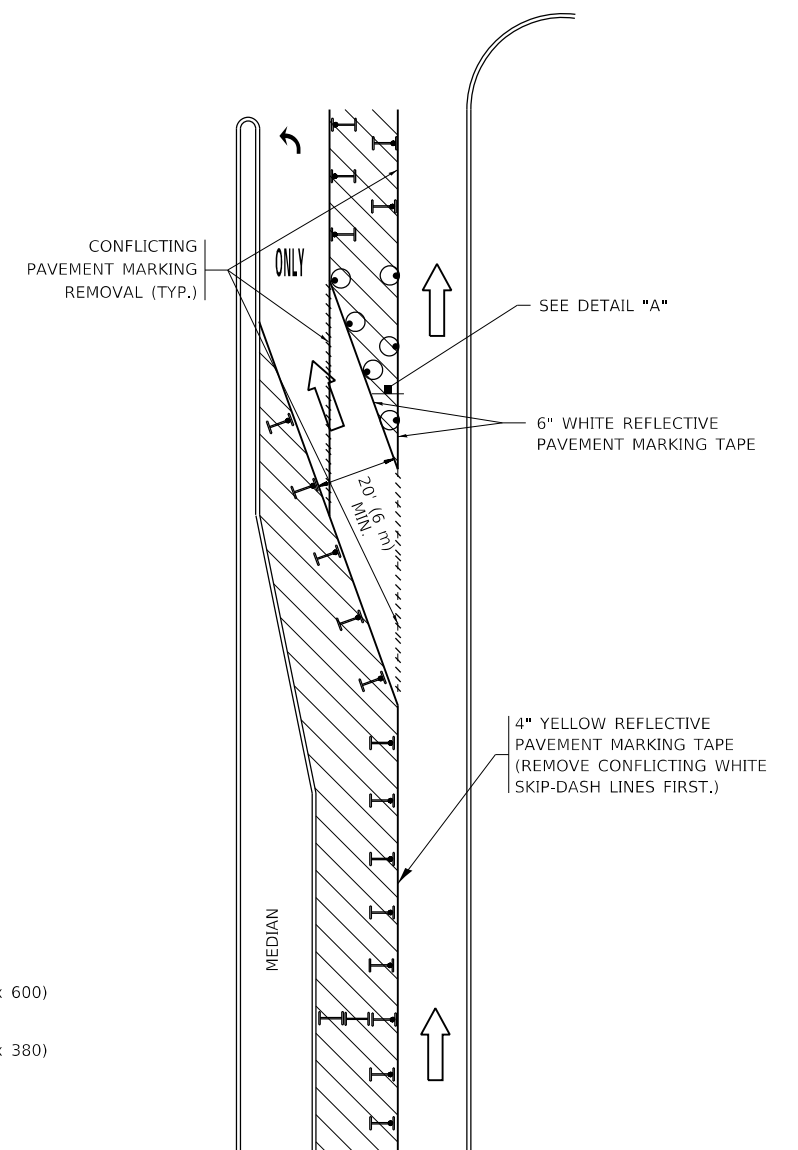


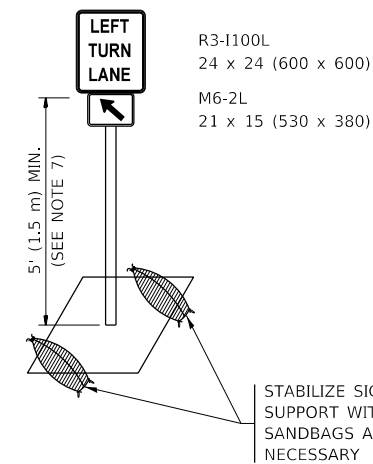
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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DRAWING: TC-14
DATE: 8/14/2020

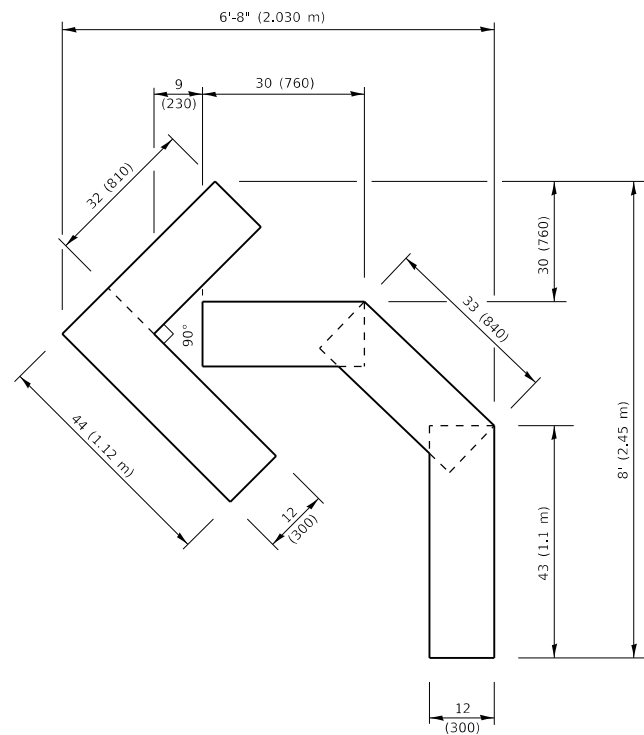
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PLOT DATE = 8/14/2020	DATE - T. RAMMACHER 01-06-00	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

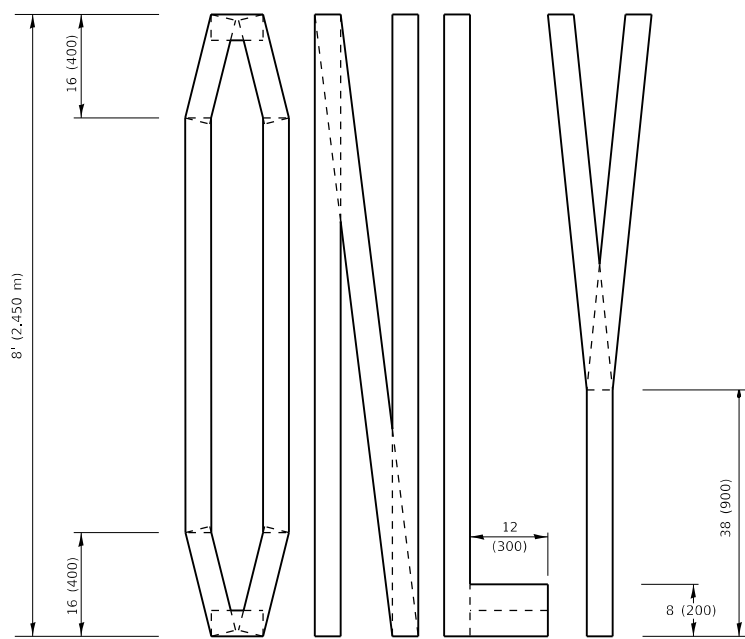
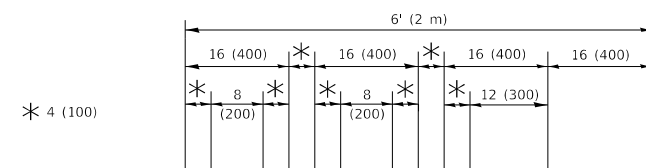
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	29
TC-14		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				



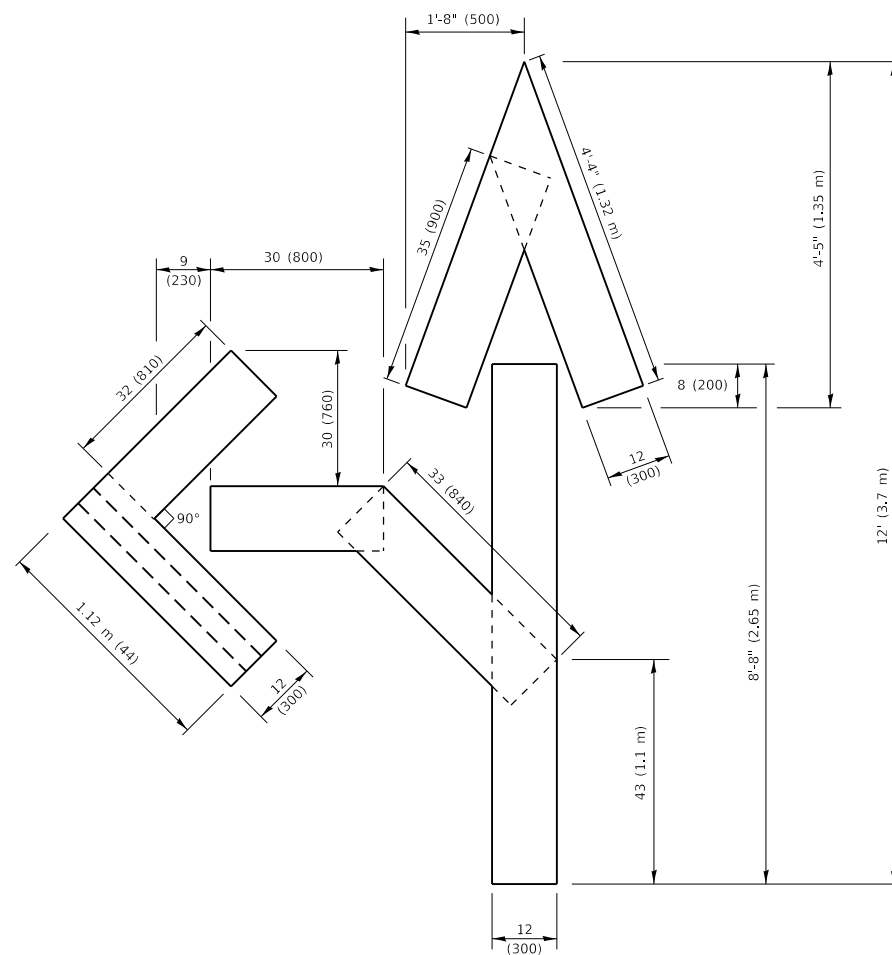
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

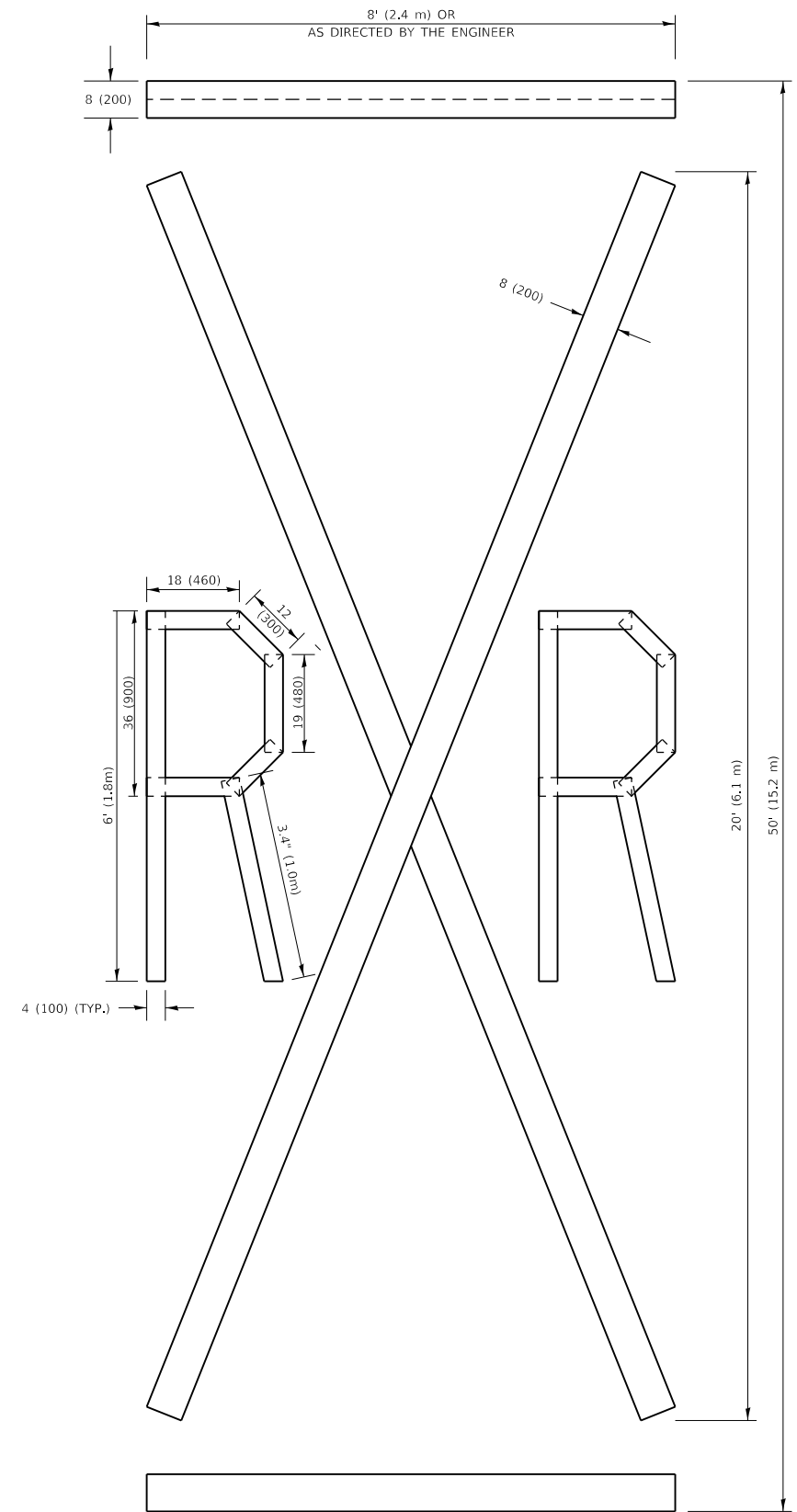


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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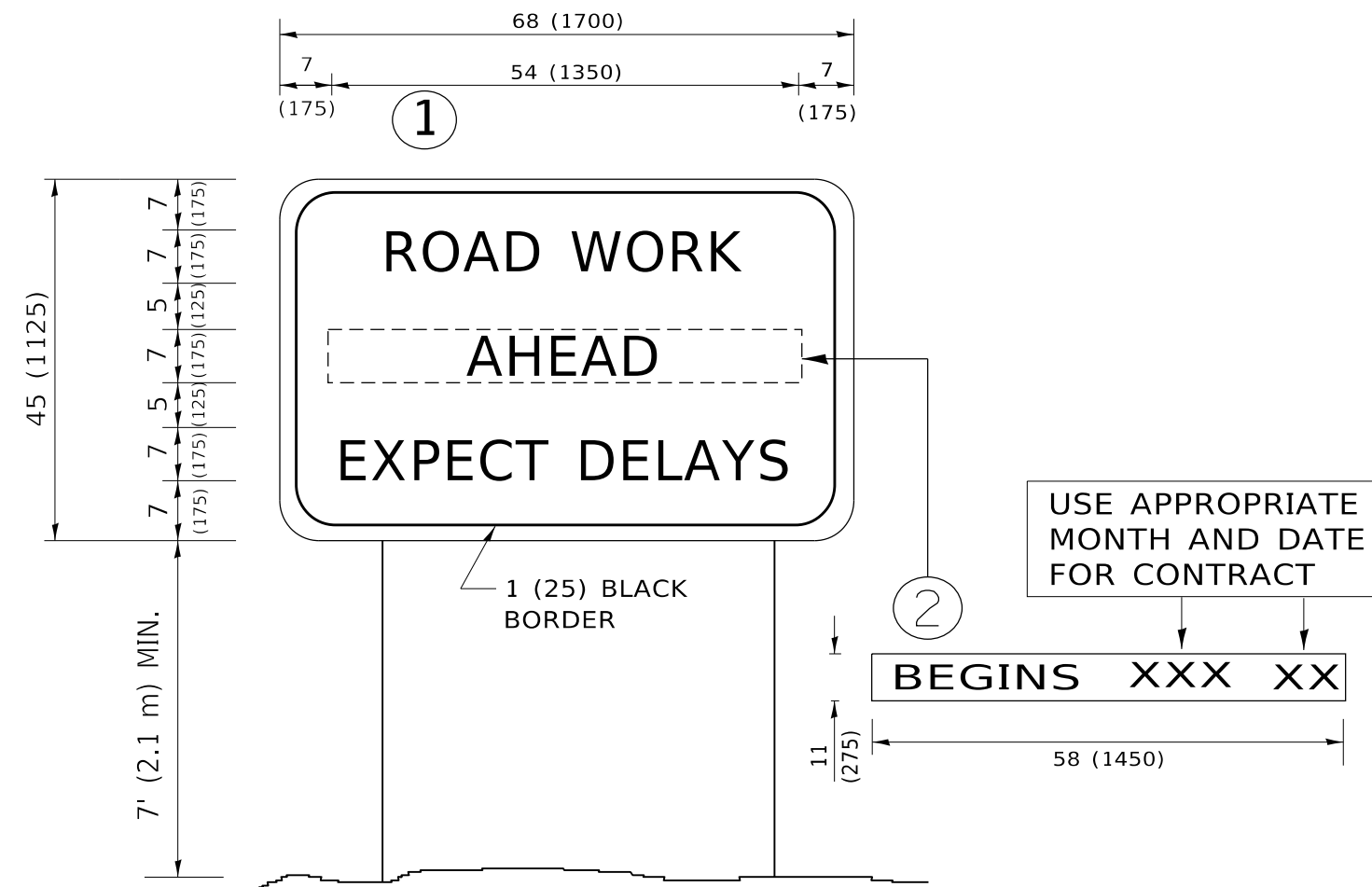
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PLOT DATE = 8/14/2020	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	30
TC-16			CONTRACT NO. 62L24	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 8/14/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

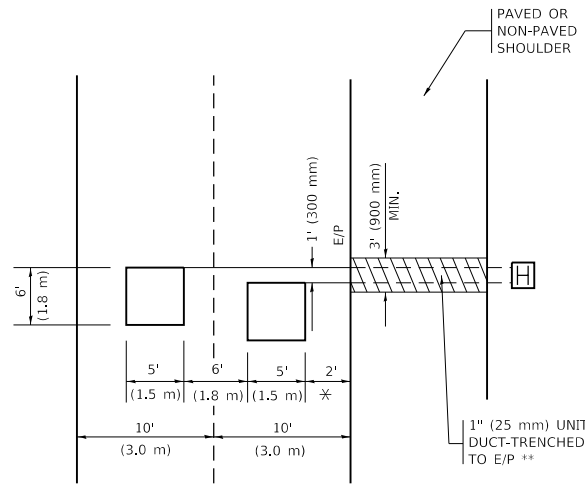
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	31
TC-22			CONTRACT NO. 62L24	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

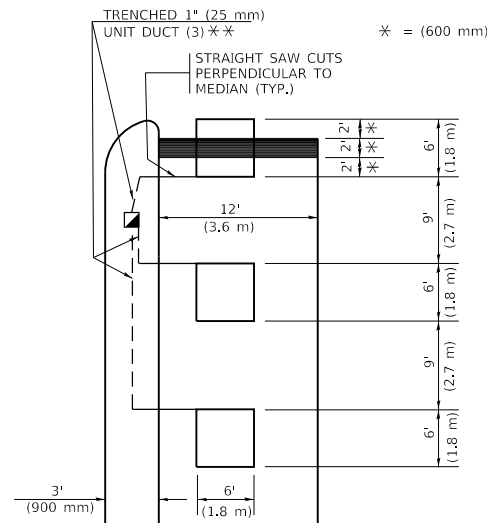


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

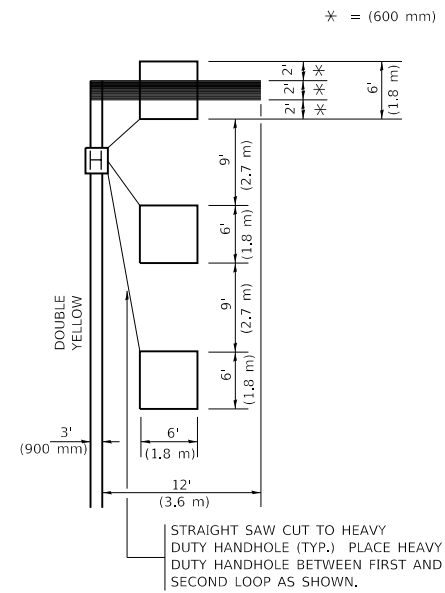


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

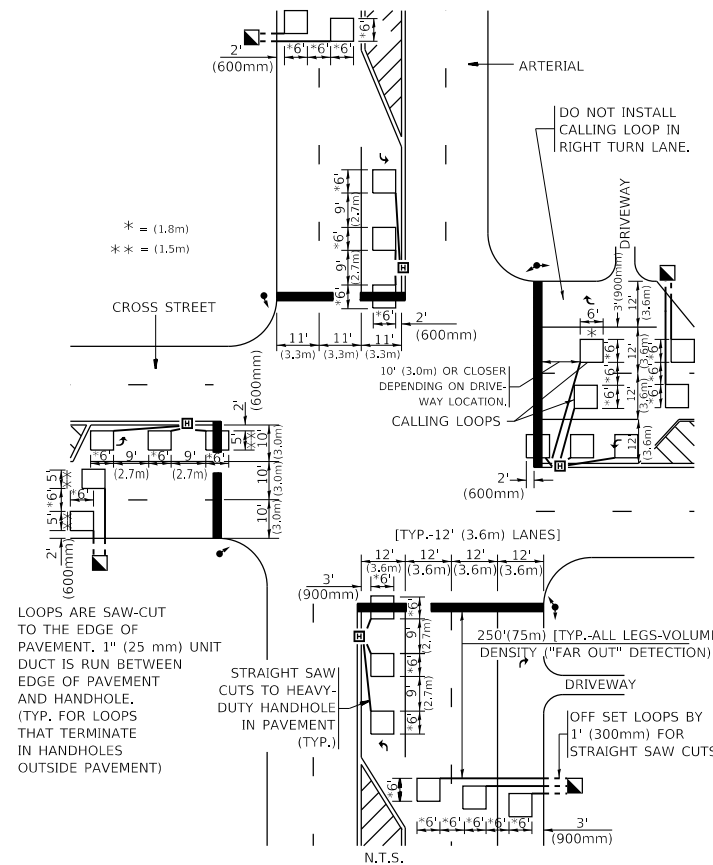
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



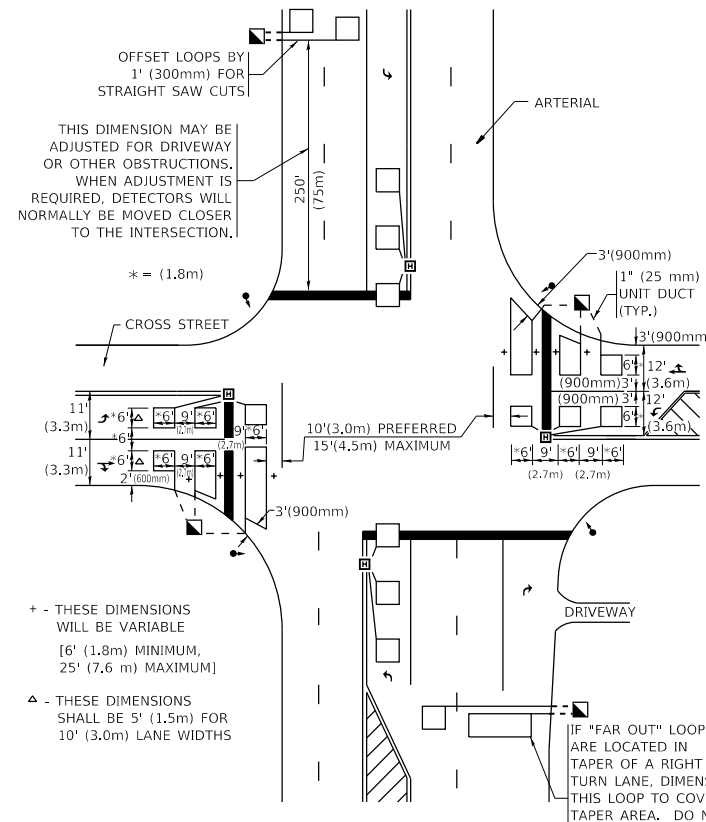
* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

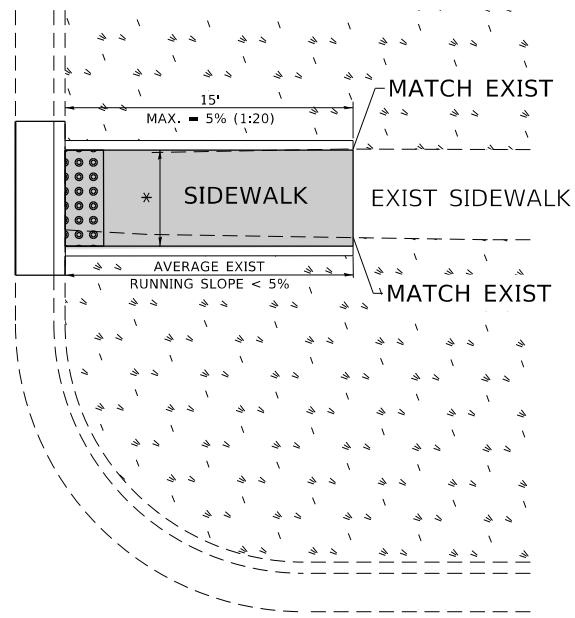
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	32
TS-07		CONTRACT NO. 62L24		
ILLINOIS FED. AID PROJECT				

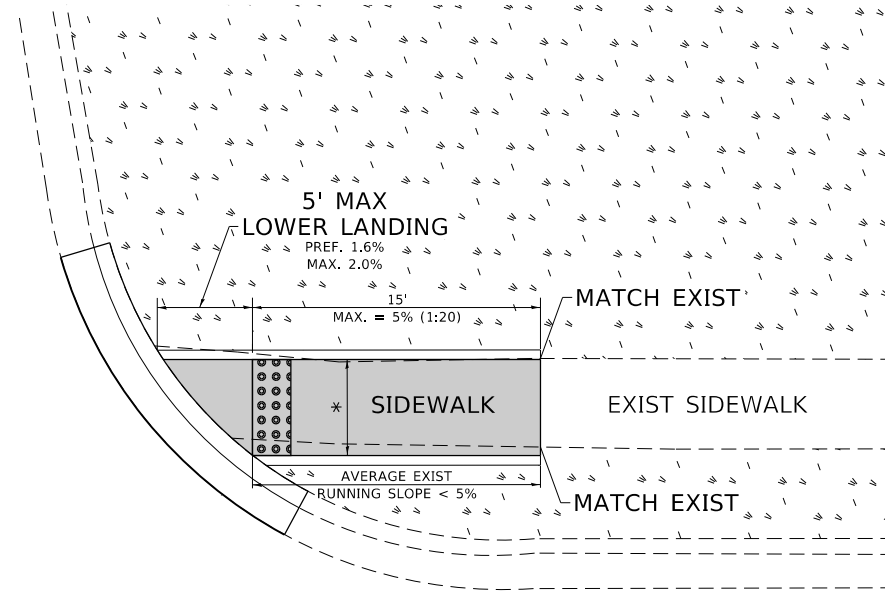
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ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

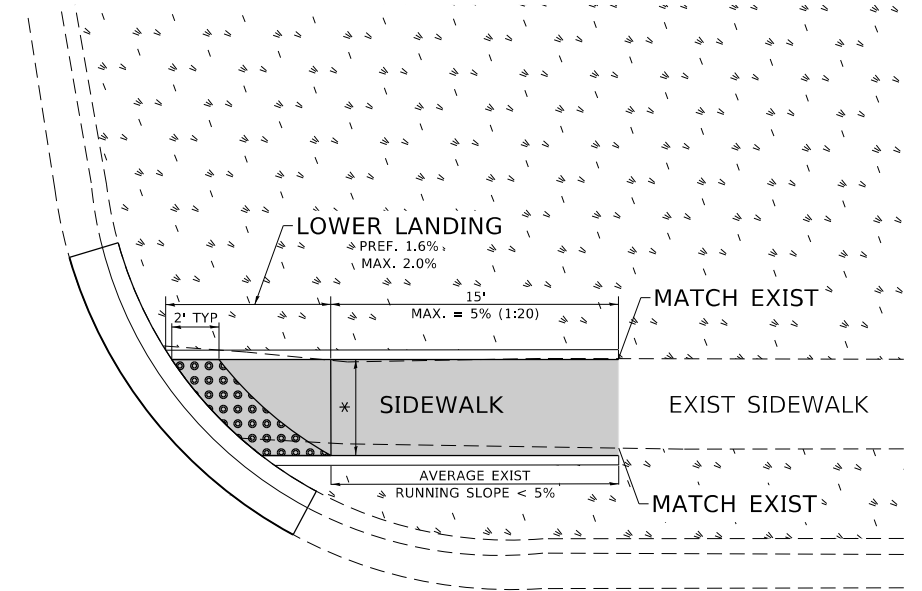
PD-01A






PD-01B



PD-01C



LEGEND

-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS
-  PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE: Model: pd01plan.dwg
PROJECT: 2020-052-RS & SW
DRAWN: R. LEDEZMA
DATE: 10/02/2019
SCALE: NONE
SHEET: 34 OF 34 SHEETS
STA. TO STA.

USER NAME = alrahamim	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/14/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

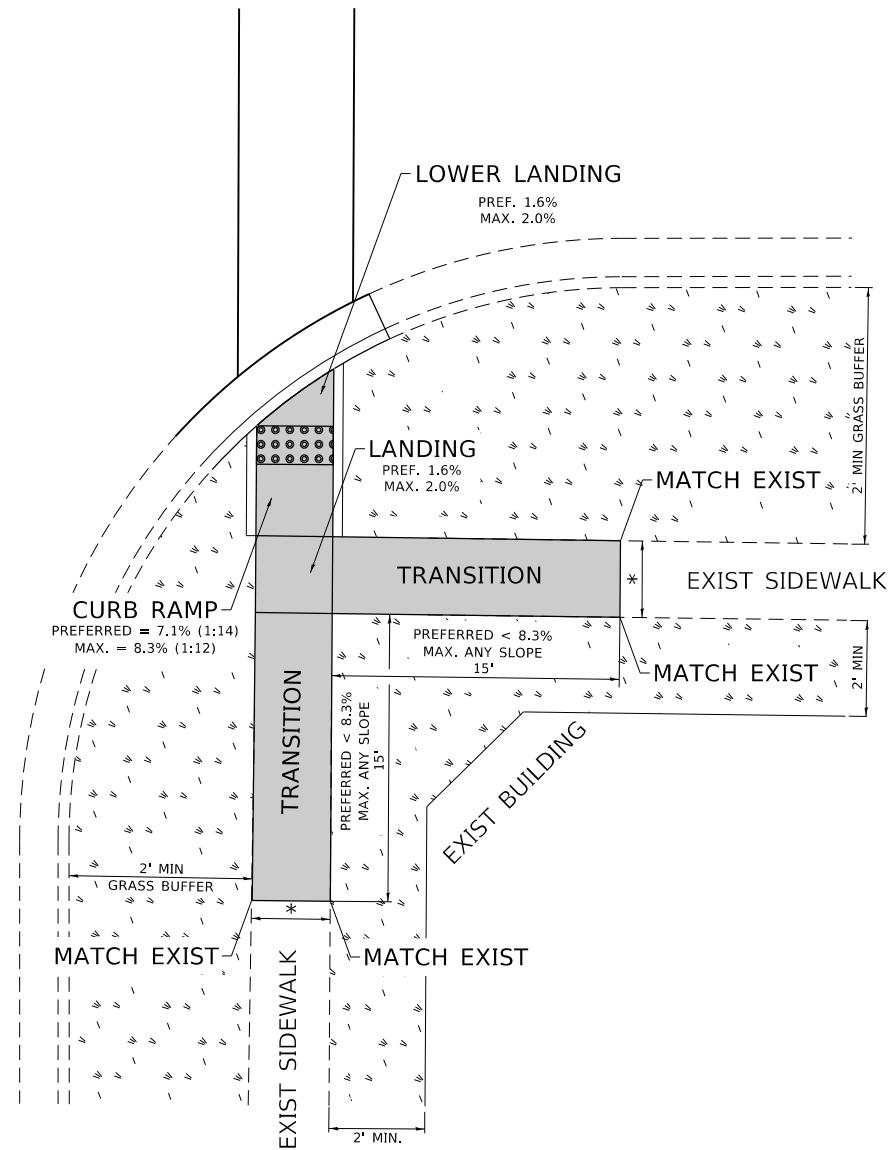
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

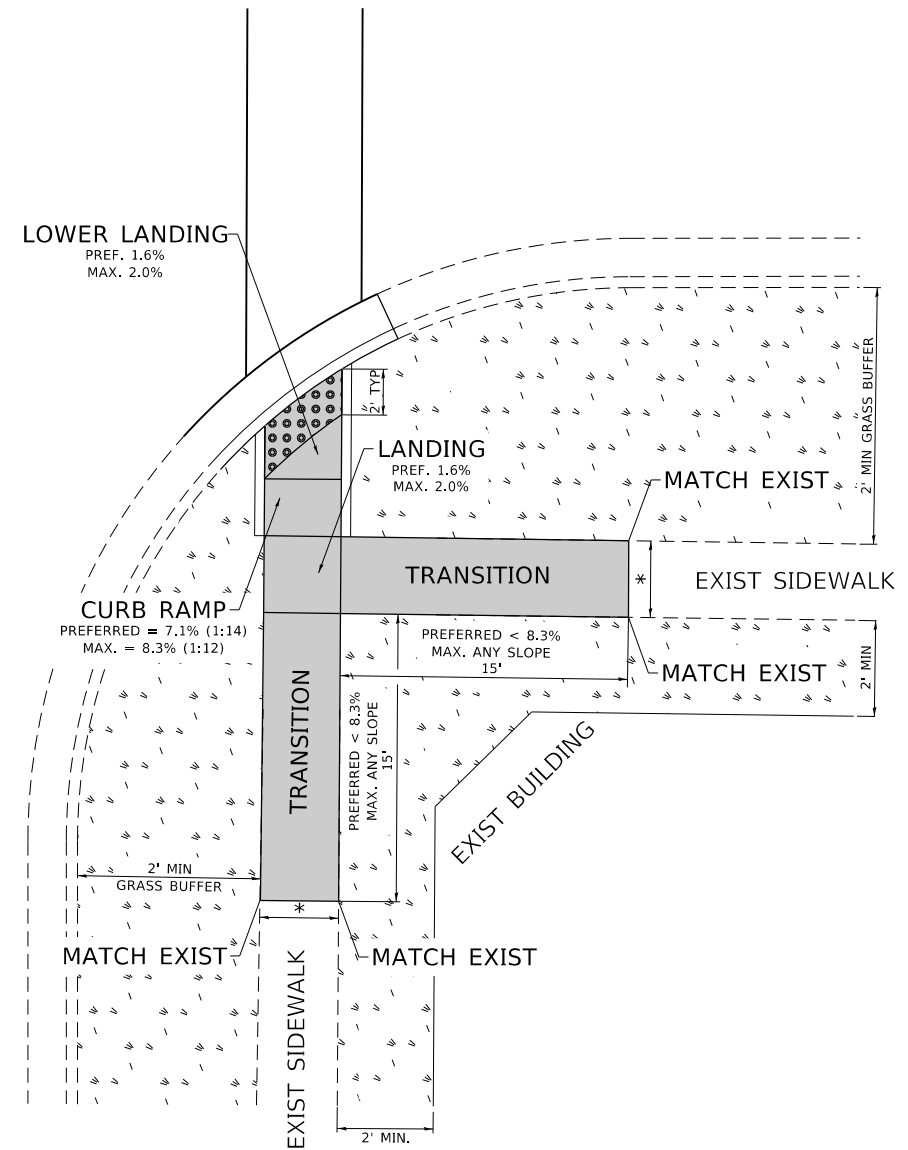
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	33
PD-01		CONTRACT NO. 62L24		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
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USER NAME = alramahimm	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/14/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2020-052-RS & SW	LAKE	34	34
PD-04		CONTRACT NO. 62L24		
		ILLINOIS FED. AID PROJECT		