

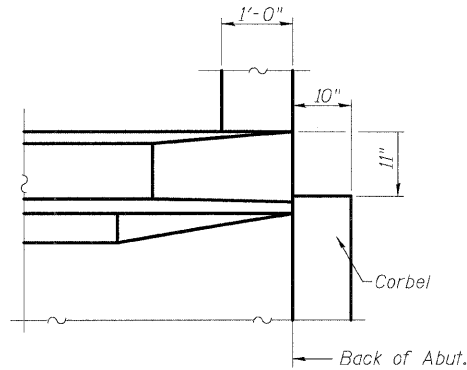
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.
F.A.P. 324	23B (1&2)F	McHENRY	17	13	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract # 60E54

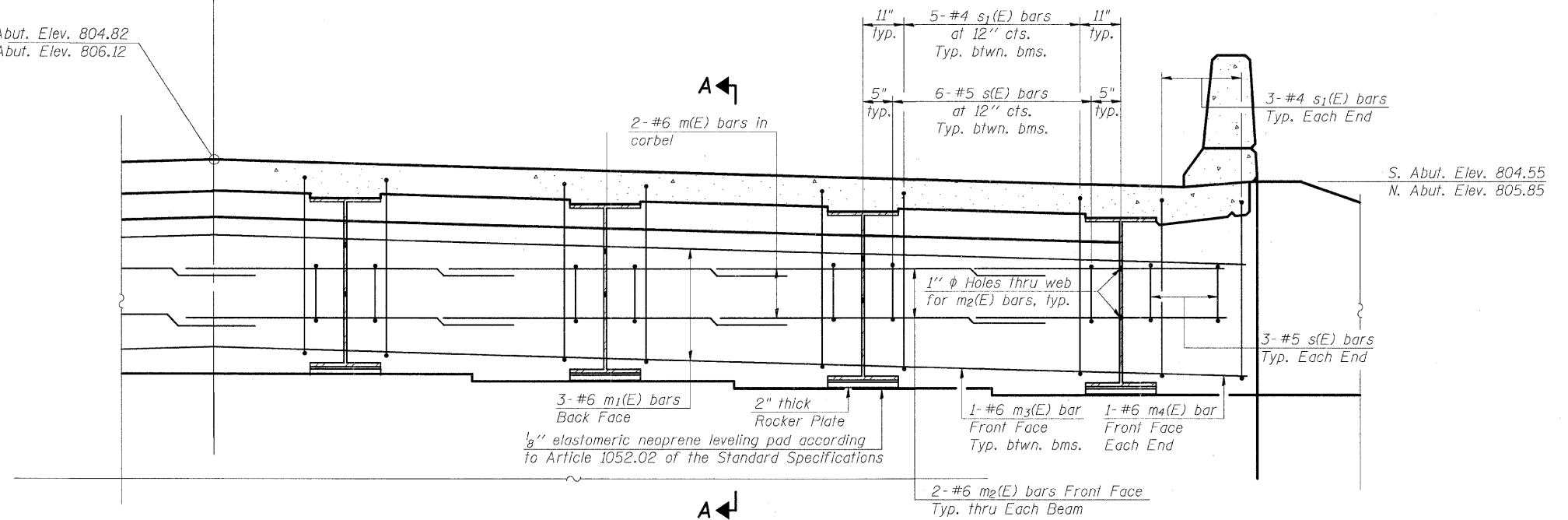
☉ IL Rte. 23
symmetrical about

S. Abut. Elev. 804.82
N. Abut. Elev. 806.12



PLAN VIEW

(Corbel Notch for Traffic Barrier)



DIAPHRAGM ELEVATION AT ABUTMENT

MIN. BAR LAP

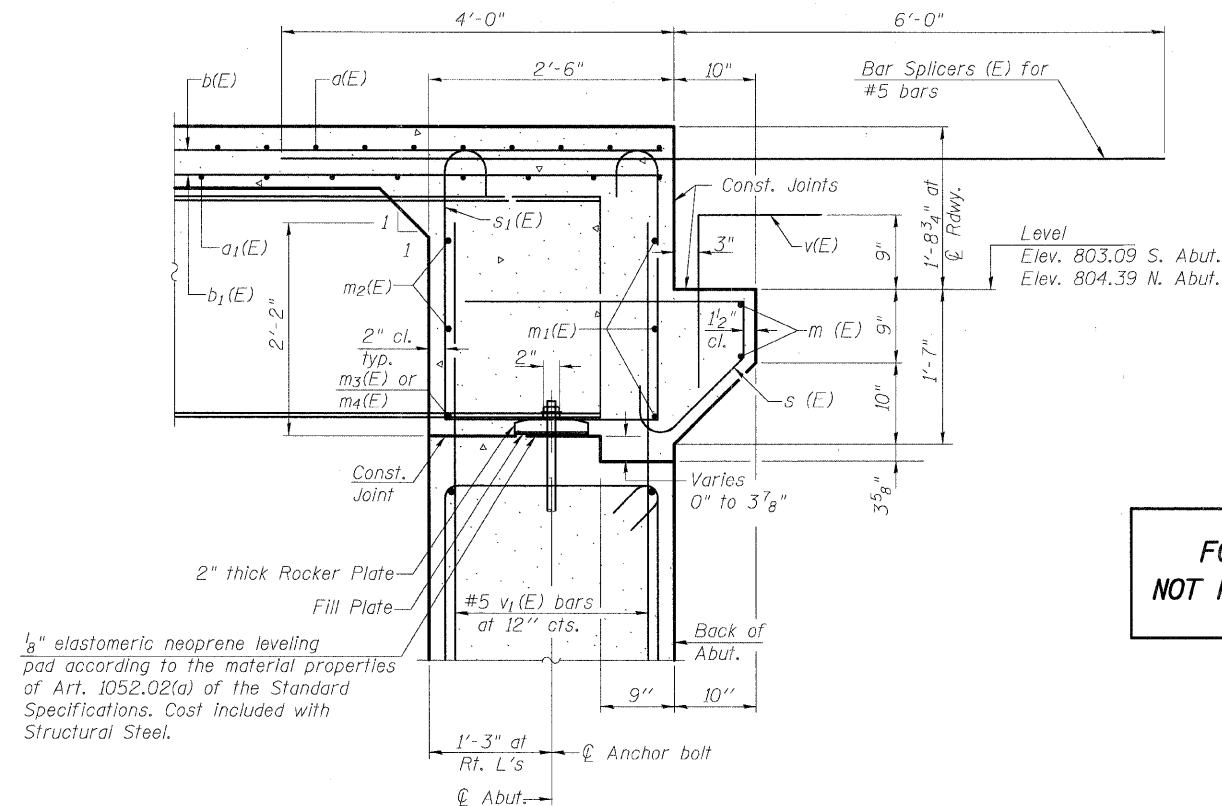
(Diaphragm)
#6 bar = 2'-7"

Notes:

Reinforcement bars in diaphragm are billed with superstructure on sheet 10 of 26.
Concrete in diaphragm is included with Concrete Superstructure on sheet 10 of 26.
For details of bars s(E) & s1(E) see sheet 10 of 26.
The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.

MIN. BAR LAP

#6 bar = 2'-9"



SECTION A-A

Dimensions at right angles to abutment, except as shown.

FOR INFORMATION ONLY
NOT PART OF THIS CONTRACT

SUPERSTRUCTURE

ILL. ROUTE 23 OVER
KISHWAUKEE RIVER
F.A.P. RT. 324 - SEC. 23B (1&2)F
McHENRY COUNTY
STATION 69+02.50
STRUCTURE NO. 056-0001



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DESIGNED	CJB
CHECKED	PWO
DRAWN	DRP
CHECKED	PJM