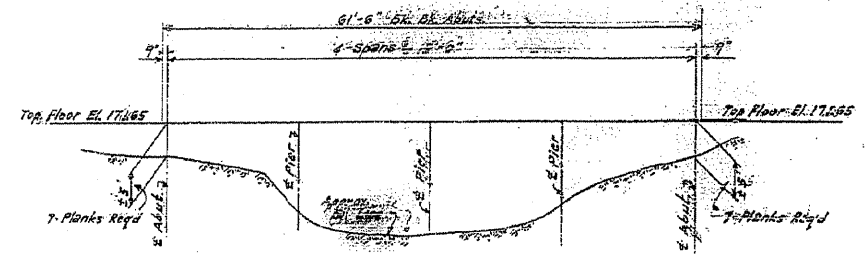


STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

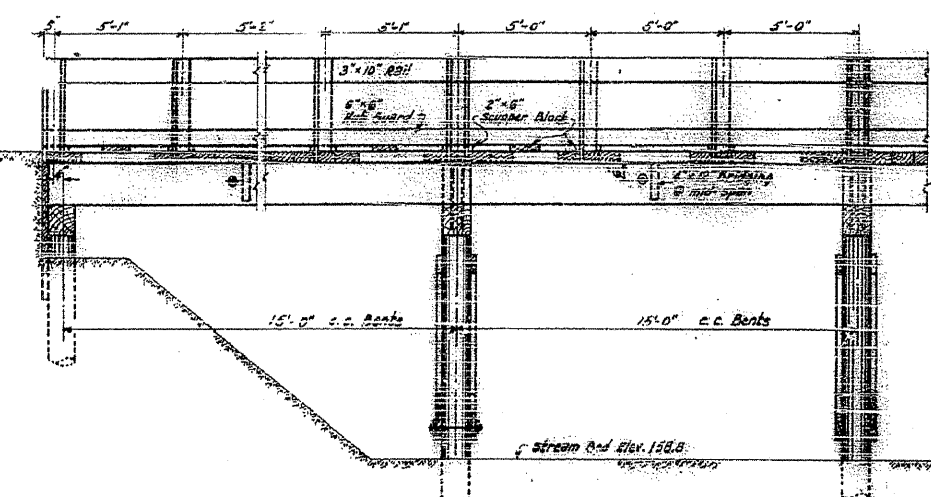
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAS 338	34BR	IRROQUOIS	49	42
STA.	TO STA.			
FED. ROAD DIST. NO. 3	ILLINOIS	FED. AID PROJECT-		

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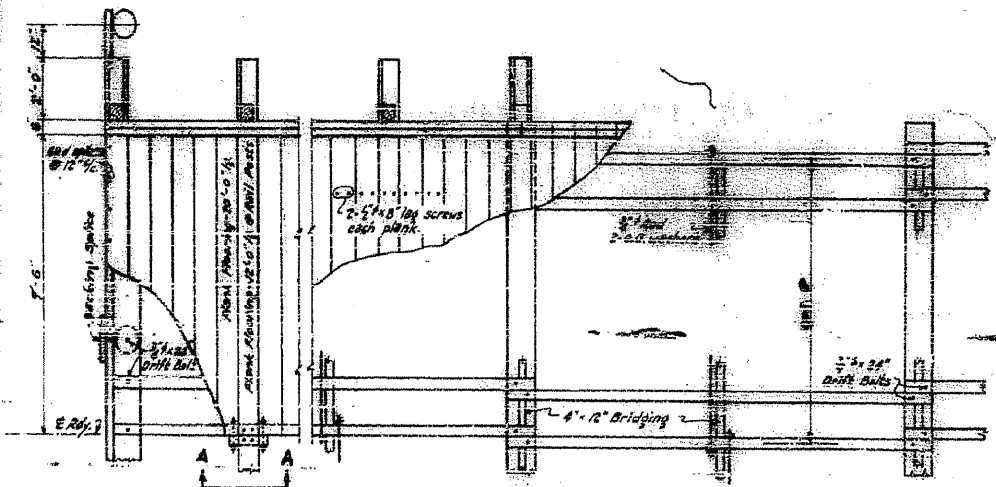
CONTRACT NO. 66610



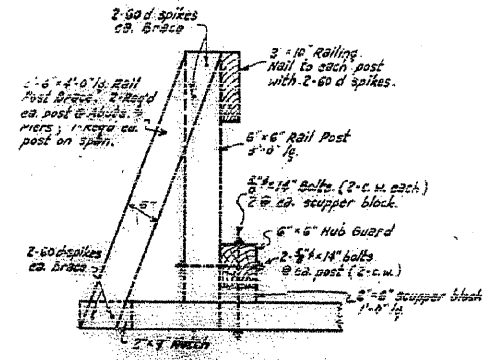
LINE DIAGRAM OF TEMP. BRIDGE
 AT STA. 2146+00



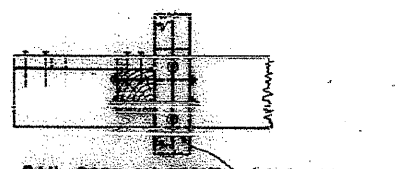
PART ELEVATION



PART PLAN

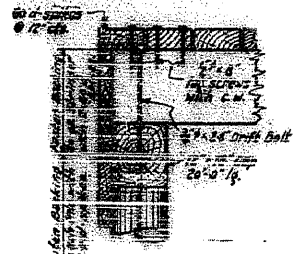


RAIL POST DETAIL

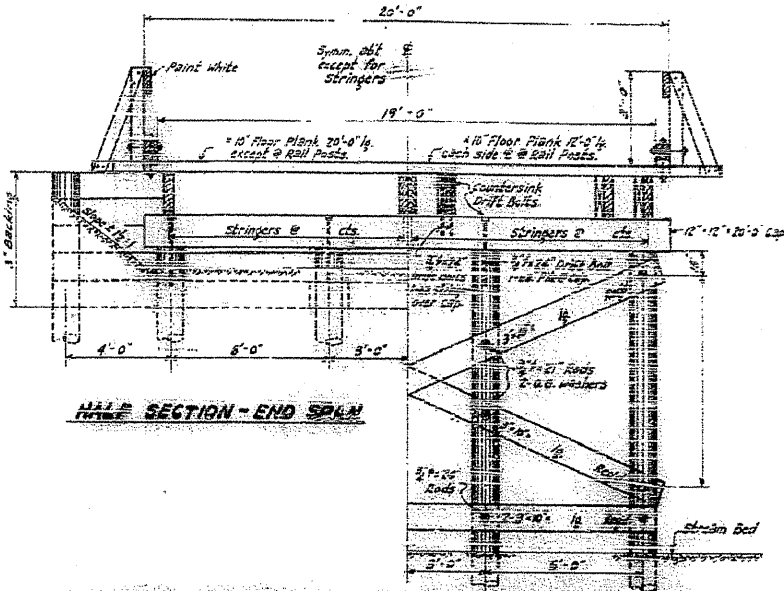


RAIL POST ON SPAN

RAIL POST AT ABUTE & PIERS



RAIL POST AT ABUTE & PIERS



HALF SECTION - END SPAN

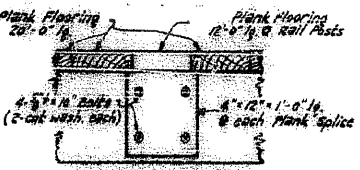
HALF SECTION - INT. SPAN

PLANK NAME	STRINGER SPACING	SPAN			
		15'-0"	15'-0"	15'-0"	15'-0"
3"	18 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"
4"	14 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"
4"	12 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"
3"	18 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"
4"	14 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"
2"	12 sps @ 15"	6'-0"	6'-0"	6'-0"	6'-0"

STRINGER SCHEDULE

GENERAL NOTES

Lumber may be native or foreign, but must be of S.P.S. new or used, and be well seasoned and free from defects.
 All rails shall be threaded 5' each end, and shall have U.S. or malleable iron washers under nuts.
 All bolts shall have cut washers under heads and nuts.
 Stringers shall be fastened to the rails with 2-60 d spikes.
 Plank flooring shall be fastened to every third stringer (beginning from the outside) with 2-60 d spikes @ 12" o.c. and cut washer under each plank. The spikes shall be turned into planks.
 Caps shall be provided for all rails.
 The bridge shall have a maximum capacity of 10 TONS and shall be driven to a maximum penetration of 18 feet from undisturbed earth to top of stringers.
 The Engineer before starting construction shall determine the position of the bridge and the location of the abutments and piers.
 The bridge shall be painted with a coat of white lead paint.
 Clearances shall conform to the following table:



SECTION A-A

DESIGNED: R. G. Sanderson
 CHECKED: M. J. White
 DRAWN: J. S. Malacki
 EXAMINED: W. E. Hansen
 PASSED: E. L. King
 DATE: Nov. 16, 1951

PROJ. P.I.-29(12)
 P.A. RT. 26 (S.B. RT. 25) SEC. 34 B-Y
 IRROQUOIS CO.
 ILLINOIS