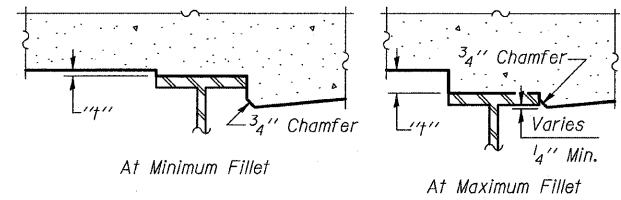


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

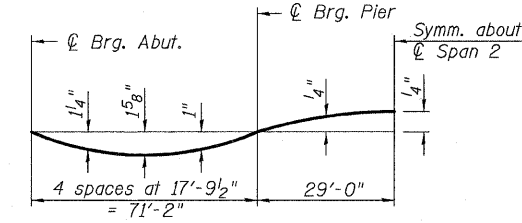
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 326	119BR	GRUNDY	68	25
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Contract #66687



To determine "f": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below and on sheet 6 of 27, minus slab thickness, equals the fillet heights "f" above top flange of beams.

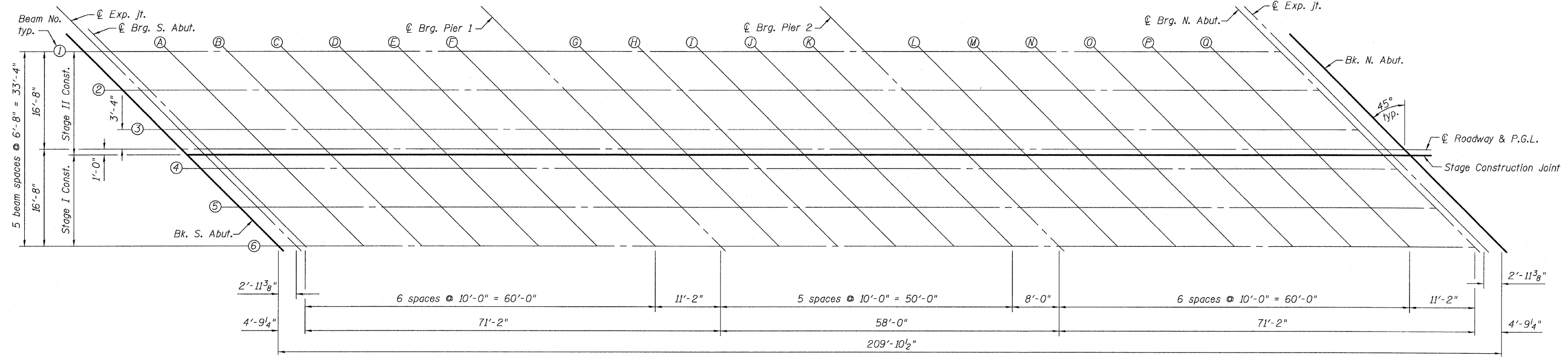
FILLET HEIGHTS



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" as shown below and on sheet 6 of 27



PLAN

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	581+44.14	-16.67	587.72	587.72
☉ Brg. S. Abut.	581+48.92	-16.67	587.73	587.73
A	581+58.92	-16.67	587.77	587.84
B	581+68.92	-16.67	587.81	587.92
C	581+78.92	-16.67	587.84	587.97
D	581+88.92	-16.67	587.86	587.99
E	581+98.92	-16.67	587.89	587.98
F	582+08.92	-16.67	587.91	587.96
☉ Brg. Pier 1	582+20.08	-16.67	587.93	587.93
G	582+30.08	-16.67	587.94	587.92
H	582+40.08	-16.67	587.95	587.93
I	582+50.08	-16.67	587.96	587.93
J	582+60.08	-16.67	587.96	587.94
K	582+70.08	-16.67	587.96	587.94
☉ Brg. Pier 2	582+78.08	-16.67	587.96	587.96
L	582+88.08	-16.67	587.95	587.99
M	582+98.08	-16.67	587.94	588.03
N	583+08.08	-16.67	587.93	588.05
O	583+18.08	-16.67	587.91	588.05
P	583+28.08	-16.67	587.89	588.01
Q	583+38.08	-16.67	587.87	587.94
☉ Brg. N. Abut.	583+49.25	-16.67	587.84	587.84
Bk. N. Abut.	583+54.02	-16.67	587.83	587.83

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	581+50.81	-10.00	587.87	587.87
☉ Brg. S. Abut.	581+55.58	-10.00	587.89	587.89
A	581+65.58	-10.00	587.92	587.99
B	581+75.58	-10.00	587.95	588.07
C	581+85.58	-10.00	587.98	588.12
D	581+95.58	-10.00	588.01	588.14
E	582+05.58	-10.00	588.03	588.12
F	582+15.58	-10.00	588.05	588.10
☉ Brg. Pier 1	582+26.75	-10.00	588.06	588.06
G	582+36.75	-10.00	588.08	588.06
H	582+46.75	-10.00	588.08	588.06
I	582+56.75	-10.00	588.09	588.06
J	582+66.75	-10.00	588.09	588.07
K	582+76.75	-10.00	588.09	588.07
☉ Brg. Pier 2	582+84.75	-10.00	588.08	588.08
L	582+94.75	-10.00	588.07	588.12
M	583+04.75	-10.00	588.06	588.15
N	583+14.75	-10.00	588.05	588.17
O	583+24.75	-10.00	588.03	588.16
P	583+34.75	-10.00	588.01	588.12
Q	583+44.75	-10.00	587.98	588.05
☉ Brg. N. Abut.	583+55.92	-10.00	587.95	587.95
Bk. N. Abut.	583+60.69	-10.00	587.94	587.94

DESIGNED	DGL
CHECKED	MJP
DRAWN	MJP
CHECKED	DGL

TOP OF SLAB ELEVATIONS
IL 47 OVER JOHNNY RUN
FAP ROUTE 326 - SECTION 119BR
GRUNDY COUNTY
STATION 582+65.75
STRUCTURE NO. 032-0112

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