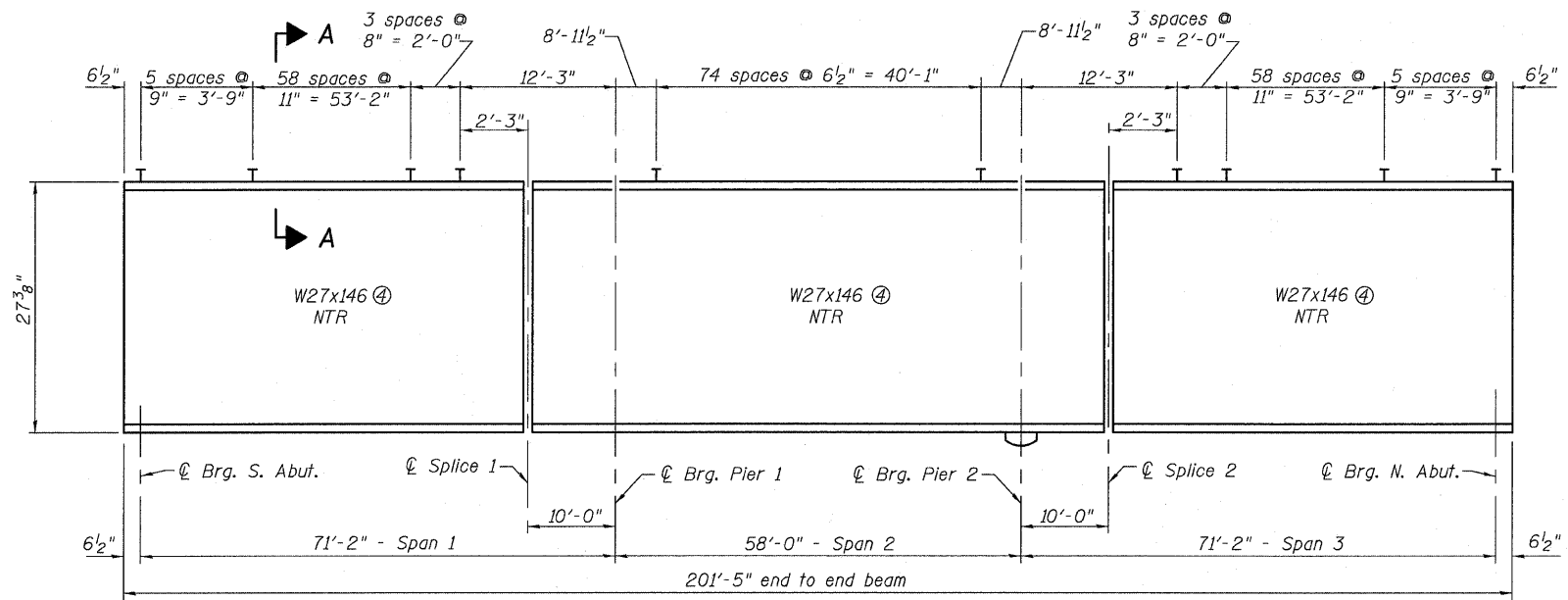


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

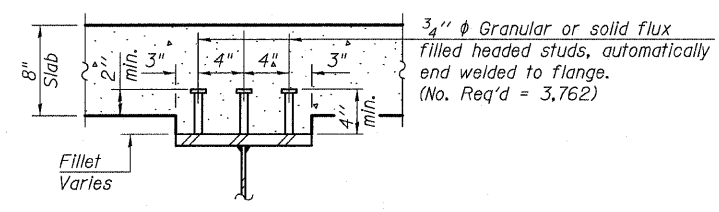
ROUTE NO.	SECTION	COUNTY	SHEET	SET	SHEET NO. 14 27 SHEETS
FAP 326	119BR	GRUNDY	68	34	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		Contract #66687



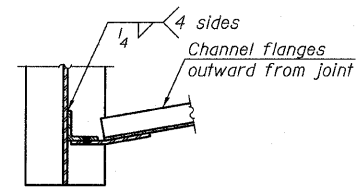
BEAM ELEVATION

TOP OF BEAM ELEVATIONS *						
	Beam #1	Beam #2	Beam #3	Beam #4	Beam #5	Beam #6
℄ Brg. S. Abut.	587.03	587.18	587.31	587.33	587.25	587.14
℄ Splice 1	587.17	587.31	587.43	587.44	587.34	587.22
℄ Brg. Pier 1	587.18	587.32	587.43	587.43	587.33	587.21
℄ Brg. Pier 2	587.21	587.33	587.43	587.43	587.31	587.18
℄ Splice 2	587.21	587.34	587.43	587.43	587.31	587.17
℄ Brg. N. Abut.	587.13	587.24	587.33	587.30	587.18	587.02

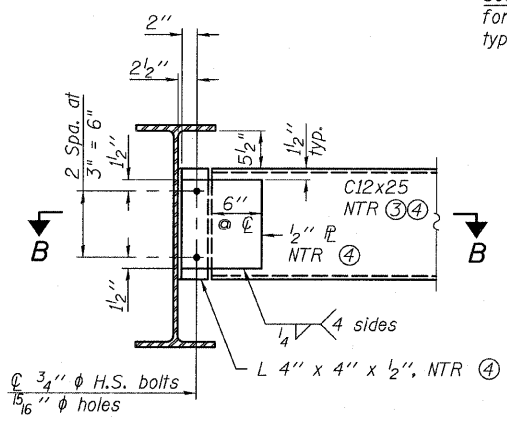
* For Fabrication Only



SECTION A-A

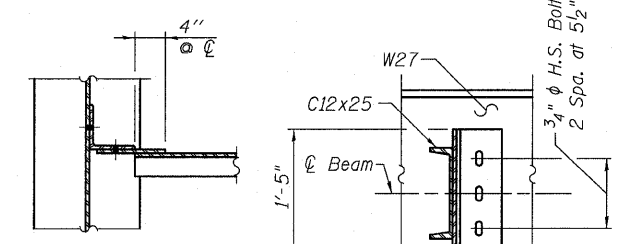


SECTION B-B

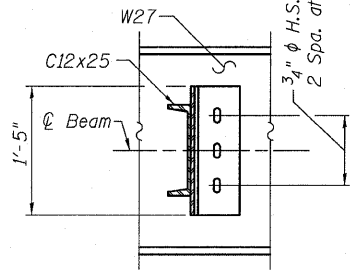


END DIAPHRAGM D1

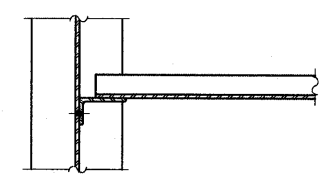
(8 Req'd)



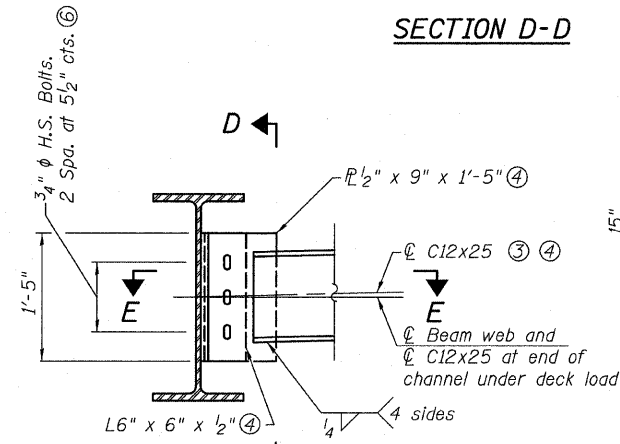
SECTION E-E



SECTION D-D

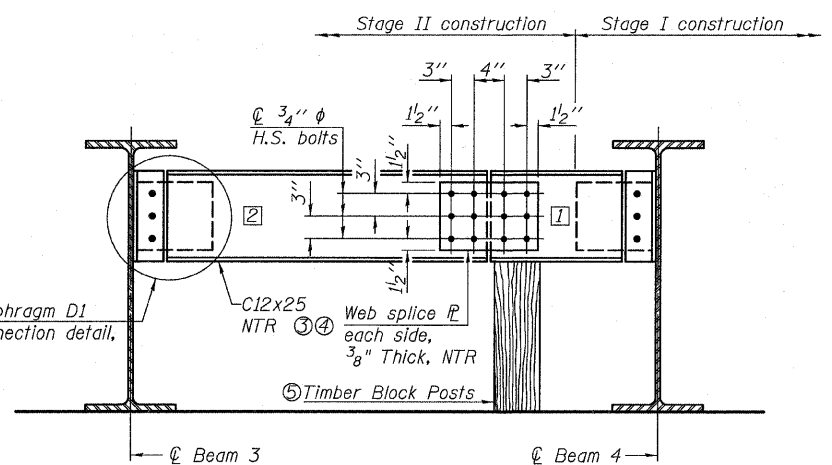


SECTION C-C



INTERIOR DIAPHRAGM D3

(11 Req'd)



END DIAPHRAGM D2

(2 Req'd)

END DIAPHRAGM STAGE CONSTRUCTION SEQUENCE

- 1.) Order Diaphragm in two sections.
- 2.) Attach section 1 of Diaphragm to Beam
- 3.) Place Timber Block Posts between section 1 of diaphragm and abutment bearing section.
- 4.) Attach section 2 of diaphragm to both Beam 3 and section 1 of diaphragm during Stage II Construction with splice plates.
- 5.) Remove Timber Block Posts.

Notes:

- 1.) Use two hardened washers per bolt for all oversized holes in diaphragm connections.
- 2.) Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.
- 3.) Alternate C12x30 channels are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section. The alternate, if utilized, shall be provided at no additional cost to the Department.
- 4.) AASHTO M 270 Grade 50W Steel.
- 5.) Cost of Timber Block Posts is included with Furnishing and Erecting Structural Steel.
- 6.) Provide 1/16 inch x 1 7/8 inch vertical slotted holes in the Beam 3 connection only. Beam 4 connection shall be similar to the typical interior diaphragm connection for diaphragm D. Slotted holes shall be provided in the plate at the plate to angle connection and in the angle at the angle to web connection. Standard 1/16 inch diameter holes shall be provided in the web of the beam and in the leg of the angle used to connect the 1/2 inch thick NTR to the angle. Use 5/16 inch structural plate washers placed over all slotted holes in diaphragm connection. The bolts for the slotted holes shall be finger tight until the second stage pour is complete and then fully tightened. The slots shall be positioned so bolts start at one end with no concrete load and finish near the opposite end under deck load, allowing maximum displacement without laterally stressing the main members.

**BEAM & FRAMING DETAILS
IL 47 OVER JOHNNY RUN
FAP ROUTE 326 - SECTION 119BR
GRUNDY COUNTY
STATION 582+65.75
STRUCTURE NO. 032-0112**

DESIGNED	MJP
CHECKED	JGS
DRAWN	MJP
CHECKED	JGS

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Design Firm License No. 184.001115