

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
FAP 845		FRANKLIN	5	2
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

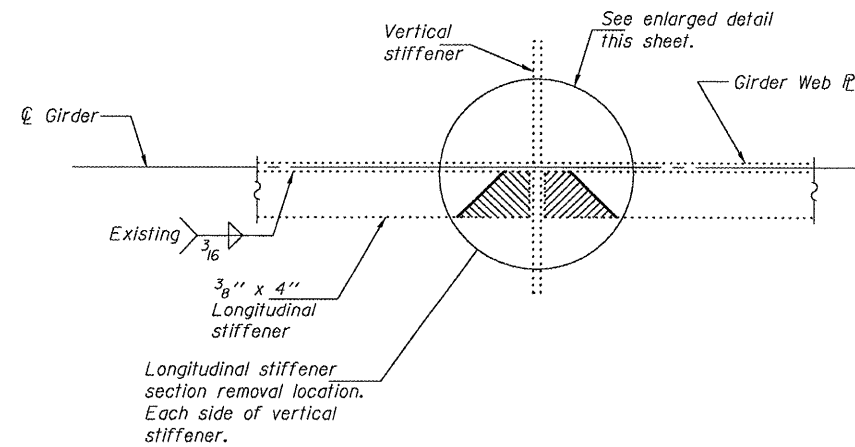
SHEET NO. 2

2 SHEETS

Contract Number: -----

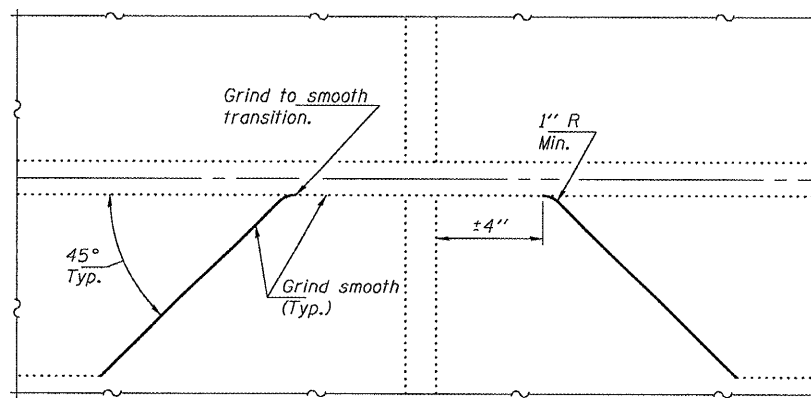
GIRDER #	SPLICE A				SPLICE B				SPLICE C				SPLICE D				SPLICE E				SPLICE F			
	TOP		BOTTOM		TOP		BOTTOM		TOP		BOTTOM		TOP		BOTTOM		TOP		BOTTOM		TOP		BOTTOM	
	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT	LEFT	RIGHT
* 1	6	0	1	4	1	4	6	0	4	1	0	6	1	4	6	0	4	1	0	6	1	4	6	0
* 2	4	1	0	6	1	4	6	0	4	1	0	6	1	4	6	0	4	1	0	6	0	6	4	1

* Looking North



REPAIR DETAIL

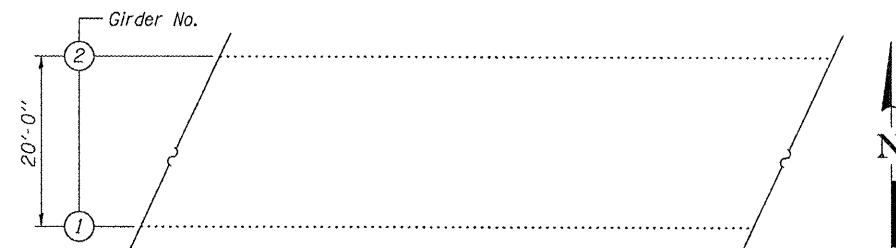
Hatched area indicates section removal.
(Showing 2 locations)



Procedure for Repair Detail:

1. Cut existing longitudinal stiffener ±4" from face of vertical stiffener and along web as shown, with a 1" R (Min) at Web. The minimum distance from cut to face of web shall be the larger of 1/4" or web to plate weld size, with removal of remaining material by grinding as described below. The cut shall be made parallel to the web without angling the cut towards the web. Equipment and method of cutting shall be approved by the Engineer. Any method of removal to be used shall ensure that no damage is done to the existing web, vertical stiffener or welds connecting these elements. Cutting shall be done in a manner such that the paint on the opposite face of the web is not damaged. If damage occurs, the damaged area shall be repainted at the contractor's expense and procedures shall be modified to prevent damage at subsequent removal locations.
2. Remove material between cut and web by grinding and grind smooth at web surface and cut end of stiffener. Web R surfaces and cut end of stiffener shall have a roughness average (Ra) of 250 μ in. or less. Grinding equipment shall be approved by the Engineer. The grinding operation should not gouge the girder web R.
3. The web surface at the modification shall be inspected using dye penetrant or magnetic particle (MT) methods. Any cracks found shall be identified and reported to the Bureau of Bridges and Structures for further disposition.
4. The exposed steel surfaces shall be cleaned and painted using an aluminum epoxy mastic primer according to Article 506.05 of the Standard Specifications.

Each 4" stiffener removal area is to be considered as one retrofit. Accepted above referenced work will be paid for at the contract unit price each for Stiffener Intersection Modification, which price shall include all materials, equipment, labor, cleaning, testing and painting.



GIRDER ORIENTATION

DESIGNED	ATH
CHECKED	AJB
DRAWN	Steffen
CHECKED	ATH AJB

OCTOBER 2, 2008
EXAMINED *Carl Proyer*
ENGINEER OF STRUCTURAL SERVICES
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

SIMD/REPS 03-02-2006

STIFFENER MODIFICATION DETAILS

FAP 845 (IL 154)
FRANKLIN COUNTY
SN 028-0051